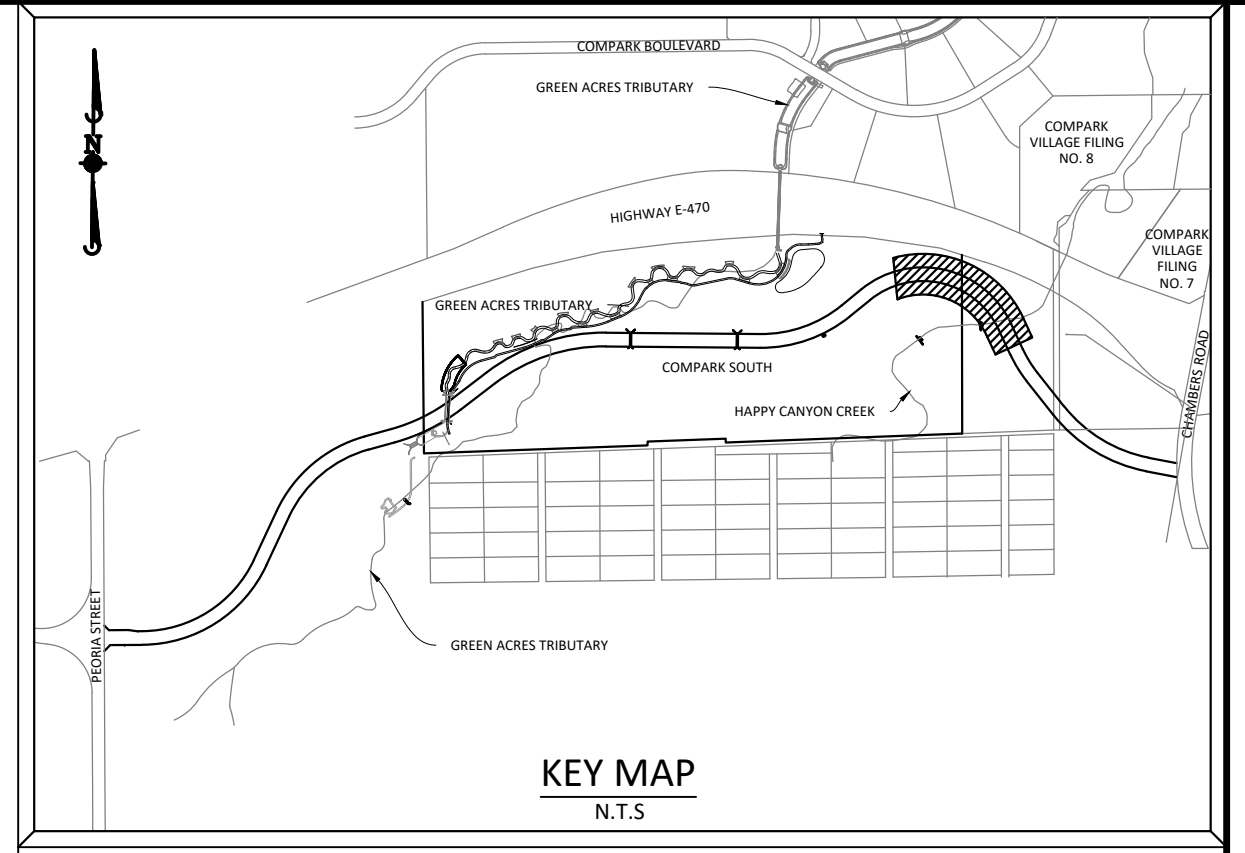
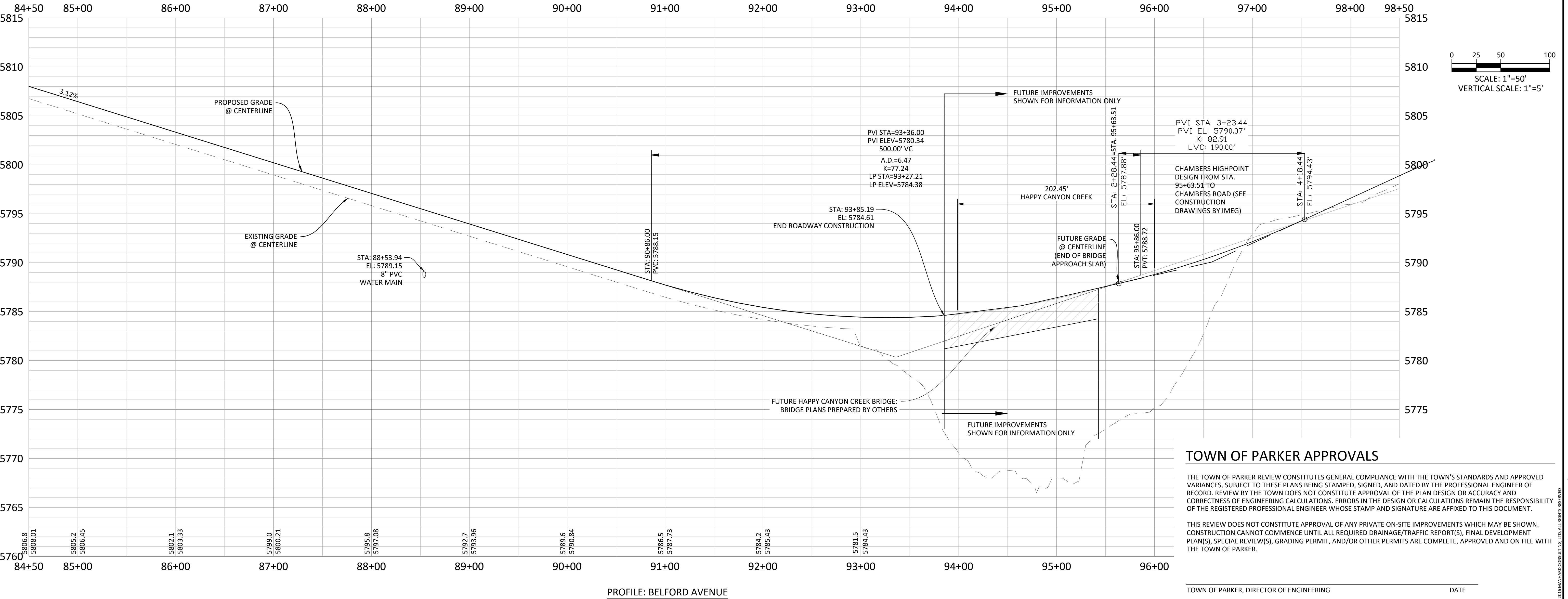
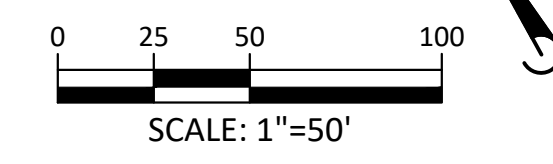


