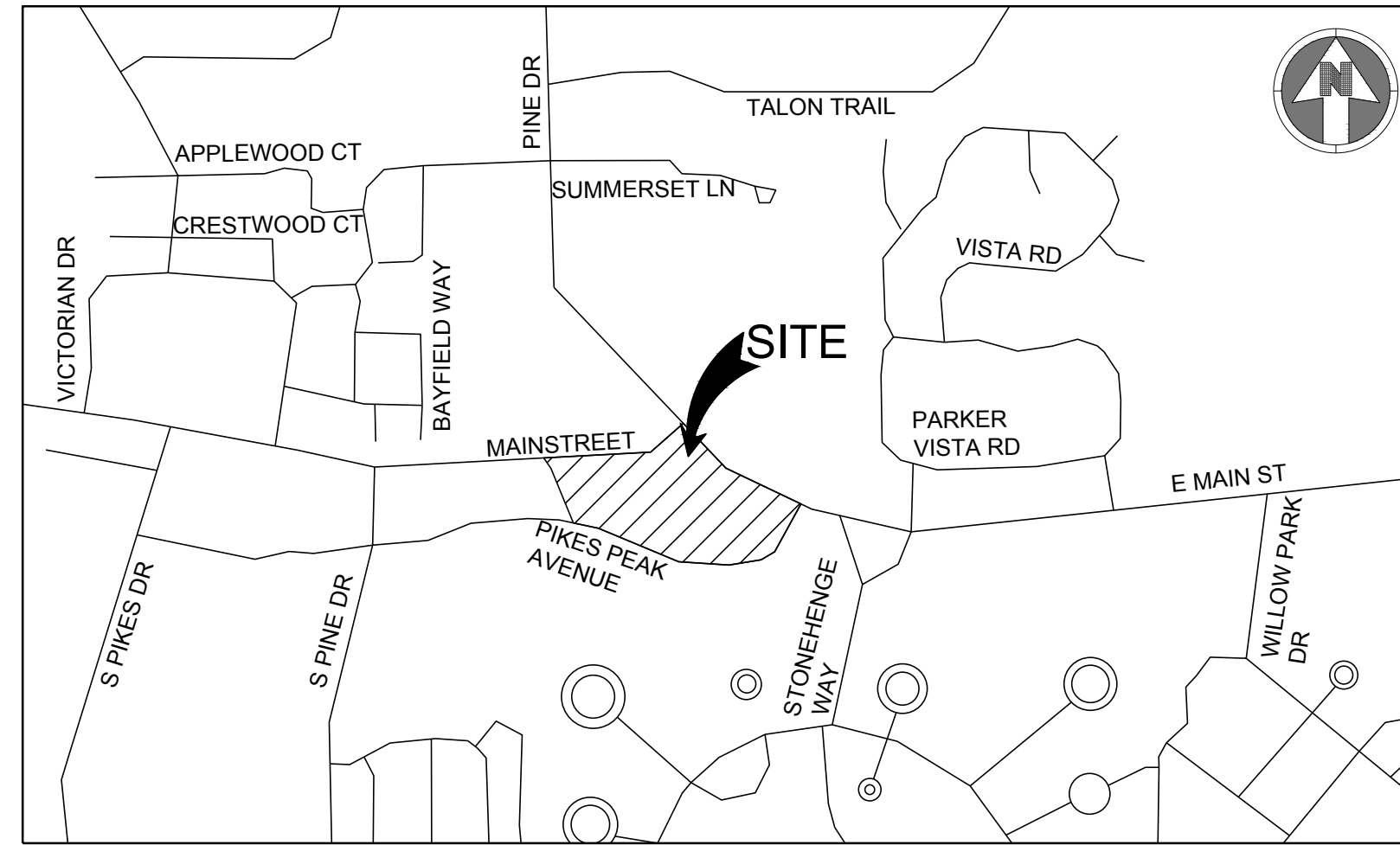


CONSTRUCTION PLANS  
FOR  
**PARKER TOWN HALL**

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED  
FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



VICINITY MAP  
NTS

Sheet Number	Sheet Title
C-000	COVER SHEET
C-001	CIVIL NOTES AND LEGEND
C-021	SITE PLAN
C-111	DEMOLITION PLAN
C-121	HORIZONTAL CONTROL PLAN
C-122	HORIZONTAL CONTROL DATA
C-211	GRADING PLAN
C-240	EROSION CONTROL COVER SHEET
C-241	INITIAL EROSION CONTROL PLAN
C-242	INTERIM EROSION CONTROL PLAN
C-243	FINAL EROSION CONTROL PLAN
C-244	OFF-SITE EROSION CONTROL PLAN
C-251	EROSION CONTROL DETAILS
C-252	EROSION CONTROL DETAILS
C-253	EROSION CONTROL DETAILS
C-254	EROSION CONTROL DETAILS
C-255	EROSION CONTROL DETAILS
C-256	EROSION CONTROL DETAILS
C-257	EROSION CONTROL DETAILS
C-258	EROSION CONTROL DETAILS
C-259	EROSION CONTROL DETAILS
C-311	UTILITY PLAN
C-331	STORM PLAN AND PROFILES
C-332	STORM PLAN AND PROFILES
C-333	STORM PLAN AND PROFILES
C-411	PAVING PLAN
C-421	SIGNAGE AND STRIPING PLAN
C-451	SITE DETAILS
C-452	SITE DETAILS
C-453	SITE DETAILS
C-454	SITE DETAILS

**ENGINEER'S STATEMENT**

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.



MEGAN HUERTLER VOGT  
REGISTERED PROFESSIONAL ENGINEER  
COLORADO NO. 47666

**S.A. MIRO INC.**  
CONSULTING ENGINEERS  
4582 South Ulster Street  
Suite 750, Denver, CO 80237  
303-741-3737  
www.samiro.com

Know what's below.  
**Call** before you dig.

NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **COVER SHEET**

FILE PATH: C:\Users\sthr\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C000-COVR.dwg C000 - 2/5/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-000**

**CONTACTS:**

OWNER'S REPRESENTATIVE	ENGINEER	SURVEYOR
TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546	S.A. MIRO INC. MEGAN H. VOGT 4582 SOUTH ULSTER STREET SUITE 750 DENVER, CO 80237 303-741-3737	AZTEC CONSULTANTS, INC. DANIEL E. DAVIS 300 EAST MINERAL AVE, SUITE 31 LITTLETON, CO 80122 P: (303)-713-1898
PUBLIC UTILITIES:	SOUTH METRO FIRE DISTRICT 9195 E. MINERAL AVENUE CENTENNIAL, CO 80112 P: (720) 989-2244	PARKER WATER & SANITATION DISTRICT 18100 WOODMAN ROAD PARKER, CO 80134 P: (303) 841-4627
	TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546	

**BASIS OF BEARING:**

BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17866 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.

**BENCHMARK:**

1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTLETON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL  
ELEVATION=5399.27 (NAVD 88).

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER

Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_

**MASTER LEGEND:**

PROPOSED		EXISTING	
	PROPERTY LINE		CURB AND GUTTER
	SECTION LINE		SIDEWALK
	RIGHT OF WAY LINE		WALL
	CENTER LINE		FENCE
	CURB AND GUTTER		SIGN
	SIDEWALK		LIGHT
	WALL		TREE
	FENCE		MINOR CONTOURS
	SIGN		MAJOR CONTOURS
	LIGHT		SPOT ELEVATION
	TREE		CABLE TV LINE
	MINOR CONTOURS		COMMUNICATION LINE
	MAJOR CONTOURS		ELECTRICAL LINE
	RIDGE LINE		OVERHEAD ELECTRICAL LINE
	SPOT ELEVATION		FIBER OPTIC LINE
	CABLE TV LINE		GAS LINE
	COMMUNICATION LINE		TELEPHONE LINE
	ELECTRICAL LINE		IRRIGATION LINE
	OVERHEAD ELECTRICAL LINE		RAW WATER LINE
	FIBER OPTIC LINE		SANITARY SEWER LINE
	GAS LINE		STEAM LINE
	TELEPHONE LINE		STORM SEWER LINE
	IRRIGATION LINE		WATER LINE
	RAW WATER LINE		EASEMENT
	SANITARY SEWER LINE		INLET
	STEAM LINE		FLARED END SECTIONS
	STORM SEWER LINE		CLEANOUT
	WATER LINE		FIRE HYDRANT
	EASEMENT		VALVE
	INLET		PEDESTAL
	FLARED END SECTIONS		UTILITY CROSSING
	CLEANOUT		MANHOLES
	FIRE HYDRANT		LAMBS TONGUE
	VALVE		ROOF DRAIN
	WATER FITTINGS		OVERFLOW
	PEDESTAL		
	UTILITY CROSSING		
	MANHOLES		
	LAMBS TONGUE		
	ROOF DRAIN		
	OVERFLOW		

**ABBREVIATIONS:**

BFV	BUTTERFLY VALVE	PCC	POINT OF COMPOUND CURVATURE
BOV	BLOW OFF VALVE	PCR	POINT OF CURB RETURN
BW	BOTTOM OF WALL @ FINISHED GRADE	PI	POINT ON INTERSECTION
CL	CENTERLINE	PRC	POINT OF REVERSE CURVATURE
CMP	CORRUGATED METAL PIPE	PT	POINT OF TANGENCY
EJ	EXPANSION JOINT	PVC	POLYVINYL CHLORIDE
ELEC.	ELECTRIC	RCP	REINFORCED CONCRETE PIPE
ELEV.	ELEVATION	RCB	REINFORCED CONCRETE BOX
EX	EXISTING	RL	RIDGE LINE
FES	FLARED END SECTION	ROW	RIGHT OF WAY
FG	FINISHED GRADE	RT	RIGHT
FS	FINISHED SURFACE	SD	STORM DRAINAGE SEWER
FL	FLOWLINE	SS	SANITARY SEWER
GB	GRADE BREAK	STA:	STATION
HP	HIGH POINT	STRM	STORM SEWER
FH	FIRE HYDRANT	TOF	TOP OF FLANGE
INV.	INVERT	TOP	TOP OF PIPE
IRR	IRRIGATION	TW	TOP OF WALL @ FINISHED GRADE
KB	KICK BLOCK	TYP	TYPICAL
LF	LINEAR FEET	VCP	VITRIFIED CLAY PIPE
LP	LOW POINT	VPC	VERTICAL POINT OF CURVATURE
LT	LEFT	VPI	VERTICAL POINT OF INTERSECTION
ME	MATCH EXISTING	VPT	VERTICAL POINT OF TANGENCY
MH	MANHOLE	WATR	WATER LINE
PC	POINT OF CURVATURE		

**TOWN OF PARKER STORM DRAINAGE INFRASTRUCTURE NOTES**

- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72" (5 COR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUTED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
- STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
- ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:
  - 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC)
  - 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
- EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS AND STRUCTURES. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
- PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

**TOWN OF PARKER DEPARTMENT OF ENGINEERING/ PUBLIC WORKS STANDARD CONSTRUCTION NOTES**

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- THE TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE TOWN INSPECTOR'S ABSENCE. HOWEVER, THE TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER DIRECTOR OF ENGINEERING/PUBLIC WORKS, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY USE PERMIT APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION."
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR. SUCH RESTORATION MAY BE REQUIRED TO BE COMPLETED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

