



**GENERAL NOTES FOR CONTRACTOR**

1. THE FOLLOWING NOTES ARE PROVIDED TO GIVE DIRECTIONS TO THE CONTRACTOR BY THE ENGINEER OF THE PLANS. THE TOWN ENGINEER'S SIGNATURE ON THESE PLANS DOES NOT CONSTITUTE APPROVAL OF ANY OF THESE NOTES AND THE TOWN WILL NOT BE HELD RESPONSIBLE FOR THEIR ENFORCEMENT.
2. CONTRACTOR AGREES THAT HE/SHE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOBSITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING: SAFETY OF ALL PERSONS AND PROPERTY, AND THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT.
3. THE CONTRACTOR SHALL BE RESPONSIBLE TO INSURE THAT ALL SLOPES, STREETS, UTILITIES, AND STORM SEWERS ARE BUILT IN ACCORDANCE WITH THESE PLANS. IF THERE IS ANY QUESTION REGARDING THESE PLANS OR FIELD STAKES, THE CONTRACTOR SHALL REQUEST AN INTERPRETATION BEFORE DOING ANY WORK BY CALLING THE ENGINEER OF RECORD AT 303-228-2300. THE CONTRACTOR SHALL ALSO TAKE THE NECESSARY STEPS TO PROTECT THE PROJECT AND ADJACENT PROPERTY FROM ANY EROSION AND SILTATION THAT RESULT FROM HIS OPERATIONS BY APPROPRIATE MEANS (SAND BAGS, HAY BALES, TEMPORARY DESILTING BASINS, DIKES, SHORING, ETC.) UNTIL SUCH TIME THAT THE PROJECT IS COMPLETED AND ACCEPTED FOR MAINTENANCE BY WHATEVER OTHER, AGENCY, OR ASSOCIATION IS TO BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE.
4. EXCEPT AS NOTED HEREON ALL UTILITY SERVICES WITHIN THIS DEVELOPMENT ARE UNDERGROUND INSTALLATIONS. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES PRIOR TO STARTING WORK NEAR THEIR FACILITIES, AND SHALL COORDINATE HIS WORK WITH COMPANY REPRESENTATIVES. FOR UTILITY MARK-OUT SERVICE, CALL 811.
5. THE EXISTENCE AND LOCATION OF UNDERGROUND UTILITIES OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED FROM A SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO OTHER EXISTING UTILITIES EXCEPT AS SHOWN ON THESE PLANS. NO REPRESENTATION IS MADE AS TO THE ACCURACY OR COMPLETENESS OF SAID UTILITY INFORMATION. THE CONTRACTOR IS REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN HEREON AND ANY OTHERS NOT OF RECORD OR NOT SHOWN ON THESE PLANS. ALL DAMAGES THERETO CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE APPROPRIATE SPECIFICATIONS AND STANDARDS AT THE EXPENSE OF THE CONTRACTOR.
6. LOCATION AND ELEVATION OF EXISTING IMPROVEMENTS TO BE CONNECTED TO SHALL BE CONFIRMED BY FIELD MEASUREMENTS PRIOR TO CONSTRUCTION OF NEW WORK.
7. CONTRACTOR SHALL MAKE EXPLORATORY EXCAVATIONS AND LOCATE EXISTING UNDERGROUND FACILITIES SUFFICIENTLY AHEAD OF CONSTRUCTION TO PERMIT REVISIONS TO PLANS IF REVISIONS ARE NECESSARY BECAUSE OF ACTUAL LOCATION OF EXISTING FACILITIES.
8. FOR ALL UTILITY TRENCHES, SOILS REPORTS SHALL BE SUBMITTED TO THE ENGINEER OF RECORD BY A QUALIFIED SOILS ENGINEER WHICH CERTIFY THAT TRENCH BACKFILL WAS COMPACTED AS DIRECTED BY THE SOILS ENGINEER IN ACCORDANCE WITH THE ON-SITE EARTHWORK SPECIFICATIONS.
9. ANY WORK DONE WITHOUT INSPECTION OR MATERIALS TESTING IS SUBJECT TO REMOVAL OR CORRECTION.
10. CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF ANY DAMAGE TO THE EXISTING IMPROVEMENTS AND REPLACEMENT TO THE SATISFACTION OF THE ENGINEER.
11. PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL SITE CONDITIONS FOR GRADING, DRAINAGE AND UNDERGROUND FACILITIES, INCLUDING LOCATION AND ELEVATION OF EXISTING UNDERGROUND FACILITIES AT CROSSINGS WITH PROPOSED UNDERGROUND FACILITIES. IF CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND SHALL NOT BEGIN CONSTRUCTION UNTIL THE CHANGED CONDITIONS HAVE BEEN EVALUATED.
12. THE CONTRACTOR SHALL FAMILIARIZE HIMSELF/HERSELF WITH THE PLANS, THE SOILS AND/OR GEOLOGY REPORTS, AND THE SITE CONDITIONS PRIOR TO COMMENCING WORK.
13. SHOULD CONFLICTING INFORMATION BE FOUND ON THE PLANS OR IN THE FIELD, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF RECORD BEFORE PROCEEDING WITH THE WORK IN QUESTION.
14. APPROVAL OF THESE PLANS BY THE TOWN DOES NOT AUTHORIZE ANY WORK TO BE PERFORMED UNTIL A PERMIT HAS BEEN ISSUED.
15. THE APPROVAL OF THIS PLAN OR ISSUANCE OF A PERMIT BY THE TOWN DOES NOT AUTHORIZE THE CONTRACTOR OR OWNER TO VIOLATE ANY FEDERAL, STATE OR COUNTY LAWS, ORDINANCES, REGULATIONS, OR POLICIES, INCLUDING, BUT NOT LIMITED TO, THE FEDERAL ENDANGERED SPECIES ACT OF 1973 AND AMENDMENTS THERETO (16 UBC SECTION 1531 ET.SEQ.)
16. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SURVEY MONUMENTS AND/OR VERTICAL CONTROL BENCHMARKS WHICH ARE DISTURBED OR DESTROYED BY CONSTRUCTION. A LAND SURVEYOR MUST FIELD LOCATE, REFERENCE, AND/OR PRESERVE ALL HISTORICAL OR CONTROLLING MONUMENTS PRIOR TO ANY EARTHWORK. IF DESTROYED, A LAND SURVEYOR SHALL REPLACE SUCH MONUMENTS WITH APPROPRIATE MONUMENTS. ALL MONUMENTS SHALL BE SET IN ACCORDANCE WITH COLORADO REVISED STATUTES TITLE 12, ARTICLE 25, PART 2. IF ANY VERTICAL CONTROL IS TO BE DISTURBED OR DESTROYED, THE TOWN FIELD SURVEY SECTION MUST BE NOTIFIED, IN WRITING, AT LEAST 3 DAYS PRIOR TO THE CONSTRUCTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE COST OF REPLACING ANY VERTICAL CONTROL BENCHMARKS DESTROYED BY THE CONSTRUCTION.
17. DEVIATIONS FROM THESE SIGNED PLANS WILL NOT BE ALLOWED UNLESS THE TOWN ENGINEER APPROVES A CONSTRUCTION CHANGE OR THE TOWN/AGENCY INSPECTOR REQUIRES THE CHANGE.
18. AS-BUILT DRAWINGS MUST BE SUBMITTED TO THE ENGINEER PRIOR TO ACCEPTANCE OF THIS PROJECT BY THE TOWN. CONTACT THE ENGINEER FOR THE REQUIREMENTS OF THE AS-BUILT DRAWINGS.
19. THE AREA WHICH IS DEFINED AS A NON GRADING AREA AND WHICH IS NOT TO BE DISTURBED SHALL BE STAKED PRIOR TO START OF THE WORK. THE PERMIT APPLICANT AND ALL OF THEIR REPRESENTATIVES OR CONTRACTORS SHALL COMPLY WITH THE REQUIREMENTS FOR PROTECTION OF THIS AREA AS REQUIRED BY ANY APPLICABLE AGENCY. ISSUANCE OF THE TOWN'S GRADING PERMIT SHALL NOT RELIEVE THE APPLICANT OR ANY OF THEIR REPRESENTATIVES OR CONTRACTORS FROM COMPLYING WITH ANY STATE OR FEDERAL REQUIREMENTS BY AGENCIES INCLUDING BUT NOT LIMITED TO COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT OR COLORADO DIVISION OF WILDLIFE. COMPLIANCE MAY INCLUDE OBTAINING PERMITS, OTHER AUTHORIZATIONS, OR COMPLIANCE WITH MANDATES BY ANY APPLICABLE STATE OR FEDERAL AGENCY.
20. EXISTING TOPOGRAPHY SHOWN WAS TAKEN FROM A SURVEY PREPARED BY **ENGINEERING SERVICE COMPANY DATED MARCH 20, 2014.**
21. NOTES AND DETAILS DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL NOTES AND TYPICAL DETAILS. WHERE NO DETAILS ARE GIVEN, CONSTRUCTION SHALL BE AS SHOWN FOR SIMILAR WORK.
22. IF AT ANY TIME DURING THE GRADING OPERATION, ANY UNFAVORABLE GEOLOGICAL CONDITIONS ARE ENCOUNTERED, GRADING IN THAT AREA WILL STOP UNTIL APPROVED CORRECTIVE MEASURES ARE OBTAINED.
23. STRAIGHT GRADE SHALL BE MAINTAINED BETWEEN CONTOUR LINES AND SPOT ELEVATIONS UNLESS OTHERWISE SHOWN ON THE PLANS.
24. ALL DEBRIS AND FOREIGN MATERIAL SHALL BE REMOVED FROM THE SITE AND DISPOSED OF AT APPROVED DISPOSAL SITES. THE CONTRACTOR SHALL OBTAIN NECESSARY PERMITS FOR THE TRANSPORTATION OF MATERIAL TO AND FROM THE SITE.
25. DIMENSIONS TO PIPELINES ARE TO CENTERLINE UNLESS OTHERWISE NOTED.
26. CONSTRUCTION STAKING FOR IMPROVEMENTS SHOWN IN THESE PLANS SHALL BE PERFORMED BY A LICENSED LAND SURVEYOR.
27. ALL DIMENSIONS ARE IN FEET OR DECIMALS THEREOF.
28. SPOT GRADES ARE TO FLOWLINE OR FINISH PAVEMENT GRADE UNLESS OTHERWISE NOTED.
29. CONTRACTOR TO BE AWARE OF ALL OVERHEAD LINES AT ALL TIMES, SO AS NOT TO DISTURB THEM.
30. WATER SHALL BE PROVIDED ONSITE AND USED TO CONTROL DUST DURING DEMOLITION AND CONSTRUCTION OPERATIONS.
31. STORM DRAINAGE SYSTEMS SHOWN ON THESE PLANS HAVE BEEN DESIGNED FOR THE FINAL SITE CONDITION AT COMPLETION OF THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ADEQUATE DRAINAGE OF THE SITE DURING INTERIM CONDITIONS OF CONSTRUCTION.
32. RETAINING WALLS LOCATED CLOSER TO THE PROPERTY LINE THAN THE HEIGHT OF THE WALL SHALL BE BACKFILLED NOT LATER THAN 10 DAYS AFTER CONSTRUCTION OF THE WALL AND NECESSARY STRUCTURAL SUPPORTING MEMBERS UNLESS RECOMMENDED OTHERWISE BY ENGINEER OF RECORD.

**TOWN OF PARKER CONSTRUCTION NOTES**

1. ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
2. ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
3. A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
4. A DEVELOPMENT REVIEW ENGINEER SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A DEVELOPMENT REVIEW ENGINEER IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE DEVELOPMENT REVIEW ENGINEER'S ABSENCE. HOWEVER, TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
5. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION. FOR INFORMATION CONTACT: DENVER INTER-UTILITY GROUP, 303-534-6700 OR 1-800-922-1987. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
6. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER PUBLIC WORKS DIRECTOR, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
7. ALL PROPOSED STREET CUTS TO EXISTING PAVEMENTS FOR UTILITIES, STORM SEWER OR FOR OTHER PURPOSES ARE LISTED AND REFERENCED BELOW:  
SANITARY SEWER CONNECTION - N/A  
WATER TIE IN - N/A  
STORM CONNECTION - N/A
8. A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY PERMIT USE APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
9. ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) 'SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION.'
10. COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
11. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
12. THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
13. PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
14. REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR.
15. ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
16. ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR 'STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY.' FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-82, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
17. IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
18. ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

**TOWN OF PARKER STORM DRAINAGE INFRASTRUCTURE NOTES**

1. ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
2. THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72" (5 CCR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
3. INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUDED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
4. STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
5. ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS
6. TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED TO MEET TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:  
6.1. 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE MANHOLES, INLETS, ETC)  
6.2. 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
7. ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
8. ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
9. JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
10. EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
11. CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
12. PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
13. TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
14. ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
15. ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
16. CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
17. PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

**ROADWAY NOTES**

1. PAVING SHALL NOT COMMENCE UNTIL A SOILS REPORT AND PAVEMENT DESIGN IS APPROVED BY THE ENGINEERING DIVISION AND SUBGRADE COMPACTION TESTS ARE SUBMITTED TO AND APPROVED BY THE DEVELOPMENT REVIEW ENGINEER.
2. STANDARD TOWN OF PARKER HANDICAP RAMPS ARE TO BE CONSTRUCTED AT ALL CURB RETURNS AND AT ALL "T" INTERSECTIONS.
3. ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
4. ALL ELEVATIONS ARE ON USGS DATUM WITH DATE. RANGE POINT OR MONUMENT SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
5. EXCEPT WHERE OTHERWISE PROVIDED FOR IN THESE PLANS AND SPECIFICATIONS, THE COLORADO DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF HIGHWAYS M AND S STANDARDS, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, LATEST EDITION, SHALL APPLY.

**TOWN OF PARKER SIGNAGE AND STRIPING NOTES**

1. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT VERSION OF THE FEDERAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), COLORADO SUPPLEMENTAL MUTCD, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL. FURTHER SPECIFICATIONS AND ILLUSTRATIONS ARE LOCATED IN THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) M AND S STANDARDS.
2. A FIELD INSPECTION OF LOCATION AND INSTALLATION OF ALL SIGNS AND MARKINGS SHALL BE PERFORMED BY THE TOWN OF PARKER.
3. THE CONTRACTOR INSTALLING SIGNS SHALL BE RESPONSIBLE FOR THE LOCATING AND PROTECTING OF ALL UNDERGROUND UTILITIES.
4. TYPE III (LIGHTED) BARRICADES SHALL BE SET AT THE ENDS OF ROADWAYS SEPARATING FINISHED (AND/OR ACCEPTED) AND UNFINISHED CONSTRUCTION AREAS AND SHALL BE MAINTAINED BY THE CONTRACTOR/DEVELOPER. A "ROAD CLOSED AHEAD" AND "TO BE EXTENDED" WARNING SIGNS SHALL BE INSTALLED APPROPRIATELY IN ADVANCE OF THE TYPE III BARRICADES.
5. SPECIAL CARE SHALL BE TAKEN IN SIGN LOCATIONS TO ENSURE AN UNOBSTRUCTED VIEW OF EACH SIGN.
6. WHERE STOP SIGN CONTROL IS APPROPRIATE, 36 INCH STOP SIGNS SHALL BE USED FOR APPROACHES TO ANY ROADWAY THAT IS CLASSIFIED AS A COLLECTOR OR GREATER.
7. A 7 FOOT MINIMUM POST LENGTH SHALL BE MAINTAINED FROM THE BOTTOM OF THE SIGN PANEL TO THE GROUND. THIS REQUIREMENT FOR VERTICAL CLEARANCE IS FOR ALL SIGNS.
8. DELINEATION OF ROADWAYS SHALL BE AS SPECIFIED IN THE COLORADO DEPARTMENT OF TRANSPORTATION M AND S STANDARDS.
9. RAISED MEDIAN ISLAND NOSES SHALL HAVE R4-7 SIGNS AT EACH END AND A 4"x12" HIGH INTENSITY YELLOW SIGN BLANK LOCATED MIDWAY BETWEEN THE R4-7 SIGN AND FINISHED GRADE ON EACH POST.
10. SIGNAGE AND STRIPING HAS BEEN DETERMINED BY INFORMATION AVAILABLE AT THE TIME OF REVIEW. PRIOR TO THE INITIATION OF ANY WARRANTY PERIOD, THE TOWN OF PARKER RESERVES THE RIGHT TO REQUIRE MODIFICATIONS TO EXISTING, OR INSTALLATION OF, ADDITIONAL SIGNAGE AND/OR PAVEMENT MARKING IF IT IS DETERMINED THAT CONDITIONS WARRANT SUCH MODIFICATION ACCORDING TO THE MUTCD OR THE CDOT M AND S STANDARDS. ALL SIGNAGE AND STRIPING SHALL FALL UNDER THE REQUIREMENTS OF THE TWO YEAR WARRANTY PERIOD FOR NEW CONSTRUCTION. ADDITIONALLY, ALL PAVEMENT MARKINGS SHALL NOT LIFT OR PEEL DURING THE FIRST YEAR AFTER INSTALLATION.
11. DIAMOND GRADE MATERIAL SHALL BE USED ON ALL STOP SIGNS AND OVERHEAD SIGNS. ALL OTHER ROADSIDE TRAFFIC CONTROL DEVICES SHALL USE HIGH INTENSITY GRADE SHEETING. NO FLUORESCENT YELLOW GREEN SHEETING SHALL BE USED UNLESS SPECIFICALLY APPROVED BY THE TOWN OF PARKER.
12. ALL STREET NAME SIGNS SHALL BE HIGH INTENSITY, EXTRUDED, GREEN AND WHITE BLADES. ARTERIAL AND COLLECTOR STREET NAME SIGNS SHALL BE 9 INCH BLADES AND HAVE MIXED CASE LETTERING. LOCAL STREET NAME SIGNS SHALL BE 6 INCHES BLADES WITH MIXED CASE LETTERING.
13. ALL LAYOUTS FOR INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
14. ALL REMOVED SIGNS SHALL BE RETURNED TO THE TOWN OF PARKER.
15. CROSSWALKS:  
•• SHALL BE CONSTRUCTED USING PREFORMED THERMO-PLASTIC OR AN APPROVED EQUAL.  
•• SHALL BE LONGITUDINAL 2 FEET BY 10 FEET (CONTINENTAL) TYPE OR OTHER APPROVED BY THE TOWN OF PARKER.  
•• SHALL LINE UP WITH HANDICAP RAMPS.  
•• SHALL BE CENTERED WITHIN LANES AND PLACED ON LANE AND CURB LINES SO AS TO AVOID VEHICLE WHEEL PATHS.
16. ALL PAVEMENT MARKING MATERIAL (INCLUDING WORDS AND SYMBOLS) SHALL BE AS FOLLOWS:  
FOR CONCRETE SURFACE:  
LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC.) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER. WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE METHYL-MYTHACRALATE, PREFORMED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), OR AS SPECIFIED.  
(SAND OR WATER BLAST CURING COMPOUND PRIOR TO INSTALLATION OF MARKINGS.)  
FOR ASPHALT SURFACE:  
LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC.) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER. WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE HOT APPLIED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), METHYL-MYTHACRALATE, OR AS SPECIFIED.
17. INSPECTION AND APPROVAL OF STRIPING AND CROSSWALK LAYOUT TO BE DONE BY TOWN OF PARKER DEVELOPMENT REVIEW ENGINEER (CALL 303-840-9546) PRIOR TO APPLICATION OF FINAL STRIPING.

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NO.	REVISION	BY	DATE	APPR.

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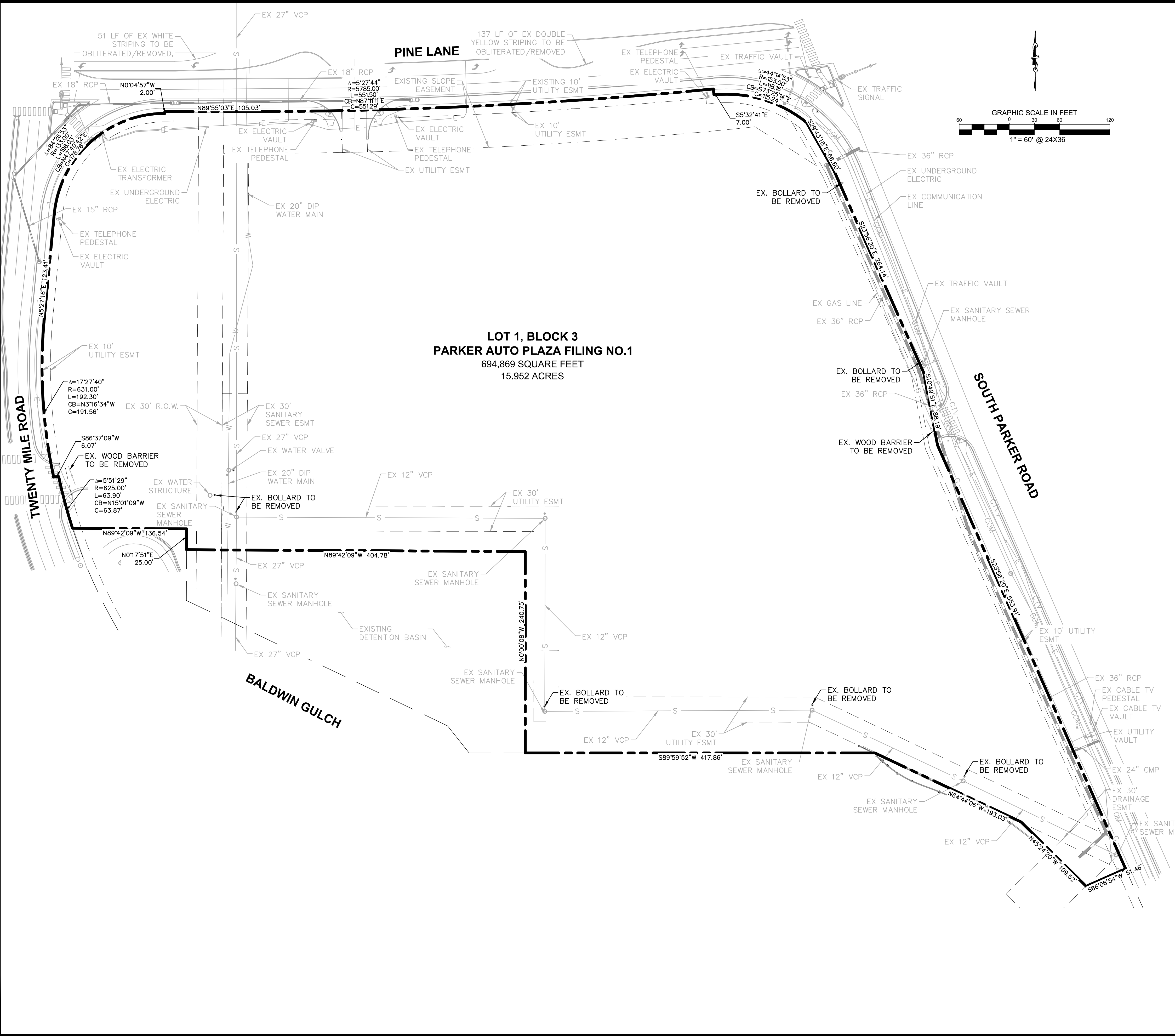
DESIGNED BY: DLS  
DRAWN BY: CTM  
CHECKED BY: DLS  
DATE: 8/17/17

PARKER & PINE  
PARKER, CO  
CONSTRUCTION DOCUMENTS  
GENERAL NOTES

PRELIMINARY  
FOR REVIEW ONLY  
NOT FOR  
CONSTRUCTION  
**Kimley»Horn**  
Kimley-Horn and Associates, Inc.  
PROJECT NO.  
096502001  
DRAWING NAME  
096502001CV\_PW  
**C1.1**



1. The design is preliminary and is intended for informational purposes only. It is not to be used for construction. The design is preliminary and is intended for informational purposes only. It is not to be used for construction. The design is preliminary and is intended for informational purposes only. It is not to be used for construction.



**LEGEND**

---	EXISTING PROPERTY LINE
---	EXISTING EASEMENT LINE
---	EXISTING STORM SEWER LINE
E	EXISTING UNDERGROUND POWER LINE
G	EXISTING GAS LINE
T	EXISTING TELECOMM LINE
COM	EXISTING COMMUNICATIONS LINE
S	EXISTING SANITARY SEWER LINE
⊙	EXISTING SANITARY SEWER MANHOLE
⊕	EXISTING STORM SEWER MANHOLE

- DEMOLITION NOTES**
- ALL EXISTING IMPROVEMENTS TO BE PROTECTED IN PLACE, UNLESS SPECIFICALLY CALLED OUT AS 'TO BE REMOVED'. CONTRACTOR TO CONFIRM ALL UTILITY DISCONNECTS HAVE BEEN COMPLETED PRIOR TO DEMOLITION. DISCONNECTS SHALL BE MADE AT THE MAIN AND ANY UTILITY LINES REMOVED FROM THE SITE AS REQUIRED BY PARKER WATER & SANITATION DISTRICT OR THE TOWN OF PARKER ENGINEERING.
  - CONTRACTOR TO ENSURE UTILITY LOCATES HAVE BEEN PERFORMED PRIOR TO DEMOLITION ACTIVITIES.
  - ALL APPLICABLE PARKER WATER & SANITATION DISTRICT STANDARDS FOR REMOVAL/ABANDONMENT AND TAP CUT OFFS SHALL BE MAINTAINED.
  - ALL DRY UTILITY RELOCATIONS, DEMOLITION, SERVICE DISRUPTIONS AND NEW CONSTRUCTION SHALL BE COORDINATED BY THE CONTRACTOR WITH ALL UTILITY PROVIDERS AND ADJACENT PROPERTIES (XCEL ENERGY, COMCAST, CENTURYLINK, IREA, ADJACENT OWNERSHIP) IN ADVANCE OF THESE ACTIVITIES.
  - IMPROVEMENTS NOT NOTED FOR DEMOLITION/REMOVAL SHALL BE PROTECTED IN PLACE. CONTRACTOR SHALL CONTACT ENGINEER OF RECORD SHOULD ANY CONFLICTS EXIST.
  - CONTRACTOR TO TAKE CARE WHILE CONDUCTING DEMOLITION AND CONSTRUCTION ACTIVITIES IN PROXIMITY OF EXISTING GAS LINES. LICENSE AGREEMENT IS REQUIRED WITH XCEL ENERGY PRIOR TO CONSTRUCTION WITHIN EXISTING GAS EASEMENTS. CONTRACTOR TO POTHOLE EXISTING UTILITIES AS REQUIRED PRIOR TO COMMENCEMENT OF DEMOLITION AND CONSTRUCTION AS NECESSARY.
  - CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH SITE CONDITIONS AT TIME OF PRICING. THIS DEMOLITION PLAN IS INTENDED TO DEPICT THE OBSERVABLE IMPROVEMENTS. ANY MISCELLANEOUS UTILITIES OR DEBRIS ENCOUNTERED DURING CONSTRUCTION SHALL BE REMOVED AS A PART OF THE BASE SCOPE OF WORK.
  - UTILITIES ARE SHOWN PER ENGINEERING SERVICE COMPANY SURVEY DATED MARCH 20, 2014. LOCATIONS ARE APPROXIMATE AND SHOULD BE VERIFIED.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE

**Kimley»Horn**  
 2017 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
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 DRAWN BY: CTM  
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 DATE: 8/17/17

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**EX. CONDITIONS & DEMOLITION PLAN**

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 Kimley-Horn and Associates, Inc.

PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001DM  
**C2.0**







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THIS DOCUMENT IS UNOFFICIAL. IT IS NOT TO BE USED FOR CONSTRUCTION. IT IS FOR INFORMATION ONLY. THE TOWN OF PARKER ASSUMES NO RESPONSIBILITY FOR THE CONSTRUCTION OF THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS. NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

#### PHASED BMP IMPLEMENTATION – INITIAL PHASE

THE INITIAL PHASE SHALL CONSIST OF THE TEMPORARY CONSTRUCTION BMPs TO MINIMIZE POTENTIAL FOR EROSION AND SEDIMENT TRANSFER WHILE MOBILIZING AND PREPARING THE SITE FOR CONSTRUCTION ACTIVITIES. THE SITE SUPERVISOR SHALL COMPLETE THE ANTICIPATED INITIAL PHASE SEQUENCING AS FOLLOWS:

- 1.CONTRACTOR TO OBTAIN TOWN EROSION CONTROL PERMIT AND STATE CONSTRUCTION ACTIVITIES DISCHARGE PERMIT IN ADVANCE OF START OF CONSTRUCTION.
- 2.INSTALL CONSTRUCTION FENCE AROUND LIMITS OF CONSTRUCTION (LOC) AT PROPERTY LINE.
- 3.INSTALL STABILIZED VEHICLE TRACKING CONTROL PAD. THIS WILL BE THE FIRST CONSTRUCTION WORK ON THE PROJECT.
- 4.PREPARE STABILIZED STAGING AREA AND CONCRETE WASHOUT.
- 5.INSTALL PERIMETER SILT FENCE, AT DOWN GRADIENT LOCATIONS.
- 6.CONSTRUCT AND STABILIZE TEMPORARY SEDIMENT BASIN WITH APPROPRIATE OUT FALL STRUCTURES AFTER RECEIPT OF GRADING PERMIT (CLEAR ONLY THOSE AREAS NECESSARY TO INSTALL THE BASIN). TEMPORARY SEDIMENT BASINS SHALL REMAIN IN PLACE UNTIL ALL STORM SEWER AND PAVEMENT BASE COURSE HAVE BEEN INSTALLED OR INSTRUCTED IN THE SEQUENCE OF NOTES, DUE TO FIELD CONDITIONS OR INSPECTOR INPUT.
- 7.INSTALL DIVERSION DITCHES.
- 8.CALL TOWN OF PARKER FOR EROSION CONTROL INSPECTION.

#### PHASED BMP IMPLEMENTATION – INTERIM PHASE

THE INTERIM PHASE SHALL CONSIST OF THE TEMPORARY CONSTRUCTION BMPs TO MINIMIZE POTENTIAL FOR EROSION AND SEDIMENT TRANSFER DURING THE CONSTRUCTION UTILITIES AND SITE GRADING ASSOCIATED WITH THE PROPOSED RESIDENTIAL BUILDING. THE SITE SUPERVISOR SHALL COMPLETE THE ANTICIPATED INTERIM PHASE SEQUENCING AS FOLLOWS:

- 1.CONFIRM EXISTING BMPs WHICH ARE TO BE MAINTAINED THROUGHOUT CONSTRUCTION ARE IN WORKING ORDER AND COMPLIANT WITH APPLICABLE REGULATIONS.
- 2.REPAIR AND/OR REPLACE ANY EXISTING BMPs WHICH ARE DEEMED INADEQUATE.
- 3.STOCKPILE MATERIALS IN ACCORDANCE WITH THE STOCKPILE MANAGEMENT (SP) CBMP.

NOTE: SOIL STOCKPILES ARE DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY TEMPORARILY CEASES OR IS ANTICIPATED TO BE CEASED FOR A PERIOD GREATER THAN 14 DAYS SHALL BE TEMPORARILY STABILIZED WITH TEMPORARY SEEDING OR OTHER STABILIZATION METHODS WITHIN 7 DAYS OF TEMPORARILY CEASING CONSTRUCTION.

NOTE: STOCKPILE AREAS SHOWN ON THE PLAN FOR REFERENCE ONLY. DUE TO THE CONSTRUCTION PHASES, REUSE OF DEMOLITION MATERIALS AND IMPORT REQUIRED, IT IS ANTICIPATED THAT ONSITE STOCKPILES WILL BE REQUIRED. CONTRACTOR SHALL IMPLEMENT STOCKPILE MANAGEMENT (SP) BMPs AT LOCATIONS REQUIRED BY CONSTRUCTION PHASING AND SEQUENCING AND NOTE SUCH ON THE STORMWATER MANAGEMENT SITE MAP.

- 4.INSTALL PRIVATE STORM, SANITARY, AND WATER FACILITIES.
- 5.INSTALL INLET PROTECTION AT ALL STORM SEWER STRUCTURES AS EACH INLET STRUCTURE IS INSTALLED. CONDUCT PERIODIC INSPECTIONS AT NOTED INTERVALS AS REQUIRED BY THE TOWN AND STATE OF COLORADO.

#### PHASED BMP IMPLEMENTATION – FINAL PHASE

THE FINAL PHASE SHALL CONSIST OF THE TEMPORARY CONSTRUCTION BMPs TO MINIMIZE POTENTIAL FOR EROSION AND SEDIMENT TRANSFER DURING THE CONSTRUCTION OF THE PROPOSED STRUCTURE AND ASSOCIATED LIMITED SITE IMPROVEMENTS. THE SITE SUPERVISOR SHALL COMPLETE THE ANTICIPATED FINAL PHASE SEQUENCING AS FOLLOWS:

- 1.BEGIN VERTICAL CONSTRUCTION OF BUILDING AND STRUCTURES.
- 2.INSTALL DRY UTILITIES AND SET TRANSFORMERS AND METER BANKS IN COORDINATION WITH XCEL ENERGY AND IREA.
- 3.PERMANENTLY STABILIZE AREAS TO BE VEGETATED AS THEY ARE BROUGHT TO FINAL GRADE.
- 4.UPON COMPLETION OF STORM DRAINAGE SYSTEM, CONTRACTOR SHALL TAKE THE SEDIMENT BASIN OFF-LINE AND ESTABLISH DETAILED CONSTRUCTION OF DRAINAGE FACILITIES.
- 5.COMPLETE GRADING AND INSTALLATION OF POND.
- 6.PREPARE SITE FOR CURB, GUTTER, AND PAVING INSTALLATION.
- 7.PAVE SITE AND INSTALL HARDSCAPE.
- 8.REFER TO LANDSCAPING PLAN FOR PLANTING INFORMATION AND INSTALL FINAL STABILIZATION MEASURES.
- 9.CONTACT THE TOWN OF PARKER FOR FINAL INSPECTION.
- 10.REMOVE BMPs.
- 11.REFER TO LANDSCAPING PLAN FOR PLANTING INFORMATION AND INSTALL FINAL STABILIZATION MEASURES.

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	IPCOG	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	IPCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **LEGEND** 1 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

1. GRADING, DRILLING, CLEARING, EXCAVATING, BACK-FILLING, SOIL STRIPPING, SOIL IMPORTING EXPORTING OR ANY OTHER FORM OF SOIL DISTURBANCE SHALL NOT COMMENCE UNTIL A TOWN OF PARKER GRADING/EXCAVATION PERMIT HAS BEEN ISSUED.
2. THE TOWN OF PARKER GRADING/EXCAVATION PERMIT IS VALID FOR A PERIOD OF 2 YEARS FROM THE DATE OF ISSUANCE. ANY LAPSE IN PERMIT COVERAGE MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER AND/OR FINES.
3. ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE CBMP PLAN INSTALLATION NOTES AND DETAILS AND/OR PERMITTED VARIANCES. ALL MATERIALS, WORKMANSHIP AND INSTALLATION METHODS ARE SUBJECT TO INSPECTION BY THE TOWN'S INSPECTOR. THE TOWN OF PARKER RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS, WORKMANSHIP AND/OR INSTALLATION METHODS THAT DO NOT CONFORM TO THE CBMP PLAN AND/OR PERMITTED VARIANCES.
4. THE EROSION CONTROL SUPERVISOR SHALL REVISE OR MODIFY THE EROSION AND SEDIMENT CONTROL MEASURES IF IT BECOMES APPARENT THAT THE ORIGINAL PLAN IS INADEQUATE, OR AS A RESULT OF DEFICIENCIES IDENTIFIED DURING INSPECTIONS PERFORMED BY THE TOWN'S INSPECTOR.
5. THE INSTALLATION OF ADDITIONAL EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs) MAY BE REQUIRED OF THE EROSION CONTROL SUPERVISOR, PROPERTY OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS AT ANY TIME THROUGHOUT THE DURATION OF THE PROJECT CONSTRUCTION AND/OR SUBSEQUENT REVEGETATION PERIOD.
6. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE SITE REMAINS IN COMPLIANCE WITH THE NOTICE OF NUISANCE REPORTS (e.g., CONSTRUCTION SITE RUNOFF CONTROL INSPECTION REPORTS), APPROVED CBMP PLAN(S) AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
7. THE EROSION CONTROL SUPERVISOR SHALL BE READILY AVAILABLE TO DISCUSS AND CORRECT ANY PROBLEMS THAT MAY ARISE RELATING TO GRADING, EROSION AND SEDIMENT CONTROL.
8. IF IT IS ANTICIPATED THAT ALL OR A PORTION OF THE PROJECT WILL OCCUR DURING SEASONS SUSCEPTIBLE TO SNOWFALL, THE USE OF CERTAIN EROSION AND SEDIMENT CONTROL BMPs ADJACENT TO PUBLIC ROADWAYS MAY NEED TO BE RECONSIDERED DUE TO THE NATURE OF SNOW REMOVAL OPERATIONS. AN APPROVED ALTERNATIVE MAY BE NECESSARY TO MINIMIZE DAMAGE FROM THESE OPERATIONS. THE TOWN OF PARKER ASSUMES NO RESPONSIBILITY FOR DAMAGE TO ANY BMPs AS A RESULT OF SNOW PLOWING AND SNOW REMOVAL.
9. AREAS OF LAND DISTURBANCE EQUAL TO 40 ACRES OR GREATER SHALL NOT BE EXPOSED FOR MORE THAN 30 CONSECUTIVE DAYS WITHOUT TEMPORARY OR PERMANENT STABILIZATION.
10. AUTHORIZED EXEMPTIONS MAY BE ALLOWED TO THE 40-ACRE LIMIT FOR REMOVAL AND STORAGE OF CUT MATERIAL WHERE (A) GEOTECHNICAL LIMITATIONS RESTRICT THE USE OF TEMPORARY OR PERMANENT STABILIZATION OF THE STORED MATERIAL (E.G., SWELLING SOILS, ROCK) AND (B) WHEN THE OWNER CAN DEMONSTRATE THAT THE 40-ACRE LIMIT IS PHYSICALLY AND/OR FINANCIALLY IMPRACTICABLE. FOR SITES GRANTED THIS EXEMPTION, A PHASING AND EARTHWORK QUANTITIES PLAN SHALL BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT AND APPROVED PRIOR TO THE COMMENCEMENT OF LAND DISTURBANCE ACTIVITIES. SUBMITTAL REQUIREMENTS INCLUDE:
  - (i) PHASING PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
  - (ii) EARTHWORK QUANTITY PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
  - (iii) BMP PLAN SHOWING SPECIFIC EROSION AND SEDIMENT CONTROLS FOR EACH PHASE.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 1 OF 4 Oct. 2013

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	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	SEEDING, MULCHING AND CRIMPING
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **LEGEND** 2 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

11. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
12. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.
13. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.
14. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.
15. ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.
16. ALL TRASH RECEPTACLES ON SITE SHALL BE EMPLOYED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONFINED TO THE RECEPTACLE.
17. ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.
18. ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REBAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.
19. THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT. FAILURE TO ABIDE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.
20. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PAVED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.
21. NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.
22. ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL BLANKET(S). TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS.
23. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.
24. A WATER SOURCE SHALL BE AVAILABLE ONSITE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FUGITIVE DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 2 OF 4 Oct. 2013

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	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **LEGEND** 3 OF 3 Oct. 2013

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25. ALL CHEMICAL OR HAZARDOUS MATERIAL SPILLS, INCLUDING CONCRETE WASHOUT WATER, WHICH MAY ENTER WATERS OF THE STATE OF COLORADO, WHICH INCLUDES BUT ARE NOT LIMITED TO, SURFACE WATER, GROUND WATER, DRY GULLIES OR STORM SEWERS LEADING TO SURFACE WATER, SHALL BE IMMEDIATELY REPORTED TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) PER 25-9-601, AND THE TOWN OF PARKER. RELEASES OF PETROLEUM PRODUCTS AND CERTAIN HAZARDOUS SUBSTANCES LISTED UNDER THE FEDERAL CLEAN WATER ACT (40 CFR PART 116) MUST BE REPORTED TO THE NATIONAL RESPONSE CENTER AND THE CDPHE. SPILLS THAT POSE AN IMMEDIATE SAFETY HAZARD SHALL BE REPORTED TO 911.
26. THE CLEANING OF CONCRETE TRUCKS AND EQUIPMENT IS RESTRICTED TO THE APPROVED CONCRETE WASHOUT LOCATION ON THE JOB SITE. CONCRETE WASH WATER SHALL NOT BE DISCHARGED TO STATE WATERS OR STORM SEWER SYSTEMS.
27. VEHICLE AND EQUIPMENT DEGREASING IS PROHIBITED ON THE JOB SITE.
28. ALL DEWATERING ON SITE SHALL BE COORDINATED WITH THE TOWN'S INSPECTOR. A STATE PERMIT MAY BE REQUIRED FOR DEWATERING. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR OBTAINING AND ADHERING TO ALL APPLICABLE PERMITS.
29. HYDRAULIC SEEDING AND/OR HYDRAULIC MULCHING ARE ONLY ALLOWED IN AREAS UNDER TEMPORARY OR PERMANENT IRRIGATION OR FOR THE PURPOSE OF TEMPORARY SOIL STABILIZATION.
30. APPLICABLE CONSTRUCTION BMPs SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL ALL LANDSCAPING HAS BEEN INSTALLED AND THE DESIRABLE VEGETATION HAS REACHED A POINT IN WHICH EROSION AND SEDIMENTATION IS NO LONGER A CONCERN AS DETERMINED BY THE TOWN'S INSPECTOR.
31. GRADING SECURITY RELEASE REQUIREMENTS:
  - 1) DEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H OR ITEM I (BELOW).
    - A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
    - B. UNIFORM PERENNIAL VEGETATION COVER HAS BEEN ESTABLISHED WITH AN INDIVIDUAL PLANT DENSITY OF AT LEAST SEVENTY PERCENT (70%) OF PRE-DISTURBANCE LEVELS.
    - C. ALL CBMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
    - D. IF ANY EROSION IS PRESENT, IT IS INSIGNIFICANT AND IS NOT LEAVING THE SITE AND/OR LEADING INTO ANY ON-SITE DRAINAGE INFRASTRUCTURE THAT MAY CONVEY SURFACE WATER OFF SITE.
    - E. WEEDS REPRESENT NO MORE THAN FIFTY PERCENT (50%) OF THE TOTAL VEGETATION ON THE SITE.
    - F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
    - G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
    - H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.
    - I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 3 OF 4 Oct. 2013

NO.	REVISION	BY	DATE	APPR.

**Kimley-Horn** 2017 KIMLEY-HORN AND ASSOCIATES, INC. 4582 South Ulster Street, Suite 1500 Denver, Colorado 80237 (303) 228-2300

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2) NONDEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H AND J, OR ITEMS I AND J (BELOW).

A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.  
 B. ALL CBMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.  
 C. EROSION IS NEGLIGIBLE, IF EVEN PRESENT.  
 D. THE VEGETATION REPRESENTS A PERENNIAL STAND OF A DENSE, UNIFORM SURFACE OF GRASS WITH NO AREA GREATER THAN ONE (1) SQUARE FOOT THAT IS BARREN OF DESIRABLE VEGETATION, INFREQUENT, WIDELY SCATTERED AREAS WHERE NATIVE VEGETATION HAS NOT YET TAKEN HOLD MAY QUALIFY FOR ACCEPTANCE AT THE DISCRETION OF THE TOWN.  
 E. WEEDS REPRESENT NO MORE THAN TEN PERCENT (10%) OF THE TOTAL VEGETATION ON SITE.  
 F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.  
 G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.  
 H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.  
 I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR THE GRADING PERMIT HAS BEEN ASSIGNED AS PROVIDED BY SECTION 11.10.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION, AT THE TIME OF CLOSING ON THE SALE OF A SITE THAT IS SUBJECT TO A GRADING PERMIT, TO ENSURE THAT THE NEW PROPERTY OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.  
 J. ALL KNOWN DRAINAGE ISSUES ASSOCIATED WITH THE PROJECT HAVE BEEN MITIGATED AND A SUFFICIENT AMOUNT OF TIME HAS PASSED TO ENSURE THAT SUCH ISSUES HAVE BEEN CORRECTED. THIS REQUIREMENT DOES NOT INCLUDE THOSE DRAINAGE ISSUES ORIGINATING ON RESIDENTIAL LOTS.

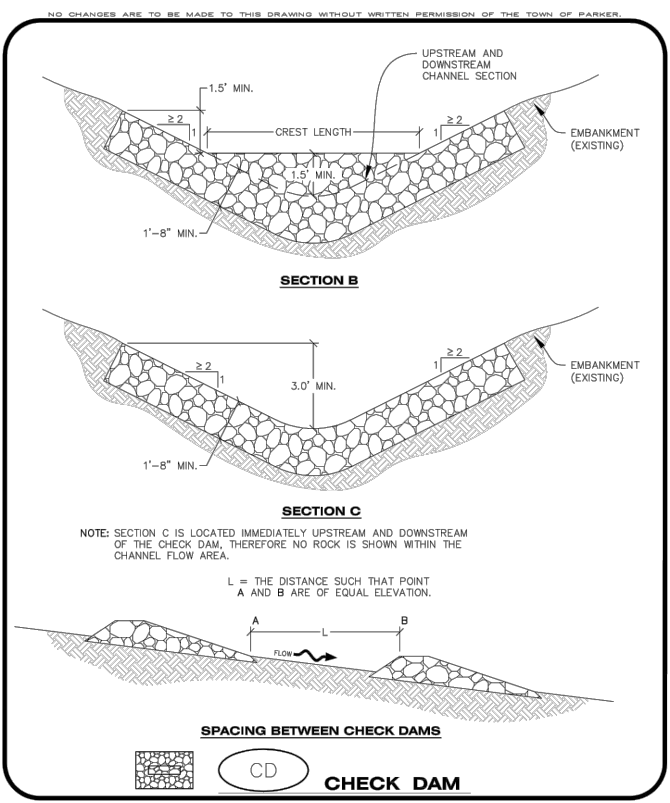
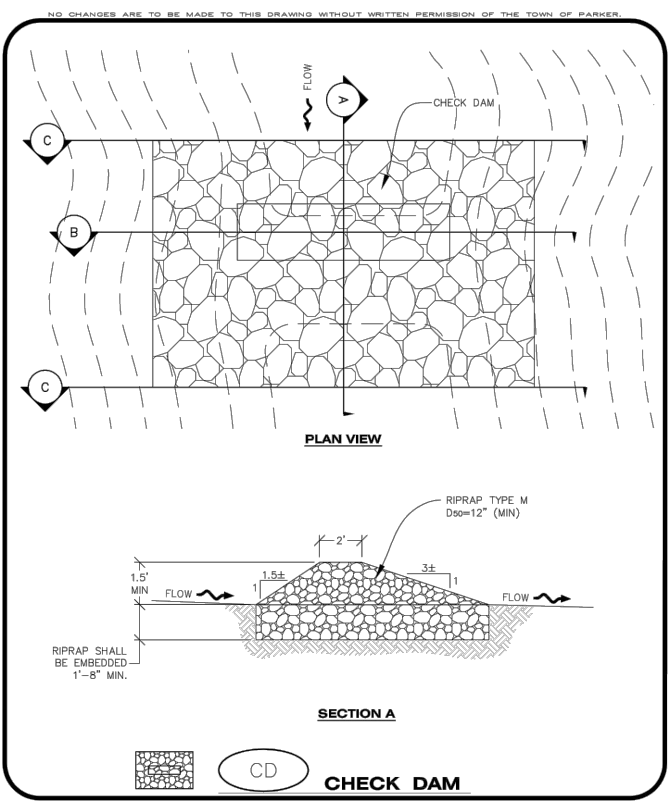
(D) NOXIOUS WEEDS MUST BE CONTROLLED AS PROVIDED UNDER STATE LAW AND SECTION 6.01.260 OF THE TOWN OF PARKER MUNICIPAL CODE. FAILURE TO CONTROL NOXIOUS WEEDS ON THE SITE MAY CONSTITUTE A NUISANCE, SUBJECT TO THE PENALTIES CONTAINED IN THE CODE.

**DEFINITIONS:**  
 DEVELOPABLE PROPERTY MEANS ANY LAND THAT HAS BEEN GRADED AND IS PART OF A PLATTED LOT OR PLATTED TRACT OF RECORD THAT WAS PLATTED FOR FUTURE DEVELOPMENT, INCLUDING RESIDENTIAL HOME CONSTRUCTION OR PUBLIC IMPROVEMENTS.  
 NONDEVELOPABLE PROPERTY MEANS LAND THAT HAS BEEN GRADED AND WILL NOT BE FURTHER DISTURBED AS PART OF ANY FUTURE DEVELOPMENT. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO: PARKS, OPEN SPACE, HOMEOWNER ASSOCIATION OR BUSINESS ASSOCIATION PLATTED TRACTS, DETENTION PONDS AND DRAINAGEWAYS.

32. FAILURE TO COMPLY WITH ANY OF THE REQUIREMENTS DESCRIBED IN THIS SECTION MAY RESULT IN THE ISSUANCE OF: A NOTICE OF INTENT TO ISSUE A STOP WORK ORDER, A STOP WORK ORDER AND/OR THE REMEDIES/PENALTIES DESCRIBED IN CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE.

33. ANY PERSON CONVICTED OF VIOLATING ANY PROVISION OF THE TOWN OF PARKER, GRADING & EARTH MOVEMENT SECTION OF THE MUNICIPAL CODE SHALL BE GUILTY OF A MISDEMEANOR AND, UPON CONVICTION, BE PUNISHED BY A FINE OF NOT MORE THAN FOUR HUNDRED NINETY NINE DOLLARS (\$499.00) FOR EACH SEPARATE OFFENSE. EACH DAY A VIOLATION CONTINUES SHALL CONSTITUTE A SEPARATE OFFENSE. THE TOWN ALSO MAY SEEK IN MUNICIPAL COURT AN INJUNCTION, ABATEMENT, RESTITUTION OR ANY OTHER REMEDY TO PREVENT, ENJOIN, ABATE OR REMOVE THE VIOLATION. A PERSON CONVICTED OF VIOLATING CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE SHALL BE LIABLE FOR THE ACTUAL COST OF REHABILITATING THE PROPERTY. THE COSTS MAY BE RECOVERED AS RESTITUTION IN MUNICIPAL COURT PROCEEDINGS OR IN A SEPARATE CIVIL ACTION.

34. THE TOWN OF PARKER RESERVES THE RIGHT TO ALLOW MODIFICATIONS AND SUBSTITUTIONS TO THE CBMP NOTES AND DETAILS WHEN SUCH MODIFICATIONS AND SUBSTITUTIONS OFFER THE SAME LEVEL OF PROTECTION AS THE STANDARD REQUIREMENTS BASED UPON THE SPECIFIC SITUATION, AS DETERMINED BY TOWN STAFF. DUE TO THE INSIGNIFICANCE AND REGULARITY OF SUCH MODIFICATIONS AND SUBSTITUTIONS, THE APPROVAL OF SUCH VARIATIONS MAY NOT BE DOCUMENTED BY TOWN STAFF.



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CHECK DAM INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF CHECK DAMS.
- CHECK DAMS SHOWN ON CBMP PLAN SHALL BE INSTALLED WHEN DIRECTED BY THE TOWN'S INSPECTOR.
- RIPRAP UTILIZED FOR CHECK DAMS SHALL HAVE A D50 MEDIAN STONE SIZE OF 12\".
- RIPRAP PAD SHALL BE TRENCHED INTO THE GROUND A MINIMUM OF 1'-8\".
- THE MAXIMUM SPACING BETWEEN CHECK DAMS SHOULD BE SUCH THAT THE BOTTOM OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM AS SHOWN IN THE DETAIL.

**CHECK DAM INSPECTION AND MAINTENANCE NOTES**

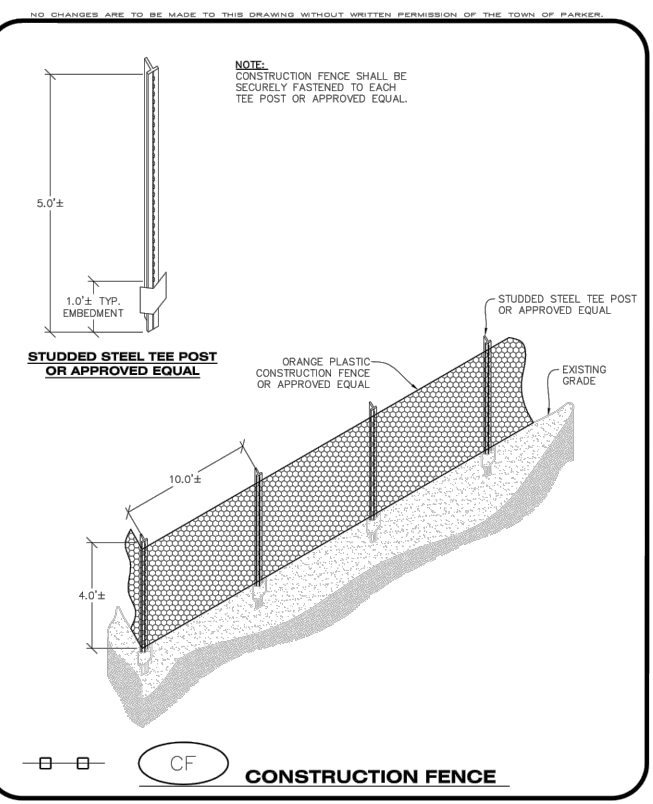
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CHECK DAMS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE CREST OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- CHECK DAMS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN CHECK DAMS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED TOPSOIL AND ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CHECK DAMS BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- IN SOME INSTANCES, CHECK DAMS MAY REMAIN IN PLACE PERMANENTLY.