PLAN: BELFORD AVENUE



SOURCE BENCHMARKS:
 DOUGLAS COUNTY BM 1.115010
 A DOUGLAS COUNTY GIS MONUMENT SET IN CONCRETE LOCATED APPROXIMATELY 130 FEET SOUTHWESTERLY OF THE CENTERLINE OF CHAMBERS ROAD AND 95 FEET NORTHWESTERLY OF THE CENTERLINE OF COMPARK BOULEVARD. ELEVATION = 5752.84 (NAVD 88)

- NOTES:**
- 1) REFER TO GENERAL NOTES ON SHEET 2 FOR ALL APPLICABLE CONSTRUCTION STANDARDS
 - 2) ALL STATIONING IS CENTERLINE UNLESS OTHERWISE NOTED. REFER TO FINAL PLAT FOR RIGHT-OF-WAY AND CENTERLINE DIMENSIONS AND HORIZONTAL CONTROL.
 - 3) ALL ELEVATIONS ARE FL UNLESS OTHERWISE NOTED. TC INDICATES BACK OF WALK FOR COMBINATION CURB, GUTTER, AND WALK, OR TOP OF CURB FOR CURB AND GUTTER.
 - 4) REFER TO STONEGATE VILLAGE METRO DISTRICT APPROVED WATER AND SANITARY SEWER CONSTRUCTION PLANS FOR ALL PROPOSED WATER AND SANITARY SEWER LOCATIONS.
 - 5) TFI INDICATES TOP FRONT OF INLET ELEVATION. TFI ELEVATIONS SHOWN ON INLETS IN PLAN VIEW ARE AT FRONT CORNERS OF INLET. REFER TO SPECIFIC INLET DETAILS ON STORM SEWER PLAN AND PROFILE.
 - 3) REFER TO SHEET 28-30 FOR STREET AND CURB DETAILS. REFER TO SHEET 31-33 FOR STORM SEWER DETAILS.



PROFILE: BELFORD AVENUE

TOWN OF PARKER APPROVALS

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TOWN OF PARKER, DIRECTOR OF ENGINEERING _____ DATE _____

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 Civil Engineering | Surveying & Geospatial Services | Construction Management | Water Resource Management

REVISIONS

DATE	DESCRIPTION
1. 10/14/2021	APPROVED PLANS
2. 08/31/2021	THIRD SUBMITTAL - RESPOND TO TOP COMMENTS
1. 06/18/2021	SECOND SUBMITTAL - RESPOND TO TOP COMMENTS

COMPARK VILLAGE SOUTH, F1 - BELFORD AVE SITE PLAN AMD
TOWN OF PARKER, COLORADO
BELFORD AVENUE - PLAN & PROFILE

PROJ. MGR.: GTT
 PROJ. ASSOC.: ASD
 DRAWN BY: JMP
 DATE: 03/15/2021

PROFESSIONAL ENGINEER
 COLORADO REG. # 2142
 GARY T. HORN

SHEET
7 OF 35
 CLCPK3

FOR REVIEW - NOT FOR CONSTRUCTION

GENERAL NOTES (CONTINUE ON SHEET 2)

