



Know what's below.
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CONSTRUCTION DOCUMENTS

QUICK N CLEAN

9572 TWENTY MILE ROAD

PARKER, COLORADO

A PORTION OF THE 1/4 OF SECTION 9, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF 6TH P.M.
COUNTY OF DOUGLAS, STATE OF COLORADO



Vicinity Map
1"=2,000'

RESOURCE LIST

APPLICANT 3K1 CONSULTING SERVICES, LLC 11811 N. TATUM BOULEVARD PHOENIX, AZ 85028 PH: (602) 850-8081	TOWN OF PARKER PLAN REVIEW 20120 E. MAIN STREET PARKER, CO 80138 CHIP KERHOVE PH: (70) 989-2244 RANDY CAPRA
OWNER QUICK N CLEAN 7291 E. ADOBE DRIVE, SUITE 115 SCOTTSDALE, AZ 85255 PH: (480) 707-3531	TOWN OF PARKER ENGINEERING / PUBLIC WORKS 20120 E. MAIN STREET PARKER, CO 80138-7335 TYLER SANDT PH: (303) 805-3182
OVERALL DEVELOPER EVT PARKER COLORADO, LLC 2710 E. CAMELBACK ROAD, SUITE 210 PHOENIX, AZ 85016 RYAN AMATO PH: (602) 468-6100	WATER & SEWER PARKER WATER AND SANITATION DISTRICT 18100 E. WOODMAN ROAD PARKER, CO 80134 DRAYTON SANDERSON/ ROBERT RAMSEY PH: (303) 841-4627
CIVIL ENGINEER CEI ENGINEERING ASSOC., INC. 710 PINEDALE AVE. FRESNO, CA JASON HATWIG, PROJECT MANAGER PH: (559) 447-3119	FIRE DEPARTMENT SOUTH METRO FIRE RESCUE AUTHORITY 9195 E. MINERAL AVE. CENTENNIAL, CO 80112 CHIP KERHOVE PH: (720) 989-2244
LANDSCAPE ARCHITECT CEI ENGINEERING ASSOC., INC. 3108 S.W. REGENCY PARKWAY, SUITE 2 BENTONVILLE, AR 72712 CHIP ASHLEY PH: (479) 273-9472	ELECTRICAL IREA 5496 US HWY 85 SEDALIA, CO 80135 PH: (303) 688-7431
ARCHITECT CHSOA 1777 S. BELLAIRE STREET, SUITE 100 DENVER, CO 80222 BILL STOW PH: (303) 962-9164	NATURAL GAS XCEL ENERGY 1800 LARIMER STREET DENVER, CO 80202 PH: (303) 571-7511
LAND SURVEYOR RUBINO SURVEYING 3312 AIRPORT ROAD BOULDER COLORADO 80301 PH: (303) 464-9515	

LEGAL DESCRIPTION

(PER THE TITLE COMMITMENT)

LOT 4,
PARKER AND FILING NO. 1,
COUNTY OF DOUGLAS,
STATE OF COLORADO.

BENCHMARK

DOUGLAS CONTROL MONUMENT #1.095035. A 3 1/2" ALUMINUM CAP.
ELEVATION = 5906.34 FEET (NAVD1988). AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS

THE BEARINGS ARE BASED ON THE WEST ROW LINE OF SOUTH PARKER ROAD ASSUMED TO BEAR
S23°56'20"E BETWEEN MONUMENTS FOUND AND DESCRIBED HEREON.

FLOOD ZONE INFORMATION

SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS
DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM.
MAP NUMBER: 08035C0067G
EFFECTIVE DATE: MARCH 16, 2016

SIGHT DISTANCE CERTIFICATION

NOT APPLICABLE

ENGINEER'S STATEMENT

I HEREBY AFFIRM THAT THESE FINAL CONSTRUCTION PLANS WERE PREPARED UNDER MY DIRECT
SUPERVISION, IN ACCORDANCE WITH ALL APPLICABLE TOWN OF PARKER STANDARDS AND STATUTES,
RESPECTIVELY, AND THAT I AM FULLY RESPONSIBLE FOR THE ACCURACY OF ALL DESIGN, REVISIONS,
AND RECORD CONDITIONS THAT I HAVE NOTED ON THESE PLANS

SHEET INDEX

TOWN OF PARKER PUBLIC WORKS CONSTRUCTION PLANS

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C3	CBMP PLAN (INITIAL)
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C3.2	CBMP DETAILS
C3.3	CBMP DETAILS
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C6	DRAINAGE PLAN

PARKER WATER AND SANITATION DISTRICT CONSTRUCTION PLANS

SHEET #	SHEET TITLE
C7	COVER SHEET
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LANDSCAPE AND IRRIGATION PLANS

SHEET #	SHEET TITLE
L1	LANDSCAPE PLAN
L2	LANDSCAPE NOTES
L3	IRRIGATION PLAN



CEI ENGINEERING ASSOCIATES, INC.
710 W. PINEDALE AVE.
FRESNO, CA 93711
PHONE: (559) 447-3119
FAX: (559) 447-3129



CLIENT
3K1 CONSULTING SERVICES, LLC.
11811 N. TATUM BOULEVARD,
PHOENIX, ARIZONA 85028
PHONE: (602) 850-8101



PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531

REVISION

NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

The Town of Parker review constitutes general compliance with the Town's Standards and approved variances, subject to these plans being stamped, signed, and dated by the professional engineer of record. Review by the Town does not constitute approval of the plan design or accuracy and correctness of engineering calculations. Errors in the design or calculations remain the responsibility of the registered professional engineer whose stamp and signature are affixed to this document.

This review does not constitute approval of any private on-site improvements which may be shown. Construction cannot commence until all required drainage/traffic reports(s), final development plan(s), special review(s), grading permit, and/or other permits are complete, approved and on file with the Town of Parker.

Jason Hatwig
Town of Parker, Director of Engineering
06/26/2023
Date

COVER SHEET

SHEET TITLE
SHEET NUMBER

C1

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-CS.DWG, SAVED BY: JACALDERON



GENERAL COVER SHEET NOTES

- A. TOPOGRAPHIC BOUNDARY SURVEY, INCLUDING PROPERTY LINES, LEGAL DESCRIPTION, EXISTING UTILITIES, SITE TOPOGRAPHY WITH SPOT ELEVATIONS, OUTSTANDING PHYSICAL FEATURES AND EXISTING STRUCTURE LOCATIONS WAS PROVIDED BY THE FOLLOWING COMPANY, AS A CONTRACTOR TO THE SELLER/OWNER:
- TOPOGRAPHY: RUBINO SURVEYING
3312 AIRPORT ROAD
BOULDER, COLORADO 80301
BOB RUBINO
303.464.9515
- B. CEI ENGINEERING AND ITS ASSOCIATES WILL NOT BE HELD RESPONSIBLE FOR THE ACCURACY OF THE SURVEY OR FOR DESIGN ERRORS OR OMISSIONS RESULTING FROM SURVEY INACCURACIES.
- C. ALL PHASES OF SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE OWNER / DEVELOPER SITE WORK SPECIFICATIONS.
- D. CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL OF EXISTING STRUCTURES, RELATED UTILITIES, PAVING, UNDERGROUND STORAGE TANKS AND ANY OTHER EXISTING IMPROVEMENTS AS NOTED. SEE SITE WORK SPECIFICATIONS.
- E. CONTRACTOR IS TO REMOVE AND DISPOSE OF ALL DEBRIS, RUBBISH AND OTHER MATERIALS RESULTING FROM PREVIOUS AND CURRENT DEMOLITION OPERATIONS. DISPOSAL WILL BE IN ACCORDANCE WITH ALL LOCAL, STATE AND/OR FEDERAL REGULATIONS GOVERNING SUCH OPERATIONS.
- F. THE GENERAL CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR AND SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- G. **WARRANTY/DISCLAIMER:** THE DESIGNS REPRESENTED IN THESE PLANS ARE IN ACCORDANCE WITH ESTABLISHED PRACTICES OF CIVIL ENGINEERING FOR THE DESIGN FUNCTIONS AND USES INTENDED BY THE OWNER AT THIS TIME. HOWEVER, NEITHER THE ENGINEER NOR ITS PERSONNEL CAN OR DO WARRANT THESE DESIGNS OR PLANS AS CONSTRUCTED EXCEPT IN THE SPECIFIC CASES WHERE THE ENGINEER INSPECTS AND CONTROLS THE PHYSICAL CONSTRUCTION ON A CONTEMPORANEOUS BASIS AT THE SITE.
- H. **SAFETY NOTICE TO CONTRACTOR:** IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. ANY CONSTRUCTION OBSERVATION BY THE ENGINEER OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES, IN, ON OR NEAR THE CONSTRUCTION SITE.
- I. ALL CONSTRUCTION IN STATE HIGHWAY DEPARTMENT RIGHT-OF-WAY SHALL BE COORDINATED WITH THE HIGHWAY DEPARTMENT RESIDENT ENGINEER.
- J. RESIDENT ENGINEERING SERVICES: WHEN REQUESTED BY THE OWNER, RESIDENT ENGINEERING SERVICES SHALL BE PROVIDED BY THE ENGINEERS ON A TIME AND FREQUENCY BASIS ACCEPTABLE TO THE CITY ENGINEER FOR IMPROVEMENTS TO PUBLIC WATER MAINS, PUBLIC SEWER, AND CITY STREETS. AT THE COMPLETION OF CONSTRUCTION, THE ENGINEER SHALL CERTIFY THE CONSTRUCTION TO BE IN COMPLIANCE WITH THE PLANS AND SPECIFICATIONS. THIS WORK WILL BE AT THE OWNER/DEVELOPER'S DIRECT EXPENSE AND SHALL BE COORDINATED WITH CEI ENGINEERING ASSOCIATES, INC. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE RESIDENT ENGINEER OF ANY PRECONSTRUCTION / CONSTRUCTION CONFERENCES AND ANY PUBLIC CONSTRUCTION 24 HOURS PRIOR TO SAID ACTION.

TOWN OF PARKER DEPARTMENT OF ENGINEERING / PUBLIC WORKS

- STANDARD CONSTRUCTION NOTES**
- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
 - ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
 - A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
 - THE TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE TOWN INSPECTOR'S ABSENCE. HOWEVER, THE TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
 - LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
 - THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER DIRECTOR OF ENGINEERING/PUBLIC WORKS, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
 - A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY USE PERMIT APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION. ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION."
 - COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
 - THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
 - THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
 - PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
 - REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR. SUCH RESTORATION MAY BE REQUIRED TO BE COMPLETED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
 - ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
 - ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY" FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-82, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
 - IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
 - ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

- ROADWAY NOTES**
- THE FOLLOWING NOTES SHALL APPEAR ON THE COVER SHEET OF ALL SUBMITTALS CONTAINING ROADWAY PLANS. IF A COVER SHEET HAS NOT BEEN USED, THEY SHALL BE PUT ON EVERY SHEET OF THE SUBMITTAL CONTAINING ROADWAY DESIGN.
- PAVING SHALL NOT COMMENCE UNTIL A SOILS REPORT AND PAVEMENT DESIGN IS APPROVED BY THE TOWN OF PARKER AND SUBGRADE COMPACTION TESTS ARE SUBMITTED TO AND APPROVED BY THE TOWN INSPECTOR.
 - STANDARD TOWN OF PARKER CURB RAMPS ARE TO BE CONSTRUCTED AT ALL CURB RETURNS AND AT ALL "T" INTERSECTIONS IN ACCORDANCE WITH THE LATEST TOWN OF PARKER STANDARDS
 - ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
 - ALL ELEVATIONS ARE ON USGS DATUM WITH DATE, RANGE POINT OR MONUMENT SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
 - EXCEPT WHERE OTHERWISE PROVIDED FOR IN THESE PLANS AND SPECIFICATIONS, THE COLORADO DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF HIGHWAYS M AND S STANDARDS, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, LATEST EDITION, SHALL APPLY.

- STORM DRAINAGE INFRASTRUCTURE NOTES**
- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
 - THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" (TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72 (5 CCR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
 - INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, FOREBAYS, TRICKLE CHANNELS, ETC). GROUDED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
 - STRUCTURAL BACKFILL (CDOT CLASS 3) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
 - ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
 - TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:
 - 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC)
 - 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
 - ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
 - ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE AVERAGE JOINT GAP THAT EXCEEDS 3/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
 - JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
 - EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS AND STRUCTURES. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
 - CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
 - PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
 - TWO (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "B" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
 - ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
 - ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
 - CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
 - PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

- SIGNAGE AND STRIPING NOTES**
- IN ADDITION TO THE NOTES IN THE ABOVE SECTION OF THESE ROADWAY STANDARDS, THE FOLLOWING NOTES SHALL APPEAR ON THE COVER SHEET OF ALL SUBMITTALS CONTAINING ROADWAYS. THE FOLLOWING NOTES SHALL BE ON ALL SIGNAGE AND STRIPING PLANS:
- ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT VERSION OF THE FEDERAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), COLORADO SUPPLEMENTAL MUTCD, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL. FURTHER SPECIFICATIONS AND ILLUSTRATIONS ARE LOCATED IN THE COLORADO DEPARTMENT OF TRANSPORTATION M AND S STANDARDS.
 - A FIELD INSPECTION OF LOCATION AND INSTALLATION OF ALL SIGNS AND MARKINGS SHALL BE PERFORMED BY THE TOWN OF PARKER.
 - THE CONTRACTOR INSTALLING SIGNS SHALL BE RESPONSIBLE FOR THE LOCATING AND PROTECTING OF ALL UNDERGROUND UTILITIES.
 - TYPE III (LIGHTED) BARRICADES SHALL BE SET AT THE ENDS OF ROADWAYS SEPARATING FINISHED (AND/OR ACCEPTED) AND UNFINISHED CONSTRUCTION AREAS AND SHALL BE MAINTAINED BY THE CONTRACTOR/DEVELOPER. A "ROAD CLOSED AHEAD" AND "TO BE EXTENDED" WARNING SIGNS SHALL BE INSTALLED APPROPRIATELY IN ADVANCE OF THE TYPE III BARRICADES.
 - SPECIAL CARE SHALL BE TAKEN IN SIGN LOCATIONS TO ENSURE AN UNOBSTRUCTED VIEW OF EACH SIGN.
 - WHERE STOP SIGN CONTROL IS APPROPRIATE, 36 INCH STOP SIGNS SHALL BE USED FOR APPROACHES TO ANY ROADWAY THAT IS CLASSIFIED AS A COLLECTOR OR GREATER.
 - A 7 FOOT MINIMUM POST LENGTH SHALL BE MAINTAINED FROM THE BOTTOM OF THE SIGN PANEL TO THE GROUND. THIS REQUIREMENT FOR VERTICAL CLEARANCE IS FOR ALL SIGNS.
 - DELINEATION OF ROADWAYS SHALL BE AS SPECIFIED IN THE COLORADO DEPARTMENT OF TRANSPORTATION M AND S STANDARDS.
 - RAISED MEDIAN ISLAND NOSES SHALL HAVE R4-7 SIGNS AT EACH END AND A 4"x12" HIGH INTENSITY YELLOW SIGN BLANK LOCATED MIDWAY BETWEEN THE R4-7 SIGN AND FINISHED GRADE ON EACH POST.
 - SIGNAGE AND STRIPING HAS BEEN DETERMINED BY INFORMATION AVAILABLE AT THE TIME OF REVIEW. PRIOR TO THE INITIATION OF ANY WARRANTY PERIOD, THE TOWN OF PARKER RESERVES THE RIGHT TO REQUIRE MODIFICATIONS TO EXISTING, OR INSTALLATION OF, ADDITIONAL SIGNAGE AND/OR PAVEMENT MARKING IF IT IS DETERMINED THAT CONDITIONS WARRANT SUCH MODIFICATION ACCORDING TO THE MUTCD OR THE CDOT M AND S STANDARDS. ALL SIGNAGE AND STRIPING SHALL FALL UNDER THE REQUIREMENTS OF THE TWO-YEAR WARRANTY PERIOD FOR NEW CONSTRUCTION. ADDITIONALLY, ALL PAVEMENT MARKINGS SHALL NOT LIFT OR PEEL DURING THE FIRST YEAR AFTER INSTALLATION.
 - DIAMOND GRADE MATERIAL SHALL BE USED ON ALL STOP SIGNS AND OVERHEAD SIGNS. ALL OTHER ROADSIDE TRAFFIC CONTROL DEVICES SHALL USE HIGH INTENSITY GRADE SHEETING. NO FLUORESCENT YELLOW GREEN SHEETING SHALL BE USED UNLESS SPECIFICALLY APPROVED BY THE TOWN OF PARKER.
 - ALL STREET NAME SIGNS SHALL BE HIGH INTENSITY, EXTRUDED, GREEN AND WHITE BLADES. ARTERIAL AND COLLECTOR STREET NAME SIGNS SHALL BE 9 INCH BLADES AND HAVE MIXED CASE LETTERING. LOCAL STREET NAME SIGNS SHALL BE 6 INCHES BLADES WITH MIXED CASE LETTERING.
 - ALL LAYOUTS FOR INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
 - ALL REMOVED SIGNS SHALL BE RETURNED TO THE TOWN OF PARKER.
 - CROSSWALKS:
 - SHALL BE CONSTRUCTED USING PREFORMED THERMO-PLASTIC OR AN APPROVED EQUAL.
 - SHALL BE LONGITUDINAL 2 FEET BY 10 FEET (CONTINENTAL) TYPE OR OTHER APPROVED BY THE TOWN OF PARKER. SHALL LINE UP WITH HANDICAP RAMPS.
 - SHALL BE CENTERED WITHIN LANES AND PLACED ON LANE AND CURB LINES SO AS TO AVOID VEHICLE WHEEL PATHS.
 - ALL PAVEMENT MARKING MATERIAL (INCLUDING WORDS AND SYMBOLS) SHALL BE AS FOLLOWS:
 - FOR CONCRETE SURFACE:
 - LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER
 - WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE METHYL-MYTHACRALATE, PREFORMED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), OR AS SPECIFIED. (SAND OR WATER BLAST CURING COMPOUND PRIOR TO INSTALLATION OF MARKINGS.)
 - FOR ASPHALT SURFACE:
 - LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER.
 - WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE HOT APPLIED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), METHYL-MYTHACRALATE, OR AS SPECIFIED.
 - INSPECTION AND APPROVAL OF STRIPING AND CROSSWALK LAYOUT TO BE DONE BY TOWN OF PARKER DEVELOPMENT REVIEW ENGINEER (CALL 303-840-9546) PRIOR TO APPLICATION OF FINAL STRIPING.



CEI ENGINEERING ASSOCIATES, INC.
710 W. PINEDALE AVE.
FRESNO, CA 93711
PHONE: (559) 447-3119
FAX: (559) 447-3129



CLIENT:
3K1 CONSULTING SERVICES, LLC.
11811 N. TATUM BOULEVARD,
PHOENIX, ARIZONA 85028
PHONE: (602) 850-8101



PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85258
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO

CONSTRUCTION DOCUMENTS



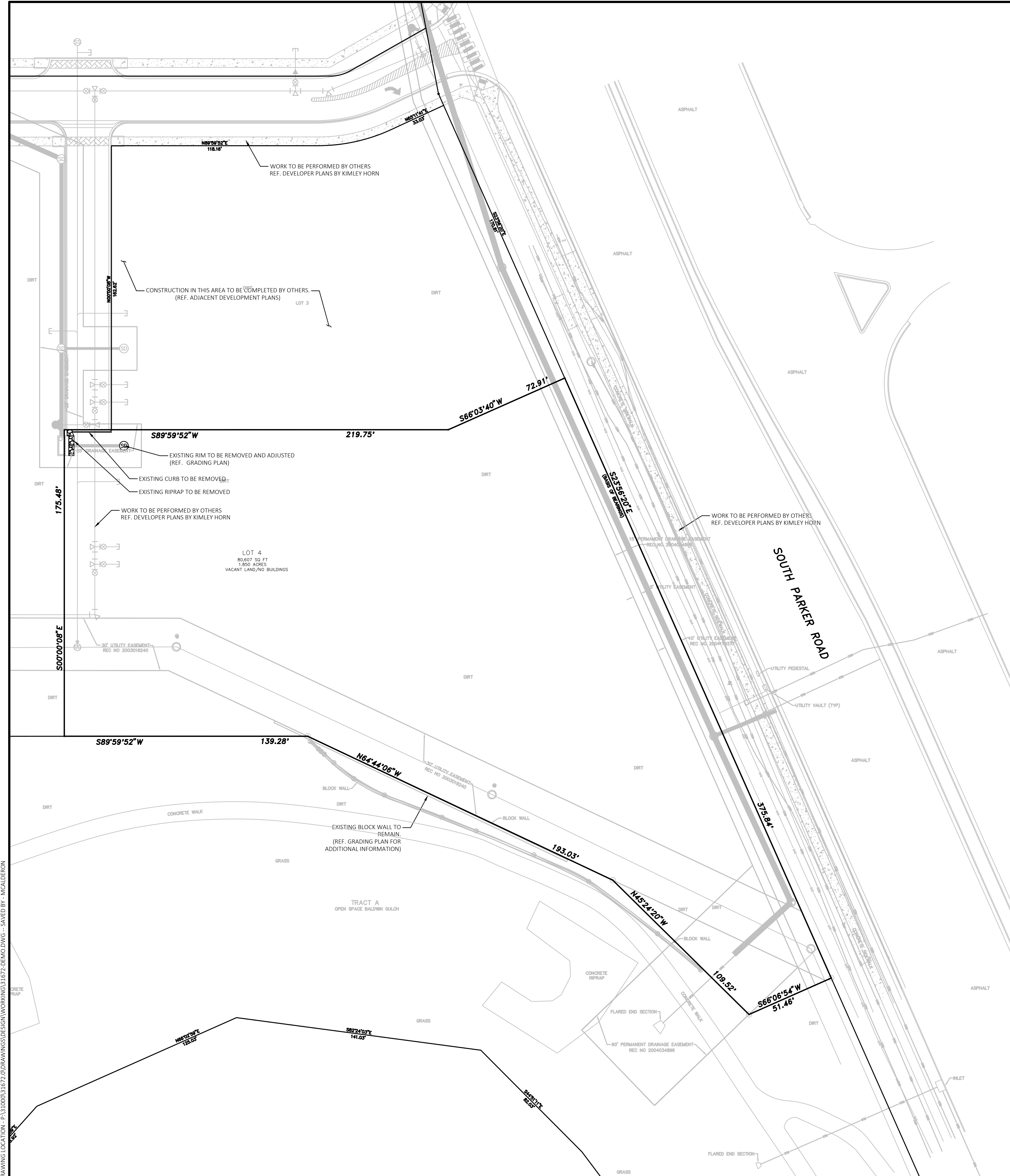
PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6



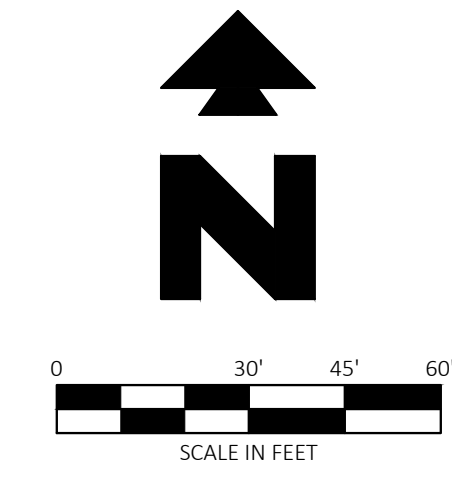
Know what's below.
Call before you dig.

GENERAL NOTES
SHEET TITLE
SHEET NUMBER

C1.1



Know what's below.
Call before you dig.



BENCHMARK
DOUGLAS CONTROL MONUMENT #1.095035, A 3 1/2" ALUMINUM CAP.
ELEVATION = 5906.34 FEET (NAVD1988), AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS
THE BEARINGS ARE BASED ON THE WEST ROW LINE OF SOUTH PARKER ROAD ASSUMED TO BEAR S23°56'20"E BETWEEN MONUMENTS FOUND AND DESCRIBED HEREON.

FLOOD ZONE INFORMATION
SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM.
MAP NUMBER: 08035C0067G
EFFECTIVE DATE: MARCH 16, 2016

- EXISTING LEGEND:**
- PROPERTY LINE/RIGHT OF WAY LINE
 - ⊙ COMMUNICATIONS MANHOLE
 - ⊙ SANITARY SEWER MANHOLE
 - ⊙ STORM SEWER MANHOLE
 - GAS --- UNDERGROUND GAS LINES
 - COM --- UNDERGROUND COMMUNICATIONS LINES
 - STM --- UNDERGROUND STORM SEWER LINES



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3K1 CONSULTING SERVICES, LLC.
11811 N. TATUM BOULEVARD,
PHOENIX, ARIZONA 85028
PHONE: (602) 850-8101



PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

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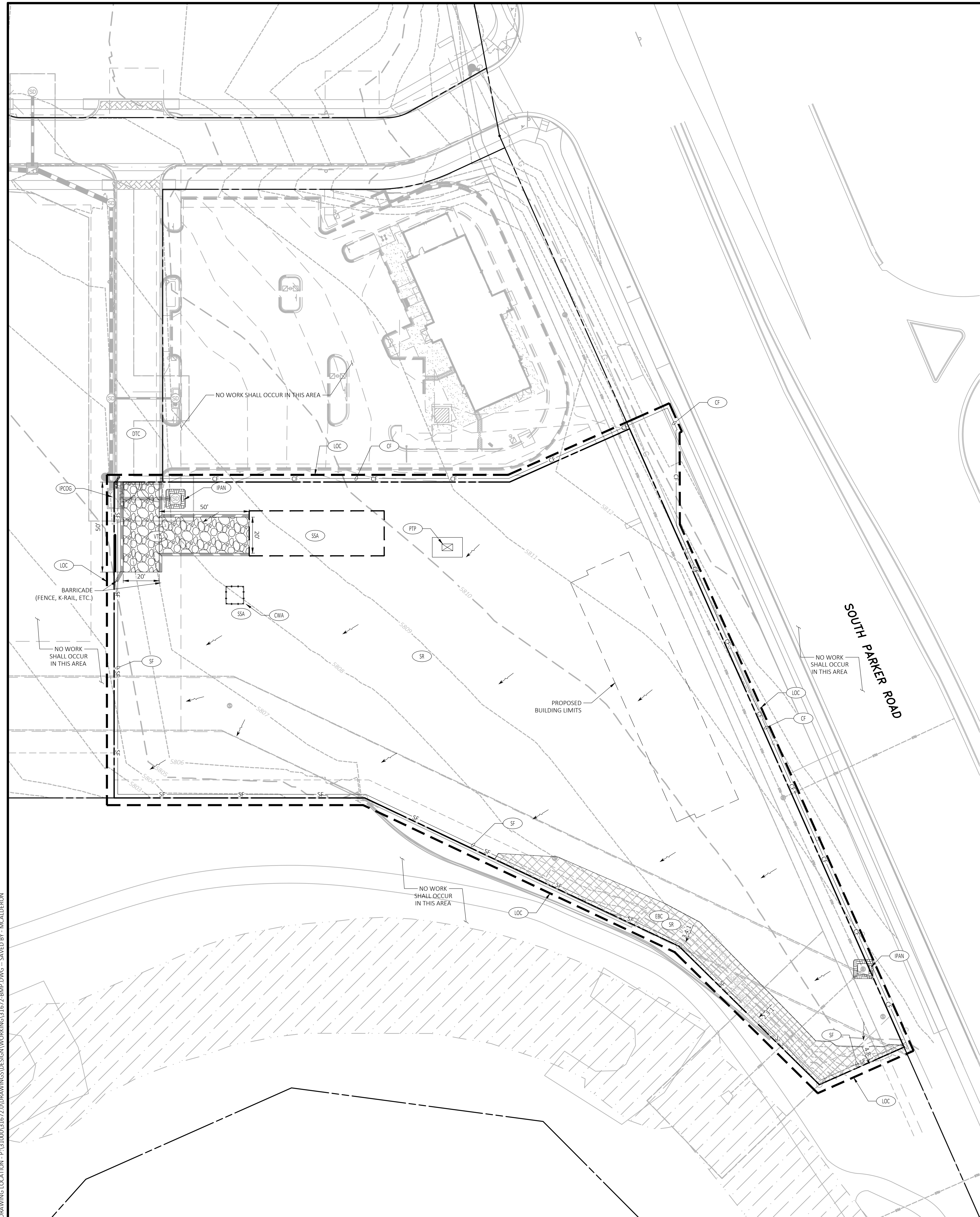
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Thomas A. Durkin
Town of Parker, Director of Engineering
Date: 06/26/2023

EXISTING CONDITION
& DEMOLITION PLAN
SHEET TITLE
SHEET NUMBER

C2

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-REMO.DWG - SAVED BY: MACALDERON



LEGAL DESCRIPTION

REFERENCE C1 - COVER SHEET FOR LEGAL DESCRIPTION.

BENCHMARK

DOUGLAS CONTROL MONUMENT #1.095035, A 3 3/4" ALUMINUM CAP.
ELEVATION = 5906.34 FEET (NAVD1988), AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS

THE BEARINGS ARE BASED ON THE WEST ROW LINE OF SOUTH PARKER ROAD ASSUMED TO BEAR S23°56'20"E BETWEEN MONUMENTS FOUND AND DESCRIBED HEREON.

FLOOD ZONE INFORMATION

SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM.
MAP NUMBER: 08535C0076
EFFECTIVE DATE: MARCH 16, 2016

NOTE:

SEE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF PORCHES, RAMPS, VESTIBULE, SLOPED PAVING, TRUCK DOCKS, BUILDING UTILITY ENTRANCE LOCATIONS AND PRECISE BUILDING DIMENSIONS.

GENERAL EROSION NOTES

- REFERENCE C3.2 - CBMP DETAILS FOR TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICE LEGEND AND GENERAL NOTES.
- LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL. THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ESC.
- THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAYBE DETERMINED BY THE TOWN AND THE E.C.S.
- ANY BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION, CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.

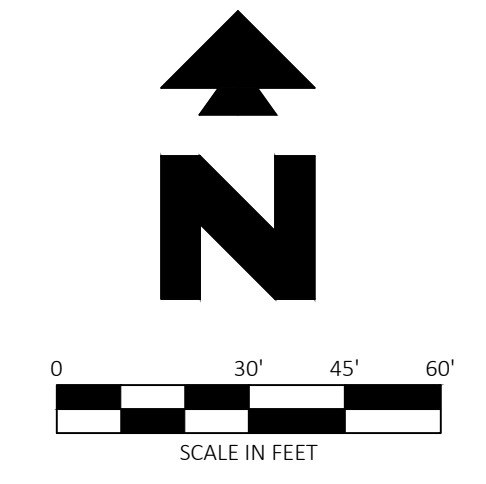
SEQUENCE OF CONSTRUCTION

NOTE: DOWNSLOPE PROTECTIVE MEASURES MUST ALWAYS BE IN PLACE BEFORE SOIL IS DISTURBED. CONSTRUCTION STEPS CAN BE IMPLEMENTED CONCURRENTLY ONLY IF ASSOCIATED DOWNSLOPE PROTECTIVE MEASURES HAVE BEEN INSTALLED FOR EACH ACTIVITY.

- INSTALL STABILIZED CONSTRUCTION ENTRANCES.
- CONSTRUCT THE SILT FENCES ON THE SITE.
- INSTALL INLET PROTECTION DEVICES ON EXISTING INLETS.
- PREPARE TEMPORARY PARKING AND STORAGE AREA.
- CLEAR AND GRUB THE SITE.
- BEGIN GRADING THE SITE.
- START CONSTRUCTION OF BUILDING PAD AND STRUCTURES.
- TEMPORARILY SEED DENUDED AREAS.
- INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, CURBS AND GUTTERS.
- INSTALL INLET PROTECTION DEVICES.
- INSTALL RIP RAP AROUND OUTLET STRUCTURES.
- PREPARE SITE FOR PAVING.
- PAVE SITE.
- COMPLETE GRADING AND INSTALL PERMANENT SEEDING AND PLANTING.
- REMOVE ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES (ONLY IF SITE IS STABILIZED).



Know what's below.
Call before you dig.



EXISTING LEGEND

- PROPERTY LINE/RIGHT OF WAY LINE
- ⊙ COMMUNICATIONS MANHOLE
- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM SEWER MANHOLE
- GAS --- UNDERGROUND GAS LINES
- COM --- UNDERGROUND COMMUNICATIONS LINES
- STM --- UNDERGROUND STORM SEWER LINES

PROPOSED LEGEND

- LOC --- LIMITS OF CONSTRUCTION BOUNDARY 1.85 AC (4' OFFSET FOR CLARITY)
- FLOW ARROW
- ⊙ CD --- CHECK DAM (REF. SHEET C3.7 - CBMP DETAILS)
- CF --- CONSTRUCTION FENCE (2' OFFSET FOR CLARITY) (REF. SHEET C3.2 & C3.3)
- CP --- CULVERT PROTECTION (REF. SHEET C3.7 - CBMP DETAILS)
- CWA --- CONCRETE WASHOUT AREA (REF. SHEET C3.3 - CBMP DETAILS)
- D --- DEWATERING (REF. SHEET C3.7 - CBMP DETAILS)
- DD --- DIVERSION DITCH (REF. SHEET C3.7 & C3.8 - CBMP DETAILS)
- DP --- DETENTION POND PROTECTION (REF. SHEET C3.8 - CBMP DETAILS)
- DT --- DEBRIS TRASH CONTROL (REF. SHEET C3.3 - CBMP DETAILS)
- ECB --- EROSION CONTROL BLANKET (REF. SHEET C3.3 - CBMP DETAILS)
- IPAN --- INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT (REF. SHEET C3.4)
- IPAP --- INLET PROTECTION FOR AREA INLETS IN PAVEMENT (REF. SHEET C3.4)
- IPCG --- INLET PROTECTION, CURB ON-GRADE, TYPE R INLET (REF. SHEET C3.4)
- IPCD --- INLET PROTECTION, CURB ON SUMP, TYPE R INLET (REF. SHEET C3.8)
- LP --- LOT PROTECTION (REF. SHEET C3.8 - CBMP DETAILS)
- MWP --- MASONRY WORK PROTECTION (REF. SHEET C3.9 - CBMP DETAILS)
- PTP --- PORTABLE TOILET PROTECTION (REF. SHEET C3.4 - CBMP DETAILS)
- RTSC --- ROUGH CUT STREET CONTROL (REF. SHEET C3.9 - CBMP DETAILS)
- RS --- ROCK SOCK (REF. SHEET C3.5 - CBMP DETAILS)
- RSS --- ROCK SOCK IN SWALE (REF. SHEET C3.9 - CBMP DETAILS)
- SB --- SWALE BALE (REF. SHEET C3.9 - CBMP DETAILS)
- SCL --- SEDIMENT CONTROL LOGS (REF. SHEET C3.5 - CBMP DETAILS)
- SF --- SILT FENCE (2' OFFSET FOR CLARITY) (REF. SHEET C3.5 & C3.6 - CBMP DETAILS)
- SMD --- SEEDING, MULCHING AND CRIMPING (REF. SHEET C3.6 - CBMP DETAILS)
- SR --- SURFACE ROUGHEN (REF. SHEET C3.6 - CBMP DETAILS)
- SSA --- STABILIZED STAGING AREA (REF. SHEET C3.6 - CBMP DETAILS)
- STP --- SIDEWALK TRANSITION PROTECTION (REF. SHEET C3.6 - CBMP DETAILS)
- SP --- STOCKPILE PROTECTION (REF. SHEET C3.9 - CBMP DETAILS)
- T --- TEMPORARY IRRIGATION (REF. SHEET C3.11 - CBMP DETAILS)
- TSSB --- TEMPORARY SEDIMENT BASIN (REF. SHEET C3.10 - CBMP DETAILS)
- VTC --- VEHICLE TRACKING CONTROL (REF. SHEET C3.10 & C3.11 - CBMP DETAILS)



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11811 N. TATUM BOULEVARD,
PHOENIX, ARIZONA 85028
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PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85257
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



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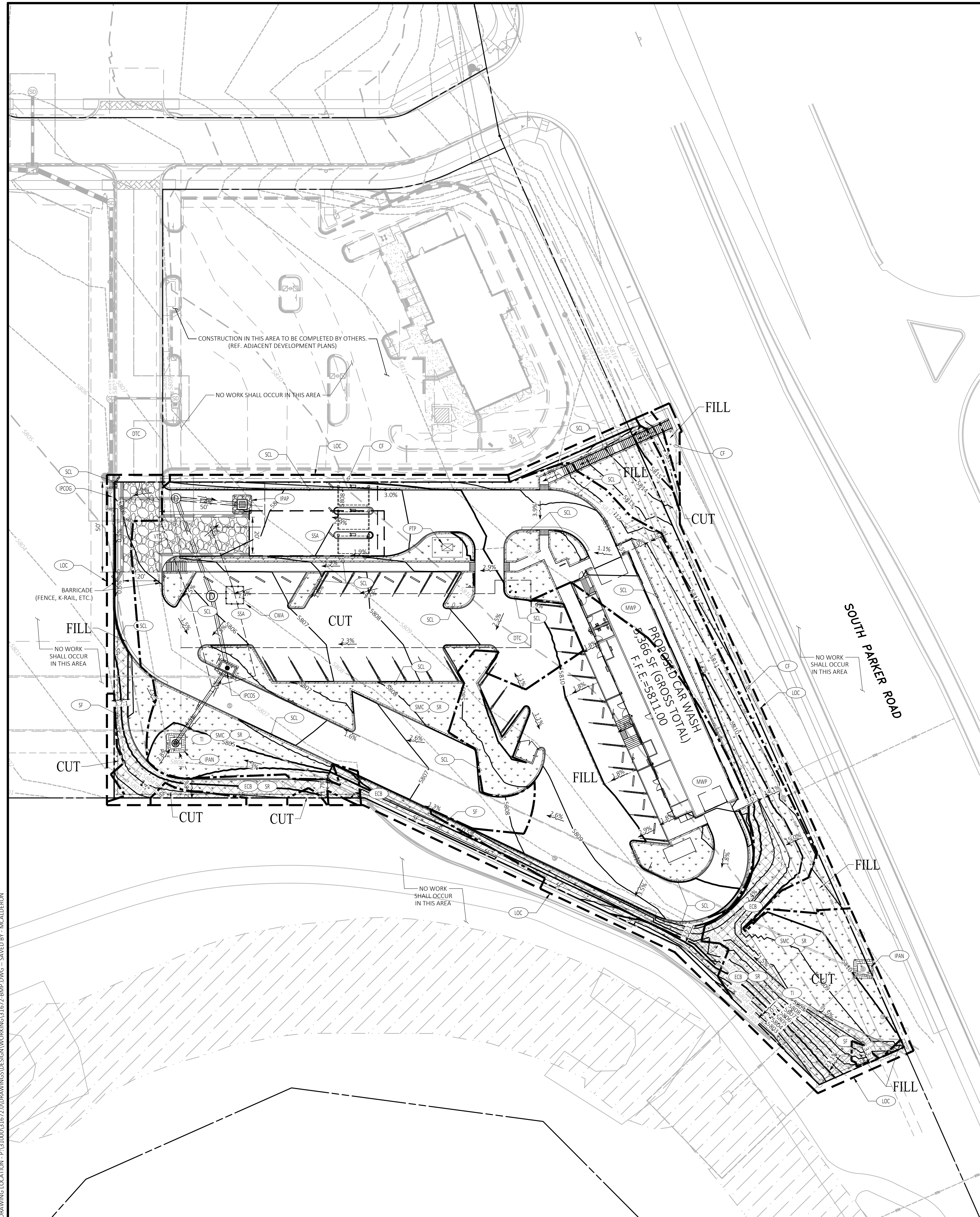
Thomas A. Durbin
Town of Parker, Director of Engineering
Date: 06/26/2023

PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP PLAN (INITIAL)
SHEET TITLE
SHEET NUMBER

C3

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-BMP.DWG - SAVED BY: MCALESDERON



LEGAL DESCRIPTION
 REFERENCE C1 - COVER SHEET FOR LEGAL DESCRIPTION.

BENCHMARK
 DOUGLAS CONTROL MONUMENT #1.095035, A 3" ALUMINUM CAP.
 ELEVATION = 5906.34 FEET (NAVD1988), AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS
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FLOOD ZONE INFORMATION
 SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM.
 MAP NUMBER: 08055C00676
 EFFECTIVE DATE: MARCH 16, 2016

NOTE:
 SEE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF PORCHES, RAMPS, VESTIBULE, SLOPED PAVING, TRUCK DOCKS, BUILDING UTILITY ENTRANCE LOCATIONS AND PRECISE BUILDING DIMENSIONS.

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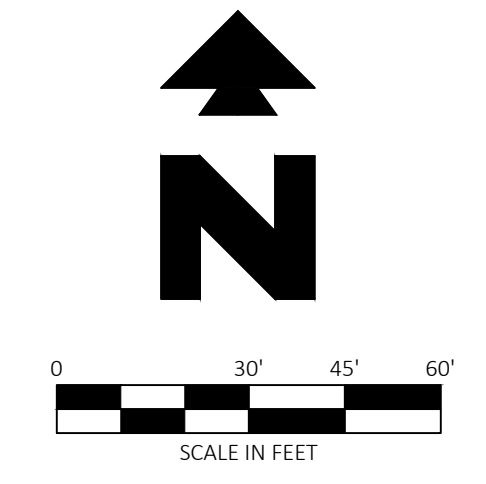
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CUT/FILL SUMMARY

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
CUT # FILL	1.000	1.000	80723.77 Sq. Ft.	1763.91 Cu. Yd.	589.54 Cu. Yd.	1174.37 Cu. Yd. <Cut>
Totals			80723.77 Sq. Ft.	1763.91 Cu. Yd.	589.54 Cu. Yd.	1174.37 Cu. Yd. <Cut>



Know what's below.
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EXISTING LEGEND

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- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM SEWER MANHOLE
- GAS --- UNDERGROUND GAS LINES
- COM --- UNDERGROUND COMMUNICATIONS LINES
- STM --- UNDERGROUND STORM SEWER LINES

PROPOSED LEGEND

- LIMITS OF CONSTRUCTION BOUNDARY 1.85 AC (4' OFFSET FOR CLARITY)
- CONTOUR ELEVATIONS
- GRADE BREAK
- STORM DRAIN
- FLOW ARROW
- ⊙ CHECK DAM (REF. SHEET C3.7 - CBMP DETAILS)
- ⊙ CONSTRUCTION FENCE (2' OFFSET FOR CLARITY) (REF. SHEET C3.2 & C3.3)
- ⊙ CULVERT PROTECTION (REF. SHEET C3.7 - CBMP DETAILS)
- ⊙ CWA CONCRETE WASHOUT AREA (REF. SHEET C3.3 - CBMP DETAILS)
- ⊙ DEWATERING (REF. SHEET C3.7 - CBMP DETAILS)
- ⊙ DIVERSION DITCH (REF. SHEET C3.7 & C3.8 - CBMP DETAILS)
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- ⊙ EROSION CONTROL BLANKET (REF. SHEET C3.3 - CBMP DETAILS)
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- ⊙ MASONRY WORK PROTECTION (REF. SHEET C3.9 - CBMP DETAILS)
- ⊙ PORTABLE TOILET PROTECTION (REF. SHEET C3.4 - CBMP DETAILS)
- ⊙ ROUGH CUT STREET CONTROL (REF. SHEET C3.9 - CBMP DETAILS)
- ⊙ ROCK SOCK (REF. SHEET C3.5 - CBMP DETAILS)
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- ⊙ SWALE BALE (REF. SHEET C3.9 - CBMP DETAILS)
- ⊙ SEDIMENT CONTROL LOGS (REF. SHEET C3.5 - CBMP DETAILS)
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- ⊙ SEEDING, MULCHING AND CRIMPING (REF. SHEET C3.6 - CBMP DETAILS)
- ⊙ SURFACE ROUGHEN (REF. SHEET C3.6 - CBMP DETAILS)
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- ⊙ TEMPORARY IRRIGATION (REF. SHEET C3.11 - CBMP DETAILS)
- ⊙ TEMPORARY SEDIMENT BASIN (REF. SHEET C3.10 - CBMP DETAILS)
- ⊙ VEHICLE TRACKING CONTROL (REF. SHEET C3.10 & C3.11 - CBMP DETAILS)



CEI ENGINEERING ASSOCIATES, INC.
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PLANS PREPARED FOR
 QUICK N CLEAN
 7291 E. ADOBE DRIVE, SUITE 115
 SCOTTSDALE, AZ 85258
 PHONE: (480) 707-3531

REVISION

NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
 NOT FOR
 CONSTRUCTION

QUICK N CLEAN
 9572 TWENTY MILE ROAD
 PARKER, COLORADO
 CONSTRUCTION DOCUMENTS



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Thomas A. Durbin
 Town of Parker, Director of Engineering
 Date: 06/26/2023

PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP PLAN
 INTERIM/FINAL
 SHEET TITLE
 SHEET NUMBER

C3.1

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-CBMP.DWG - SAVED BY: MICALDERON

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- CD CHECK DAM
- CF CONSTRUCTION FENCE
- CP CULVERT PROTECTION
- CWA CONCRETE WASHOUT AREA
- D DEWATERING
- DD DIVERSION DITCH
- DP DETENTION POND PROTECTION
- DTC DEBRIS TRASH CONTROL
- ECB EROSION CONTROL BLANKET
- IPAN INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
- IPAP INLET PROTECTION FOR AREA INLETS IN PAVEMENT
- IPCOG INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
- IPCOS INLET PROTECTION, CURB ON SUMP, TYPE R INLET

Town of Parker COLORADO | **CBMP** | **LEGEND**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

11. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
12. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.
13. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.
14. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.
15. ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.
16. ALL TRASH RECEPTACLES ON SITE SHALL BE EMPTIED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONFINED TO THE RECEPTACLE.
17. ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.
18. ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REBAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.
19. THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT. FAILURE TO ABIDE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.
20. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PAVED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.
21. NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.
22. ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL BLANKET(S). TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS.
23. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.
24. A WATER SOURCE SHALL BE AVAILABLE ONSITE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FUGITIVE DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.

Town of Parker COLORADO | **CBMP** | **GEN NOTES**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 4
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- LP LOT PROTECTION
- MWP MASONRY WORK PROTECTION
- PTP PORTABLE TOILET PROTECTION
- RCSC ROUGH CUT STREET CONTROL
- RS ROCK SOCK
- RSS ROCK SOCK IN SWALE
- SB STRAW BALE
- SCL SEDIMENT CONTROL LOGS
- SF SILT FENCE
- SMC SEEDING, MULCHING AND CRIMPING
- SR SURFACE ROUGHING
- SSA STABILIZED STAGING AREA
- STP SIDEWALK TRANSITION PROTECTION

Town of Parker COLORADO | **CBMP** | **LEGEND**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

25. ALL CHEMICAL OR HAZARDOUS MATERIAL SPILLS, INCLUDING CONCRETE WASHOUT WATER, WHICH MAY ENTER WATERS OF THE STATE OF COLORADO, WHICH INCLUDES BUT ARE NOT LIMITED TO, SURFACE WATER, GROUND WATER, DRY GULLIES OR STORM SEWERS LEADING TO SURFACE WATER, SHALL BE IMMEDIATELY REPORTED TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) PER 25-8-601, AND THE TOWN OF PARKER. RELEASES OF PETROLEUM PRODUCTS AND CERTAIN HAZARDOUS SUBSTANCES LISTED UNDER THE FEDERAL CLEAN WATER ACT (40 CFR PART 116) MUST BE REPORTED TO THE NATIONAL RESPONSE CENTER AND THE CDPHE. SPILLS THAT POSE AN IMMEDIATE SAFETY HAZARD SHALL BE REPORTED TO 911.
26. THE CLEANING OF CONCRETE TRUCKS AND EQUIPMENT IS RESTRICTED TO THE APPROVED CONCRETE WASHOUT LOCATION ON THE JOB SITE. CONCRETE WASH WATER SHALL NOT BE DISCHARGED TO STATE WATERS OR STORM SEWER SYSTEMS.
27. VEHICLE AND EQUIPMENT DEGREASING IS PROHIBITED ON THE JOB SITE.
28. ALL DEWATERING ON SITE SHALL BE COORDINATED WITH THE TOWN'S INSPECTOR. A STATE PERMIT MAY BE REQUIRED FOR DEWATERING. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR OBTAINING AND ADHERING TO ALL APPLICABLE PERMITS.
29. HYDRAULIC SEEDING AND/OR HYDRAULIC MULCHING ARE ONLY ALLOWED IN AREAS UNDER TEMPORARY OR PERMANENT IRRIGATION OR FOR THE PURPOSE OF TEMPORARY SOIL STABILIZATION.
30. APPLICABLE CONSTRUCTION BMPs SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL ALL LANDSCAPING HAS BEEN INSTALLED AND THE DESIRABLE VEGETATION HAS REACHED A POINT IN WHICH EROSION AND SEDIMENTATION IS NO LONGER A CONCERN AS DETERMINED BY THE TOWN'S INSPECTOR.
31. GRADING SECURITY RELEASE REQUIREMENTS:
 - 1) DEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H OR ITEM I (BELOW).
 - A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
 - B. UNIFORM PERENNIAL VEGETATION COVER HAS BEEN ESTABLISHED WITH AN INDIVIDUAL PLANT DENSITY OF AT LEAST SEVENTY PERCENT (70%) OF PRE-DISTURBANCE LEVELS.
 - C. ALL CBMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
 - D. IF ANY EROSION IS PRESENT, IT IS INSIGNIFICANT AND IS NOT LEAVING THE SITE AND/OR LEADING INTO ANY ON-SITE DRAINAGE INFRASTRUCTURE THAT MAY CONVEY SURFACE WATER OFF SITE.
 - E. WEEDS REPRESENT NO MORE THAN FIFTY PERCENT (50%) OF THE TOTAL VEGETATION ON THE SITE.
 - F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
 - G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
 - H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.
 - I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR ASSIGNMENT AS PROVIDED BY SECTION 11.10.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION AT THE TIME OF CLOSING TO ENSURE THAT THE NEW SITE OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.

Town of Parker COLORADO | **CBMP** | **GEN NOTES**
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- TI TEMPORARY IRRIGATION
- TSB TEMPORARY SEDIMENT BASIN
- VTC VEHICLE TRACKING CONTROL

Town of Parker COLORADO | **CBMP** | **LEGEND**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- 2) NONDEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H AND J, OR ITEMS I AND J (BELOW).
 - A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
 - B. ALL CBMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
 - C. EROSION IS NEGLIGIBLE, IF EVEN PRESENT.
 - D. THE VEGETATION REPRESENTS A PERENNIAL STAND OF A DENSE, UNIFORM SURFACE OF GRASS WITH NO AREA GREATER THAN ONE (1) SQUARE FOOT THAT IS BARREN OF DESIRABLE VEGETATION. INFREQUENT, WIDELY SCATTERED AREAS WHERE NATIVE VEGETATION HAS NOT YET TAKEN HOLD MAY QUALIFY FOR ACCEPTANCE AT THE DISCRETION OF THE TOWN.
 - E. WEEDS REPRESENT NO MORE THAN TEN PERCENT (10%) OF THE TOTAL VEGETATION ON SITE.
 - F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
 - G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
 - H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.
 - I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR THE GRADING PERMIT HAS BEEN ASSIGNED AS PROVIDED BY SECTION 11.10.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION, AT THE TIME OF CLOSING ON THE SALE OF A SITE THAT IS SUBJECT TO A GRADING PERMIT, TO ENSURE THAT THE NEW PROPERTY OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.
 - J. ALL KNOWN DRAINAGE ISSUES ASSOCIATED WITH THE PROJECT HAVE BEEN MITIGATED AND A SUFFICIENT AMOUNT OF TIME HAS PASSED TO ENSURE THAT SUCH ISSUES HAVE BEEN CORRECTED. THIS REQUIREMENT DOES NOT INCLUDE THOSE DRAINAGE ISSUES ORIGINATING ON RESIDENTIAL LOTS.
 - (D) NOXIOUS WEEDS MUST BE CONTROLLED AS PROVIDED UNDER STATE LAW AND SECTION 6.01.260 OF THE TOWN OF PARKER MUNICIPAL CODE. FAILURE TO CONTROL NOXIOUS WEEDS ON THE SITE MAY CONSTITUTE A NUISANCE, SUBJECT TO THE PENALTIES CONTAINED IN THE CODE.
- DEFINITIONS:**
DEVELOPABLE PROPERTY MEANS ANY LAND THAT HAS BEEN GRADED AND IS PART OF A PLATTED LOT OR PLATTED TRACT OF RECORD THAT WAS PLATTED FOR FUTURE DEVELOPMENT, INCLUDING RESIDENTIAL HOME CONSTRUCTION OR PUBLIC IMPROVEMENTS.
NONDEVELOPABLE PROPERTY MEANS LAND THAT HAS BEEN GRADED AND WILL NOT BE FURTHER DISTURBED AS PART OF ANY FUTURE DEVELOPMENT. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO: PARKS, OPEN SPACE, HOMEOWNER ASSOCIATION OR BUSINESS ASSOCIATION PLATTED TRACTS, DETENTION PONDS AND DRAINAGEWAYS.
 32. FAILURE TO COMPLY WITH ANY OF THE REQUIREMENTS DESCRIBED IN THIS SECTION MAY RESULT IN THE ISSUANCE OF: A NOTICE OF INTENT TO ISSUE A STOP WORK ORDER, A STOP WORK ORDER AND/OR THE REMEDIES/PENALTIES DESCRIBED IN CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE.
 33. ANY PERSON CONVICTED OF VIOLATING ANY PROVISION OF THE TOWN OF PARKER GRADING & EARTH MOVEMENT SECTION OF THE MUNICIPAL CODE SHALL BE GUILTY OF A MISDEMEANOR AND, UPON CONVICTION, BE PUNISHED BY A FINE OF NOT MORE THAN FOUR HUNDRED NINETY NINE DOLLARS (\$499.00) FOR EACH SEPARATE OFFENSE. EACH DAY A VIOLATION CONTINUES SHALL CONSTITUTE A SEPARATE OFFENSE. THE TOWN ALSO MAY SEEK IN MUNICIPAL COURT AN INJUNCTION, ABATEMENT, RESTITUTION OR ANY OTHER REMEDY TO PREVENT, INJUNCTION, ABATE OR REMOVE THE VIOLATION. A PERSON CONVICTED OF VIOLATING CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE SHALL BE LIABLE FOR THE ACTUAL COST OF REHABILITATING THE PROPERTY. THE COSTS MAY BE RECOVERED AS RESTITUTION IN MUNICIPAL COURT PROCEEDINGS OR IN A SEPARATE CIVIL ACTION.
 34. THE TOWN OF PARKER RESERVES THE RIGHT TO ALLOW MODIFICATIONS AND SUBSTITUTIONS TO THE CBMP NOTES AND DETAILS WHEN SUCH MODIFICATIONS AND SUBSTITUTIONS OFFER THE SAME LEVEL OF PROTECTION AS THE STANDARD REQUIREMENTS BASED UPON THE SPECIFIC SITUATION, AS DETERMINED BY TOWN STAFF DUE TO THE INSIGNIFICANCE AND REGULARITY OF SUCH MODIFICATIONS AND SUBSTITUTIONS. THE APPROVAL OF SUCH VARIATIONS MAY NOT BE DOCUMENTED BY TOWN STAFF.