**GENERAL NOTES:**

- THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS AT AND ADJACENT TO THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THE WORK SHOWN ON THE PLANS OR DESCRIBED IN THE SPECIFICATIONS IN A SATISFACTORY MANNER. THE CONTRACTOR SHALL FURNISH ALL MATERIALS, EQUIPMENT, TOOLS, LABOR, AND INCIDENTALS TO COMPLETE THE WORK.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AT THEIR EXPENSE TO COMPLETE ALL GRADING, UTILITY WORK AND INSTALLATION OF EROSION CONTROL MEASURES.
- THE CONTRACTOR SHALL COORDINATE WORK WITH ALL ADJACENT PROJECTS.
- DURING CONSTRUCTION IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING UTILITY LINES SHOWN ON THESE PLANS AND THOSE UTILITY LINES WHICH MAY NOT BE SHOWN ON THESE PLANS.
- EXISTING SITE INFORMATION SHOWN ON THESE DRAWINGS IS BASED UPON AVAILABLE SURVEY BY AZTEC CONSULTANTS, INC DATED 08/11/2022. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF THE COMPLETENESS OR ACCURACY OF THE EXISTING CONDITIONS PRESENTED. THE CONTRACTOR SHALL INSPECT THE SITE TO ASSESS EXISTING CONDITIONS, AND SATISFY THEMSELVES THAT THE DOCUMENTS ACCURATELY REPRESENT THE SITE CONDITIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES FOUND DURING INITIAL INSPECTIONS, OR SUBSEQUENTLY, SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY FOR RESOLUTION RELATIVE TO THE INTENDED CONSTRUCTION.
- THE EXISTING UTILITIES SHOWN ON THESE PLANS HAVE BEEN LOCATED USING FIELD SURVEY DATA LOCATING MANHOLES, WATER VALVES, ELECTRICAL EQUIPMENT, AND OTHER FEATURES FOUND AT THE SURFACE. NO SUBSURFACE EXPLORATION HAS BEEN DONE TO VERIFY EXACT LOCATION OF EXISTING UTILITIES. THE CONTRACTOR SHALL POTHOLE EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICT PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT AND DELAY RELATED TO UNVERIFIED EXISTING CONDITIONS, WHERE THE CONTRACTOR FINDS CONFLICTS OR DISCREPANCIES, THEY SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY APPROPRIATE ENTITIES FOR UTILITY LOCATES AND START OF CONSTRUCTION PRIOR TO ANY WORK BEING STARTED.
- THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH ALL AFFECTED PARTIES PRIOR TO ANY OUTAGES OR SHUTDOWN OF SERVICE.
- ALL QUANTITIES SHOWN ON THESE PLANS ARE SHOWN FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL QUANTITIES PRIOR TO CONSTRUCTION AND ORDERING OF ANY MATERIALS.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL FOLLOW/VERIFY THE INTENT OF THE DESIGN AT ALL LOCATIONS OF TIE-IN TO EXISTING FEATURES.

**DEMOLITION NOTES:**

- UNLESS OTHERWISE NOTED, EXISTING UTILITIES AND STRUCTURES SHALL REMAIN AND BE PROTECTED TO REMAIN.
- THE CONTRACTOR SHALL PROTECT ALL ADJACENT IMPROVEMENTS (BUILDINGS, ROADWAYS, FENCES, PARKING LOTS, UTILITIES, ETC.) FROM DAMAGE AND EROSION. ALL DISTURBED AREAS OUTSIDE OF THE LIMITS OF WORK SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.

**HORIZONTAL CONTROL NOTES:**

- BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.
- THE CONTRACTOR SHALL VERIFY THAT BUILDING GRID LOCATIONS ARE CONSISTENT WITH LATEST ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.

**PAVING NOTES:**

- THE CONTRACTOR SHALL ADJUST EXISTING AND PROPOSED MANHOLES, WATER VALVES AND OTHER SURFACE APPURTENANCES WITHIN THE LIMITS OF WORK TO FINISHED GRADE.
- THE CONTRACTOR SHALL REFER TO ARCHITECTURAL/LANDSCAPE PLANS FOR CONCRETE SCORING PATTERNS, COLOR AND FINISHING.
- PROPOSED PAVEMENT DESIGN IS BASED UPON THE GEOTECHNICAL REPORT BY KUMAR & ASSOCIATES DATED 01/17/2023. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF PAVEMENT PERFORMANCE.

**UTILITY NOTES:**

- LOCATION OF ALL EXISTING UTILITIES (PRIVATE AND PUBLIC) SHALL BE IDENTIFIED AND VERIFIED BY THE CONTRACTOR PRIOR TO MOBILIZATION, CONSTRUCTION AND ORDERING OF MATERIALS. IF ANY CONFLICTS OR DISCREPANCIES ARE FOUND THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY. THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT, REPAIR AND DELAYS RELATED TO ANY AND ALL UNVERIFIED EXISTING CONDITIONS.
- ALL UTILITY LENGTHS AND QUANTITIES SHOWN ON THESE PLANS ARE FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND QUANTITIES PRIOR TO ORDERING OF ANY MATERIALS.
- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
- WHERE FLARED END SECTIONS ARE REQUIRED, THE LENGTH OF PIPE INCLUDES THE FLARED END SECTION.
- ALL UTILITY WORK SHALL BE COMPLETED PRIOR TO ANY PAVING.
- CONTRACTOR SHALL SURVEY THE LOCATION AND ELEVATION OF ALL INSTALLED UTILITIES PRIOR TO TRENCH BACKFILL, AND PROVIDE SURVEY INFORMATION TO THE ENGINEER UPON COMPLETION OF THE PROJECT FOR AS-BUILT DOCUMENTATION.