- ALL GUARDRAILS SHOWN ARE MASH 2016 TL-3 COMPLIANT.
- RATE OF SLOPE DEPENDS ON GUARDRAIL LOCATION:
 - FOR GUARDRAIL FACE 2 FT OR LESS FROM THE NORMAL EDGE OF PAVED SHOULDER, CONTINUE THE RATE OF SLOPE OF THE NORMAL PAVED SHOULDER TO THE BREAKPOINT.
 - FOR GUARDRAIL FACE MORE THAN 2 FT FROM THE NORMAL EDGE OF THE PAVED SHOULDER, THE SLOPE SHALL BE 10:1 OR FLATTER.
- WHEN SPECIFIED ON THE PLANS, EXTEND A 2 IN. MINIMUM THICKNESS PAVED SURFACE TO 1 FT BEHIND THE GUARDRAIL POSTS OR TO THE EXISTING CONTROL CURB AS SHOWN ON PLANS. ASPHALT CUTTING & PATCHING OR OTHER APPROVED METHOD SHALL BE USED TO MINIMIZE DAMAGE TO ALL PAVED SURFACES UNDER GUARDRAIL INSTALLATIONS. ALL REPAIRS TO THE PAVED AREA WILL NOT BE MEASURED AND PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE WORK. A MINIMUM 3 IN. THICK FIBER REINFORCED CONCRETE PAVEMENT MAY ALSO BE USED FOR PAVING BENEATH THE GUARDRAIL. INSTALL THE POST IN A 1/2 IN. OVERSIZED FORMED HOLE FOR GUARDRAIL RUNS AND TERMINALS AS DIRECTED. PAVEMENT FOR GUARDRAIL SURFACE WILL BE MADE UNDER A PAVEMENT OR CONCRETE PAY ITEM WITH QUANTITIES SHOWN ON THE PLANS.
- THE MINIMUM GUARDRAIL OFFSET FROM PAVED SHOULDER EDGE SHALL BE:
 - 0 FT FOR SHOULDERS 8 FT OR WIDER
 - 2 FT FOR SHOULDERS 6 FT OR LESS
 THE GUARDRAIL OFFSET FROM PAVED INSIDE SHOULDER EDGE OF A DIVIDED HIGHWAY SHALL BE:
 - 0 FT MINIMUM FOR SHOULDERS 6 FT OR WIDER
 - 2 FT DESIRABLE FOR 4 FT SHOULDERS
 THE ABOVE 2 FT GUARDRAIL TO SHOULDER OFFSET IS DESIRABLE BUT NOT REQUIRED FOR:
 - FOR AN EXISTING HIGHWAY WITH A DESIGN SPEED LESS THAN 50 MPH, THE MINIMUM OFFSET IS 4 FT FROM THE TRAVELED WAY.
 - FOR A ONE-WAY ONE-LANE RAMP, AND WHERE ONE OR MORE OF THE FOLLOWING ARE TRUE:
 - THE NON-OFFSET GUARDRAIL BEGINS AT LEAST 100 FT BEYOND RAMP NOSE.
 - THE NON-OFFSET GUARDRAIL IS NOT LOCATED ON THE RAMP EXIT OR ENTRANCE CURVE CONNECTION TO THE MAJOR HIGHWAY.
 - THE RAMP SHOULDERS ARE 4 FT OR WIDER.
 USE OF GREATER THAN MINIMUM OFFSET DIMENSIONS IS ENCOURAGED TO MEET THE DESIRABLE GOAL OF PLACING THE GUARDRAIL AS FAR AS POSSIBLE FROM THE TRAVEL WAY, EVEN FOR SHORT DISTANCES, WHILE PROVIDING A SMOOTH CHANGE IN GUARDRAIL ALIGNMENT.
- IF 2 FT CANNOT BE PROVIDED BETWEEN THE BACK OF THE GUARDRAIL POST AND THE BREAKPOINT, USE 7 FT GUARDRAIL POSTS. REFER TO THE "RESTRICTIVE ROADSIDE INSTALLATION" DETAIL.
- WHEN SPECIFIED ON THE PLANS, INSTALL 4 IN. HIGH TYPE 6 CURB WITH ITS FACE AT OR BEHIND THE RAIL FACE AS AN ALTERNATIVE WHEN SPECIFIED ON THE PLANS, INSTALL A 2 IN. x 6 IN. TREATED (ASHITO M 133) WOOD CURB FASTEN WITH A 4 IN. LAG BOLT AND WASHER AT EACH WOOD POST, OR WITH A 1 1/4 IN. DIA. BOLT WITH WASHER AND NUT AT EACH STEEL POST. IF THE 2 IN. x 6 IN. WOOD CURB IS SPECIFIED, IT WILL BE INCLUDED IN THE COST OF THE GUARDRAIL. IF APPROVED BY THE ENGINEER, A 2 IN. x 4 IN. TREATED WOOD CURB MAY BE SUBSTITUTED FOR THE 2 IN. x 6 IN. CURB AND SET ON TOP OF PAVEMENT SURFACE AND ATTACHED AS DESCRIBED ABOVE. NO SPLICING SHALL BE ALLOWED IN WOOD CURBS. ADJACENT BOARDS SHALL BE BUTTED TOGETHER AND BOLTED AT A POST LOCATION. JOINTS SHALL BE LOCATED AT THE POSTS.

