

# HORSESHOE RIDGE SUBDIVISION 1ST AMENDMENT

## LOT 2, BLOCK 1, HORSESHOE RIDGE SUBDIVISION 1ST AMENDMENT

### A PARCEL OF LAND LOCATED IN THE SE 1/4 OF SECTION 28, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN

## SITE PLAN REVIEW

### LEGAL DESCRIPTION

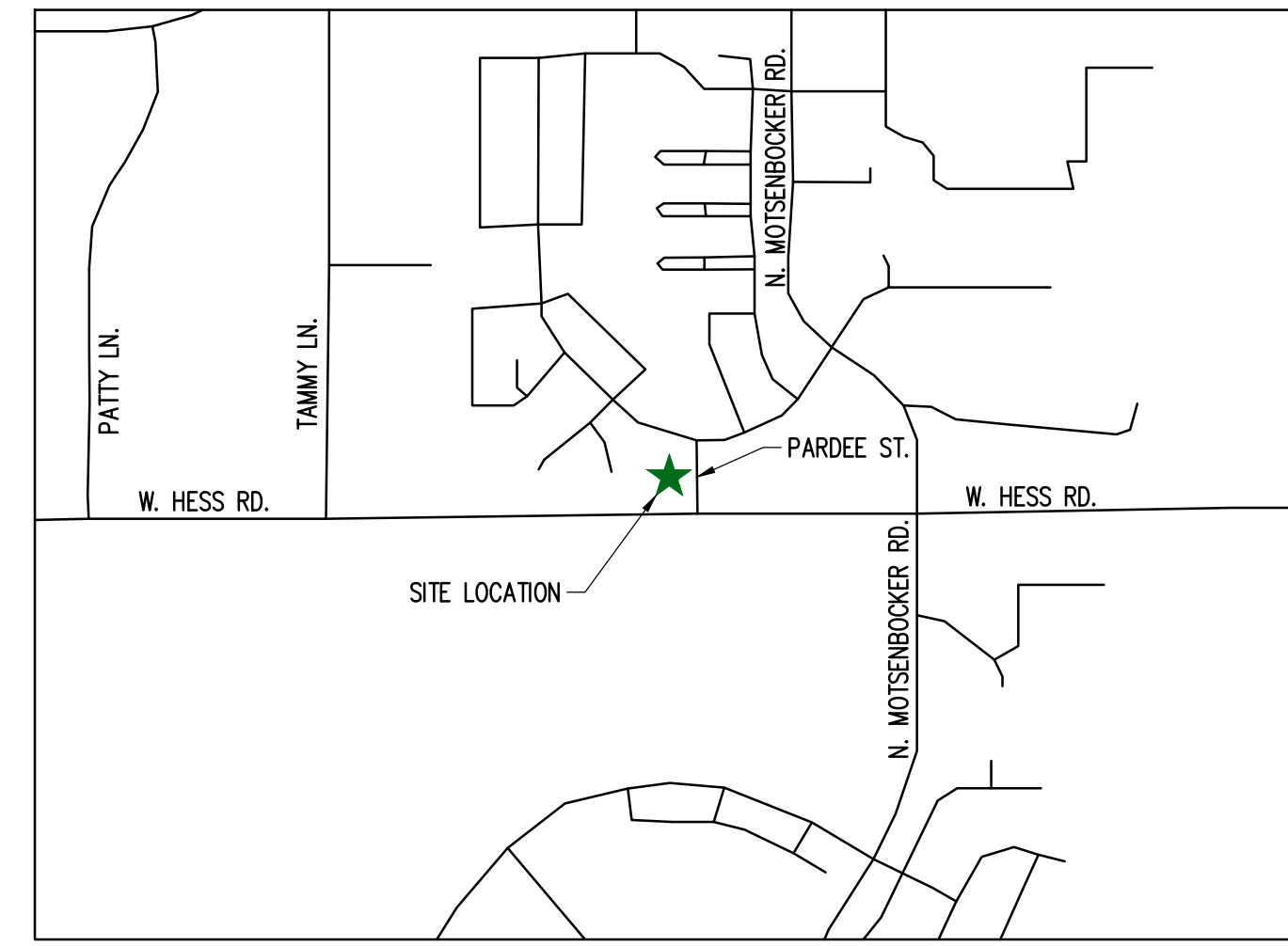
LOT 2, BLOCK 1, HORSESHOE RIDGE SUBDIVISION 1ST AMENDMENT

A PARCEL OF LAND LOCATED IN THE SE 1/4 OF SECTION 28, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF THE 6TH PRINCIPAL MERIDIAN

CONTAINING 41,493 SF (0.953 AC), MORE OR LESS.