**Parker COLORADO** | **CBMP** | **GEN NOTES**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 4 OF 4  
 Oct. 2013

**Parker COLORADO** | **CBMP** | **CD**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
 Oct. 2013

**Parker COLORADO** | **CBMP** | **CD**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
 Oct. 2013

**Parker COLORADO** | **CBMP** | **CD**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
 Oct. 2013



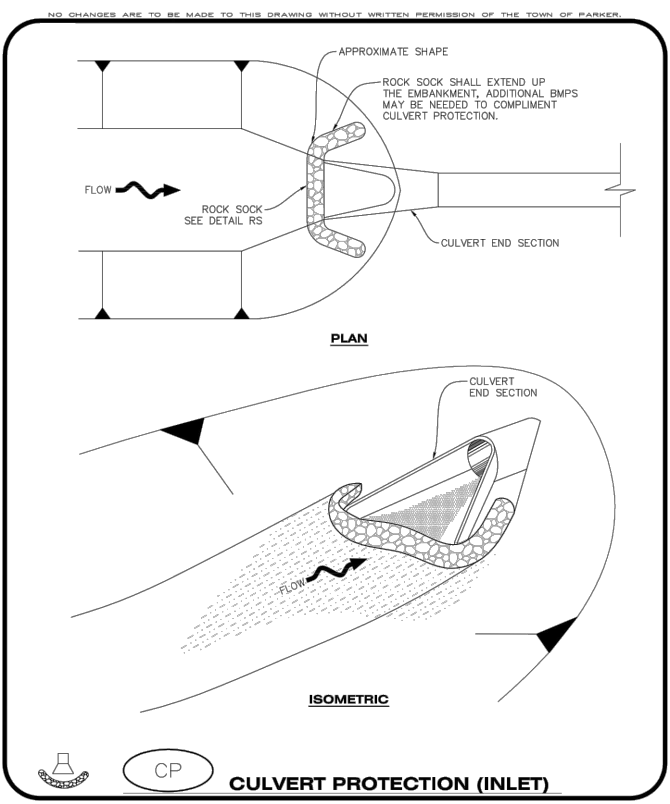
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CONSTRUCTION FENCE INSTALLATION NOTES**

- THE CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO EACH POST OR APPROVED EQUAL.

**CONSTRUCTION FENCE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONSTRUCTION FENCE AND MAKE ANY NECESSARY REPAIRS.
- CONSTRUCTION FENCE SHALL BE REPAIRED WHEN THE FENCING MATERIAL FALLS OUT OF COMPLIANCE WITH THE NOTES AND DETAILS.



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CULVERT PROTECTION (INLET) INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF CULVERT PROTECTION (INLET).
- ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
- ROCK SOCK SHALL BE APPROXIMATELY 12" IN DIAMETER.
- ROCK SOCK SHALL EXTEND ABOVE THE FLOW LINE ELEVATION ON BOTH SIDES OF THE CULVERT END SECTION.

**CULVERT PROTECTION (INLET) INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CULVERT PROTECTION (INLET).
- AT A MINIMUM, ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE ROCK SOCK.
- CULVERT PROTECTION (INLET) IS TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE CULVERT INLET PROTECTION IS REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CULVERT INLET PROTECTION TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**Parker COLORADO** | **CBMP** | **CF**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

**Parker COLORADO** | **CBMP** | **CF**  
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**Parker COLORADO** | **CBMP** | **CP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

**Parker COLORADO** | **CBMP** | **CP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
 Oct. 2013

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 DRAWN BY: CTM  
 CHECKED BY: DLS  
 DATE: 8/17/17

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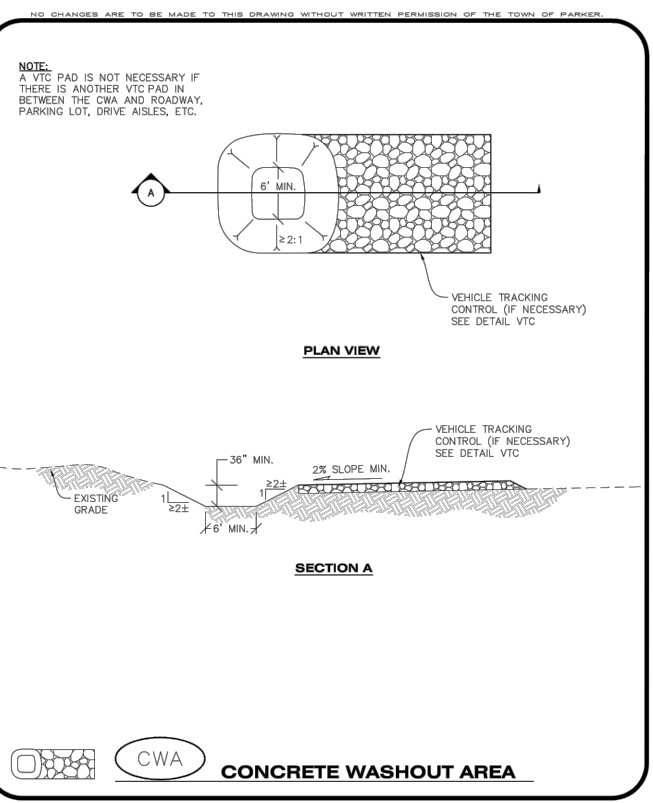
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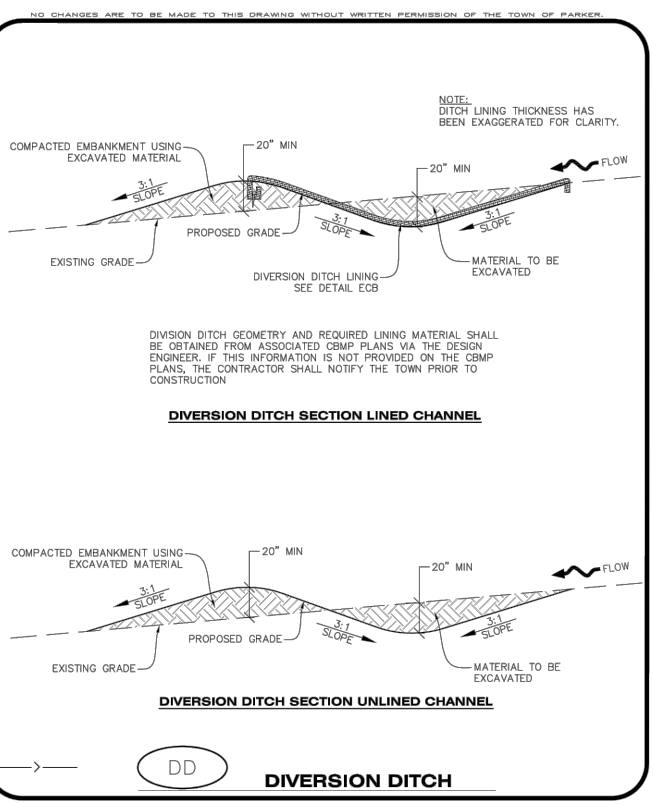
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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CWA** 1 OF 2 Oct. 2013



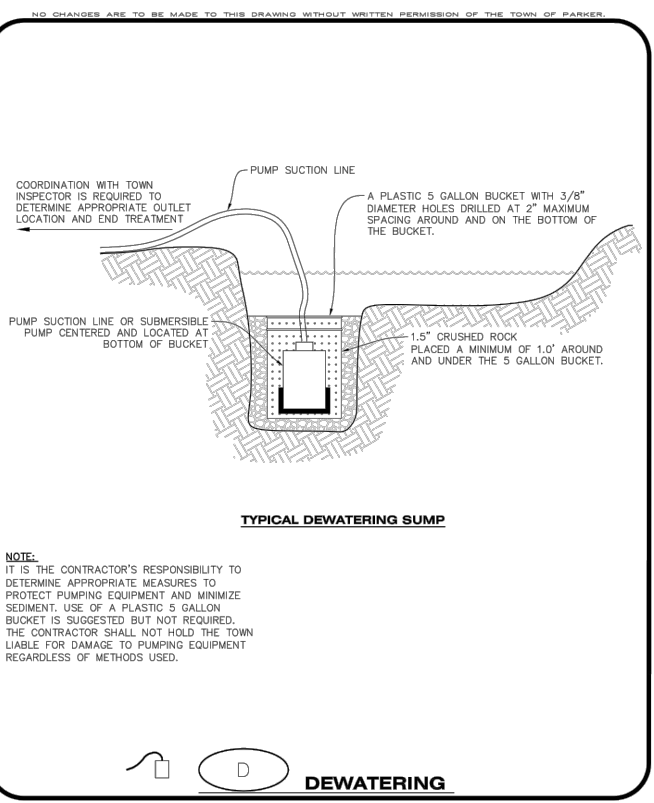
**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DD** 1 OF 2 Oct. 2013

- NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.
- CONCRETE WASHOUT AREA INSTALLATION NOTES**
1. CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
  2. A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
  3. IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.
- CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
  2. CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
  3. CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
  4. CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
  5. CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
  6. WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

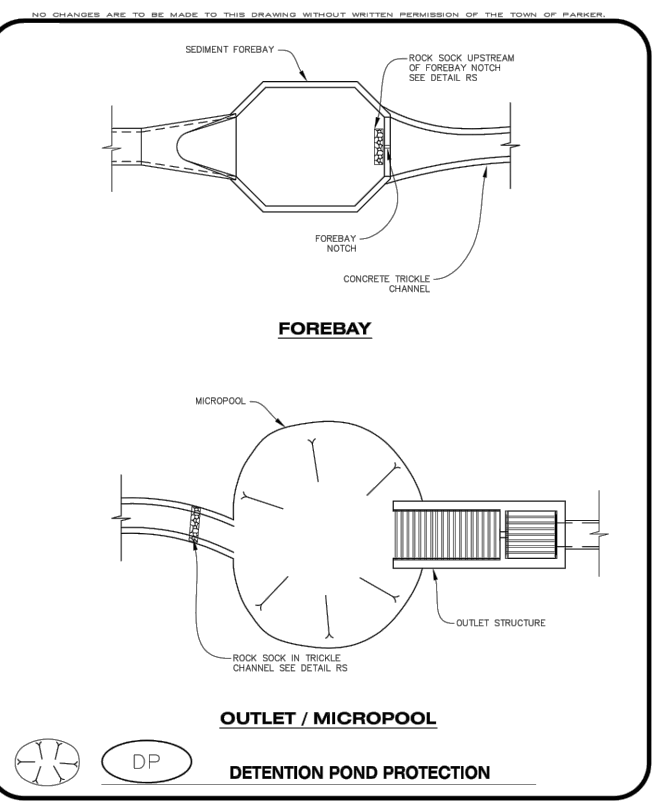
**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CWA** 2 OF 2 Oct. 2013

- NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.
- DIVERSION DITCH INSTALLATION NOTES**
1. SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
  2. A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
  3. ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
  4. THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
  5. THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.
- DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
  2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
  3. DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
  4. DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DD** 2 OF 2 Oct. 2013



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **D** 1 OF 2 Oct. 2013



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DP** 1 OF 2 Oct. 2013

- NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.
- DEWATERING INSTALLATION NOTES**
1. IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).
- DEWATERING MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **D** 2 OF 2 Oct. 2013

- NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.
- DETENTION POND PROTECTION INSTALLATION NOTES**
1. DETENTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
  2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
  3. ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).
- DETENTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
  2. ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
  3. OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DP** 2 OF 2 Oct. 2013

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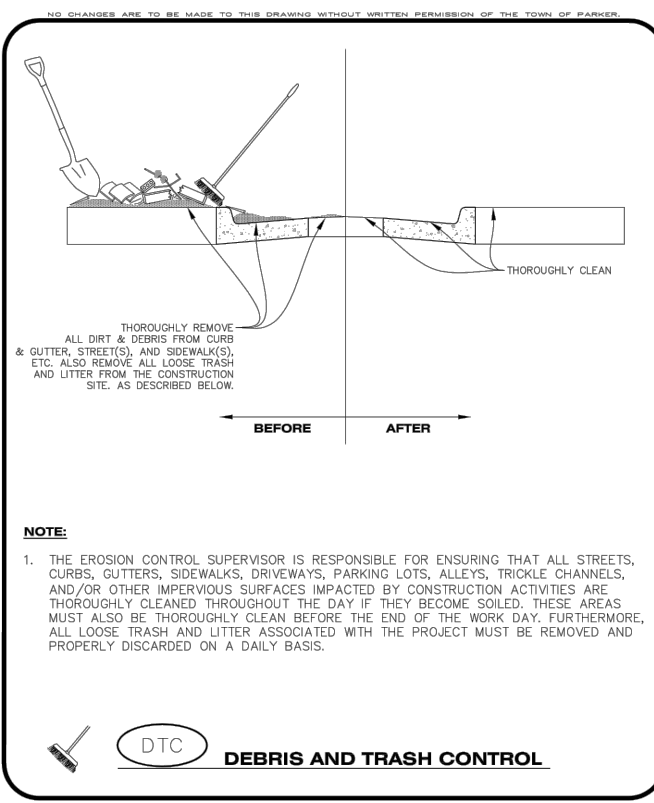
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**DEBRIS CONTROL NOTES:**

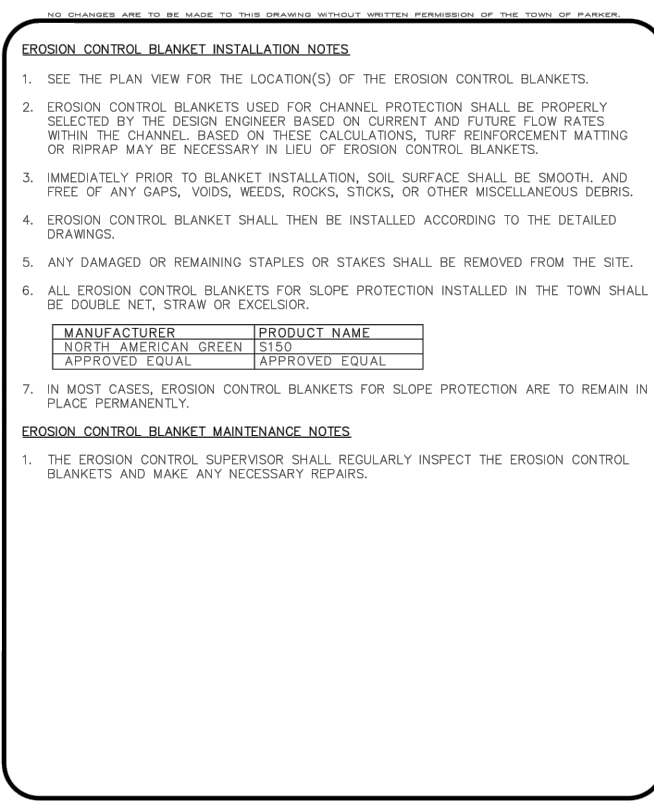
1. A COMBINATION OF SURFACE SCRAPING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THESE AREAS.
2. ALL CHEMICAL SPILLS AND/OR STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EXTENT PRACTICABLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VAC-TRUCK.
3. ON-SITE PERSONNEL, DELIVERY DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUAL DEBRIS AND TRASH CONTROL.

**NOTE:**

1. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY. FURTHERMORE, ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

**DTC DEBRIS AND TRASH CONTROL**

**CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES** | **DTC** 1 OF 2 Oct. 2013



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**EROSION CONTROL BLANKET INSTALLATION NOTES**

1. SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
2. EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL BASED ON THESE CALCULATIONS. TURF REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.
3. IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH, AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.
4. EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.
5. ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
6. ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELSIOR.

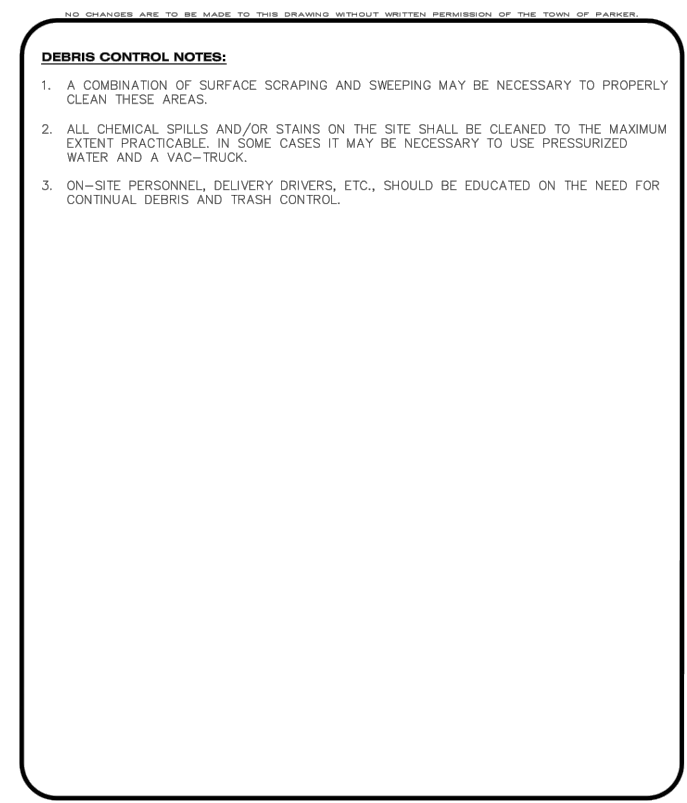
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NORTH AMERICAN GREEN	S150
APPROVED EQUAL	APPROVED EQUAL

**EROSION CONTROL BLANKET MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

**IPAN INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT**

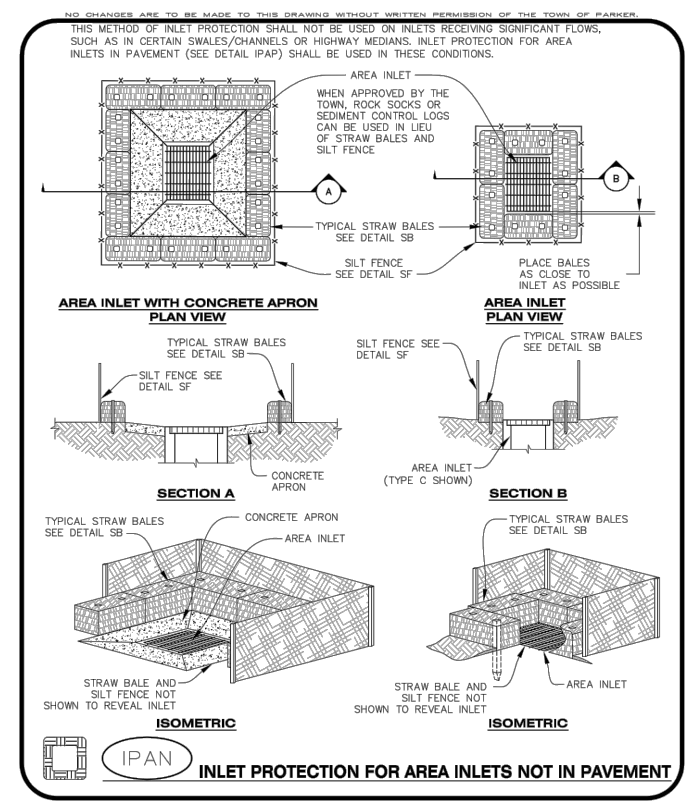
**CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPAN** 1 OF 1 Oct. 2013



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**EROSION CONTROL BLANKET (SLOPE)**

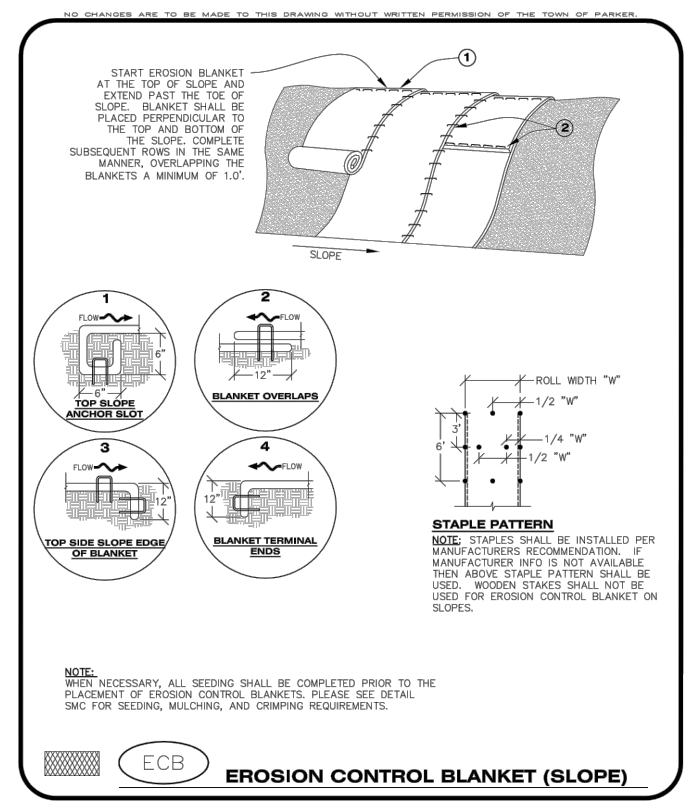
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**EROSION CONTROL BLANKET (CHANNEL)**

**CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES** | **ECB** 2 OF 3 Oct. 2013



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**AREA INLET PROTECTION INSTALLATION NOTES**

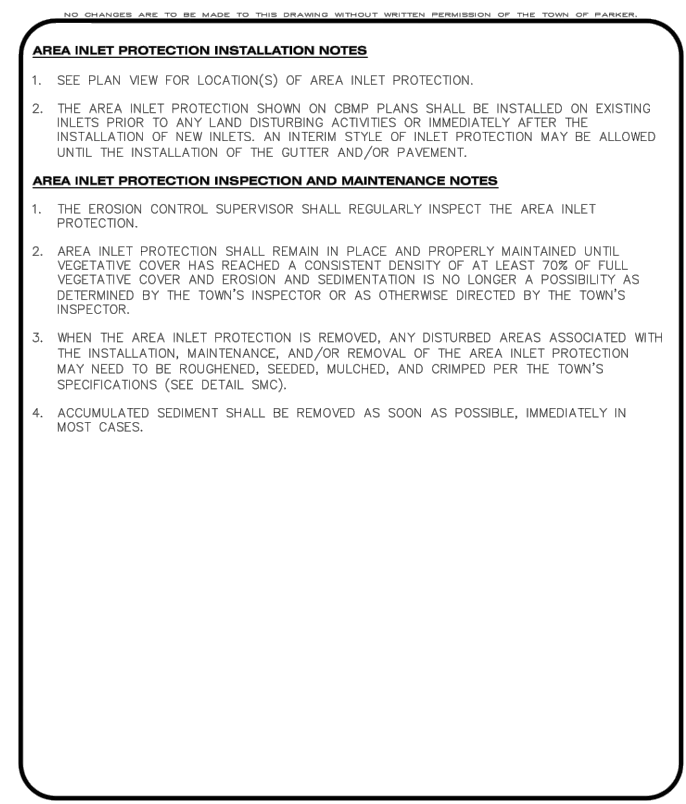
1. SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
2. THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
2. AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
3. WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
4. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

**IPAP INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

**CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPAP** 1 OF 1 Oct. 2013



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**AREA INLET WITH CONCRETE APRON PLAN VIEW**

**AREA INLET PLAN VIEW**

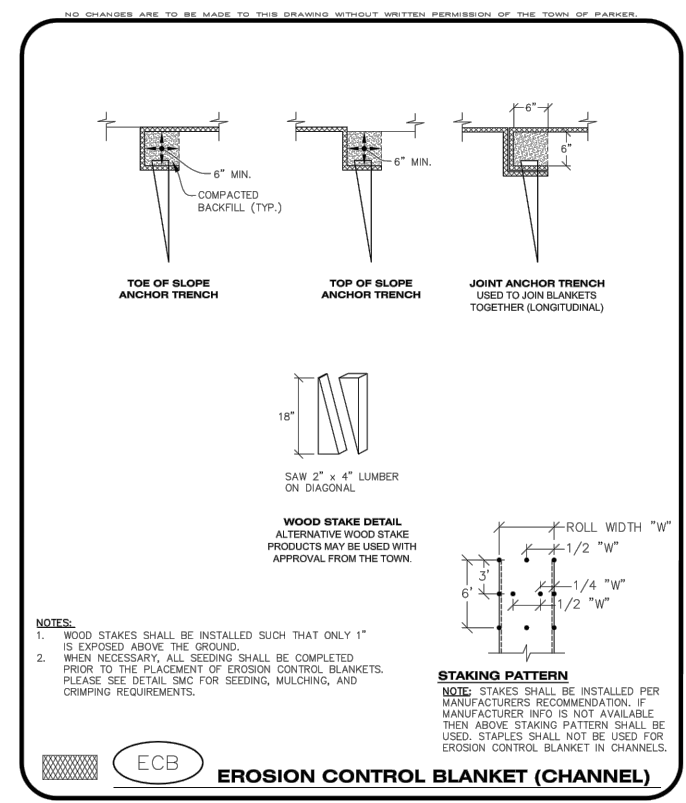
**SECTION A**

**SECTION B**

**ISOMETRIC**

**IPAP INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

**CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPAP** 1 OF 1 Oct. 2013



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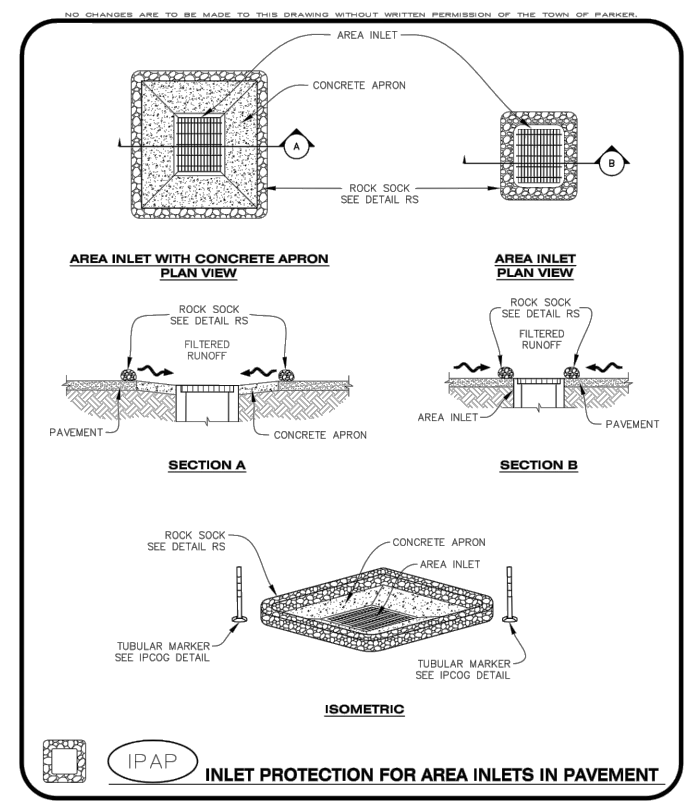
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**SECTION B**