SECTION A-A
LEAVE-OUT AREA FOR GUARDRAIL POSTS LOCATED IN PAVEMENT
NOTE: LEAVE-OUT AREAS SHALL BE PROVIDED FOR ALL GUARDRAIL POSTS LOCATED IN PAVEMENT TO ALLOW THE POSTS TO ROTATE IN THEIR EMBEDMENT SUCH THAT VEHICLE IMPACT LOADS ARE DISTRIBUTED THROUGH THE POST INTO THE EMBEDMENT MATERIAL PRIOR TO THE POSTS BREAKING PREMATURELY.

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Sheet Revisions

Date	Comments
03/05/20	Revised Per Note 1 to show MASH compliant

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Project Development Branch JBK

MIDWEST GUARDRAIL SYSTEM (MGS) TYPE 3 W-BEAM 31 INCHES
STANDARD PLAN NO. M-606-1
Standard Sheet No. 1 of 19
Issued by the Project Development Branch: July 31, 2019 Project Sheet Number:

GENERAL NOTES (CONTINUED FROM SHEET 1)

- SEE SHEETS 7 AND 9 FOR CURB TREATMENTS AT GUARDRAIL TERMINALS.
- IF THIS DIMENSION WILL BE LESS THAN 28 INCHES, RESET GUARDRAIL HEIGHT TO 28 INCHES OR ABOVE.
- ALL W-BEAM SPLICES AND SPLICES OF TERMINAL CONNECTORS TO W-BEAM SHALL BE LAPED IN THE DIRECTION OF TRAFFIC UNLESS OTHERWISE NOTED IN THE PLANS OR BY THE MANUFACTURER.
- MATERIAL TYPE AND SHAPE OF POSTS AND BLOCKS SHALL BE THE SAME THROUGHOUT THE PROJECT EXCEPT WHEN SPECIFIC POSTS AND BLOCKS ARE SPECIFIED, IN AT END ARCHWAYS AND BOX CULVERTS.
- WHEN SPECIFIED IN THE CONTRACT, 7 FT POSTS SHALL BE INSTALLED INSTEAD OF THE STANDARD 6 FT POSTS. THE 7 FT POSTS SHALL BE MARKED WITH THE NUMBER 7 TO ENSURE PERMANENT IDENTIFICATION. STEEL POSTS SHALL BE STAMPED PRIOR TO GALVANIZING. THE NUMBER 7 SHALL BE A MINIMUM 2 IN. TALL AND LOCATED AS SHOWN ON THE ELEVATION VIEW ON SHEET 1.
- THE STANDARD 3 IN. x 1 1/2 IN. x 1/2 IN. RECTANGULAR WASHER USED UNDER POST BOLT HEADS IN THE PAST MAY REMAIN IN EXISTING INSTALLATIONS BUT SHALL NOT BE USED IN NEW CONSTRUCTION, REPAIRS, OR RESETTING OF RAIL, EXCEPT WHEN SPECIFICALLY IDENTIFIED ON THE STANDARD PLAN.
- STANDARD GALVANIZED ROUND STEEL WASHERS SHALL BE USED UNDER ALL WITS IN CONTACT WITH WOOD POSTS.
- AN ADDITIONAL HOLE SHALL BE PROVIDED IN THE POSTS TO FACILITATE FUTURE RAISING OF THE RAIL ELEMENTS AND BLOCKS FOR OVERLAYS. POSTS PROVIDED MAY ALSO HAVE ADDITIONAL HOLES UP TO 4 PER FLANGE FOR MEDIAN GUARDRAIL APPLICATION.
- RETROREFLECTOR TABS SHALL BE INSTALLED AT 25 FT INTERVALS (SEE SHEETS 6 AND 8 FOR EXCEPTIONS). RETROREFLECTOR TABS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE WORK. THE TABS SHALL BE INSTALLED ON SPLICE BENTS NOT ON POST BOLTS AND SHALL BE MOUNTED SO THE BOLT SLOT FACES AWAY FROM TRAFFIC AND THE RETROREFLECTOR SURFACE FACES THE APPROACHING TRAFFIC FOR ONE-WAY ROADS FOR TWO-WAY ROADS, BOTH SIDES OF THE TABS SHALL BE RETROREFLECTIVE, SO THAT DELINEATION IS PROVIDED FOR BOTH DIRECTIONS OF TRAVEL. THE RETROREFLECTIVE SHEETING COLOR SHALL MATCH THE COLOR OF THE ADJACENT TRAVEL WAY EDGE LINE. SEE THE RETROREFLECTOR TAB DETAIL ON SHEET 3.
- AT THE TIME OF INSTALLATION, WOOD POSTS OR BLOCKS WITH SEASONING CHECKS GREATER THAN 1/4 IN. SHALL NOT BE USED WHEN THE CHECK EXTENDS THE FULL LENGTH OF THE PIECE.
- WOOD BLOCKS SHALL BE CUT FROM THE SAME CROSS-SECTION SPECIES, AND GRADE, AND SHALL RECEIVE THE SAME PRESERVATIVE TREATMENT AS THE POSTS WHEN WOOD POSTS ARE USED.
- REFERENCES SUCH AS @PDB01, @PDE01, AND @PWE01 IN THIS STANDARD PLAN SPECIFY HARDWARE DETAILS FROM 08A GUIDE TO STANDARDIZED HIGHWAY BARRIER HARDWARE PREPARED BY THE ASHTO-AASHTO-AASHTO JOINT COOPERATIVE COMMITTEE.
- RAIL BLOCKS MANUFACTURED FROM SYNTHETIC MATERIAL WILL BE ACCEPTED AS ALTERNATIVES TO WOOD BLOCKS FOR USE WITH STEEL POSTS PROVIDED THAT THE BLOCKS HAVE RECEIVED TRMA APPROVAL.
- WOOD POSTS SHALL BE MADE OF TIMBER WITH AN EXTREME FIBER STRESS IN BENDING OF 1200 PSI STRESS GRADING AND POST DIMENSIONS SHALL CONFORM WITH THE RULES OF THE WEST COAST INSPECTION BUREAU, OR THE SOUTHERN PINE BUREAU, OR THE WESTERN WOOD PRODUCTS ASSOCIATION. TIMBER FOR POSTS SHALL BE EITHER ROUGH SAWN (UNPLANED) OR S4S (DRESSED) FIBER SIDED WITH NOMINAL DIMENSIONS INDICATED. ONLY ONE TYPE OF SURFACE FINISH SHALL BE USED FOR POSTS AND BLOCKS IN ANY ONE CONTINUOUS LENGTH OF GUARDRAIL.
- GLULAM POSTS AND BLOCKS WILL BE ACCEPTED AS ALTERNATIVES PROVIDED THAT THE SUPPLIED MATERIALS HAVE RECEIVED TRMA APPROVAL AND ARE CERTIFIED AS IDENTICAL TO THE SPECIMENS USED FOR TESTING AND APPROVAL.
- PRESSURE TREATMENT OF POSTS AND BLOCKS SHALL CONFORM TO ASHTO M 133 EXCEPT THAT BLOCKS NEED NOT BE INCISED. PRESERVATION ASSAY RETENTION REPORTS SHALL BE SUBMITTED TO THE ENGINEER.
- W-BEAM AND THREE-BEAM GUARDRAIL POSTS SHALL BE MANUFACTURED USING ASHTO M 270 (ASTM A 709) GRADE 36 STEEL UNLESS CORROSION RESISTANT STEEL IS REQUIRED, IN WHICH CASE THE POST SHALL BE MANUFACTURED FROM ASHTO M 270 (ASTM A 709) GRADE 50 LOW STEEL. THE DIMENSIONS OF THE CROSS-SECTION SHALL CONFORM TO A W6 X 9 SECTION AS DEFINED IN ASHTO M 180 (ASTM A 6) W6 X 8.5 WIDE FLANGE STEEL. POSTS ARE AN ACCEPTABLE ALTERNATIVE TO THE W6 X 9.
- AFTER THE SECTION IS CUT AND ALL HOLES ARE DRILLED OR PUNCHED THE COMPONENT SHALL BE ZINC-COATED CONFORMING TO ASHTO M 133 (ASTM A 123) UNLESS CORROSION-RESISTANT STEEL IS USED. WHEN CORROSION-RESISTANT STEEL IS USED THE PORTION OF THE POST TO BE EMBEDDED IN SOIL SHALL BE ZINC-COATED CONFORMING TO ASHTO M 133 (ASTM A 123) AND THE PORTION ABOVE THE SOIL SHALL NOT BE ZINC-COATED, PAINTED OR OTHERWISE TREATED.
- FIELD MODIFICATION TO RAIL ELEMENTS IS ALLOWED PER MANUFACTURER'S RECOMMENDATIONS, OR WITH THE APPROVAL OF THE STANDARDS AND SPECIFICATIONS UNIT. POSTS SHALL NOT BE MODIFIED. COMPONENTS ON WHICH THE SPLITTER COATING HAS BEEN DAMAGED SHALL BE EITHER RE-GALVANIZED OR RE-COATED IN CONFORMANCE WITH ASHTO M 133 OR PAINTED WITH ONE FULL BRUSH COAT OF ZINC RICH PAINT CONFORMING TO MILITARY SPECIFICATION DDD-P-2033A.