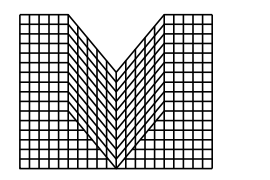
**GRADING NOTES:**

- SITE GRADING (EXCAVATION, EMBANKMENTS, AND COMPACTION) SHALL CONFORM TO THE RECOMMENDATIONS OF THE LATEST GEOTECHNICAL REPORT FOR THIS PROPERTY.
- SPOT ELEVATIONS MAY BE ABBREVIATED FOR CLARITY. REFER TO THE ADJACENT CONTOUR LABELS FOR FULL ELEVATIONS.
- MAXIMUM PROPOSED SLOPES SHALL BE 4:1 UNLESS NOTED OTHERWISE.
- SPOT ELEVATIONS ARE TO FLOWLINE UNLESS NOTED OTHERWISE.
- PROPOSED GRADING SHALL MEET AND MATCH EXISTING GRADE AT LIMITS OF DISTURBANCE.
- PROPOSED GRADING AS SHOWN REFLECTS PLANNED FINISHED GRADE. THE CONTRACTOR SHALL ACCOUNT FOR PAVEMENT DEPTH, SUBGRADE REQUIREMENTS, OVEREXCAVATION, TOPSOIL, AND ANY PLANNED SURFACING CONSTRUCTION.
- MAXIMUM TEMPORARY SLOPES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEER'S RECOMMENDATIONS AND IN COMPLIANCE WITH OSHA REQUIREMENTS.
- THE CONTRACTOR SHALL CONFORM TO OVEREXCAVATION REQUIREMENTS DETAILED WITHIN THE PROJECT SPECIFICATIONS AND GEOTECHNICAL REPORT. OVEREXCAVATION REQUIREMENTS ARE NOT DEPICTED ON THE CIVIL PLANS.

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Town of Parker, Director of Engineering/Public Works Date



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CONSULTING ENGINEERS

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Suite 750, Denver, CO 80237  
303-741-3737  
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Know what's below.  
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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
NO.	NO.
1	2

**PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138  
CIVIL NOTES AND LEGEND**

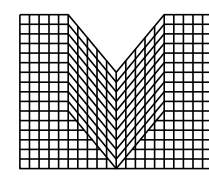
PROJECT:  
DRAWING TITLE:  
FILE PATH: C:\Users\rdh\OneDrive\Documents\S.A. Miros\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C000-COVR.dwg C001 - 2/6/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:

**C-001**

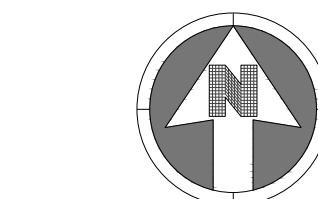


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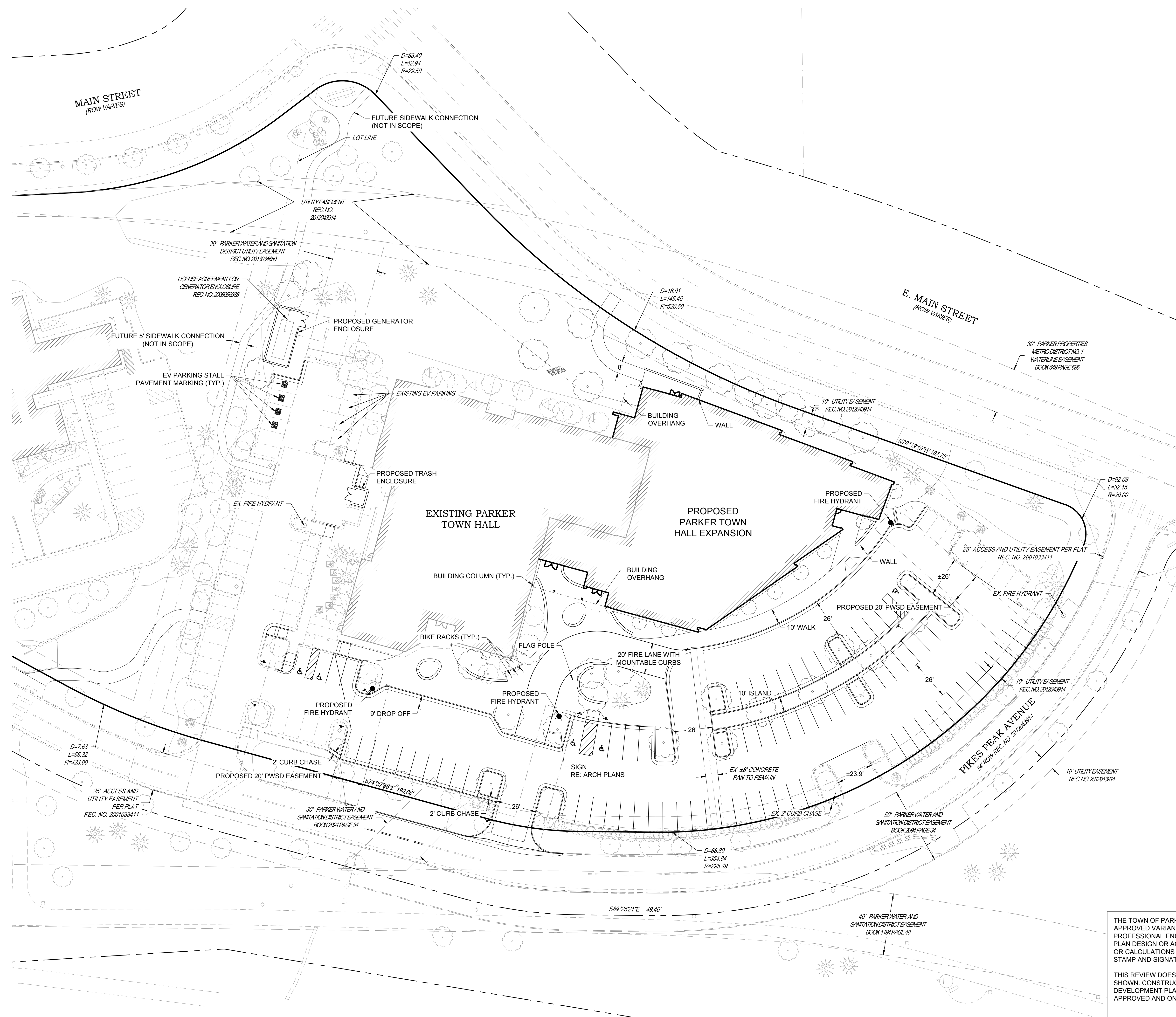


30 0 30

SCALE: 1" = 30'

NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- ADD ALTERNATE 1 - SEAL COAT EXISTING ASPHALT AND RESTRIPE PARKING LOT.
- ADD ALTERNATE 2 - ADD ADDITIONAL PEDESTRIAN CROSSING INCLUDING PUSH BUTTON SIGNAL AND RAISED CROSSWALK ON PIKES PEAK AVENUE.
- A SITE INSPECTION IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION.
- NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THE CURB GUTTER AND FIRST LIFT OF ASPHALT ARE INSTALLED



DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

NO.	DESCRIPTION
1	
2	

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

DRAWING TITLE: SITE PLAN

FILE PATH: C:\Users\sthd\CAAC\Docs\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C021-SITE-PLAN.dwg C021 - 2/5/2024

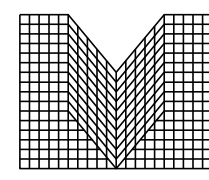


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER: C-021

THE TOWN OF PARKER CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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11/03/2023

02/05/2024

DESCRIPTION

FIRST TOWN SUBMITTAL

SECOND TOWN SUBMITTAL

NO.

1

2

PROJECT

PARKER TOWN HALL EXPANSION

20120 MAIN STREET

PARKER, CO 80138

DRAWING TITLE

DEMOLITION PLAN

FILE PATH: C:\Users\mhd\CAAC\Docs\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C111-DEM PLAN.dwg C111 - 2/5/2024

DESIGNED BY:

RJH

DRAWN BY:

DEI

CHECKED BY:

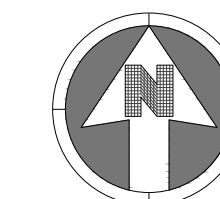
MHV

MIRO JOB NO.

22139

DRAWING NUMBER:

C-111



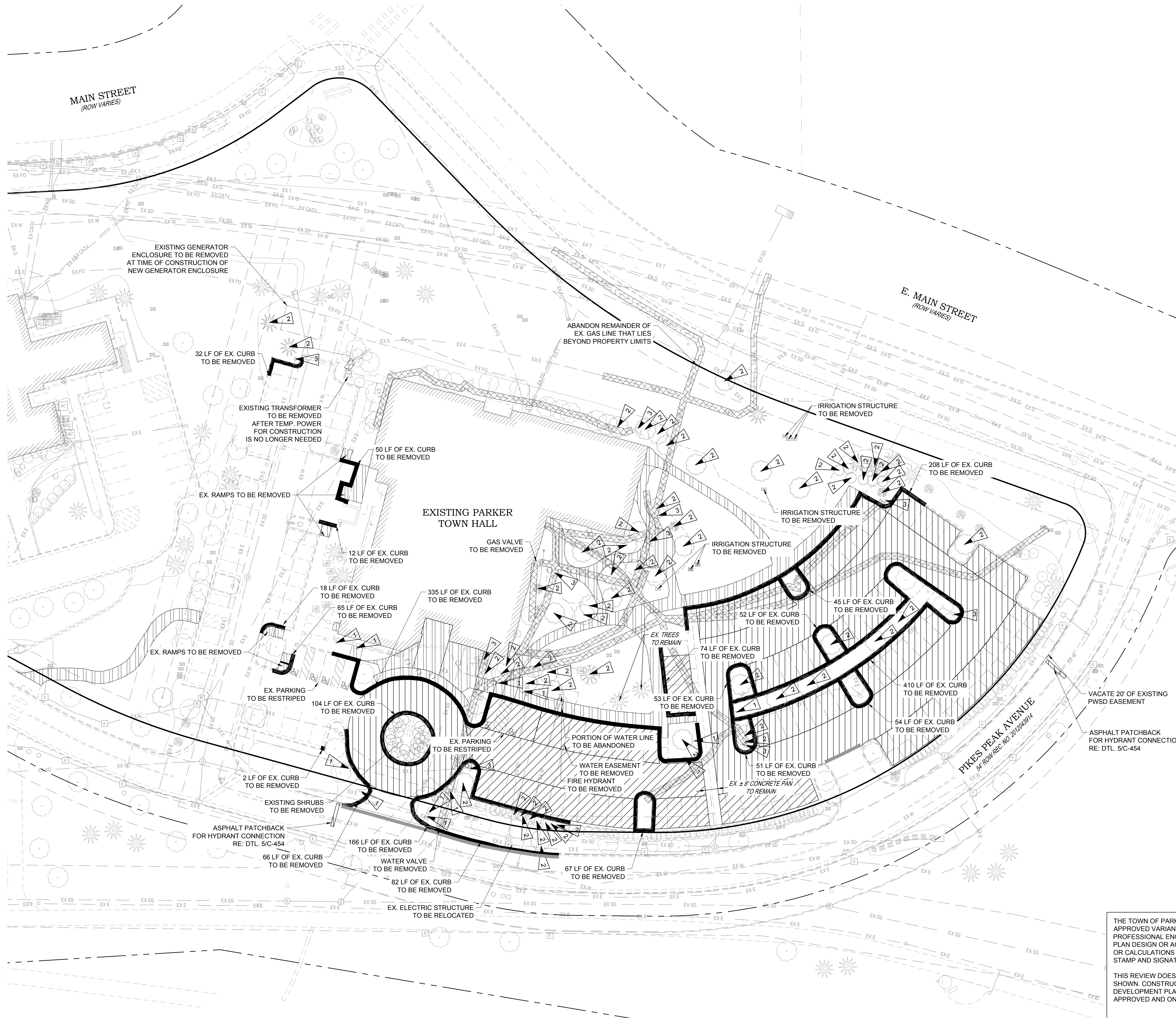
30 0 30  
SCALE: 1" = 30'

NOTES:

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- A SITE INSPECTION IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION. NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THAT CURB, GUTTER, AND FIRST LIFT OF ASPHALT ARE INSTALLED

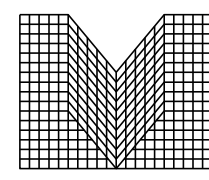
LEGEND:

- UTILITY TO BE REMOVED
- CURB AND GUTTER REMOVAL
- CONCRETE SIDEWALK REMOVAL
- ASPHALT PAVEMENT REMOVAL
- UTILITY TO BE ABANDONED
- SAWCUT LINE
- REMOVE/ RELOCATE SIGN
- REMOVE/ RELOCATE TREE
- REMOVE/ RELOCATE LIGHT POLE



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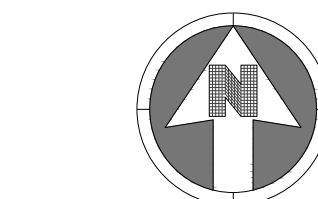


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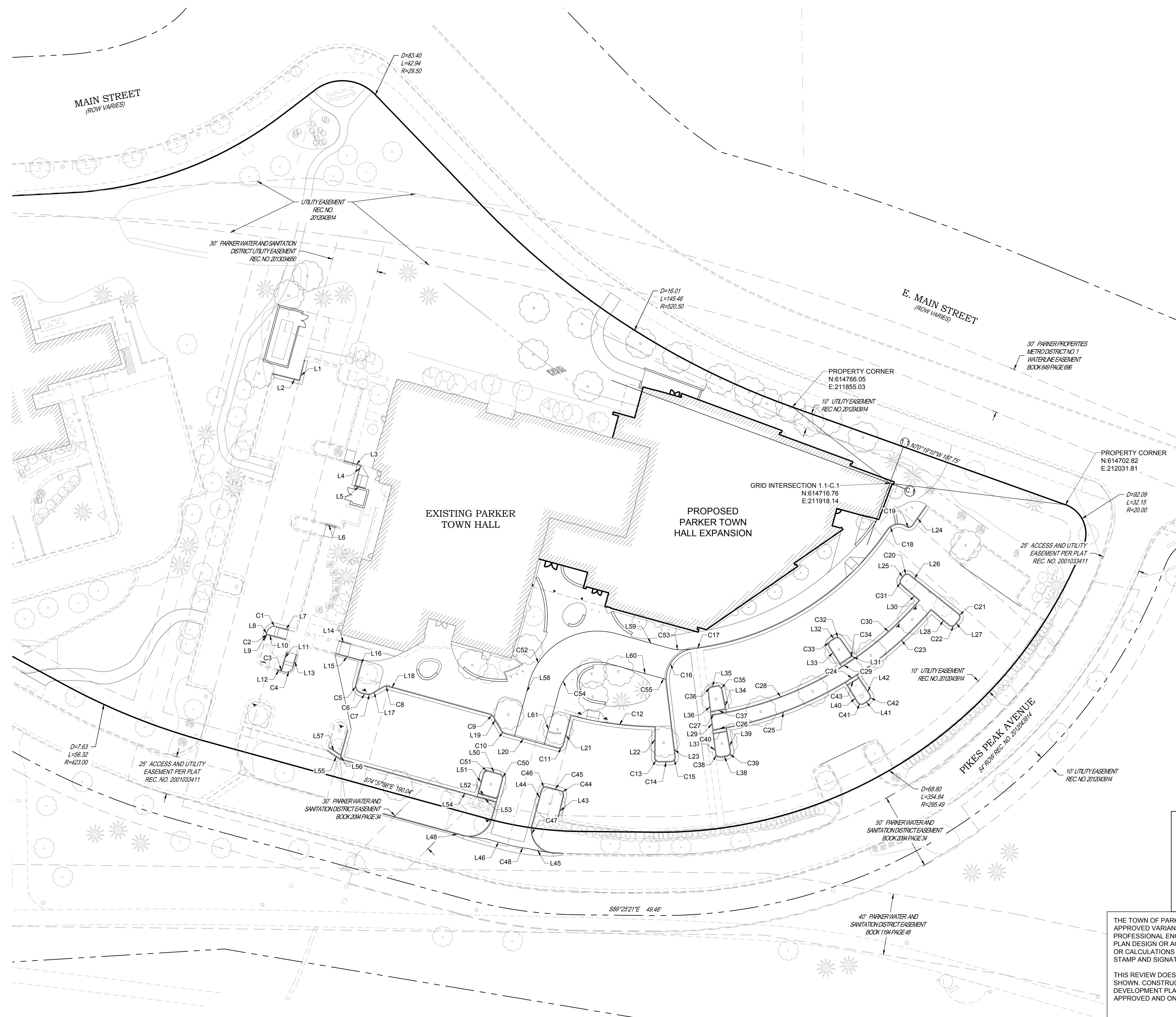


30 0 30

SCALE: 1" = 30'

NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.



**BASIS OF BEARING:**  
BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH  $00^{\circ}25'07''$  EAST, A DISTANCE OF 2,648.44 FEET.

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Town of Parker, Director of Engineering/Public Works Date

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

NO.	DESCRIPTION
1	
2	

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138**

DRAWING TITLE: **HORIZONTAL CONTROL PLAN**

FILE PATH: C:\Users\sthd\OneDrive\Documents\S. A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C121-HORZ-PLAN.dwg C121 - 2/5/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-121**