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**IPAP INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

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**AREA INLET WITH CONCRETE APRON PLAN VIEW**

**AREA INLET PLAN VIEW**

**SECTION A**

**SECTION B**

**ISOMETRIC**

**IPAP INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

**CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPAP** 1 OF 1 Oct. 2013

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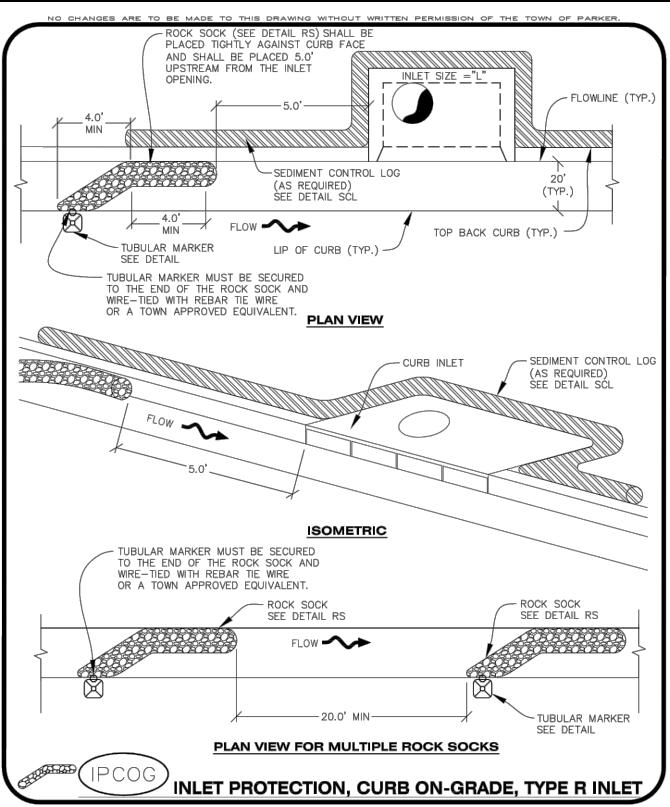
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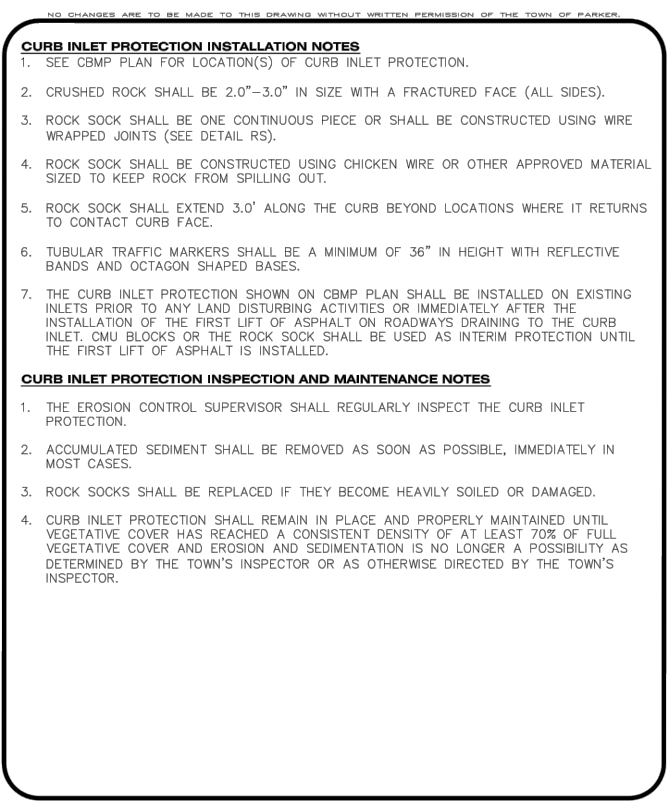
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 DRAWING NAME  
096502001EC\_DT  
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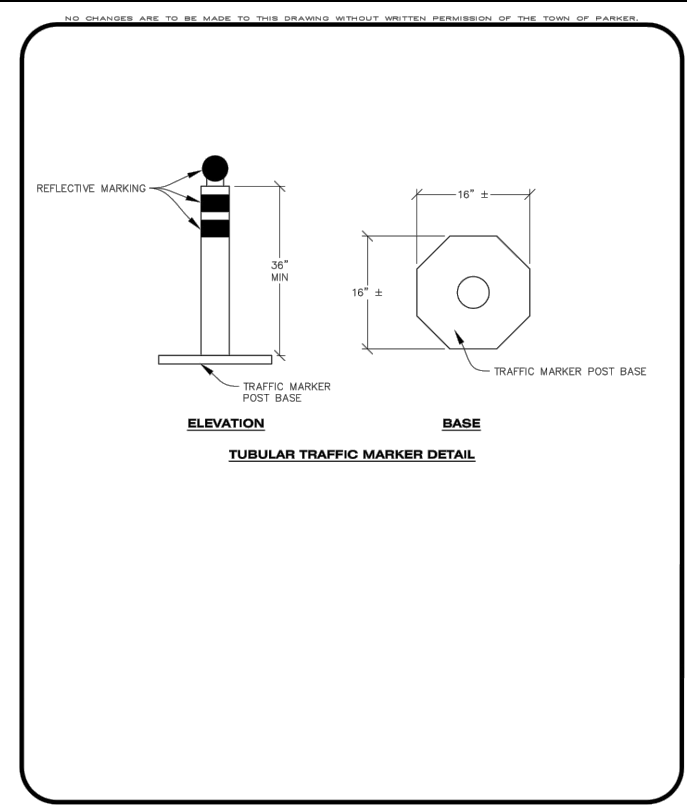
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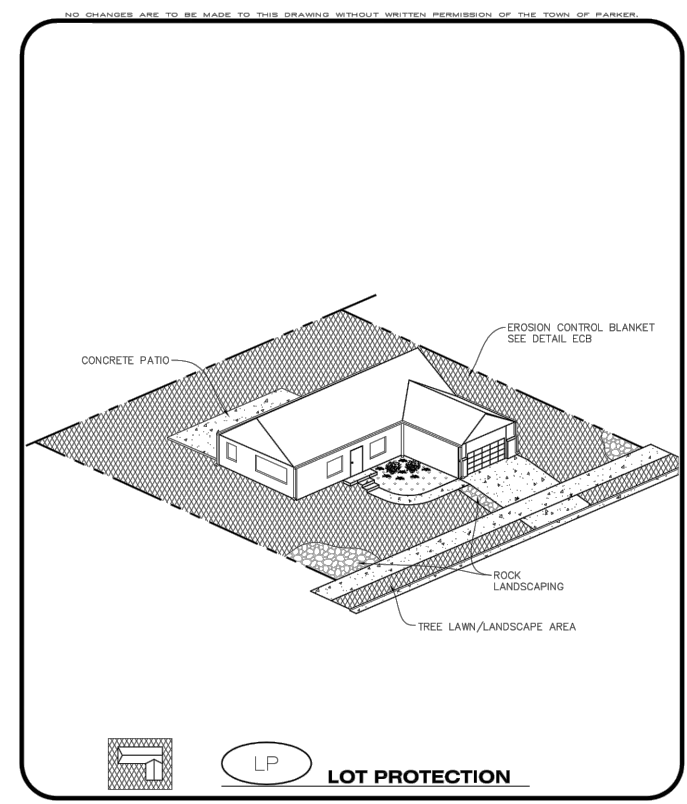
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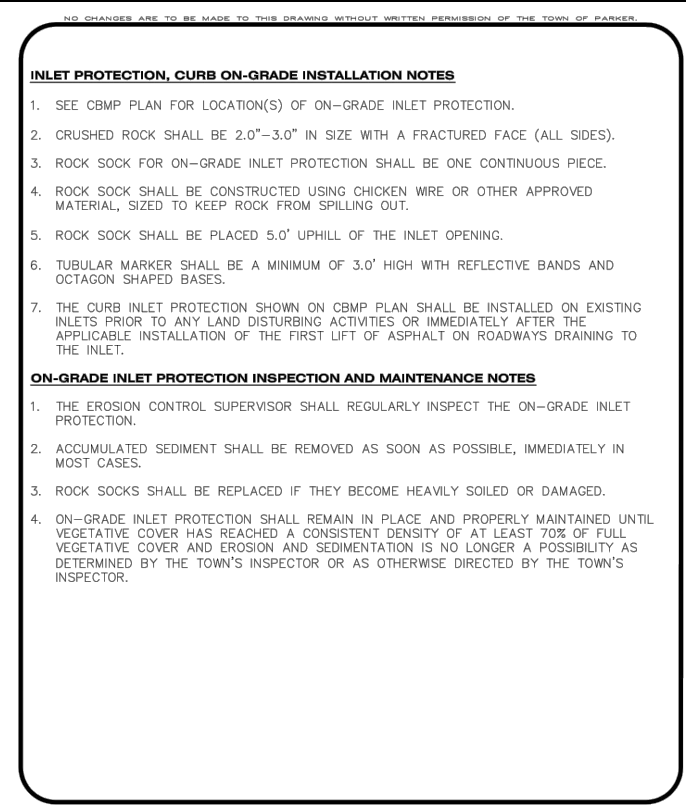
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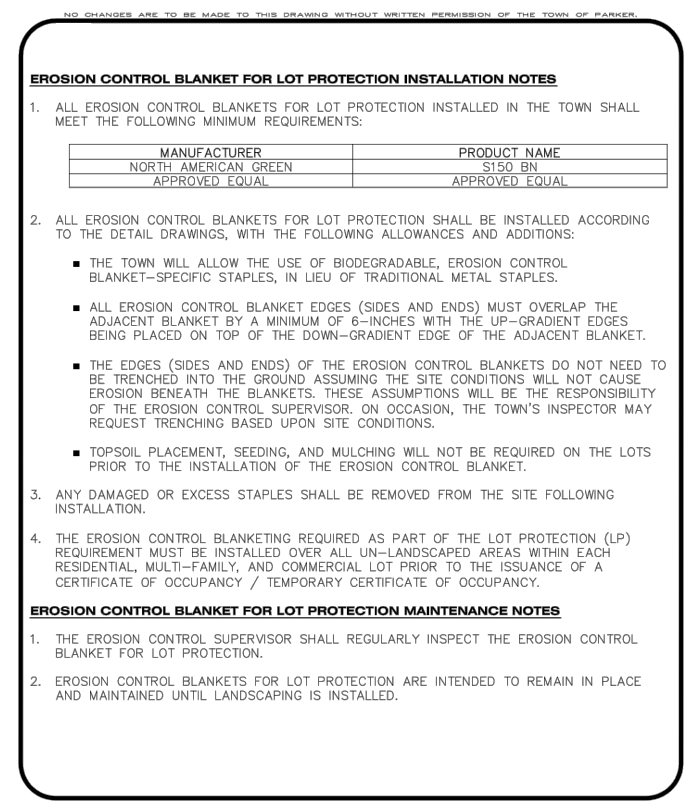
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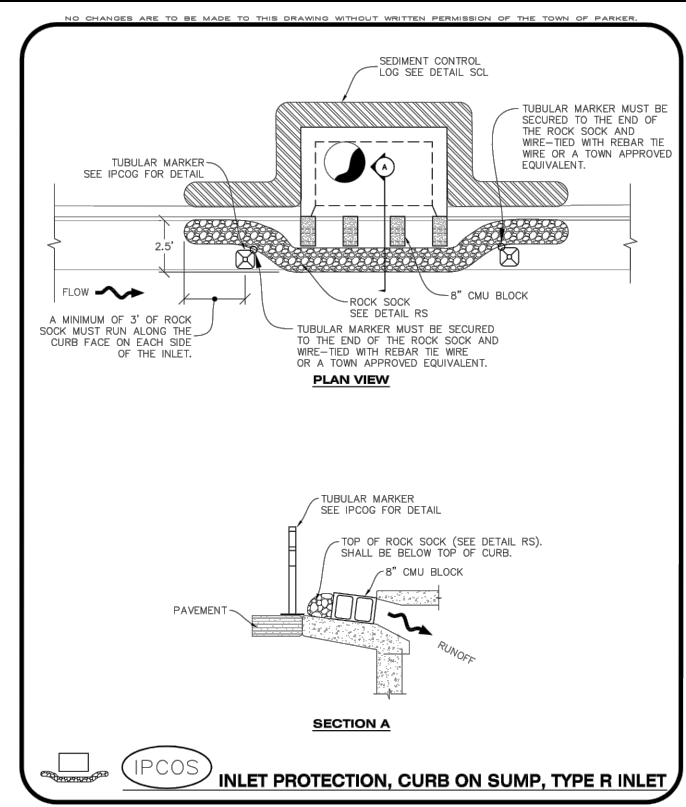
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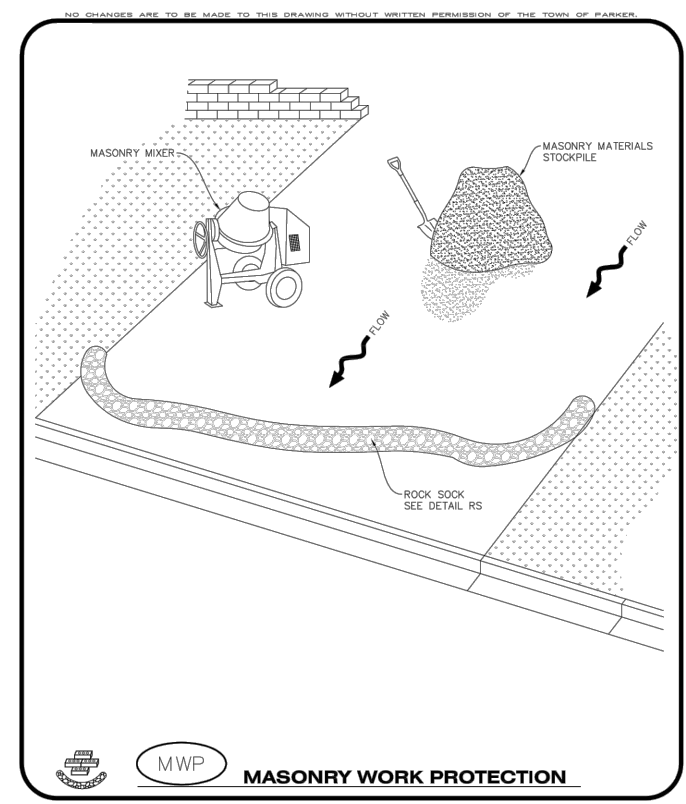
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**MASONRY WORK PROTECTION INSTALLATION NOTES**

- MASONRY WORK PROTECTION MAY NEED TO BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
- A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.

**MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
- ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASH OUT AREA.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 WWP 2 OF 2 Oct. 2013

**ROUGH CUT STREET CONTROL INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
- THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.

**ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
- ROUGH CUT STREET CONTROL SHALL BE REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
- ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUB-GRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE RCSC SHOULD BE REMOVED IN INCREMENTS BASED ON SUBGRADE PREPARATION.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 RCSC 2 OF 2 Oct. 2013

**PORTABLE TOILET PROTECTION INSTALLATION NOTES**

CONTRACTOR SHALL ANCHOR PORTABLE TOILET TO THE GROUND, AT A MINIMUM OF TWO OPPOSING CORNERS (ON A DIAGONAL) USING U-SHAPED REBAR STAKES

**PORTABLE TOILET PROTECTION**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 PTP 1 OF 2 Oct. 2013

**ROCK SOCK INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
- CRUSHED ROCK SHALL BE APPROXIMATELY 2.0"-3.0" GRANITE IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.
- MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
- TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCATION SUSCEPTIBLE TO VEHICLE OR PEDESTRIAN TRAFFIC. TUBULAR MARKERS SHALL CONFORM TO THE TUBULAR MARKER DETAIL.

**ROCK SOCK INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**ROCK SOCK**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 RS 1 OF 2 Oct. 2013

**PORTABLE TOILET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE PORTABLE TOILET PROTECTION.
- PORTABLE TOILETS SHALL BE SERVICED AT THE NECESSARY INTERVALS TO ELIMINATE THE POSSIBILITY OF OVERFLOW.
- WHEN THE PORTABLE TOILETS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE TOILETS MAY NEED TO BE LANDSCAPED OR ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- PORTABLE TOILETS THAT ARE NOT CONSISTENTLY MAINTAINED IN ACCORDANCE WITH THESE REQUIREMENTS MAY NEED TO BE CLUSTERED TOGETHER, IN ONE CENTRALIZED LOCATION IN ORDER TO INCREASE COMPLIANCE AND REDUCE THE CHANCE OF A SPILL.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 PTP 2 OF 2 Oct. 2013

**ROCK SOCK IN SWALE INSTALLATION NOTES**

**ROCK SOCK IN SWALE**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 RSS 2 OF 2 Oct. 2013

**ROUGH CUT STREET CONTROL INSTALLATION NOTES**

**ROUGH CUT STREET CONTROL**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 RCSC 1 OF 2 Oct. 2013

**ROCK SOCK IN SWALE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
 RSS 1 OF 1 Oct. 2013

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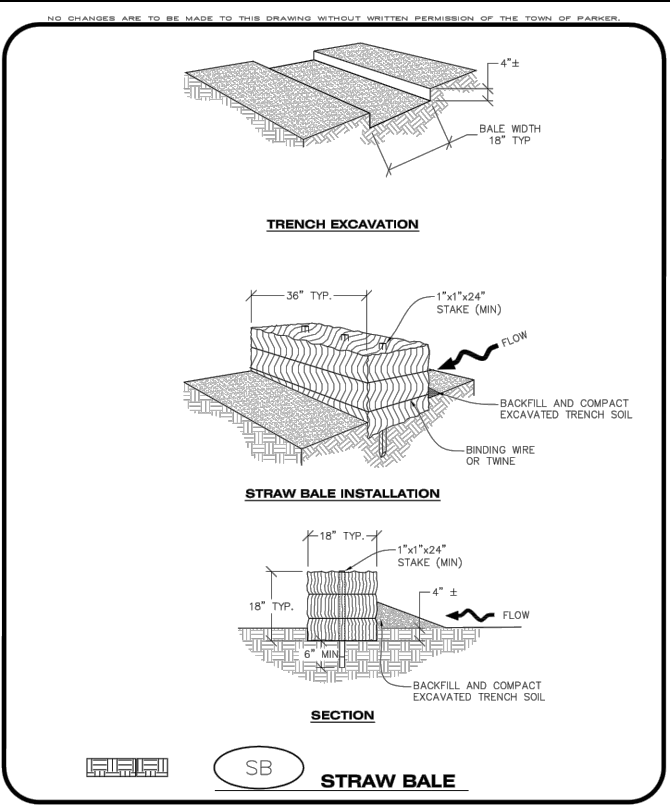
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**STRAW BALE INSTALLATION NOTES**

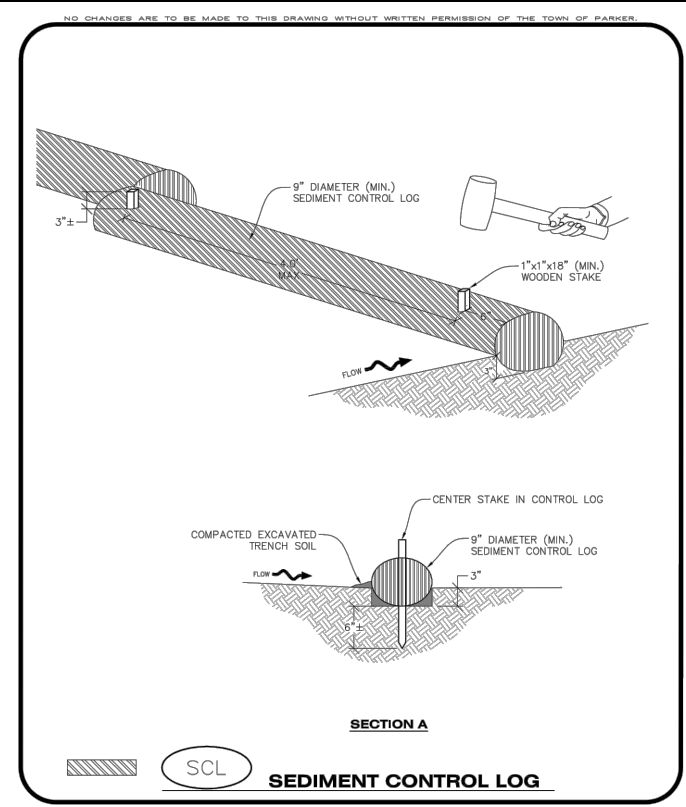
- SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
- TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36"X18"X18".
- TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1"x1"x24".
- WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
- STRAW BALES SHALL BE SPACED AND POSITIONED ACCORDING TO DETAILS.

**STRAW BALE INSPECTION AND MAINTENANCE NOTES**

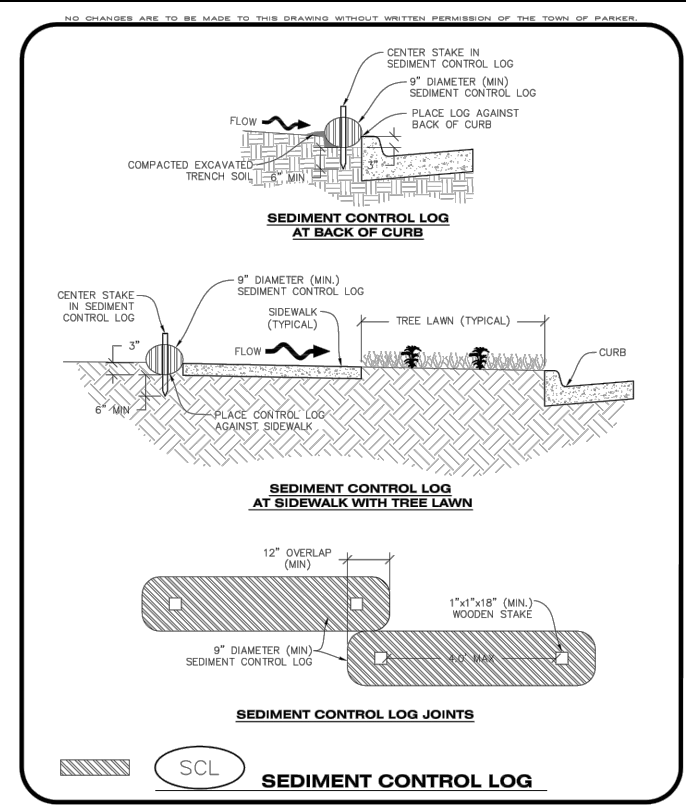
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
- STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
- STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN INSPECTOR.
- WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**SCL**

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 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
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**CBMP** | SCL  
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**SEDIMENT CONTROL LOG INSTALLATION NOTES**

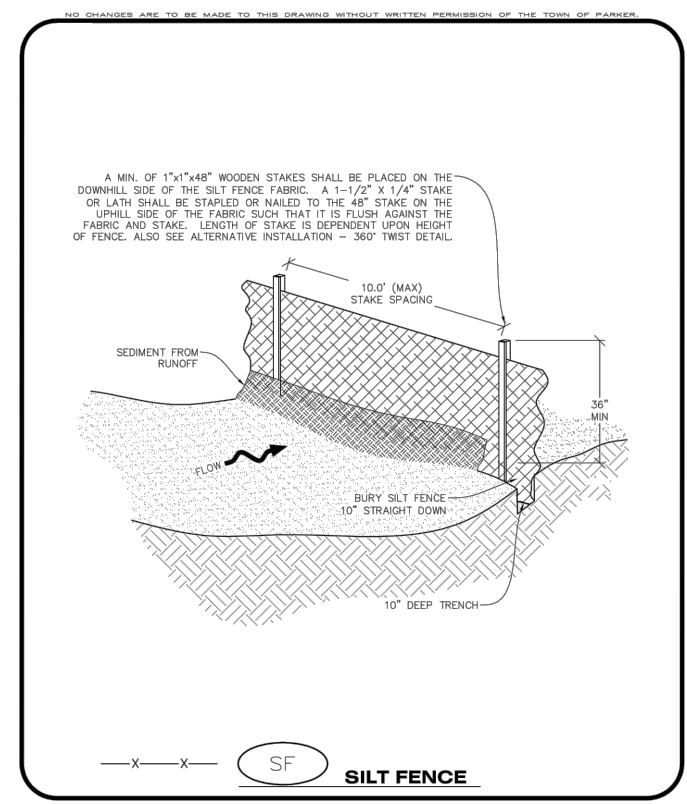
- SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT CONTROL LOGS.
- ALL SEDIMENT CONTROL LOGS SHALL BE INSTALLED FREE OF DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- SEDIMENT CONTROL LOGS SHALL BE INSTALLED IMMEDIATELY ADJACENT TO AN IMPERVIOUS SURFACE SUCH AS A CURB HEAD, SIDEWALK, INLET LID, ETC. NO GAPS SHALL EXIST BETWEEN THE SEDIMENT CONTROL LOG AND THE IMPERVIOUS SURFACE.
- A UNIFORM 3" DEEP ANCHOR TRENCH (APPROX) IN THE SHAPE OF A HALF-SPHERE SHALL BE EXCAVATED USING A TRENCHER, SPADE-SHAPED SHOVEL, OR PICK. THE ANCHOR TRENCH SHALL BE SIZED TO ALLOW FOR THE SEDIMENT CONTROL LOG TO SEAT TIGHTLY AGAINST THE ANCHOR TRENCH.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH AND PROPERLY COMPACTED.
- ANCHOR TRENCH SHALL BE RELATIVELY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT.
- ALL SEDIMENT CONTROL LOGS SHALL BE PLACED 3" (APPROX.) BELOW THE GROUND AND PULLED TIGHT ON BOTH ENDS TO REMOVE ANY CURVES OR SNAGS.
- THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS RELATIVELY FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED AGAINST THE GROUND AND SEDIMENT CONTROL LOG USING A SHOVEL, OR SIMILAR DEVICE.
- SEDIMENT CONTROL LOG STAKES SHALL BE MADE OF WOOD AND SECURELY ANCHOR THE SCL IN PLACE.
- STAKES SHALL BE PLACED ON 4.0' CENTERS AND EMBEDDED APPROXIMATELY 6" INTO THE GROUND. STAKES THAT ARE BROKEN PRIOR TO OR DURING INSTALLATION SHALL BE REPLACED.
- SEDIMENT CONTROL LOGS SHALL OVERLAP A MINIMUM OF 12". THE OVERLAPPING SHALL OCCUR ON THE UP-GRADE SIDE OF THE LOGS.
- SEDIMENT CONTROL LOGS SHALL BE STAKED WITHIN 6" FROM EACH END.
- SEDIMENT CONTROL LOGS THAT ARE INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN THE CONCRETE AND THE LOG. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