WOOD POST & BLOCK (PDB01 & PDE01)
MINIMUM DIMENSIONS ARE SHOWN FOR THE POSTS & BLOCKS

STEEL POST & NOTCHED BLOCK (PWE01)
MINIMUM DIMENSIONS ARE SHOWN FOR THE POSTS & BLOCKS

DOUBLE BLOCK AND GUARDRAIL TYPE 3 (DOUBLE) FOR MEDIAN BARRIER

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Sheet Revisions

Date	Comments
03/05/20	Revised Per Note 1 to show MASH compliant

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Project Development Branch JBK

MIDWEST GUARDRAIL SYSTEM (MGS) TYPE 3 W-BEAM 31 INCHES
STANDARD PLAN NO. M-606-1
Standard Sheet No. 2 of 19
Issued by the Project Development Branch: July 31, 2019 Project Sheet Number:

TERMINAL SECTION (FLARED)

W-BEAM RAIL SPlice

TERMINAL SECTION (CONNECTOR)

THREE BEAM TERMINAL SECTION (CONNECTOR)

THREE BEAM DETAIL

RETROREFLECTOR TAB
NOTE: RETROREFLECTOR TABS SHALL BE MANUFACTURED FROM 12 TO 14 GAUGE STEEL AND SHALL CONFORM TO THE REQUIREMENTS OF S STANDARD S-602-L.