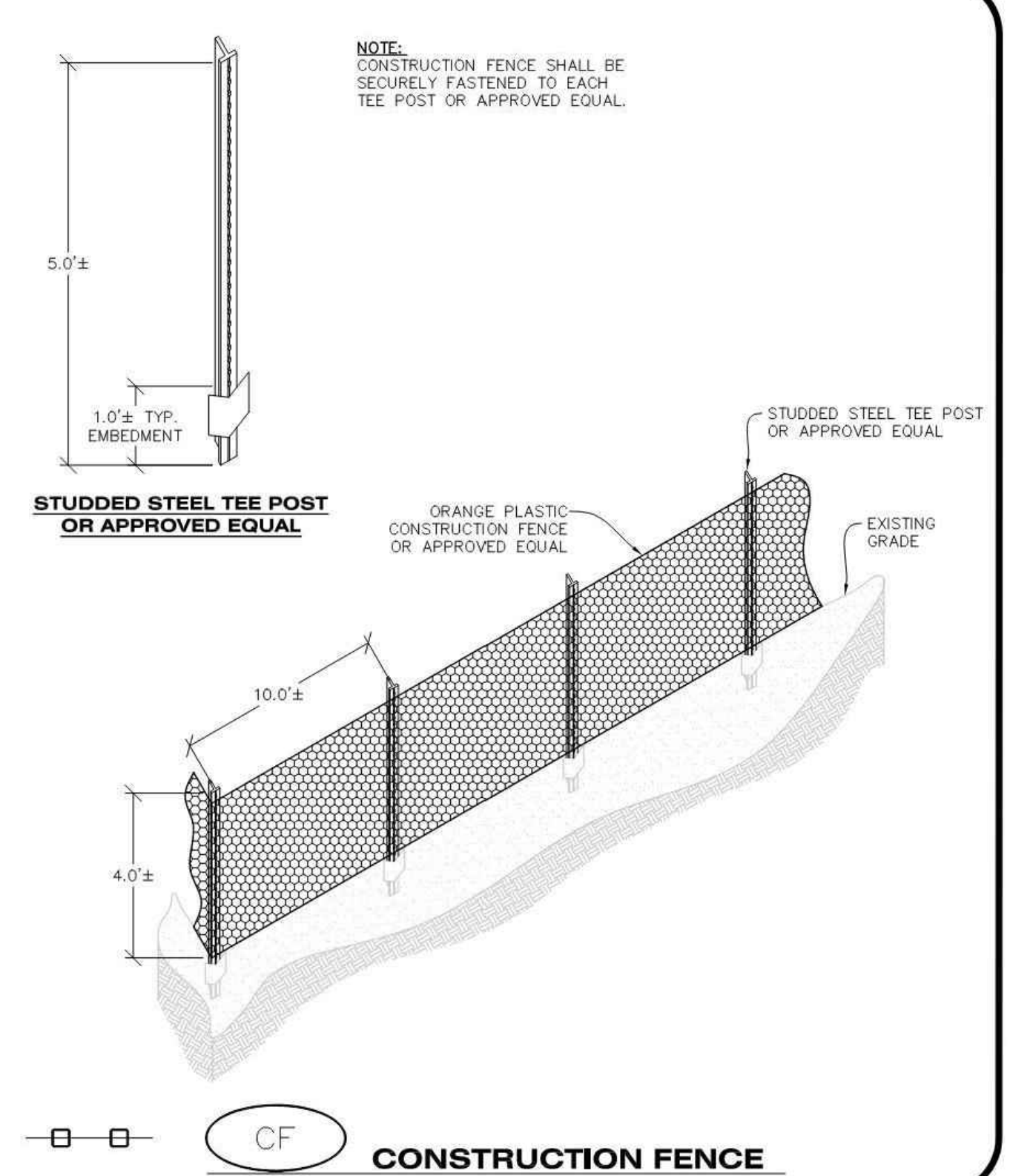
Town of Parker COLORADO | **CBMP** | **GEN NOTES**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 4 OF 4
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

1. GRADING, DRILLING, CLEARING, EXCAVATING, BACK-FILLING, SOIL STRIPPING, SOIL IMPORTING EXPORTING OR ANY OTHER FORM OF SOIL DISTURBANCE SHALL NOT COMMENCE UNTIL A TOWN OF PARKER GRADING/EXCAVATION PERMIT HAS BEEN ISSUED.
2. THE TOWN OF PARKER GRADING/EXCAVATION PERMIT IS VALID FOR A PERIOD OF 2 YEARS FROM THE DATE OF ISSUANCE. ANY LAPSE IN PERMIT COVERAGE MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER AND/OR FINES.
3. ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE CBMP PLAN INSTALLATION NOTES AND DETAILS AND/OR PERMITTED VARIANCES. ALL MATERIALS, WORKMANSHIP AND INSTALLATION METHODS ARE SUBJECT TO INSPECTION BY THE TOWN'S INSPECTOR. THE TOWN OF PARKER RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS, WORKMANSHIP AND/OR INSTALLATION METHODS THAT DO NOT CONFORM TO THE CBMP PLAN AND/OR PERMITTED VARIANCES.
4. THE EROSION CONTROL SUPERVISOR SHALL REVISE OR MODIFY THE EROSION AND SEDIMENT CONTROL MEASURES IF IT BECOMES APPARENT THAT THE ORIGINAL PLAN IS INADEQUATE, OR AS A RESULT OF DEFICIENCIES IDENTIFIED DURING INSPECTIONS PERFORMED BY THE TOWN'S INSPECTOR.
5. THE INSTALLATION OF ADDITIONAL EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs) MAY BE REQUIRED OF THE EROSION CONTROL SUPERVISOR, PROPERTY OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS AT ANY TIME THROUGHOUT THE DURATION OF THE PROJECT CONSTRUCTION AND/OR SUBSEQUENT REVEGETATION PERIOD.
6. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE SITE REMAINS IN COMPLIANCE WITH THE NOTICE OF NUISANCE REPORTS (o.k.o. CONSTRUCTION SITE RUNOFF CONTROL INSPECTION REPORTS), APPROVED CBMP PLAN(S) AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
7. THE EROSION CONTROL SUPERVISOR SHALL BE READILY AVAILABLE TO DISCUSS AND CORRECT ANY PROBLEMS THAT MAY ARISE RELATING TO GRADING, EROSION AND SEDIMENT CONTROL.
8. IF IT IS ANTICIPATED THAT ALL OR A PORTION OF THE PROJECT WILL OCCUR DURING SEASONS SUSCEPTIBLE TO SNOWFALL, THE USE OF CERTAIN EROSION AND SEDIMENT CONTROL BMPs ADJACENT TO PUBLIC ROADWAYS MAY NEED TO BE RECONSIDERED DUE TO THE NATURE OF SNOW REMOVAL OPERATIONS. AN APPROVED ALTERNATIVE MAY BE NECESSARY TO MINIMIZE DAMAGE FROM THESE OPERATIONS. THE TOWN OF PARKER ASSUMES NO RESPONSIBILITY FOR DAMAGE TO ANY BMPs AS A RESULT OF SNOW PLOWING AND SNOW REMOVAL.
9. AREAS OF LAND DISTURBANCE EQUAL TO 40 ACRES OR GREATER SHALL NOT BE EXPOSED FOR MORE THAN 30 CONSECUTIVE DAYS WITHOUT TEMPORARY OR PERMANENT STABILIZATION.
10. AUTHORIZED EXEMPTIONS MAY BE ALLOWED TO THE 40-ACRE LIMIT FOR REMOVAL AND STORAGE OF CUT MATERIAL WHERE (A) GEOTECHNICAL LIMITATIONS RESTRICT THE USE OF TEMPORARY OR PERMANENT STABILIZATION OF THE STORED MATERIAL (E.G., SWELLING SOILS, ROCK) AND (B) WHEN THE OWNER CAN DEMONSTRATE THAT THE 40-ACRE LIMIT IS PHYSICALLY AND/OR FINANCIALLY IMPRACTICABLE. FOR SITES GRANTED THIS EXEMPTION, A PHASING AND EARTHWORK QUANTITIES PLAN SHALL BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT AND APPROVED PRIOR TO THE COMMENCEMENT OF LAND DISTURBANCE ACTIVITIES. SUBMITTAL REQUIREMENTS INCLUDE:
 - (i) PHASING PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
 - (ii) EARTHWORK QUANTITY PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
 - (iii) BMP PLAN SHOWING SPECIFIC EROSION AND SEDIMENT CONTROLS FOR EACH PHASE.

Town of Parker COLORADO | **CBMP** | **GEN NOTES**
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Town of Parker COLORADO | **CBMP** | **CF**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
Oct. 2013



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REVISION		
NO.	DESCRIPTION	DATE

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CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
SHEET TITLE
SHEET NUMBER

C3.2

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-CBMP-DWG - SAVED BY: MCALESDON

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

CONSTRUCTION FENCE INSTALLATION NOTES

1. THE CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO EACH POST OR APPROVED EQUAL.

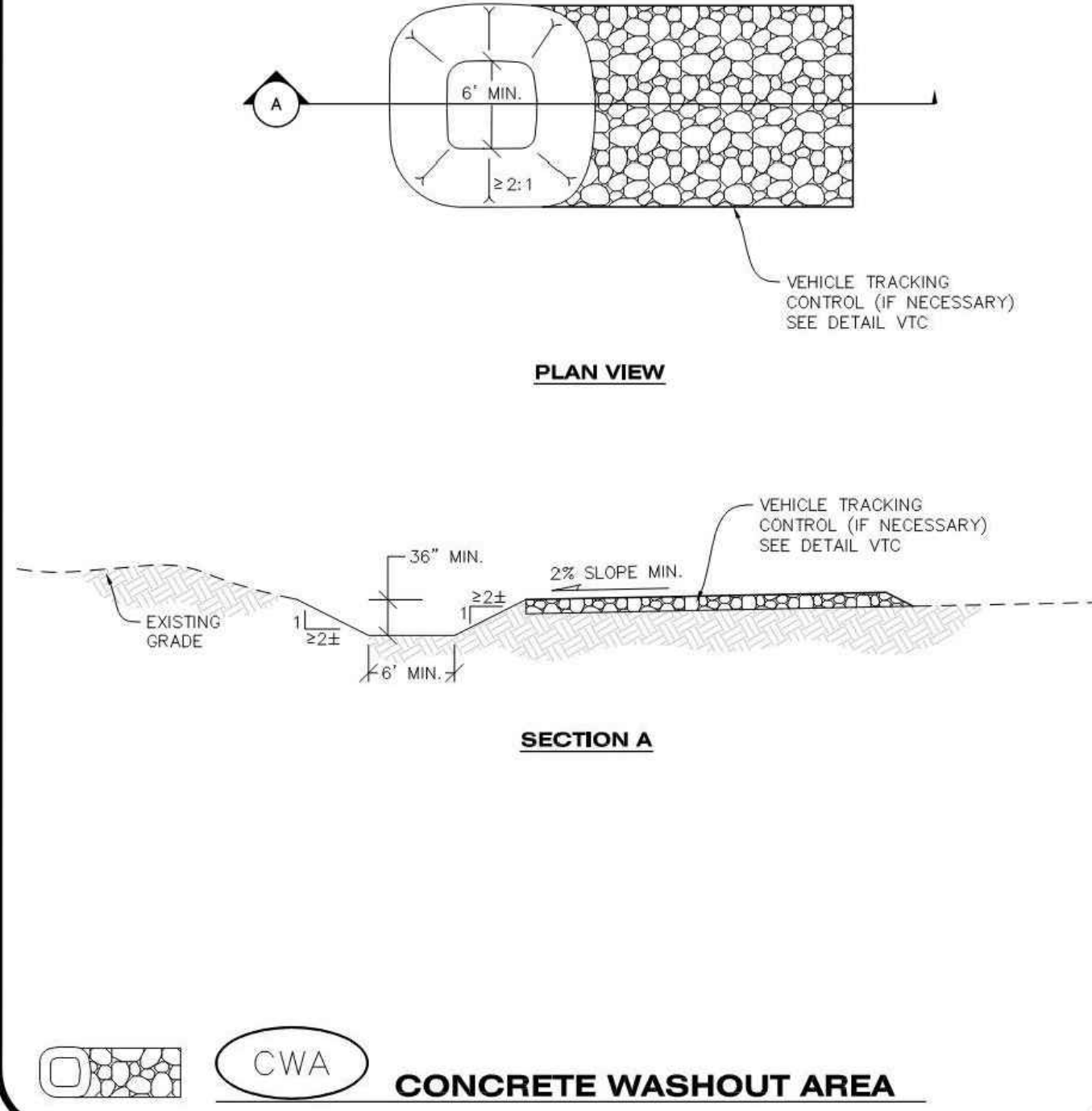
CONSTRUCTION FENCE INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONSTRUCTION FENCE AND MAKE ANY NECESSARY REPAIRS.
2. CONSTRUCTION FENCE SHALL BE REPAIRED WHEN THE FENCING MATERIAL FALLS OUT OF COMPLIANCE WITH THE NOTES AND DETAILS.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

NOTE:
 A VTC PAD IS NOT NECESSARY IF THERE IS ANOTHER VTC PAD IN BETWEEN THE CWA AND ROADWAY, PARKING LOT, DRIVE AISLES, ETC.



CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
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CONCRETE WASHOUT AREA INSTALLATION NOTES

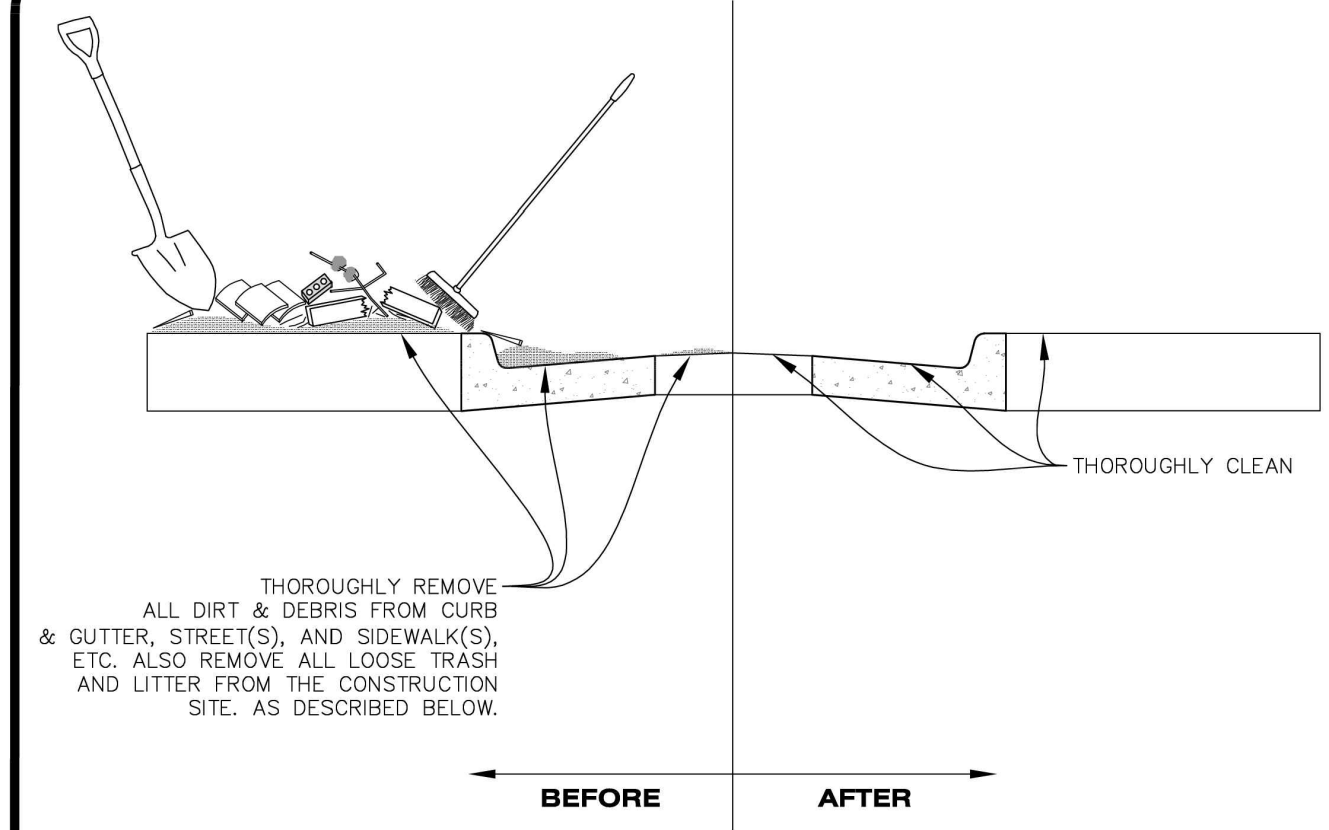
1. CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
2. A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
3. IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.

CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
2. CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
3. CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
4. CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
5. CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
6. WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
 2 OF 2
 Oct. 2013

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NOTE:

1. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY. FURTHERMORE, ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
 1 OF 2
 Oct. 2013

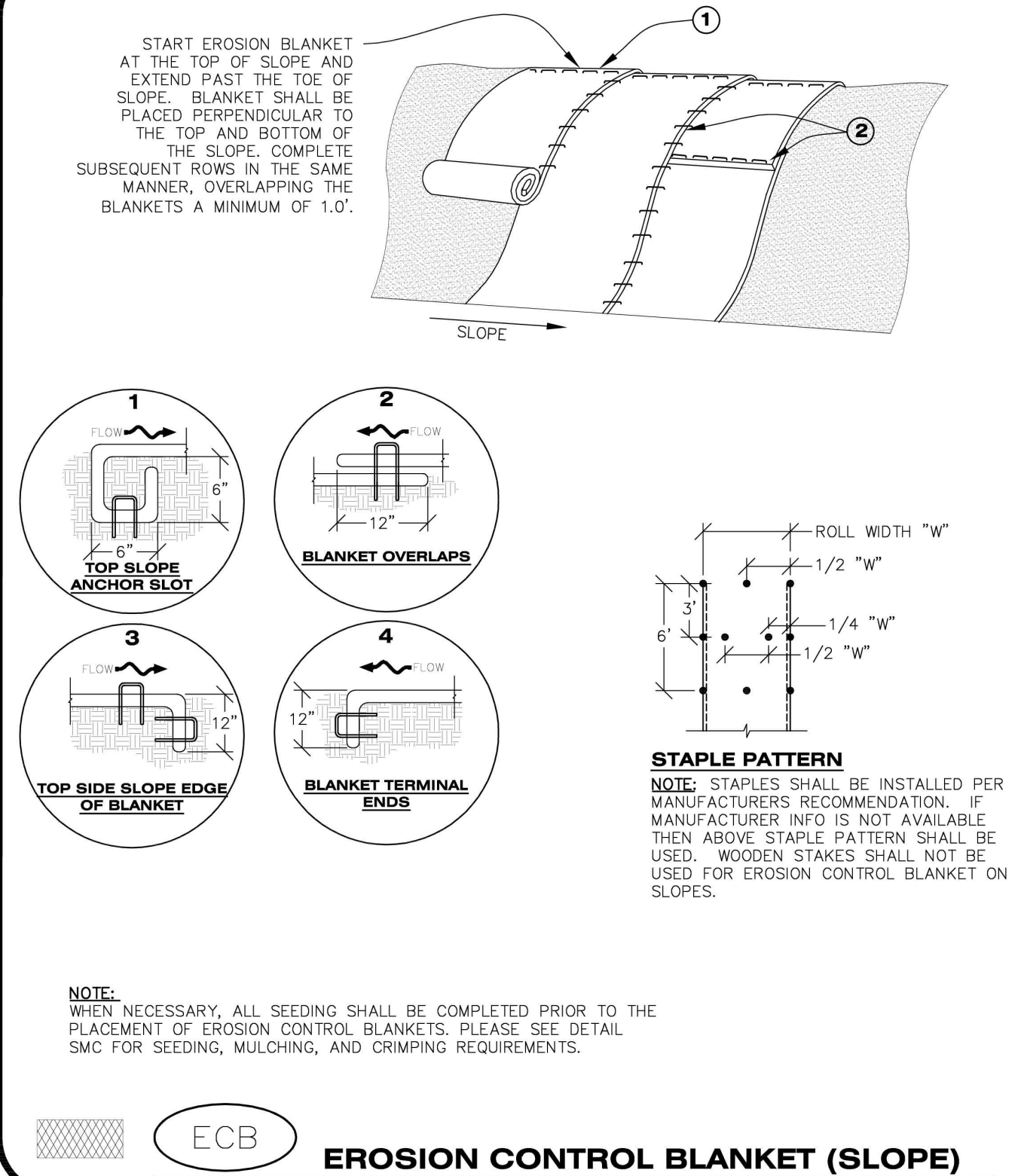
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

DEBRIS CONTROL NOTES:

1. A COMBINATION OF SURFACE SCRAPING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THESE AREAS.
2. ALL CHEMICAL SPILLS AND/OR STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EXTENT PRACTICABLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VAC-TRUCK.
3. ON-SITE PERSONNEL, DELIVERY DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUAL DEBRIS AND TRASH CONTROL.

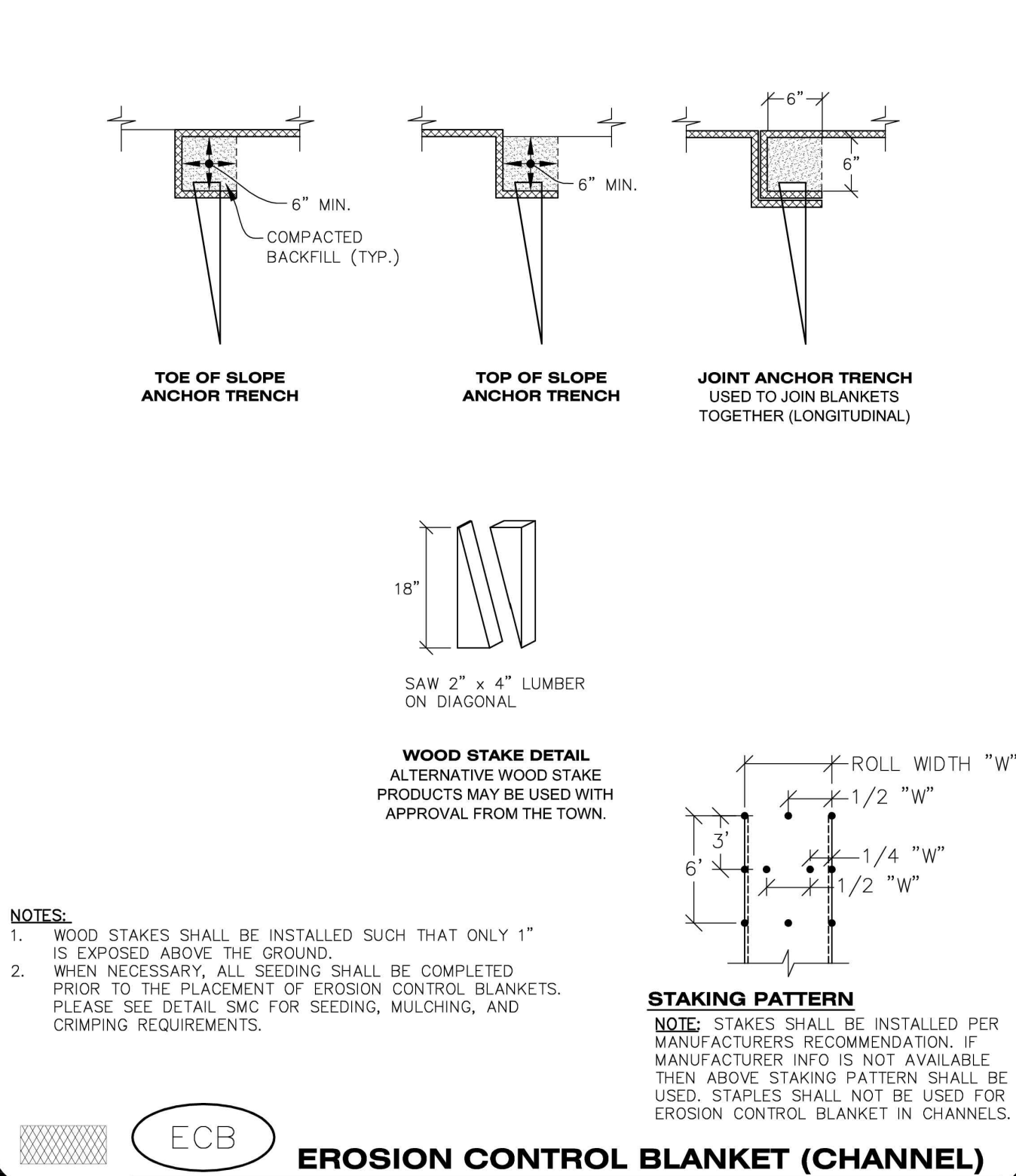
CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
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 Oct. 2013

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CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
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CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
 2 OF 3
 Oct. 2013

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EROSION CONTROL BLANKET INSTALLATION NOTES

1. SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
 2. EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THESE CALCULATIONS, TURF REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.
 3. IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.
 4. EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.
 5. ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
 6. ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELSIOR.
- | MANUFACTURER | PRODUCT NAME |
|----------------------|----------------|
| NORTH AMERICAN GREEN | S150 |
| APPROVED EQUAL | APPROVED EQUAL |
7. IN MOST CASES, EROSION CONTROL BLANKETS FOR SLOPE PROTECTION ARE TO REMAIN IN PLACE PERMANENTLY.

EROSION CONTROL BLANKET MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
 3 OF 3
 Oct. 2013



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CONSTRUCTION DOCUMENTS



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CBMP DETAILS
 SHEET TITLE
 SHEET NUMBER

C3.3

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

AREA INLET PROTECTION INSTALLATION NOTES

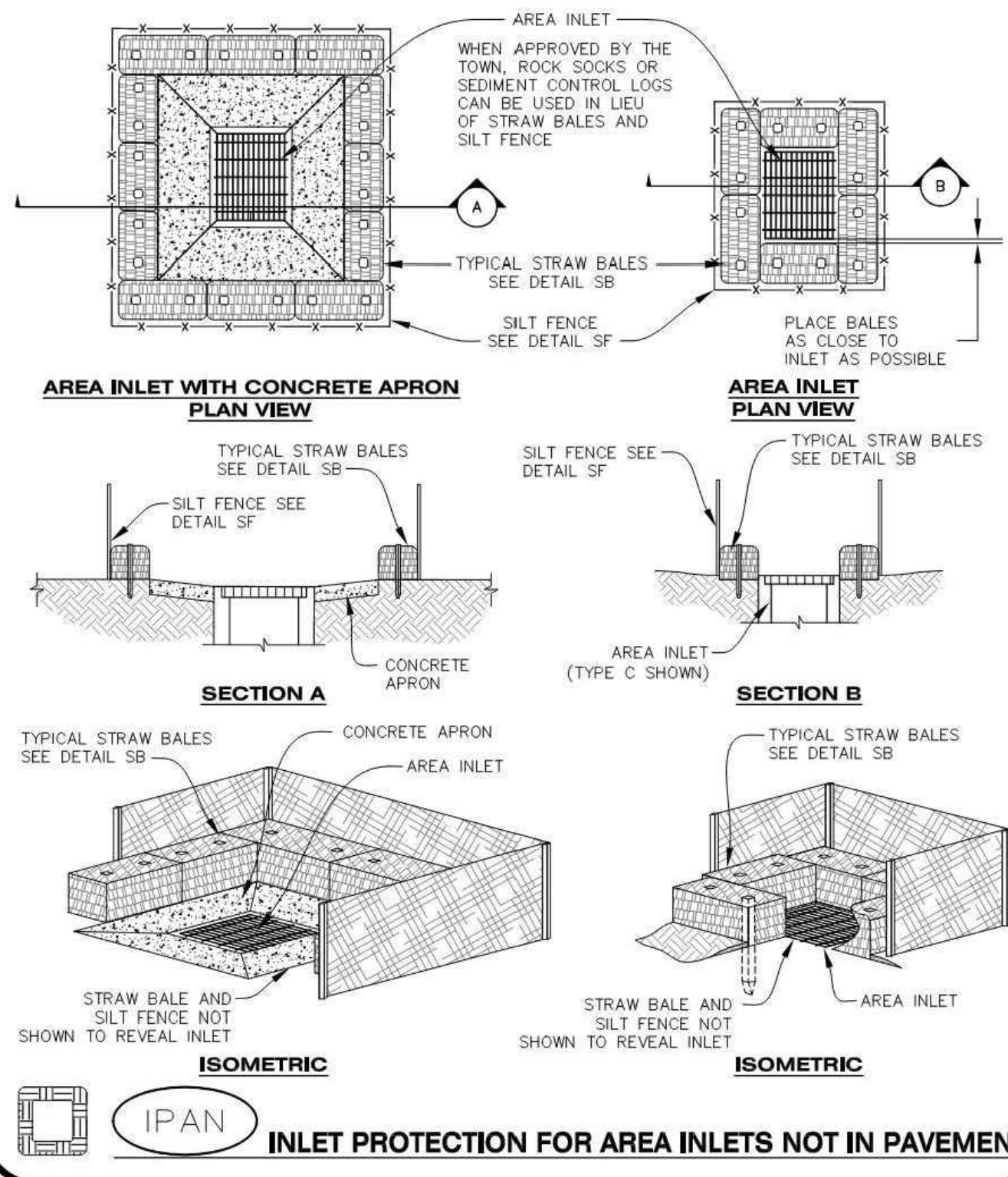
1. SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
2. THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
2. AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
3. WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
4. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

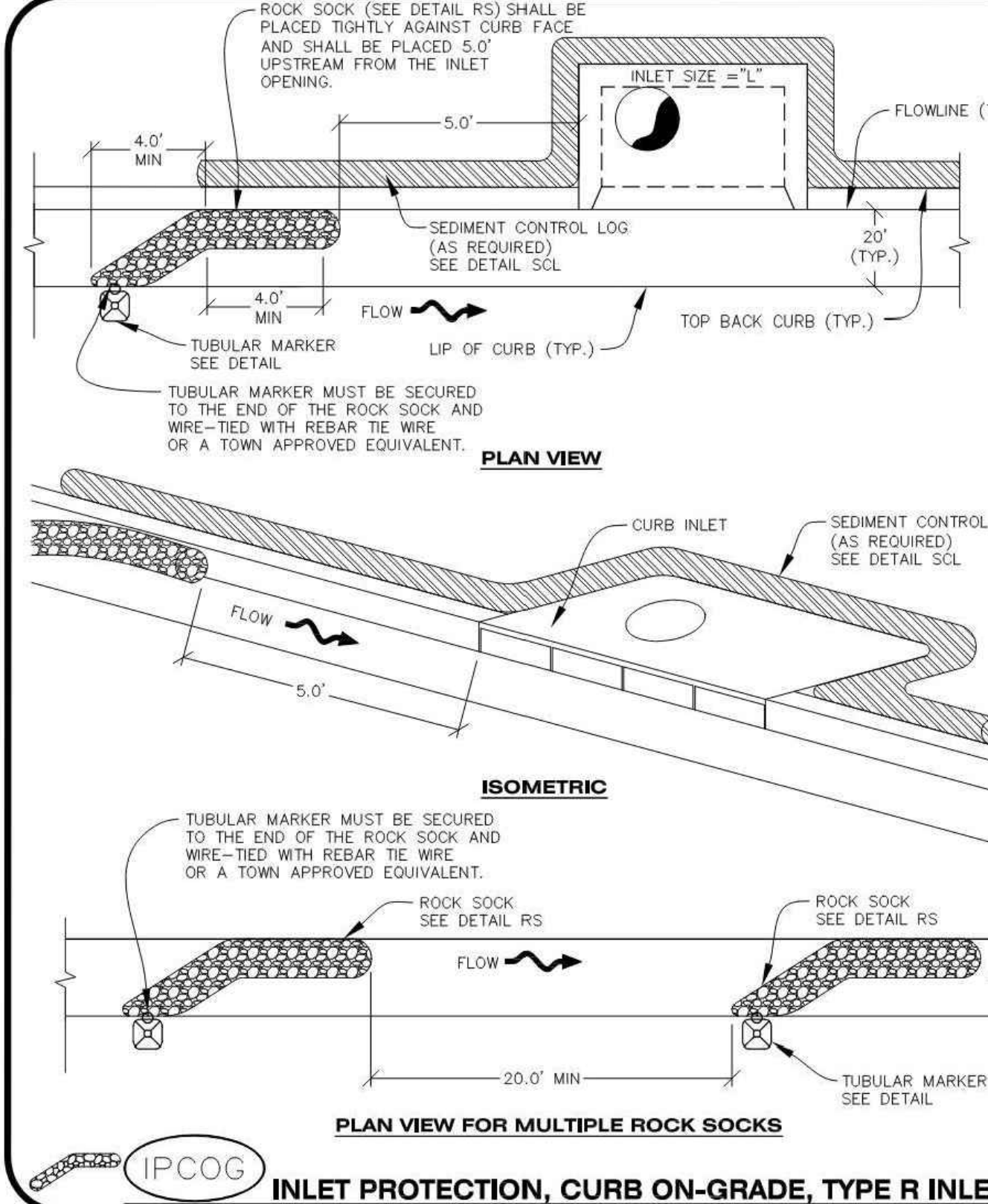
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

THIS METHOD OF INLET PROTECTION SHALL NOT BE USED ON INLETS RECEIVING SIGNIFICANT FLOWS, SUCH AS IN CERTAIN SWALES/CHANNELS OR HIGHWAY MEDIANS. INLET PROTECTION FOR AREA INLETS IN PAVEMENT (SEE DETAIL IPAP) SHALL BE USED IN THESE CONDITIONS.



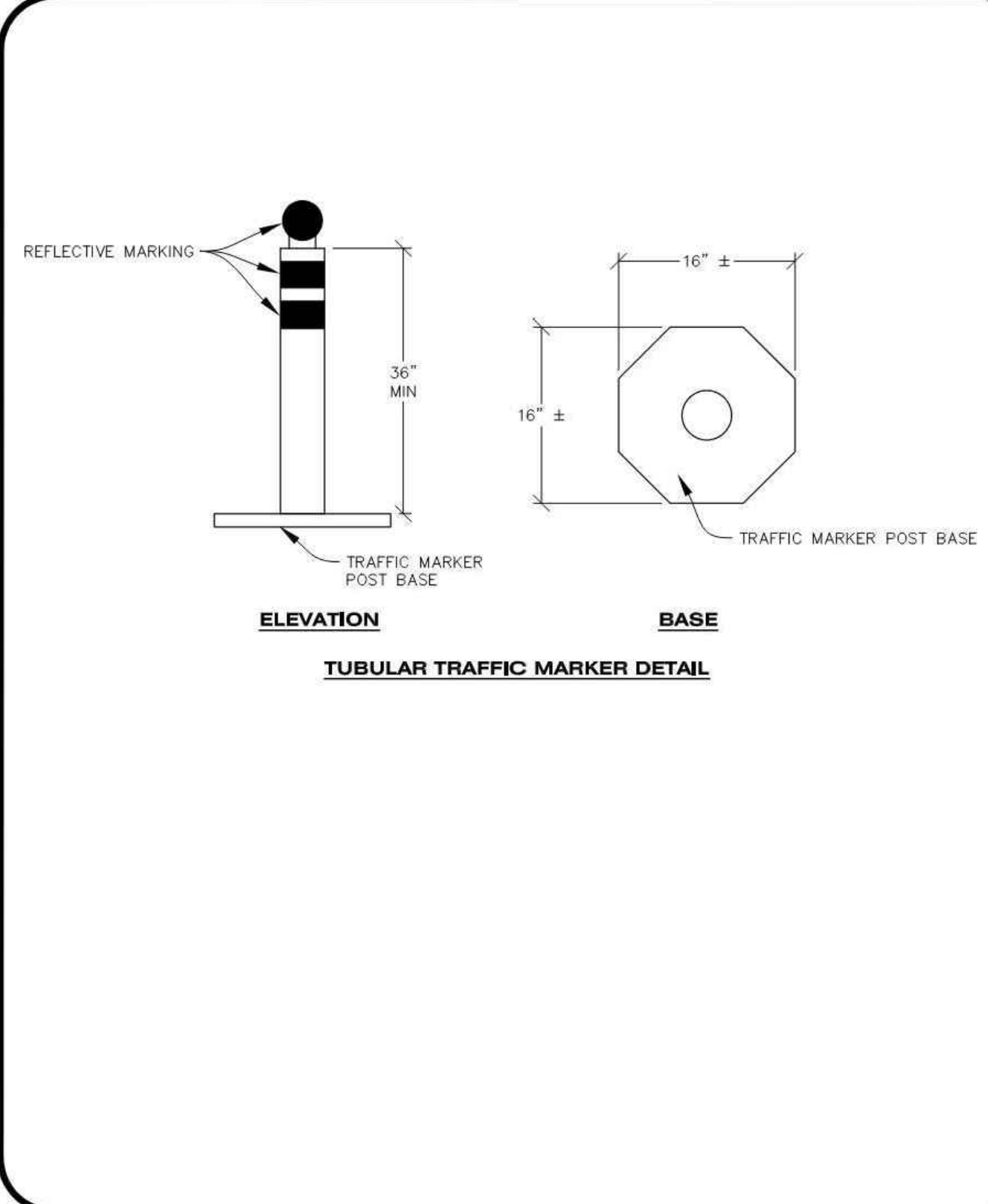
CBMP | **IPA**
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1
 Oct. 2013

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CBMP | **IPCOG**
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3
 Oct. 2013

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CBMP | **IPCOG**
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3
 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

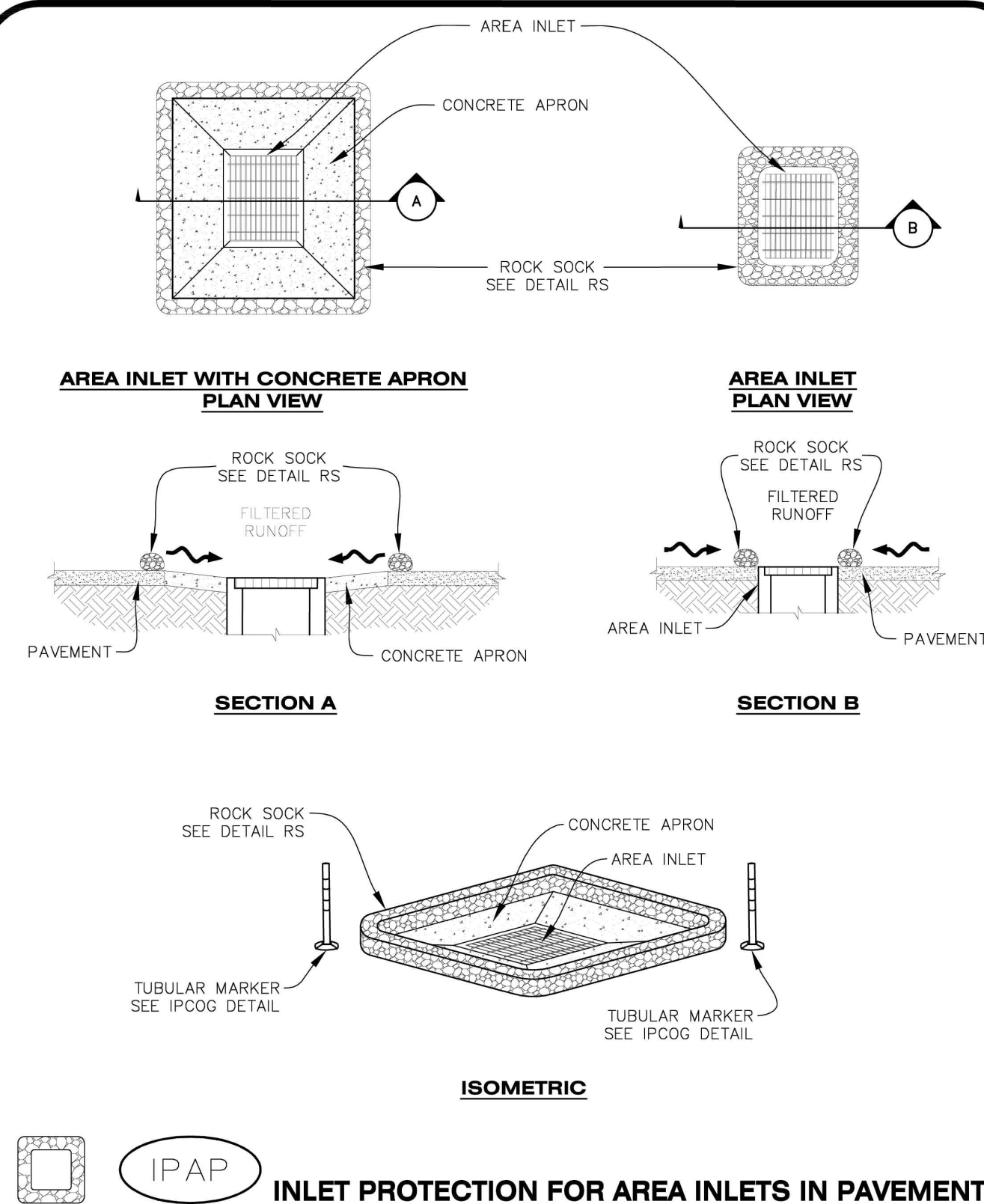
INLET PROTECTION, CURB ON-GRADE INSTALLATION NOTES

1. SEE CBMP PLAN FOR LOCATION(S) OF ON-GRADE INLET PROTECTION.
2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
3. ROCK SOCK FOR ON-GRADE INLET PROTECTION SHALL BE ONE CONTINUOUS PIECE.
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.
5. ROCK SOCK SHALL BE PLACED 5.0' UPHILL OF THE INLET OPENING.
6. TUBULAR MARKER SHALL BE A MINIMUM OF 3.0' HIGH WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
7. THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE APPLICABLE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE INLET.

ON-GRADE INLET PROTECTION INSPECTION AND MAINTENANCE NOTES

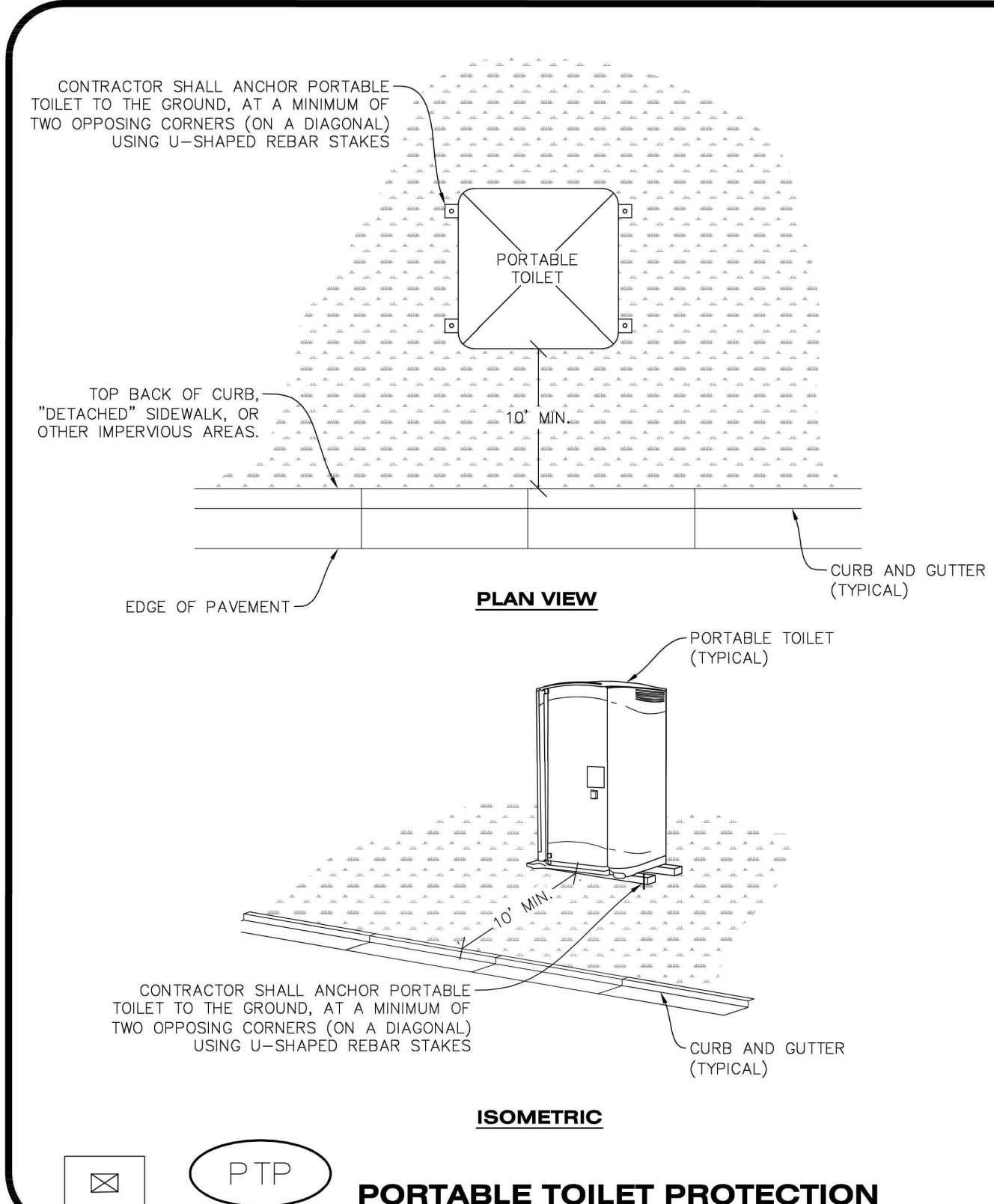
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ON-GRADE INLET PROTECTION.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
4. ON-GRADE INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.



CBMP | **IPAP**
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1
 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.



CBMP | **PTP**
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

PORTABLE TOILET PROTECTION INSTALLATION NOTES

1. PORTABLE TOILETS SHALL BE PLACED A MINIMUM OF 10.0' BEHIND ALL CURBS, SIDEWALKS, AND OTHER IMPERVIOUS AREAS.
2. ALL PORTABLE TOILETS MUST BE GROUPED TOGETHER.
3. PORTABLE TOILETS SHALL BE SECURELY ANCHORED TO THE GROUND USING U-SHAPED REBAR STAKES.
4. U-SHAPED REBAR STAKES SHALL BE POSITIONED ON AT LEAST 2 OPPOSING (DIGITAL) CORNERS.

PORTABLE TOILET PROTECTION INSPECTION AND MAINTENANCE NOTES

2. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE PORTABLE TOILET PROTECTION.
3. PORTABLE TOILETS SHALL BE SERVICED AT THE NECESSARY INTERVALS TO ELIMINATE THE POSSIBILITY OF OVERFLOW.
4. WHEN THE PORTABLE TOILETS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE TOILETS MAY NEED TO BE LANDSCAPED OR ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
5. PORTABLE TOILETS THAT ARE NOT CONSISTENTLY MAINTAINED IN ACCORDANCE WITH THESE REQUIREMENTS MAY NEED TO BE CLUSTERED TOGETHER, IN ONE CENTRALIZED LOCATION IN ORDER TO INCREASE COMPLIANCE AND REDUCE THE CHANCE OF A SPILL.

CBMP | **PTP**
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
 Oct. 2013



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11811 N. TATUM BOULEVARD,
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QUICK N CLEAN
PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

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NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS

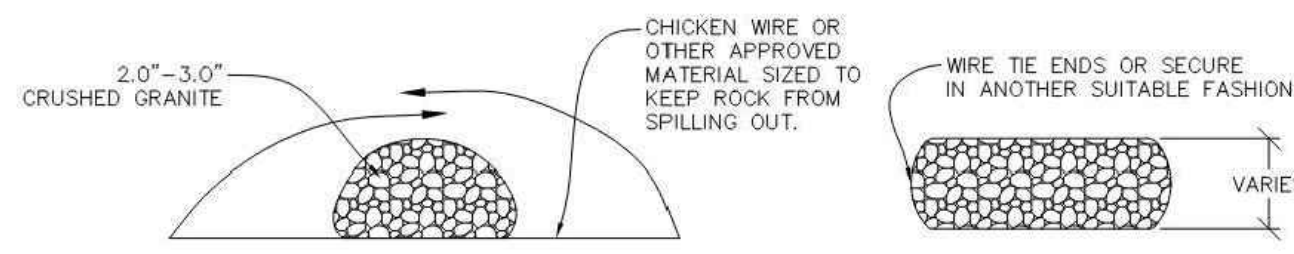


PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
SHEET TITLE
SHEET NUMBER
C3.4

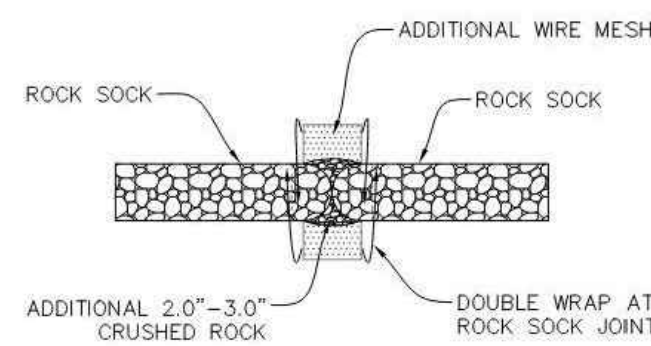
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

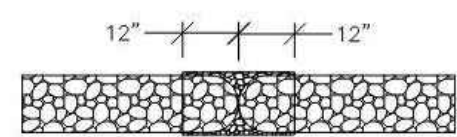


CROSS SECTION

COMPLETED ROCK SOCK



JOINT PLAN VIEW



JOINT ELEVATION

ANY GAP AT JOINT SHALL BE FILLED WITH AN ADEQUATE AMOUNT OF 2.0"-3.0" CRUSHED ROCK AND DOUBLE WRAPPED WITH ADDITIONAL CHICKEN WIRE (OR APPROVED EQUAL) SECURED TO ENDS OF ROCK REINFORCED SOCK.

RS ROCK SOCK

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES RS 1 OF 2 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

ROCK SOCK INSTALLATION NOTES

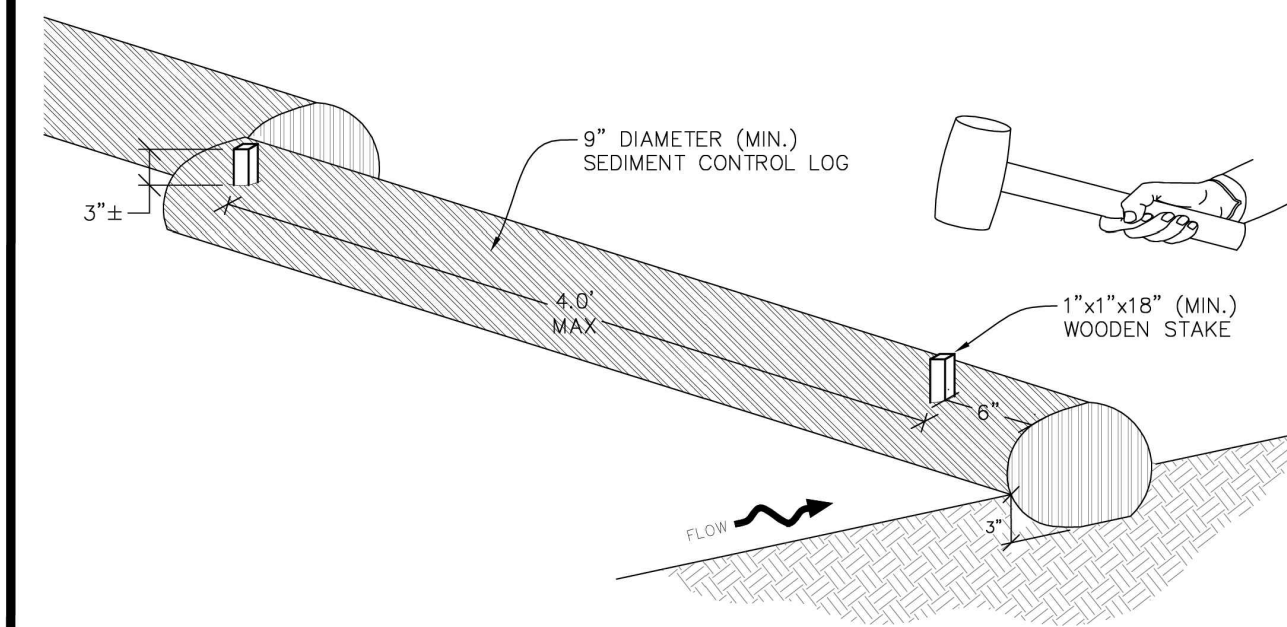
- 1. SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
2. CRUSHED ROCK SHALL BE APPROXIMATELY 2.0"-3.0" GRANITE IN SIZE WITH A FRACTURED FACE (ALL SIDES).
3. ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
5. MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
6. TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCATION SUSCEPTIBLE TO VEHICLE OR PEDESTRIAN TRAFFIC. TUBULAR MARKERS SHALL CONFORM TO THE TUBULAR MARKER DETAIL.