### TOWN OF PARKER - CONSTRUCTION NOTES

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- A TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE DEVELOPMENT REVIEW ENGINEER'S ABSENCE. HOWEVER, TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION. FOR INFORMATION CONTACT: DENVER INTER-UTILITY GROUP, 303-534-6700 OR 1-800-922-1987. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER PUBLIC WORKS DIRECTOR, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- ALL PROPOSED STREET CUTS TO EXISTING PAVEMENTS FOR UTILITIES, STORM SEWER OR FOR OTHER PURPOSES ARE LISTED AND REFERENCED BELOW:  
EXAMPLES: WATER TIE-IN SHEET 3  
STORM SEWER CONNECTION SHEET 6
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY PERMIT USE APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION.
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.



### VICINITY MAP

### SHEET INDEX

CV-1.0	COVER SHEET
C-1.0	SITE PLAN
A-1	PROPOSED EXTERIOR ELEVATIONS
ESP1.1	PHOTOMETRIC PLAN
ESP1.2	PHOTOMETRIC CUT SHEET
L-1	LANDSCAPE PLAN

### PROJECT CONTACTS:

**PROPERTY OWNER**  
BRYTAR COMPANIES  
8117 PRESTON ROAD, #300  
DALLAS, TX 75225  
(214) 632-4860  
CONTACT: SCOTT REMPHREY

**APPLICANT**  
BRYTAR COMPANIES  
8117 PRESTON ROAD, #300  
DALLAS, TX 75225  
(214) 632-4860  
CONTACT: SCOTT REMPHREY

**CIVIL ENGINEER**  
RIDGETOP ENGINEERING AND CONSULTING, LLC  
541 E. GARDEN DRIVE, UNIT N  
WINDSOR, CO 80550  
(970) 663-4552  
CONTACT: MIKE BEACH, P.E.

**ARCHITECT**  
ROGUE ARCHITECTURE  
4100 WADSWORTH BLVD, SUITE 300  
WHEAT RIDGE, CO 80033  
(720) 599-3330  
CONTACT: SCOTT BODUCH

**SURVEYOR**  
P.L.S. GROUP  
532 WEST 66TH STREET  
LOVELAND, CO 80538  
(970) 669-2100  
CONTACT: BRYAN SHORT

**PARKER PUBLIC WORKS**  
ASSOCIATE PLANNER  
20120 E. MAIN STREET  
PARKER, CO 80138  
(303) 805-3199  
CONTACT: STACEY NERGER

**GAS**  
XCEL ENERGY  
180 LARIMER ST.  
DENVER, CO 80202  
(800) 895-2999

**SOUTH METRO FIRE RESCUE AUTHORITY**  
9195 E. MINERAL AVE.  
CENTENNIAL, CO 80112  
(720) 488-7200  
CONTACT: RANDY CAPRA

**INTERNATIONAL RURAL ELECTRIC ASSOC.**  
5496 NORTH US HWY 85  
P.O. BOX DRAWER A  
SEDALIA, CO 80135  
(303) 688-3100  
CONTACT: BROOK KAUFMAN

### SITE DATA

SITE	=	0.953 AC (41,493 SF)
TOTAL	=	0.953 AC (41,493 SF)
USE:	RETAIL/RESTAURANT	
BUILDINGS:	±5,000 SF	
RETAIL:	±1,700 SF	
RESTAURANT:	±3,300 SF	
ZONE:	PLANNED DEVELOPMENT	

REQUIRED PARKING : 39 STALLS  
RETAIL: 1/300 SF NET LEASABLE AREA  
RESTAURANT: 1/100 SF NET LEASABLE AREA  
(1700/300 = 5.667 OR 6 STALLS)  
(3300/100 = 33 STALLS)

STANDARD PARKING PROVIDED	37
HC STALLS	2
TOTAL PARKING PROVIDED	39

BICYCLE PARKING: 8

STANDARD STALL DIMENSIONS: 9' X 19'  
STALLS NEXT TO LANDSCAPE: 10' X 19'

DRIVE AISLE WIDTH: 24'

### BUILDING DATA

RESTAURANT (DRIVE THROUGH)	= ± 3,300 SF
RETAIL	= ± 1,700 SF
TOTAL	= ± 10,000 SF

### SIGHT DISTANCE CERTIFICATION:

I, THE UNDERSIGNED, HEREBY CERTIFY THAT THE PROPOSED SUBDIVISION, THE BIG TOOL BOX, IS DESIGNED IN ACCORDANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.