BUTTON HEAD BOLT WITH OVAL SHOULDER

WASHER

HEX NUT

RECTANGULAR WASHER (TO BE USED ONLY WHERE SPECIFIED)

DIAMETER & TYPE (INCHES)	1 1/2" BLOCKS L = LENGTH (INCHES)	THREAD LENGTH (INCHES)	INTENDED USE	ASHTO-AASHTO-AASHTO STANDARD NUMBER	NO. BOLTS, NUTS & WASHERS
3/4"	1 1/4"	FULL (1 1/2")	ALL RAIL SPLICES	FBB01	8 PER SPLICE*
22	MIN. 2 1/2"	SINGLE BLOCK & POST (TIMBER)		FBB04	1 PER POST
33	MIN. 2"	DOUBLE BLOCK & POST (TIMBER)		FBB05	1 PER POST
14	MIN. 2"	FASTEN NOTCHED BLOCK TO STEEL POST		FBB03	1 PER BLOCK

WASHERS NOT USED AT RAIL SPLICES

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STANDARD PLAN NO. M-606-1
Standard Sheet No. 3 of 19
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TOWN OF PARKER, COLORADO
GUARDRAIL DETAILS

COMPARK VILLAGE SOUTH, F1 - BELFORD AVE SITE PLAN AMD

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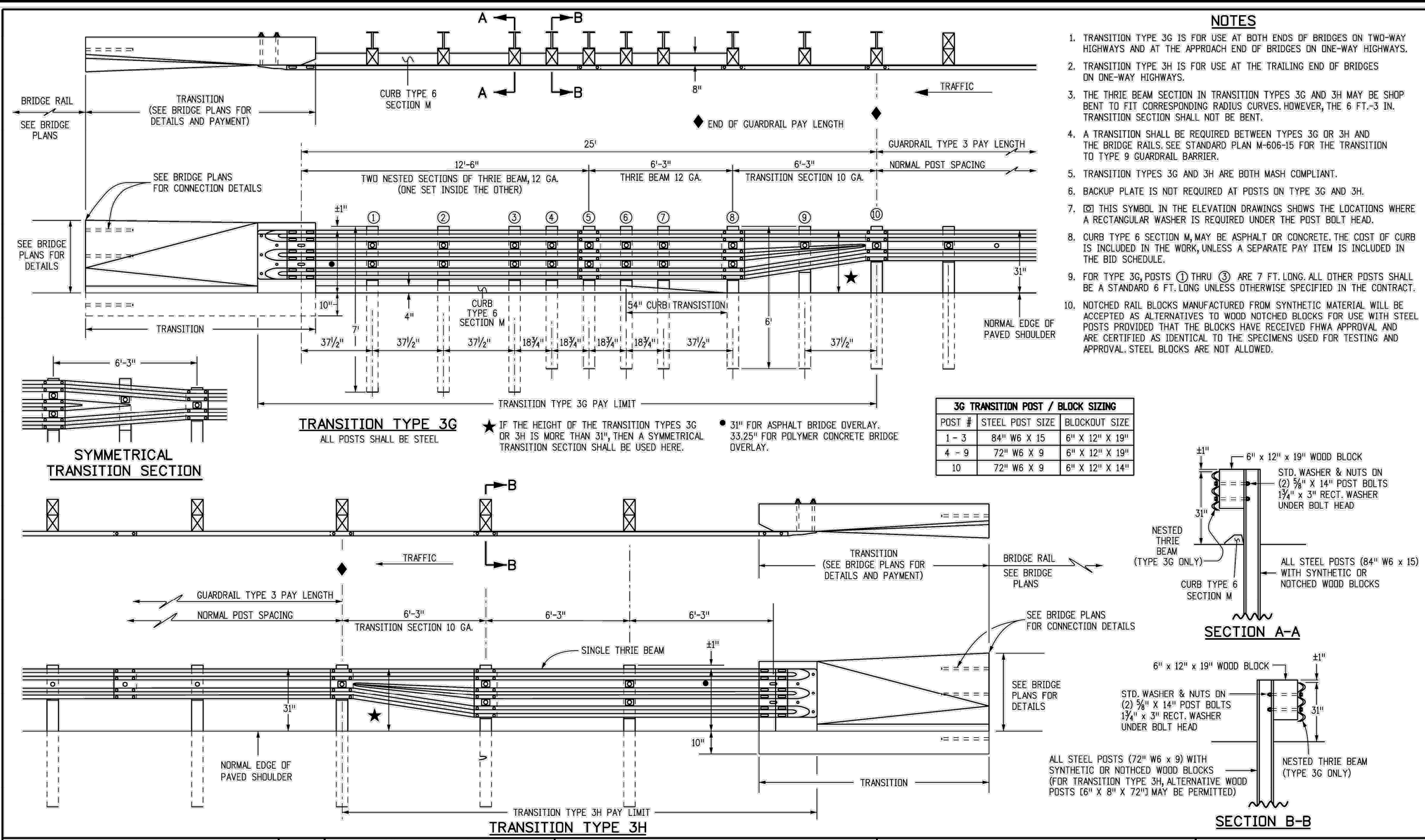
TOWN OF PARKER, DIRECTOR OF ENGINEERING DATE