ROCK SOCK INSPECTION AND MAINTENANCE NOTES

- 1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
2. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
3. ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES RS 2 OF 2 Oct. 2013

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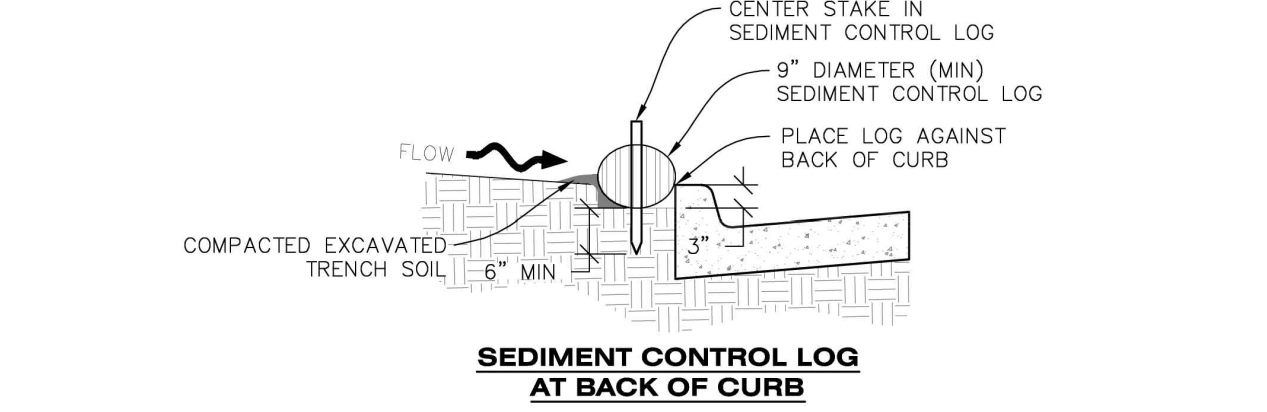


SECTION A

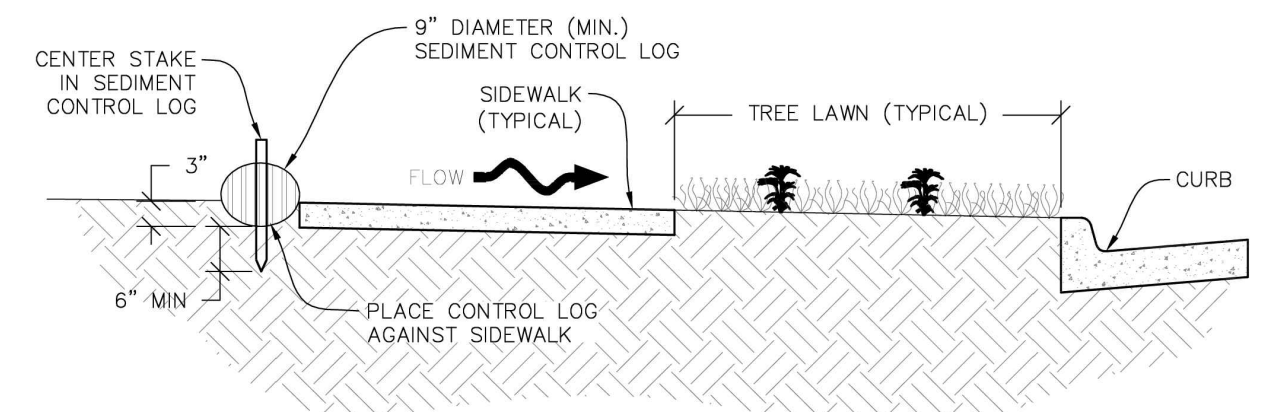
SCL SEDIMENT CONTROL LOG

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SCL 1 OF 3 Oct. 2013

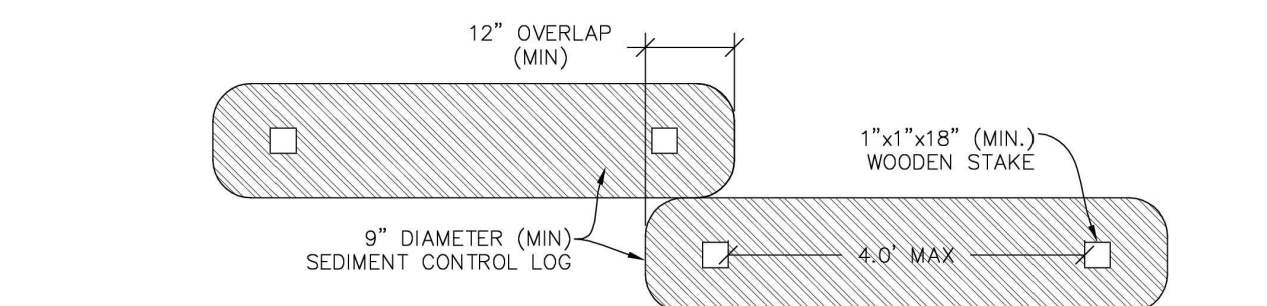
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SEDIMENT CONTROL LOG AT BACK OF CURB



SEDIMENT CONTROL LOG AT SIDEWALK WITH TREE LAWN



SEDIMENT CONTROL LOG JOINTS

SCL SEDIMENT CONTROL LOG

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SCL 2 OF 3 Oct. 2013

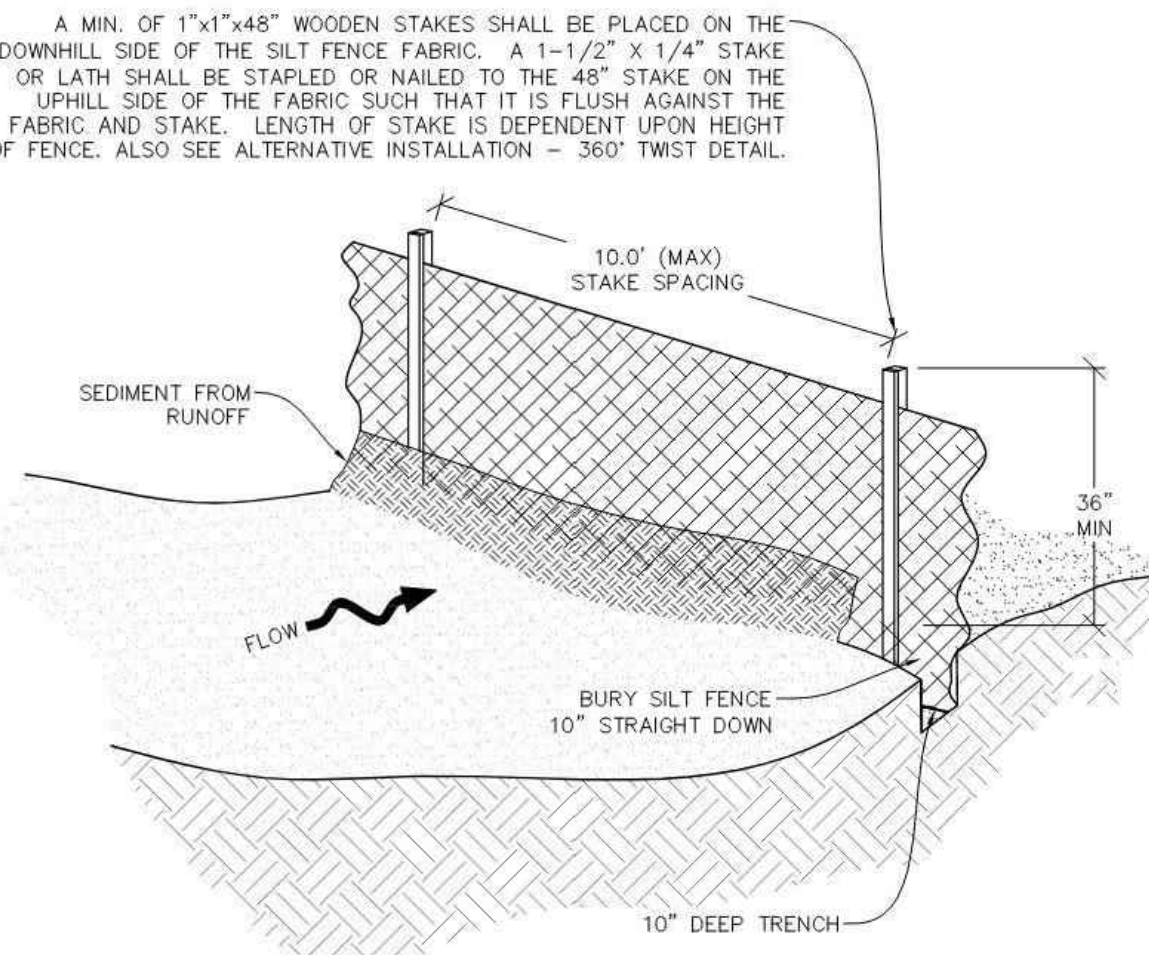
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

SEDIMENT CONTROL LOG INSTALLATION NOTES

- 1. SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT CONTROL LOGS.
2. ALL SEDIMENT CONTROL LOGS SHALL BE INSTALLED FREE OF DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
3. SEDIMENT CONTROL LOGS SHALL BE INSTALLED IMMEDIATELY ADJACENT TO AN IMPERVIOUS SURFACE SUCH AS A CURB HEAD, SIDEWALK, INLET LID, ETC. NO GAPS SHALL EXIST BETWEEN THE SEDIMENT CONTROL LOG AND THE IMPERVIOUS SURFACE.
4. A UNIFORM 3" DEEP ANCHOR TRENCH (APPROX.) IN THE SHAPE OF A HALF-SPHERE SHALL BE EXCAVATED USING A TRENCHER, SPADE-SHAPED SHOVEL, OR PICK. THE ANCHOR TRENCH SHALL BE SIZED TO ALLOW FOR THE SEDIMENT CONTROL LOG TO SEAT TIGHTLY AGAINST THE ANCHOR TRENCH.
5. EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH AND PROPERLY COMPACTED.
6. ANCHOR TRENCH SHALL BE RELATIVELY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT.
7. ALL SEDIMENT CONTROL LOGS SHALL BE PLACED 3" (APPROX.) BELOW THE GROUND AND PULLED TIGHT ON BOTH ENDS TO REMOVE ANY CURVES OR SNAGS.
8. THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS RELATIVELY FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED AGAINST THE GROUND AND SEDIMENT CONTROL LOG USING A SHOVEL, OR SIMILAR DEVICE.
9. SEDIMENT CONTROL LOG STAKES SHALL BE MADE OF WOOD AND SECURELY ANCHOR THE SCL IN PLACE.
10. STAKES SHALL BE PLACED ON 4.0' CENTERS AND EMBEDDED APPROXIMATELY 6" INTO THE GROUND. STAKES THAT ARE BROKEN PRIOR TO OR DURING INSTALLATION SHALL BE REPLACED.
11. SEDIMENT CONTROL LOGS SHALL OVERLAP A MINIMUM OF 12". THE OVERLAPPING SHALL OCCUR ON THE UP-GRADE SIDE OF THE LOGS.
12. SEDIMENT CONTROL LOGS SHALL BE STAKED WITHIN 6" FROM EACH END.
13. SEDIMENT CONTROL LOGS THAT ARE INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN THE CONCRETE AND THE LOG. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

SEDIMENT CONTROL LOG INSPECTION AND MAINTENANCE NOTES

- 1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEDIMENT CONTROL LOGS.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF EXPOSED LOG.
3. SEDIMENT CONTROL LOGS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. SEDIMENT CONTROL LOGS SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR OR DAMAGE THAT WOULD PREVENT THE SCL FROM FUNCTIONING AS DESIGNED.
5. WHEN THE SEDIMENT CONTROL LOGS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SEDIMENT CONTROL LOGS MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SM).



SF SILT FENCE

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SCL 3 OF 3 Oct. 2013

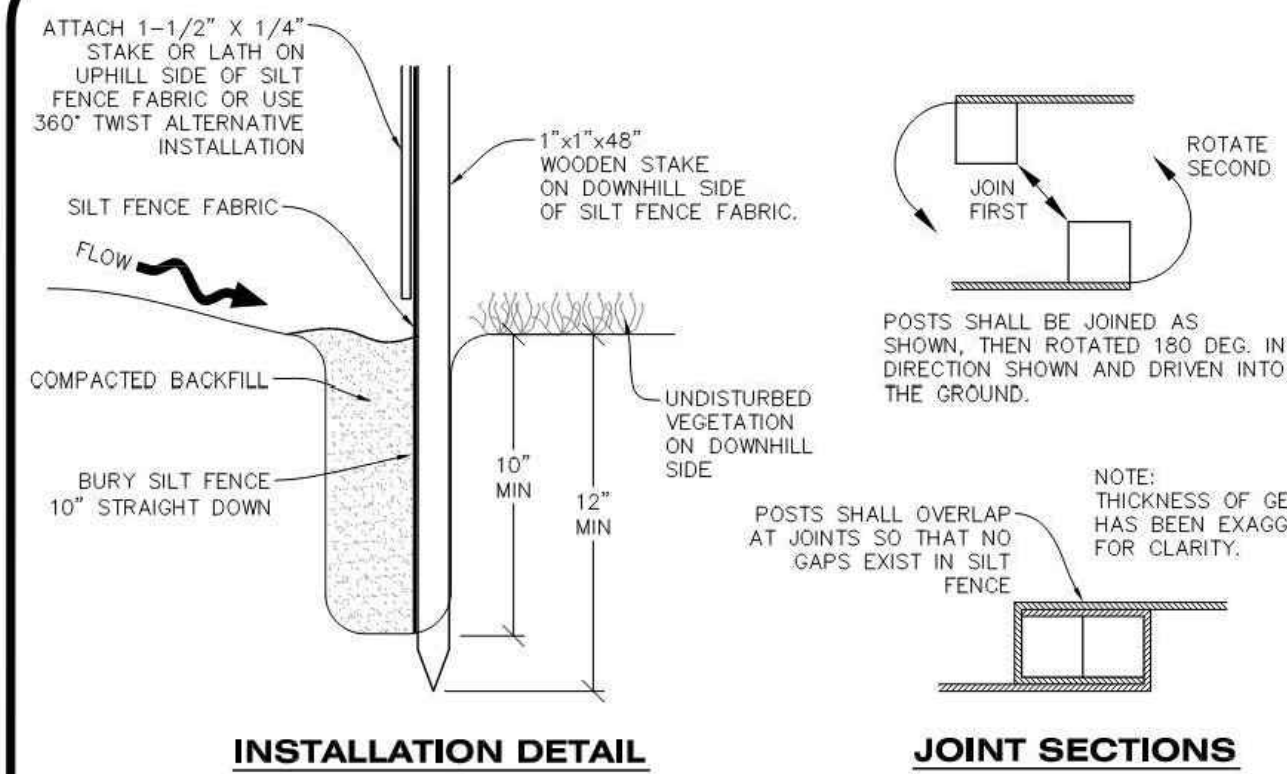
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

SEDIMENT CONTROL LOG INSTALLATION NOTES

- 1. SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT CONTROL LOGS.
2. ALL SEDIMENT CONTROL LOGS SHALL BE INSTALLED FREE OF DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
3. SEDIMENT CONTROL LOGS SHALL BE INSTALLED IMMEDIATELY ADJACENT TO AN IMPERVIOUS SURFACE SUCH AS A CURB HEAD, SIDEWALK, INLET LID, ETC. NO GAPS SHALL EXIST BETWEEN THE SEDIMENT CONTROL LOG AND THE IMPERVIOUS SURFACE.
4. A UNIFORM 3" DEEP ANCHOR TRENCH (APPROX.) IN THE SHAPE OF A HALF-SPHERE SHALL BE EXCAVATED USING A TRENCHER, SPADE-SHAPED SHOVEL, OR PICK. THE ANCHOR TRENCH SHALL BE SIZED TO ALLOW FOR THE SEDIMENT CONTROL LOG TO SEAT TIGHTLY AGAINST THE ANCHOR TRENCH.
5. EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH AND PROPERLY COMPACTED.
6. ANCHOR TRENCH SHALL BE RELATIVELY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT.
7. ALL SEDIMENT CONTROL LOGS SHALL BE PLACED 3" (APPROX.) BELOW THE GROUND AND PULLED TIGHT ON BOTH ENDS TO REMOVE ANY CURVES OR SNAGS.
8. THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS RELATIVELY FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED AGAINST THE GROUND AND SEDIMENT CONTROL LOG USING A SHOVEL, OR SIMILAR DEVICE.
9. SEDIMENT CONTROL LOG STAKES SHALL BE MADE OF WOOD AND SECURELY ANCHOR THE SCL IN PLACE.
10. STAKES SHALL BE PLACED ON 4.0' CENTERS AND EMBEDDED APPROXIMATELY 6" INTO THE GROUND. STAKES THAT ARE BROKEN PRIOR TO OR DURING INSTALLATION SHALL BE REPLACED.
11. SEDIMENT CONTROL LOGS SHALL OVERLAP A MINIMUM OF 12". THE OVERLAPPING SHALL OCCUR ON THE UP-GRADE SIDE OF THE LOGS.
12. SEDIMENT CONTROL LOGS SHALL BE STAKED WITHIN 6" FROM EACH END.
13. SEDIMENT CONTROL LOGS THAT ARE INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN THE CONCRETE AND THE LOG. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

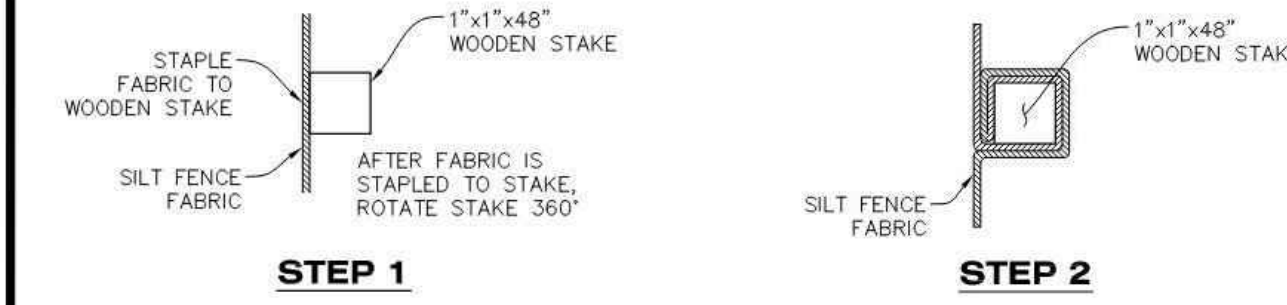
Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SF 1 OF 4 Oct. 2013

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INSTALLATION DETAIL

JOINT SECTIONS



STEP 1

STEP 2

ALTERNATIVE INSTALLATION - 360° TWIST

SF SILT FENCE

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SF 2 OF 4 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

SILT FENCE INSTALLATION NOTES

- 1. SEE CBMP PLAN FOR LOCATION(S) OF SILT FENCE.
2. ALL SILT FENCE SHALL BE INSTALLED IN GOOD CONDITION AND FREE OF ANY DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
3. A UNIFORM 10" DEEP ANCHOR TRENCH SHALL BE EXCAVATED USING A TRENCHER.
4. A 10" DEEP ANCHOR SUT SHALL BE FORMED IF USING A STATIC SLICING METHOD.
5. EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH.
6. ANCHOR TRENCH SHALL BE GENERALLY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT OF THE SILT FENCE.
7. THE ANCHOR TRENCH SHALL BE THOROUGHLY BACKFILLED WITH SOIL THAT IS GENERALLY FREE OF ROCKS AND DEBRIS.
8. ALL EXCAVATED MATERIAL SHALL BE PLACED ON THE UP-GRADE SIDE OF THE SILT FENCE.
9. STAKES SHALL BE POSITIONED ON THE DOWNHILL SIDE OF THE SILT FENCE FABRIC AND PLACED ON 10.0' CENTERS OR LESS. STAKES SHALL BE EMBEDDED A MINIMUM OF 12" INTO THE GROUND. A WOODEN LATH SHALL BE ATTACHED TO THE OPPOSING (UPHILL) SIDE OF THE STAKE FOR ADDED STRENGTH AND SUPPORT. THE LATH SHALL HAVE THE FOLLOWING DIMENSIONS: 1"x4"x24".
10. SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD NOT BE SIGNIFICANT SAGGING ALONG ANY PORTION OF THE SILT FENCE AFTER IT HAS BEEN ANCHORED TO THE STAKES.
11. SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES AND LATHS USING STAPLES OR NAILS OF AN APPROXIMATE LENGTH ENOUGH STAPLES AND NAILS SHOULD BE PLACED ALONG THE LATH TO ENSURE PROPER ATTACHMENT.
12. SILT FENCE FABRIC SHALL MEET THE FOLLOWING MANDATORY REQUIREMENTS:
PROPERTIES TEST METHOD MANDATORY REQUIREMENTS
GRAB TENSILE STRENGTH ASTM D 4632 > 124 LBS
MULLEN BURST STRENGTH ASTM D 3786 > 300 PSI
PUNCTURE STRENGTH ASTM D 4833 > 60 LBS
TRAPEZOID TEAR STRENGTH ASTM D 4533 > 65 LBS
UV RESISTANCE ASTM D 4355 > 80% AT 500 HOURS OF UV EXPOSURE
FLOW RATE ASTM D 4491 > 10 GAL/MIN/FT2
13. AN ORIGINAL PRODUCT SPECIFICATION SHEET FROM THE SILT FENCE MANUFACTURER SHALL BE MADE AVAILABLE AT THE REQUEST OF THE TOWN'S INSPECTOR. THE PRODUCT SPECIFICATION SHEET SHALL PROVIDE THE RESULTS FOR THE TEST METHODS ABOVE.
14. SILT FENCE JOINTS SHALL BE CONNECTED ACCORDING TO THE ATTACHED DRAWING.
15. SILT FENCE THAT IS INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN CONCRETE AND THE SILT FENCE. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

Logo for Parker Colorado CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SF 3 OF 4 Oct. 2013



CEI ENGINEERING ASSOCIATES, INC. 710 W. PINDALE AVE. FRESNO, CA 93711. PHONE: (559) 447-3119. FAX: (559) 447-3129.



CLIENT: 3K1 CONSULTING SERVICES, LLC. 11811 N. TATUM BOULEVARD, PHOENIX, ARIZONA 85028. PHONE: (602) 850-8101.



PLANS PREPARED FOR QUICK N CLEAN 7291 E. ADOBE DRIVE, SUITE 115 SCOTTSDALE, AZ 85255. PHONE: (480) 707-3531

Table with columns: NO., DESCRIPTION, DATE, REVISION.

FOR REVIEW ONLY NOT FOR CONSTRUCTION

QUICK N CLEAN 9572 TWENTY MILE ROAD PARKER, COLORADO CONSTRUCTION DOCUMENTS



Table with columns: PROFESSIONAL OF RECORD, PROJECT MANAGER, DESIGNER, CEI PROJECT NUMBER, DATE, REVISION.

CBMP DETAILS SHEET TITLE SHEET NUMBER

C3.5

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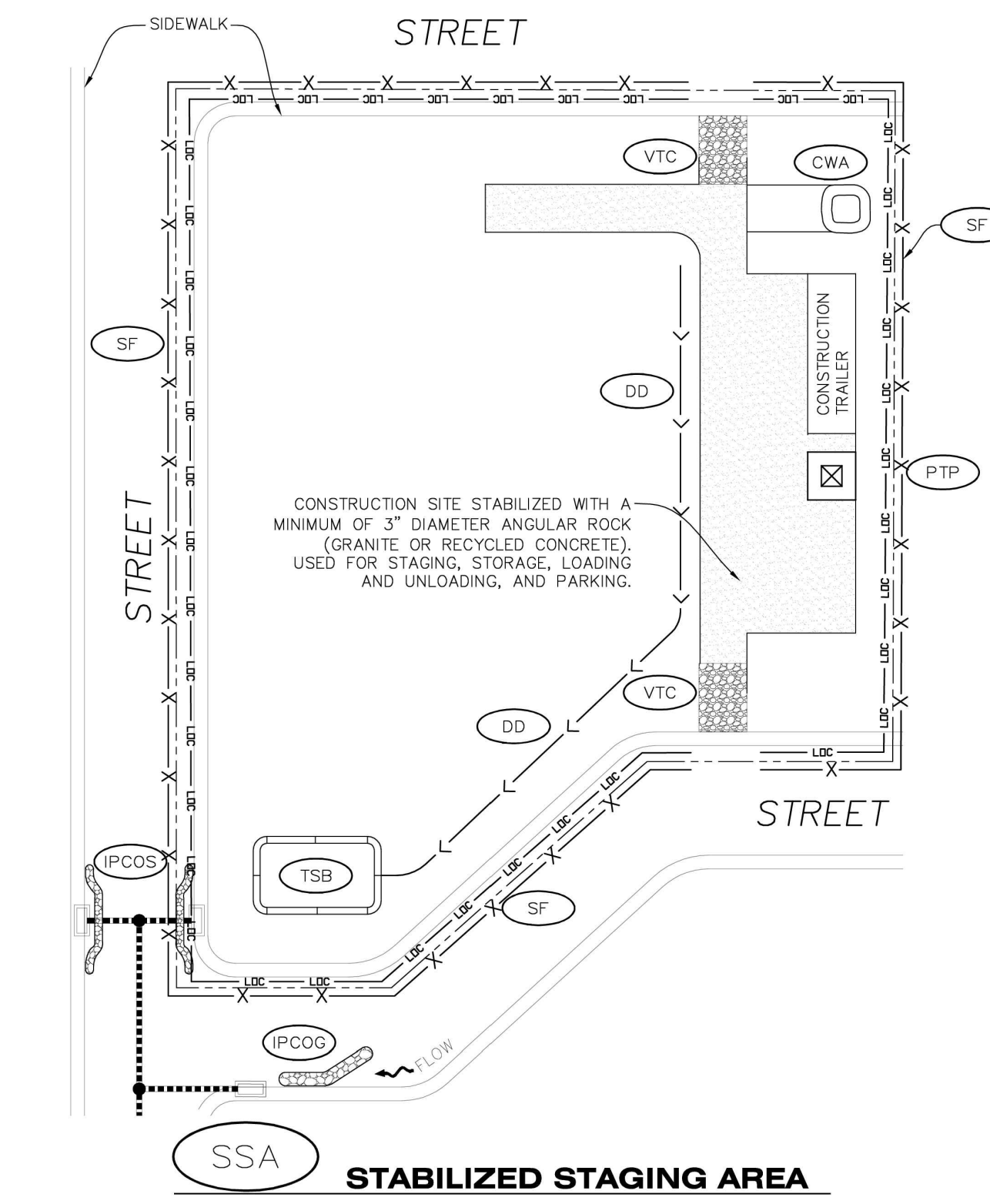
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

SILT FENCE INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SILT FENCE.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED REGULARLY.
3. SILT FENCE SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. SILT FENCE SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR AND/OR DAMAGE.
5. WHEN THE SILT FENCE IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SILT FENCE MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

Town of Parker COLORADO | **CBMP** | **SF**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 4 OF 4
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.



Town of Parker COLORADO | **CBMP** | **SSA**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

STABILIZED STAGING AREA INSTALLATION NOTES

1. SEE CBMP PLAN FOR LOCATION OF STAGING AREA. CONTRACTOR MAY MODIFY LOCATION AND SIZE OF STABILIZED STAGING AREA WITH TOWN APPROVAL.
2. STABILIZED STAGING AREA SHALL BE LARGE ENOUGH TO FULLY CONTAIN PARKING, STORAGE, AND LOADING OPERATIONS.
3. THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM OF 3" DIAMETER OF ANGULAR ROCK (GRANITE OR RECYCLED CONCRETE).
4. SSA FOR SMALLER SITES MAY NOT BE PRACTICAL. IN THESE AND SIMILAR SITUATIONS, VARIANCES MAY BE PERMITTED BY THE TOWN.

STABILIZED STAGING AREA INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STAGING AREA.
2. STABILIZED STAGING AREA SHALL BE ENLARGED AS NECESSARY TO CONTAIN PARKING, STORAGE, LOADING, AND UNLOADING.

Town of Parker COLORADO | **CBMP** | **SSA**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

SEEDING AND MULCHING SHALL BE PERFORMED ACCORDING TO THE ACCOMPANYING DETAIL(S) AND TEXT. NO EXCEPTIONS SHALL BE MADE

1. SEE PLAN VIEW FOR:
 - LOCATION(S) OF SEEDING AND MULCHING
 - TYPE OF SEED MIX
2. SEED MIXES MAY CONFORM TO THE TABLE PROVIDED WITH THE SMC NOTES OR ALTERNATIVES MAY BE ALLOWED WITH PRIOR PERMISSION BY THE TOWN'S INSPECTOR.
3. SEEDING MAY BE PERFORMED YEAR ROUND ASSUMING THE SOIL IS NOT FROZEN. SEEDING DURING TIMES OF EXTREME TEMPERATURES SHOULD BE AVOIDED IF POSSIBLE.
4. AT THE BEGINNING OF THE LAND DISTURBANCE ACTIVITIES, IT IS HIGHLY RECOMMENDED THAT AN APPROPRIATE AMOUNT OF NATIVE TOPSOIL BE STRIPPED FROM THE SITE AND STOCKPILED. ALL AREAS, PRIOR TO PERMANENT SEEDING AND MULCHING, WILL LIKELY NEED TO BE COVERED WITH AN APPROPRIATE LAYER OF TOPSOIL. THIS REQUIREMENT APPLIES TO ALL AREAS WHERE NATIVE SEEDING IS SPECIFIED ON THE CBMP PLAN AND/OR LANDSCAPING PLANS.
5. IT IS STRONGLY RECOMMENDED THAT SAMPLES FROM THE STRIPPED TOPSOIL BE PROPERLY COLLECTED AND TESTED BY A QUALIFIED LABORATORY TO ENSURE ADEQUATE NUTRIENT CONTENT PRIOR TO SEEDING AND MULCHING. IF IT IS DISCOVERED THAT THE TOPSOIL IS VOID OF THE NUTRIENTS NECESSARY TO SUCCESSFULLY ESTABLISH THE REQUIRED VEGETATION, THEN THE APPROPRIATE AMENDMENTS SHALL BE ADDED.
6. ALL AREAS TO BE SEEDED AND MULCHED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING DETAILS AND NOTES. SURFACE ROUGHENING SHALL OCCUR AFTER PLACEMENT OF THE TOPSOIL.
7. WHEN INSTALLED WITH A DRILL SEEDER, SEED SHALL BE PLACED AT A DEPTH OF 1/4 - 1/2 INCH. ROW SPACING SHALL BE NO MORE THAN 6-INCHES.
8. ALL AREAS INCAPABLE OF BEING DRILL SEEDED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING NOTES OR EFFECTIVELY ROUGHENED USING A HARROW OR OTHER SUCH IMPLEMENT. ALL AREAS SHALL BE UNIFORMLY HAND BROADCASTED WITH THE PROPER SEED MIX APPLIED AT TWO TIMES THE DRILL SEEDED RATE. BROADCASTED AREAS SHALL THEN BE RE-HARROWED OR RE-RAKED USING A HARD-TIPPED RAKE TO ENSURE THAT SEEDS ARE BURIED TO AN APPROXIMATE DEPTH OF 1/4 - 1/2 INCH.
9. AFTER SEEDING HAS BEEN COMPLETED, MULCH SHALL BE UNIFORMLY APPLIED AT A RATE OF 2 TONS/ACRE (4,000 LBS/ACRE). MULCH SHALL BE MECHANICALLY CRIMPED TO A DEPTH OF 2 INCHES USING A CRIMPER. MULCH SHALL BE HAND CRIMPED AND COVERED WITH A TACKIFIER IN AREAS WHERE MECHANICAL CRIMPING IS NOT POSSIBLE. WHEN SOILS PERMIT, ALL MULCH SHALL BE CRIMPED SUCH THAT THE INDIVIDUAL PIECES OF STRAW OR HAY FORM EXAGGERATED V-SHAPES PROTRUDING OUT OF THE GROUND SEVERAL INCHES.
10. IN CERTAIN INSTANCES, IT MAY BE NECESSARY TO APPLY A TACKIFIER IN ORDER TO HELP WITH STRAW DISPLACEMENT. TACKIFIER SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.

Town of Parker COLORADO | **CBMP** | **SMC**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

SEEDING AND MULCHING MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEEDING AND MULCHING.
2. ANY SEEDED AND MULCHED AREAS THAT BECOME DAMAGED SHALL BE REPAIRED WITHIN THE TIME FRAME SPECIFIED BY THE TOWN'S INSPECTOR.

WEED MANAGEMENT

1. ALL HERBICIDES SHALL BE APPLIED BY COMMERCIAL PESTICIDE APPLICATORS LICENSED BY THE COLORADO DEPARTMENT OF AGRICULTURE AS QUALIFIED APPLICATORS. THE CONTRACTOR SHALL FURNISH DOCUMENTATION OF SUCH LICENSING PRIOR TO HERBICIDE APPLICATION.
2. HERBICIDE APPLICATION METHOD SHALL BE SUCH THAT PLANT GROWTH OUTSIDE THE DESIGNATED TREATMENT AREAS WILL NOT BE DAMAGED. ALL DAMAGE CAUSED BY IMPROPER HERBICIDE APPLICATION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
3. HERBICIDES SHALL BE APPLIED DURING THE APPROPRIATE SEASONS, WHEN TARGET PLANTS ARE ACTIVELY GROWING.
4. AFTER THE GRASS SEED IS ESTABLISHED, APPROPRIATE HERBICIDES SHALL BE APPLIED TO CONTROL THE REMAINING WEEDS TO ENSURE A TIMELY RETURN OF THE FINANCIAL SECURITY. PROPER TIMING OF HERBICIDE APPLICATIONS ARE NECESSARY TO ACHIEVE THE SUPPRESSION OF WEED SEED PRODUCTION AND DEPLETION OF WEED ROOT MASS. ULTIMATELY, THE HERBICIDES USED SHALL BE BASED UPON THE TARGET WEEDS.
5. HERBICIDE TREATMENTS SHALL CONTINUE AT AN APPROPRIATE RATE UNTIL IT IS EVIDENT THAT WEED GROWTH PRESENCE AND GROWTH IS MINIMAL AND MAY BE CONTROLLED THROUGH MOWING AND/OR ANNUAL HERBICIDE TREATMENT.

Town of Parker COLORADO | **CBMP** | **SMC**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

TOWN OF PARKER, SEED MIX 1

- 20% CANADA WILD RYE
- 15% CRESTED WHEATGRASS
- 15% SLENDER WHEATGRASS
- 10% ANNUAL RYEGRASS
- 10% SHEEP FESCUE
- 10% BIG BLUESTEM
- 10% SIDEOATS GRAMA
- 5% CANADA BLUEGRASS
- 5% BLUE GRAMA

SEEDING RATE:

- DRILLED: 25 LBS/ACRE
- BROADCAST: 50 LBS/ACRE

TOWN OF PARKER, SEED MIX 2

- 22% SLENDER WHEATGRASS
- 18% SODAR STREAMBANK WHEATGRASS
- 13% ARIZONA FESCUE
- 13% BLUE GRAMA
- 12% BUFFALOGRASS
- 12% BARLEY OR OATS
- 5% SPIKE MUHLY
- 5% INDIAN RICEGRASS

SEEDING RATE:

- DRILLED: 25 LBS/ACRE
- BROADCAST: 50 LBS/ACRE

TOWN OF PARKER, SEED MIX 3 (LOW-GROWTH MIX)

- 25% EPHRAIM CRESTED WHEATGRASS
- 23% SHEEP FESCUE
- 18% PERENNIAL RYEGRASS
- 13% CANADA BLUEGRASS
- 12% BARLEY OR OATS
- 9% BLUE FESCUE

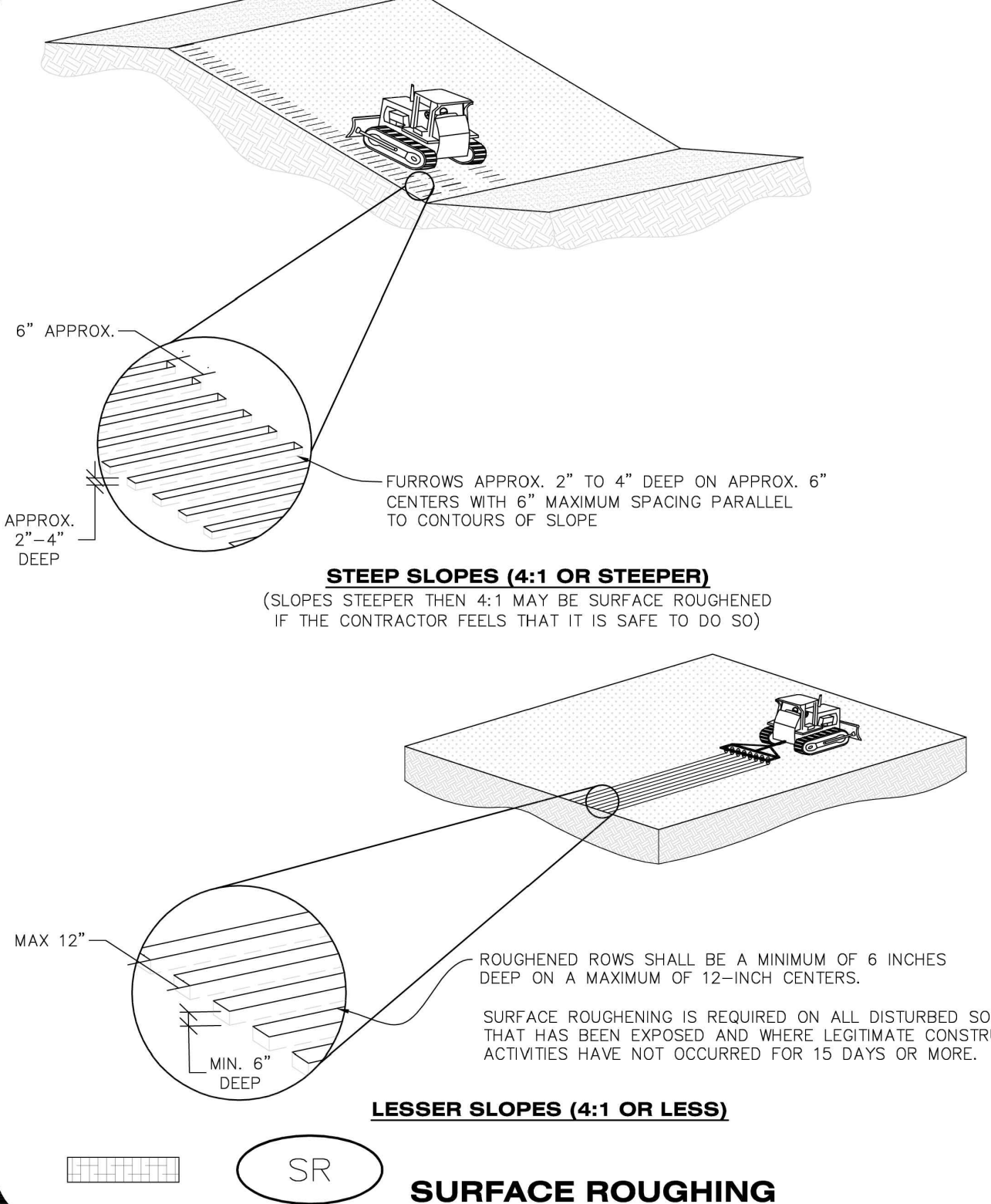
SEEDING RATE:

- DRILLED: 25 LBS/ACRE
- BROADCAST: 50 LBS/ACRE

SEED MIX 4:
OTHER SEED MIXES APPROVED BY THE TOWN OF PARKER

Town of Parker COLORADO | **CBMP** | **SMC**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.



Town of Parker COLORADO | **CBMP** | **SR**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

SURFACE ROUGHENING INSTALLATION NOTES

1. SEE CBMP PLAN FOR LOCATION(S) OF SURFACE ROUGHENING.
2. DISTURBED AREAS THAT REMAIN INACTIVE FOR 15 DAYS OR MORE MUST RECEIVE SURFACE ROUGHENING OR ANOTHER APPROVED BMP FROM THE SDCM. DETERMINATION OF JOB SITE INACTIVITY IS AT THE DISCRETION OF THE TOWN'S INSPECTOR.
3. FOR STEEP SLOPES (3:1 OR STEEPER), IT IS ACCEPTABLE TO "TRACK" THE SLOPES, ACCORDING TO THE CBMP DETAILS.
4. SCHEDULES FOR REQUIRING STABILIZATION MAY BE MODIFIED BY THE PERMITEE TO ALLOW FOR SPECIAL CONSIDERATIONS SUCH AS STABILIZING ACCESS AREAS AND AREAS IN CLOSE PROXIMITY TO CONTINUING CONSTRUCTION.

SURFACE ROUGHENING INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL PROACTIVELY INSPECT THE SURFACE ROUGHENING.

Town of Parker COLORADO | **CBMP** | **SR**
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Oct. 2013



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REVISION		
NO.	DESCRIPTION	DATE

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CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS

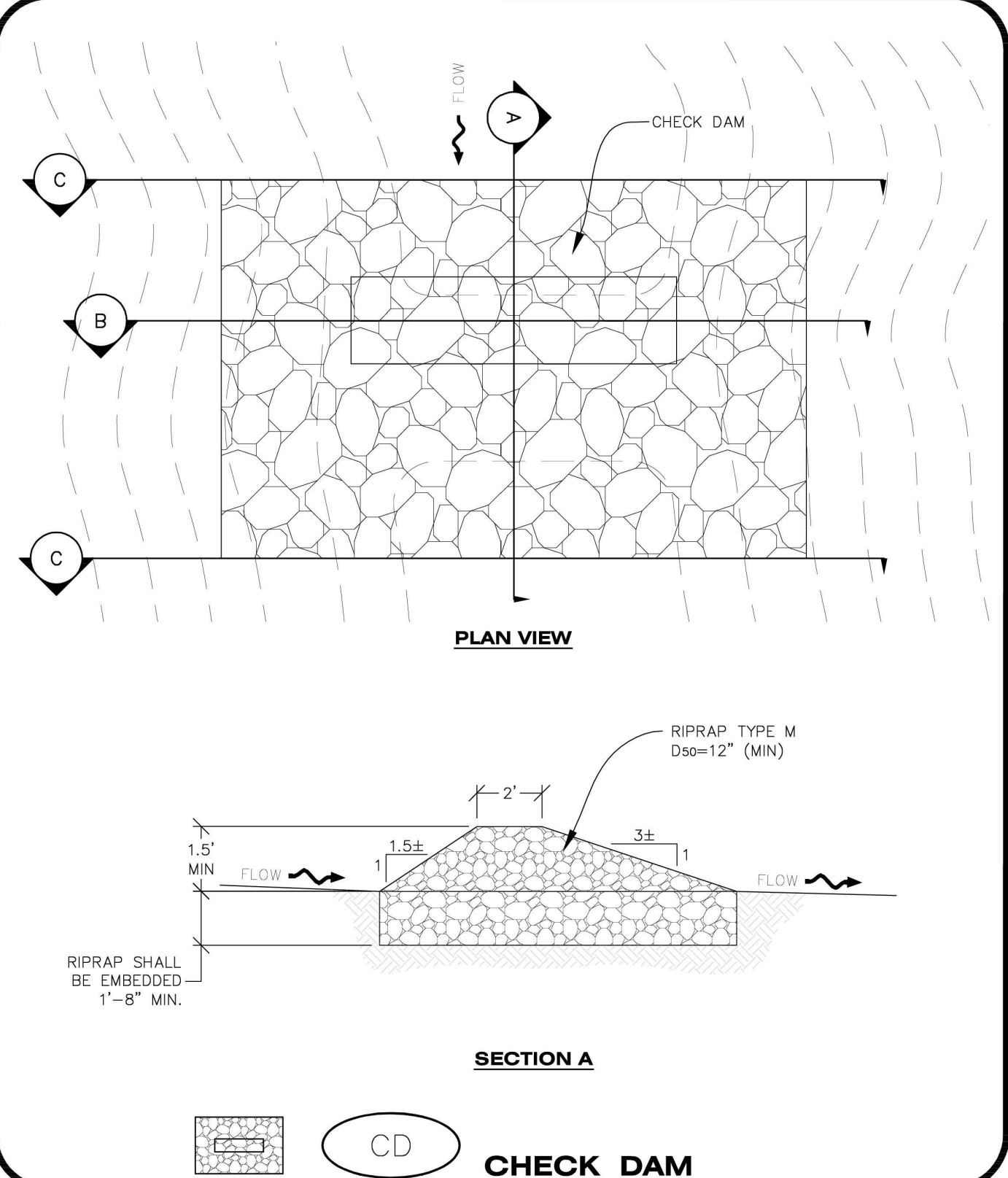


PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
SHEET TITLE
SHEET NUMBER

C3.6

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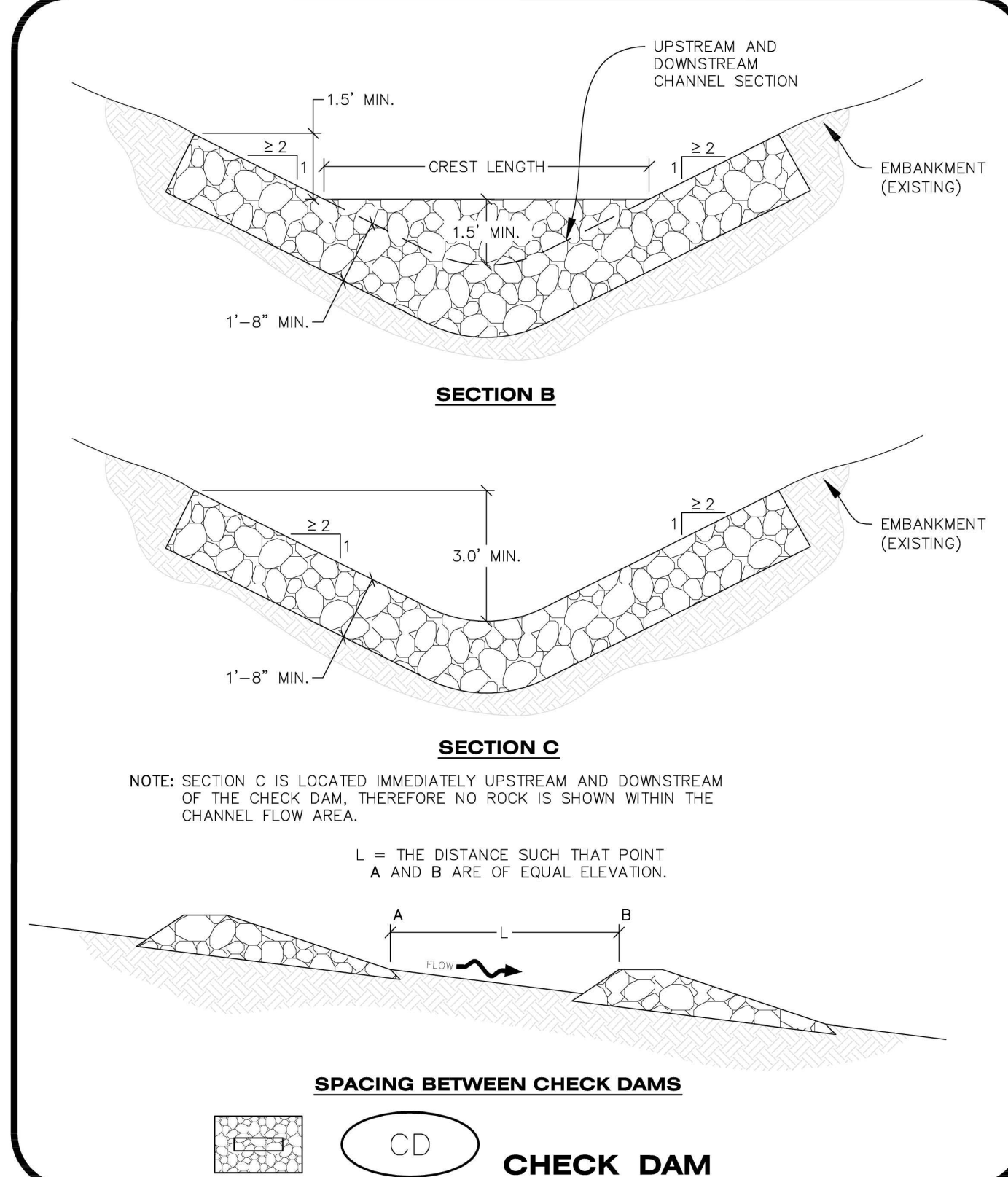
CD CHECK DAM

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

1 OF 3 Oct. 2013

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CD CHECK DAM

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

2 OF 3 Oct. 2013

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CHECK DAM INSTALLATION NOTES

- SEE PLAN VIEW FOR LOCATION(S) OF CHECK DAMS.
- CHECK DAMS SHOWN ON CBMP PLAN SHALL BE INSTALLED WHEN DIRECTED BY THE TOWN'S INSPECTOR.
- RIPRAP UTILIZED FOR CHECK DAMS SHALL HAVE A D50 MEDIAN STONE SIZE OF 12".
- RIPRAP PAD SHALL BE TRENCHED INTO THE GROUND A MINIMUM OF 1'-8".
- THE MAXIMUM SPACING BETWEEN CHECK DAMS SHOULD BE SUCH THAT THE BOTTOM OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM AS SHOWN IN THE DETAIL.

CHECK DAM INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CHECK DAMS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE CREST OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- CHECK DAMS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN CHECK DAMS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED TOPSOIL AND ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CHECK DAMS TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- IN SOME INSTANCES, CHECK DAMS MAY REMAIN IN PLACE PERMANENTLY.

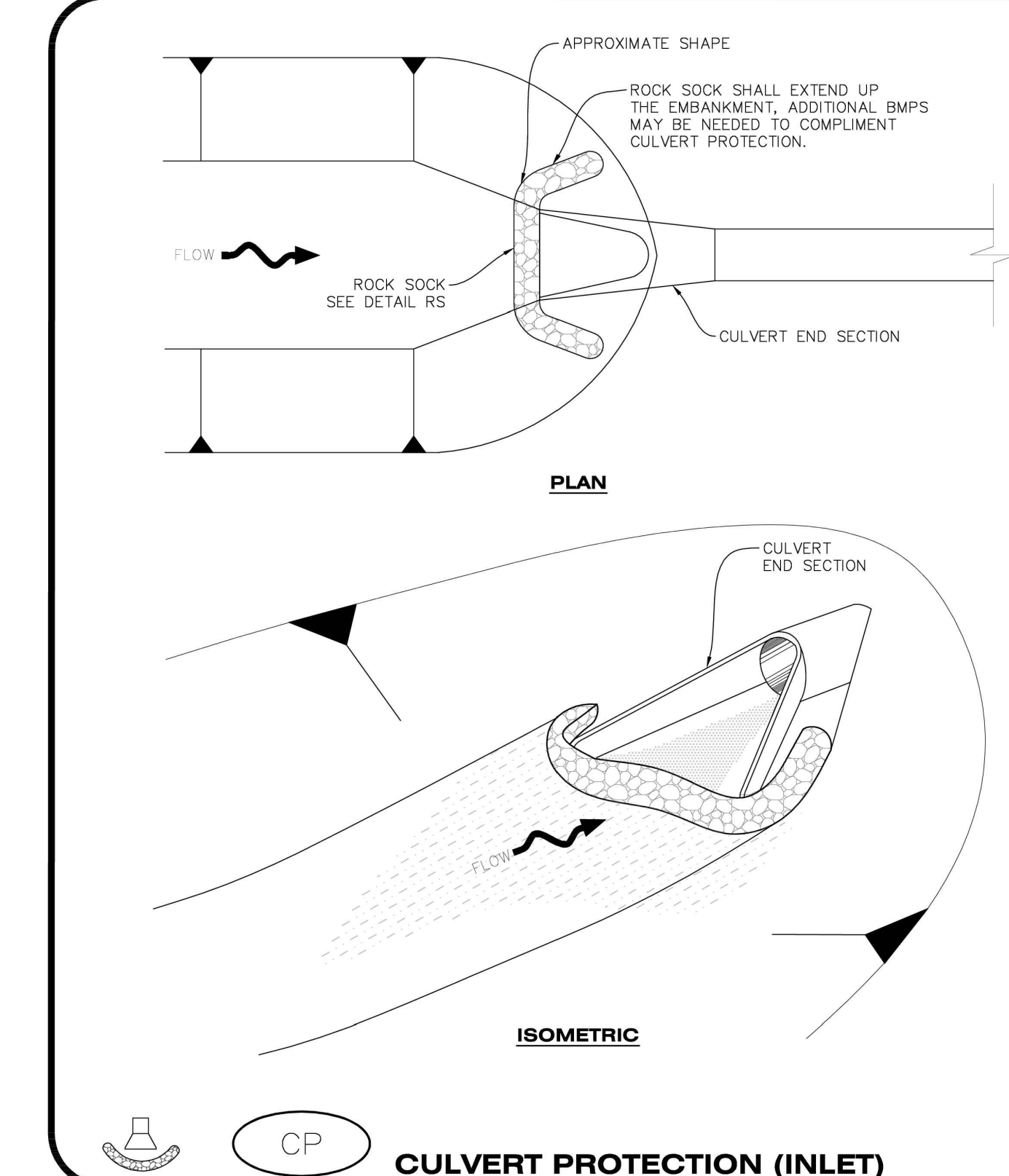
CD CHECK DAM

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

3 OF 3 Oct. 2013

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CP CULVERT PROTECTION (INLET)

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

1 OF 2 Oct. 2013

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CULVERT PROTECTION (INLET) INSTALLATION NOTES

- SEE PLAN VIEW FOR LOCATION(S) OF CULVERT PROTECTION (INLET).
- ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
- ROCK SOCK SHALL BE APPROXIMATELY 12" IN DIAMETER.
- ROCK SOCK SHALL EXTEND ABOVE THE FLOW LINE ELEVATION ON BOTH SIDES OF THE CULVERT END SECTION.