CURVE TABLE						
SEGMENT	LENGTH	RADIUS	CHORD DIRECTION	CHORD LENGTH	START N.E	END N.E
C1	7.82'	5.00'	S60°12'50"W	7.05'	614623.91, 211520.91	614620.41, 211514.79
C2	1.56'	1.00'	S60°12'50"W	1.41'	614618.76, 211515.36	614618.06, 211514.14
C3	1.60'	1.00'	N61°19'20"E	1.44'	614595.30, 211523.22	614595.99, 211524.48
C4	8.00'	5.00'	N61°19'20"E	7.18'	614594.59, 211525.54	614598.03, 211531.83
C5	8.11'	5.00'	S31°22'20"E	7.25'	614586.87, 211572.28	614580.68, 211576.05
C6	3.54'	250.00'	S78°13'50"E	3.54'	614580.68, 211576.05	614579.96, 211579.51
C7	3.58'	5.00'	N80°50'10"E	3.51'	614579.96, 211579.51	614580.52, 211582.98
C8	3.93'	5.00'	S82°50'20"W	3.83'	614584.97, 211593.75	614584.49, 211589.95
C9	3.92'	5.00'	N52°09'40"W	3.82'	614564.79, 211661.66	614567.14, 211658.64
C10	3.92'	5.00'	S52°09'40"E	3.82'	614556.75, 211666.24	614554.41, 211669.26
C11	4.75'	3.00'	N60°01'00"E	4.27'	614545.13, 211703.02	614547.26, 211706.71
C12	55.00'	227.36'	S82°16'00"E	54.87'	614566.23, 211711.68	614558.84, 211766.05
C13	4.75'	3.00'	S44°32'40"E	4.27'	614539.23, 211765.77	614536.18, 211768.77
C14	7.54'	250.00'	N89°14'40"E	7.54'	614536.18, 211768.77	614536.28, 211776.30
C15	4.99'	3.00'	N40°45'40"E	4.43'	614536.28, 211776.30	614539.64, 211779.20
C16	31.37'	20.00'	S38°04'40"W	28.25'	614609.51, 211790.89	614587.27, 211773.47
C17	157.40'	178.00'	N59°32'30"E	152.32'	614608.90, 211785.14	614686.11, 211916.44
C18	13.70'	9.00'	S77°49'50"W	12.42'	614688.72, 211928.58	614686.11, 211916.44
C19	4.52'	3.00'	N78°18'30"E	4.10'	614688.52, 211928.91	614689.35, 211932.92
C20	4.67'	3.00'	S83°55'40"W	4.21'	614657.30, 211931.19	614656.85, 211927.00
C21	4.75'	3.00'	N6°08'20"W	4.27'	614628.14, 211962.95	614632.39, 211962.49
C22	4.75'	3.00'	N85°26'50"E	4.27'	614624.81, 211956.21	614625.15, 211960.47
C23	64.00'	232.00'	N48°42'00"E	63.80'	614592.49, 211896.94	614634.60, 211944.87
C24	9.00'	232.00'	N57°42'50"E	9.00'	614587.69, 211889.33	614592.49, 211896.94
C25	83.00'	232.00'	N69°04'20"E	82.56'	614558.20, 211812.22	614587.69, 211889.33
C26	9.01'	232.00'	N80°26'10"E	9.01'	614556.70, 211803.33	614558.20, 211812.22
C27	10.36'	222.00'	N80°08'30"E	10.36'	614566.63, 211802.14	614568.41, 211812.34
C28	80.53'	222.00'	N68°24'40"E	80.09'	614568.41, 211812.34	614597.88, 211886.81
C29	9.79'	222.00'	N56°45'20"E	9.79'	614597.88, 211886.81	614603.24, 211895.00
C30	56.59'	222.00'	N48°11'20"E	56.44'	614603.24, 211895.00	614640.87, 211937.07
C31	4.67'	3.00'	S4°31'40"E	4.21'	614654.90, 211925.38	614650.70, 211925.71
C32	4.67'	3.00'	N79°05'30"W	4.21'	614615.62, 211886.49	614616.42, 211882.36
C33	4.67'	3.00'	S12°36'20"W	4.21'	614614.73, 211879.78	614610.62, 211878.86
C34	9.71'	220.00'	N56°45'20"E	9.71'	614599.57, 211885.75	614604.89, 211893.87
C35	4.67'	3.00'	N55°46'50"W	4.21'	614583.14, 211809.42	614585.51, 211805.94
C36	4.61'	3.00'	S37°07'20"W	4.17'	614584.97, 211802.85	614581.65, 211800.33
C37	10.21'	220.00'	N80°08'00"E	10.21'	614586.62, 211801.90	614570.37, 211811.95
C38	4.88'	3.00'	S53°25'50"E	4.36'	614541.90, 211805.11	614539.30, 211808.61
C39	4.75'	3.00'	N34°40'10"E	4.27'	614539.97, 211812.57	614543.48, 211815.00
C40	9.15'	234.00'	N80°26'30"E	9.15'	614554.72, 211803.57	614556.23, 211812.59
C41	4.75'	3.00'	S76°31'30"E	4.27'	614574.87, 211897.09	614573.87, 211901.24
C42	4.75'	3.00'	N11°57'00"E	4.27'	614575.81, 211904.31	614579.99, 211905.19
C43	9.08'	234.00'	N57°42'50"E	9.08'	614585.98, 211890.37	614590.83, 211898.04
C44	4.70'	3.00'	N31°34'20"W	4.23'	614514.24, 211706.84	614517.85, 211704.63
C45	8.76'	276.00'	S75°33'10"E	8.76'	614520.04, 211696.14	614517.85, 211704.63
C46	4.72'	3.00'	S60°20'00"W	4.24'	614520.04, 211696.14	614517.94, 211692.45
C47	26.79'	15.00'	S35°51'00"E	23.37'	614496.08, 211686.47	614477.14, 211700.16
C48	12.97'	96.00'	S78°34'10"E	12.96'	614482.98, 211669.48	614480.41, 211682.18
C50	4.71'	3.00'	N29°38'00"W	4.24'	614524.83, 211667.38	614528.52, 211665.29
C51	4.71'	3.00'	S60°22'00"W	4.24'	614530.39, 211658.48	614528.29, 211654.79
C52	72.25'	46.00'	S60°18'10"W	65.05'	614619.29, 211740.92	614587.07, 211684.42
C53	9.27'	26.00'	S84°54'50"E	9.22'	614609.71, 211775.96	614608.90, 211785.14
C54	40.84'	26.00'	S60°18'10"W	36.77'	614600.00, 211735.65	614581.79, 211703.71
C55	2.72'	46.00'	S76°24'00"E	2.72'	614590.42, 211770.68	614589.78, 211773.33