**SEDIMENT CONTROL LOG INSPECTION AND MAINTENANCE NOTES**

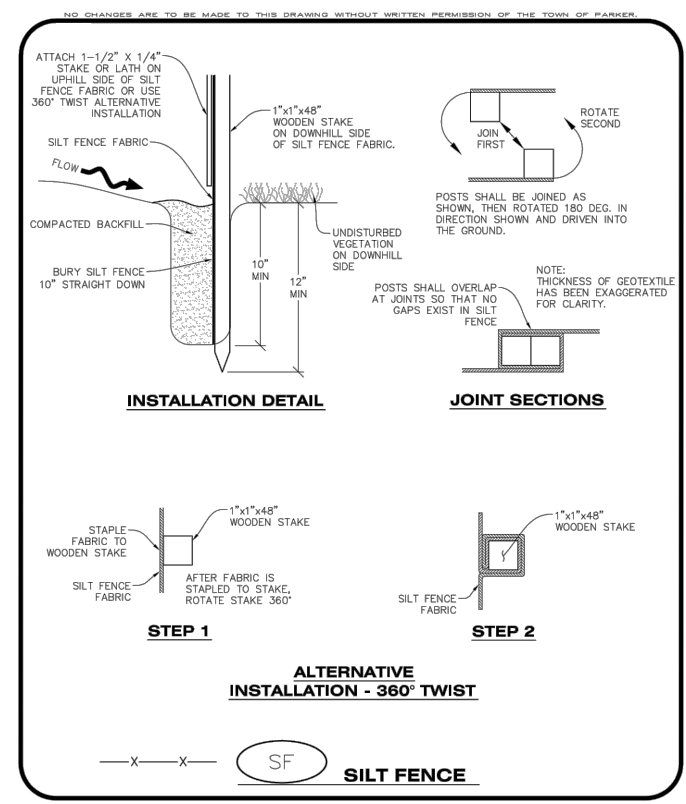
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEDIMENT CONTROL LOGS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF EXPOSED LOG.
- SEDIMENT CONTROL LOGS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- SEDIMENT CONTROL LOGS SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR OR DAMAGE THAT WOULD PREVENT THE SCL FROM FUNCTIONING AS DESIGNED.
- WHEN THE SEDIMENT CONTROL LOGS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SEDIMENT CONTROL LOGS MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**SCL**

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 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
 Oct. 2013



**CBMP** | SF  
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 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 4  
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**SILT FENCE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SILT FENCE.
- ALL SILT FENCE SHALL BE INSTALLED IN GOOD CONDITION AND FREE OF ANY DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- A UNIFORM 10" DEEP ANCHOR TRENCH SHALL BE EXCAVATED USING A TRENCHER.
- A 10" DEEP ANCHOR SLIT SHALL BE FORMED IF USING A STATIC SLICING METHOD.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH.
- ANCHOR TRENCH SHALL BE GENERALLY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT OF THE SILT FENCE.
- THE ANCHOR TRENCH SHALL BE THOROUGHLY BACKFILLED WITH SOIL THAT IS GENERALLY FREE OF ROCKS AND DEBRIS.
- ALL EXCAVATED MATERIAL SHALL BE PLACED ON THE UP-GRADE SIDE OF THE SILT FENCE.
- STAKES SHALL BE POSITIONED ON THE DOWNHILL SIDE OF THE SILT FENCE FABRIC AND PLACED ON 10.0' CENTERS OR LESS. STAKES SHALL BE EMBEDDED A MINIMUM OF 12" INTO THE GROUND. A WOODEN LATH SHALL BE ATTACHED TO THE OPPOSING (UPHILL) SIDE OF THE STAKE FOR ADDED STRENGTH AND SUPPORT. THE LATH SHALL HAVE THE FOLLOWING DIMENSIONS: 1"x3/4"x24".
- SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD NOT BE SIGNIFICANT SAGGING ALONG ANY PORTION OF THE SILT FENCE AFTER IT HAS BEEN ANCHORED TO THE STAKES.
- SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES AND LATHS USING STAPLES OR NAILS OF AN APPROXIMATE LENGTH. ENOUGH STAPLES AND NAILS SHOULD BE PLACED ALONG THE LATH TO ENSURE PROPER ATTACHMENT.
- SILT FENCE FABRIC SHALL MEET THE FOLLOWING MANDATORY REQUIREMENTS:

PROPERTIES	TEST METHOD	MANDATORY REQUIREMENTS
GRAB TENSILE STRENGTH	ASTM D 4632	≥ 124 LBS
MULLEN BURST STRENGTH	ASTM D 3786	≥ 300 PSI
PUNCTURE STRENGTH	ASTM D 4833	≥ 60 LBS
TRAPEZOID TEAR STRENGTH	ASTM D 4533	≥ 65 LBS
UV RESISTANCE	ASTM D 4355	> 80% AT 500 HOURS OF UV EXPOSURE
FLOW RATE	ASTM D 4491	≥ 10 GAL/MIN/FT2

- AN ORIGINAL PRODUCT SPECIFICATION SHEET FROM THE SILT FENCE MANUFACTURER SHALL BE MADE AVAILABLE AT THE REQUEST OF THE TOWN'S INSPECTOR. THE PRODUCT SPECIFICATION SHEET SHALL PROVIDE THE RESULTS FOR THE TEST METHODS ABOVE.
- SILT FENCE JOINTS SHALL BE CONNECTED ACCORDING TO THE ATTACHED DRAWING.
- SILT FENCE THAT IS INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN CONCRETE AND THE SILT FENCE. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

**SF**

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 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 4  
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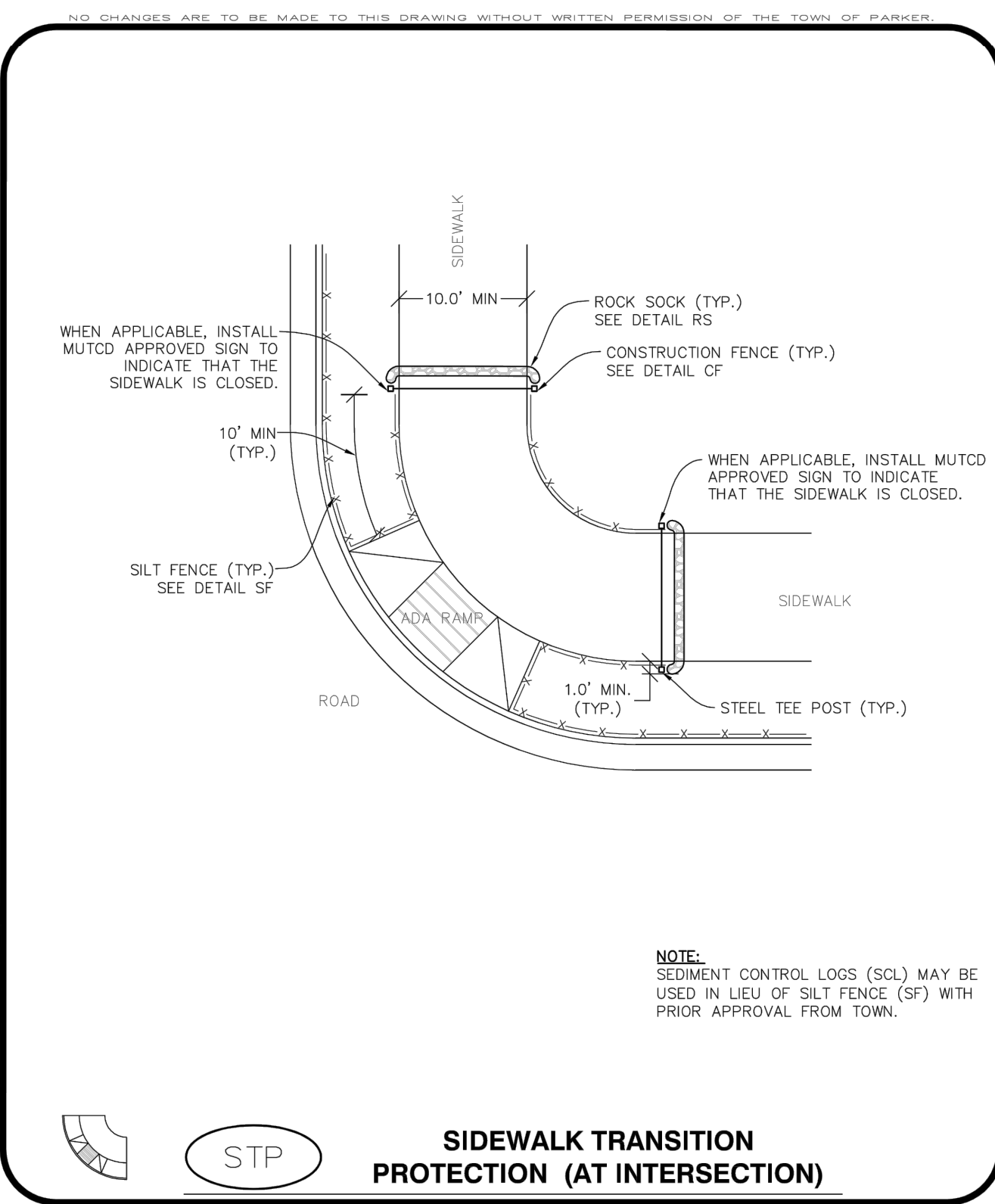
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 096502001EC\_DT  
**C3.8**



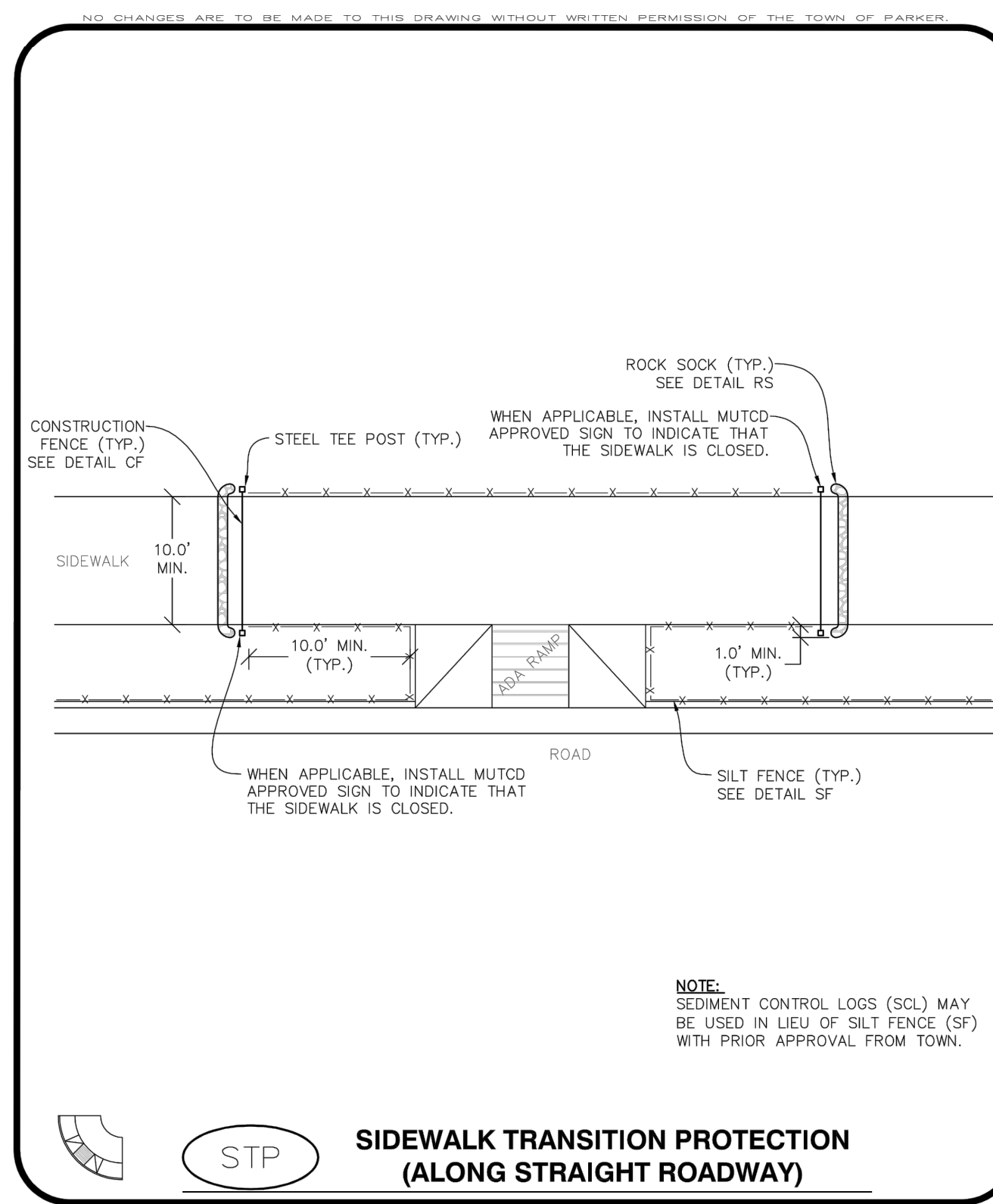


1. The user shall verify the accuracy of the information provided in this drawing. The user shall be responsible for any errors or omissions in this drawing. The user shall be responsible for any damage or injury resulting from the use of this drawing. The user shall be responsible for any costs incurred in the use of this drawing. The user shall be responsible for any legal action resulting from the use of this drawing.



**STP**  
**SIDEWALK TRANSITION PROTECTION (AT INTERSECTION)**

**CBMP** | **STP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
 Oct. 2013



**STP**  
**SIDEWALK TRANSITION PROTECTION (ALONG STRAIGHT ROADWAY)**

**CBMP** | **STP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
 Oct. 2013

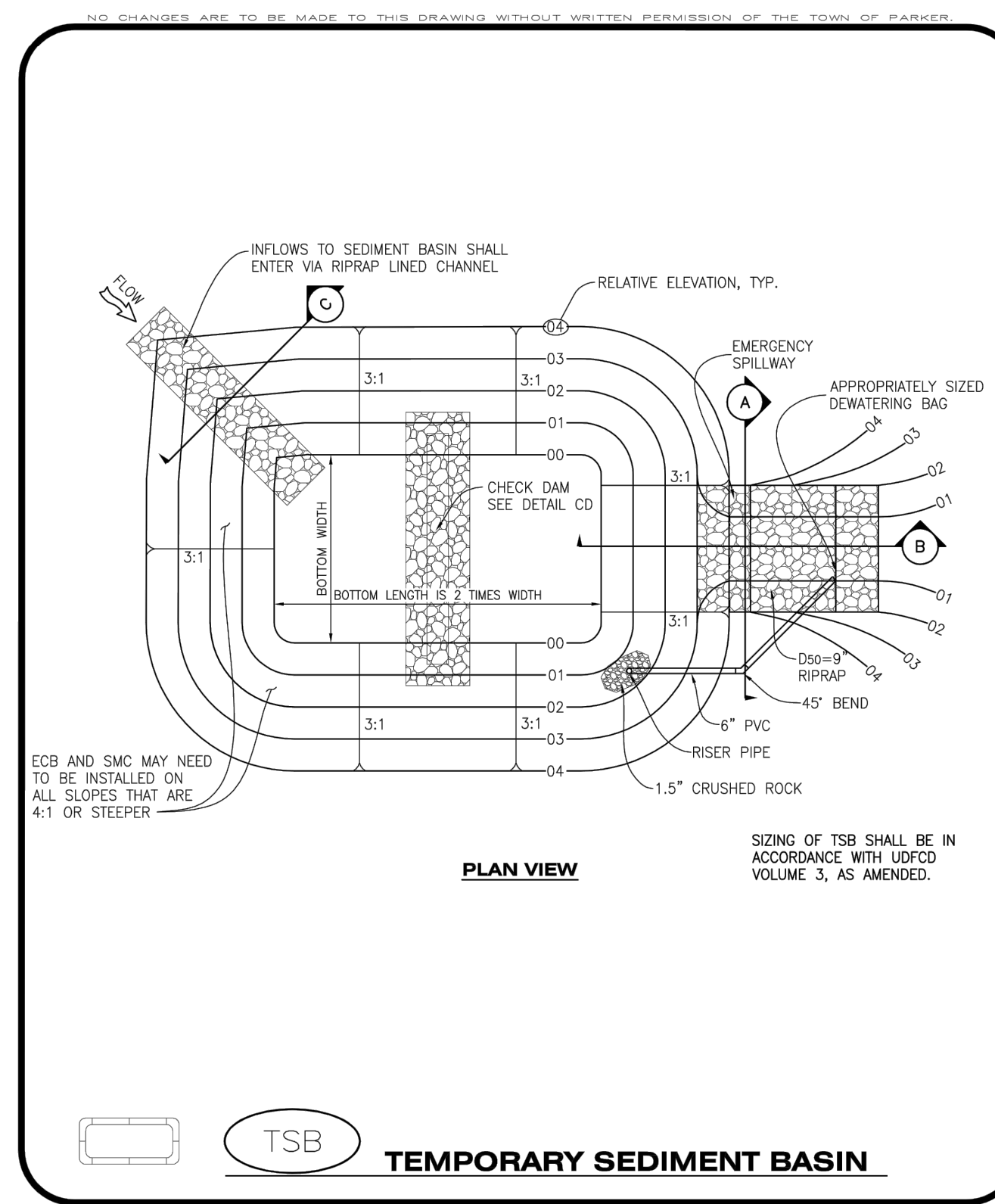
**SIDEWALK TRANSITION PROTECTION INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF SIDEWALK TRANSITION PROTECTION.
- ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
- SILT FENCE SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL SF).
- CONSTRUCTION FENCE SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL CF).
- SEDIMENT CONTROL LOGS MAY BE USED IN LIEU OF SILT FENCE WITH PRIOR APPROVAL FROM THE TOWN.

**SIDEWALK TRANSITION PROTECTION INSPECTION & MAINTENANCE NOTES**

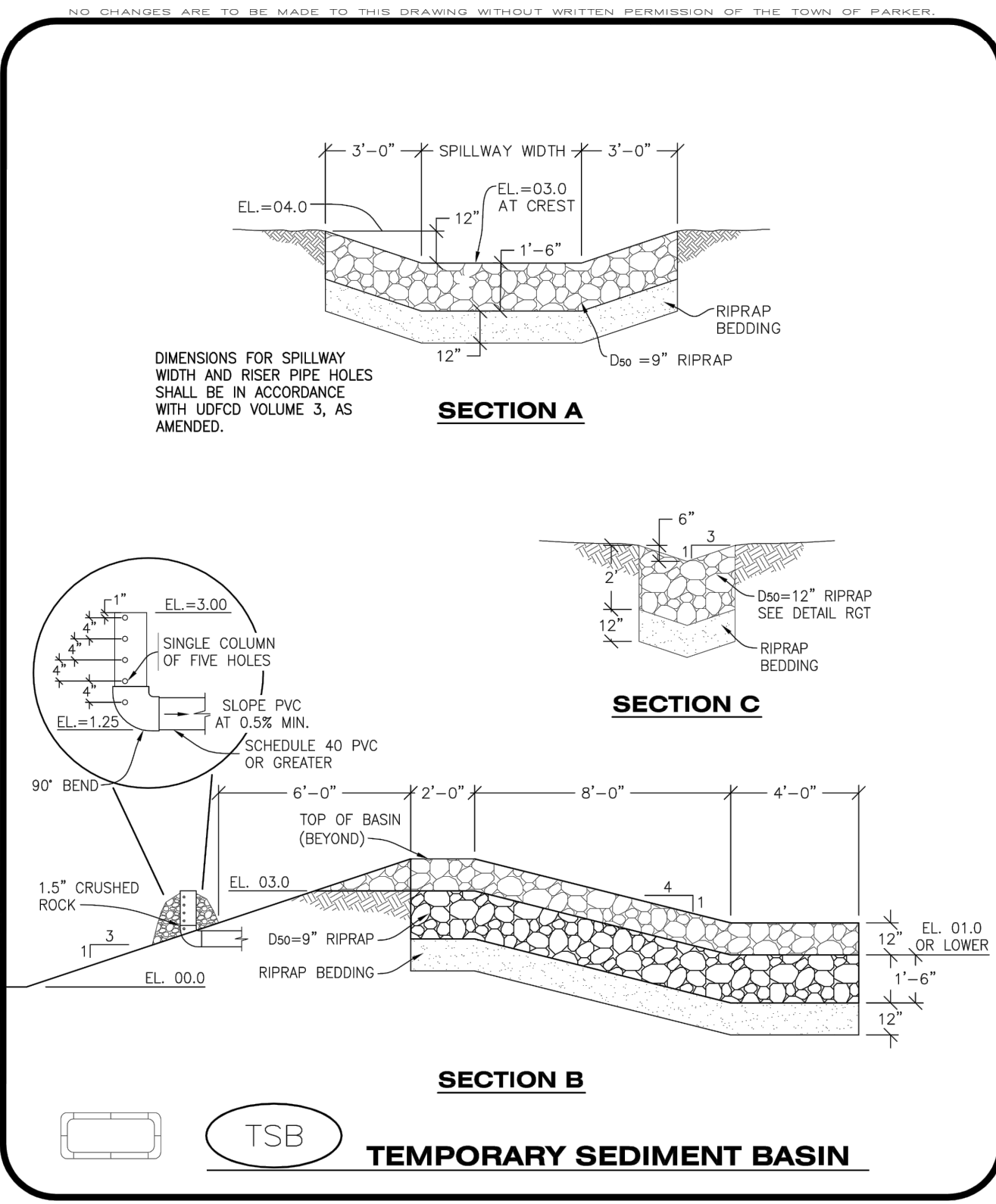
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SIDEWALK TRANSITION INSPECTION.

**CBMP** | **STP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
 Oct. 2013



**TSB**  
**TEMPORARY SEDIMENT BASIN**

**CBMP** | **TSB**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
 Oct. 2013



**TSB**  
**TEMPORARY SEDIMENT BASIN**

**CBMP** | **TSB**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
 Oct. 2013

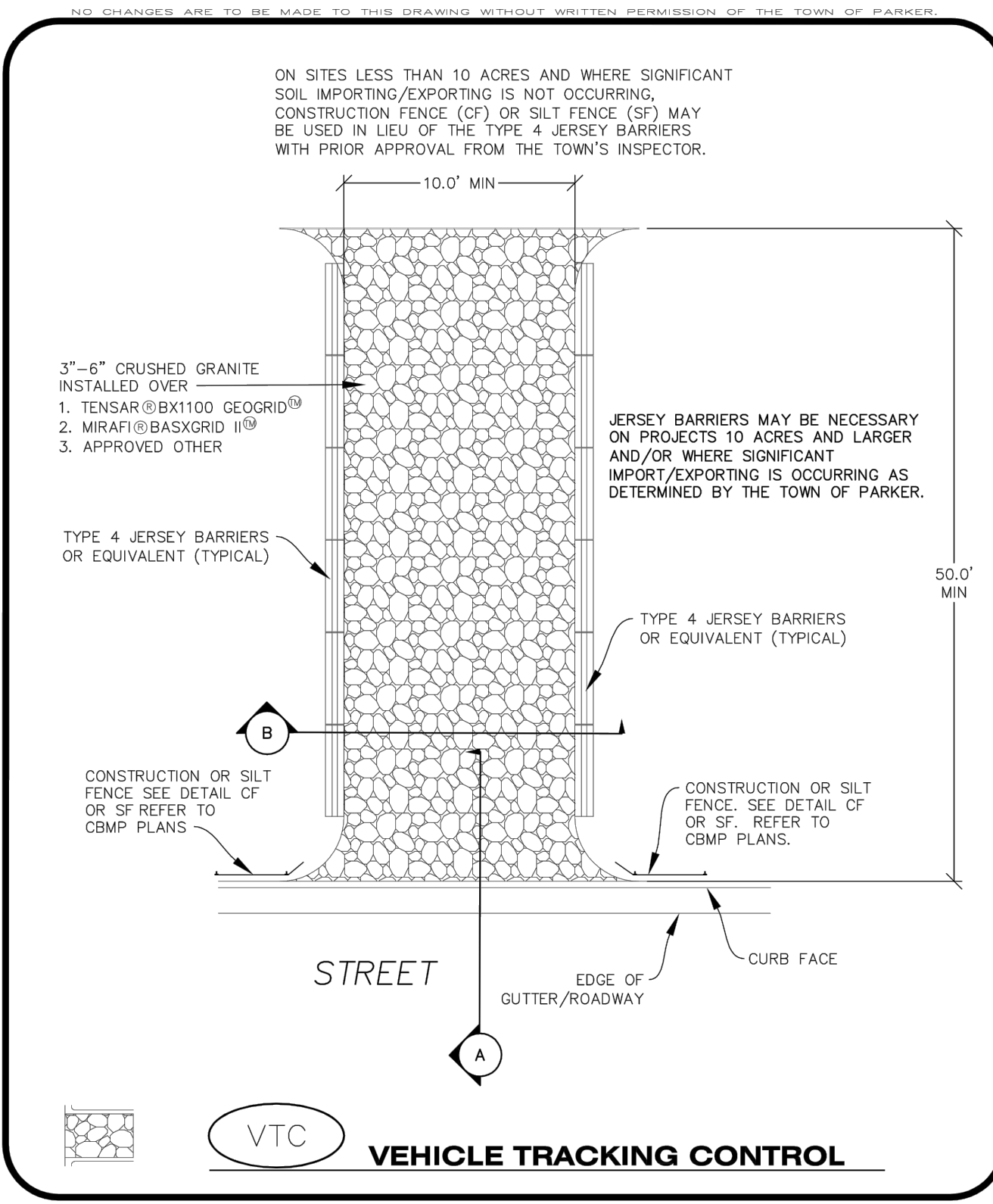
**TEMPORARY SEDIMENT BASIN INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT BASIN(S).
- THE TEMPORARY SEDIMENT BASIN(S) SHALL BE INSTALLED AND FUNCTIONING PRIOR TO ANY OTHER GRADING ACTIVITIES.
- THE EXACT DIMENSIONS AND DETAILS OF THE TEMPORARY SEDIMENT BASIN SHALL BE DETERMINED BY THE DESIGN ENGINEER, IN ACCORDANCE WITH UDFCD VOLUME 3, AS AMENDED.
- EMBANKMENT MATERIAL SHALL CONSIST OF SOIL FREE OF DEBRIS, ORGANIC MATERIAL, AND ROCKS OR CONCRETE GREATER THAN 3" AND SHALL HAVE A MINIMUM OF 15% BY WEIGHT PASSING THE NO. 200 SIEVE.
- EMBANKMENT MATERIAL SHALL BE COMPACTED TO A MINIMUM OF 95% DENSITY, AND WITHIN +/- 2% OF OPTIMUM MOISTURE IN ACCORDANCE WITH ASTM D698.
- AN APPROPRIATELY SIZED DEWATERING BAG SHALL BE SECURED TO THE END OF THE DISCHARGE PIPE. THE DEWATERING BAG SHALL BE REPLACED ONCE SEDIMENT ACCUMULATION REACHES 50%.