MICHAEL R. BEACH, PE #45088

### ENGINEER'S CERTIFICATION:

I, THE UNDERSIGNED, HEREBY CERTIFY THAT THE PROPOSED SUBDIVISION, THE BIG TOOL BOX, IS DESIGNED IN ACCORDANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.

MICHAEL R. BEACH, PE #45088

The Town of Parker review constitutes general compliance with the Town's Standards and approved variances, subject to these plans being stamped, signed, and dated by the professional engineer of record. Review by the Town does not constitute approval of the plan design or accuracy and correctness of engineering calculations. Errors in the design or calculations remain the responsibility of the registered professional engineer whose stamp and signature are affixed to this document.

This review does not constitute approval of any private on-site improvements which may be shown. Construction cannot commence until all required drainage/traffic report(s), final development plan(s), special review(s), grading permit, and/or other permits are complete, approved and on file with the Town of Parker.

Town of Parker, Director of Engineering

Date

LOGO



SEAL

PROJECT TITLE

RETAIL

LOT 2, BLOCK 1  
HORSESHOE RIDGE  
PARKER, CO 80134

PREPARED FOR

BRYTAR  
COMPANIES

8117 PRESTON RD. #300  
DALLAS, TX 75225

SUBMITTAL

SITE PLAN REVIEW

DRAWN BY: BSA

CHECKED BY: MRB

PROJECT NO.: 18-075-001

REVISIONS

DATE

5/28/2019

SHEET TITLE

COVER SHEET

SHEET INFORMATION

CV-1

1 of 2



NEW CONSTRUCTION:

# PARKER MIXED-USE RETAIL

HESS ROAD & PARDEE STREET  
PARKER, COLORADO



8111 Preston Road, Suite 300  
Dallas, Texas 75225

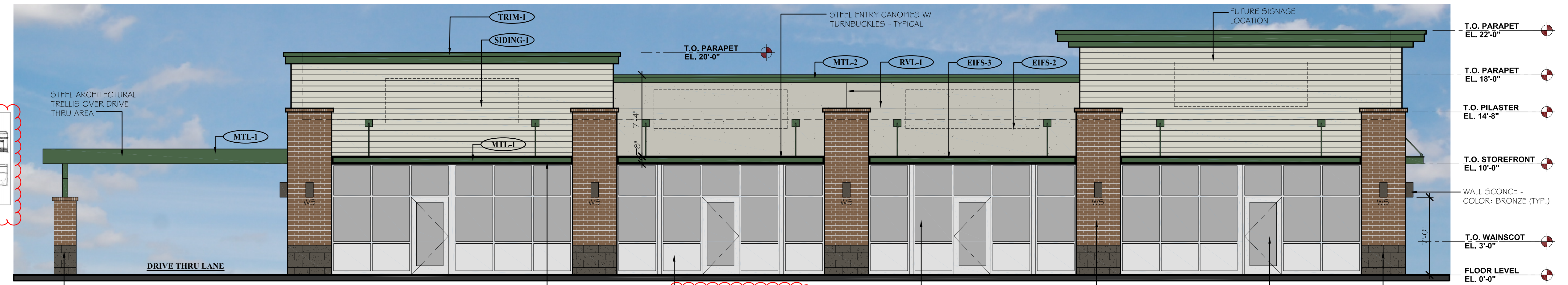
DATE: 05.22.2019  
05.28.2019

ISSUE: PROGRESS REVIEW  
SITE PLAN SUBMITTAL

4100 Wadsworth Boulevard  
Suite 300  
Wheat Ridge  
CO 80033  
720-599-3330 - P  
303-987-2304 - X

## ROGUE ARCHITECTURE

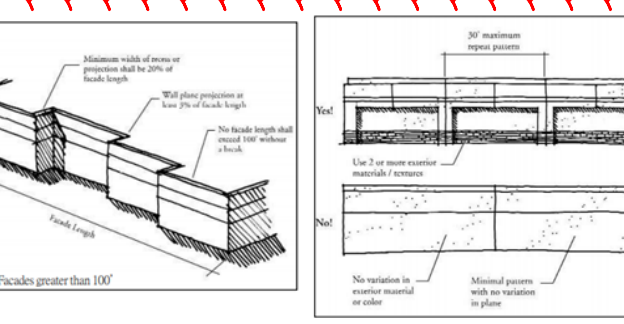
CHALLENGING THE STATUS QUO



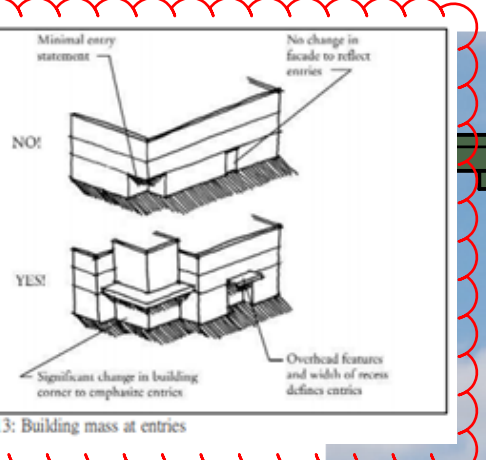
### PROPOSED SOUTH ELEVATION (FRONT)

SCALE: 3/16" = 1'-0"

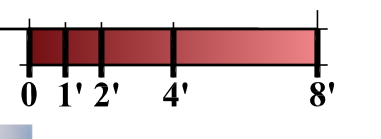
The architectural features shall incorporate wall plane projections or recesses having a depth of at least 3 percent of length of the facade. Please adjust accordingly. See examples below.



Significant change in the building corners should be provided to emphasize entries. Please adjust accordingly. See examples below.



Quality Materials shall be used. "Clear anodized insulated metal panel" areas should be replaced with a quality material, such as brick. This will enhance the required "Top, Middle, and Base" design principle.



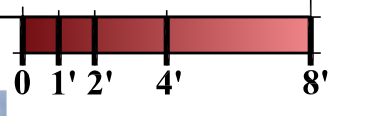
### PROPOSED EAST ELEVATION (RIGHT SIDE)

SCALE: 3/16" = 1'-0"



### PROPOSED WEST ELEVATION (LEFT SIDE)

SCALE: 3/16" = 1'-0"