34 OF 35
CLCPK3 CLCPK3

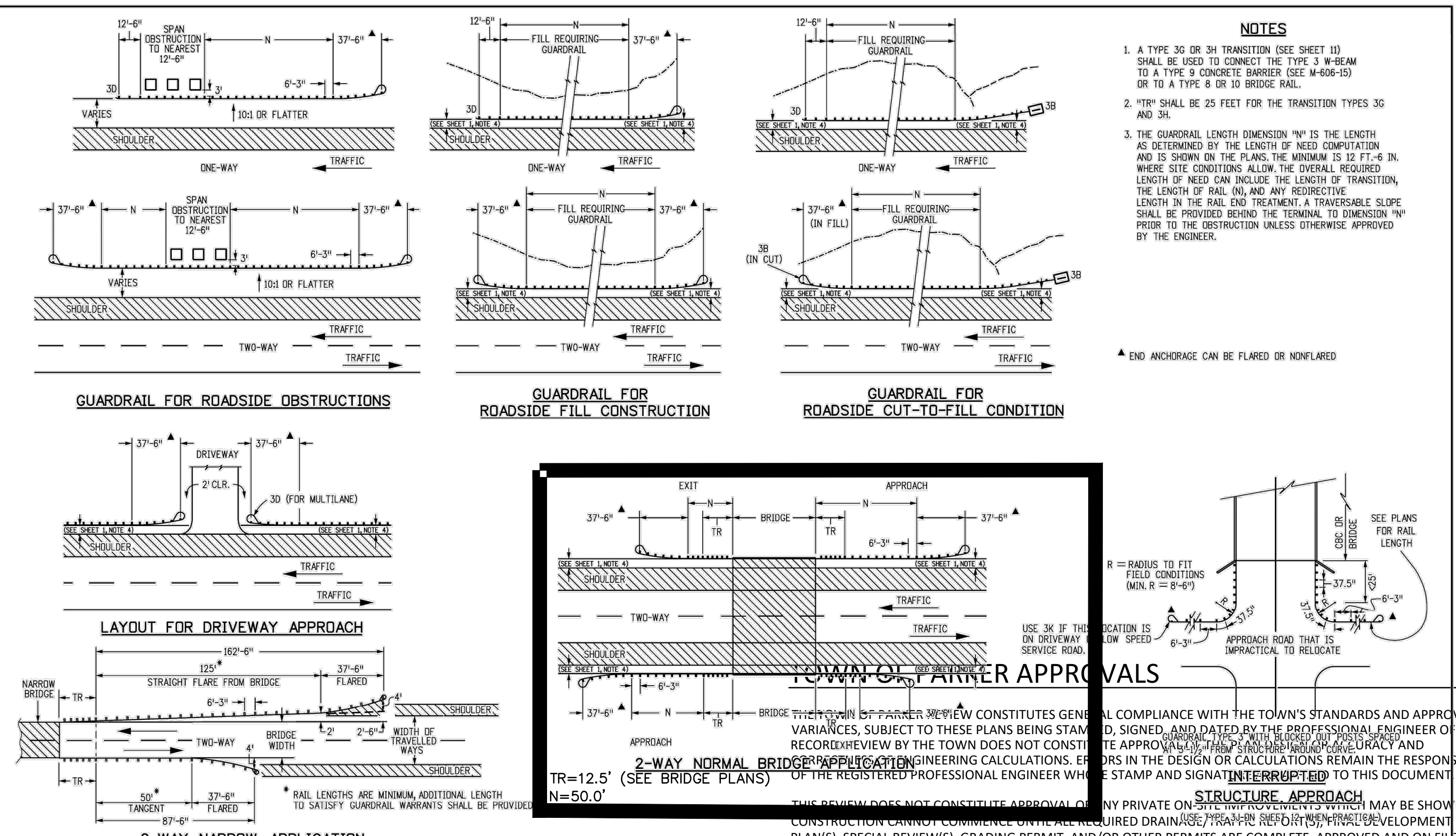
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PROJ. MGR.: GTT
PROJ. ASSOC.: ASD
DRAWN BY: JMP
DATE: 03/15/2021