**TEMPORARY SEDIMENT BASIN INSPECTION AND MAINTENANCE NOTES**

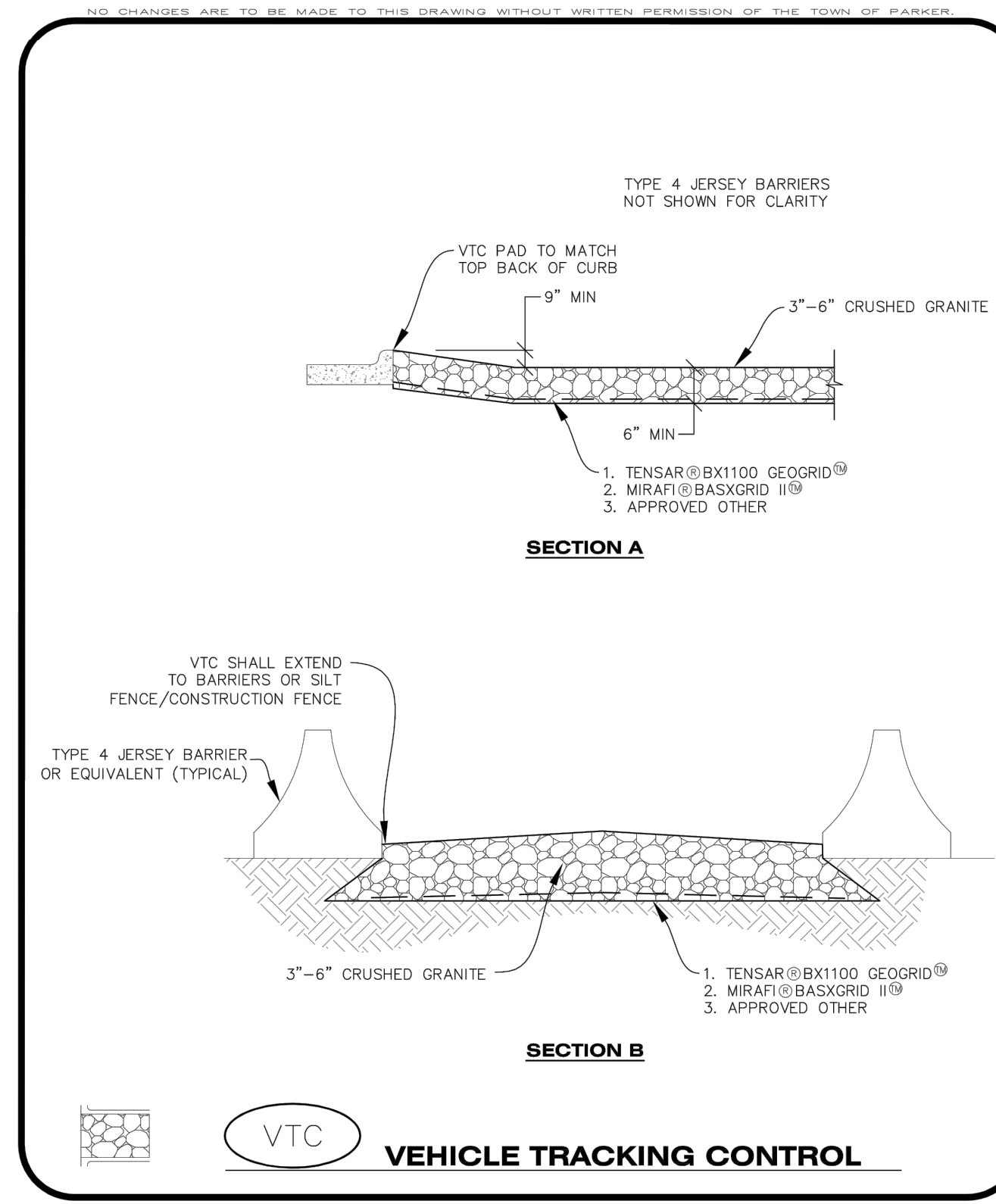
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE TEMPORARY SEDIMENT BASIN.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT REACHES A DEPTH OF 2.0', OR WITHIN 2.0' OF THE SPILLWAY CREST, OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- SEDIMENT BASINS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL UPSTREAM VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR.

**CBMP** | **TSB**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
 Oct. 2013



**VTC**  
**VEHICLE TRACKING CONTROL**

**CBMP** | **VTC**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
 Oct. 2013



**VTC**  
**VEHICLE TRACKING CONTROL**

**CBMP** | **VTC**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
 Oct. 2013

NO.	REVISION	BY	DATE

**Kimley»Horn**  
 2017 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: CTM  
 CHECKED BY: DLS  
 DATE: 8/17/17

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**CBMP DETAILS**

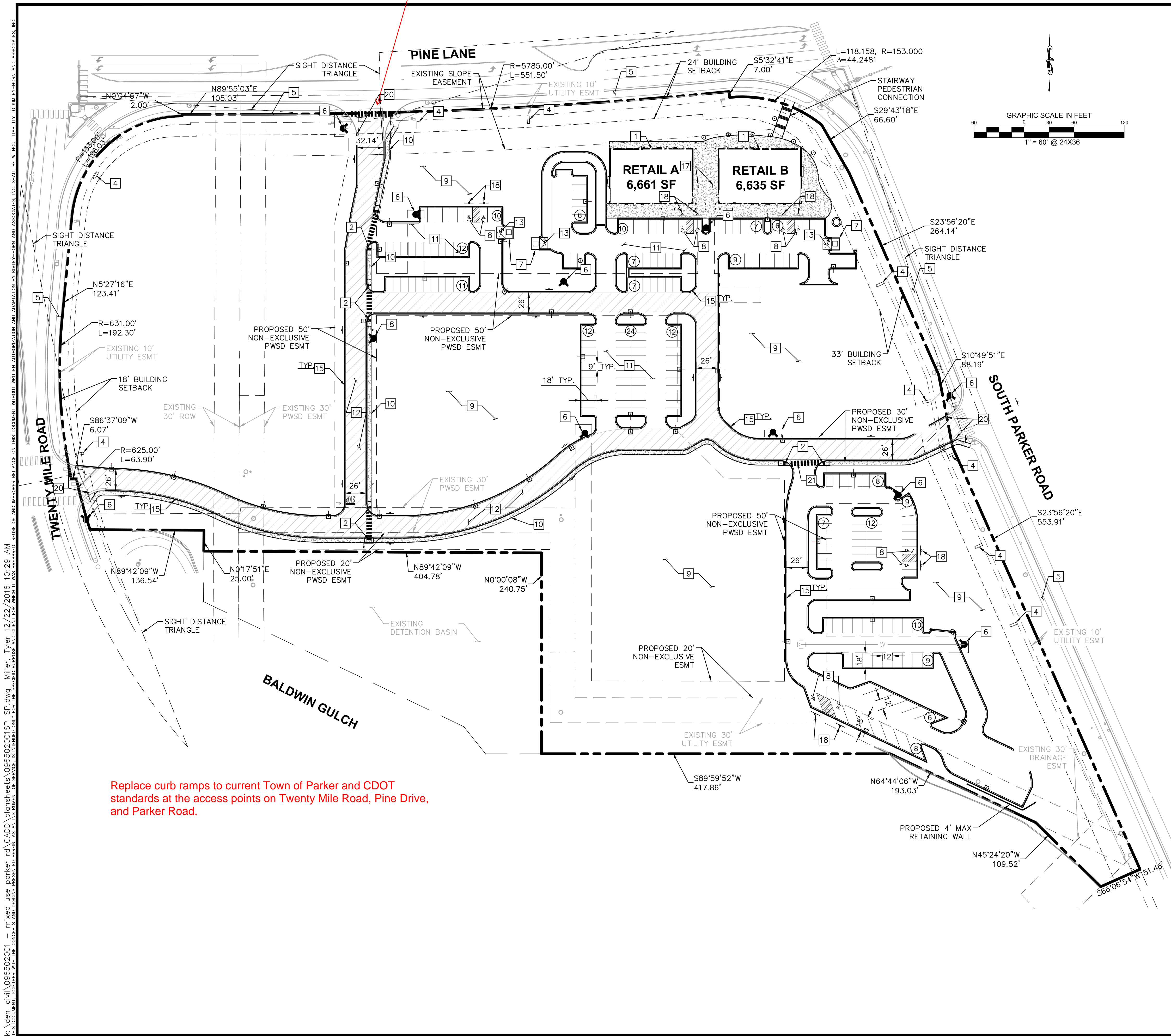
**PRELIMINARY**  
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**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001EC\_DT  
**C3.10**





Per previous discussions with Town staff there will be no Left Turn allowed. Show striping, signage, and median limiting the turning movements.



**LEGEND**

- PROPERTY LINE
- ADA ROUTE
- EASEMENT
- BUILDING SETBACK
- NUMBER OF PARKING SPACES
- PROPOSED FIRE HYDRANT
- ACCESSIBLE PARKING SPACE
- HEAVY DUTY ASPHALT PAVING
- STANDARD DUTY ASPHALT PAVING

- KEY NOTES**
- 1 PROPOSED BUILDINGS
  - 2 PROPOSED ACCESSIBLE RAMP W/ 36" DEEP DETECTABLE WARNING
  - 3 PROPOSED 5' WIDE PEDESTRIAN CONNECTION.
  - 4 PROPOSED MONUMENT SIGN.
  - 5 EXISTING 6' SIDEWALK TO REMAIN.
  - 6 PROPOSED FIRE HYDRANT.
  - 7 PROPOSED TRASH ENCLOSURE.
  - 8 PROPOSED ADA PARKING STALLS.
  - 9 PROPOSED PERMANENT STABILIZATION. REFER TO LANDSCAPE PLANS
  - 10 PROPOSED 5' CONCRETE SIDEWALK.
  - 11 PROPOSED STANDARD DUTY ASPHALT PAVING.
  - 12 PROPOSED HEAVY DUTY ASPHALT PAVING.
  - 13 PROPOSED HEAVY DUTY CONCRETE PAVING.
  - 14 PROPOSED SITE LIGHTING.
  - 15 PROPOSED 18" VERTICAL CURB
  - 16 PROPOSED BOLLARD.
  - 17 PROPOSED BICYCLE PARKING (4).
  - 18 PROPOSED BOLLARD-MOUNTED ADA PARKING SIGN.
  - 19 NOT USED
  - 20 PROPOSED CURB TRANSITION FROM 18" CURB AND GUTTER (ON-SITE) TO 30" CURB AND GUTTER (OFF-SITE).
  - 21 PROPOSED CROSSSPAN GUTTER

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

Replace curb ramps to current Town of Parker and CDOT standards at the access points on Twenty Mile Road, Pine Drive, and Parker Road.

L:\parker\096502001 - revised user: parker (A:\CADD\lhp\lhp\096502001.SP\_04.dwg Miller, T:\12/12/2016 10:29 AM  
 THIS DOCUMENT, TOGETHER WITH THE CONCEPTS AND DESIGNS PRESENTED HEREIN, AS AN INSTRUMENT OF SERVICE, IS INTENDED ONLY FOR THE SPECIFIC PROJECT AND CLIENT TO WHOM IT WAS PREPARED. REUSE OF AND IMPROPER RELIANCE ON THIS DOCUMENT WITHOUT WRITTEN AUTHORIZATION AND ADAPTATION BY KIMLEY-HORN AND ASSOCIATES, INC. SHALL BE WITHOUT LIABILITY TO KIMLEY-HORN AND ASSOCIATES, INC.

NO.	REVISION	BY	DATE	APPROVED

**Kimley»Horn**  
 2017 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: CTM  
 CHECKED BY: DLS  
 DATE: 8/17/17

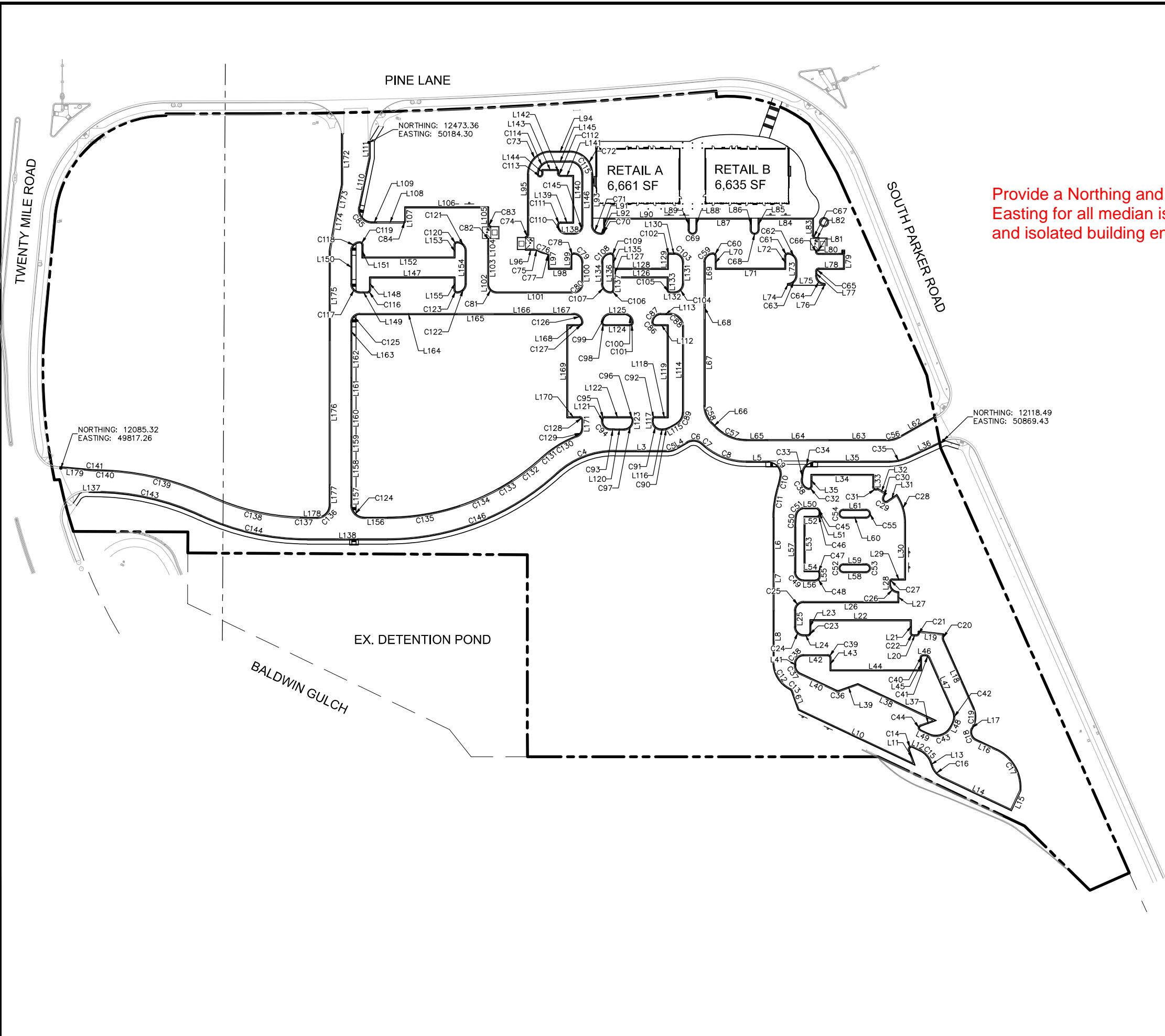
**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**SITE PLAN**

**PRELIMINARY**  
 FOR REVIEW ONLY  
 NOT FOR  
 CONSTRUCTION  
**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

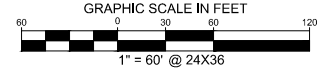
PROJECT NO.  
096502001  
 DRAWING NAME  
096502001SP\_SP  
**C4.0**



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Provide a Northing and Easting for all median islands and isolated building enclaves



--- PROPERTY LINE  
 --- PROPOSED CONCRETE CURB  
 --- EXISTING CURB

**HORIZONTAL CONTROL NOTES**

1. REFER TO SHEETS C4.2 FOR HORIZONTAL CONTROL LINE AND CURVE TABLES

**BASIS OF BEARINGS**

BEARINGS SHOWN HEREON ARE BASED UPON THE WEST LINE OF THE SW 1/4 OF SECTION 10, TOWNSHIP 6 SOUTH, RANGE 66 WEST, OF THE 6TH PRINCIPAL MERIDIAN BEARING S00°17'51"W AS REFERENCED ON THE PLAT OF PARKER AUTO PLAZA FILING NO.1 AND BOUND BY THE MONUMENTS SHOWN HEREON.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR.

**Kimley»Horn**  
 2017 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: CTM  
 CHECKED BY: DLS  
 DATE: 8/17/17

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**HORIZONTAL CONTROL PLAN**

PRELIMINARY  
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 CONSTRUCTION

**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

PROJECT NO.  
 096502001

DRAWING NAME  
 096502001HC

C4.1



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Line Table		
Line #	Length	Direction
L3	65.136	N90° 00' 00.00"E
L4	16.864	N60° 00' 00.00"E
L5	25.896	N90° 00' 00.00"E
L6	64.979	S0° 00' 00.00"E
L7	46.000	S0° 00' 00.00"E
L8	61.500	S0° 00' 00.00"E
L9	24.222	S19° 44' 06.00"E
L10	152.735	S64° 44' 06.00"E
L11	18.213	N19° 44' 06.00"W
L12	9.728	S64° 44' 06.00"E
L13	4.745	S19° 44' 06.00"E
L14	89.968	S64° 44' 06.00"E
L15	25.981	S25° 15' 54.00"W
L16	26.907	N64° 44' 06.00"W
L17	3.755	N25° 15' 54.00"E
L18	87.659	N23° 56' 20.00"W
L19	30.538	N83° 41' 57.14"W
L20	3.257	N90° 00' 00.00"E
L21	15.000	S0° 00' 00.00"E
L22	120.000	N90° 00' 00.00"E
L23	15.000	N0° 00' 00.00"E
L24	4.808	N90° 00' 00.00"E
L25	19.348	S0° 00' 00.00"E
L26	112.855	N90° 00' 00.00"W
L27	12.000	S0° 00' 00.00"E
L28	1.966	N0° 00' 00.00"E
L29	15.000	S90° 00' 00.00"E
L30	68.038	N0° 00' 00.00"E
L31	13.533	S55° 36' 07.19"W
L32	11.344	N58° 34' 33.89"W
L33	14.642	S0° 00' 00.00"E
L34	74.000	N90° 00' 00.00"E
L35	13.731	N0° 00' 00.00"E
L36	88.703	N90° 00' 00.00"W
L37	49.709	S65° 11' 41.01"W
L38	18.213	N70° 15' 54.00"E
L39	101.823	N64° 44' 06.00"W
L40	23.385	S70° 15' 54.00"W
L41	47.520	N64° 44' 06.00"W
L42	2.575	N0° 00' 00.00"E
L43	28.808	N90° 00' 00.00"E
L44	15.000	S0° 00' 00.00"E
L45	108.000	N90° 00' 00.00"E
L46	15.000	N0° 00' 00.00"E
L47	2.225	N90° 00' 00.00"E
L48	70.154	S23° 56' 20.00"E
L49	5.345	S25° 15' 54.00"W
L50	10.845	N64° 44' 06.00"W
L51	12.961	N90° 00' 00.00"E
L52	2.000	S0° 00' 00.00"E
L53	15.000	N90° 00' 00.00"W
L54	65.000	S0° 00' 00.00"E
L55	15.000	N90° 00' 00.00"E
L56	3.000	S0° 00' 00.00"E
L57	13.855	N90° 00' 00.00"W
L58	53.348	N0° 00' 00.00"E
L59	27.000	N90° 00' 00.00"W
L60	27.000	N90° 00' 00.00"E
L61	27.000	N90° 00' 00.00"E

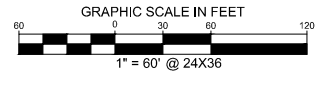
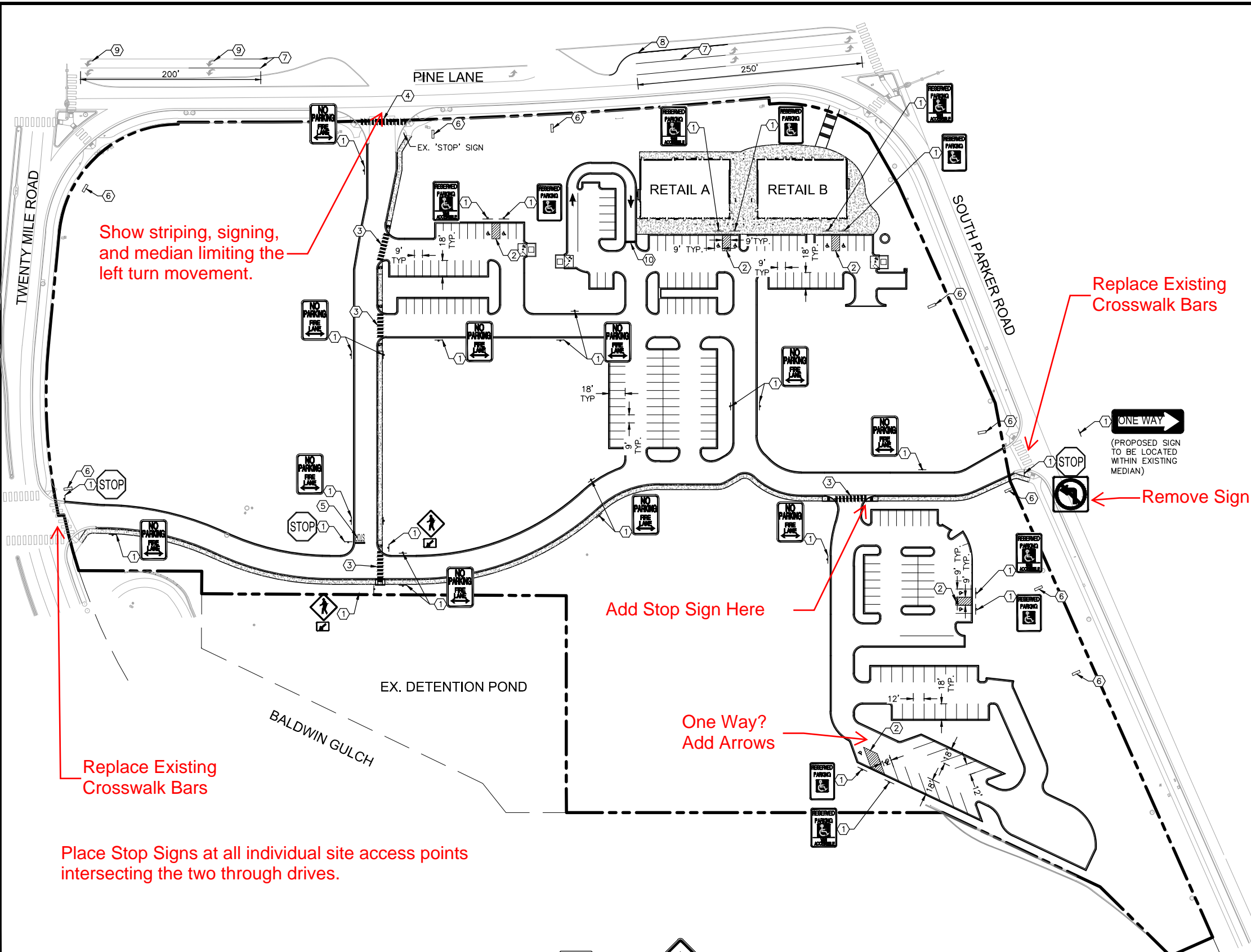
Line Table		
Line #	Length	Direction
L62	48.955	S60° 26' 52.61"W
L63	71.362	N90° 00' 00.00"W
L64	73.000	N90° 00' 00.00"W
L65	25.459	N90° 00' 00.00"W
L66	7.635	N45° 00' 00.00"W
L67	102.744	N0° 00' 00.00"E
L68	42.000	S0° 00' 00.00"E
L69	26.000	S0° 00' 00.00"E
L70	15.000	N0° 00' 00.00"E
L71	83.000	N90° 00' 00.00"W
L72	15.000	S0° 00' 00.00"E
L73	16.000	N0° 00' 00.00"E
L74	1.000	S0° 00' 00.00"E
L75	44.000	N90° 00' 00.00"E
L76	1.000	N0° 00' 00.00"E
L77	5.000	S0° 00' 00.00"E
L78	30.000	N90° 00' 00.00"W
L79	18.000	S0° 00' 00.00"E
L80	31.000	N90° 00' 00.00"E
L81	1.000	N90° 00' 00.00"W
L82	4.000	N90° 00' 00.00"W
L83	20.000	N0° 00' 00.00"E
L84	65.000	N90° 00' 00.00"W
L85	13.500	S0° 00' 00.00"E
L86	13.500	N0° 00' 00.00"E
L87	65.000	N90° 00' 00.00"W
L88	13.500	S0° 00' 00.00"E
L89	13.500	N0° 00' 00.00"E
L90	101.000	N90° 00' 00.00"W
L91	15.000	S0° 00' 00.00"E
L92	1.000	S90° 00' 00.00"W
L93	60.868	N0° 00' 00.00"E
L94	22.712	N90° 00' 00.00"W
L95	72.077	S0° 00' 00.00"E
L96	12.337	N90° 00' 00.00"E
L97	15.194	S0° 00' 00.00"E
L98	27.000	N90° 00' 00.00"E
L99	15.000	N0° 00' 00.00"E
L100	26.000	S0° 00' 00.00"E
L101	92.124	N90° 00' 00.00"W
L102	2.282	N0° 00' 00.00"E
L103	33.234	S0° 00' 00.00"E
L104	15.183	S0° 00' 00.00"E
L105	19.791	N0° 00' 00.00"E
L106	99.000	N90° 00' 00.00"W
L107	16.000	S0° 00' 00.00"W
L108	31.729	N90° 00' 00.00"E
L109	5.918	N90° 00' 00.00"W
L110	59.828	N11° 38' 07.76"E
L111	20.000	N0° 00' 00.00"E
L112	15.000	N90° 00' 00.00"E
L113	11.000	N90° 00' 00.00"W
L114	102.262	N0° 00' 00.00"E
L115	10.158	N60° 00' 00.00"E
L116	2.203	N90° 00' 00.00"E
L117	1.341	S0° 00' 00.00"E
L118	15.000	N90° 00' 00.00"W
L119	110.000	S0° 00' 00.00"E
L120	11.476	N90° 00' 00.00"W
L121	1.401	N0° 00' 00.00"E