CULVERT PROTECTION (INLET) INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CULVERT PROTECTION (INLET).
- AT A MINIMUM, ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE ROCK SOCK.
- CULVERT PROTECTION (INLET) IS TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE CULVERT INLET PROTECTION IS REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CULVERT INLET PROTECTION TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

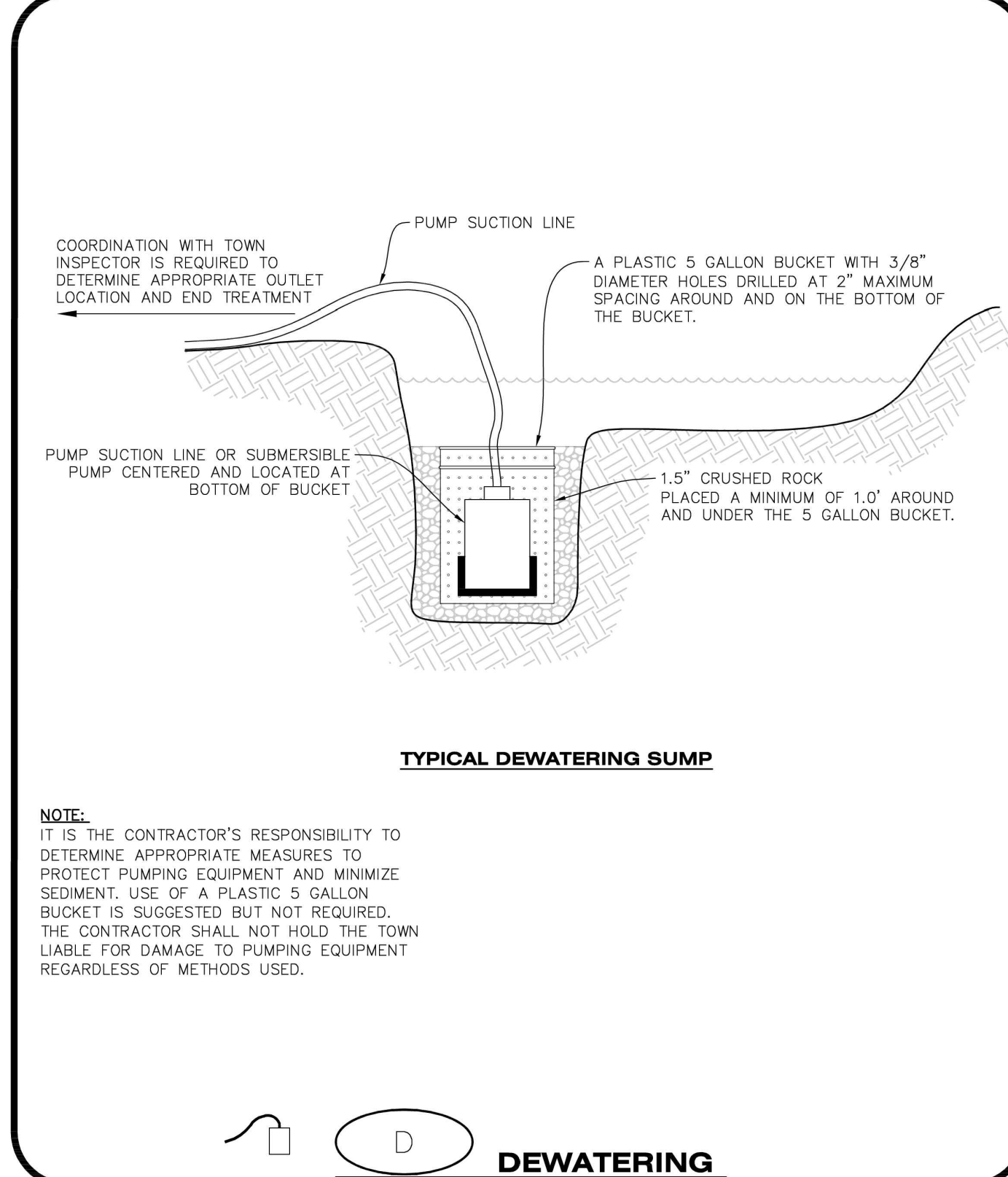
CP CULVERT PROTECTION (INLET)

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

2 OF 2 Oct. 2013

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D DEWATERING

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

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DEWATERING INSTALLATION NOTES

- IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

DEWATERING MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

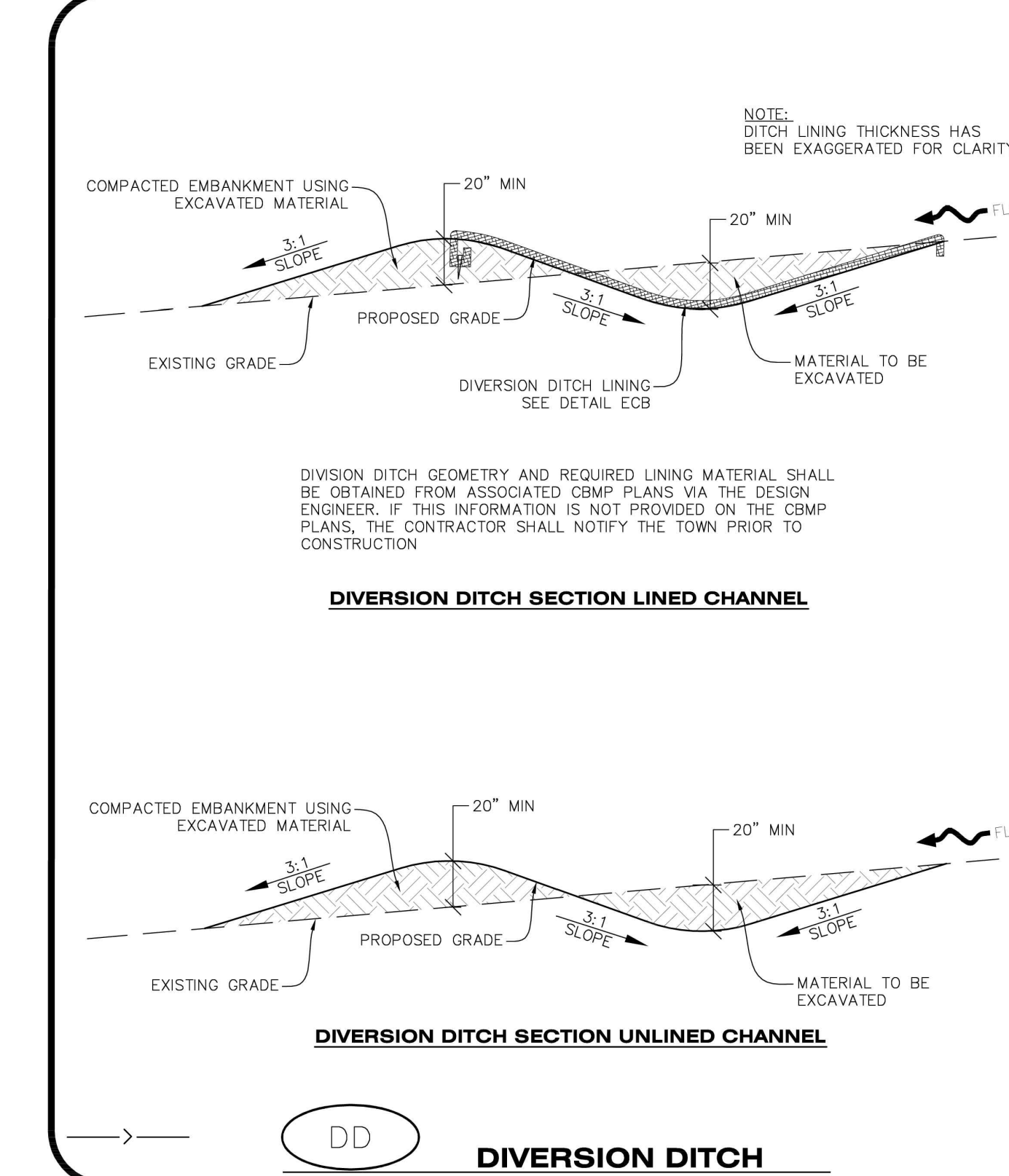
D DEWATERING

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

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DD DIVERSION DITCH

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES

Town of Parker COLORADO

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REVISION		
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9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
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CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
SHEET TITLE
SHEET NUMBER
C3.7

DRAWING LOCATION - P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-CBMP.DWG - SAVED BY - MCALESDON

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DIVERSION DITCH INSTALLATION NOTES

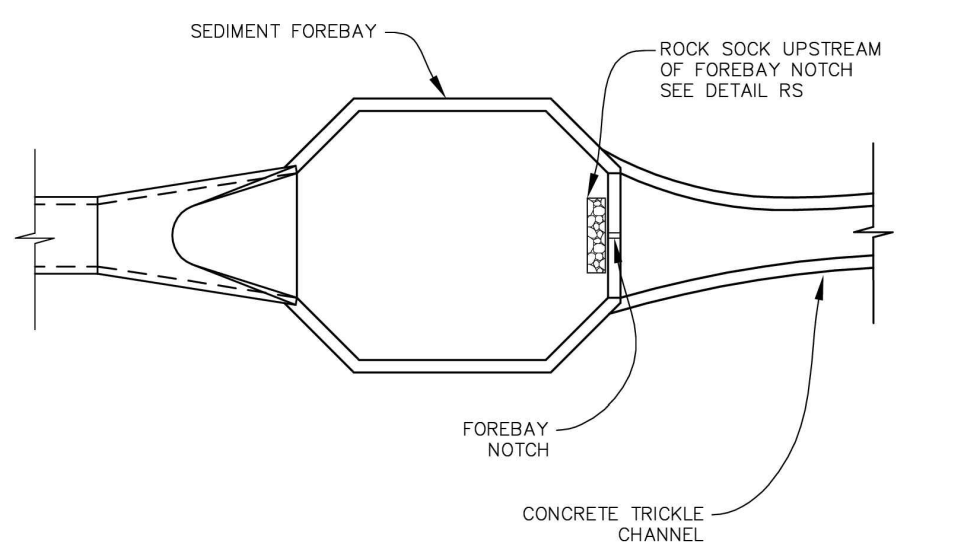
- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
- A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
- ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
- THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
- THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES

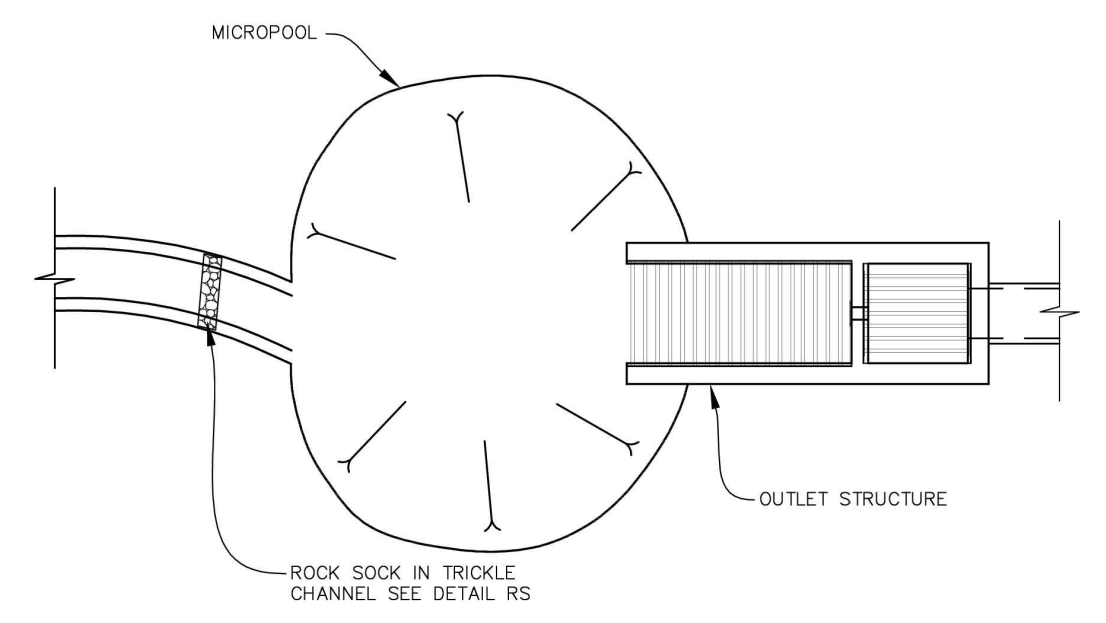
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
- DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
- DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
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FOREBAY



OUTLET / MICROPOOL

DETENTION POND PROTECTION
DP
1 OF 2
Oct. 2013

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DETENTION POND PROTECTION INSTALLATION NOTES

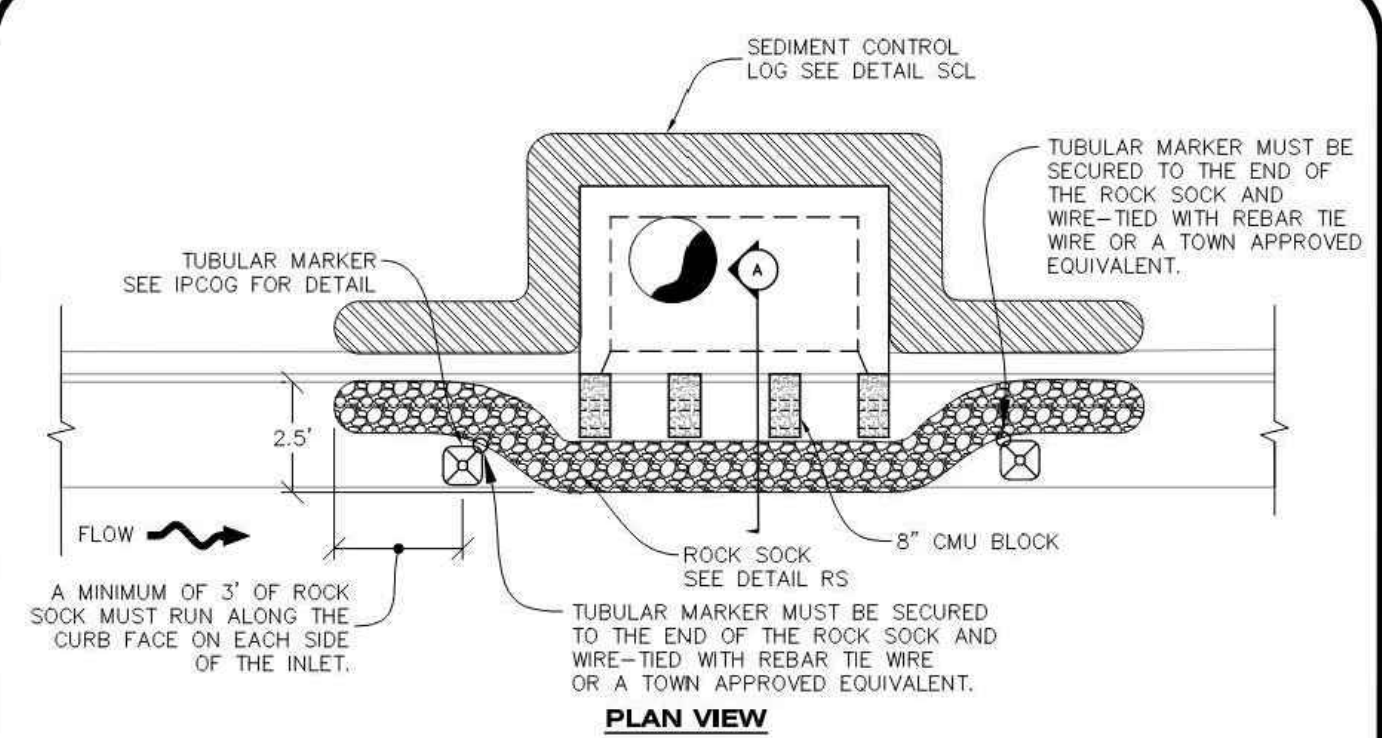
- DETENTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).

DETENTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES

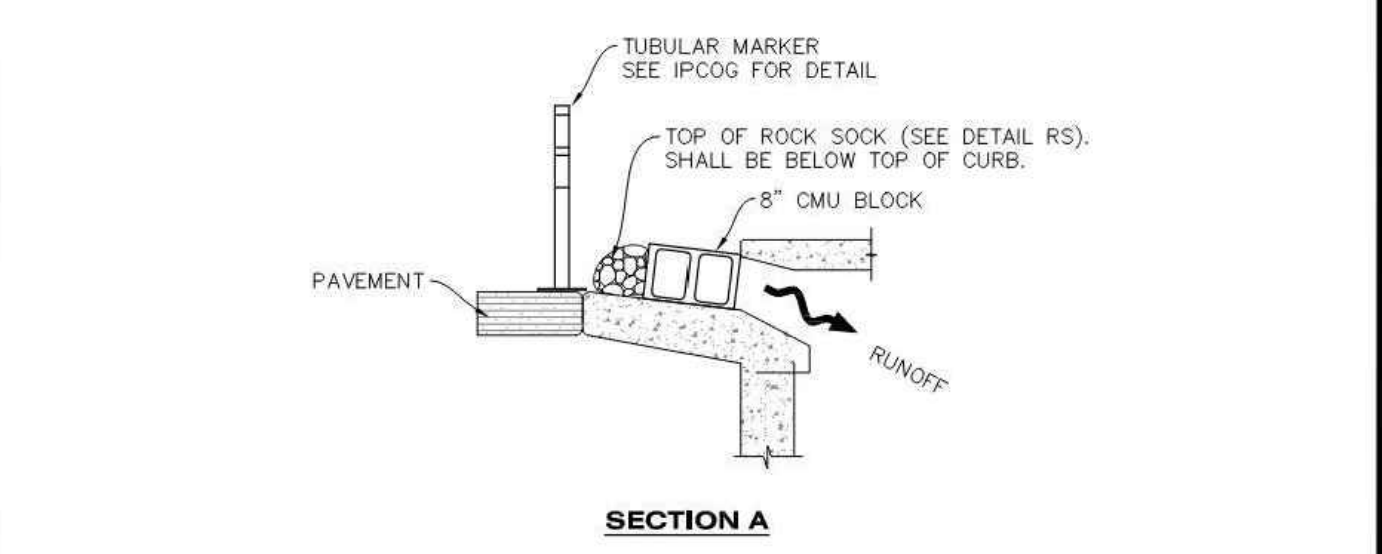
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
- OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
DP
2 OF 2
Oct. 2013

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PLAN VIEW



SECTION A

INLET PROTECTION, CURB ON SUMP, TYPE R INLET
IPCOS
1 OF 2
Oct. 2013

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
IPCOS
1 OF 2
Oct. 2013

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CURB INLET PROTECTION INSTALLATION NOTES

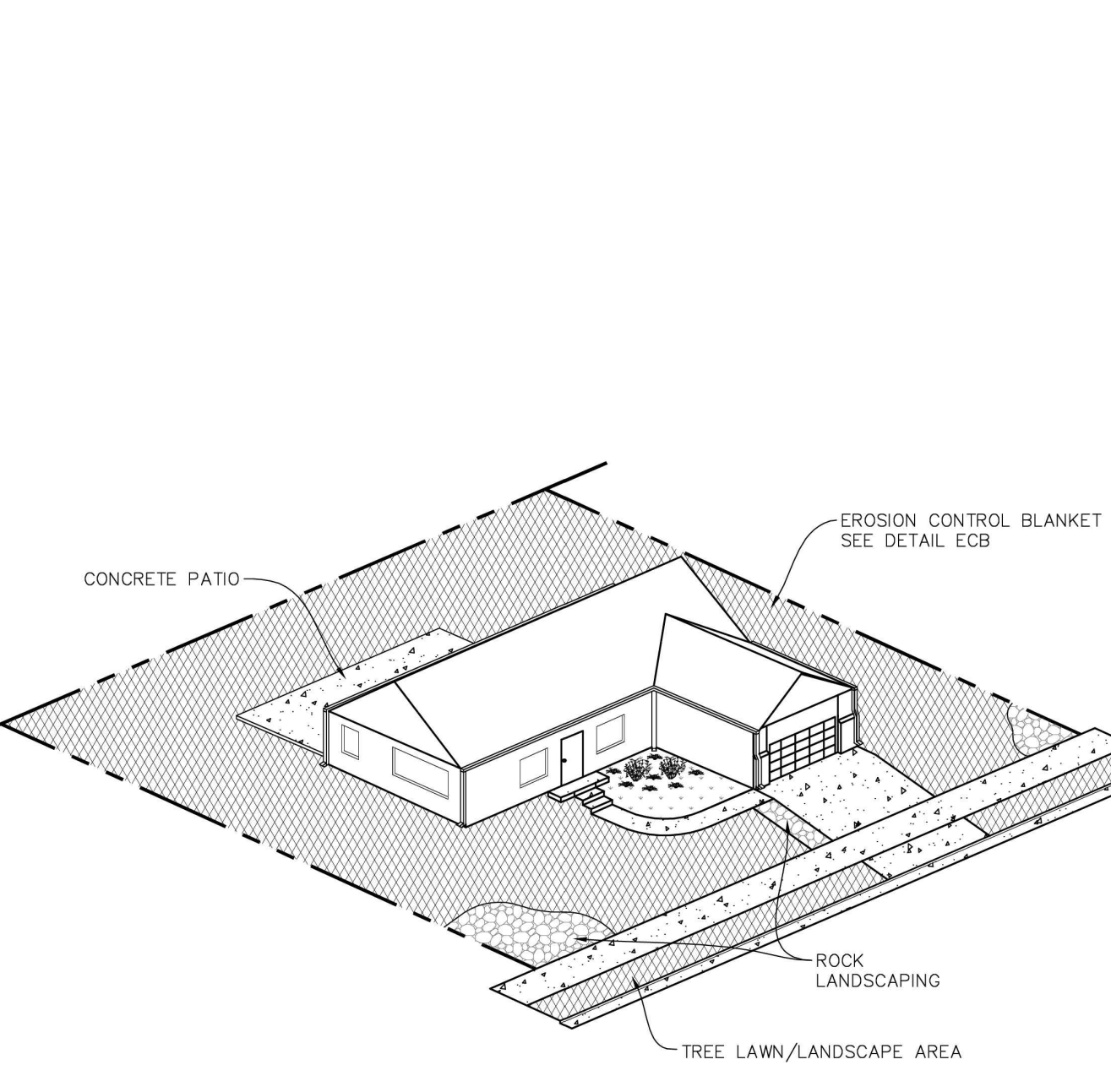
- SEE CBMP PLAN FOR LOCATION(S) OF CURB INLET PROTECTION.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- ROCK SOCK SHALL EXTEND 3.0' ALONG THE CURB BEYOND LOCATIONS WHERE IT RETURNS TO CONTACT CURB FACE.
- TUBULAR TRAFFIC MARKERS SHALL BE A MINIMUM OF 36" IN HEIGHT WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
- THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE CURB INLET. CMU BLOCKS OR THE ROCK SOCK SHALL BE USED AS INTERIM PROTECTION UNTIL THE FIRST LIFT OF ASPHALT IS INSTALLED.

CURB INLET PROTECTION INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CURB INLET PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- CURB INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
IPCOS
2 OF 2
Oct. 2013

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LOT PROTECTION
LP
1 OF 2
Oct. 2013

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
LP
1 OF 2
Oct. 2013

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EROSION CONTROL BLANKET FOR LOT PROTECTION INSTALLATION NOTES

- ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION INSTALLED IN THE TOWN SHALL MEET THE FOLLOWING MINIMUM REQUIREMENTS:

MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN APPROVED EQUAL	S150 BN APPROVED EQUAL

- ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION SHALL BE INSTALLED ACCORDING TO THE DETAIL DRAWINGS, WITH THE FOLLOWING ALLOWANCES AND ADDITIONS:
 - THE TOWN WILL ALLOW THE USE OF BIODEGRADABLE, EROSION CONTROL BLANKET-SPECIFIC STAPLES, IN LIEU OF TRADITIONAL METAL STAPLES.
 - ALL EROSION CONTROL BLANKET EDGES (SIDES AND ENDS) MUST OVERLAP THE ADJACENT BLANKET BY A MINIMUM OF 6-INCHES WITH THE UP-GRADIENT EDGES BEING PLACED ON TOP OF THE DOWN-GRADIENT EDGE OF THE ADJACENT BLANKET.
 - THE EDGES (SIDES AND ENDS) OF THE EROSION CONTROL BLANKETS DO NOT NEED TO BE TRENCHED INTO THE GROUND ASSUMING THE SITE CONDITIONS WILL NOT CAUSE EROSION BENEATH THE BLANKETS. THESE ASSUMPTIONS WILL BE THE RESPONSIBILITY OF THE EROSION CONTROL SUPERVISOR. ON OCCASION, THE TOWN'S INSPECTOR MAY REQUEST TRENCHING BASED UPON SITE CONDITIONS.
 - TOPSOIL PLACEMENT, SEEDING, AND MULCHING WILL NOT BE REQUIRED ON THE LOTS PRIOR TO THE INSTALLATION OF THE EROSION CONTROL BLANKET.
- ANY DAMAGED OR EXCESS STAPLES SHALL BE REMOVED FROM THE SITE FOLLOWING INSTALLATION.
- THE EROSION CONTROL BLANKETING REQUIRED AS PART OF THE LOT PROTECTION (LP) REQUIREMENT MUST BE INSTALLED OVER ALL UN-LANDSCAPED AREAS WITHIN EACH RESIDENTIAL, MULTI-FAMILY, AND COMMERCIAL LOT PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY / TEMPORARY CERTIFICATE OF OCCUPANCY.

EROSION CONTROL BLANKET FOR LOT PROTECTION MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKET FOR LOT PROTECTION.
- EROSION CONTROL BLANKETS FOR LOT PROTECTION ARE INTENDED TO REMAIN IN PLACE AND MAINTAINED UNTIL LANDSCAPING IS INSTALLED.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
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PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
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REVISION		
NO.	DESCRIPTION	DATE

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QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO

CONSTRUCTION DOCUMENTS



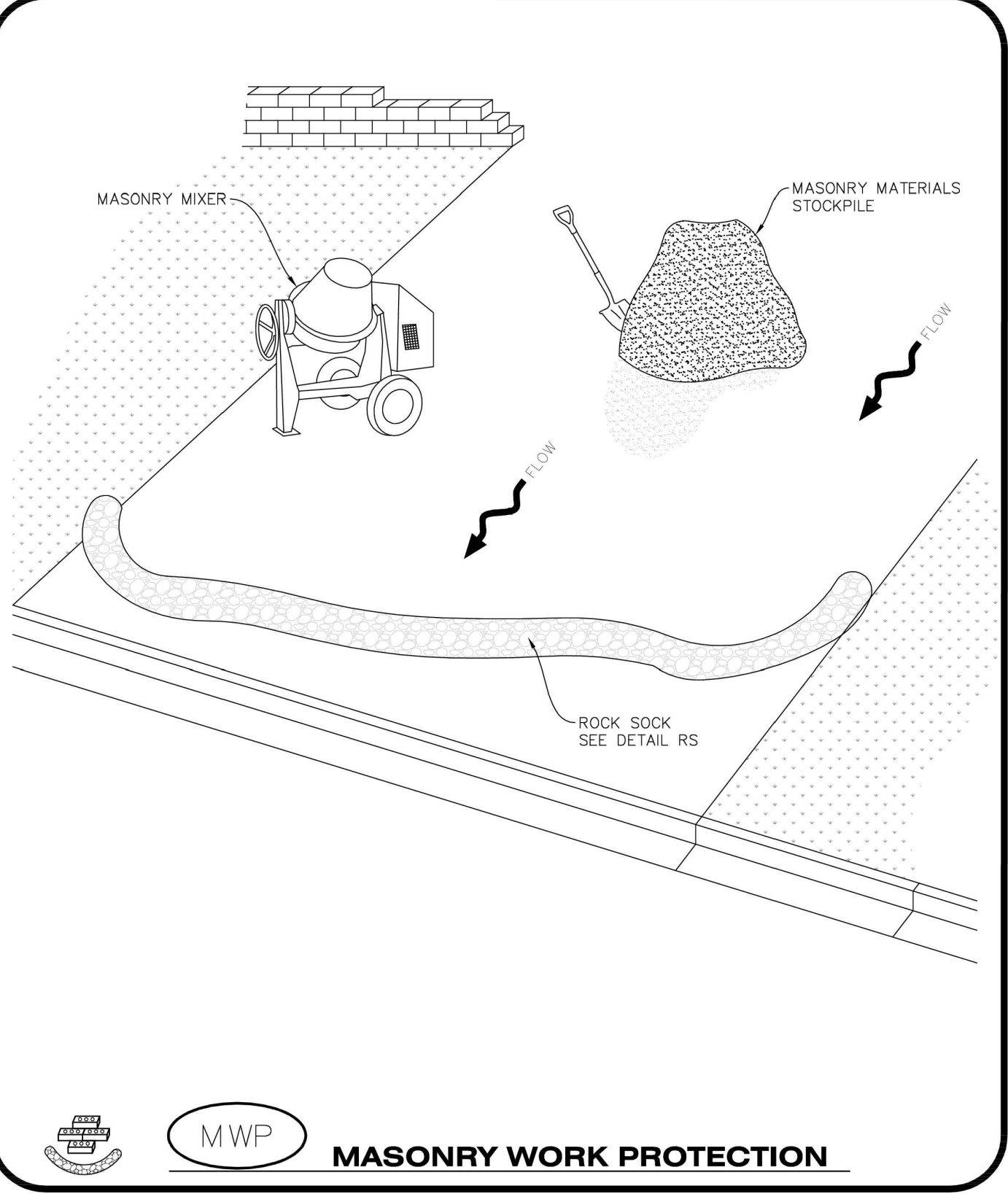
PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
SHEET TITLE
SHEET NUMBER

C3.8

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-CBMP.DWG - SAVED BY: MCALESDERON

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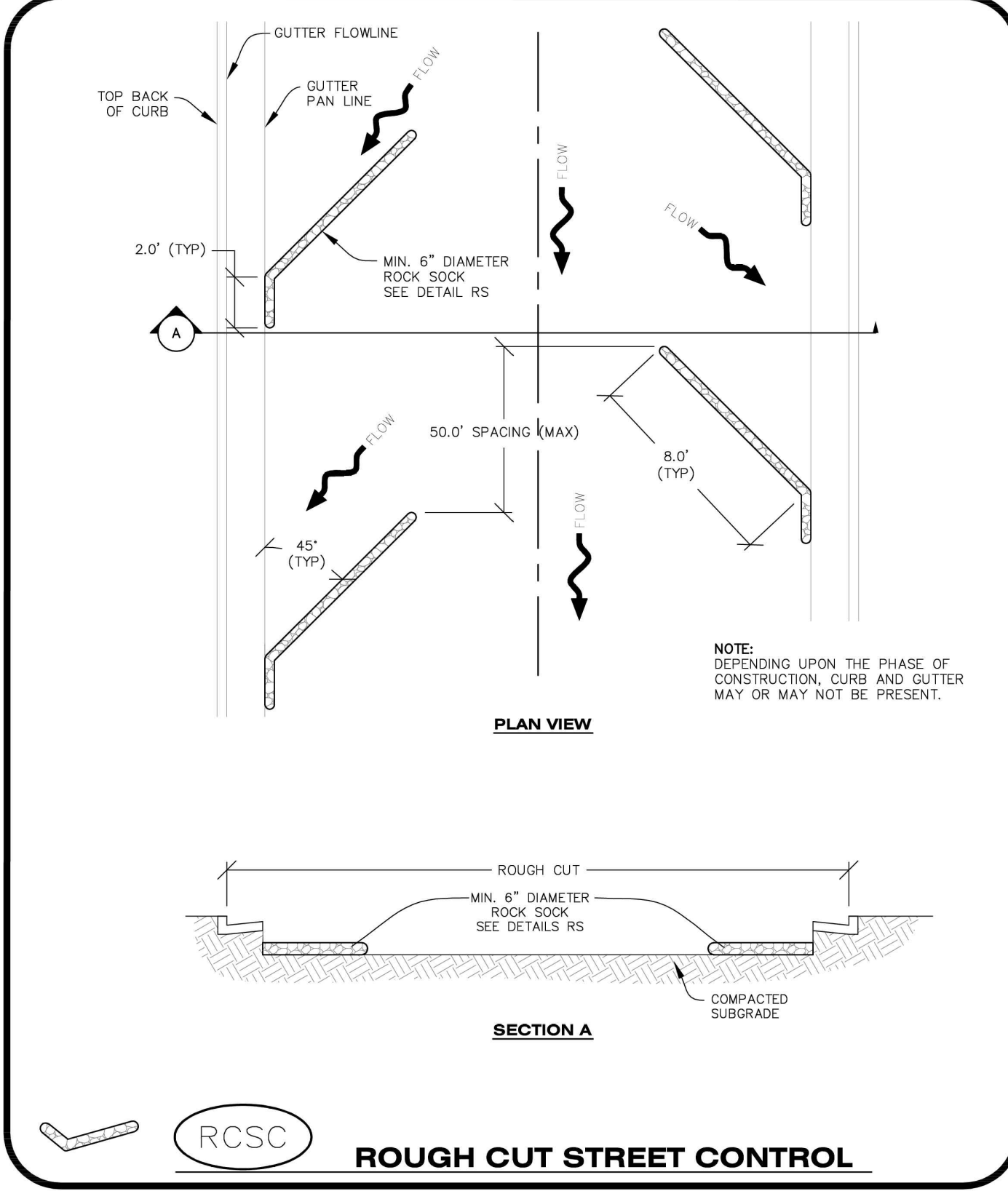
MWP **MASONRY WORK PROTECTION**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- MASONRY WORK PROTECTION INSTALLATION NOTES**
1. MASONRY WORK PROTECTION MAY NEED TO BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
 2. A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
 3. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
 4. ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
 5. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.
- MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
 2. ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASH OUT AREA.
 3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.

Parker **CBMP** **MWP**
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2
Oct. 2013

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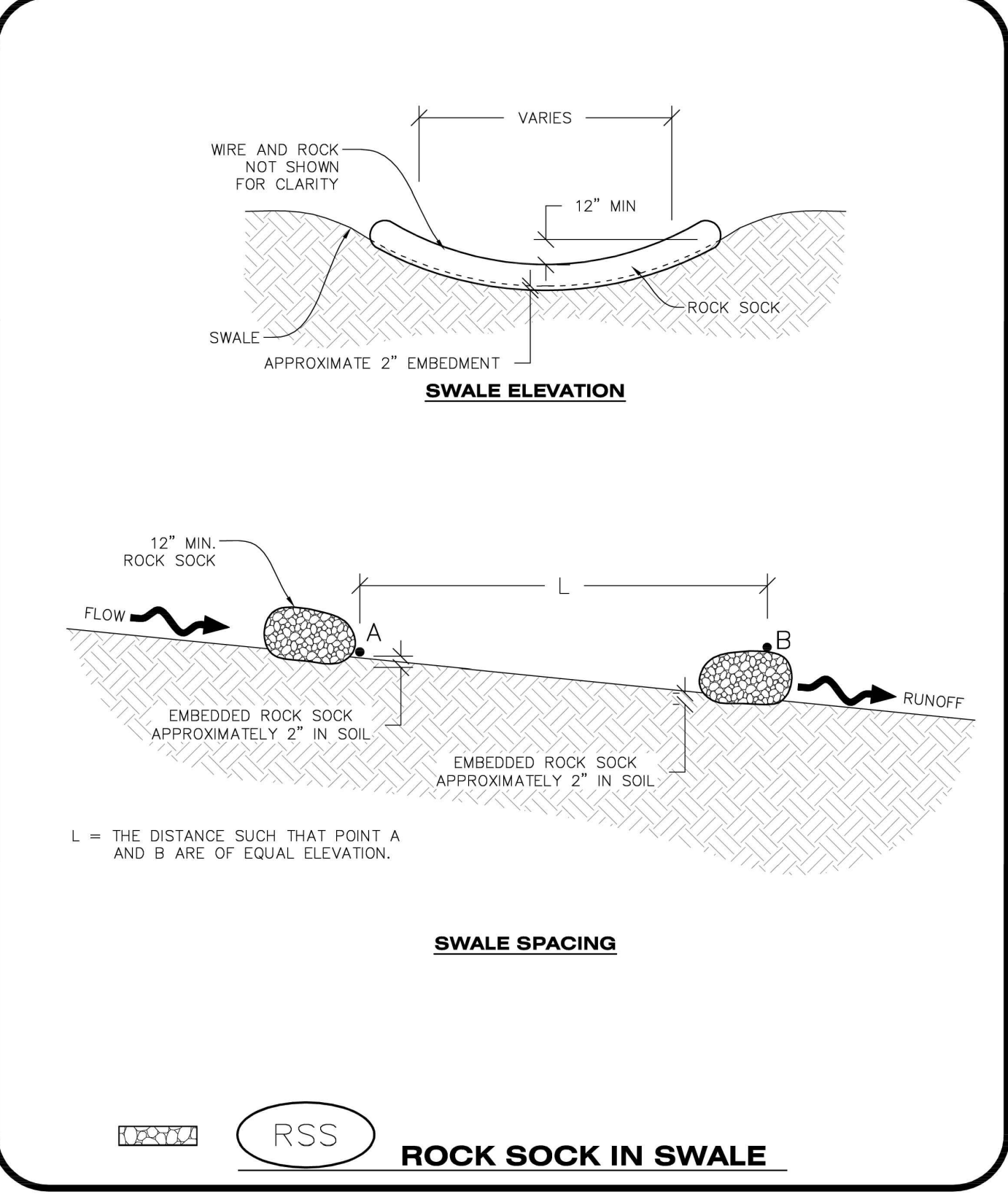
RCSC **ROUGH CUT STREET CONTROL**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- ROUGH CUT STREET CONTROL INSTALLATION NOTES**
1. SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
 2. THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.
- ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
 2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
 3. ROUGH CUT STREET CONTROL SHALL BE REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
 4. ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUB-GRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE RCSC SHOULD BE REMOVED IN INCREMENTS BASED ON SUBGRADE PREPARATION.

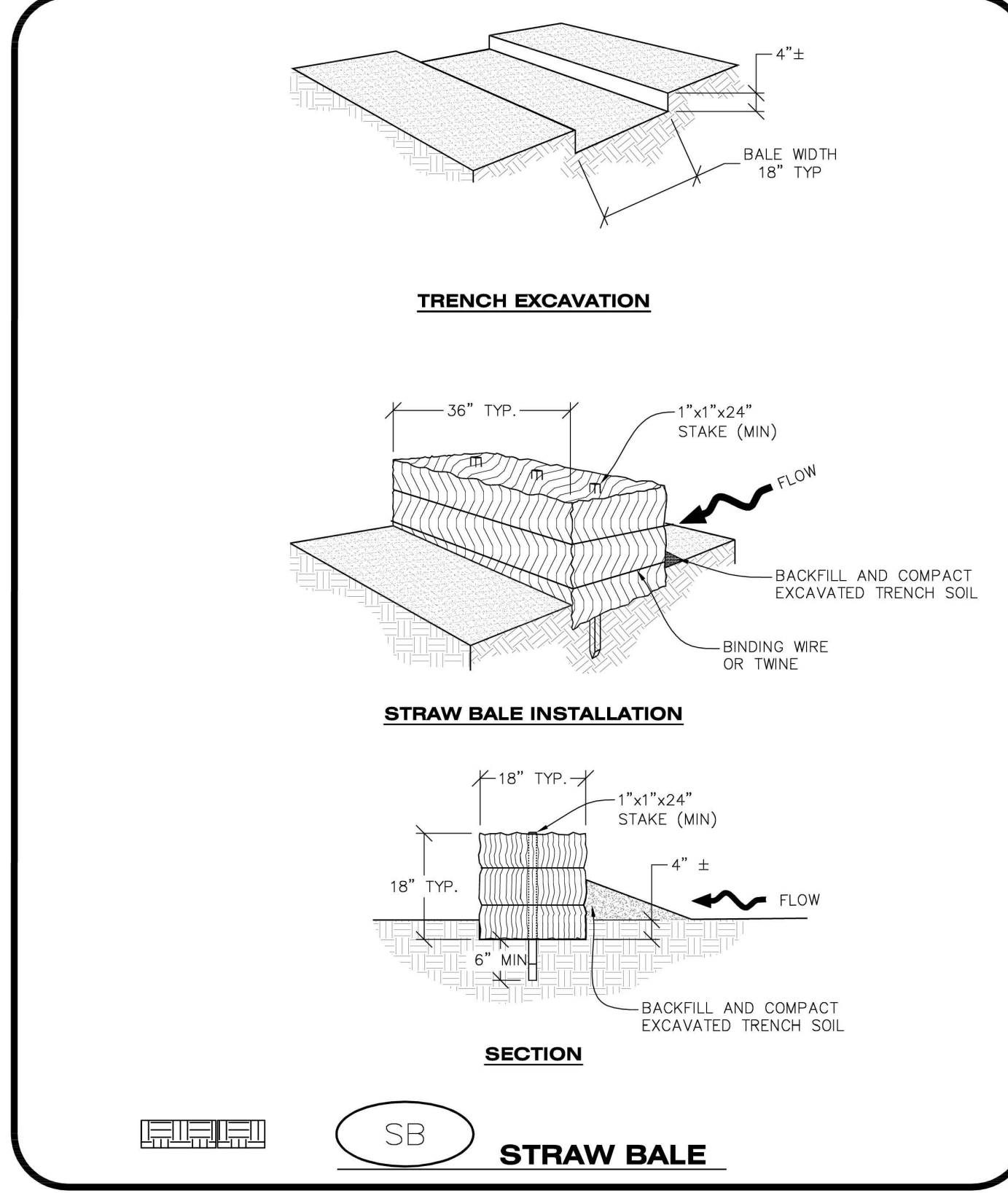
Parker **CBMP** **RCSC**
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2
Oct. 2013

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RSS **ROCK SOCK IN SWALE**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.



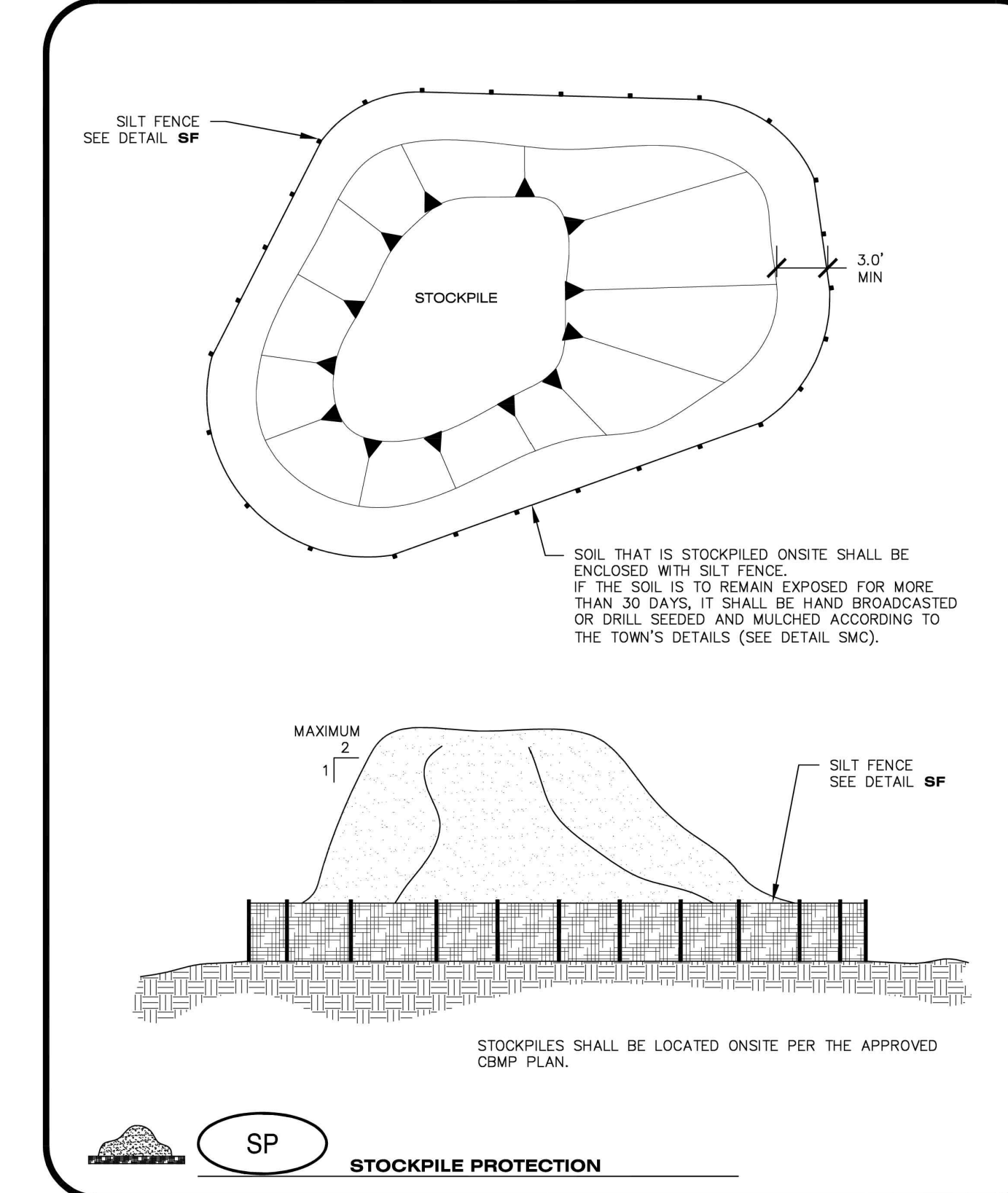
SB **STRAW BALE**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- STRAW BALE INSTALLATION NOTES**
1. SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
 2. TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36"x18"x18".
 3. TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1"x1"x24".
 4. WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
 5. STRAW BALES SHALL BE SPACED AND POSITIONED ACCORDING TO DETAILS.
- STRAW BALE INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
 2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
 3. STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
 4. STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN INSPECTOR.
 5. WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

Parker **CBMP** **SB**
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2
Oct. 2013

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Parker **CBMP** **SP**
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REVISION		
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PARKER, COLORADO
CONSTRUCTION DOCUMENTS

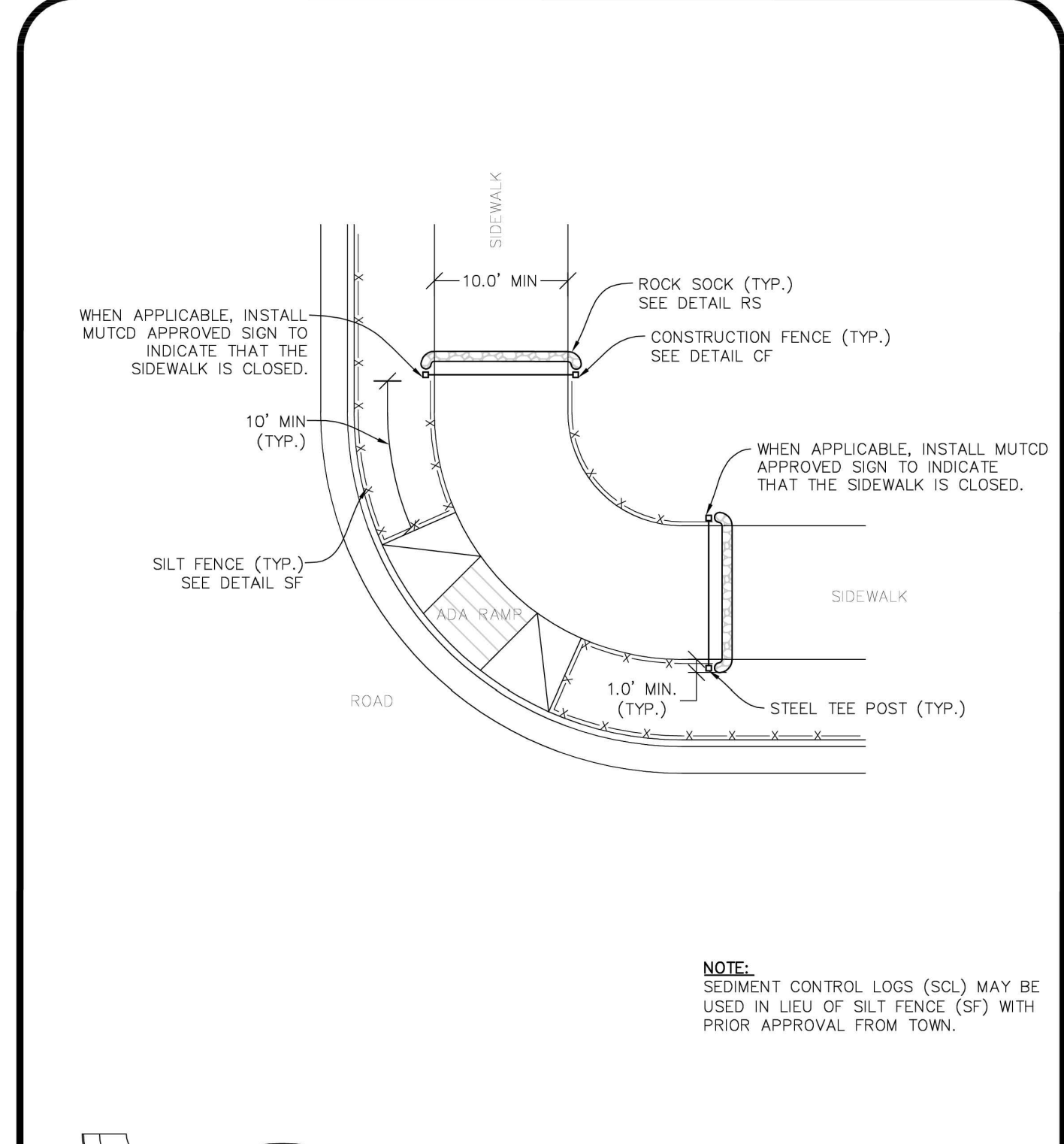


PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
SHEET TITLE
SHEET NUMBER
C3.9

DRAWING LOCATION - P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-CBMP.DWG - SAVED BY - MCALESDON

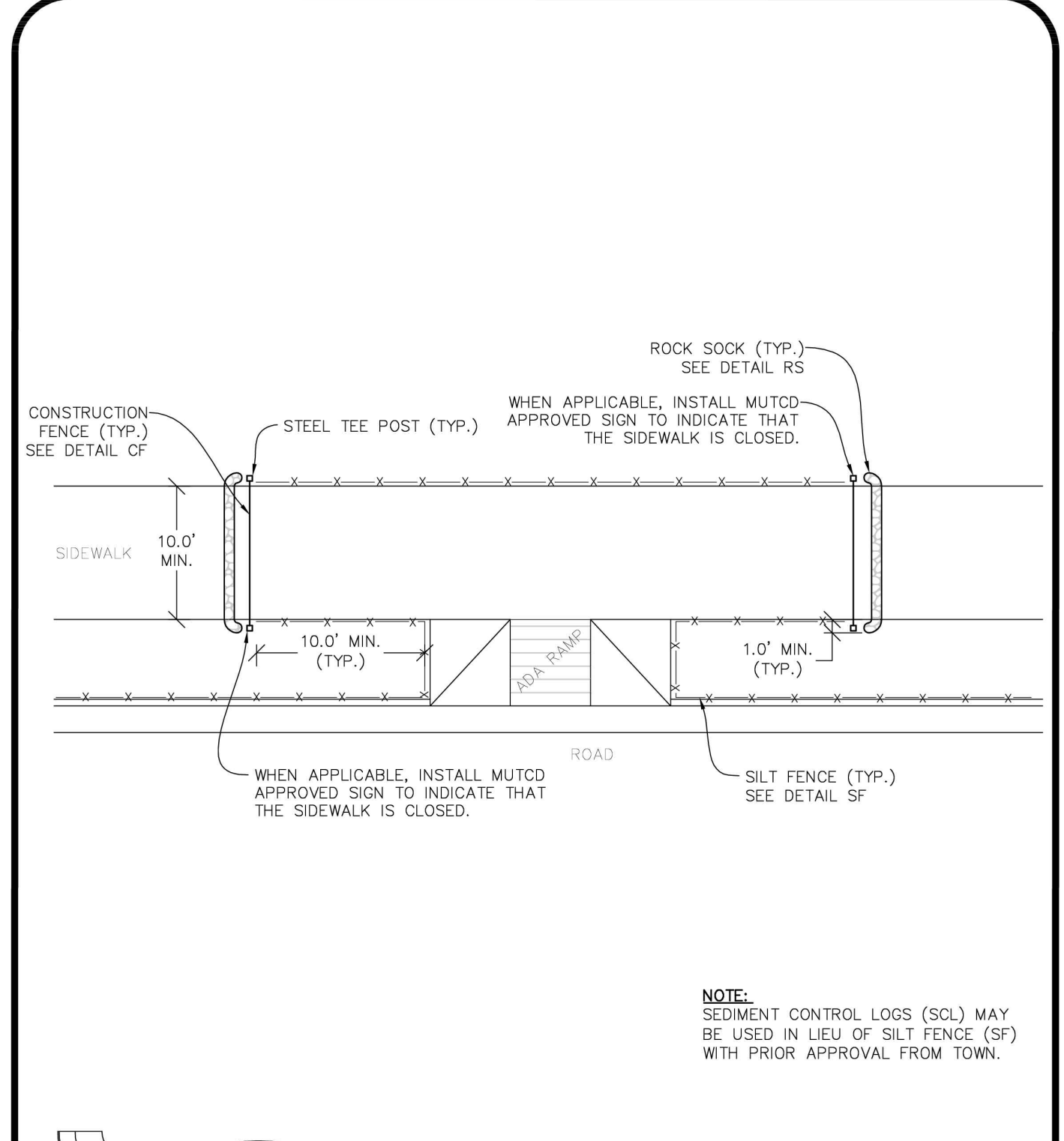
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STP
SIDEWALK TRANSITION PROTECTION (AT INTERSECTION)

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
STP
 1 OF 3
 Oct. 2013

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STP
SIDEWALK TRANSITION PROTECTION (ALONG STRAIGHT ROADWAY)

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
STP
 2 OF 3
 Oct. 2013

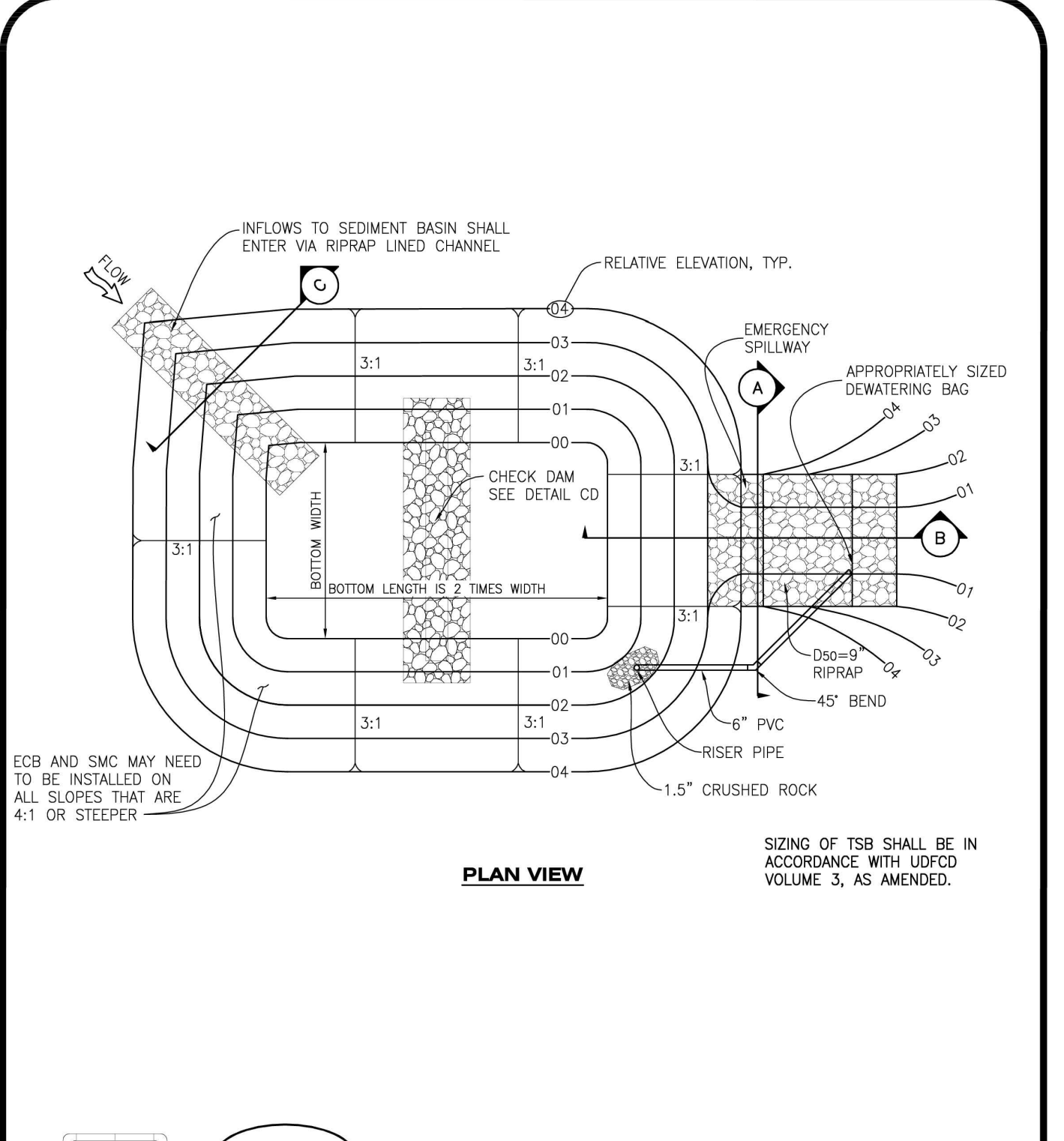
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- SIDEWALK TRANSITION PROTECTION INSTALLATION NOTES**
1. SEE PLAN VIEW FOR LOCATION(S) OF SIDEWALK TRANSITION PROTECTION.
 2. ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
 3. SILT FENCE SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL SF).
 4. CONSTRUCTION FENCE SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL CF).
 5. SEDIMENT CONTROL LOGS MAY BE USED IN LIEU OF SILT FENCE WITH PRIOR APPROVAL FROM THE TOWN.