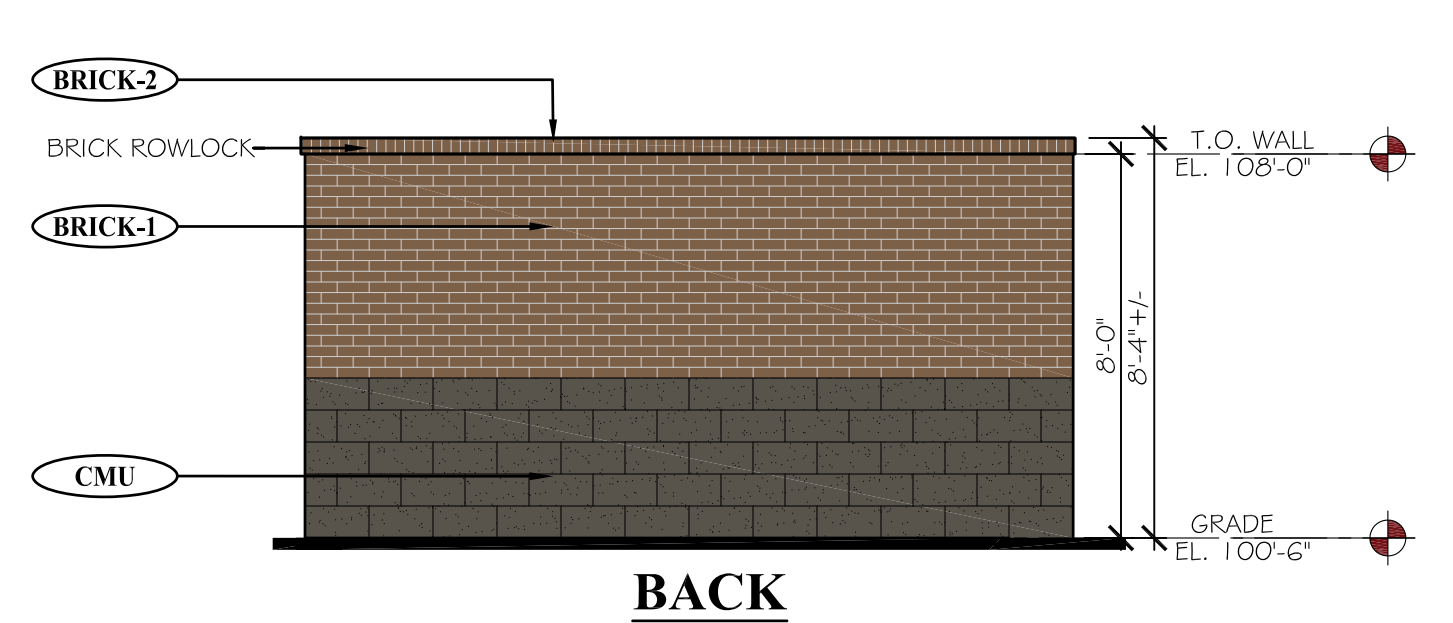
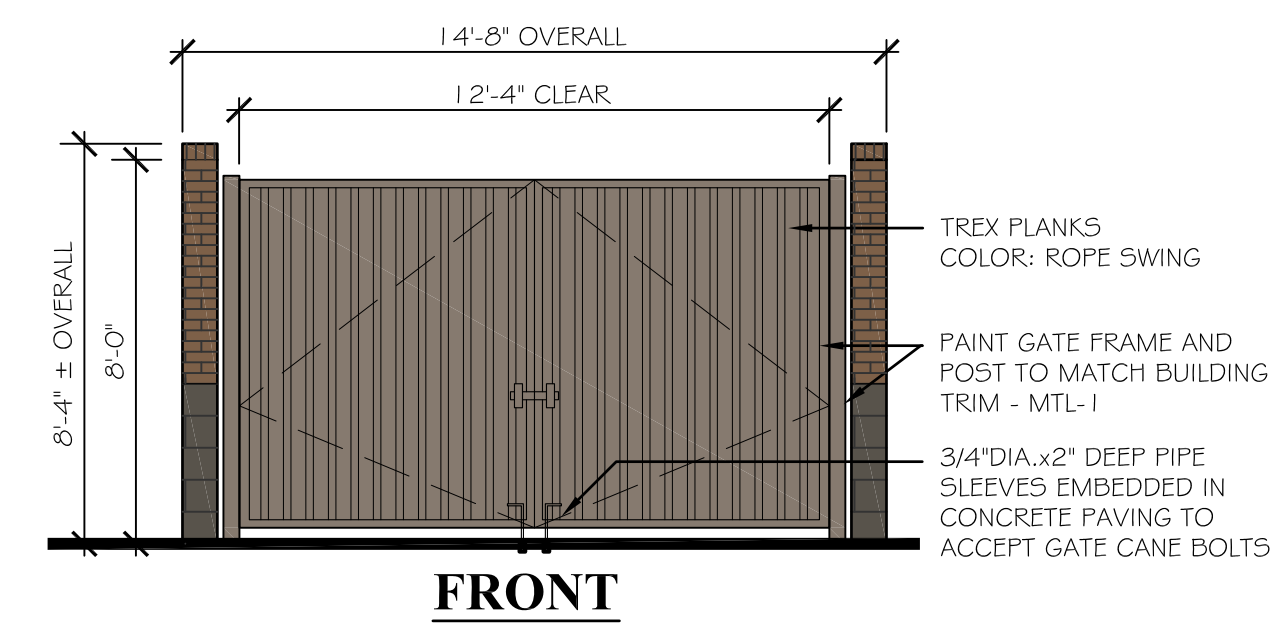
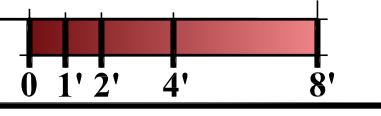
Areas fronting the street shall be architecturally enhanced (reduce the amount of EIFS being used). This should include enhanced corners and plane projections.

Buildings with flat roofs shall be designed to create visual interest by using variations in parapet height, articulation of cornice lines, decorative scuppers or other features. Provide a greater variation in the parapet heights to enhance the overall image of high quality and permanence.



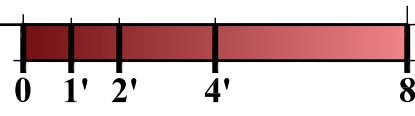
### PROPOSED NORTH ELEVATION (REAR)

SCALE: 3/16" = 1'-0"



### PROPOSED TRASH ENCLOSURE ELEVATIONS

SCALE: 1/4" = 1'-0"