Line Table		
Line #	Length	Direction
L122	30.000	N90° 00' 00.00"E
L123	1.341	S0° 00' 00.00"E
L124	30.000	N90° 00' 00.00"W
L125	16.000	N90° 00' 00.00"E
L126	63.000	N90° 00' 00.00"W
L127	10.000	N0° 00' 00.00"E
L128	63.000	N90° 00' 00.00"E
L129	15.000	N0° 00' 00.00"E
L130	5.000	N90° 00' 00.00"E
L131	26.000	S0° 00' 00.00"E
L132	5.000	N90° 00' 00.00"W
L133	15.000	N0° 00' 00.00"E
L134	26.000	N0° 00' 00.00"E
L135	15.000	S0° 00' 00.00"E
L136	10.000	S0° 00' 00.00"E
L137	6.145	S88° 56' 41.38"E
L138	15.000	S0° 00' 00.00"E
L138	95.566	N90° 00' 00.00"E
L138	8.712	N90° 00' 00.00"W
L139	15.000	N90° 00' 00.00"E
L140	56.000	N0° 00' 00.00"E
L141	15.000	N90° 00' 00.00"W
L142	4.000	N0° 00' 00.00"E
L143	19.000	N90° 00' 00.00"W
L144	5.132	S0° 00' 00.00"E
L145	22.712	S90° 00' 00.00"E
L146	60.868	S0° 00' 00.00"E
L147	101.000	N90° 00' 00.00"E
L148	14.941	N0° 00' 00.00"E
L149	9.000	N90° 00' 00.00"E
L150	39.490	S0° 00' 00.00"W
L151	15.000	N0° 00' 00.00"E
L152	110.000	N90° 00' 00.00"W
L153	15.000	S0° 00' 00.00"W
L154	39.490	N0° 00' 00.00"E
L155	14.941	S0° 00' 00.00"W
L156	28.119	N90° 00' 00.00"E
L157	21.605	S0° 00' 00.00"W
L158	44.000	S0° 00' 00.00"W
L159	16.000	S0° 00' 00.00"W
L160	44.000	S0° 00' 00.00"W
L161	26.555	S0° 00' 00.00"W
L162	44.000	S0° 00' 00.00"W
L163	21.445	S0° 00' 00.00"W
L164	116.000	N90° 00' 00.00"W
L165	44.000	N90° 00' 00.00"W
L166	66.098	N90° 00' 00.00"E
L167	29.026	N90° 00' 00.00"E
L168	15.000	N90° 00' 00.00"W
L169	110.000	S0° 00' 00.00"E
L170	15.000	N90° 00' 00.00"E
L171	10.106	S0° 00' 00.00"E
L172	61.112	S0° 00' 00.00"E
L173	33.591	S11° 38' 07.76"W
L174	46.643	S9° 43' 41.42"W
L175	106.990	S0° 00' 00.00"W
L176	171.000	S0° 00' 00.00"W
L177	25.161	S0° 00' 00.00"W
L178	11.446	N90° 00' 00.00"W
L179	37.167	N84° 51' 01.91"W

Curve Table			
Curve #	Length	Radius	Delta
C4	74.339	100.000	42.5930
C5	7.854	15.000	30.0000
C6	19.271	15.000	73.6093
C7	5.983	76.000	4.5105
C8	51.863	76.000	39.0988
C9	25.637	15.000	97.9271
C10	12.112	74.000	9.3775
C11	38.055	126.000	17.3046
C12	40.045	36.000	63.7334
C13	2.304	3.000	43.9984
C14	7.069	3.000	135.0000
C15	19.635	25.000	45.0000
C16	19.635	25.000	45.0000
C17	60.146	50.000	68.9226
C18	15.708	10.000	90.0000
C19	21.469	25.000	49.2039
C20	8.411	10.000	48.1897
C21	6.196	3.000	118.3359
C22	4.712	3.000	90.0000
C23	4.712	3.000	90.0000
C24	15.708	10.000	90.0000
C25	15.708	10.000	90.0000
C26	15.708	10.000	90.0000
C27	4.712	3.000	90.0000
C28	34.821	58.000	34.3980
C29	6.702	3.000	128.0030
C30	7.324	10.000	41.9613
C31	7.039	3.000	134.4337
C32	5.539	3.000	105.7803
C33	7.024	100.000	4.0245
C34	22.500	15.000	85.9426
C35	21.647	50.000	24.8053
C36	3.927	5.000	45.0000
C37	11.298	10.000	64.7350
C38	14.364	10.000	82.3016
C38	15.708	10.000	90.0000
C39	4.712	3.000	90.0000
C40	4.712	3.000	90.0000
C41	5.765	5.000	66.0611
C42	12.882	15.000	49.2039
C43	31.416	20.000	90.0000
C44	7.069	3.000	135.0000
C45	7.854	5.000	90.0000
C46	4.712	3.000	90.0000
C47	4.712	3.000	90.0000
C48	7.854	5.000	90.0000
C49	15.708	10.000	90.0000
C50	14.104	100.000	8.0812
C51	14.298	10.000	81.9188
C52	14.137	4.500	180.0000
C53	14.137	4.500	180.0000
C54	14.137	4.500	180.0000
C55	14.137	4.500	180.0000
C56	12.379	24.000	29.5521
C57	39.270	50.000	45.0000
C58	23.562	30.000	45.0000
C59	15.708	10.000	90.0000
C60	4.712	3.000	90.0000
C61	4.712	3.000	90.0000
C62	15.708	10.000	90.0000

Curve Table			
Curve #	Length	Radius	Delta
C63	15.708	10.000	90.0000
C64	15.708	10.000	90.0000
C65	4.712	3.000	90.0000
C66	6.283	2.000	180.0000
C67	4.712	3.000	90.0000
C68	14.137	4.500	180.0000
C69	14.137	4.500	180.0000
C70	4.712	3.000	90.0000
C71	15.708	10.000	90.0000
C72	42.412	27.000	90.0000
C73	42.412	27.000	90.0000
C74	4.712	3.000	90.0000
C75	1.781	3.000	34.0150
C76	11.899	25.000	27.2713
C77	4.359	3.000	83.2563
C78	4.712	3.000	90.0000
C79	15.708	10.000	90.0000
C80	15.708	10.000	90.0000
C81	15.708	10.000	90.0000
C82	4.712	3.000	90.0000
C83	4.712	3.000	90.0000
C84	3.142	2.000	90.0000
C85	26.608	15.000	101.6355
C86	4.712	3.000	90.0000
C87	15.708	10.000	90.0000
C88	23.562	15.000	90.0000
C89	15.708	15.000	60.0000
C90	7.854	15.000	30.0000
C91	15.708	10.000	90.0000
C92	4.712	3.000	90.0000
C93	4.228	126.000	1.9226
C94	16.005	10.000	91.6994
C95	4.712	3.000	

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**LEGEND**

- PROPERTY LINE
- ACCESSIBLE PARKING SPACES
- DIRECTIONAL ARROWS
- SIGN

**SIGNING AND STRIPING NOTES**

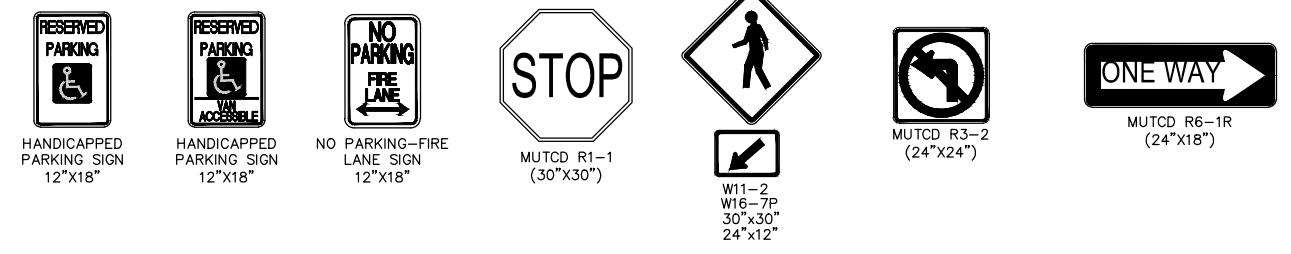
- ① ALL SIGNS AND ROADWAY MARKINGS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ② TWO COATS WHITE LATEX TRAFFIC PAINT (4" MIN. WIDTH) REQUIRED FOR PAVEMENT STRIPING EXCEPT TWO COATS BLUE TRAFFIC PAINT SHALL BE USED FOR HANDICAP PARKING AREA AND TWO COATS YELLOW TRAFFIC PAINT FOR FIRE LANE MARKINGS (18" LETTERS, 2" PAINT STROKE).
- ③ 6' CROSSWALK STRIPING SHALL BE PAINTED 2' WIDE SINGLE WHITE SOLID LINE AT 4'-0" O.C. PERPENDICULAR TO THE PATH OF TRAVEL
- ④ 10' CROSSWALK STRIPING SHALL BE 2' WIDE SOLID RECESS PREFORMED PLASTIC AT 4'-0" O.C. PERPENDICULAR TO THE PATH OF TRAVEL
- ⑤ 12" STOP BAR. SEE DETAIL ON SHEET C4.4
- ⑥ PROPOSED MONUMENT SIGN (REFER TO ARCH. PLANS)
- ⑦ EXTEND EXISTING 6" SOLID WHITE STRIPE **8" Solid White**
- ⑧ 6" SOLID DOUBLE YELLOW STRIPE **4" Double Yellow**
- ⑨ DIRECTIONAL PAVEMENT MARKING. SEE DETAIL SHEET C4.4
- ⑩ 'DO NOT ENTER/ONE WAY STRIPING.' SEE DETAIL ON SHEET C4.4

- NOTES:
1. SEE SIGN POST DETAIL ON SHEET C4.4 FOR INSTALLATION AND POST/BASE DIMENSIONS.
  2. ALL PARKING ADJACENT TO LANDSCAPE ISLANDS SHALL BE 10' WIDE FROM STRIPE TO FACE OF CURB.
  3. CONTRACTOR TO REMOVE ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE FINAL SIGNAGE AND STRIPING PLAN.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_



Show striping, signing, and median limiting the left turn movement.

Replace Existing Crosswalk Bars

Remove Sign

Add Stop Sign Here

One Way? Add Arrows

Replace Existing Crosswalk Bars

Place Stop Signs at all individual site access points intersecting the two through drives.

**Kimley-Horn**  
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 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

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 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**SIGNAGE AND STRIPING PLAN**

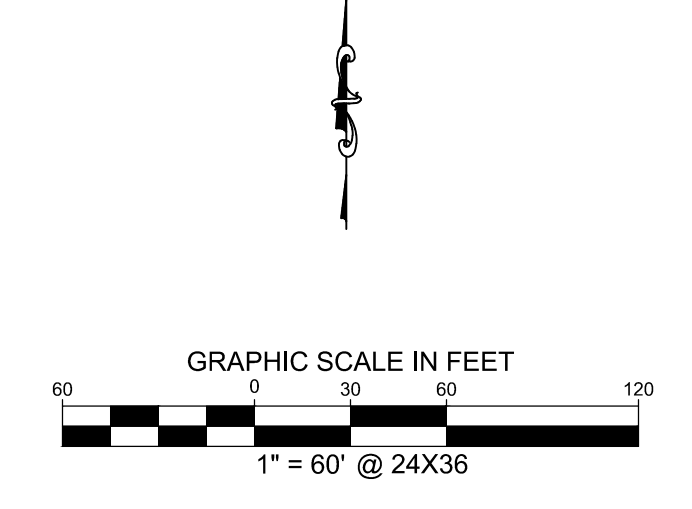
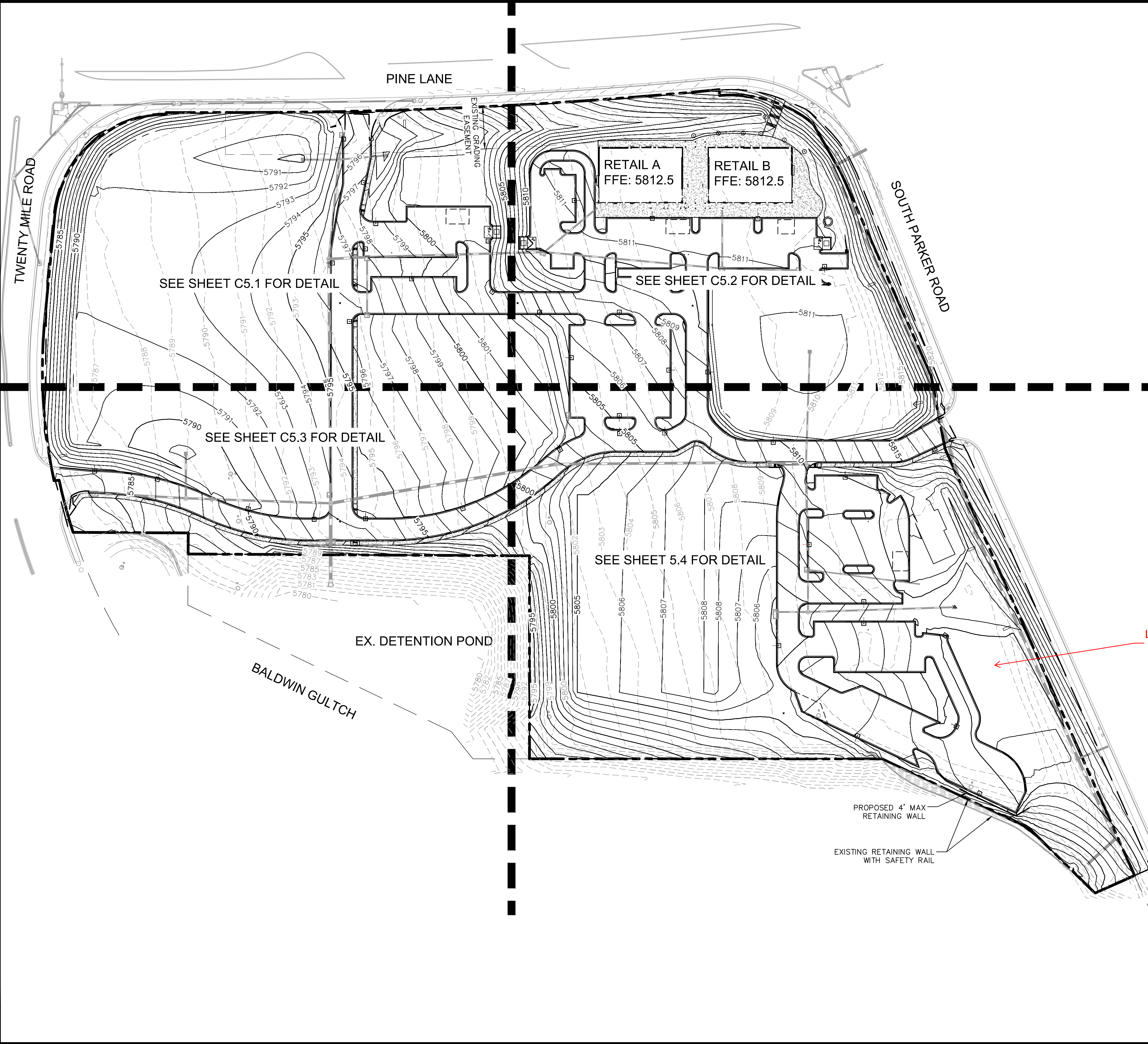
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**C4.3**





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**LEGEND**

	EXISTING PROPERTY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	PROPOSED STORM INLET
	DELINEATION OF ADA PARKING AND ACCESS. SLOPES NOT TO EXCEED 2% IN THIS AREA.

**BENCHMARK**

DOUGLAS CONTROL MONUMENT #1.095035, A 3 1/4" ALUMINUM CAP.

ELEVATION = 5906.34 FEET (NAVD 1988), AS PUBLISHED BY DOUGLAS COUNTY.

NO.	REVISION	BY	DATE	APPR

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**PARKER & PINE**  
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 CONSTRUCTION DOCUMENTS  
**OVERALL GRADING PLAN**

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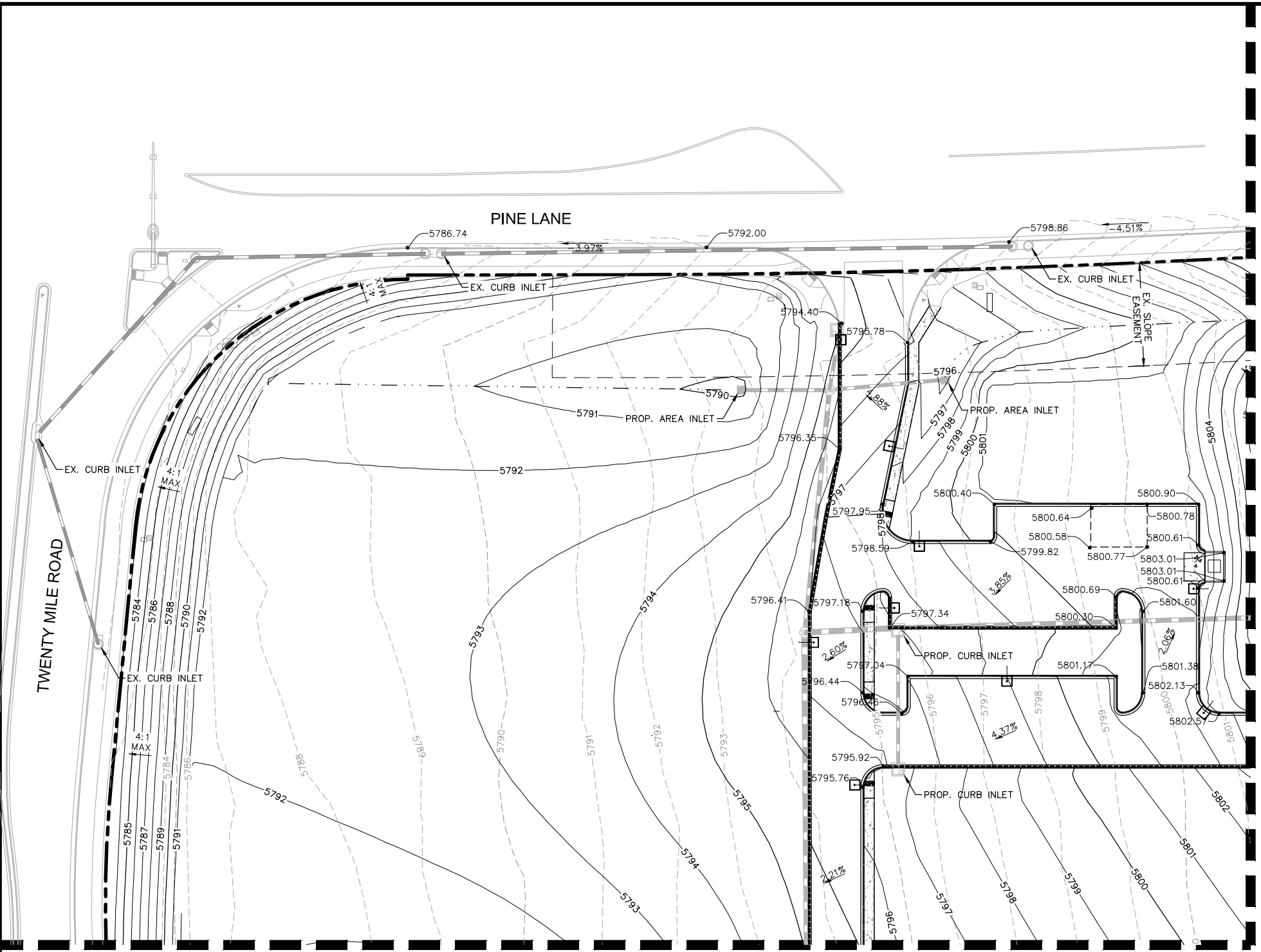
PROJECT NO.  
096502001

DRAWING NAME  
096502001 OGD

C5.0



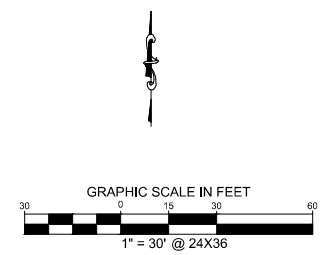
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SEE SHEET C5.3 FOR CONTINUATION

SEE SHEET C5.2 FOR CONTINUATION

Label all manhole, inlet, and pipe and inlet size and type on plan sheets consistent with the Drainage Report



- LEGEND**
- PROPERTY LINE
  - - - - - EXISTING CONTOUR
  - 5771 — PROPOSED CONTOUR
  - 1.00% → PROPOSED FLOW ARROW WITH SLOPE
  - 5771 ○ PROPOSED SPOT ELEVATION
  - ○ — EXISTING STORM SEWER
  - ○ — PROPOSED STORM SEWER
  - PROPOSED STORM INLET
  - TP TOP OF PAVEMENT
  - TS TOP OF SLAB (DUMPSTER PAD)
  - SW TOP OF SIDEWALK
  - ME MATCH EXISTING ELEVATION
  - DELINEATION OF ADA PARKING AND ACCESS, SLOPES NOT TO EXCEED 2% IN THIS AREA.
  - - - - - PROPOSED VALLEY
  - - - - - PROPOSED RIDGE
  - CATCH CURB

**BENCHMARK**  
 DOUGLAS CONTROL MONUMENT #1.095035, A 3 1/4" ALUMINUM CAP.  
 ELEVATION = 5906.34 FEET (NAVD 1988), AS PUBLISHED BY DOUGLAS COUNTY.

NO.	REVISION	BY	DATE	APPR.

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**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
 DETAILED GRADING PLAN

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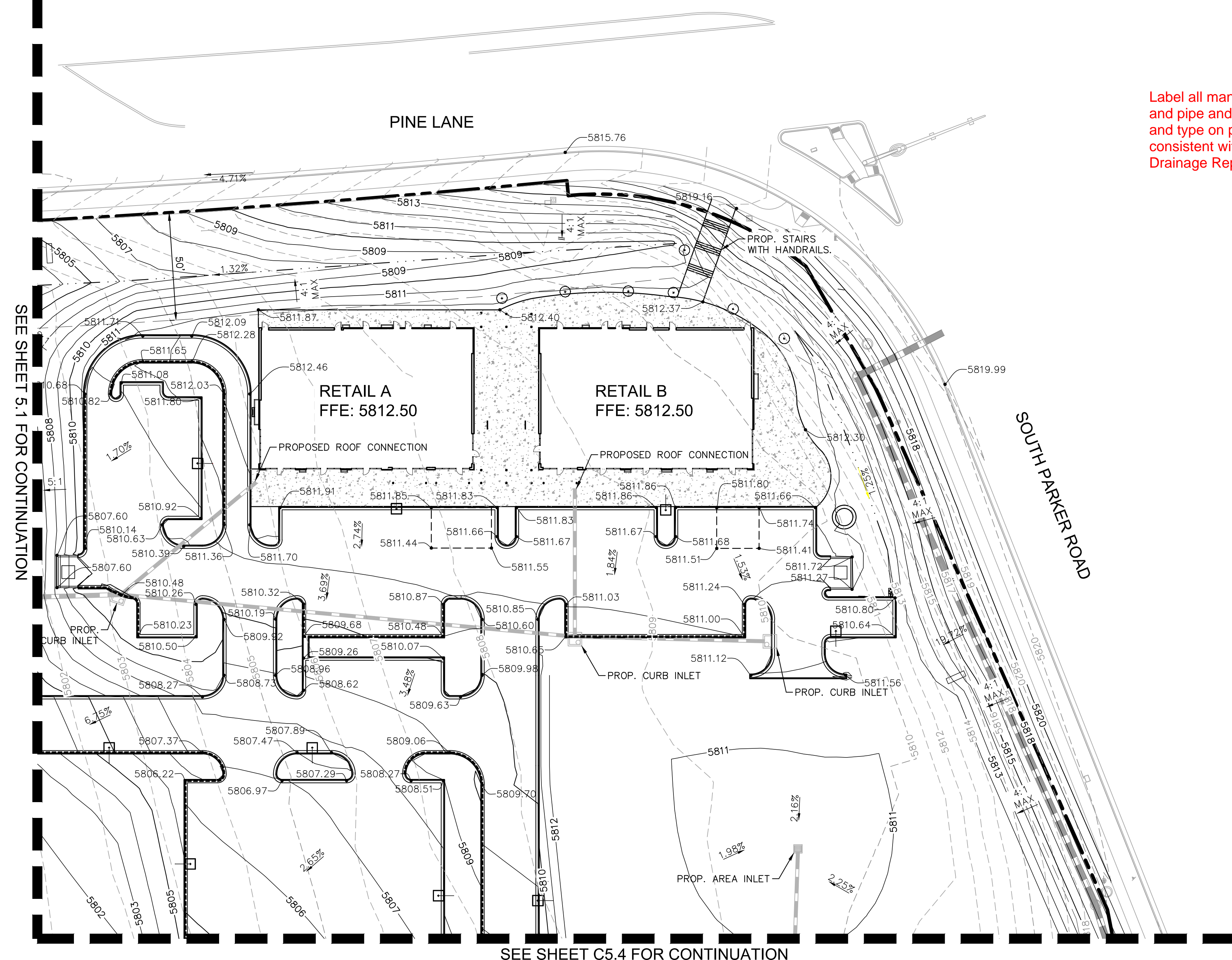
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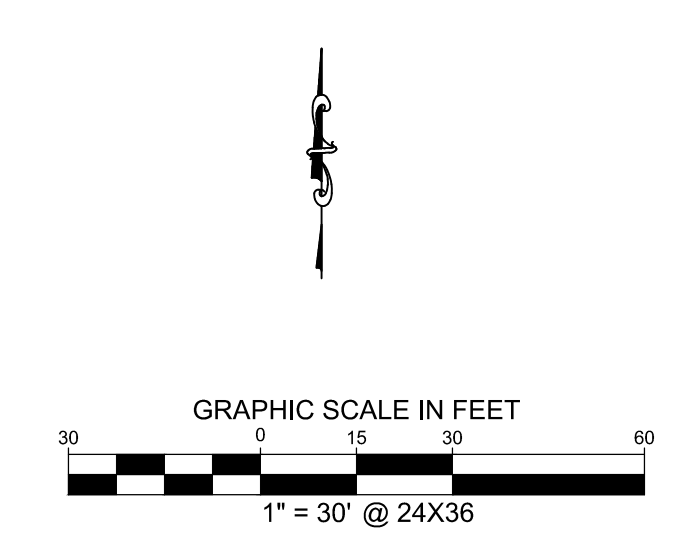
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Label all manhole, inlet, and pipe and inlet size and type on plan sheets consistent with the Drainage Report



**LEGEND**

	PROPERTY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED FLOW ARROW WITH SLOPE
	PROPOSED SPOT ELEVATION
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	PROPOSED STORM INLET
	TP TOP OF PAVEMENT
	TS TOP OF SLAB (DUMPSTER PAD)
	SW TOP OF SIDEWALK
	ME MATCH EXISTING ELEVATION
	DELINEATION OF ADA PARKING AND ACCESS. SLOPES NOT TO EXCEED 2% IN THIS AREA.
	PROPOSED VALLEY
	PROPOSED RIDGE
	CATCH CURB

**BENCHMARK**  
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 ELEVATION = 5906.34 FEET (NAVD 1988), AS PUBLISHED BY DOUGLAS COUNTY.