- SIDEWALK TRANSITION PROTECTION INSPECTION & MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SIDEWALK TRANSITION INSPECTION.

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
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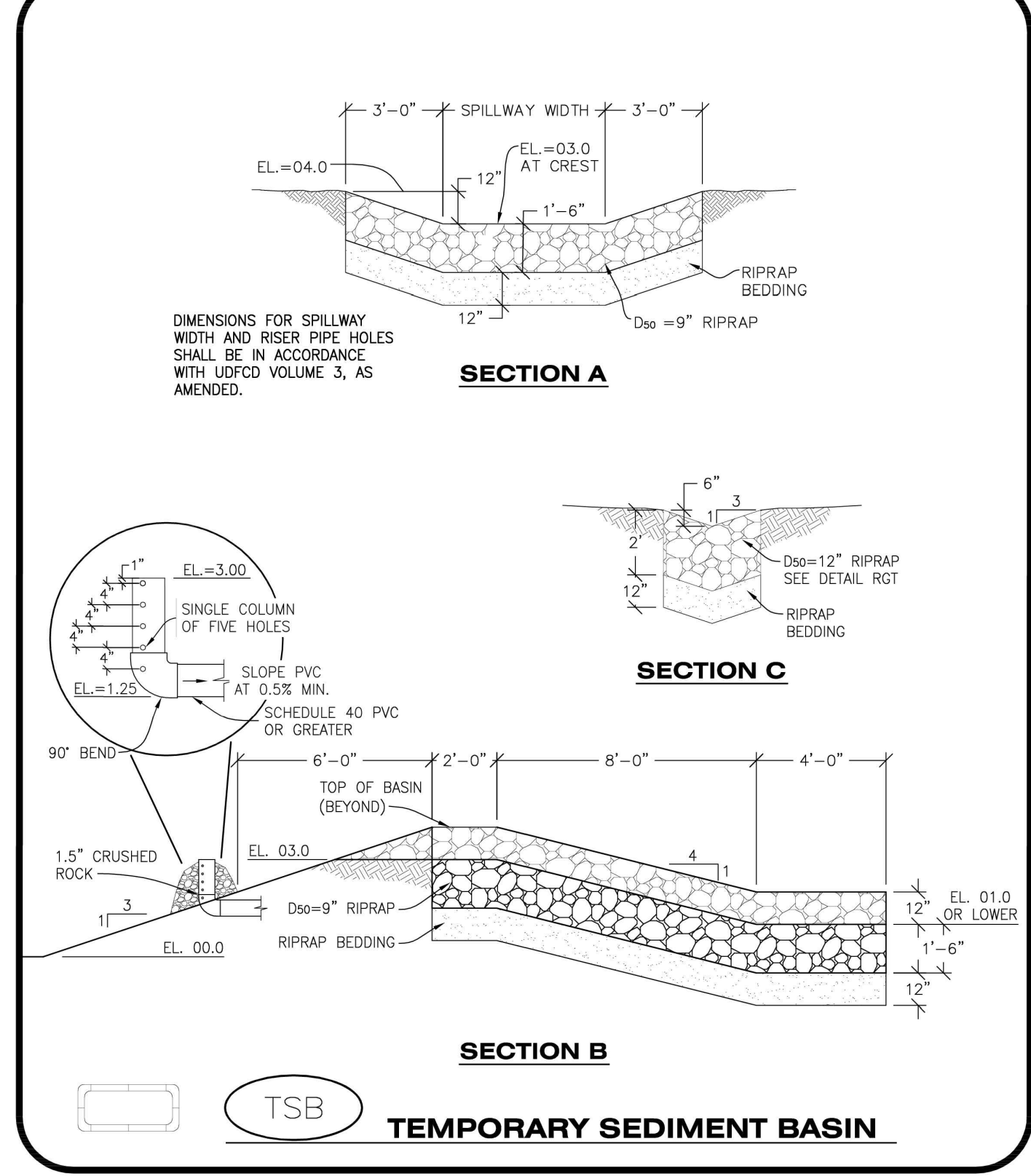
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.



TSB
TEMPORARY SEDIMENT BASIN

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
TSB
 1 OF 3
 Oct. 2013

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TSB
TEMPORARY SEDIMENT BASIN

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
TSB
 2 OF 3
 Oct. 2013

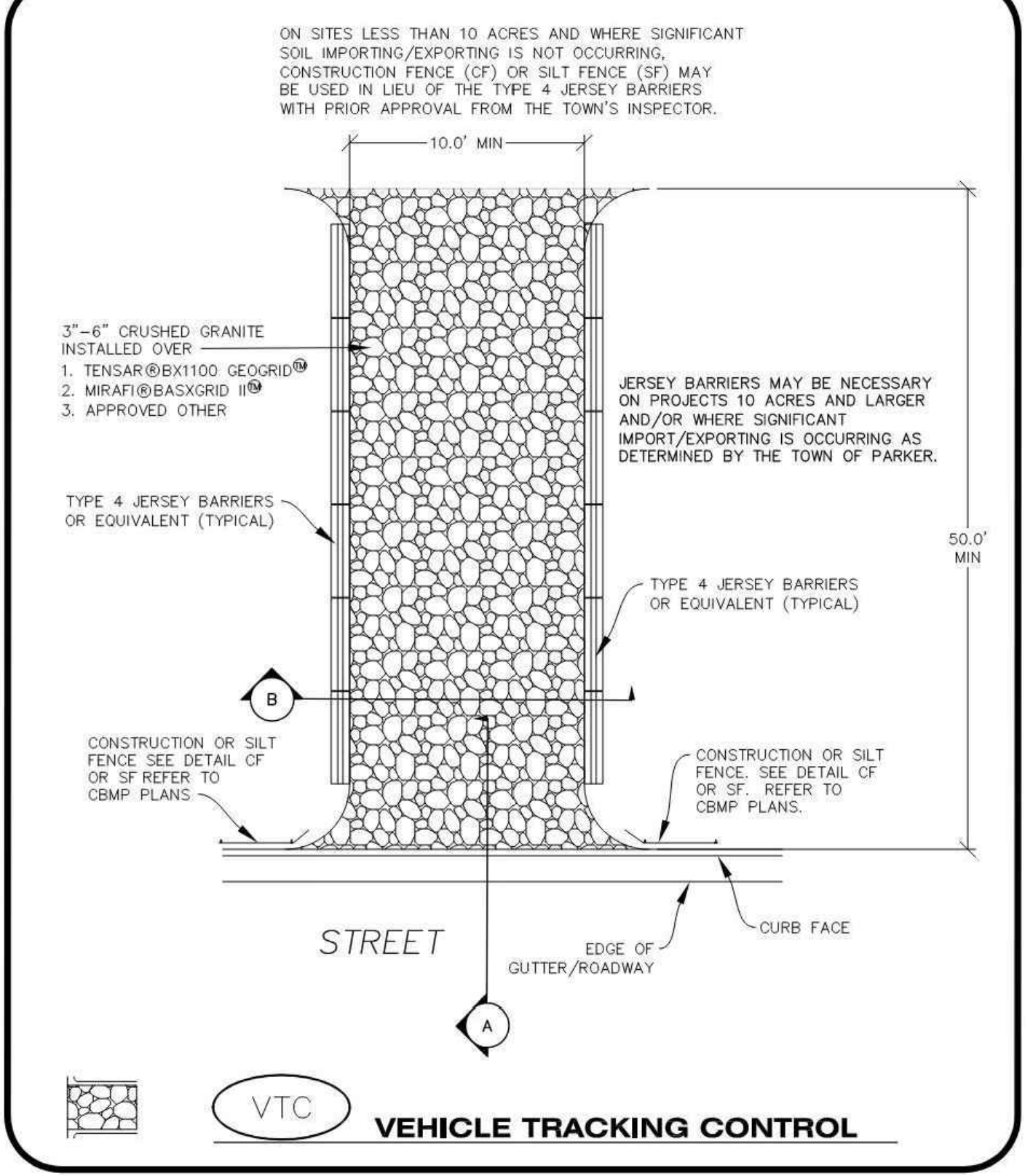
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

- TEMPORARY SEDIMENT BASIN INSTALLATION NOTES**
1. SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT BASIN(S).
 2. THE TEMPORARY SEDIMENT BASIN(S) SHALL BE INSTALLED AND FUNCTIONING PRIOR TO ANY OTHER GRADING ACTIVITIES.
 3. THE EXACT DIMENSIONS AND DETAILS OF THE TEMPORARY SEDIMENT BASIN SHALL BE DETERMINED BY THE DESIGN ENGINEER, IN ACCORDANCE WITH UDFCD VOLUME 3, AS AMENDED.
 4. EMBANKMENT MATERIAL SHALL CONSIST OF SOIL FREE OF DEBRIS, ORGANIC MATERIAL, AND ROCKS OR CONCRETE GREATER THAN 3" AND SHALL HAVE A MINIMUM OF 15% BY WEIGHT PASSING THE NO. 200 SIEVE.
 5. EMBANKMENT MATERIAL SHALL BE COMPACTED TO A MINIMUM OF 95% DENSITY, AND WITHIN +/- 2% OF OPTIMUM MOISTURE IN ACCORDANCE WITH ASTM D698.
 6. AN APPROPRIATELY SIZED DEWATERING BAG SHALL BE SECURED TO THE END OF THE DISCHARGE PIPE. THE DEWATERING BAG SHALL BE REPLACED ONCE SEDIMENT ACCUMULATION REACHES 50%.

- TEMPORARY SEDIMENT BASIN INSPECTION AND MAINTENANCE NOTES**
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE TEMPORARY SEDIMENT BASIN.
 2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT REACHES A DEPTH OF 2.0', OR WITHIN 2.0' OF THE SPILLWAY CREST, OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
 3. SEDIMENT BASINS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL UPSTREAM VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR.

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
TSB
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 Oct. 2013

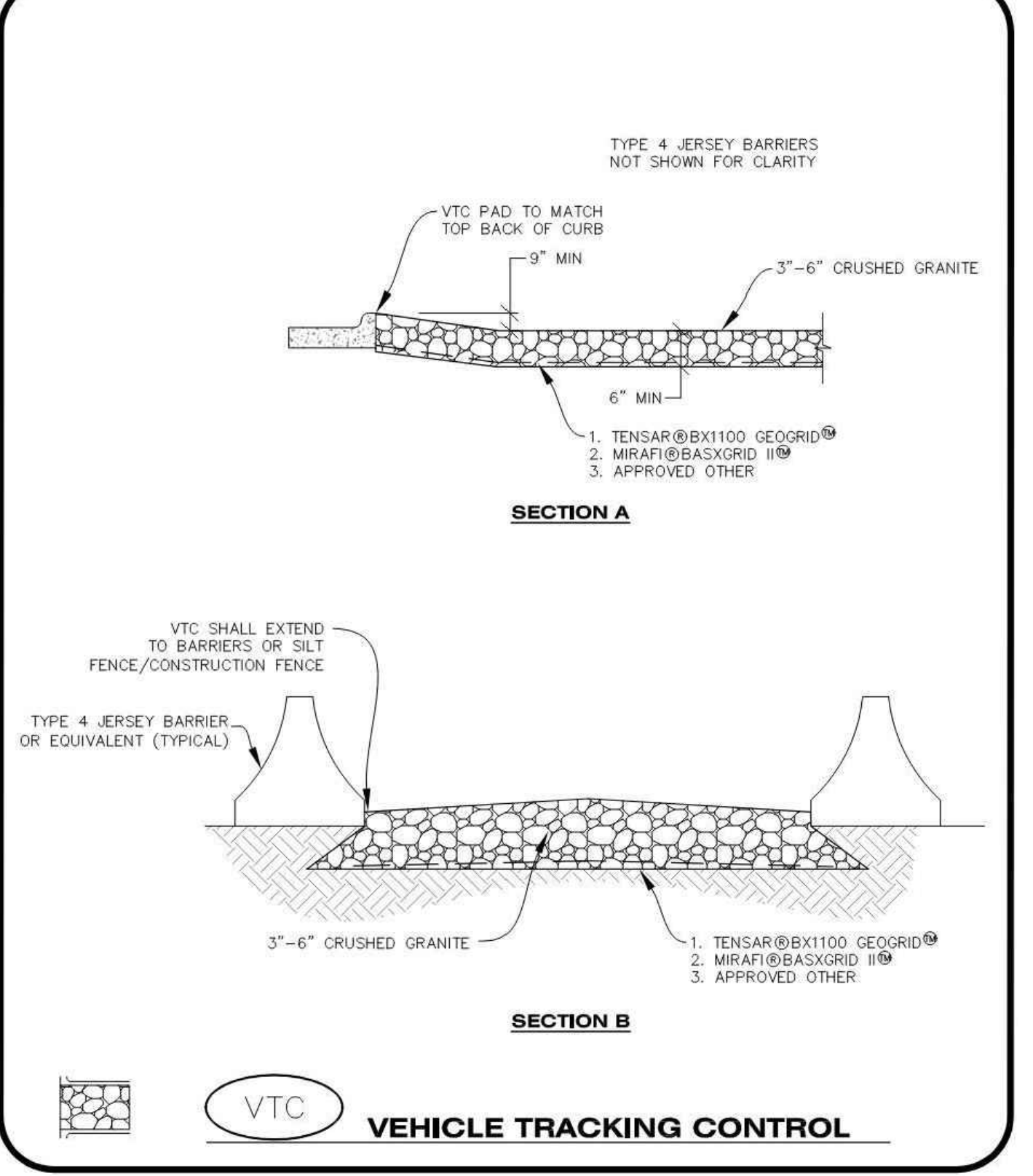
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VTC
VEHICLE TRACKING CONTROL

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
VTC
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 Oct. 2013

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VTC
VEHICLE TRACKING CONTROL

CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
VTC
 2 OF 3
 Oct. 2013



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REVISION		
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QUICK N CLEAN
 9572 TWENTY MILE ROAD
 PARKER, COLORADO
 CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

CBMP DETAILS
 SHEET TITLE
 SHEET NUMBER

C3.10

DRAWING LOCATION: P:\31600\31672\DRAWINGS\DESIGN\WORKING\31672-BMP.DWG - SAVED BY: MCALESDON

VEHICLE TRACKING CONTROL PAD INSTALLATION NOTES

1. SEE CBMP PLAN FOR LOCATION(S) OF VEHICLE TRACKING CONTROL PAD(S).
2. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PAVED AREAS AND WHERE ALTERNATIVE BMP'S ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.
3. THE VEHICLE TRACKING CONTROL PAD(S) INDICATED ON CBMP PLAN SHALL BE INSTALLED PRIOR TO ANY LAND DISTURBING ACTIVITIES.
4. VEHICLE TRACKING CONTROL PADS SHALL BE A MINIMUM OF 50-FOOT LONG AND 10-FOOT WIDE, UNLESS A VARIANCE HAS BEEN GRANTED BY THE TOWN'S INSPECTOR.
5. A BIAxIAL GEO-GRID SHALL BE PLACED UNDER THE VEHICLE TRACKING CONTROL PAD PRIOR TO THE PLACEMENT OF ROCK. THE AREA SHALL BE FREE FROM ANY VOIDS, ROCKS AND DEBRIS. THE BIAxIAL GEO-GRID SHALL BE TENSAR BX1100, MIRAFI BASXGRID II, OR AN APPROVED EQUAL. GEO-GRID SHALL BE PLACED, AND APPROPRIATELY OVERLAPPED IF NECESSARY, TO COVER THE ENTIRE LENGTH AND WIDTH OF THE VEHICLE TRACKING CONTROL PAD.
6. CRUSHED ROCK SHALL BE A MINIMUM OF 3-6" GRANITE WITH A FRACTURED FACE (ALL SIDES).

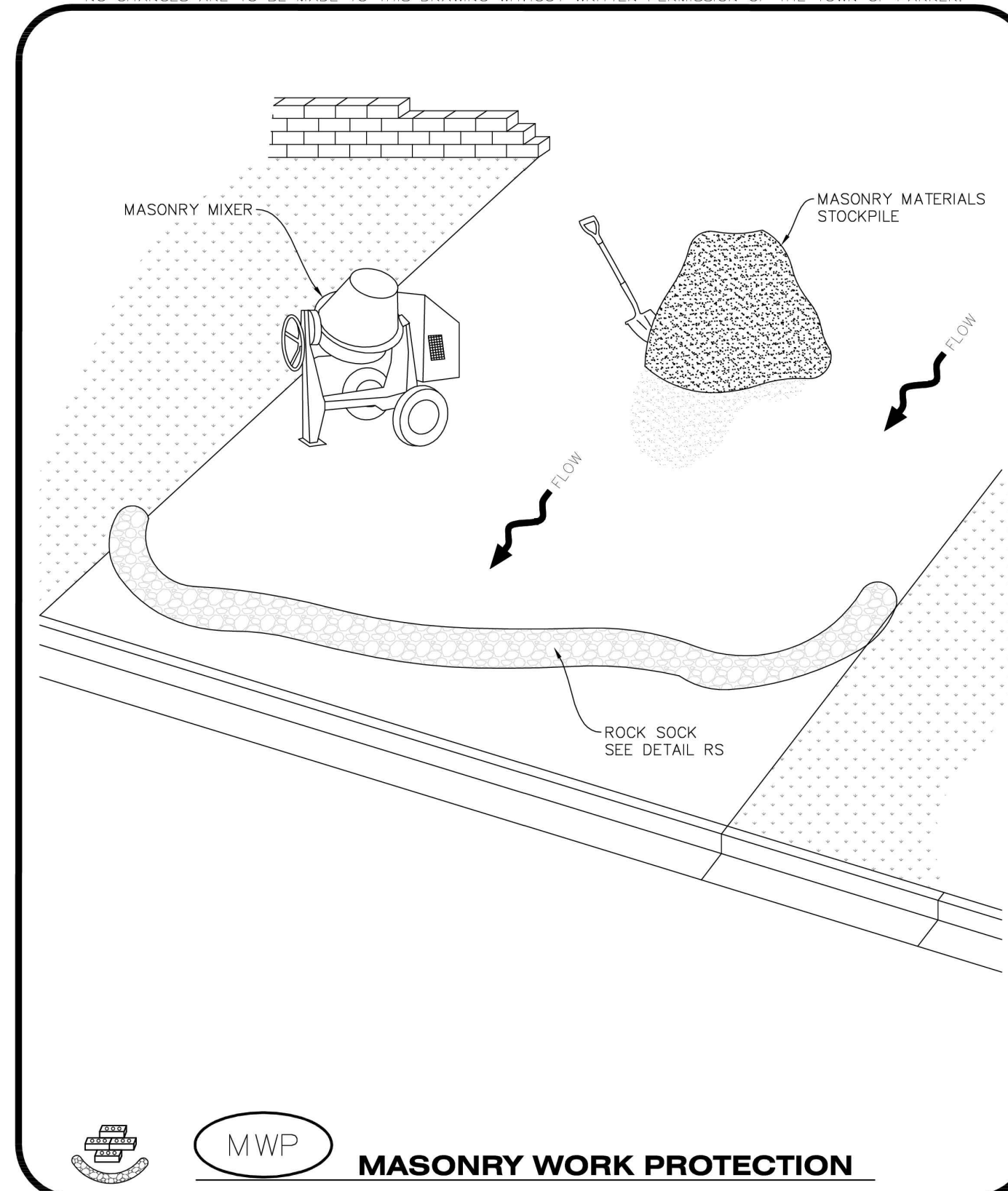
VEHICLE TRACKING CONTROL PAD INSTALLATION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE VEHICLE TRACKING CONTROL PAD.
2. WHEN THE VEHICLE TRACKING CONTROL PAD IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE VEHICLE TRACKING CONTROL PAD SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
3. THE VEHICLE TRACKING CONTROL PAD SHALL BE MAINTAINED SUCH THAT THE ROCK REMAINS RELATIVELY LOOSE AND ACCUMULATED MUD AND OTHER DEBRIS IS REGULARLY REMOVED.

To allow for a proper and timely establishment of native grasses, temporary irrigation is required over all areas where permanent native seeding is proposed. This includes, but is not limited to: detention ponds, drainageways, park and trail areas, general open space, etc. In addition to the landscape plans, please show the temporary irrigation symbol (TI) on the CBMP Plan over all areas where permanent native seeding is proposed. As a reminder, when working within Parker Water and Sanitation District's (PWSD) jurisdiction, the following is required:

- Temporary irrigation for native seed must be supplied by a fire hydrant hook-up. Use of proposed/existing landscape irrigation taps is not allowed. Please indicate on the irrigation plans fire hydrants that are to be used for the temporary irrigation mainline and provide a detail for the connection. This detail should show the hydrant, PRV, PWSD water meter, a 2" backflow assembly, a 2" x 2" cam lock for fire department quick disconnect and a support system for all components
- The following note must be shown on the Irrigation plan set "Temporary irrigation for establishment of native vegetation must be installed above ground, and removed immediately after establishment is complete, or in no case, any longer than one growing season." PWSD may allow temporary irrigation to continue for one additional growing season with prior approval.
- The following note must be shown on the Irrigation plan set "All temporary irrigation components including but not limited to mainlines, laterals, valves, heads and quick couplers must be installed above ground."
- All temporary irrigation components including but not limited to mainlines, laterals, valves, heads and quick couplers must be clearly labeled on the plans.
- Provide detail drawings for all above ground components including but not limited to mainlines, laterals, valves, heads and quick couplers.

If the project is outside of PWSD's jurisdiction, check with the applicable water provider regarding their specific requirements for temporary irrigation.




CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
 MWP
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 Oct. 2013

MASONRY WORK PROTECTION INSTALLATION NOTES

1. MASONRY WORK PROTECTION MAY NEED TO BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
2. A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
3. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
4. ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
5. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.

MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
2. ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASH OUT AREA.
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.


CBMP
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 MWP
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 Oct. 2013


CBMP
 CONSTRUCTION BEST MANAGEMENT PRACTICES
 VTC
 3 OF 3
 Oct. 2013



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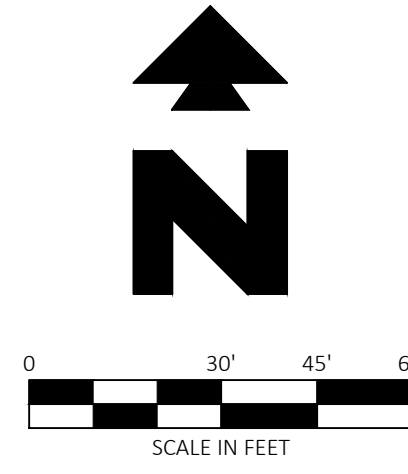
PROFESSIONAL OF RECORD	TAB
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CBMP DETAILS
 SHEET TITLE
 SHEET NUMBER

C3.11



Know what's below. Call before you dig.



LEGAL DESCRIPTION

REFERENCE C1 - COVER SHEET FOR LEGAL DESCRIPTION. BENCHMARK DOUGLAS CONTROL MONUMENT #1.095035, A 3" ALUMINUM CAP. ELEVATION = 5906.34 FEET (NAVD1988), AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS

THE BEARINGS ARE BASED ON THE WEST ROW LINE OF SOUTH PARKER ROAD ASSUMED TO BEAR S23°56'20"E BETWEEN MONUMENTS FOUND AND DESCRIBED HEREON.

FLOOD ZONE INFORMATION

SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM. MAP NUMBER: 08035C0067G EFFECTIVE DATE: MARCH 16, 2016

NOTE:

SEE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF PORCHES, RAMPS, VESTIBULE, SLOPED PAVING, TRUCK DOCKS, BUILDING UTILITY ENTRANCE LOCATIONS AND PRECISE BUILDING DIMENSIONS.

GENERAL SITE NOTES

- A. ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND O.S.H.A. STANDARDS. B. PRIOR TO CONSTRUCTION WITHIN ANY EXISTING PUBLIC RIGHT-OF-WAY, THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE AUTHORITY HAVING JURISDICTION. C. CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS. D. ALL DISTURBED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL, SEED, MULCH AND WATER UNTIL A HEALTHY STAND OF GRASS IS ESTABLISHED. E. ALL CURBED RADII ARE TO BE 2' OR 10' UNLESS OTHERWISE NOTED. STRIPED RADII ARE TO BE 5'. F. ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED. G. EXISTING STRUCTURES TO REMAIN WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED / ADJUSTED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID. H. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, (UNLESS OTHERWISE NOTED ON PLANS) INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID. I. SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY A LAND SURVEYOR. J. THE SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE PROJECT CONTRACT REQUIREMENTS. K. REFER TO ARCH. PLANS FOR Pylon AND/OR MONUMENT SIGNS. L. REFER TO ARCH. PLANS FOR SITE LIGHTING ELECTRICAL PLAN. M. ALL ACCESSIBLE STALLS SHALL BE MARKED WITH THE INTERNATIONAL SYMBOL OF ACCESSIBILITY AND A WARNING THAT VEHICLES IN VIOLATION OF THE LOCAL MUNICIPAL CODE SHALL BE TOWED AWAY. THE INTERNATIONAL SYMBOL AND TOW-AWAY WARNING SHALL BE POSTED CONSPICUOUSLY ON SEVEN FOOT POLES. N. ALL ACCESSIBLE PARKING STALLS SHALL BE PLACED ADJACENT TO FACILITY ACCESS RAMPS OR IN STRATEGIC AREAS WHERE THE OPERATOR SHALL NOT HAVE TO WHEEL OR WALK BEHIND PARKED VEHICLES WHILE TRAVELING TO OR FROM ACCESSIBLE PARKING STALLS AND RAMPS. O. MAXIMUM SLOPE IN ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2% (1:48) IN ANY DIRECTION. P. ANY FACILITIES THAT ARE TO REMAIN AND ARE DAMAGED BY THE CONTRACTOR AS A RESULT OF CONSTRUCTION, SHALL BE REPLACED PER CURRENT CONTRACT REQUIREMENTS AT THE SOLE EXPENSE OF THE CONTRACTOR. Q. CONTRACTOR SHALL ENSURE ADA CONFORMANCE OF ALL EXISTING SIDEWALKS AND CURB RAMPS WITHIN PUBLIC RIGHT-OF-WAY WITHIN THE PROJECT LIMITS. ALL EXISTING SIDEWALKS AND CURB RAMPS SHALL CONFORM TO THE LATEST ADOPTED VERSION (AND ALL AMENDMENTS) OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN & THE ARCHITECTURAL BARRIERS TEXAS ACCESSIBILITY STANDARDS (TAS). ANY EXISTING SIDEWALK THAT IS NOT IN CONFORMANCE WITH ADA REGULATIONS SHALL BE REMOVED AND REPLACED PER CITY OF DALLAS CODES AND COST SHALL BE INCLUDED IN BASE BID. R. ALL ADA SIGNS SHALL BE PERMITTED SEPARATELY.

SITE DETAILS (REF DETAIL SHEET C4.1)

- 01A TYPE A CONCRETE CURB AND GUTTER
01C TYPE C CONCRETE RAISED CURB AND GUTTER
01E TYPE E CURB
01P RAISED CURB AND GUTTER
03D CONCRETE SIDEWALK
03M WHEELCHAIR RAMP IN SIDEWALK
05A GUARD POST (SINGLE)
70A CONCRETE ACCESSIBLE RAMP AND HANDRAIL
73E REINFORCED REVERSED CURB WITHOUT FOOTING
73F REINFORCED REVERSED CURB WITH FOOTING

SITE NOTES

- 01A SEEDED GREEN AREA
02B TRANSFORMER PAD (PER ELEC. CO. AND/OR ARCH. PLANS).
02E TRASH DUMPSTER ENCLOSURE (PER ARCH. PLANS).
21A TAPER CURB TO MATCH EXISTING CURB.
70B FOUR (4) FOOT SCREEN WALL (REF ARCH PLANS).
70C "VACUTECH" SHADE CLOTH CANOPY, REF ARCHITECTURAL PLANS VACUUM CANOPY BY OTHERS AND SHOWN ON THIS PLAN FOR REFERENCE. REFER TO VENDOR PLANS FOR FINAL PLACEMENT AND INSTALL REQUIREMENTS.
70D "VACUTECH" VACUUM ENCLOSURE, REF ARCHITECTURAL PLANS VACUUM CANOPY BY OTHERS AND SHOWN ON THIS PLAN FOR REFERENCE. REFER TO VENDOR PLANS FOR FINAL PLACEMENT AND INSTALL REQUIREMENTS.
70E "VACUTECH" VACUUM (PALM ARCH), REF ARCHITECTURAL PLANS VACUUM CANOPY BY OTHERS AND SHOWN ON THIS PLAN FOR REFERENCE. REFER TO VENDOR PLANS FOR FINAL PLACEMENT AND INSTALL REQUIREMENTS.
70H EMPLOYEE PARKING & ESTABLISHMENT PARKING
70L BICYCLE RACK (PER ARCH. PLANS)
70M PAY STATION (REF ARCH. PLANS)
70N 3" DEEP DETECTABLE WARNING TO BE INSTALLED AT FULL WIDTH OF LANDING
70P WHEEL STOP, REF. ARCH. PLANS FOR FINAL PLACEMENT



Vicinity Map

1"=2,000'

Table with 2 columns: Field Name and Value. Includes sections for PROJECT INFORMATION, SITE INFORMATION, ZONING INFORMATION, and PARKING REQUIREMENTS.

EXISTING LEGEND

- PROPERTY LINE/RIGHT OF WAY LINE
COMMUNICATIONS MANHOLE
SANITARY SEWER MANHOLE
STORM SEWER MANHOLE
UNDERGROUND GAS LINES
UNDERGROUND COMMUNICATIONS LINES
UNDERGROUND STORM SEWER LINES

PROPOSED

- CONCRETE CURB
BUILDING CONTROL POINT
PROPOSED PARKING SPACES
PEDESTRIAN PATH OF TRAVEL
FIRE APPARATUS (REFER TO C4.2 - SITE DETAILS FOR SPECIFICATIONS)

The Town of Parker review constitutes general compliance with the Town's Standards and approved variances, subject to these plans being stamped, signed, and dated by the professional engineer of record. Review by the Town does not constitute approval of the plan design or accuracy and correctness of engineering calculations. Errors in the design or calculations remain the responsibility of the registered professional engineer whose stamp and signature are affixed to this document.

This review does not constitute approval of any private on-site improvements which may be shown. Construction cannot commence until all required drainage/traffic reports, final development plans, special reviews, grading permit, and/or other permits are complete, approved and on file with the Town of Parker.