EXTERIOR MATERIAL SCHEDULE							
LABEL	MANUFACTURER	SIZE/TYPE	FINISH COLOR	LABEL	MANUFACTURER	SIZE/TYPE	FINISH COLOR
STRENT-1	KAWNEER	TRIFAB 451	CLEAR ANODIZED W/ CLEAR GLASS	SIDING-1	JAMES HARDIE	HARDIEPLANK, LAP SIDING	COLOR: COBBLESTONE
BRICK-1	SUMMIT BRICK (OR EQUIV.)	MODULAR	GRAPHITE GRAIN	TRIM-1	JAMES HARDIE	HARDITRIM BOARDS	PRODUCT: 5/4 SMOOTH COLOR: SW #7048 URBANE BRONZE
BRICK-2	SUMMIT BRICK (OR EQUIV.)	ROWLOCK SILL	GRAPHITE GRAIN	MTL-1	PAINTED	STEEL SUNSCREEN	SHERWIN WILLIAMS - #0041 HUNTER GREEN
CMU-1	BASALITE (OR EQUIV.)	INTEGRAL COLOR W/ ASSOCIATED CAP	CHARCOAL #G05 SPLIT-FACE	MTL-2	MBCI	METAL TRIM # DOWNSPOUTS	HUNTER GREEN
EIFS-1	DRYVIT OR SIMILAR	FIELD (DARK)	DRYVIT SUEDE #105 PEBBLE TEXTURE	PAINT-1	BENJAMIN MOORE	DOORS & FRAMES	SHERWIN WILLIAMS #6073 PERFECT GREIGE
EIFS-2	DRYVIT OR SIMILAR	BANDING (LIGHT)	DRYVIT LIGHT SERENITY #300 PEBBLE TEXTURE	RVL-1	DRYVIT OR SIMILAR	3/4" ROUTED REVEAL	COLOR TO MATCH ADJACENT FINISH
EIFS-3	DRYVIT OR SIMILAR	FIELD (MEDIUM)	DRYVIT PRAIRIE CLAY #111 PEBBLE TEXTURE				

Indicate if the EIFS being used is water managed or barrier type.

R:\Projects\2019\_20 - Brytar - Parker, CO Retail\CAD\Site Submittal\2019\_20 - A-1 - Proposed Elevations.dwg

SITE PLAN SUBMITTAL 05.28.2019

DATE: 05.28.19  
DRAWN: ROGUE  
CHECKED: SAB  
ROGUE PROJECT NO.: 2019.30

PROPOSED EXTERIOR ELEVATIONS

## A-1

SHEET:

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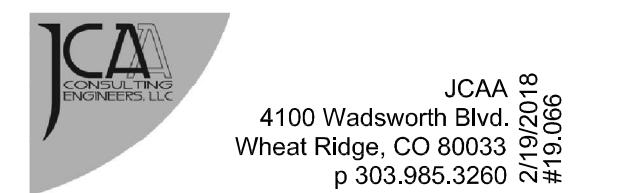
NEW CONSTRUCTION:

# PARKER MIXED-USE RETAIL

HESS ROAD & PARDEE  
STREET  
PARKER, COLORADO



8111 Preston Road, Suite 300  
Dallas, Texas 75225



JCAA  
4100 Wadsworth Blvd.  
Wheat Ridge, CO 80033  
p 303.985.3260  
#16.066

DATE: 05.28.2019  
ISSUE: SITE PLAN SUBMITTAL

4100  
Wadsworth Boulevard  
Suite 300  
Wheat Ridge  
CO 80033  
720-599-3330 - P  
303-987-2304 - X

# ROGUE

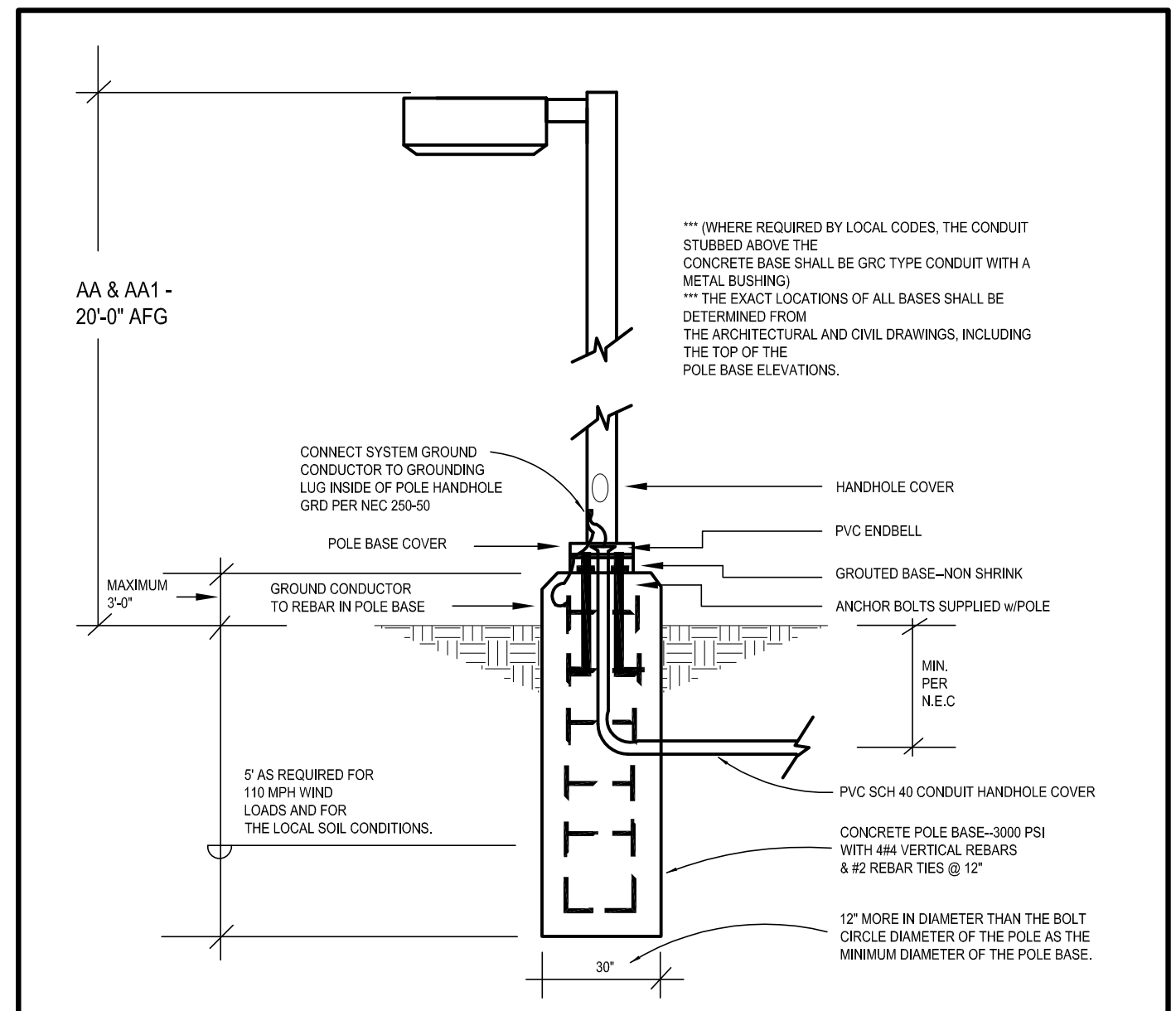
CHALLENGING THE STATUS QUO

DATE: 04.20.19  
DRAWN: ROGUE  
CHECKED: SAB  
ROGUE PROJECT NO.: 2019.30

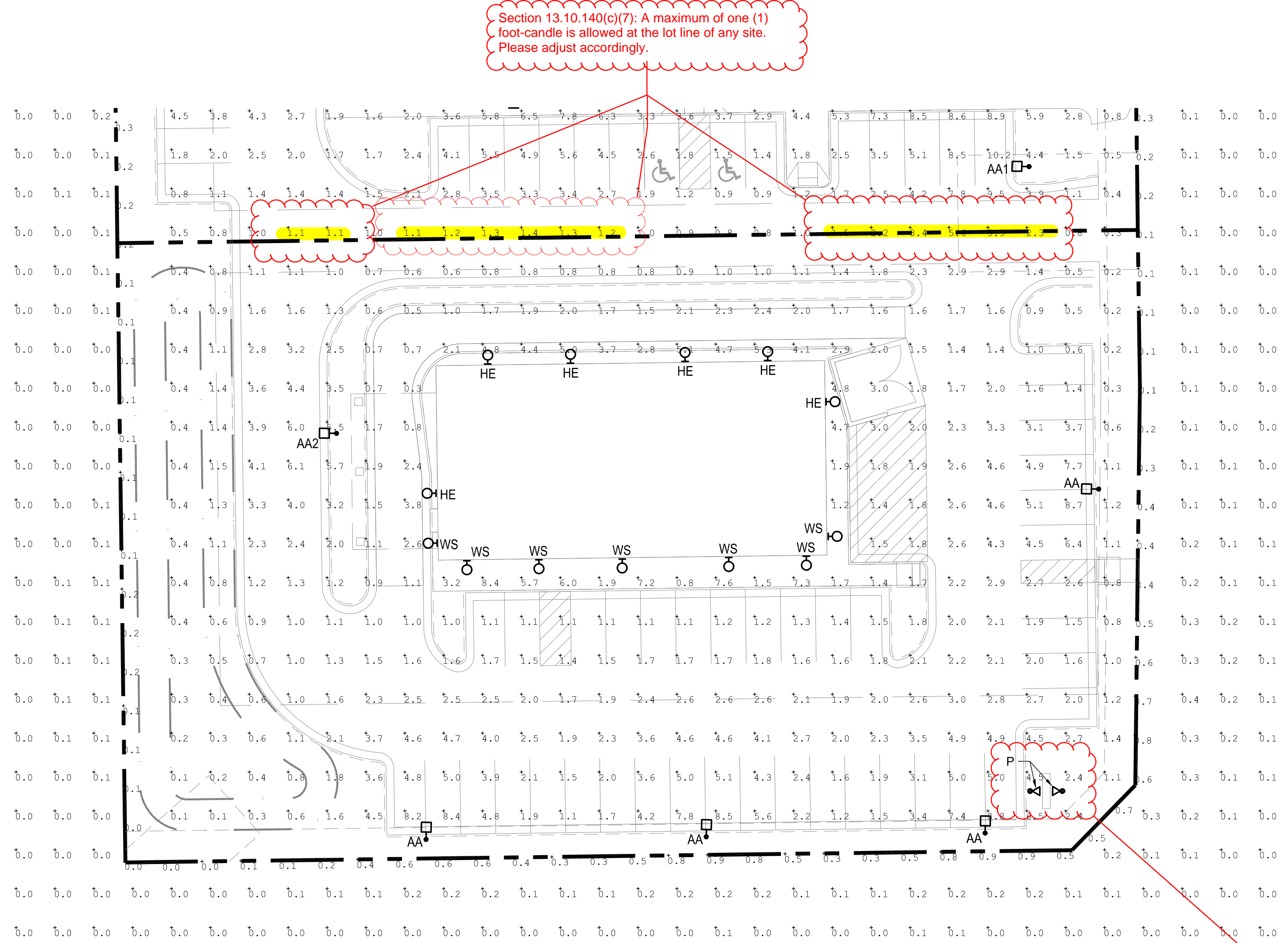
PHOTOMETRIC PLAN  
SHEET: **ESP1.1**

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SITE PLAN SUBMITTAL 05.28.2019



**2** TYPE "AA & AA1" POLE BASE DETAIL  
scale: N.T.S.



**1** PHOTOMETRIC SITE PLAN  
scale: 1"=20'-0"  
NORTH

Signs are not being evaluated with this site plan review. All sign lighting will be reviewed at the time of a separate sign permit. Please add a note indicating this lighting is not being reviewed at this time but under separate permit.