SEE SHEET S.1 FOR CONTINUATION

SEE SHEET C5.4 FOR CONTINUATION

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 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**DETAILED GRADING PLAN**

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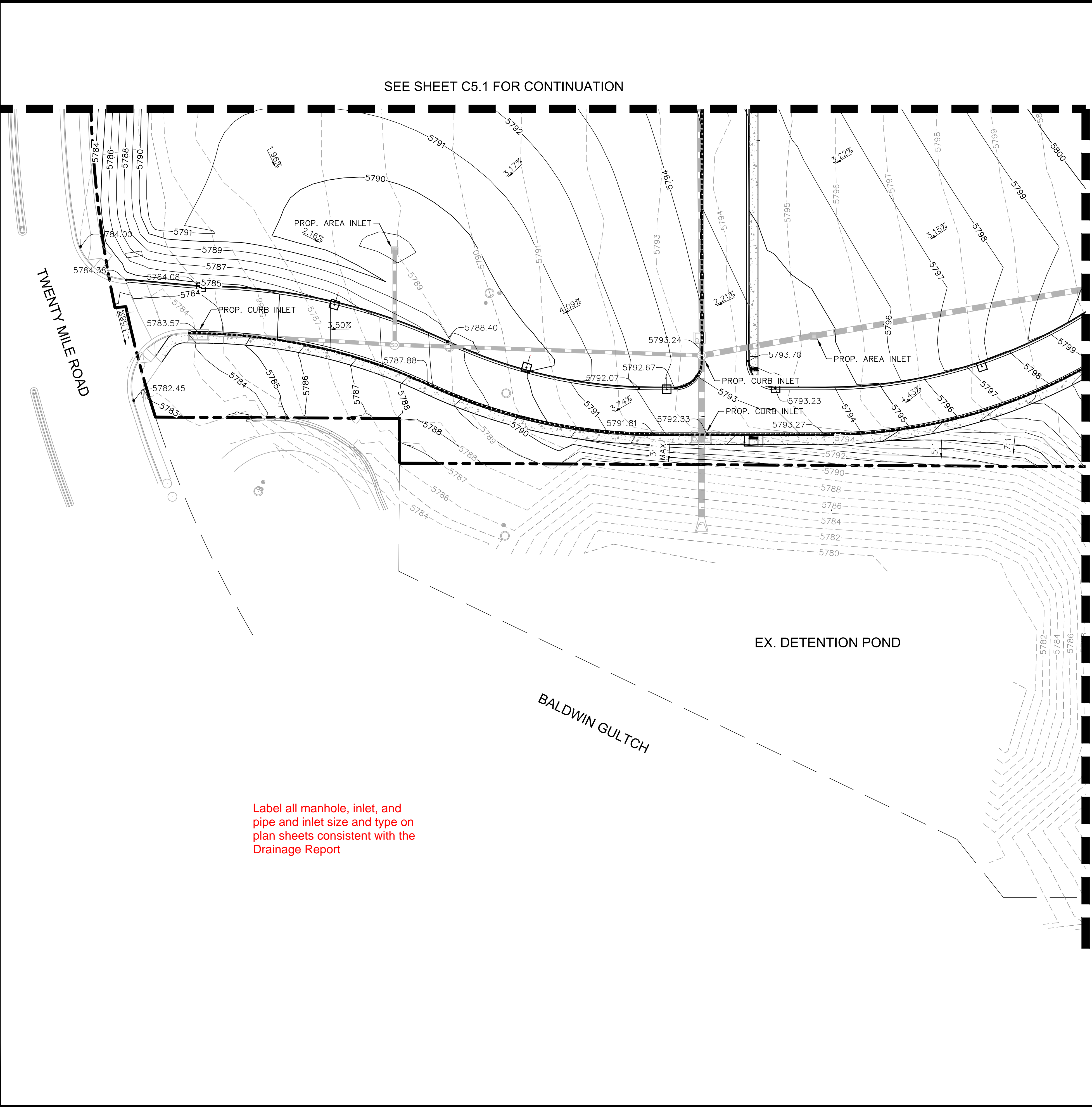
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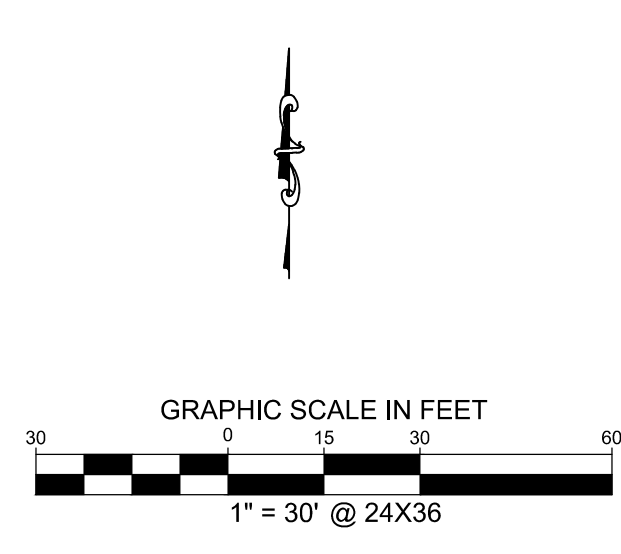
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Label all manhole, inlet, and  
 pipe and inlet size and type on  
 plan sheets consistent with the  
 Drainage Report



**LEGEND**

- PROPERTY LINE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPOSED FLOW ARROW WITH SLOPE
- PROPOSED SPOT ELEVATION
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
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**BENCHMARK**

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**MANHOLE BOX BASE**

MANHOLE SIZE	TYPE	NO. OF BARS	BAR SIZE	BAR LENGTH	FORMULAS
481	4	1	0.688	10.0	481 BAR LENGTH = 10" + 20" I.D.
482	4	8	0.688	10.0	482 BAR LENGTH = I.D. + 20"
501	5	1	1.043	10.0	501 BAR LENGTH = 24" + I.D. + 20"
502	5	1	1.043	10.0	502 BAR LENGTH = 24" + I.D. + 20"
503	5	8	1.043	10.0	503 NUMBER BARS REQ'D = 3 + (24" + I.D. + 20") / 3"
504	5	1	1.043	10.0	504 NUMBER BARS REQ'D = 2 + (24" + I.D. + 20") / 3"
1101	11	1	5.313	10.0	1101 BAR LENGTH = 21" + I.D. + 20"
1102	11	1	5.313	10.0	1102 BAR LENGTH = 21" + I.D. + 20"
1103	11	1	5.313	10.0	1103 BAR LENGTH = 21" + I.D. + 20"

**QUANTITIES FOR CONCRETE MANHOLE BOX BASE**

REINFORCING STEEL TOTAL	CONCRETE - CURB VOLUMES - TOTAL	CONCRETE - MANHOLE BOX BASE - TOTAL
596.8	1,037.5	1,074.0

**Computer File Information**  
 Creation Date: 07/04/06  
 Last Modification Date: 07/04/06  
 Drawing File Name: 604202003.dwg  
 CAD Ver.: MicroStation V8  
 Scale: Not to Scale  
 Units: English

**Sheet Revisions**

No.	Date	Comments
1		

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 Fax: (303) 757-9820  
 Project Development Branch SRJ/LTA

**MANHOLES**  
 STANDARD PLAN NO. M-604-20  
 Sheet No. 1 of 3

**MANHOLE RISER DETAIL**

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 Drawing File Name: 604202003.dwg  
 CAD Ver.: MicroStation V8  
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**Sheet Revisions**

No.	Date	Comments
1		

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**MANHOLES**  
 STANDARD PLAN NO. M-604-20  
 Sheet No. 2 of 3

**T-BASE MANHOLES**

**Computer File Information**  
 Creation Date: 07/04/06  
 Last Modification Date: 07/04/06  
 Drawing File Name: 604202003.dwg  
 CAD Ver.: MicroStation V8  
 Scale: Not to Scale  
 Units: English

**Sheet Revisions**

No.	Date	Comments
1		

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 Fax: (303) 757-9820  
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**MANHOLES**  
 STANDARD PLAN NO. M-604-20  
 Sheet No. 3 of 3

**CURB AND GUTTERS**

**Computer File Information**  
 Creation Date: 07/04/06  
 Last Modification Date: 09/16/04  
 Drawing File Name: 604202004.dwg  
 CAD Ver.: MicroStation V8  
 Scale: Not to Scale  
 Units: English

**Sheet Revisions**

No.	Date	Comments
1	07/24/02	Changed to show curbs from 30" to 36"
2	05/16/04	Revised gutter cross section to match All Time

**Colorado Department of Transportation**  
 4201 East Arkansas Avenue  
 Denver, Colorado 80222  
 Phone: (303) 757-9083  
 Fax: (303) 757-9820  
 Project Development Branch DLM/LTA

**CURB, GUTTERS, AND SIDEWALKS**  
 STANDARD PLAN NO. M-609-1  
 Sheet No. 1 of 4

**TYPICAL BOLLARD DETAIL**

3"  
 6"  
 2" WIDE WHITE REFLECTIVE TAPE  
 4" DIAM STEEL PIPE  
 PAINT POST YELLOW  
 GRADE  
 SET POST IN CONCRETE  
 18"  
 6"  
 12"

**CURB TRANSITION DETAIL**

6"  
 12"  
 24"  
 18"  
 BACK OF CURB  
 FACE OF CURB

Update all details to most current 2012 CDOT details. Include inlet specific details.

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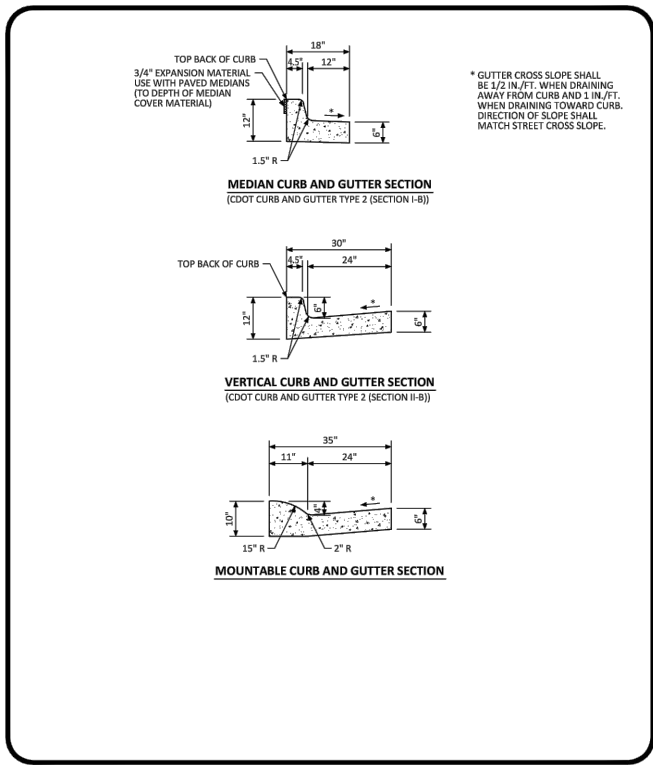
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 CONSTRUCTION DOCUMENTS  
 DETAILS

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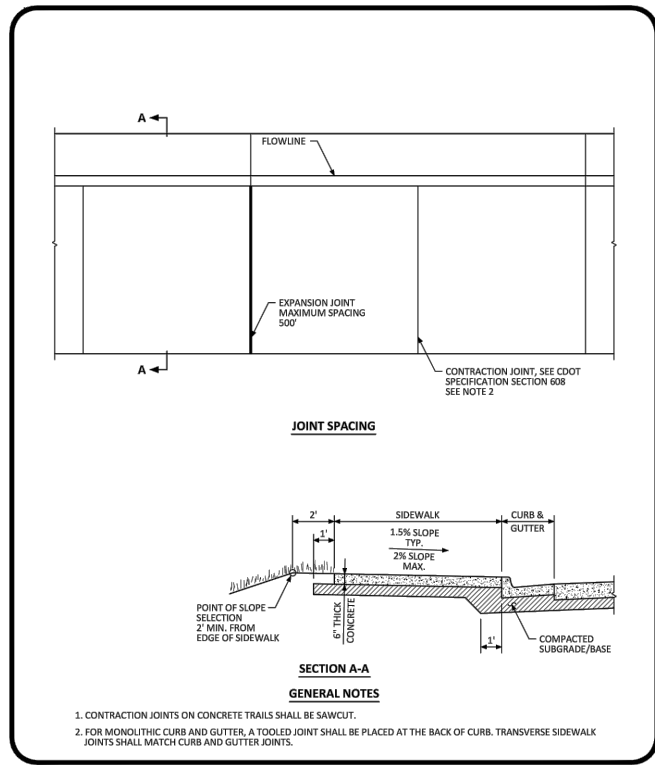
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**CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL**

DATE: AUGUST 2014

DETAIL: **3**

1 OF 1



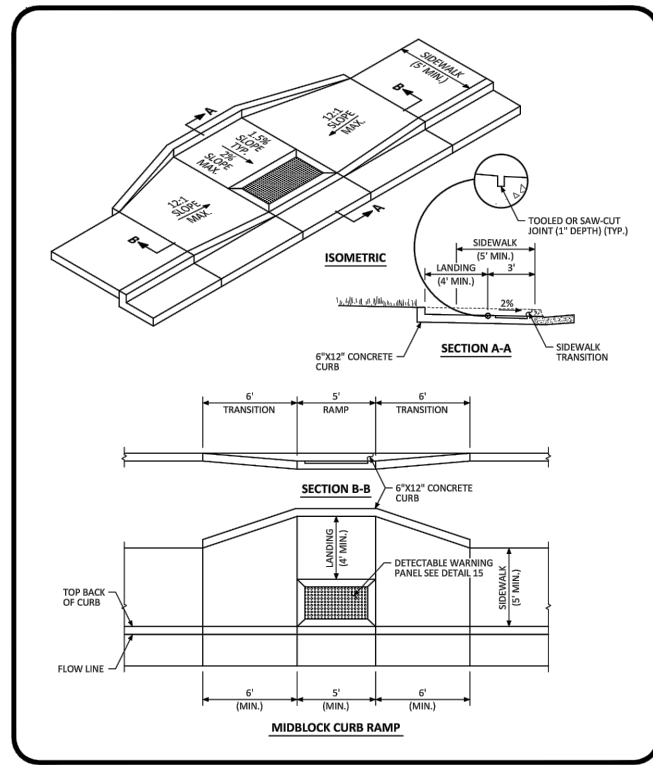
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**ATTACHED SIDEWALK STANDARD DETAIL**

DATE: AUGUST 2014

DETAIL: **4**

2 OF 2



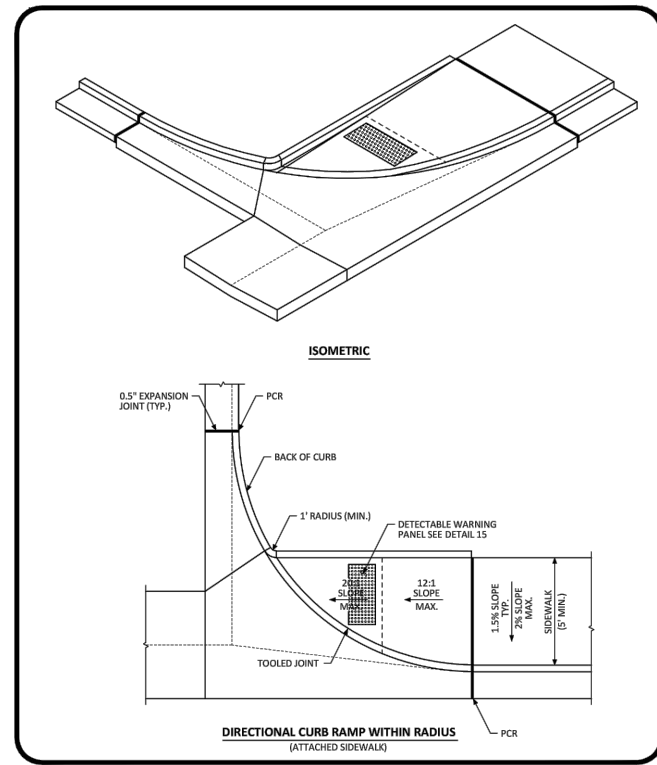
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**MIDBLOCK CURB RAMP LAYOUT STANDARD DETAIL**

DATE: AUGUST 2014

DETAIL: **17**

2 OF 2



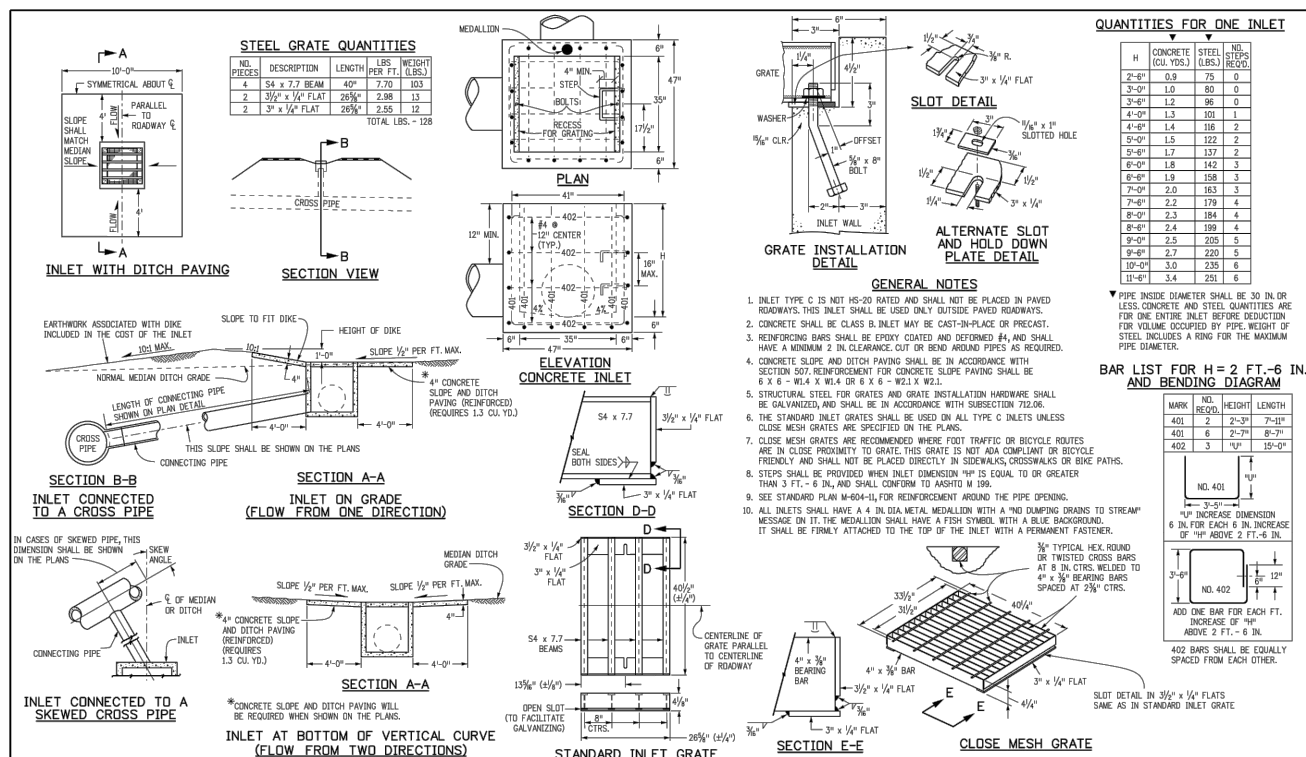
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**DIRECTIONAL CURB RAMP WITHIN RADIUS LAYOUT STANDARD DETAIL**

DATE: AUGUST 2014

DETAIL: **18**

1 OF 2



Computer File Information: Creation Date: 07/04/12, Last Modification Date: 07/04/12, Full Path: www.colorado.gov/info/business/designsupport, Drawing File Name: 60401201.dgn, CAD Ver.: MicroStation V8, Scale: Not to Scale, Units: English

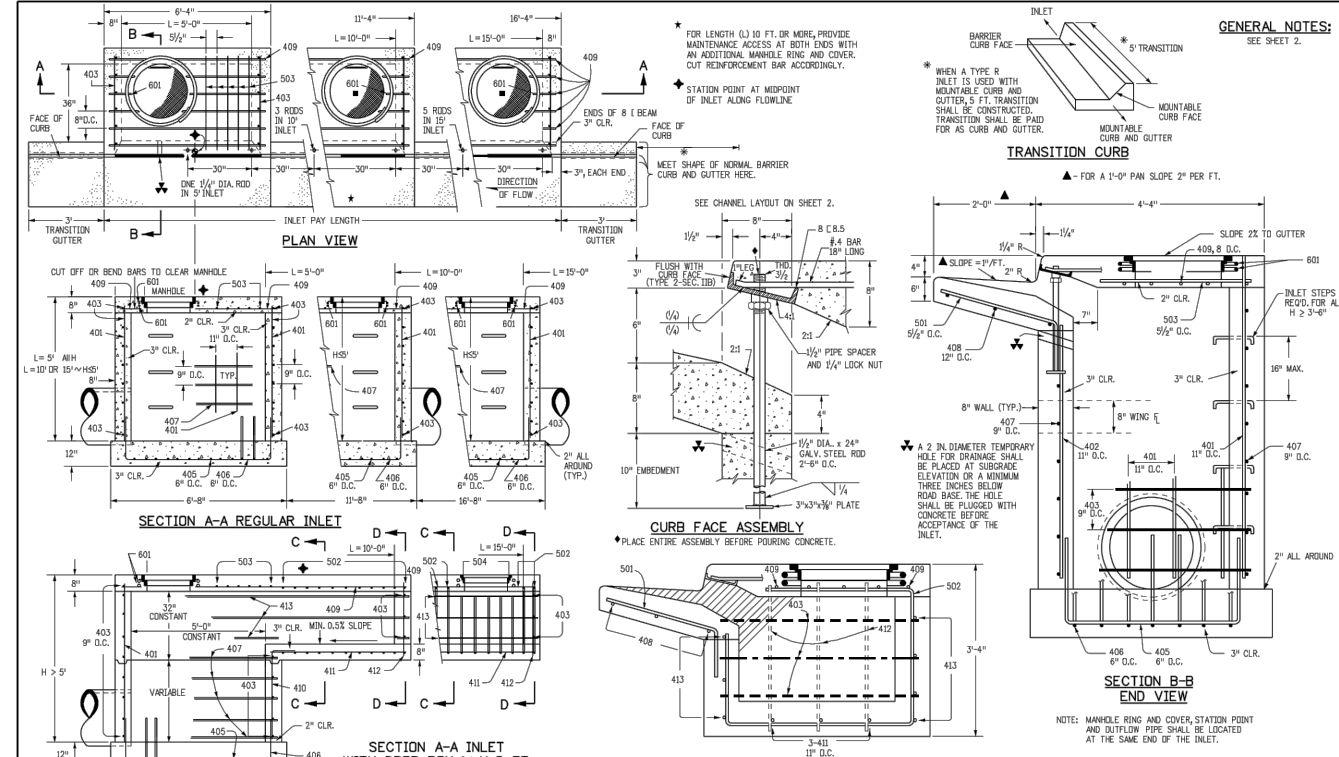
Colorado Department of Transportation, 4201 East Arkansas Avenue, Denver, Colorado 80222, Phone: (303) 757-9083, Fax: (303) 757-9820, Project Development Branch DD/LTA

**INLET, TYPE C**

STANDARD PLAN NO. **M-604-10**

Sheet No. 1 of 1

Issued By: Project Development Branch July 4, 2012



Computer File Information: Creation Date: 07/04/12, Last Modification Date: 07/04/12, Full Path: www.colorado.gov/info/business/designsupport, Drawing File Name: 60401201.dgn, CAD Ver.: MicroStation V8, Scale: Not to Scale, Units: English

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**INLET, TYPE R**

STANDARD PLAN NO. **M-604-12**

Sheet No. 1 of 2

Issued By: Project Development Branch July 4, 2012

Include Town of Parker Standard Detail 25-1 and 25-2



NO.	REVISION	BY	DATE	APPR.

**Kimley»Horn**  
 2017 KIMLEY-HORN AND ASSOCIATES, INC.  
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DESIGNED BY: DLS  
 DRAWN BY: CTM  
 CHECKED BY: DLS  
 DATE: 8/17/17

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
 DETAILS

PRELIMINARY  
 FOR REVIEW ONLY  
 NOT FOR  
 CONSTRUCTION  
**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

PROJECT NO.  
 096502001

DRAWING NAME  
 096502001PW-DT

C6.1