Town of Parker, Director of Engineering Date 06/26/2023



CEI ENGINEERING ASSOCIATES, INC. 710 W. PINEDALE AVE. FRESNO, CA 93711



CLIENT 3K1 CONSULTING SERVICES, LLC. 11811 N. TATUM BOULEVARD, PHOENIX, ARIZONA 85028



PLANS PREPARED FOR QUICK N CLEAN 7291 E. ADOBE DRIVE, SUITE 115 SCOTTSDALE, AZ 85255

REVISION table with columns: NO., DESCRIPTION, DATE

FOR REVIEW ONLY NOT FOR CONSTRUCTION

QUICK N CLEAN 9572 TWENTY MILE ROAD PARKER, COLORADO

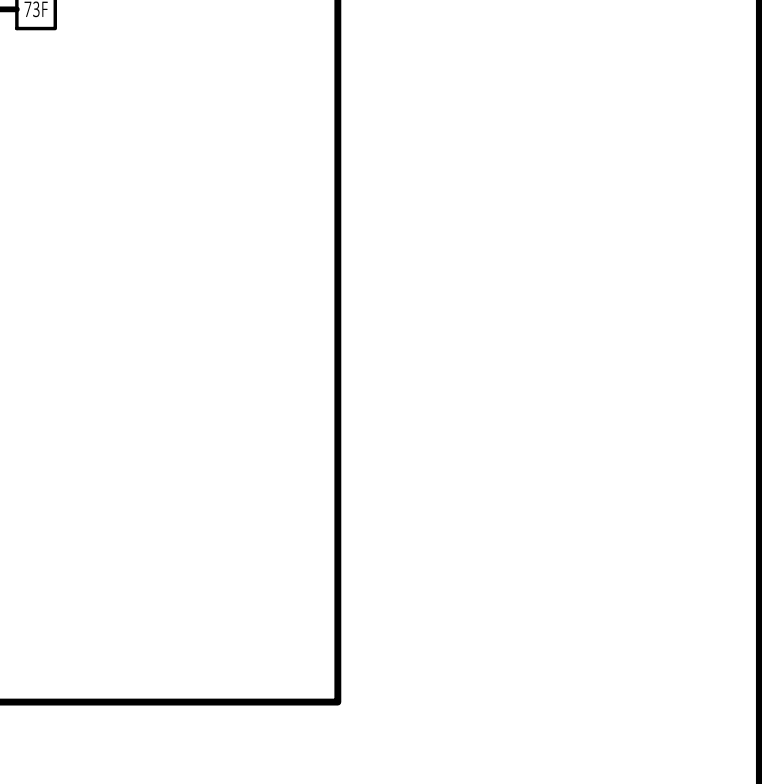
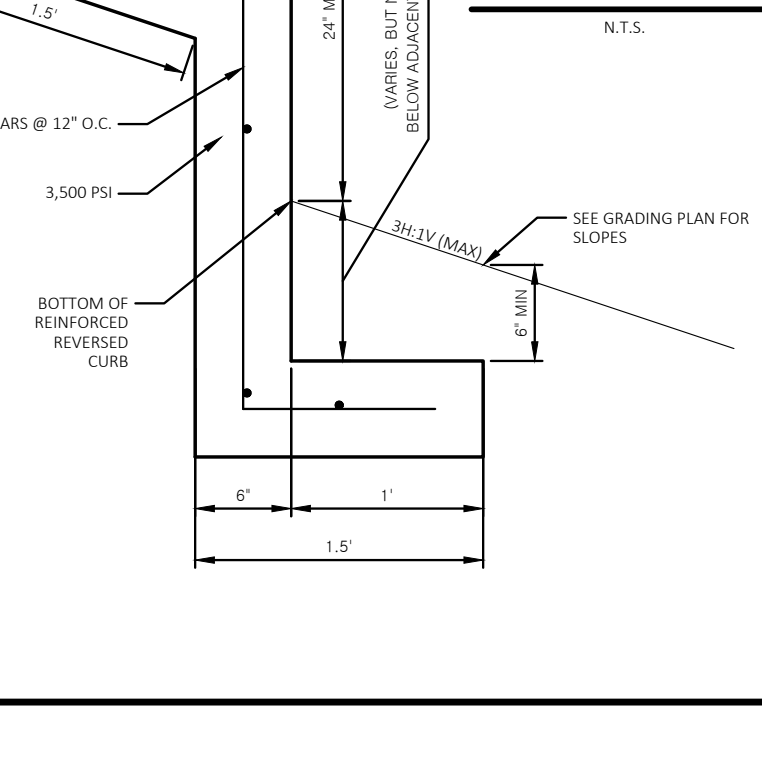
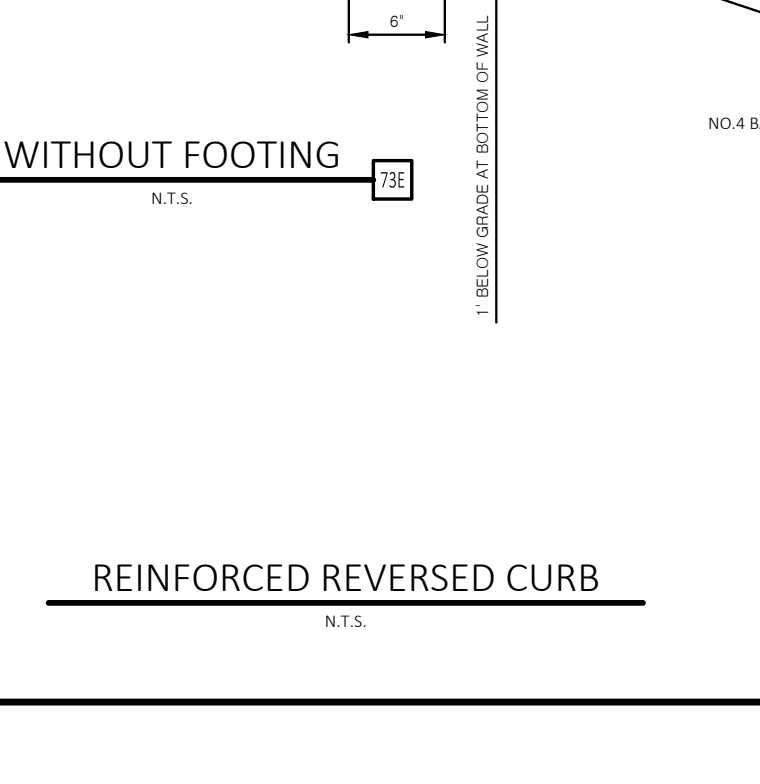
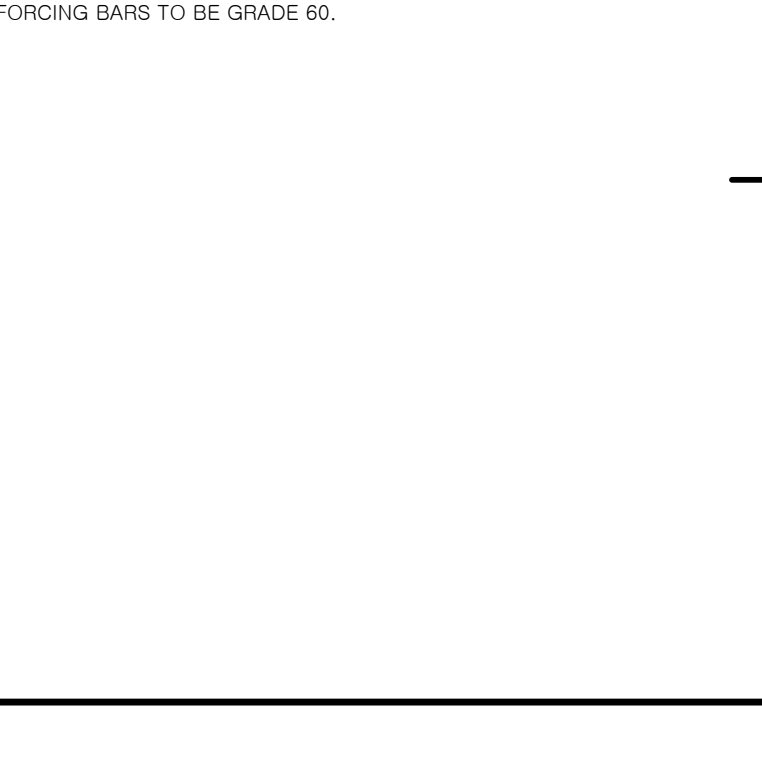
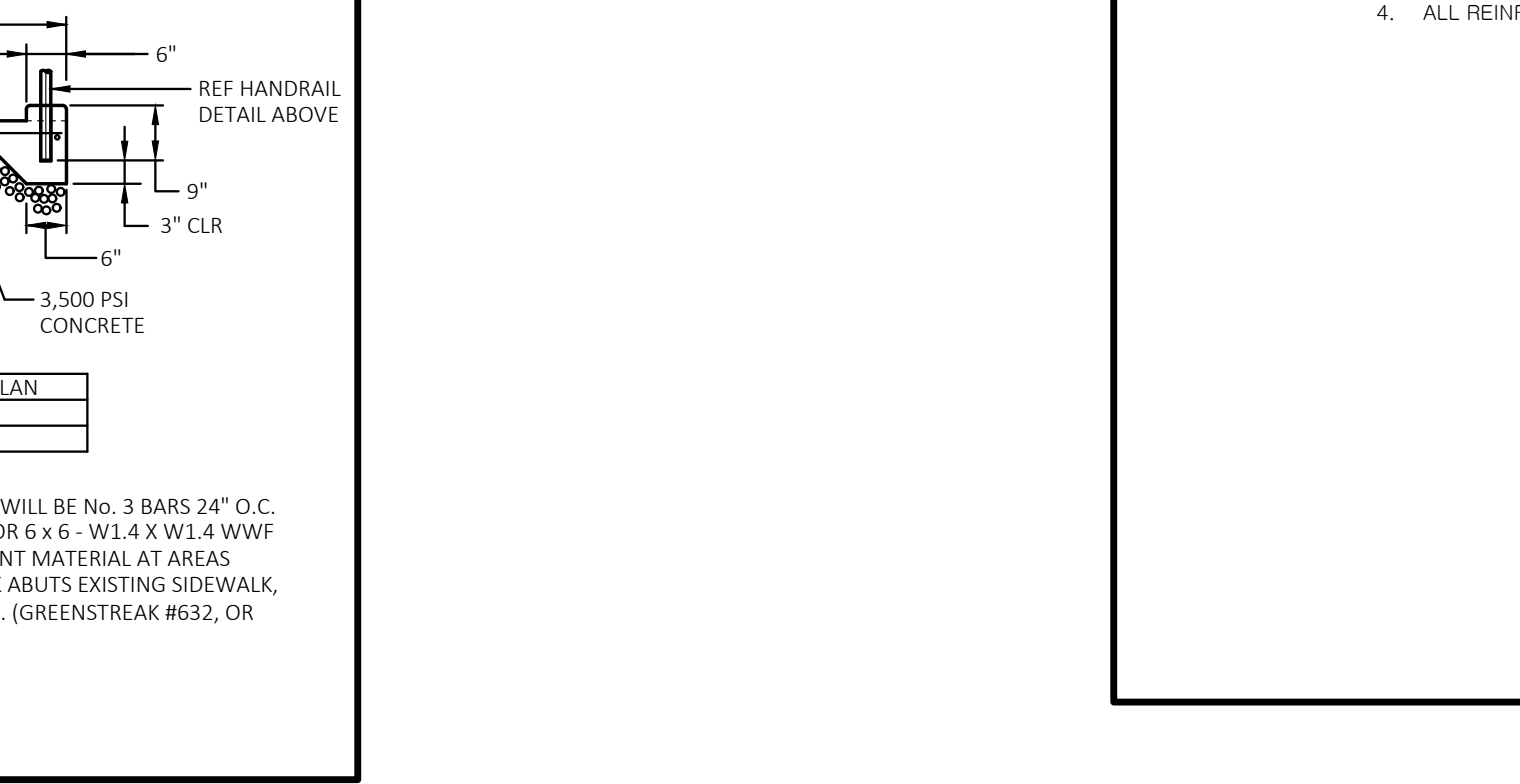
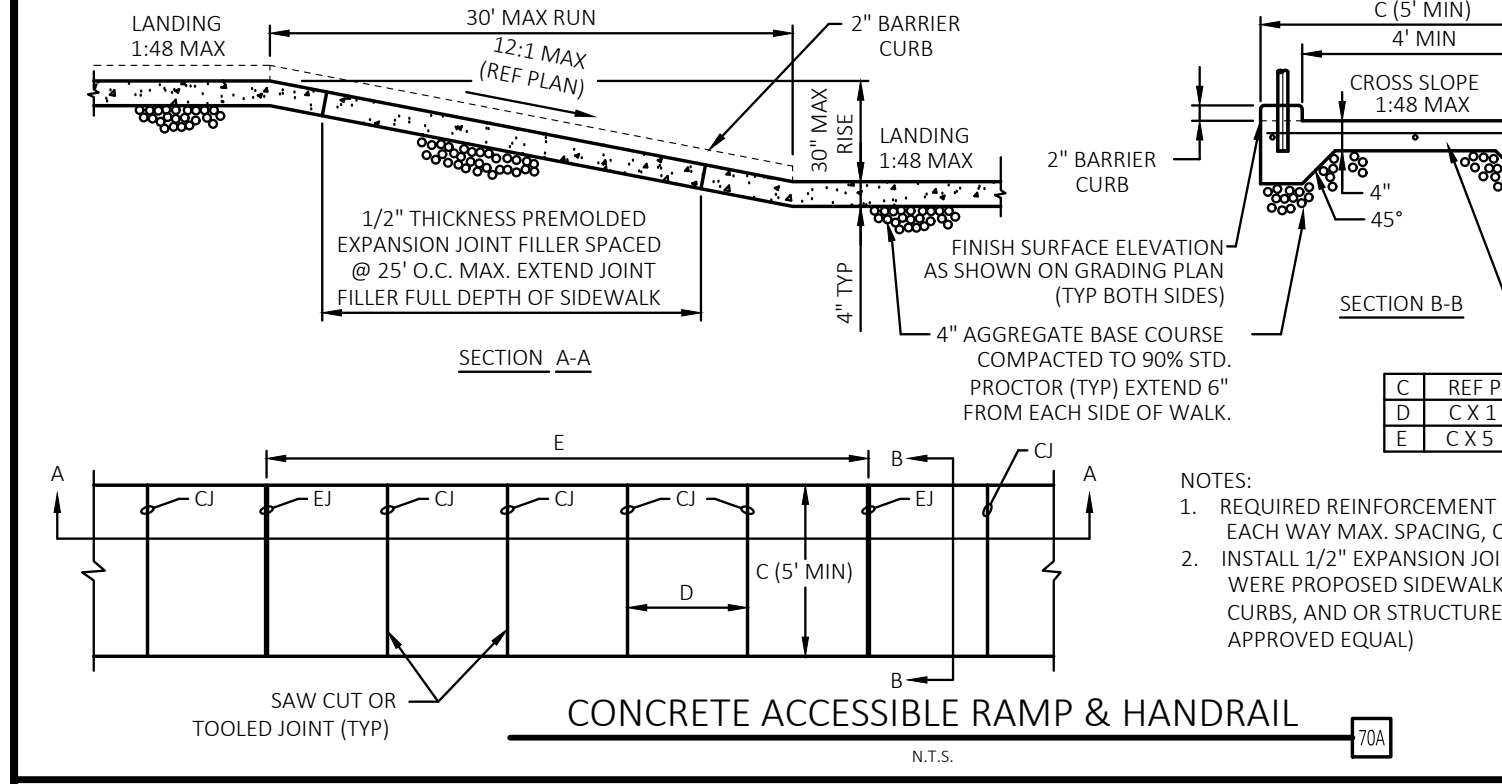
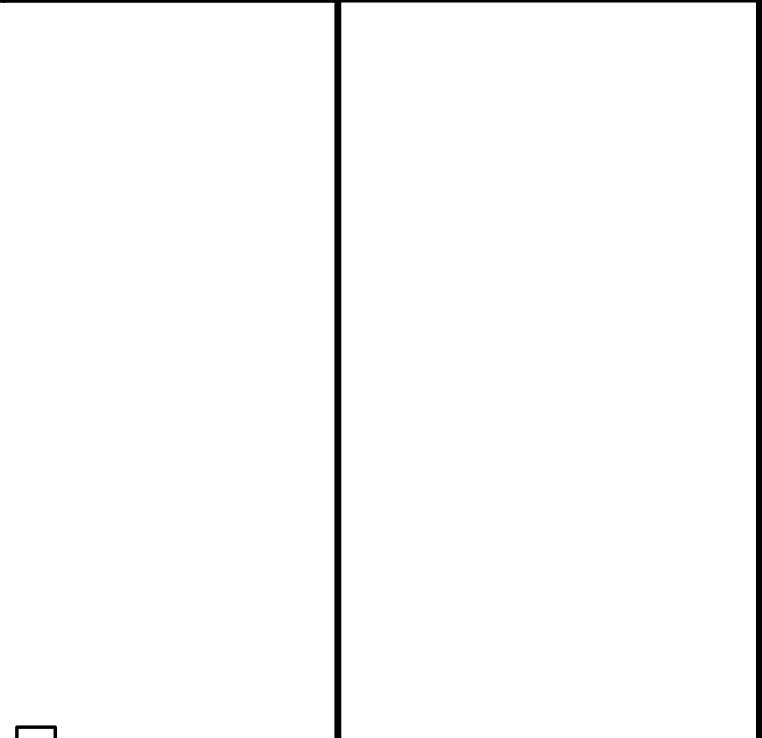
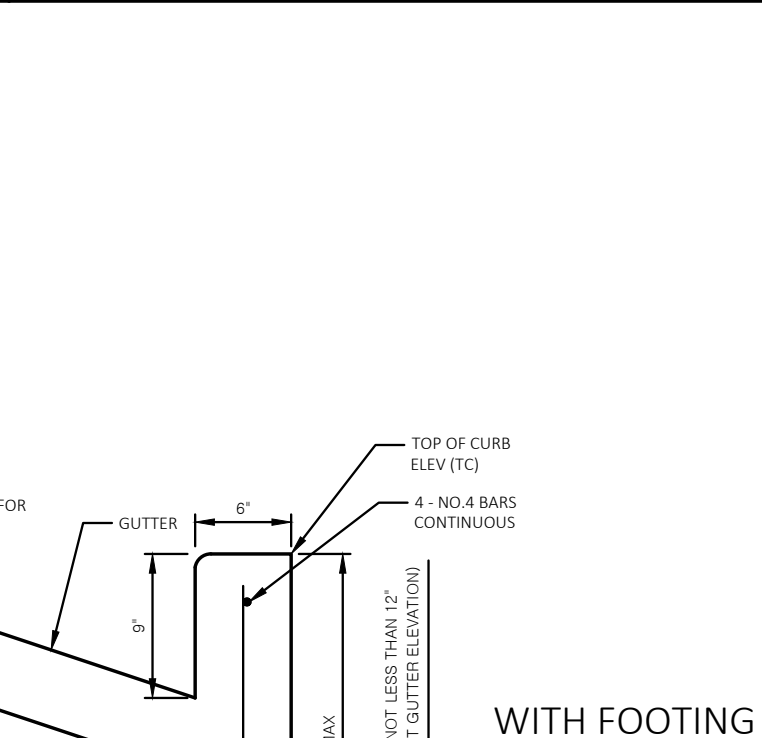
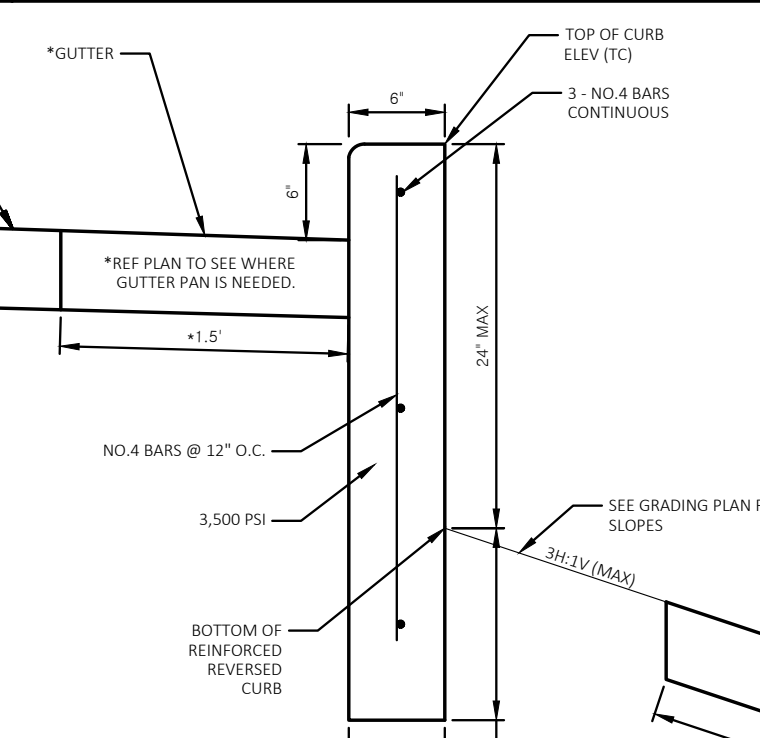
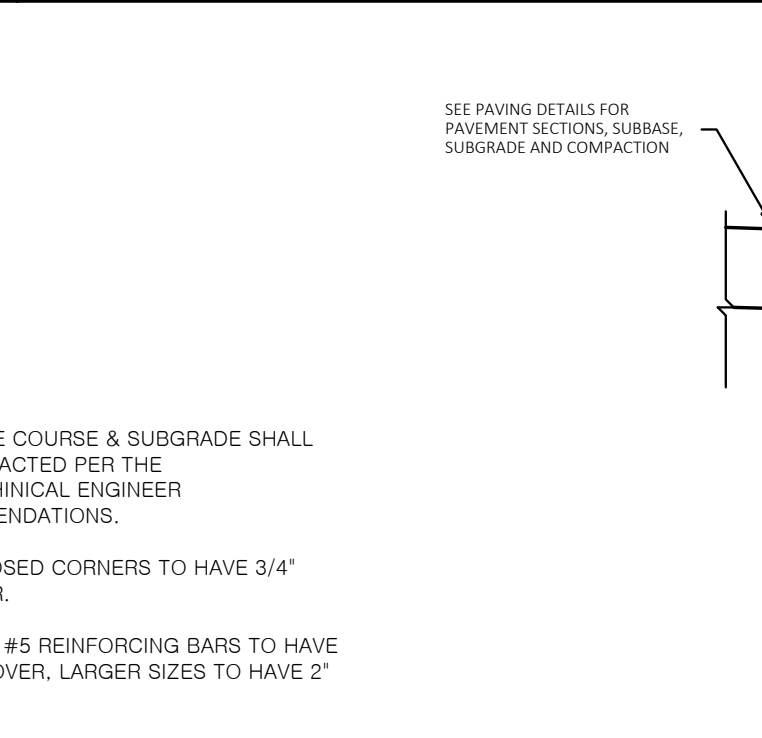
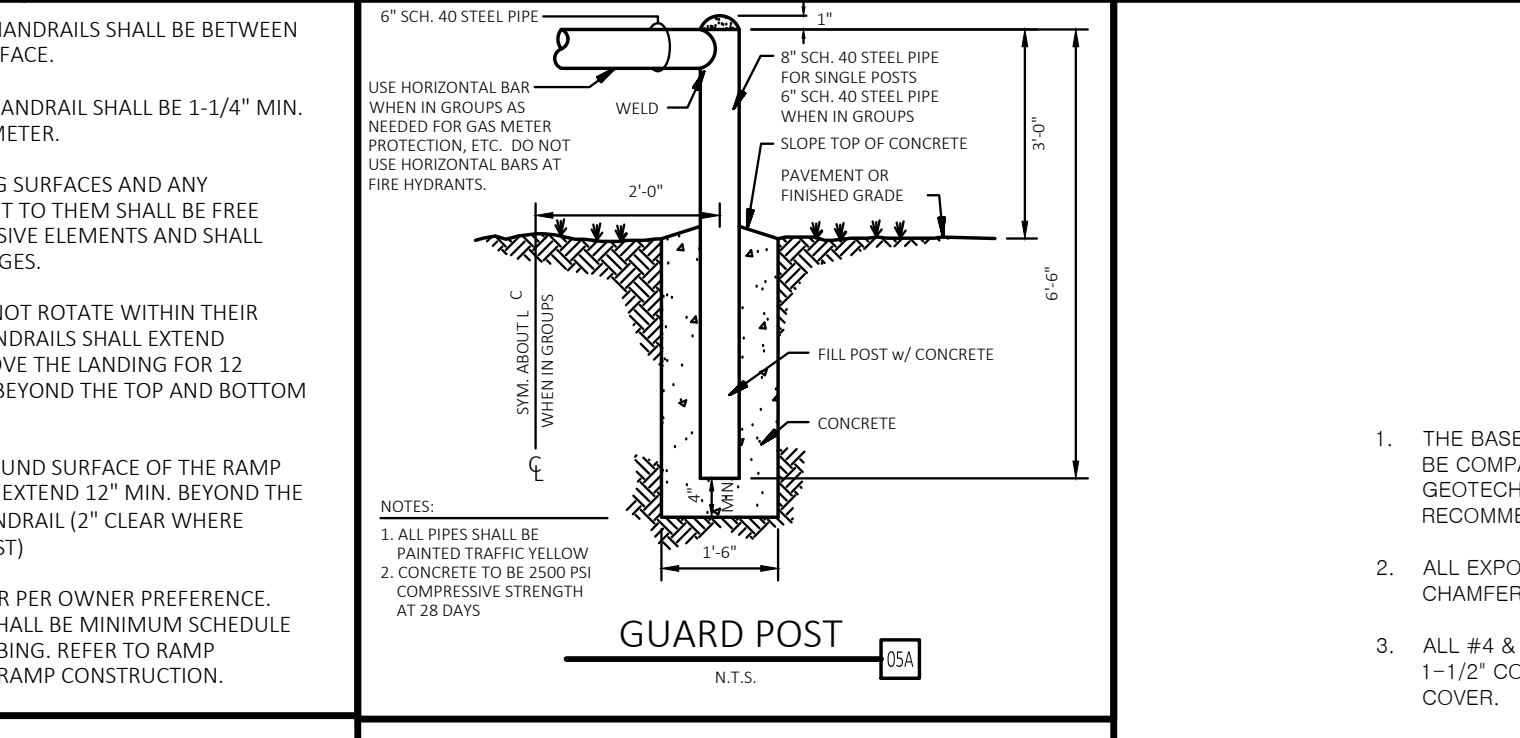
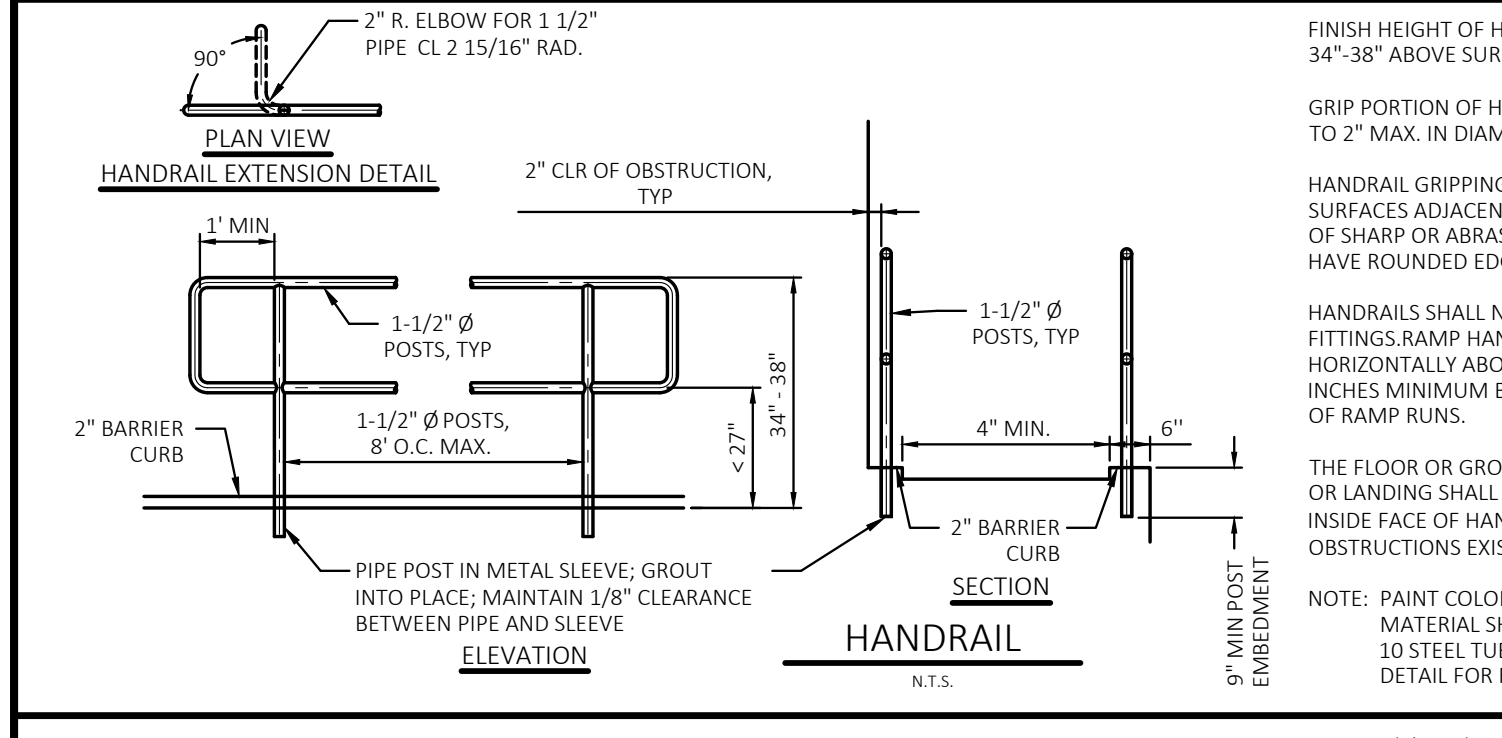
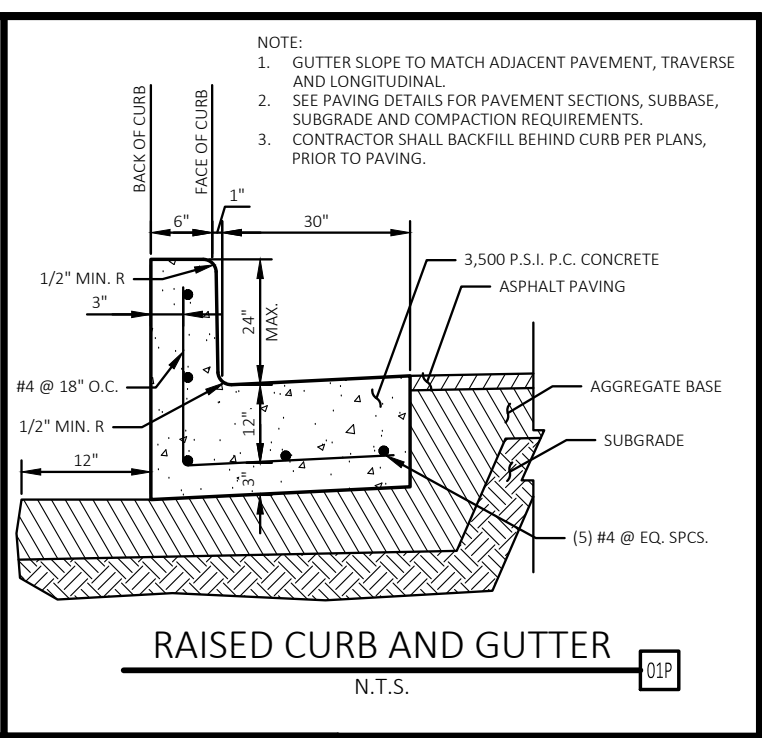
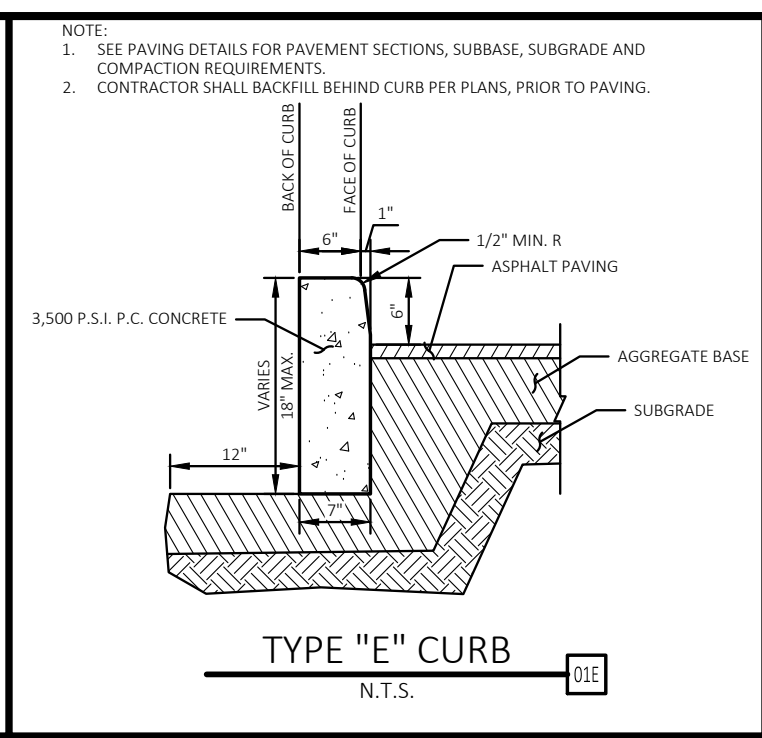
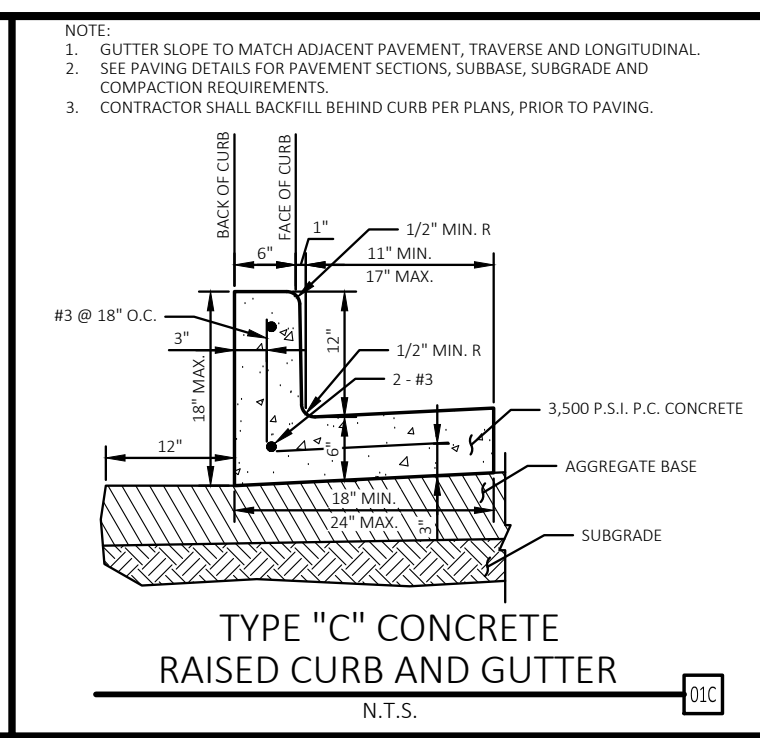
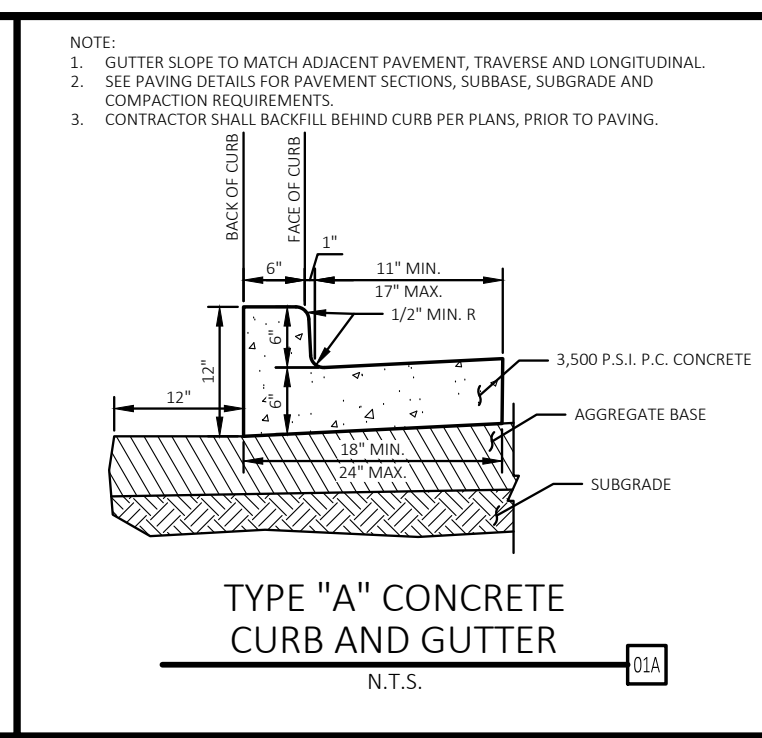
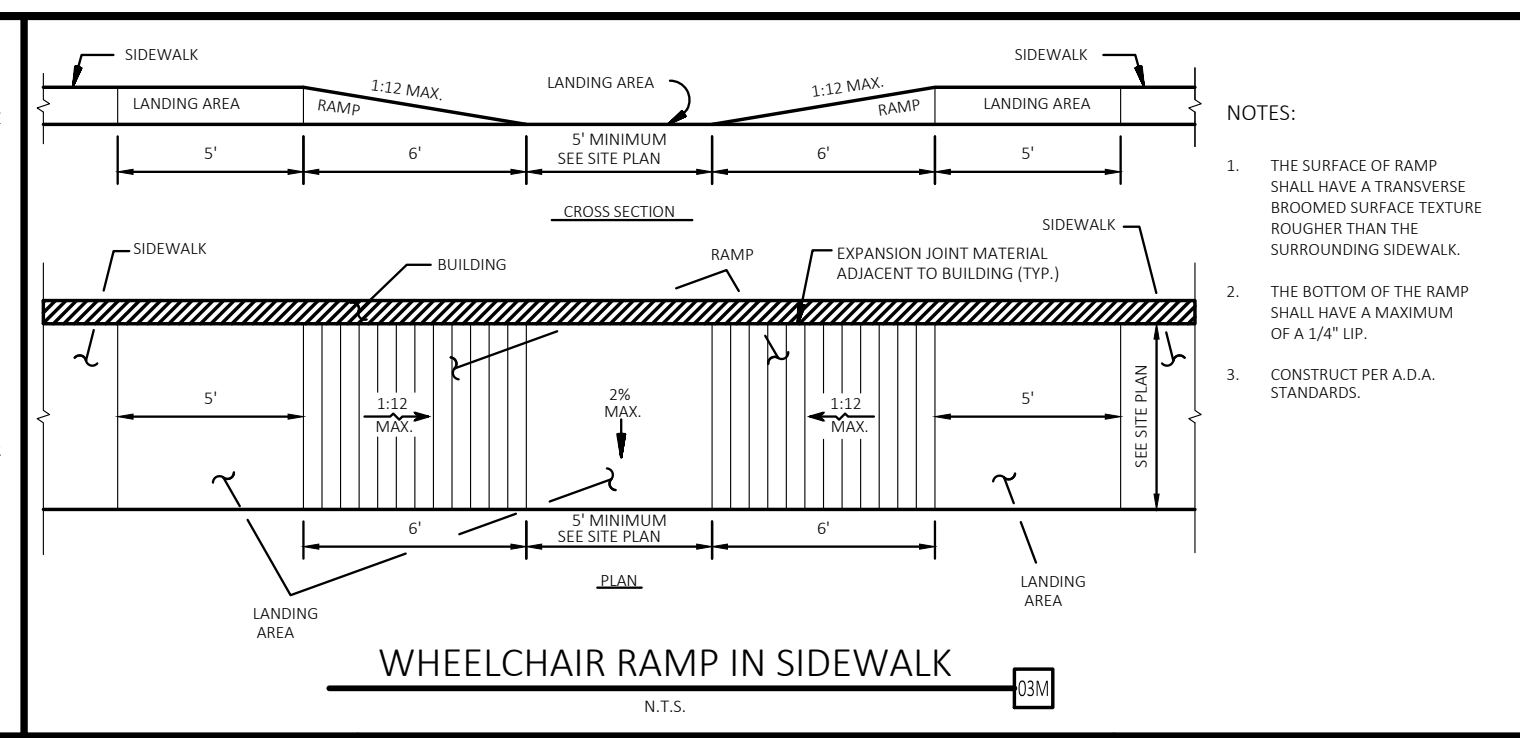
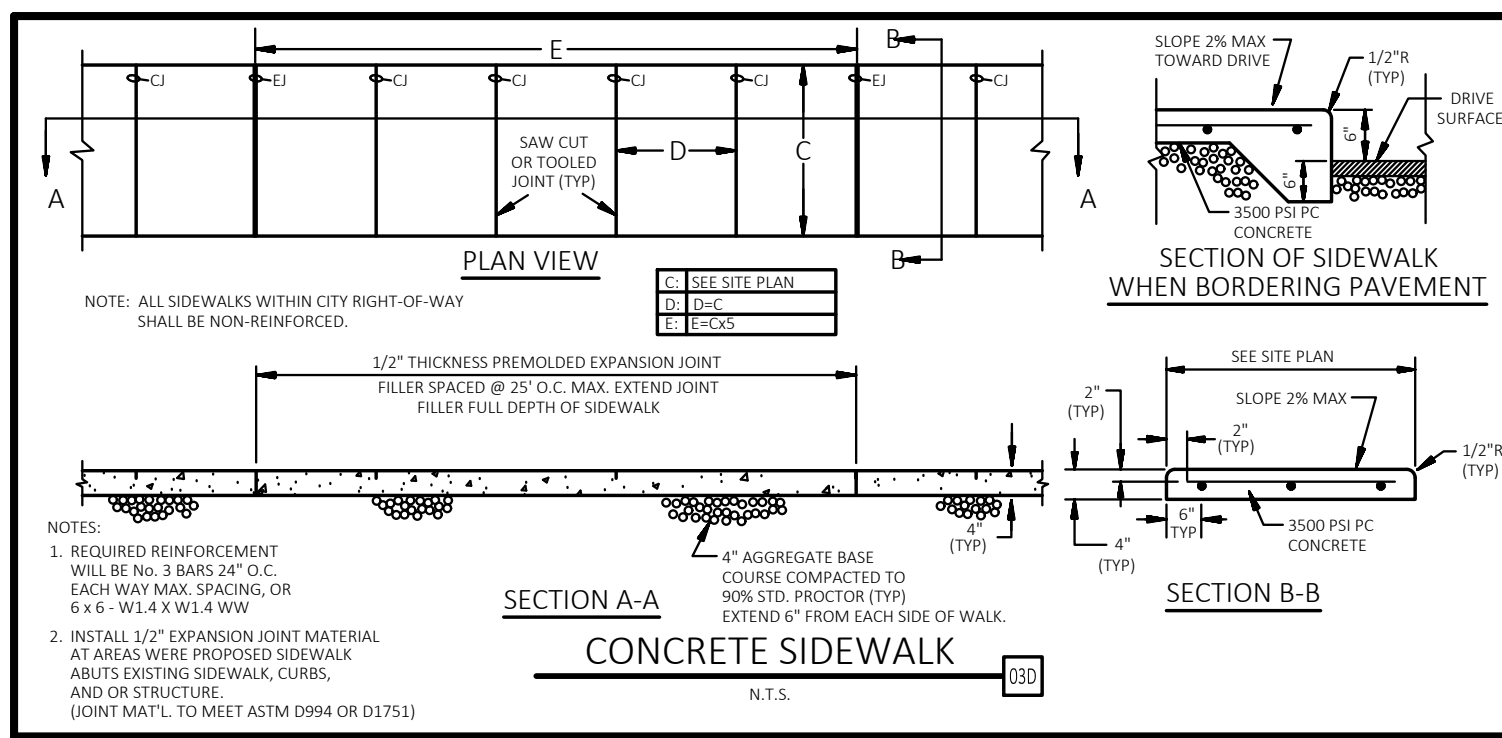
CONSTRUCTION DOCUMENTS



Table with 2 columns: Field Name and Value. Includes PROJECT MANAGER, DESIGNER, DATE, REVISION.

SITE PLAN SHEET TITLE SHEET NUMBER

C4



CEI ENGINEERING ASSOCIATES, INC.
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FAX: (559) 447-3129

3K1
CONSULTING SERVICES, LLC.
CLIENT
3K1 CONSULTING SERVICES, LLC.
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531



PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

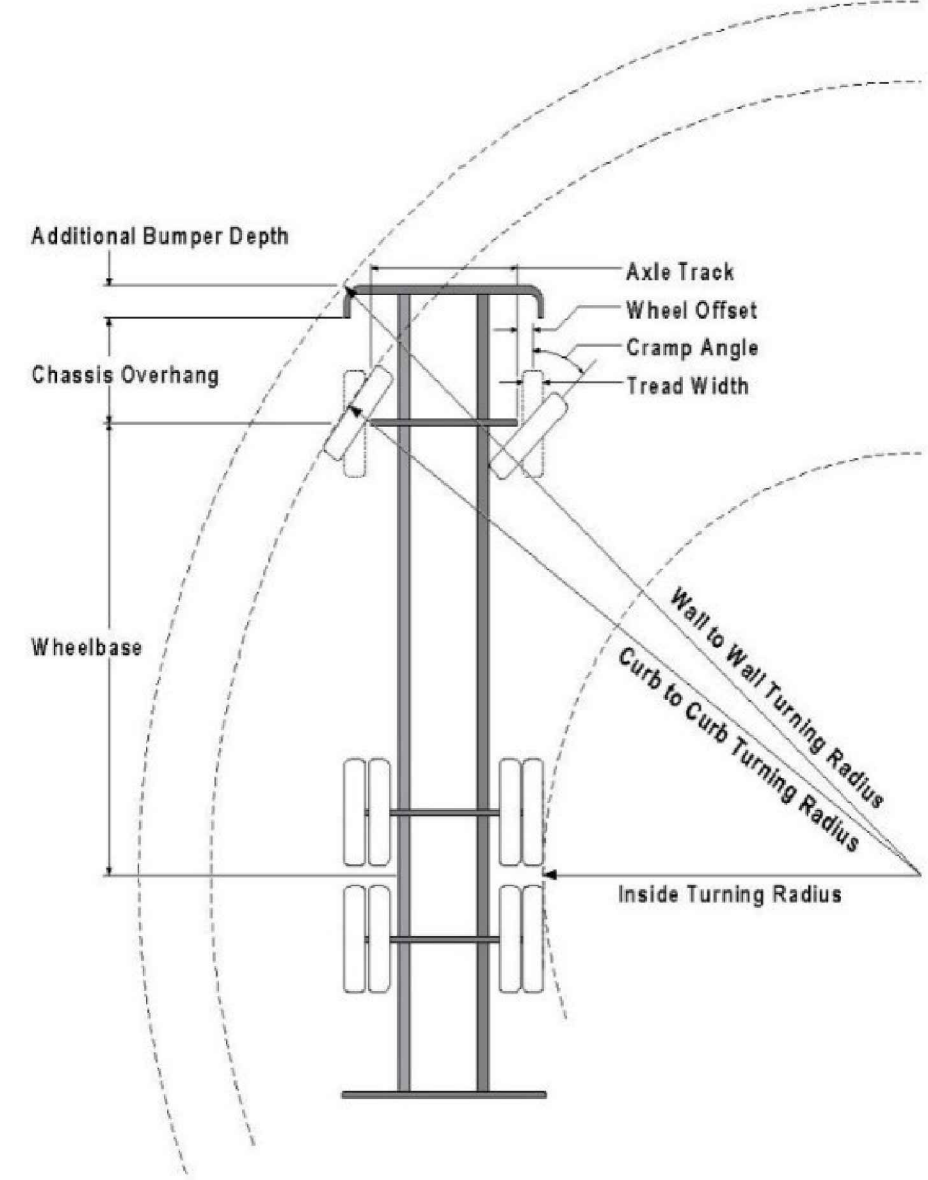
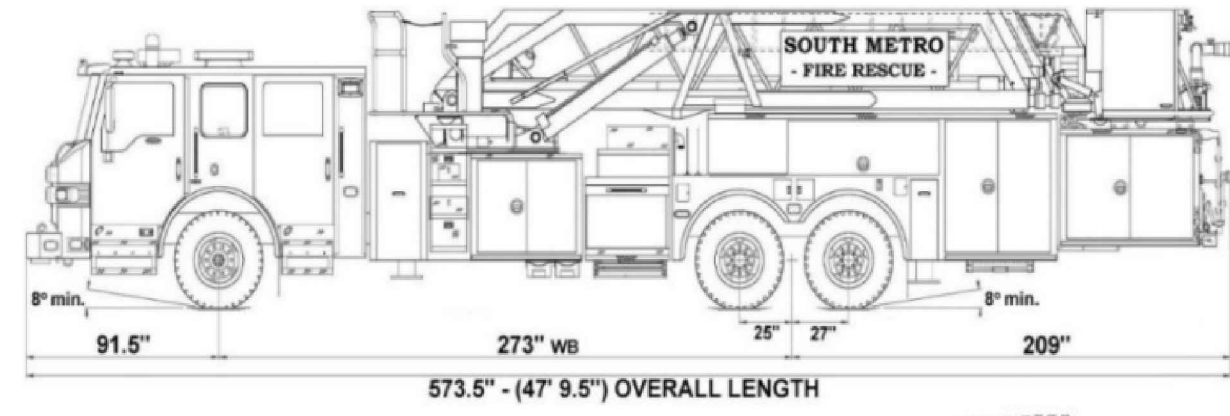
SITE DETAILS
SHEET TITLE
SHEET NUMBER

C4.1

DRAWING LOCATION - P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-01.DWG - SAVED BY - MACALDERON

FIRE ACCESS ROAD DESIGN CRITERIA

VEHICLE SPECIFICATIONS are provided for the largest apparatus in use by South Metro Fire Rescue. Fire Apparatus Access Roads shall be capable of accommodating this apparatus.



Vehicle Specifications

- Length: 47' 9.5"
- Width: 8' 5" - (10' 1" mirror to mirror)
- Height: 10' 9"
- Wheelbase: 273 in.
- Design load: 80,000 pounds
- Inside Cramp Angle: 40°
- Axle Track: 83"
- Wheel Offset: 5.3"
- Tread Width: 13.5"
- Turning Radii:**
- Inside Turn: 26 ft. 1 in.
- Curb to curb: 41 ft. 11 in.
- Wall to wall: 46 ft. 8 in.

DRAWING LOCATION - P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-01.DWG - SAVED BY - JACALDERON



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PHONE: (602) 850-8101



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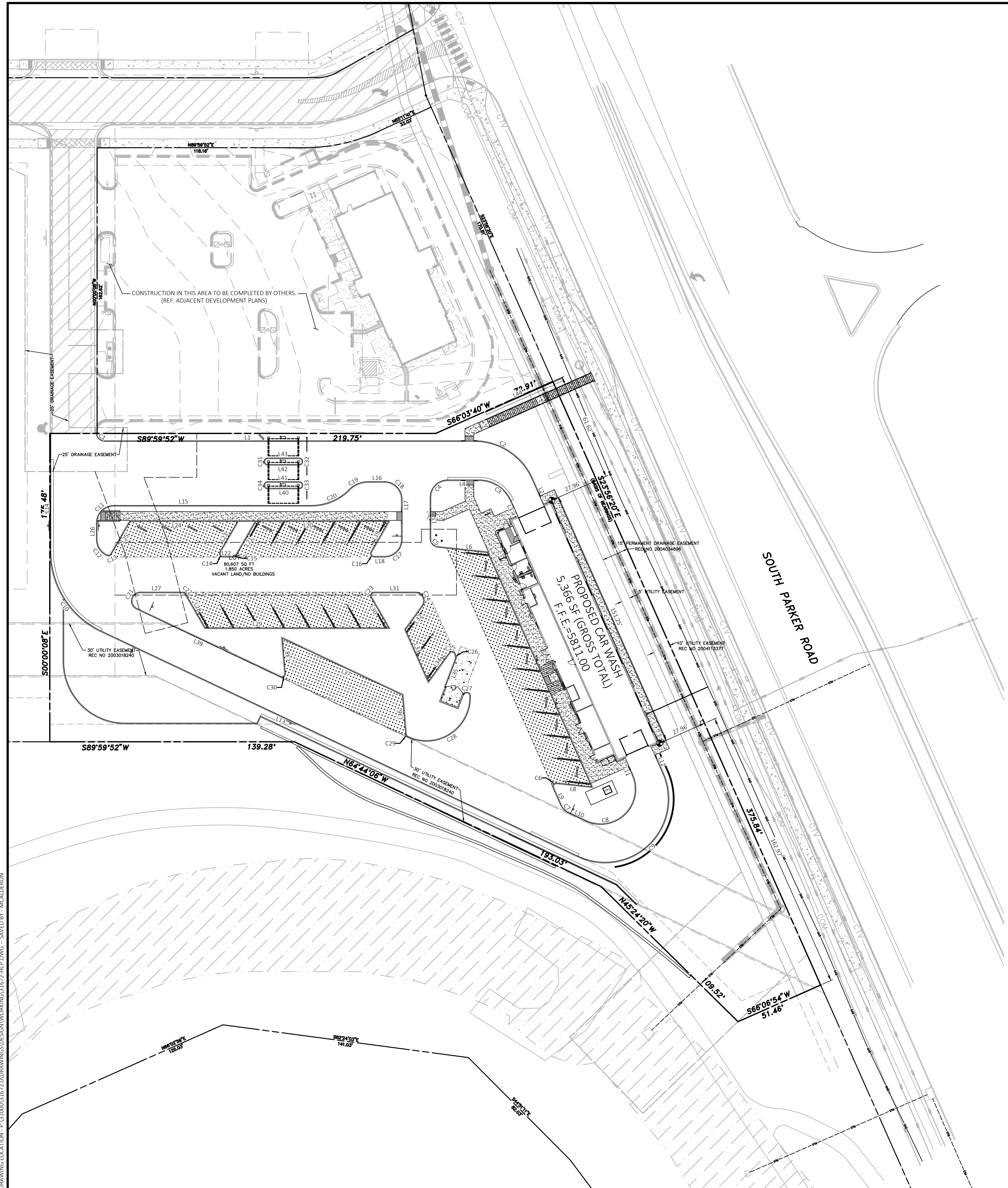
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PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

SITE DETAILS
SHEET TITLE
SHEET NUMBER

C4.2



Parcel Line Table		
Line #	Length	Direction
L3	9.75	N23° 56' 20.00"W
L4	18.11	S89° 59' 49.79"W
L5	23.29	S0° 00' 08.00"E
L6	28.49	S83° 56' 20.00"E
L7	145.88	S23° 56' 20.00"E
L8	20.02	N83° 56' 20.28"W
L9	10.20	S23° 56' 20.00"E
L10	10.49	S64° 44' 06.00"E
L11	10.22	N23° 56' 20.00"W
L12	16.59	S23° 04' 32.16"E
L13	300.26	N64° 44' 06.00"W
L14	67.26	N0° 00' 08.00"W
L42	18.00	S89° 59' 52.00"W
L43	18.00	N89° 59' 52.00"E
L40	18.00	S89° 59' 52.00"W
L41	18.00	N89° 59' 52.00"E
L15	112.45	N89° 59' 52.00"E
L16	9.42	N89° 59' 56.25"E
L17	22.84	S0° 00' 08.00"E
L18	5.86	S89° 59' 53.97"W
L19	20.02	N29° 59' 51.72"E
L20	75.63	S89° 59' 52.00"W
L21	22.33	S29° 59' 51.72"W
L22	5.77	S89° 59' 52.00"W
L23	20.02	N29° 59' 51.72"E
L24	61.20	S89° 59' 52.00"W
L25	21.51	S29° 59' 52.00"W
L26	9.34	N0° 00' 08.00"W
L27	24.10	N89° 59' 52.00"E
L28	22.33	S30° 00' 08.00"E
L29	104.50	N89° 59' 52.00"E
L30	20.02	N30° 00' 07.72"W
L31	23.43	N89° 59' 52.00"E
L32	16.51	S36° 03' 40.00"W
L33	32.33	S23° 56' 20.00"E
L34	20.02	N36° 03' 40.28"E
L35	16.45	S21° 03' 40.00"W
L36	22.33	N4° 44' 06.01"W
L37	75.63	N64° 44' 06.00"W
L38	20.02	S4° 44' 05.72"E
L39	90.51	N64° 44' 06.00"W
L44	47.99	N66° 03' 40.00"E
L1	209.91	N89° 59' 57.01"E
L2	16.28	S23° 56' 20.00"E

Curve Table			
Curve #	Length	Radius	Delta
C3	23.06	20.00	66.06
C4	15.71	10.00	90.00
C5	7.32	5.00	83.94
C6	4.19	2.00	120.00
C7	7.12	10.00	40.80
C8	48.59	20.00	139.20
C9	87.46	36.00	139.20
C10	56.49	50.00	64.73
C31	6.28	2.00	180.00
C32	6.28	2.00	180.00
C33	6.28	2.00	180.00
C34	6.28	2.00	180.00
C11	15.71	10.00	90.00
C12	11.46	10.00	65.69
C13	2.94	2.00	84.31
C14	4.19	2.00	120.00
C15	2.09	2.00	59.99
C16	4.19	2.00	119.99
C17	15.71	10.00	90.00
C18	15.71	10.00	90.00
C19	11.93	15.00	45.57
C20	23.86	30.00	45.58
C21	13.50	5.00	154.74
C22	2.09	2.00	60.00
C23	4.19	2.00	120.00
C24	11.00	5.00	126.06
C25	4.19	2.00	120.00
C26	3.93	5.00	45.00
C27	5.48	2.00	156.93
C28	51.17	25.00	117.28
C29	2.09	2.00	60.00
C30	4.19	2.00	120.00
C1	7.85	5.00	90.00
C2	41.80	36.00	66.52

Know what's below.
Call before you dig.

EXISTING LEGEND

- PROPERTY LINE/RIGHT OF WAY LINE
- ⊙ COMMUNICATIONS MANHOLE
- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM SEWER MANHOLE
- GAS UNDERGROUND GAS LINES
- COM UNDERGROUND COMMUNICATIONS LINES
- STM UNDERGROUND STORM SEWER LINES

PROPOSED

- PROPERTY LINE/RIGHT OF WAY LINE
- CONCRETE CURB AND GUTTER. SEE DETAIL 01A/01B.
- BUILDING CONTROL POINT

BENCHMARK

DOUGLAS CONTROL MONUMENT #1.095035, A 3" ALUMINUM CAP.
ELEVATION = 5906.34 FEET (NAVD1988). AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS

THE BEARINGS ARE BASED ON THE WEST ROW LINE OF SOUTH PARKER ROAD ASSUMED TO BEAR S23°56'20"E BETWEEN MONUMENTS FOUND AND DESCRIBED HEREON.

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Tom
Town of Parker, Director of Engineering
Date: 06/26/2023



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REVISION

NO.	DESCRIPTION	DATE
REV-X		XX/XX/XXXX

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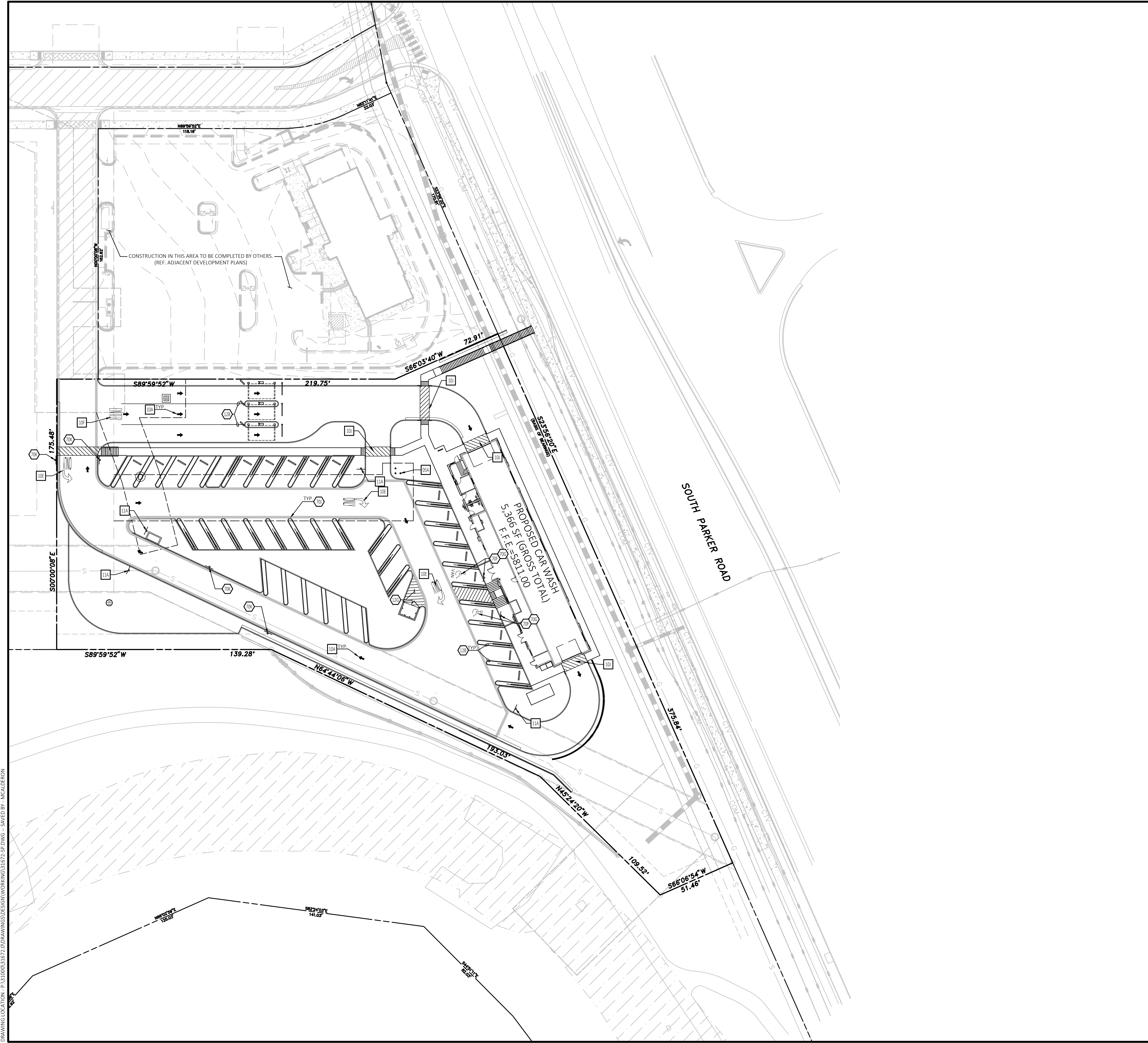
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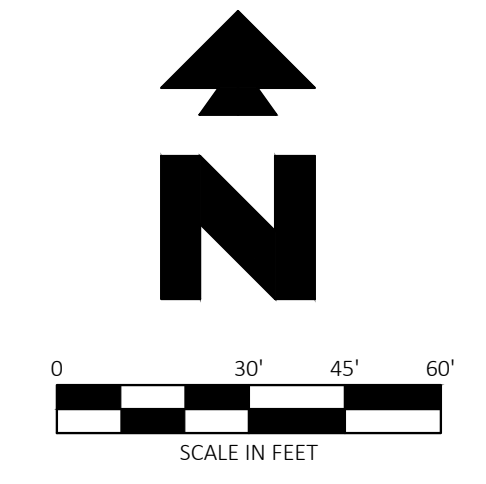
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PROJECT MANAGER: ASD
DESIGNER: RM
CEI PROJECT NUMBER: 31672
DATE: 3/23/2023
REVISION: REV-6

HORIZONTAL
CONTROL PLAN
SHEET TITLE
SHEET NUMBER

C4.3



Know what's below.
Call before you dig.



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- ⊙ STORM SEWER MANHOLE
- GAS — UNDERGROUND GAS LINES
- COM — UNDERGROUND COMMUNICATIONS LINES
- STM — UNDERGROUND STORM SEWER LINES

GENERAL SITE NOTES

- A. UNLESS OTHERWISE SHOWN, CALLED OUT OR SPECIFIED HEREON OR WITHIN THE CONTRACT REQUIREMENTS:
 ALL PARKING LOT SIGN BASE SUPPORTS (EXCLUDING ACCESSIBLE PARKING SIGNAGE) SHALL BE INSTALLED PER DETAIL 12F.
 ALL PARKING LOT STRIPING SHALL BE PAINTED WITH 4 INCH YELLOW PAINT (TYP.).
 ACCESSIBLE PARKING STALL STRIPING AND ISA SYMBOL SHALL BE PAINTED PER ARCH PLANS.

SIGNAGE AND STRIPING NOTES

- 12B 4 INCH TRAFFIC YELLOW ALIGNMENT STRIPES (TYP.)
- 12G 4 INCH-WIDE PAINTED WHITE STRIPES, 2.0" O.C. @ 45°
- 70F "UNAUTHORIZED VEHICLES" WARNING SIGN (PER ARCH. PLANS)
- 70G ACCESSIBLE PARKING OFF-STREET (PER ARCH. PLANS)
- 70J FIRE LANE MARKING PER TOWN OF PARKER STANDARDS
- 70K NO PARKING FIRE LANE SIGN PER TOWN OF PARKER STANDARDS

SIGNAGE AND STRIPING DETAILS (REF DETAIL SHEET C4.5)

- 10A TRAFFIC FLOW ARROW
- 10E RIGHT/LEFT TURN ONLY PAVEMENT MARKING
- 10F "ENTER" PAVEMENT MARKING W/ARROW
- 10I CROSSING MARKING
- 11A "DO NOT ENTER" SIGN

LEGAL DESCRIPTION

REFERENCE C1 - COVER SHEET FOR LEGAL DESCRIPTION.

BENCHMARK

DOUGLAS CONTROL MONUMENT #1.095035. A 3 1/2" ALUMINUM CAP.
 ELEVATION = 5906.34 FEET (NAVD1988). AS PUBLISHED BY DOUGLAS COUNTY

BASIS OF BEARINGS

THE BEARINGS ARE BASED ON THE WEST ROW LINE OF SOUTH PARKER ROAD ASSUMED TO BEAR S23°56'20"E BETWEEN MONUMENTS FOUND AND DESCRIBED HEREON.

FLOOD ZONE INFORMATION

SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM.
 MAP NUMBER: 08035C0067G
 EFFECTIVE DATE: MARCH 16, 2016

NOTE:

SEE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF PORCHES, RAMPS, VESTIBLES, SLOPED PAVING, TRUCK DOCKS, BUILDING UTILITY ENTRANCE LOCATIONS AND PRECISE BUILDING DIMENSIONS.

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

The Town of Parker review constitutes general compliance with the Town's Standards and approved variances, subject to these plans being stamped, signed, and dated by the professional engineer of record. Review by the Town does not constitute approval of the plan design or accuracy and correctness of engineering calculations. Errors in the design or calculations remain the responsibility of the registered professional engineer whose stamp and signature are affixed to this document.