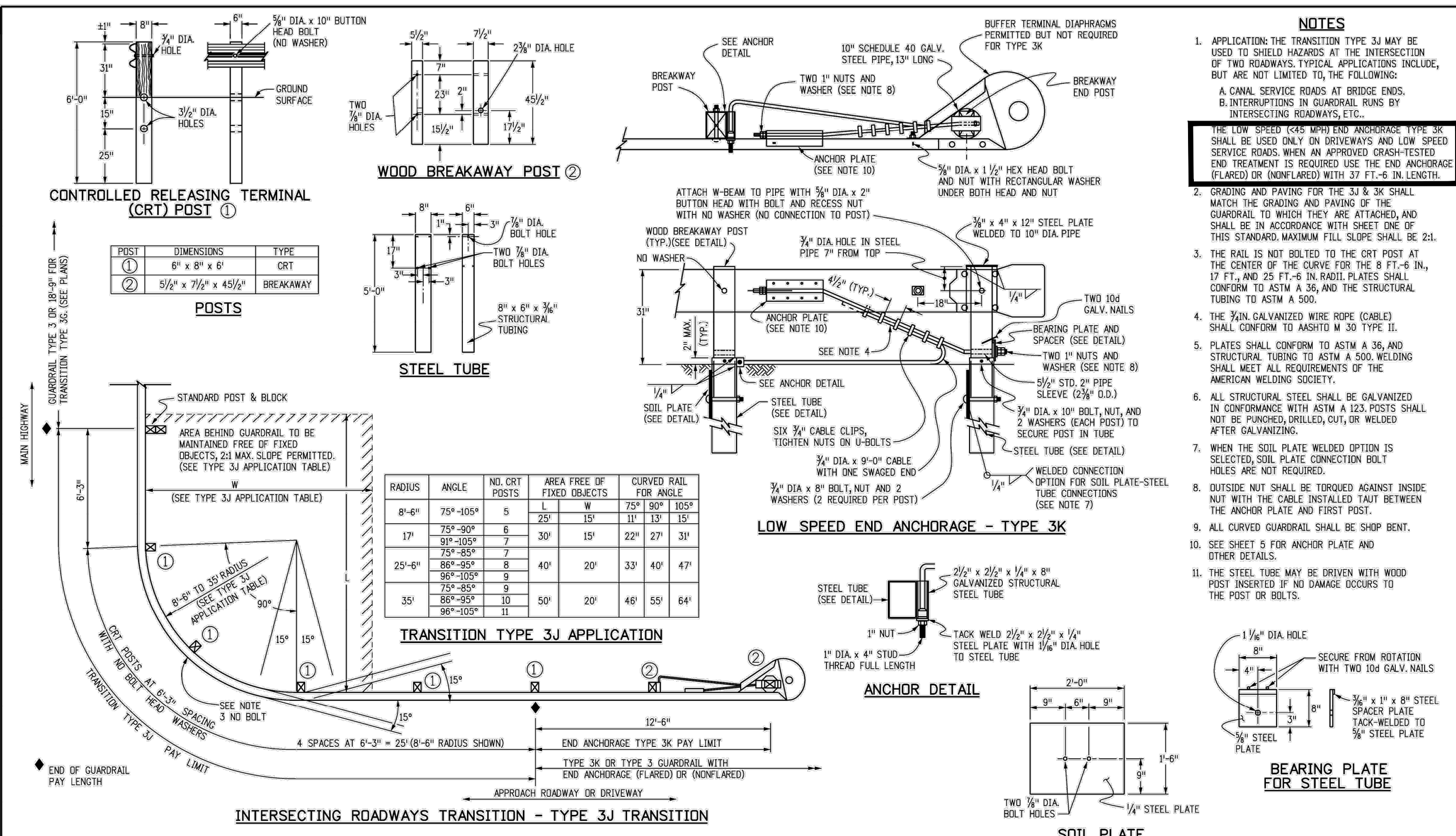
34 OF 35
CLCPK3 CLCPK3



Computer File Information	Sheet Revisions	Colorado Department of Transportation	MIDWEST	STANDARD PLAN NO.
Creation Date: 07/31/19	Date: 03/05/20	2829 West Howard Place CDOT HQ, 3rd Floor Denver, CO 80204 Phone: 303-757-9021 FAX: 303-757-9868	GUARDRAIL SYSTEM (MGS)	M-606-1
Designer Initials: JBK	Revised See Notes 3 & 5. Detailed transition section added to show the "bridge rail" detail and their location within the "M" note.	Project Development Branch	TYPE 3 W-BEAM 31 INCHES	Standard Sheet No. 11 of 19
Last Modification Date: 03/05/20			Issued by the Project Development Branch: July 31, 2019	Project Sheet Number:
Detailer Initials: LTA				
CAD Ver: MicroStation V8 Scale: Not to Scale Units: English				



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Designer Initials: JBK	Revised See Notes 3 & 5. Detailed transition section added to show the "bridge rail" detail and their location within the "M" note.	Project Development Branch	TYPE 3 W-BEAM 31 INCHES	Standard Sheet No. 17 of 19
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Civil Engineering | Surveying & Geospatial Services | Construction Management

Water Resource Management

DATE: _____

REVISIONS:

NO.	DATE	DESCRIPTION
1	12/01/2021	ADD BRIDGE APPROACH GUARDRAIL
2	10/14/2021	APPROVED PLANS
3	10/14/2021	APPROVED PLANS
4	08/12/2021	THIRD SUBMITTAL - RESPOND TO TOP COMMENTS
5	06/18/2021	SECOND SUBMITTAL - RESPOND TO TOP COMMENTS

COMPARK VILLAGE SOUTH, F1 - BELFORD AVE SITE PLAN AND GUARDRAIL DETAILS

TOWN OF PARKER, COLORADO

GUARDRAIL DETAILS

FOR REVIEW - NOT FOR CONSTRUCTION

PROJ. MGR.: GHT

PROJ. ASSOC.: ASD

DRAWN BY: JMP

DATE: 03/15/2021

35 OF 35

CLCPK3 CLCPK3

PROFESSIONAL ENGINEER
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