There must be a numeric value associate with the Max/Min and Avg/Min ratio.  
If the ratios are zero please remove the zero values from the area and recalculate.

SITE LUMINAIRE SCHEDULE									
FIXT. TYPE	NAME	MANUFACTURER CATALOG NUMBER	FINISH	LAMP DATA	LAMP WATTS	MOUNTING METHOD	MTG HEIGHT	DESCRIPTION	VOLTS
AA	LSI INDUSTRIES	XGBM-FT-SS-128-NW-UE-BLK-SSQB5-S07G-20-S-B LK-SF-DGP	BLK	LED	188	POLE	20'-0"	SITE LIGHTING SINGLE HEAD TYPE FT WITH HOUSE SIDE SHIELD	UNV
AA2	LSI INDUSTRIES	XGBM-3-SS-128-NW-UE-BLK-SSQB5-S07G-20-S-BL K-SF-DGP	BLK	LED	188	POLE	20'-0"	SITE LIGHTING SINGLE HEAD TYPE 3 WITH HOUSE SIDE SHIELD	UNV
HE	WILLIAMS	WVPHL30/740 TFT-WHT-SDGL-EM/10WC-DIM-UNV	BLK	LED	72	SURFACE	9'-0"	WALL PACK	UNV
WS	CONTECH	CYL6230KMVDUXWCLR-BZ-RDB	BRZ	LED	14	SURFACE	6'-0"	WALL SCONCE	UNV
P	KIM	EL218-S-3-BL-3K-UV-DB	BLK	LED	16	GROUND	-	WATERPROOF SPOT LIGHT FOR MONUMENT SIGN	UNV

Calculation Summary							
Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
PROPERTY LINE	Illuminance	Fc	0.30	1.0	0.0	N.A.	N.A.
RETAIL PARKING AREA	Illuminance	Fc	3.06	8.8	1.1	2.78	8.00