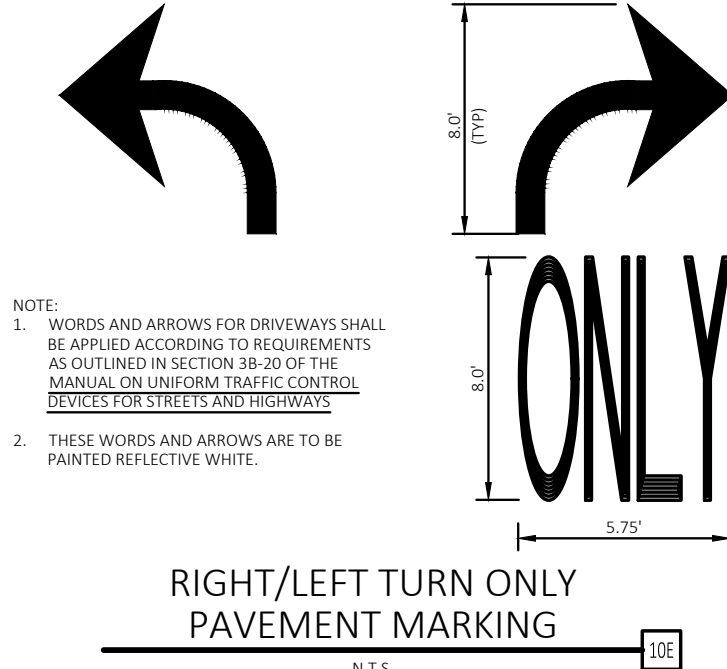
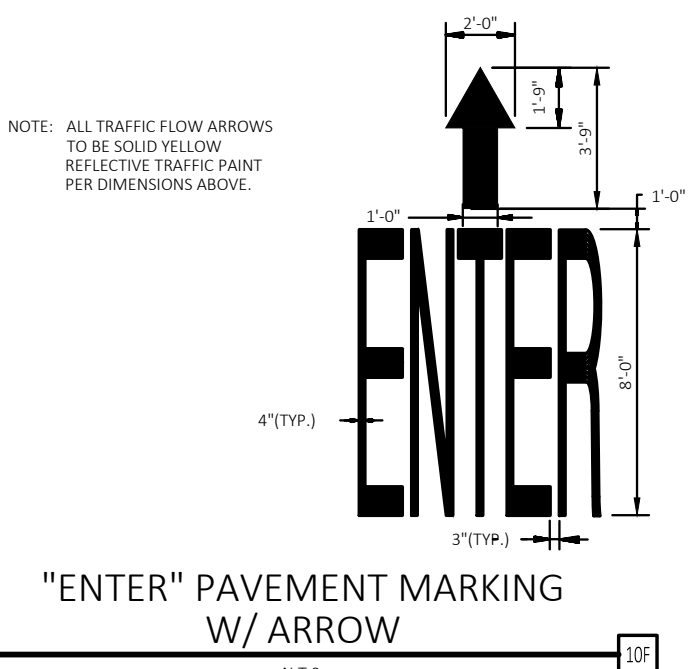
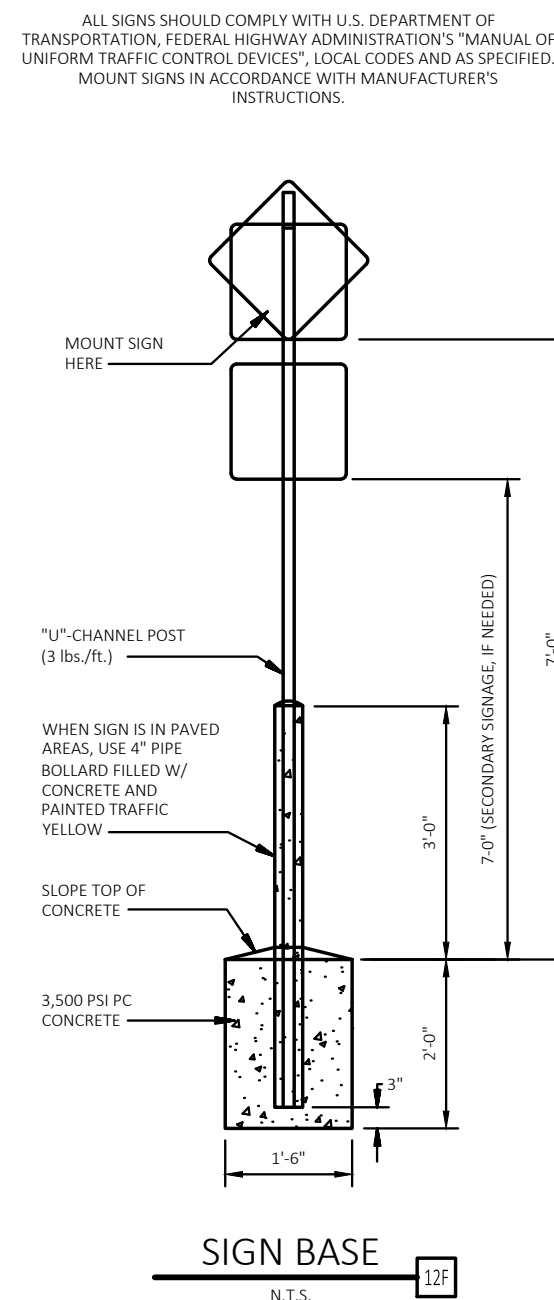
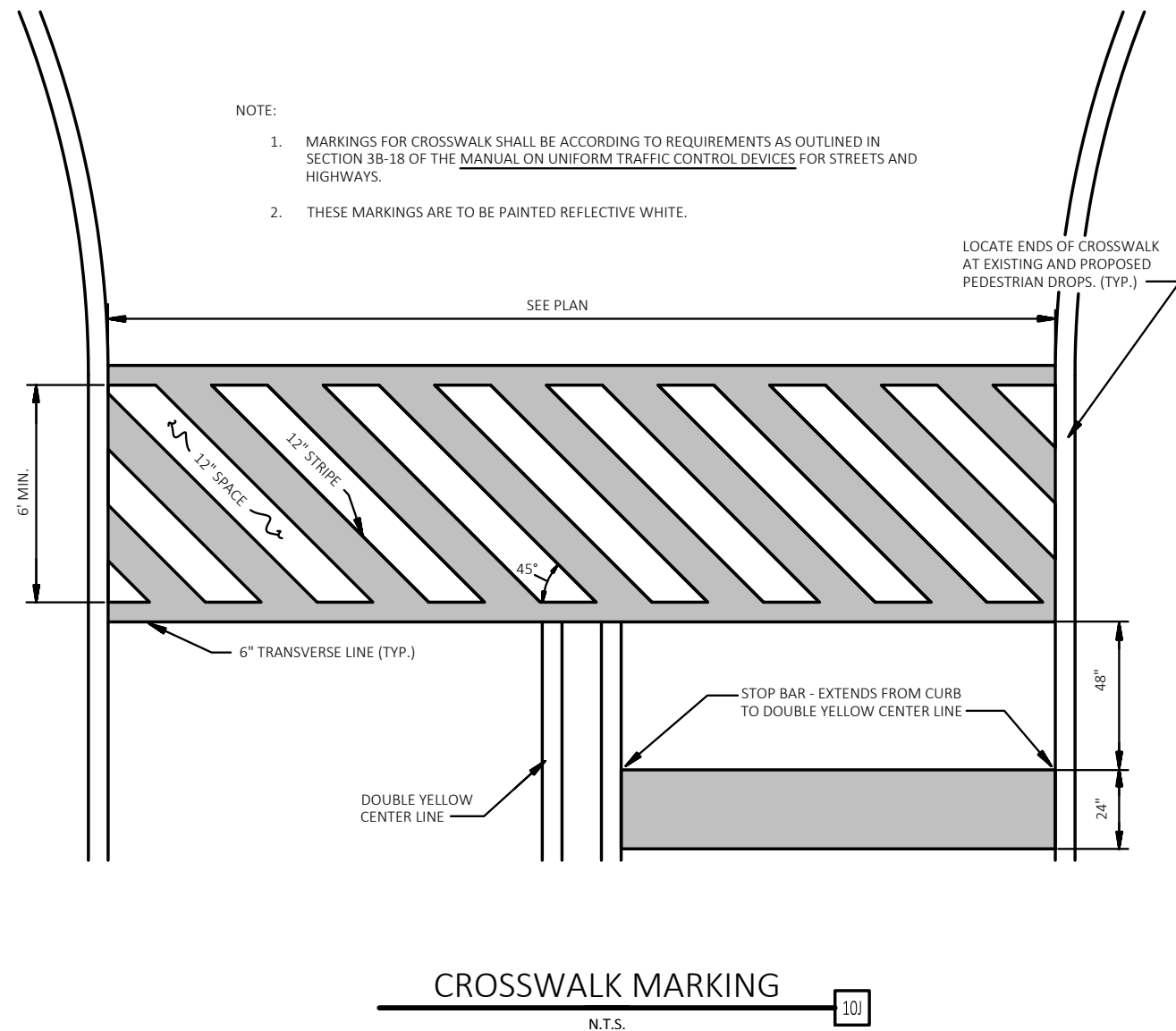
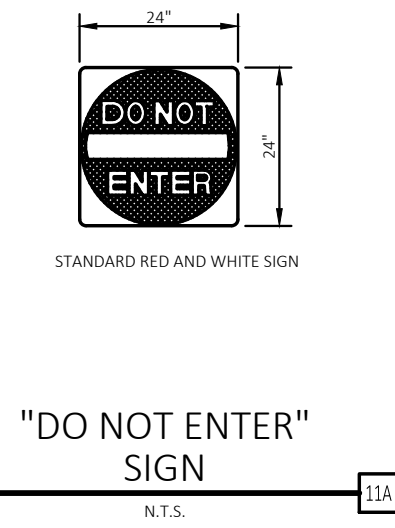
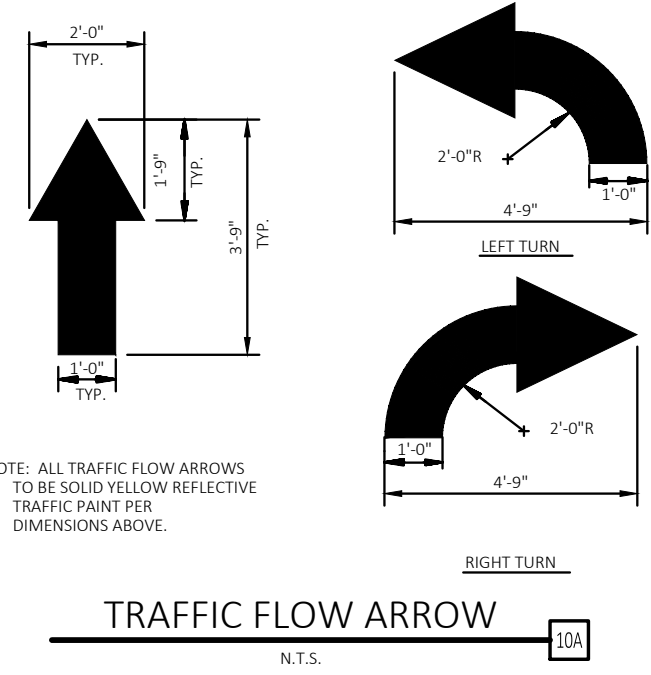
This review does not constitute approval of any private on-site improvements which may be shown. Construction cannot commence until all required drainage/traffic reports, final development plan(s), special review(s), grading permit, and/or other permits are complete, approved and on file with the Town of Parker.

Thomas A. Durkin
Town of Parker, Director of Engineering
Date: 06/26/2023

SIGNAGE AND STRIPING PLAN
SHEET TITLE
SHEET NUMBER

C4.4

DRAWING LOCATION: P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-SP.DWG - SAVED BY: MACALDERON
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11811 N. TATUM BOULEVARD,
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PHONE: (602) 850-8101



PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

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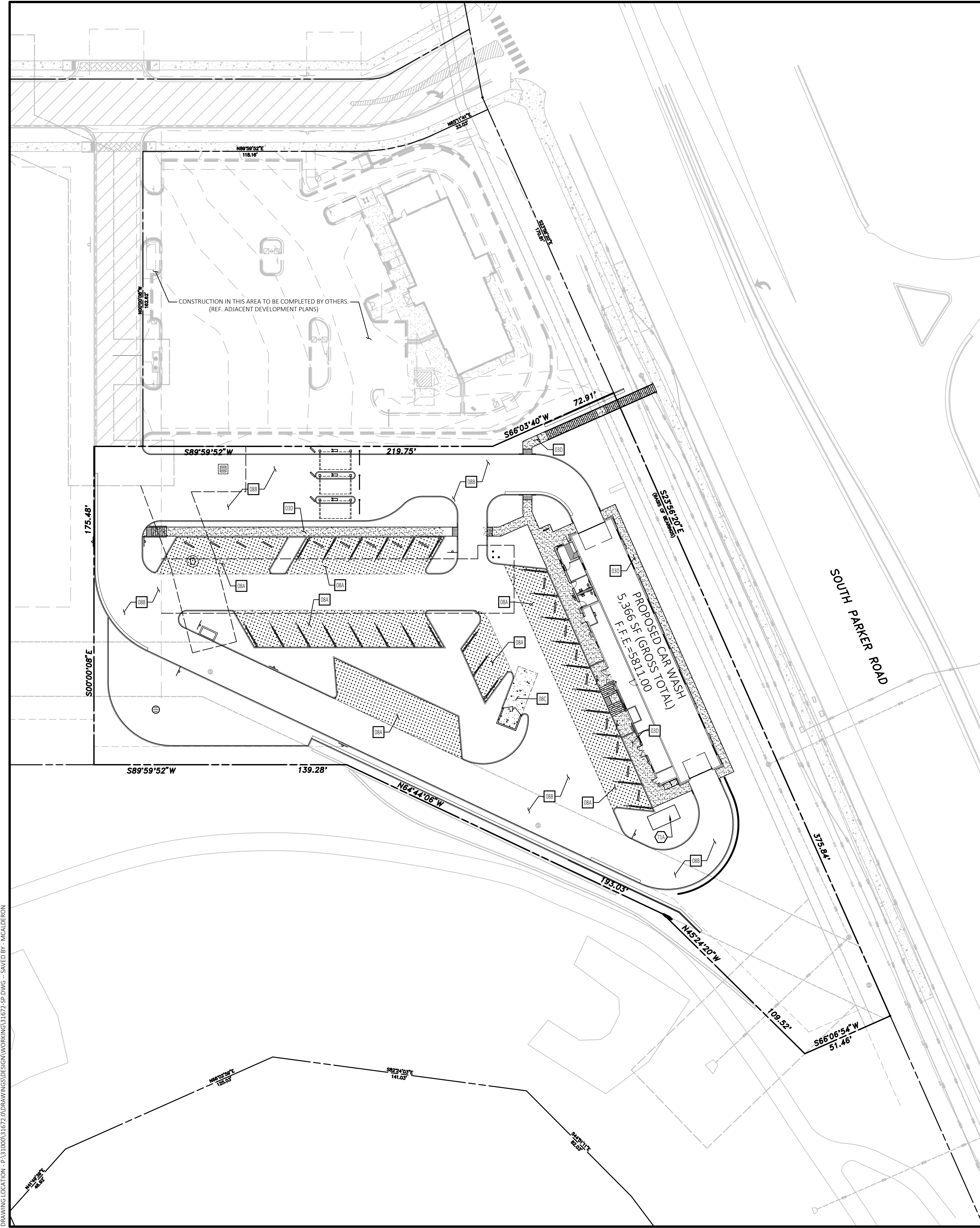
QUICK N CLEAN
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SIGNAGE AND STRIPING PLAN
DETAILS
SHEET TITLE
SHEET NUMBER

C4.5



LEGAL DESCRIPTION
 REFERENCE C1 - COVER SHEET FOR LEGAL DESCRIPTION.

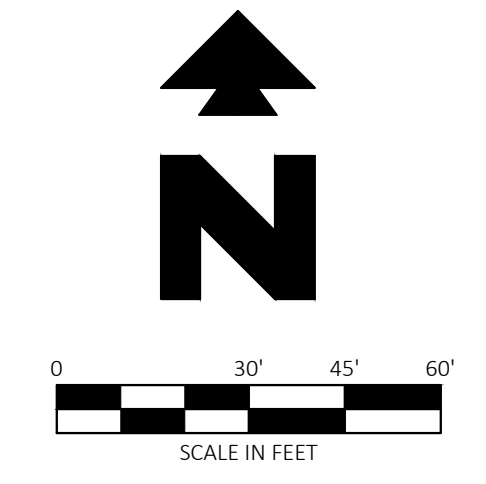
BENCHMARK
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Know what's below.
 Call before you dig.



- EXISTING LEGEND**
- P PROPERTY LINE/RIGHT OF WAY LINE
 - ⊙ COMMUNICATIONS MANHOLE
 - ⊙ SANITARY SEWER MANHOLE
 - ⊙ STORM SEWER MANHOLE
 - GAS UNDERGROUND GAS LINES
 - COM UNDERGROUND COMMUNICATIONS LINES
 - STM UNDERGROUND STORM SEWER LINES

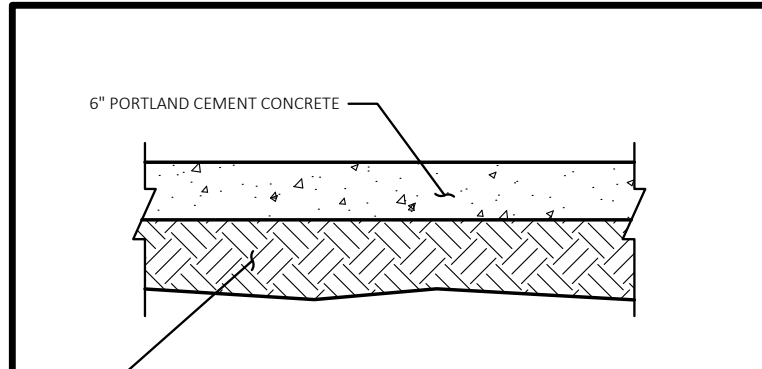
- PROPOSED LEGEND**
- CONCRETE CURB AND GUTTER.
 - ▨ LIMITS OF CONCRETE SIDEWALK PER DETAIL 03D
 - ▨ LIMITS OF HEAVY DUTY CONCRETE PER DETAIL 08C
 - ▨ LIMITS OF STANDARD DUTY ASPHALT PER DETAIL 08A
 - ▨ LIMITS OF HEAVY DUTY ASPHALT PER DETAIL 08B

GENERAL SITE NOTES

- A. ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND D.S.H.A. STANDARDS.
- B. PRIOR TO CONSTRUCTION WITHIN ANY EXISTING PUBLIC RIGHT-OF-WAY, THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS FROM THE AUTHORITY HAVING JURISDICTION.
- C. CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- D. ALL DISTURBED AREAS ARE TO RECEIVE FOUR INCHES OF TOPSOIL, SEED, MULCH AND WATER UNTIL A HEALTHY STAND OF GRASS IS ESTABLISHED.
- E. ALL CURBED RADII ARE TO BE 2' OR 10' UNLESS OTHERWISE NOTED. STRIPED RADII ARE TO BE 5'.
- F. ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- G. EXISTING STRUCTURES TO REMAIN WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED / ADJUSTED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID.
- H. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, (UNLESS OTHERWISE NOTED ON PLANS) INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID.
- I. SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY A LAND SURVEYOR.
- J. THE SITE WORK FOR THIS PROJECT SHALL MEET OR EXCEED THE PROJECT CONTRACT REQUIREMENTS.
- K. REFER TO ARCH. PLANS FOR PYLON AND/OR MONUMENT SIGNS.
- L. REFER TO ARCH. PLANS FOR SITE LIGHTING ELECTRICAL PLAN.
- M. UNLESS OTHERWISE SHOWN, CALLED OUT OR SPECIFIED HEREON OR WITHIN THE CONTRACT REQUIREMENTS:
 UNLESS OTHERWISE NOTED, PAVEMENT SHALL BE INSTALLED IN ACCORDANCE WITH DETAIL 08A OVER THE ENTIRE PARKING LOT AREA.
 UNLESS OTHERWISE NOTED, ALL CONCRETE PAVING SHALL INCLUDE CONCRETE JOINTS
- N. ALL ACCESSIBLE STALLS SHALL BE MARKED WITH THE INTERNATIONAL SYMBOL OF ACCESSIBILITY AND A WARNING THAT VEHICLES IN VIOLATION OF THE LOCAL MUNICIPAL CODE SHALL BE TOWED AWAY. THE INTERNATIONAL SYMBOL AND TOW-AWAY WARNING SHALL BE POSTED CONSPICUOUSLY ON SEVEN-FOOT POLES.
- O. ALL ACCESSIBLE PARKING STALLS SHALL BE PLACED ADJACENT TO FACILITY ACCESS RAMPS OR IN STRATEGIC AREAS WHERE THE OPERATOR SHALL NOT HAVE TO WHEEL OR WALK BEHIND PARKED VEHICLES WHILE TRAVELING TO OR FROM ACCESSIBLE PARKING STALLS AND RAMPS.
- P. MAXIMUM SLOPE IN ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2% (1:48) IN ANY DIRECTION.
- Q. ANY FACILITIES THAT ARE TO REMAIN AND ARE DAMAGED BY THE CONTRACTOR AS A RESULT OF CONSTRUCTION, SHALL BE REPLACED PER CURRENT CONTRACT REQUIREMENTS AT THE SOLE EXPENSE OF THE CONTRACTOR.
- R. REFER TO DETAIL 08R FOR CONCRETE JOINT DETAILS.
- S. CONTRACTOR SHALL ENSURE ADA CONFORMANCE OF ALL EXISTING SIDEWALKS AND CURB RAMPS WITHIN PUBLIC RIGHT-OF-WAY WITHIN THE PROJECT LIMITS. ALL EXISTING SIDEWALKS AND CURB RAMPS SHALL CONFORM TO THE LATEST ADOPTED VERSION (AND ALL AMENDMENTS) OF THE ADA STANDARDS FOR ACCESSIBLE DESIGN & THE ARCHITECTURAL BARRIERS TEXAS ACCESSIBILITY STANDARDS (TAS). ANY EXISTING SIDEWALK THAT IS NOT IN CONFORMANCE WITH ADA REGULATIONS SHALL BE REMOVED AND REPLACED PER CITY OF DALLAS CODES AND COST SHALL BE INCLUDED IN BASE BID.
- T. ALL ADA SIGNS SHALL BE PERMITTED SEPARATELY.

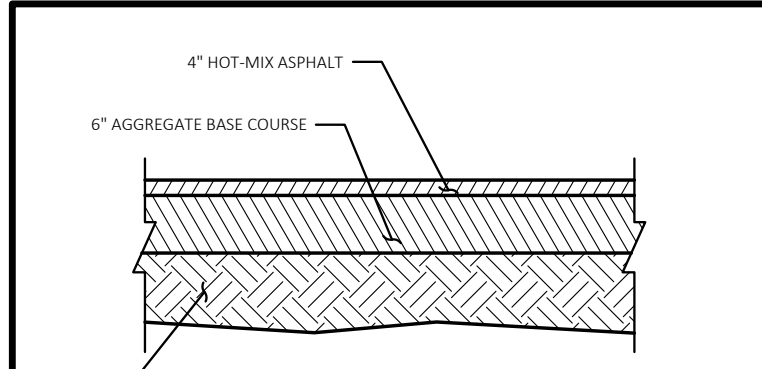
- PAVING DETAILS**
- 03D CONCRETE SIDEWALK
 - 08A STANDARD DUTY ASPHALT PAVING
 - 08B HEAVY DUTY ASPHALT PAVING
 - 08C HEAVY DUTY CONCRETE PAVING
- PAVING NOTES**
- 71A TRANSFORMER SLAB PER ELECTRIC COMPANY STANDARDS.

NOTE:
 SEE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF PORCHES, RAMPS, VESTIBULE, SLOPED PAVING, TRUCK DOCKS, BUILDING UTILITY ENTRANCE LOCATIONS AND PRECISE BUILDING DIMENSIONS.



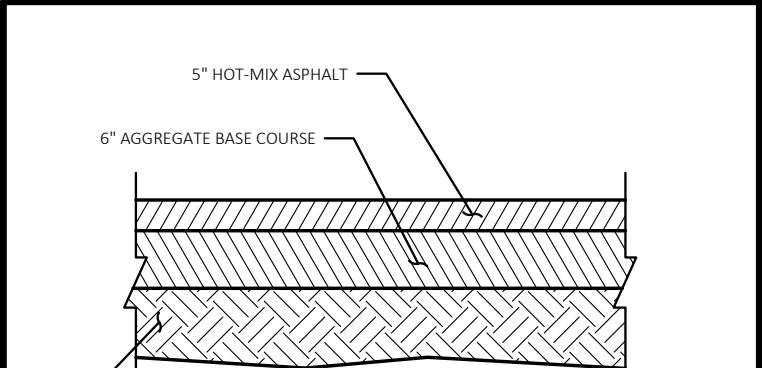
1. PAVEMENT DETAILS ARE PROVIDED FOR REFERENCE ONLY. CONTRACTOR SHALL VERIFY CONFORMANCE WITH THE SUBSURFACE EXPLORATION PREPARED BY CTL THOMPSON INCORPORATED REPORT DATED OCTOBER 11, 2018 PROJECT NUMBER 0M49.636-125-R1, PRIOR TO CONSTRUCTION.
2. PAVEMENTS WILL REQUIRE FIRM, UNIFORM SUPPORT CONDITIONS. SUBGRADE SHALL BE STRIPPED OF ORGANIC MATTER, SCARIFIED TO A DEPTH OF 2 FEET BELOW EXISTING GRADES, MOISTURE CONDITIONED TO WITHIN 2% OPTIMUM MOISTURE CONTENT FOR SAND AND WITHIN 0 AND 4% ABOVE OPTIMUM FOR CLAY, AND RE-COMPACTED TO AT LEAST 90% STANDARD PROCTOR DRY DENSITY (ASTM D 698/ASTM D 1557).
3. SUBGRADE SHOULD BE PROOF ROLLED WITH A LOADED TANDEM AXLE TRUCK DUMP TRUCK TO DISCLOSE SOFT AREAS. SOFT AREAS SHALL BE REWORKED AND RE-COMPACTED. SUBGRADE COMPACTION SHALL BE COMPLETED IN ACCORDANCE WITH THE FILL AND BACKFILL SECTION OF THE GEOTECH REPORT.
4. CONTRACTOR SHALL ENSURE THE ALL PAVEMENT MATERIAL AND CONSTRUCTION GUIDELINES FOR FLEXIBLE AND RIGID PAVEMENTS AS PROVIDED WITHIN APPENDIX B OF THE GEOTECH REPORT ARE MET.

HEAVY DUTY CONCRETE PAVING
 N.T.S.



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STANDARD DUTY ASPHALT PAVING
 N.T.S.



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HEAVY DUTY ASPHALT PAVING
 N.T.S.



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PLANS PREPARED FOR
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 SCOTTSDALE, AZ 85255
 PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
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 CONSTRUCTION

QUICK N CLEAN
 9572 TWENTY MILE ROAD
 PARKER, COLORADO

CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

PAVING PLAN
 SHEET TITLE
 SHEET NUMBER

C4.6

REVISION		
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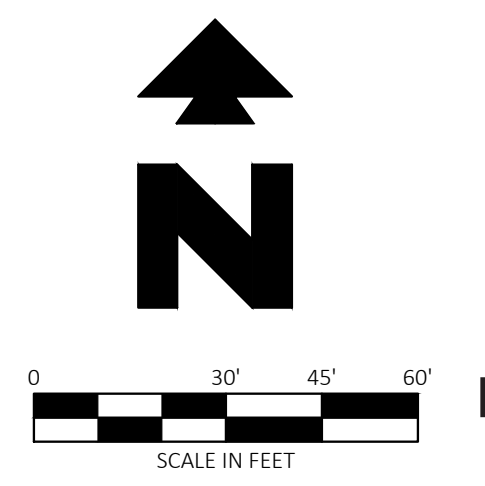
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	RM1672
DATE	3/23/2023
REVISION	REV-6

GRADING PLAN
SHEET TITLE
SHEET NUMBER

C5



Know what's below.
Call before you dig.

EXISTING LEGEND:

- PROPERTY LINE/RIGHT OF WAY LINE
- COMMUNICATIONS MANHOLE
- SANITARY SEWER MANHOLE
- STORM SEWER MANHOLE
- UNDERGROUND GAS LINES
- UNDERGROUND COMMUNICATIONS LINES
- UNDERGROUND STORM SEWER LINES

PROPOSED

- GRADE BREAK
- XXX CONTOUR ELEVATIONS
- STORM DRAIN
- XX.XX SPOT ELEVATIONS:
- BC = BOTTOM OF REINFORCED REVERSE CURB
- EG = EXISTING GRADE
- FFE = FINISH FLOOR ELEVATION
- FG = FINISH GRADE
- FL = FLOW LINE
- FW = FRONT OF WALL
- GB = GRADE BREAK
- RIM = TOP OF STRUCTURE
- TC = TOP OF CURB
- TW = TOP OF WALL

GENERAL GRADING NOTES

- A. PRIOR TO INSTALLATION OF STORM OR SANITARY SEWER, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS PRIOR TO CONSTRUCTION. THE ENGINEER WILL BE HELD HARMLESS IN THE EVENT THE ENGINEER IS NOT NOTIFIED OF DESIGN CONFLICTS.
- B. CONTRACTOR HAS THE OPTION TO BID THE FOLLOWING MATERIAL FOR THE STORM SEWER SYSTEM EXCEPT WHERE OTHERWISE NOTED: RCP, OR HDPE AS INDICATED ON THIS PLAN WHERE THE WORD PIPE IS USED. ALL PIPES SHALL HAVE A MAXIMUM ROUGHNESS COEFFICIENT ("N") OF 0.013 AND SHALL MEET OR EXCEED THE PIPE MANUFACTURERS REQUIREMENTS FOR MINIMUM AND MAXIMUM COVER. CONTRACTOR SHALL REFER TO THE PROJECT CONTRACT REQUIREMENTS FOR STORM SEWER SYSTEMS FOR ACCEPTABLE TYPE AND MATERIAL.
- C. ALL SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND 4" OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS (SEE LANDSCAPE PLAN FOR SEED MIX AND PROPER APPLICATION RATE). ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- D. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
- E. UNLESS OTHERWISE SHOWN, CALLED OUT OR SPECIFIED HEREON OR WITHIN THE SPECIFICATIONS OF THE LOCAL AUTHORITIES:
ALL STORM DRAIN PIPE BEDDING SHALL BE INSTALLED PER DETAIL 28A (REF. CS-2 - GRADING DETAILS). ALL STORM DRAIN PIPES ARE MEASURED FROM CENTER OF STRUCTURES AND ENDS OF FLARED END SECTIONS.

GRADING DETAILS (REF. GRADING DETAIL SHEET)

- 01P RAISED CURB AND GUTTER (REF. CS-2 - GRADING DETAILS)
- 73A 6" CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT - PER PARKER COLORADO STANDARD DETAIL 29 (REF. CS-3 - GRADING DETAILS)
- 73B 10" CDOT TYPE R CURB INLET W/ TOWN OF PARKER MANHOLE COVER (REF. CS-3 - GRADING DETAILS)
- 73C 5" CDOT MANHOLE W/TOWN OF PARKER MANHOLE COVER (REF. CS-4 - GRADING DETAILS)
- 73D CDOT TYPE 13 INLET (REF. CS-4 - GRADING DETAILS)
- 73E REINFORCED REVERSE CURB WITHOUT FOOTING (REF. CS-2 - GRADING DETAILS)
- 73F REINFORCED REVERSE CURB WITH FOOTING (REF. CS-2 - GRADING DETAILS)

GRADING NOTES

- 09A DOWN SPOUTS - (TYP. - PER ARCH. PLANS)
- 09H REMOVE TOP OF EXISTING DRAINAGE STRUCTURE AND ADJUST RIM TO ELEVATION TO MATCH FINISH GRADE. IF EXISTING STRUCTURE TOP IS A GRATE, REPLACE WITH TOWN OF PARKER MANHOLE COVER.
- 18D MATCH EXISTING PAVEMENT ELEVATIONS.
- 19A EXISTING TO REMAIN.
- 21A TAPER CURB TO MATCH EXISTING CURB.
- 21B TAPER CURB FROM 6 INCHES TO 0 INCHES OVER 2 FEET.
- 73A RETAINING WALL (PER ARCH. PLANS)
- 73B FOUR (4) FOOT SCREEN WALL (PER ARCH. PLANS)
- 73C LANDSCAPE SWALE TERMINATION (REF. SECTION VIEW THIS SHEET)
- 73D TRANSITION FROM 6" CURB AND GUTTER TO 12" RAISED CURB AND GUTTER

ADA STANDARD NOTE

THE PARTIES RESPONSIBLE FOR THIS PLAN HAVE FAMILIARIZED THEMSELVES WITH ALL CURRENT ACCESSIBILITY CRITERIA AND SPECIFICATIONS AND THE PROPOSED PLAN REFLECTS ALL SITE ELEMENTS REQUIRED BY THE APPLICABLE ADA DESIGN STANDARDS AND GUIDELINES AS PUBLISHED BY THE UNITED STATES DEPARTMENT OF JUSTICE. APPROVAL OF THIS PLAN BY THE CITY OF PARKER DOES NOT ASSURE COMPLIANCE WITH THE ADA OR ANY OTHER FEDERAL OR STATE ACCESSIBILITY LAWS OR ANY REGULATIONS OR GUIDELINES ENACTED OR PROMULGATED UNDER OR WITH RESPECT TO SUCH LAWS. SOLE RESPONSIBILITY FOR COMPLIANCE WITH FEDERAL AND STATE ACCESSIBILITY LAWS LIES WITH THE PROPERTY OWNER.

BENCHMARK

DOUGLAS CONTROL MONUMENT #1.095035, A 3" ALUMINUM CAP.
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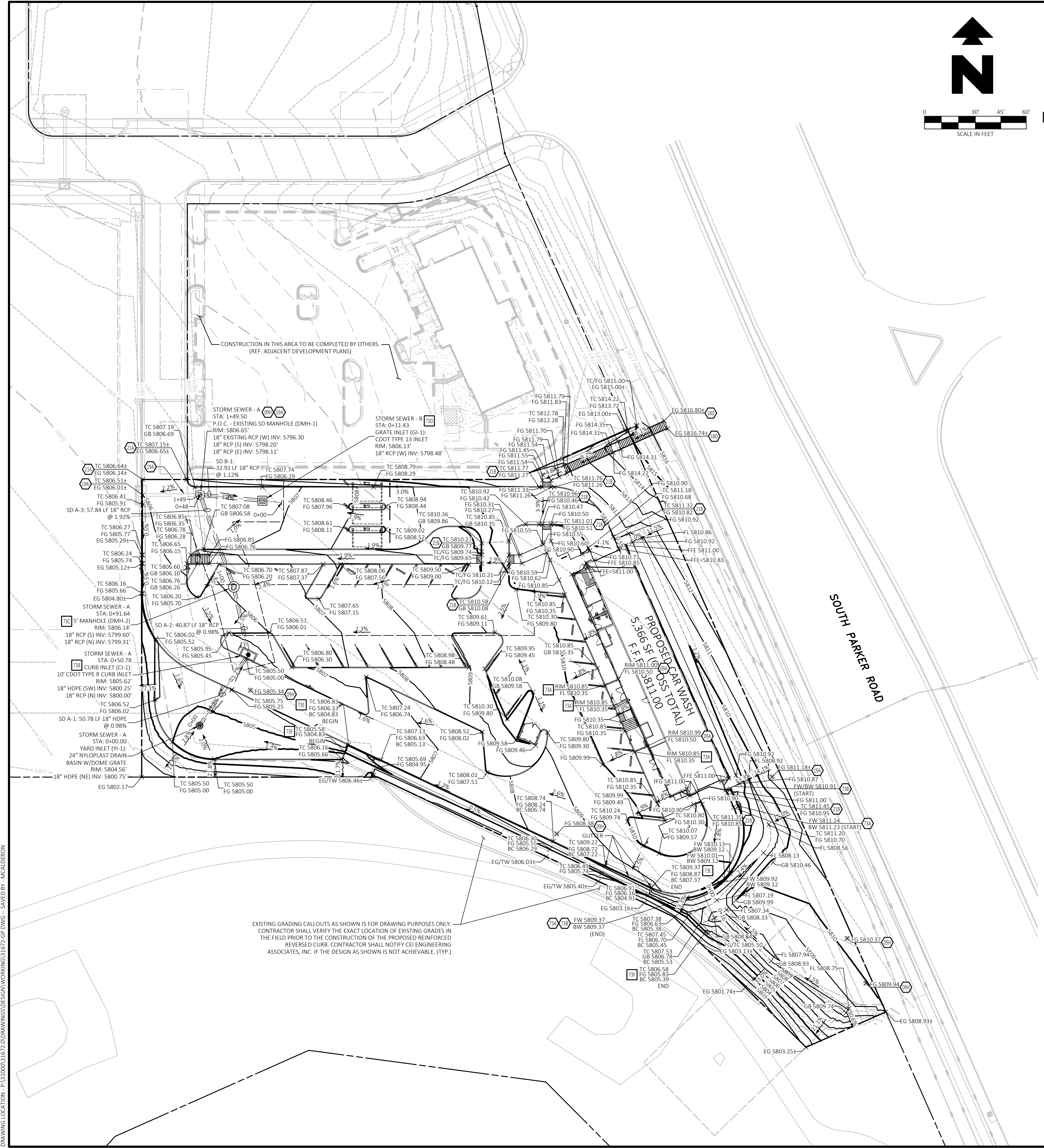
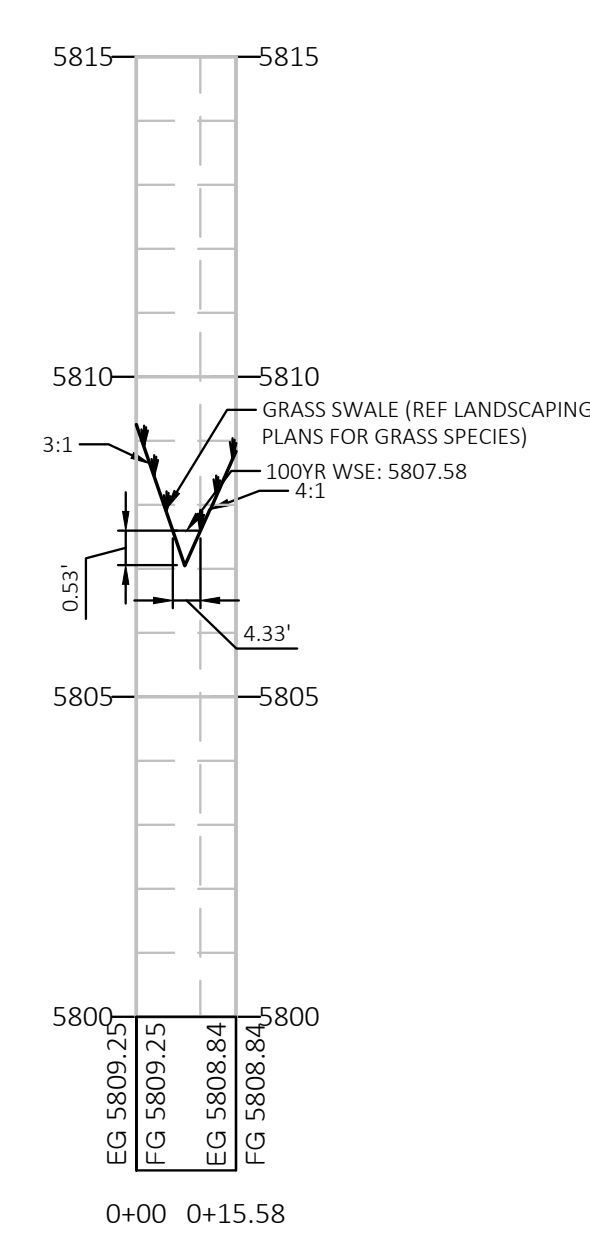
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LANDSCAPE SWALE CROSS SECTION PROFILE
VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'



CONSTRUCTION IN THIS AREA TO BE COMPLETED BY OTHERS.
(REF. ADJACENT DEVELOPMENT PLANS)

STORM SEWER - A
STA: 1+49.50
P.O.C. - EXISTING SD MANHOLE (DMH-1)
RIM: 5806.65'
18" EXISTING RCP (W) INV: 5796.30
18" RCP (S) INV: 5798.20'
18" RCP (E) INV: 5798.11'

STORM SEWER - B
STA: 0+11.43
GRATE INLET (G-1)
CDOT TYPE 13 INLET
RIM: 5806.13'
18" RCP (W) INV: 5798.48'

SD A-3: 57.84 LF 18" RCP @ 1.92%
TC 5806.44
EG 5806.14
TC 5806.51
EG 5806.01
TC 5806.41
FG 5805.91
TC 5806.41
FG 5805.78
FG 5805.28
EG 5805.24
TC 5806.29
FG 5806.15
EG 5805.124
TC 5806.16
FG 5805.66
EG 5804.804
TC 5806.20
FG 5805.70

SD A-2: 40.87 LF 18" RCP @ 0.98%
TC 5806.02
FG 5805.52
TC 5806.02
FG 5805.45
TC 5806.16
FG 5805.66
TC 5806.02
FG 5805.02
TC 5806.01
FG 5805.51

SD A-1: 50.78 LF 18" HDPE @ 0.90%
STA: 0+00.00
YARD INLET (Y-1)
24" NYLOPLAST DRAIN
BASIN W/DOME GRATE
RIM: 5804.56'
18" HDPE (NE) INV: 5800.75'
EG 5802.17

EXISTING GRADING CALLOUTS AS SHOWN IS FOR DRAWING PURPOSES ONLY. CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF EXISTING GRADES IN THE FIELD PRIOR TO THE CONSTRUCTION OF THE PROPOSED REINFORCED REVERSE CURB. CONTRACTOR SHALL NOTIFY CEI ENGINEERING ASSOCIATES, INC. IF THE DESIGN AS SHOWN IS NOT ACHIEVABLE. (TYP.)

DRAWING LOCATION - P:\330003\3303\DRAWINGS\DESIGN\WORKING\3303-GRP-DWG - SAVED BY - MACALDERON

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06/26/2023
Town of Parker, Director of Engineering Date

REVISION		
NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

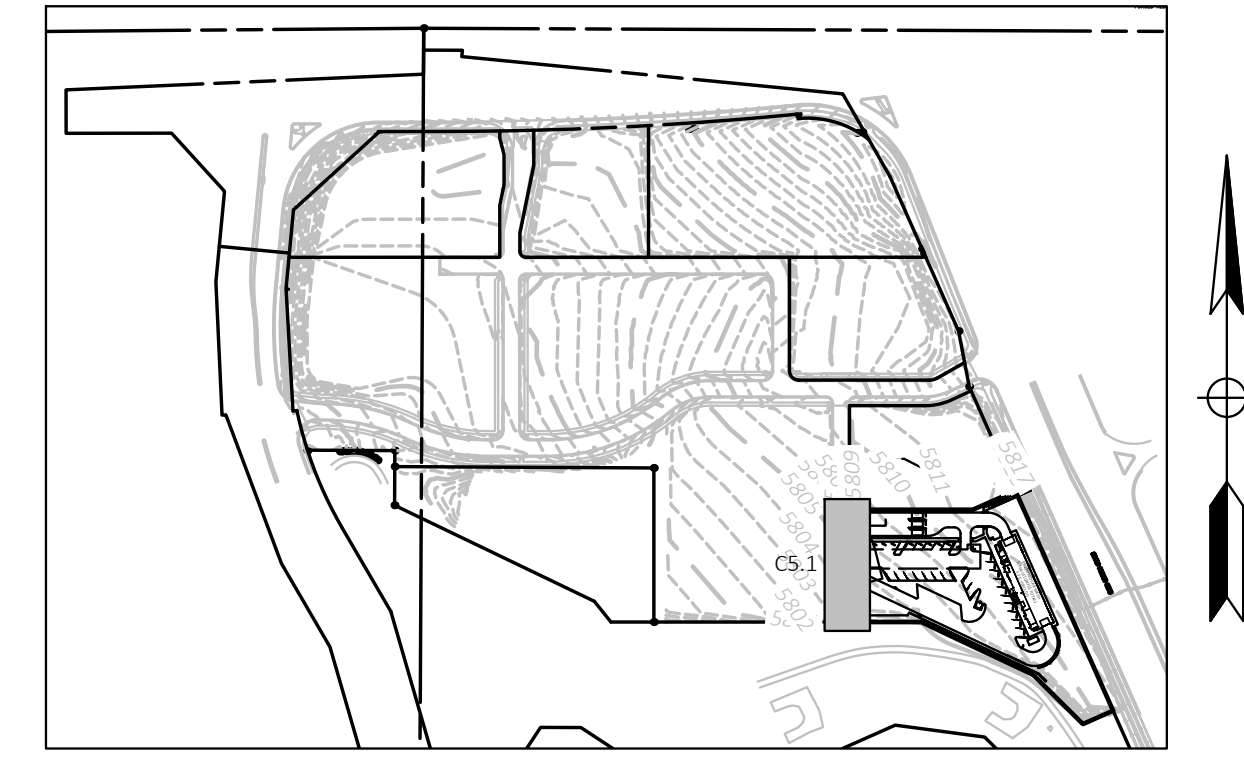
QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD	TAB
PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

STORM-A PLAN AND
PROFILE
SHEET TITLE
SHEET NUMBER

C5.1



KEY MAP
NOT TO SCALE

EXISTING LEGEND:

- PROPERTY LINE/RIGHT OF WAY LINE
- (C) COMMUNICATIONS MANHOLE
- (S) SANITARY SEWER MANHOLE
- (D) STORM SEWER MANHOLE
- GAS - UNDERGROUND GAS LINES
- COM - UNDERGROUND COMMUNICATIONS LINES
- STM - UNDERGROUND STORM SEWER LINES

PROPOSED

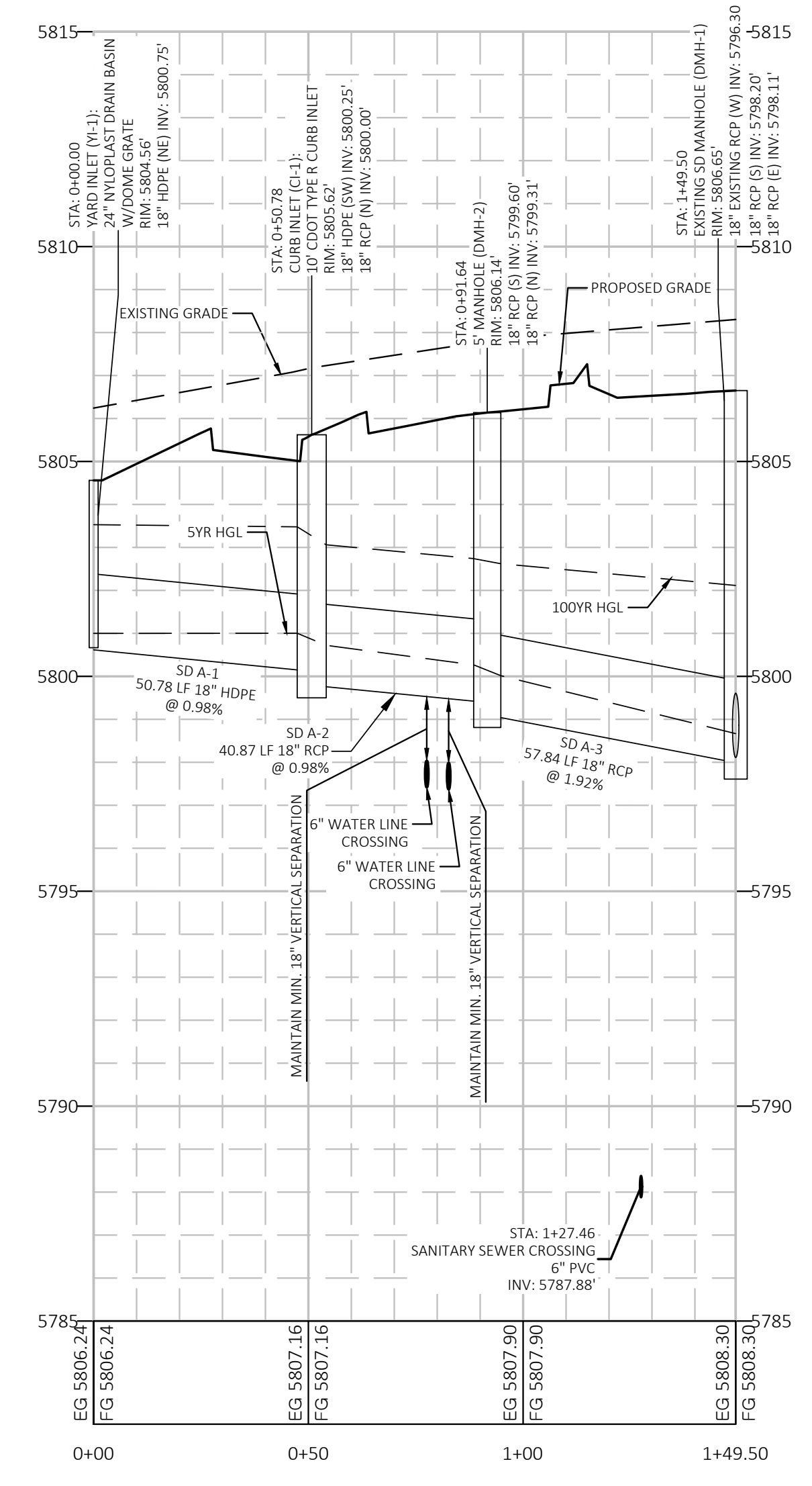
- GRADE BREAK
- XXX --- CONTOUR ELEVATIONS
- STORM DRAIN

STORM SEWER - STRUCTURE TABLE		
NAME	DETAILS	STATION
YARD INLET (YI-1)	RIM: 5804.56 SD A-1 (NE) INV: 5800.75'	STORM SEWER - A STA: 0+00.00
CURB INLET (CI-1)	RIM: 5805.62 SD A-1 (SW) INV: 5800.25' SD A-2 (N) INV: 5800.00'	STORM SEWER - A STA: 0+50.78
5' MANHOLE (DMH-2)	RIM: 5806.14 SD A-2 (S) INV: 5799.60' SD A-3 (N) INV: 5799.31'	STORM SEWER - A STA: 0+91.64
EXISTING SD MANHOLE (DMH-1)	RIM: 5806.65 SD A-3 (S) INV: 5798.20' SD B-1 (E) INV: 5798.11'	STORM SEWER - A STA: 1+49.50
GRATE INLET (GI-1)	RIM: 5806.13 SD B-1 (W) INV: 5798.48'	STORM SEWER - B STA: 0+11.43

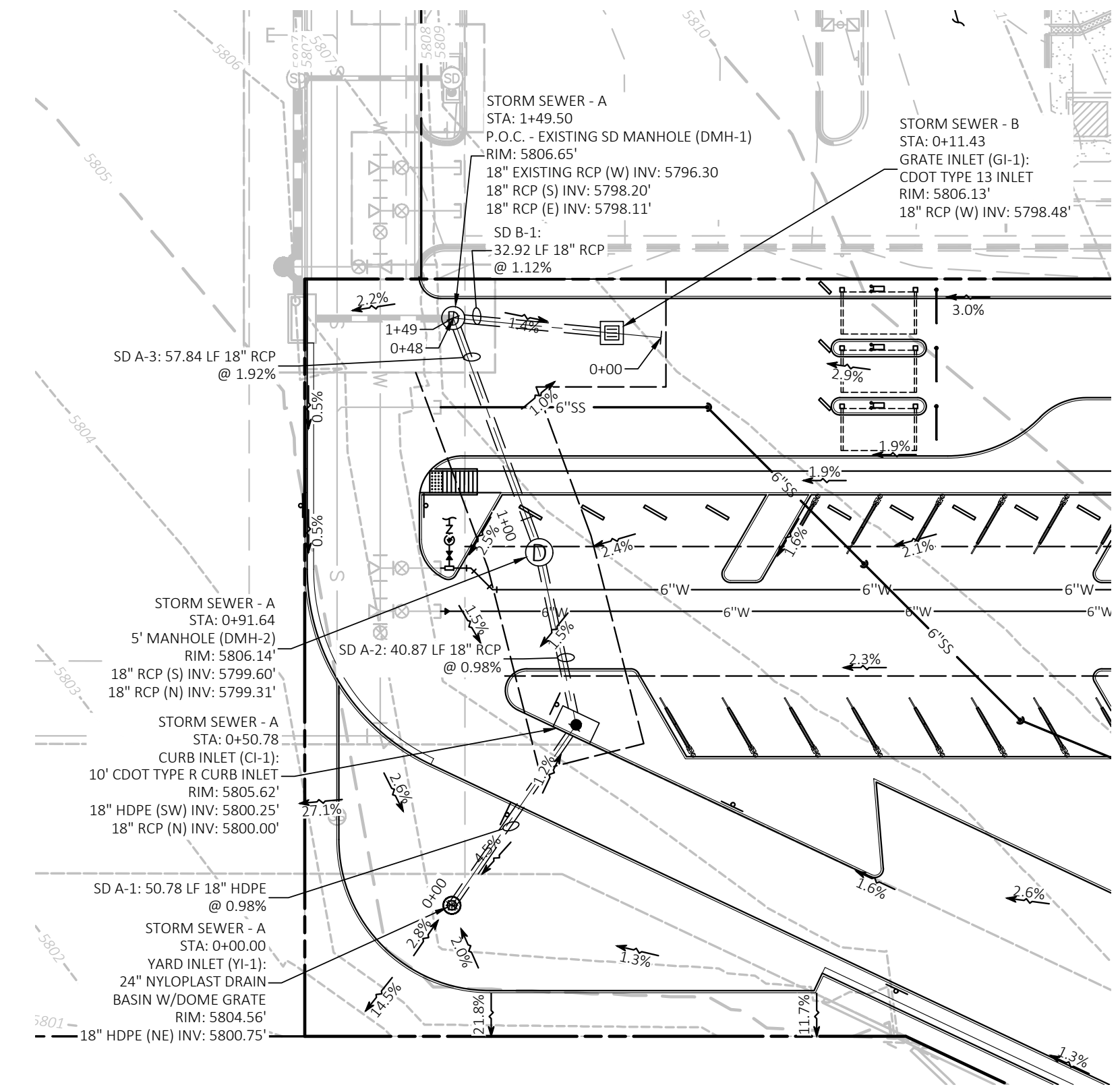
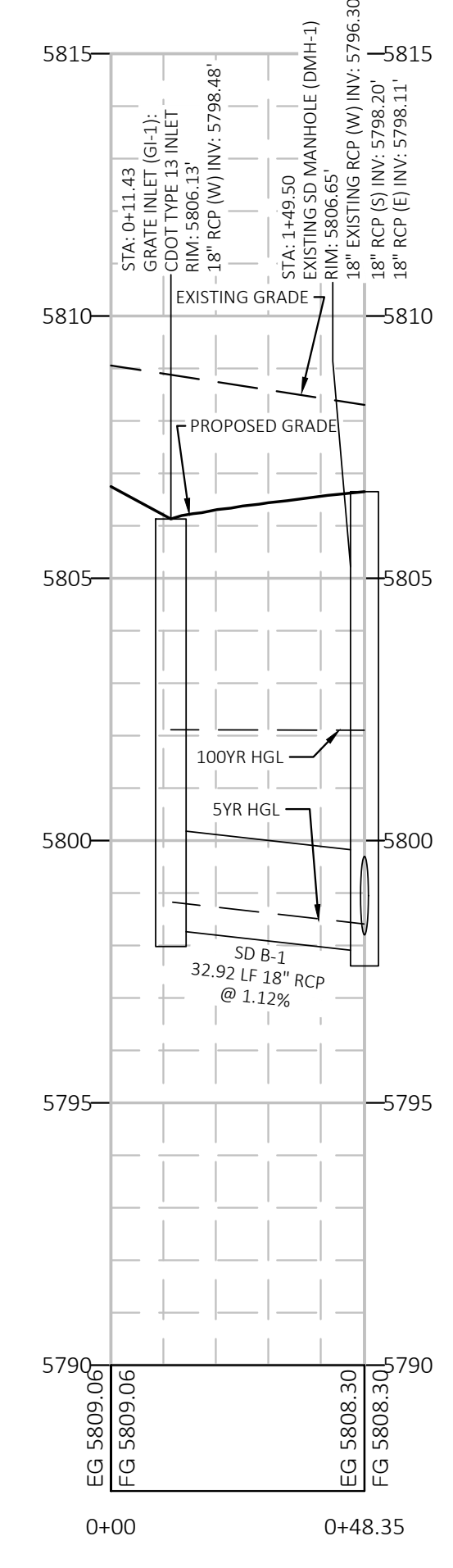


Know what's below.
Call before you dig.

STORM SEWER - A PROFILE
VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'



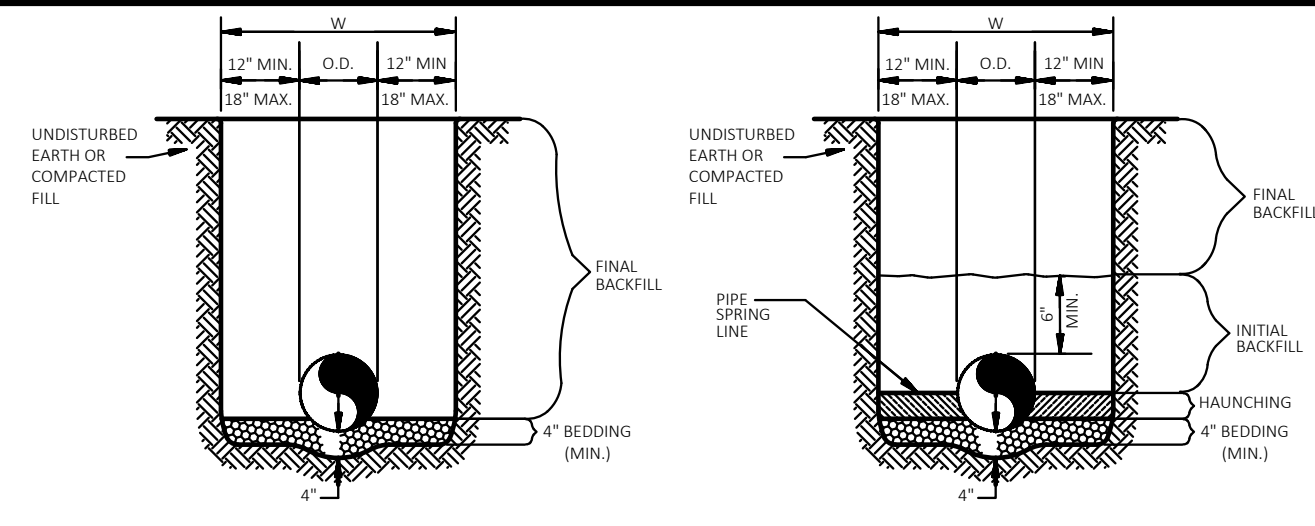
STORM SEWER - B PROFILE
VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'



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Thomas A. Durkin
Town of Parker, Director of Engineering
Date: 06/26/2023



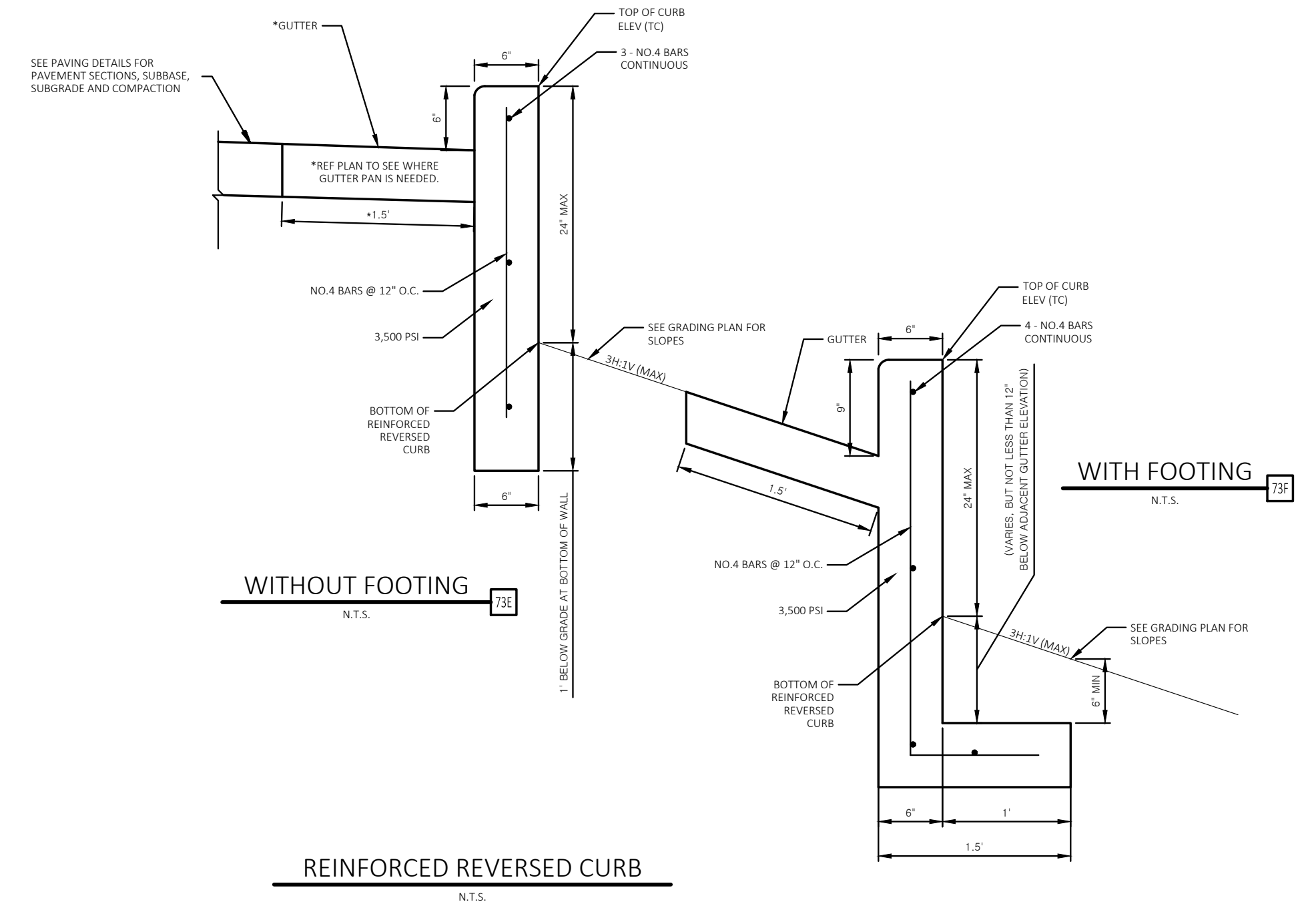
RIGID PIPE N.T.S. **FLEXIBLE PIPE** N.T.S.

GENERAL NOTES

1. BEDDING SHALL BE CLASS I-A WORKED BY HAND. IF GROUNDWATER IS ANTICIPATED, THEN BEDDING SHALL BE CLASS II COMPACTED TO 85% STANDARD PROCTOR. (SEE SPECIFICATIONS FOR GRADATION)
2. HAUNCHING SHALL BE WORKED AROUND THE PIPE BY HAND TO ELIMINATE VOIDS AND SHALL BE CLASS I-A OR CLASS II COMPACTED TO 85% PROCTOR.
3. INITIAL BACKFILL SHALL BE CLASS I-A WORKED BY HAND, OR CLASS II OR CLASS III COMPACTED TO 85% STANDARD PROCTOR.
4. INITIAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS II COMPACTED TO 90% STANDARD PROCTOR.
5. FINAL BACKFILL SHALL BE CLASS I, II, OR III COMPACTED AS NOTED IN NOTES 3, AND 4.
6. FINAL BACKFILL NOT UNDER PAVED AREAS CAN BE CLASS IV-A COMPACTED TO 95% STANDARD PROCTOR.
7. ALL MATERIALS ARE CLASSIFIED IN ACCORDANCE WITH ASTM D 2321-89.
8. ALL MATERIALS SHALL BE INSTALLED IN MAXIMUM 8" LOOSE LIFTS IN ACCORDANCE WITH ASTM D 1557-14. CLASS III AND IV-A MATERIALS SHALL BE COMPACTED NEAR OPTIMUM MOISTURE CONTENT.
9. FILL SALVAGED FROM EXCAVATION SHALL BE FREE OF DEBRIS, ORGANICS AND ROCKS LARGER THAN 3".
10. ALL TRENCH EXCAVATIONS SHALL BE SLOPED, SHORED, SHETTED, BRACED, OR OTHERWISE SUPPORTED IN COMPLIANCE WITH OSHA REGULATIONS AND LOCAL ORDINANCES. (SEE SPECIFICATIONS)

STORM SEWER TRENCH AND BEDDING N.T.S.

1. THE BASE COURSE & SUBGRADE SHALL BE COMPACTED PER THE GEOTECHNICAL ENGINEER RECOMMENDATIONS.
2. ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER.
3. ALL #4 & #5 REINFORCING BARS TO HAVE 1-1/2" COVER, LARGER SIZES TO HAVE 2" COVER.
4. ALL REINFORCING BARS TO BE GRADE 60.



REINFORCED REVERSED CURB N.T.S.

DRAWING LOCATION - P:\31000\31672\DRAWINGS\DESIGN\WORKING\31672-SP.DWG - SAVED BY - MACALBERON

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[Signature]
Town of Parker, Director of Engineering

06/26/2023
Date



CEI ENGINEERING ASSOCIATES, INC.
710 W. PINEDALE AVE.
FRESNO, CA 93711
PHONE: (559) 447-3119
FAX: (559) 447-3129



CLIENT
3K1 CONSULTING SERVICES, LLC.
11811 N. TATUM BOULEVARD,
PHOENIX, ARIZONA 85028
PHONE: (602) 850-8101



PLANS PREPARED FOR
QUICK N CLEAN
7291 E. ADOBE DRIVE, SUITE 115
SCOTTSDALE, AZ 85255
PHONE: (480) 707-3531

REVISION		
NO.	DESCRIPTION	DATE

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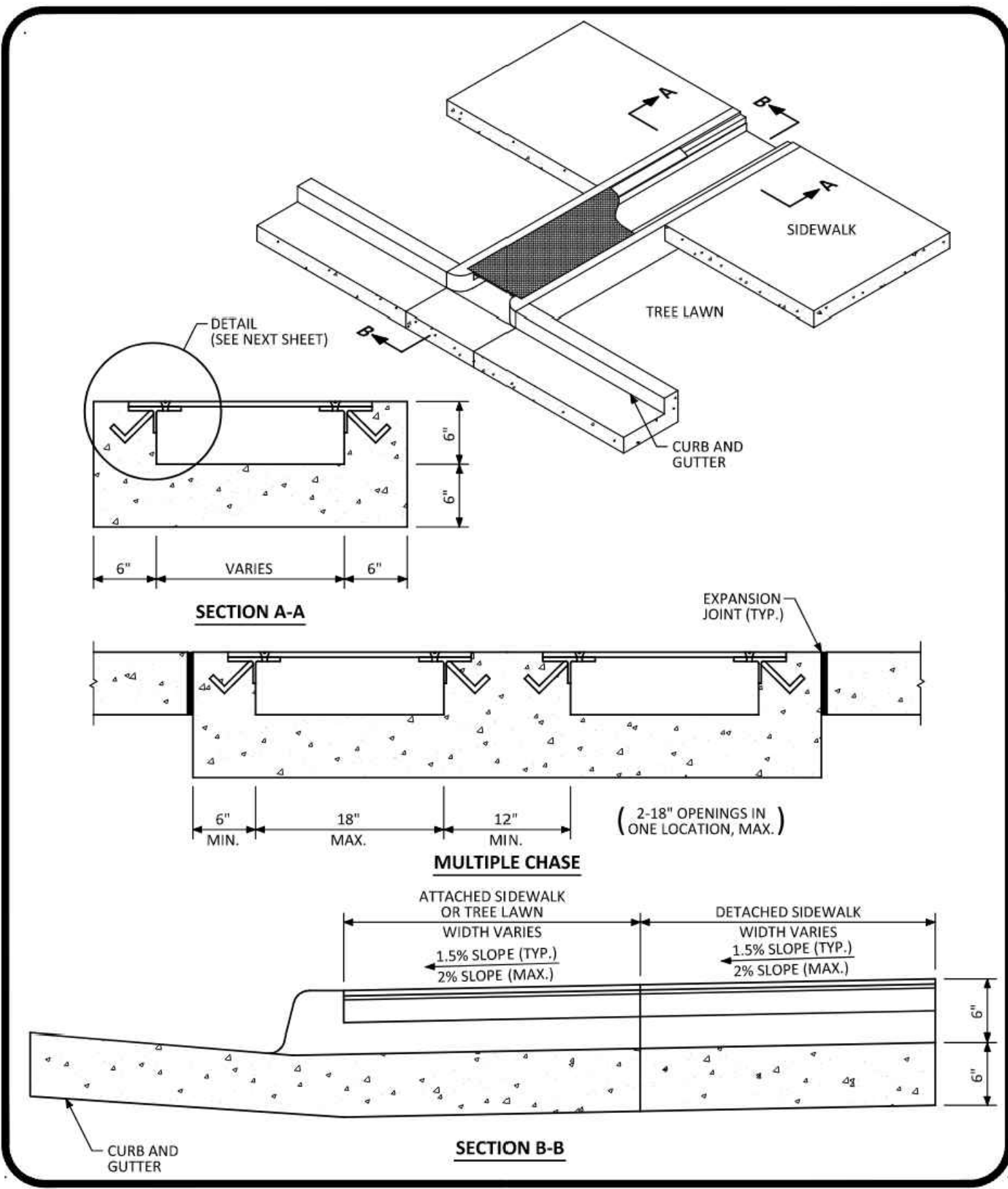
QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO
CONSTRUCTION DOCUMENTS



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REVISION	REV-6

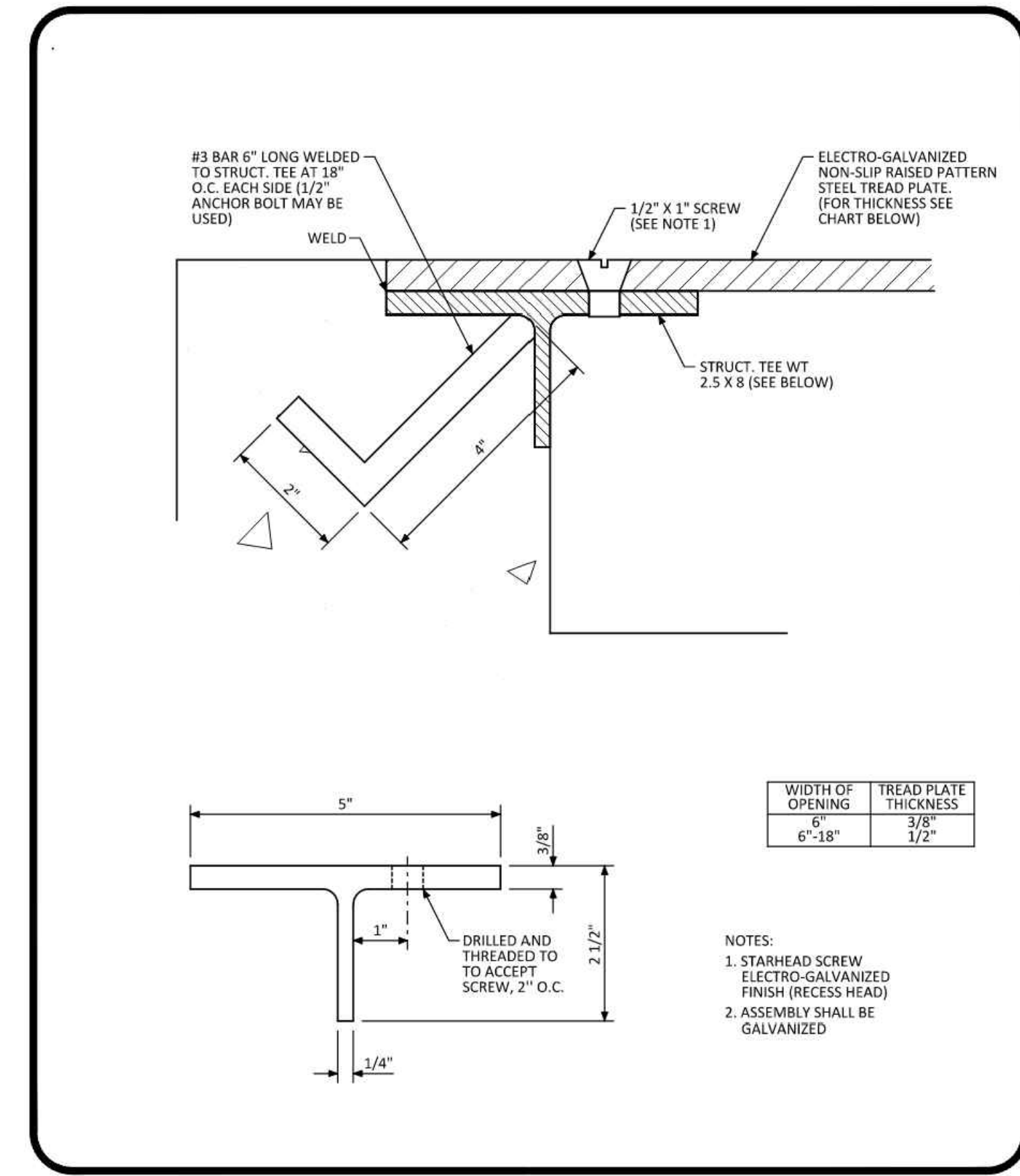
GRADING DETAILS
SHEET TITLE
SHEET NUMBER

C5.2



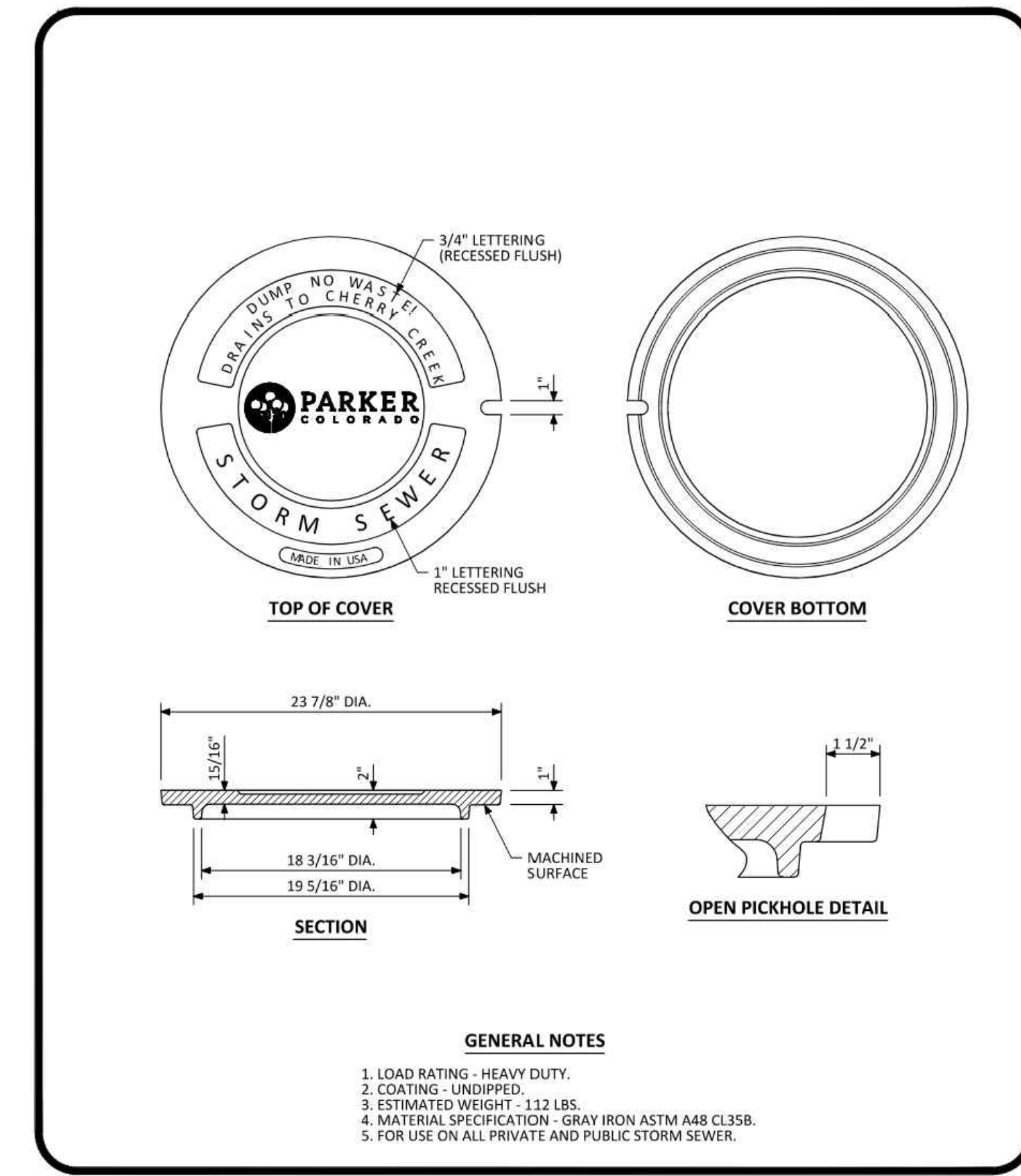
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

PARKER COLORADO **CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL** DATE DECEMBER 2016 **29** DETAIL 1 OF 2



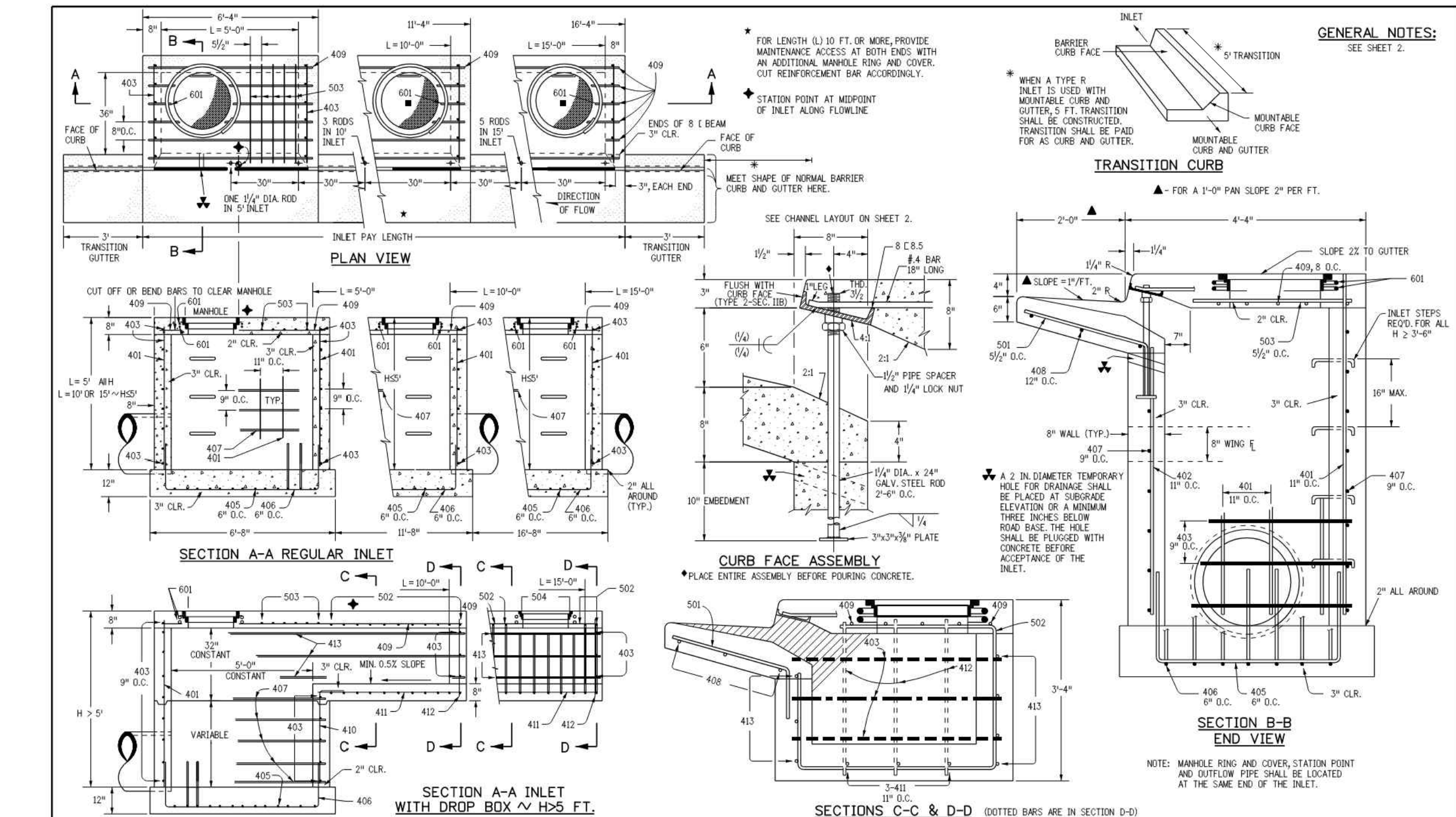
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PARKER COLORADO **CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL** DATE DECEMBER 2016 **29** DETAIL 2 OF 2



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

PARKER COLORADO **MANHOLE COVER STANDARD DETAIL** DATE NOVEMBER 2020 **33** DETAIL 1 OF 1



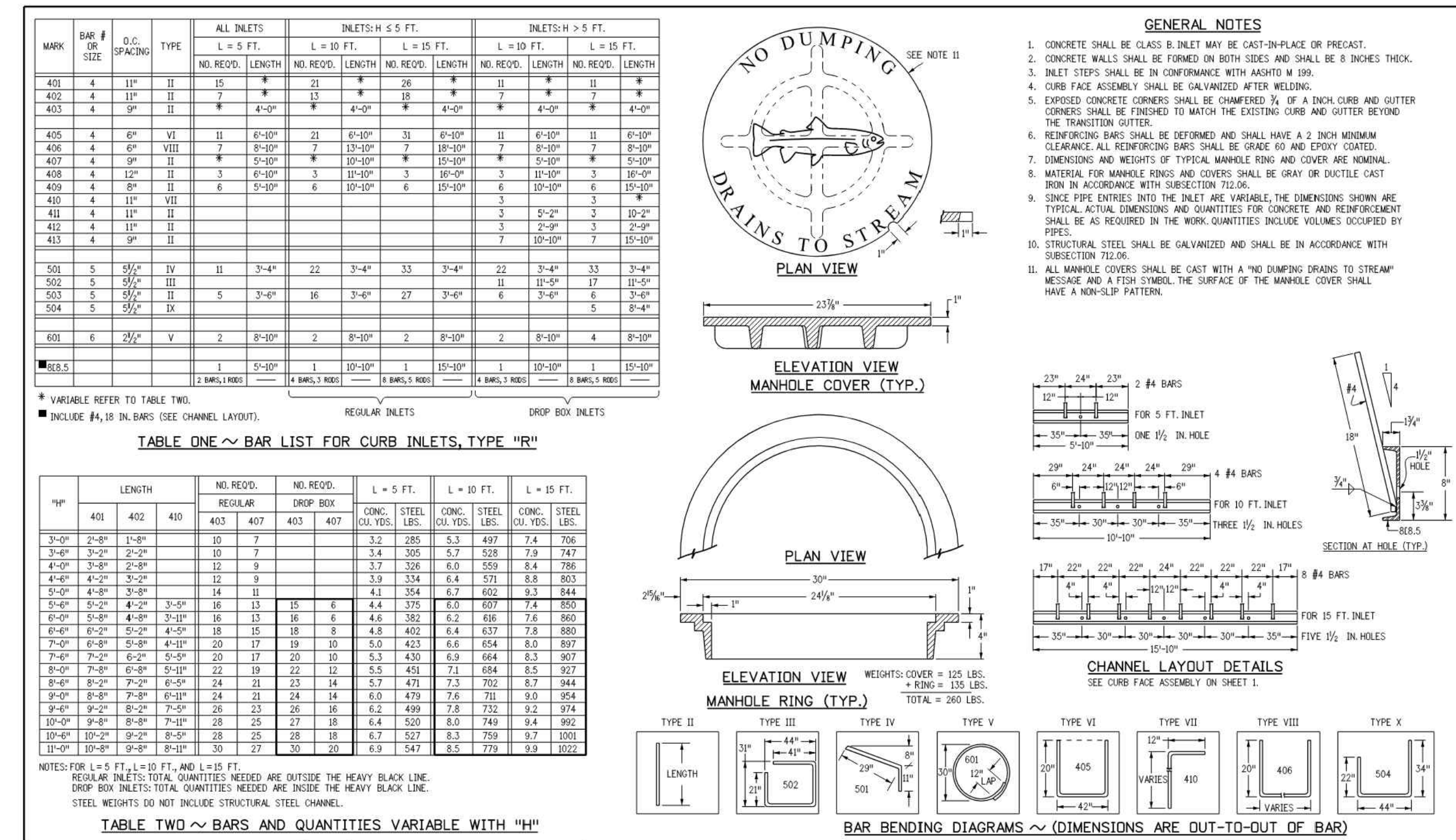
Computer File Information
 Creation Date: 07/31/19
 Designer: JBK
 Last Modification Date: 07/31/19
 Databases: LTA

Sheet Revisions

Colorado Department of Transportation
 2829 West Howard Place
 COOT, H3, 3rd Floor
 Denver, CO 80204
 Phone: 303-757-9921 FAX: 303-757-9988

CURB INLET TYPE R
 STANDARD PLAN NO. M-604-12
 Standard Sheet No. 1 of 2

Issued by the Project Development Branch: July 31, 2019 Project Sheet Number:



Computer File Information
 Creation Date: 07/31/19
 Designer: JBK
 Last Modification Date: 07/31/19
 Databases: LTA

Sheet Revisions

Colorado Department of Transportation
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CURB INLET TYPE R
 STANDARD PLAN NO. M-604-12
 Standard Sheet No. 2 of 2

Issued by the Project Development Branch: July 31, 2019 Project Sheet Number:



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PLANS PREPARED FOR
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REVISION

NO.	DESCRIPTION	DATE

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QUICK N CLEAN
 9572 TWENTY MILE ROAD
 PARKER, COLORADO

CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD TAB
 PROJECT MANAGER ASD
 DESIGNER RM
 CEI PROJECT NUMBER 31672
 DATE 3/23/2023
 REVISION REV-6

GRADING DETAILS
 SHEET TITLE
 SHEET NUMBER

C5.3

DRAWING LOCATION: P:\33000\31672\DRAWINGS\31672-GR.DWG - SAVED BY: JACALDERON

REVISION		
NO.	DESCRIPTION	DATE

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QUICK N CLEAN
9572 TWENTY MILE ROAD
PARKER, COLORADO

CONSTRUCTION DOCUMENTS



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PROJECT MANAGER	ASD
DESIGNER	RM
CEI PROJECT NUMBER	31672
DATE	3/23/2023
REVISION	REV-6

GRADING DETAILS
SHEET TITLE
SHEET NUMBER

C5.4

GENERAL NOTES

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- THE PRECAST FLAT TOP MAY BE USED ON ANY MANHOLE. THE ECCENTRIC CONE MAY BE USED WHEN THE MANHOLE "H" HEIGHT IS AT LEAST 3 FT.
- THE MANHOLE RING FRAME SHALL BE SET IN A BED OF GROUT. THE FRAME SHALL BE SURROUNDED WITH A CEMENT GROUT IN UNPAVED AREA OR A CONCRETE COLLAR IN PAVED AREA. SEE DETAILS ON SHEETS 2 AND 3.
- DECISION OF SOIL BASE IS BASED ON STRAIGHT RUNS OF PIPE OR CHANGE IN DIRECTION OF LESS THAN 45°. SPECIAL DESIGN IS REQUIRED FOR 45° OR GREATER.
- PRECAST MANHOLES AND REINFORCEMENT SHALL CONFORM TO AASHTO M 199 (ASTM C 478).
- CAST-IN-PLACE MANHOLES SHALL BE CLASS B CONCRETE.
- STEPS SHALL BE PROVIDED WHEN THE MANHOLE DEPTH EXCEEDS 3 FT. & IN AND SHALL CONFORM TO AASHTO M 199.
- ALL REINFORCING STEEL SHALL BE GRADE 60 AND EPOXY COATED. VERTICAL STEEL SHALL BE PLACED AT CENTERLINE OF WALL. ALL BARS SHALL HAVE A 2 IN. MINIMUM CLEARANCE.
- ALL PIPE ENTRIES INTO THE BASE OF MANHOLE SHALL BE CONNECTED BY OPEN CHANNELIZATION ADJUSTED FOR PIPE SIZE, SHAPE, SLOPE, AND DIRECTION OF FLOW. DETAILS SHOWN ARE TYPICAL. FOR INSTALLATIONS WITH ALL INVERTS OF SAME RELATIVE ELEVATION FOR EXCESSIVE ELEVATION DIFFERENCE BETWEEN INVERTS, SPECIAL BASE/CHANNEL DETAILS WILL BE SHOWN ON THE PLANS.
- FLOW CHANNELS AND INVERTS SHALL BE FORMED BY SHAPING WITH CLASS B CONCRETE OR APPROVED GROUT.
- STIP-OUTS SHALL EXTEND 2 FT. MINIMUM BEYOND OUTSIDE WALL SURFACE OF MANHOLE AND BE SATISFACTORILY PLUGGED.
- THE SLOPE OF THE MANHOLE COVER SHALL MATCH THE ROADWAY PROFILE AND CROSS SLOPE.
- WHEN FINAL GRADE IS PAVEMENT SURFACE, RECESS MANHOLE RING AND COVER 1/4" MIN. TO 1/2" MAX.

QUANTITIES FOR CONCRETE MANHOLE BOX BASE

MARK	SIZE	TYPE	WT. #/FT.	REINFORCING STEEL TOTAL	CONCRETE - CYCLES - TOTAL	FORMULAS
401	4	I	0.668	18	18	401 BAR LENGTH = 32" + 2W + 1D.
402	4	III	0.668	18	18	402 BAR LENGTH = 1D. + 2W
501	5	I	1.043	17	17	501 BAR LENGTH = 24" + 1D. + 2W
502	5	I	1.043	17	17	502 NUMBER BARS REQ'D. = 3 + ((24+1D+2W)/H)
503	5	II	1.043	17	17	503 BAR LENGTH = 4" + (2W+1D+2W)/2
504	5	I	1.043	17	17	504 NUMBER BARS REQ'D. = 2 + ((2W+1D+2W)/H)
1101	11	I	5.313	4	4	1101 BAR LENGTH = 21" + 1D. + 2W
1102	11	I	5.313	4	4	TYPE I BENDING
1103	11	I	5.313	4	4	TYPE II BENDING

GENERAL NOTES

- CONCRETE SHALL BE CLASS B INLET MAY BE CAST-IN-PLACE OR PRECAST.
- CAST-IN-PLACE CONCRETE WALLS SHALL BE FORMED ON BOTH SIDES.
- EXPOSED CONCRETE CORNERS SHALL BE CHAMFERED 3/4" OF A INCH.
- REINFORCING BARS SHALL BE DEFORMED #4 AND SHALL HAVE A 2 INCH MINIMUM CLEARANCE. ALL REINFORCING BARS SHALL BE GRADE 60 AND EPOXY COATED.
- STEPS SHALL BE PROVIDED WHEN INLET DIMENSION "H" IS EQUAL TO OR GREATER THAN 3 FEET 6 INCHES AND SHALL CONFORM TO AASHTO M 199.
- ALL GRATES AND FRAMES SHALL BE GRAY OR DUCTILE CAST IRON IN ACCORDANCE WITH SUBSECTION 712.06. GRATES AND FRAMES SHALL BE DESIGNED TO WITHSTAND HS 20 LOADINGS.
- STATION POINT IS AT THE CENTER OF THE INLET.
- GRATE SHALL HAVE "NO WASTE GRABS TO STREAM" MESSAGE CAST ON SURFACE.

QUANTITIES FOR ONE INLET

H	CONCRETE CU. YDS.	REINFORCING # LB.	NO. OF BARS	MAXIMUM PIPE I.D. IN.	SEC. #	SEC. B/B
3'-0"	1.3	72	4	18	18	18
3'-6"	1.5	76	4	24	18	18
4'-0"	1.6	80	5	30	18	18
4'-6"	1.8	104	6	30	18	18
5'-0"	1.9	109	6	30	18	18
5'-6"	2.1	122	7	30	18	18
6'-0"	2.2	136	8	30	18	18
6'-6"	2.4	141	8	30	18	18
7'-0"	2.5	154	9	30	18	18
7'-6"	2.7	168	10	30	18	18
8'-0"	2.8	173	10	30	18	18
8'-6"	3.0	187	11	30	18	18
9'-0"	3.1	200	12	30	18	18
9'-6"	3.3	205	12	30	18	18
10'-0"	3.4	219	13	30	18	18

Computer File Information
Creation Date: 07/31/19
Designer: JBK
Last Modification Date: 07/31/19
Detailer: LTA
CAD Ver.: MicroStation V8 Scale: Not to Scale Units: English

Sheet Revisions

Date:	Comments:

Colorado Department of Transportation
2829 West Howard Place
CDOT HQ, 3rd Floor
Denver, CO 80204
Phone: 303-757-9021 FAX: 303-757-9868

Project Development Branch JBK

CONCRETE INLET TYPE 13

STANDARD PLAN NO. M-604-13
Standard Sheet No. 1 of 1
Project Sheet Number:

GENERAL NOTES

- SINCE ALL PIPE ENTRIES INTO THE BASE ARE VARIABLE, THE DIMENSIONS SHOWN ARE TYPICAL. ACTUAL DIMENSIONS AND QUANTITIES FOR CONCRETE AND REINFORCEMENT SHALL BE AS REQUIRED IN THE WORK.
- THE PRECAST FLAT TOP MAY BE USED ON ANY MANHOLE. THE ECCENTRIC CONE MAY BE USED WHEN THE MANHOLE "H" HEIGHT IS AT LEAST 3 FT.
- THE MANHOLE RING FRAME SHALL BE SET IN A BED OF GROUT. THE FRAME SHALL BE SURROUNDED WITH A CEMENT GROUT IN UNPAVED AREA OR A CONCRETE COLLAR IN PAVED AREA. SEE DETAILS ON SHEETS 2 AND 3.
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- CAST-IN-PLACE MANHOLES SHALL BE CLASS B CONCRETE.
- STEPS SHALL BE PROVIDED WHEN THE MANHOLE DEPTH EXCEEDS 3 FT. & IN AND SHALL CONFORM TO AASHTO M 199.
- ALL REINFORCING STEEL SHALL BE GRADE 60 AND EPOXY COATED. VERTICAL STEEL SHALL BE PLACED AT CENTERLINE OF WALL. ALL BARS SHALL HAVE A 2 IN. MINIMUM CLEARANCE.
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- FLOW CHANNELS AND INVERTS SHALL BE FORMED BY SHAPING WITH CLASS B CONCRETE OR APPROVED GROUT.
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- THE SLOPE OF THE MANHOLE COVER SHALL MATCH THE ROADWAY PROFILE AND CROSS SLOPE.
- WHEN FINAL GRADE IS PAVEMENT SURFACE, RECESS MANHOLE RING AND COVER 1/4" MIN. TO 1/2" MAX.

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502	5	I	1.043	17	17	502 NUMBER BARS REQ'D. = 3 + ((24+1D+2W)/H)
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1101	11	I	5.313	4	4	1101 BAR LENGTH = 21" + 1D. + 2W
1102	11	I	5.313	4	4	TYPE I BENDING
1103	11	I	5.313	4	4	TYPE II BENDING

T-BASE MANHOLES NOTES

- THE T-BASE SECTION SHALL BE SHOP-FABRICATED FOR DELIVERY TO THE CONSTRUCTION SITE AS A COMPLETE UNIT.
- THESE DETAILS SHOW ONLY THE CONCEPTUAL AND STANDARD DIMENSIONAL REQUIREMENTS FOR TYPE T-BASE MANHOLES. THE CONTRACTOR SHALL FURNISH DETAILED SHOP DRAWINGS FOR APPROVAL PRIOR TO FABRICATION. THE DETAILS SHOWN HEREIN APPLY ONLY TO 48 IN. AND GREATER DIAMETER PIPES.
- EXCEPT FOR CLASS OF PIPE SPECIFICATIONS FOR THE MANHOLE SHALL BE THE SAME AS THOSE REQUIRED FOR THE ADJOINING PIPE.
- THE T-BASE SECTION SHALL MAINTAIN ITS INTERNAL SHAPE AND FLOW AREA. GROUTING OR FILLING SHALL BE APPLIED SO AS TO NOT DISTURB THE NORMAL FLOW OR REDUCE THE AREA.

QUANTITIES FOR ONE INLET

H	CONCRETE CU. YDS.	REINFORCING # LB.	NO. OF BARS	MAXIMUM PIPE I.D. IN.	SEC. #	SEC. B/B
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9'-0"	3.1	200	12	30	18	18
9'-6"	3.3	205	12	30	18	18
10'-0"	3.4	219	13	30	18	18

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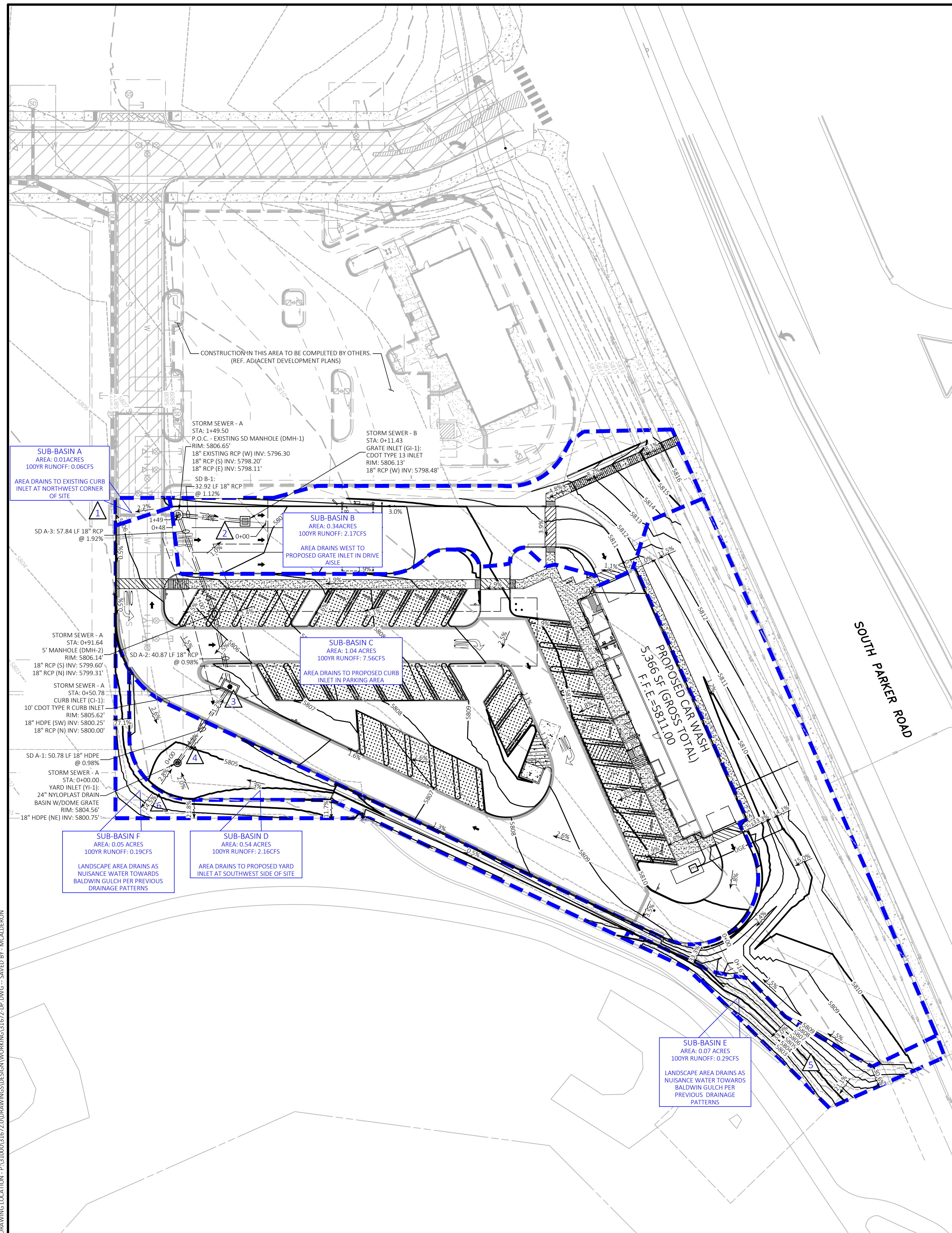
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Denver, CO 80204
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Project Development Branch JBK

MANHOLES

STANDARD PLAN NO. M-604-20
Standard Sheet No. 3 of 3
Project Sheet Number:



POST DEVELOPMENT STORMWATER RUNOFF
 Rational Method
 Project: Quick N Clean
 Address: Parker Rd. & Pine Lane Ave.
 Parker, CO
 Based Upon: City of Parker and UDFCD Drainage Manual

Time of Concentration

Min	Notes
Tc	Min. Tc for urbanized conditions

Sub-Basin A (Onsite and Offsite)

Total Area	0.01 acres	350 SF	Notes
Paved Area	0.01 acres	350 SF	Area drains to existing curb inlet at northwest corner of site
Drive and Walks	0.00 acres	0 SF	
Lawns, Sandy Soil	0.00 acres	0 SF	
% Impervious (Composite)	100%		
Hydrologic Soil Group B			
Runoff Coefficient (UDFCD NRCS Table)	5yr	100yr	
One Hour Point Rainfall (in) (Table 5.1 Town of Parker Manual)	5yr	100yr	0.89
Rainfall Intensity (in/hr) (Equation RA-S UDFCD)	5yr	100yr	2.6
Runoff (cfs)	5yr	100yr	8.82
			0.06

Sub-Basin B (Onsite and Offsite)

Total Area	0.34 acres	14,760 SF	Notes
Paved Area	0.20 acres <td>8,845 SF</td> <td>Area drains to proposed grate inlet in drive aisle</td>	8,845 SF	Area drains to proposed grate inlet in drive aisle
Drive and Walks	0.01 acres <td>512 SF</td> <td></td>	512 SF	
Lawns, Sandy Soil	0.12 acres <td>5,403 SF</td> <td></td>	5,403 SF	
% Impervious (Composite)	66%		
Hydrologic Soil Group B			
Runoff Coefficient (UDFCD NRCS Table)	5yr	100yr	
One Hour Point Rainfall (in) (Table 5.1 Town of Parker Manual)	5yr	100yr	0.72
Rainfall Intensity (in/hr) (Equation RA-S UDFCD)	5yr	100yr	2.6
Runoff (cfs)	5yr	100yr	8.82
			2.17

Sub-Basin C (Onsite and Offsite)

Total Area	1.04 acres	45,192 SF	Notes
Paved Area	0.68 acres <td>29,807 SF</td> <td>Area drains towards proposed curb inlet (C-1) in parking area</td>	29,807 SF	Area drains towards proposed curb inlet (C-1) in parking area
Drive and Walks	0.06 acres <td>2,590 SF</td> <td></td>	2,590 SF	
Roofs	0.12 acres <td>5,336 SF</td> <td></td>	5,336 SF	
Lawns, Sandy Soil	0.17 acres <td>7,459 SF</td> <td></td>	7,459 SF	
% Impervious (Composite)	82%		
Hydrologic Soil Group B			
Runoff Coefficient (UDFCD NRCS Table)	5yr	100yr	
One Hour Point Rainfall (in) (Table 5.1 Town of Parker Manual)	5yr	100yr	0.82
Rainfall Intensity (in/hr) (Equation RA-S UDFCD)	5yr	100yr	2.6
Runoff (cfs)	5yr	100yr	8.82
			7.56

Sub-Basin D (Onsite and Offsite)

Total Area	0.54 acres	23,679 SF	Notes
Paved Area	0.02 acres <td>674 SF</td> <td>Area drains towards proposed yard inlet (Y-1) at southwest corner of site</td>	674 SF	Area drains towards proposed yard inlet (Y-1) at southwest corner of site
Lawns, Sandy Soil	0.53 acres <td>23,005 SF</td> <td></td>	23,005 SF	
% Impervious (Composite)	5%		
Hydrologic Soil Group B			
Runoff Coefficient (UDFCD NRCS Table)	5yr	100yr	
One Hour Point Rainfall (in) (Table 5.1 Town of Parker Manual)	5yr	100yr	0.45
Rainfall Intensity (in/hr) (Equation RA-S UDFCD)	5yr	100yr	2.6
Runoff (cfs)	5yr	100yr	8.82
			2.16

Sub-Basin E (Onsite and Offsite)

Total Area	0.07 acres	3,225 SF	Notes
Lawns, Sandy Soil	0.07 acres <td>3,225 SF</td> <td>Landscape area drains as nuisance flow towards Baldwin Gulch per previous drainage patterns.</td>	3,225 SF	Landscape area drains as nuisance flow towards Baldwin Gulch per previous drainage patterns.
% Impervious (Composite)	2%		
Hydrologic Soil Group B			
Runoff Coefficient (UDFCD NRCS Table)	5yr	100yr	
One Hour Point Rainfall (in) (Table 5.1 Town of Parker Manual)	5yr	100yr	0.44
Rainfall Intensity (in/hr) (Equation RA-S UDFCD)	5yr	100yr	2.6
Runoff (cfs)	5yr	100yr	8.82
			0.29

Sub-Basin F (Onsite and Offsite)

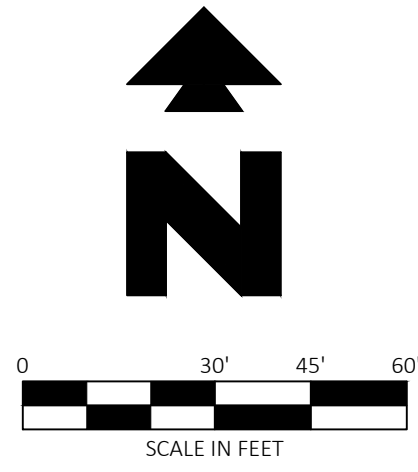
Total Area	0.05 acres	2,100 SF	Notes
Lawns, Sandy Soil	0.05 acres <td>2,100 SF</td> <td>Landscape area drains as nuisance flow towards Baldwin Gulch per previous drainage patterns.</td>	2,100 SF	Landscape area drains as nuisance flow towards Baldwin Gulch per previous drainage patterns.
% Impervious (Composite)	2%		
Hydrologic Soil Group B			
Runoff Coefficient (UDFCD NRCS Table)	5yr	100yr	
One Hour Point Rainfall (in) (Table 5.1 Town of Parker Manual)	5yr	100yr	0.44
Rainfall Intensity (in/hr) (Equation RA-S UDFCD)	5yr	100yr	2.6
Runoff (cfs)	5yr	100yr	8.82
			0.19

Total Runoff (CFS)

Total Area	2.05 acres	89,306 SF	
Paved Area	0.90 acres	39,002 SF	
Drive and Walks	0.09 acres	3,776 SF	
Roofs	0.12 acres	5,336 SF	
Lawns, Sandy Soil	0.95 acres	41,192 SF	
% Impervious (Composite)	62%		
Hydrologic Soil Group B			
Runoff (cfs)	5yr	100yr	
			12.44



Know what's below.
Call before you dig.



EXISTING LEGEND:

- PROPERTY LINE/RIGHT OF WAY LINE
- ⊙ COMMUNICATIONS MANHOLE
- ⊙ SANITARY SEWER MANHOLE
- ⊙ STORM SEWER MANHOLE
- GAS — UNDERGROUND GAS LINES
- COM — UNDERGROUND COMMUNICATIONS LINES
- SIM — UNDERGROUND STORM SEWER LINES

PROPOSED LEGEND

- BASIN BOUNDARY
- - - - - STORM DRAIN
- ⚠ DESIGN POINT

FLOOD ZONE INFORMATION

SUBJECT PROPERTY IS LOCATED WITHIN ZONE "X", AREAS OF MINIMAL FLOOD HAZARD, AS DETERMINED BY THE NATIONAL FLOOD INSURANCE PROGRAM.
 MAP NUMBER: 08035C0067G
 EFFECTIVE DATE: MARCH 16, 2016

BENCHMARK

DOUGLAS CONTROL MONUMENT # 1.095035, A 3" ALUMINUM CAP.
 ELEVATION = 5906.34 FEET (NAVD1988), AS PUBLISHED BY DOUGLAS COUNTY

PROPOSED DRAINAGE MAP AND BASIN ARE BASED ON BASIN NO. 8 IN PARKER AND PINE RETAIL DRAINAGE REPORT PREPARED BY KIMLEY HORN ENGINEERING NOVEMBER 2019

DETENTION IS PROVIDED FOR THE OVERALL DEVELOPMENT PER OVERALL DEVELOPER PLANS

THE RUNOFF FROM THE PROPOSED QUICK N CLEAN SITE IS LESS THAN OR EQUAL TO THE RUNOFF ALLOCATED TO BASIN NO. 8 IN THE PARKER AND PINE RETAIL DRAINAGE REPORT

Runoff Summary

BASIN ID	AREA Ac	Q ₅ CFS	Q ₁₀₀ CFS
1.1	1.43	4.93	10.37
1.2	1.97	6.77	14.25
2.0	0.88	3.00	6.33
3.0	1.97	6.80	14.29
4.1	1.14	3.91	8.23
4.2	0.77	2.65	5.56
5.0	1.13	3.89	8.18
6.0	2.22	7.62	16.06
7.0	1.02	3.48	7.35
8.0	1.94	6.13	12.90
9.0	0.10	0.42	0.81
10.0	0.16	0.65	1.26
11.0	0.21	0.86	1.66
12.0	0.14	0.57	1.09
13.0	0.08	0.32	0.62
14.0	0.17	0.67	1.30
15.0	0.18	0.72	1.40
16.0	0.17	0.67	1.30
17.0	0.11	0.45	0.87
18.0	0.39	1.59	3.09

← BASIN NO. 8 RUNOFF VALUES PER PARKER AND PINE RETAIL DRAINAGE REPORT

The Town of Parker review constitutes general compliance with the Town's Standards and approved variances, subject to these plans being stamped, signed, and dated by the professional engineer of record. Review by the Town does not constitute approval of the plan design or accuracy and correctness of engineering calculations. Errors in the design or calculations remain the responsibility of the registered professional engineer whose stamp and signature are affixed to this document.

This review does not constitute approval of any private on-site improvements which may be shown. Construction cannot commence until all required drainage/traffic reports, final development plan(s), special review(s), grading permit, and/or other permits are complete, approved and on file with the Town of Parker.

[Signature]
Town of Parker, Director of Engineering

06/26/2023
Date



CEI ENGINEERING ASSOCIATES, INC.
 710 W. PINEDALE AVE.
 FRESNO, CA 93711
 PHONE: (559) 447-3119
 FAX: (559) 447-3129



CLIENT
 3K1 CONSULTING SERVICES, LLC.
 11811 N. TATUM BOULEVARD,
 PHOENIX, ARIZONA 85028
 PHONE: (602) 850-8101



PLANS PREPARED FOR
 QUICK N CLEAN
 7291 E. ADOBE DRIVE, SUITE 115
 SCOTTSDALE, AZ 85255
 PHONE: (480) 707-3531

REVISION

NO.	DESCRIPTION	DATE

FOR REVIEW ONLY
NOT FOR
CONSTRUCTION

QUICK N CLEAN
 9572 TWENTY MILE ROAD
 PARKER, COLORADO
 CONSTRUCTION DOCUMENTS



PROFESSIONAL OF RECORD TAB
 PROJECT MANAGER ASD
 DESIGNER RM
 CEI PROJECT NUMBER 31672
 DATE 3/23/2023
 REVISION REV-6

DRAINAGE PLAN
 SHEET TITLE
 SHEET NUMBER

C6