LINE TABLE				
SEGMENT	LENGTH	DIRECTION	STARTING N.E	ENDING N.E
L1	13.62'	S14°00'39"W	614796.36, 211539.22	614783.15, 211535.92
L2	18.21'	N75°59'21"W	614783.15, 211535.92	614787.55, 211518.26
L3	11.07'	S74°42'16"E	614731.06, 211564.58	614728.14, 211575.27
L4	13.72'	N15°37'16"E	614714.92, 211571.57	614728.14, 211575.27
L5	6.93'	S74°42'16"E	614715.40, 211571.71	614713.58, 211578.38
L6	6.83'	N74°49'40"W	614688.37, 211559.51	614690.16, 211552.92
L7	8.00'	N74°58'26"W	614621.84, 211528.63	614623.91, 211520.91
L8	1.45'	S15°24'02"W	614620.41, 211514.79	614619.01, 211514.41
L9	2.15'	S15°24'02"W	614619.01, 211514.41	614616.94, 211513.84
L10	12.02'	N74°58'26"W	614615.64, 211526.97	614618.76, 211515.36
L11	11.93'	S15°28'28"W	614607.48, 211527.67	614595.99, 211524.48
L12	4.24'	N72°49'40"W	614594.59, 211525.54	614595.84, 211521.48
L13	8.00'	S15°28'28"W	614605.74, 211533.97	614598.03, 211531.83
L14	8.00'	N74°46'21"W	614612.38, 211569.11	614614.48, 211561.91
L15	17.72'	N74°55'09"W	614601.68, 211576.27	614606.29, 211559.16
L16	15.34'	S15°04'51"W	614601.68, 211576.27	614586.87, 211572.28
L17	8.03'	N60°18'26"E	614580.52, 211582.98	614584.49, 211589.95
L18	67.29'	S74°37'56"E	614584.97, 211593.75	614567.14, 211658.64
L19	9.25'	S29°41'34"E	614564.79, 211661.66	614556.75, 211666.24
L20	35.01'	N74°37'56"W	614545.13, 211703.02	614554.41, 211669.26
L21	19.60'	S14°39'52"W	614566.23, 211711.68	614547.26, 211706.71
L22	19.62'	N0°48'13"E	614539.23, 211765.77	614558.84, 211766.05
L23	47.97'	N6°51'27"W	614539.64, 211779.20	614587.27, 211773.47
L24	15.44'	N35°09'56"E	614689.35, 211932.92	614701.97, 211941.81
L25	2.54'	N39°42'02"E	614654.90, 211925.38	614656.85, 211927.00
L26	40.00'	N51°29'12"W	614632.39, 211962.49	614657.30, 211931.19
L27	3.89'	N39°39'17"E	614625.15, 211960.47	614628.14, 211962.95
L28	14.98'	S49°12'15"E	614634.60, 211944.87	614624.81, 211956.21
L29	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L30	15.02'	S49°06'45"E	614650.70, 211925.71	614640.87, 211937.07
L31	15.02'	N34°30'28"W	614603.24, 211895.00	614615.62, 211886.49
L32	3.09'	N56°45'22"E	614614.73, 211879.78	614616.42, 211882.36
L33	15.02'	S31°58'46"E	614610.62, 211878.86	614597.88, 211886.81
L34	15.02'	N11°11'46"W	614568.41, 211812.34	614583.14, 211809.42
L35	3.14'	N80°04'12"E	614584.97, 211802.85	614585.51, 211805.94
L36	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L37	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L38	4.01'	N80°29'03"E	614539.30, 211808.61	614539.97, 211812.57
L39	14.98'	N10°40'38"W	614543.48, 211815.00	614558.20, 211812.22
L40	14.98'	S31°10'33"E	614587.69, 211889.33	614574.87, 211897.09
L41	3.63'	N57°42'47"E	614573.87, 211901.24	614575.81, 211904.31
L42	14.98'	N33°23'54"W	614579.99, 211905.19	614592.49, 211896.94
L43	16.32'	S13°19'10"W	614514.24, 211706.84	614498.36, 211703.08
L44	22.66'	N15°18'26"E	614496.08, 211686.47	614517.94, 211692.45
L45	16.25'	S79°16'45"E	614480.41, 211682.18	614477.38, 211698.15
L46	13.07'	N74°46'42"W	614482.98, 211669.48	614486.41, 211656.87
L48	79.01'	S74°46'42"E	614509.16, 211565.62	614488.42, 211641.86
L50	7.06'	S74°37'56"E	614530.39, 211658.48	614528.52, 211665.29
L51	14.98'	N15°22'04"E	614513.84, 211650.82	614528.29, 211654.79
L52	13.06'	S74°37'56"E	614515.77, 211651.35	614512.31, 211663.94
L53	13.06'	S74°37'56"E	614513.84, 211650.82	614510.38, 211663.41
L54	92.00'	N74°37'56"W	614513.84, 211650.82	614538.23, 211562.11
L55	10.15'	N55°50'28"W	614538.23, 211562.11	614543.92, 211553.71
L56	19.51'	N15°22'04"E	614538.23, 211562.11	614557.03, 211567.28
L57	9.47'	N55°50'28"W	614540.26, 211562.67	614545.58, 211554.84
L58	35.51'	S15°18'26"W	614587.07, 211684.42	614552.82, 211675.04
L59	36.32'	N74°42'16"W	614609.71, 211775.96	614619.29, 211740.92
L60	36.32'	S74°42'16"E	614600.00, 211735.65	614590.42, 211770.68
L61	35.53'	S15°18'26"W	614581.79, 211703.71	614547.52, 211694.33

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Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_



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Suite 750, Denver, CO 80237  
303-741-3737  
www.samiro.com



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NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

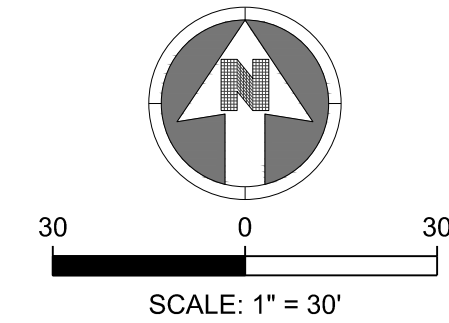
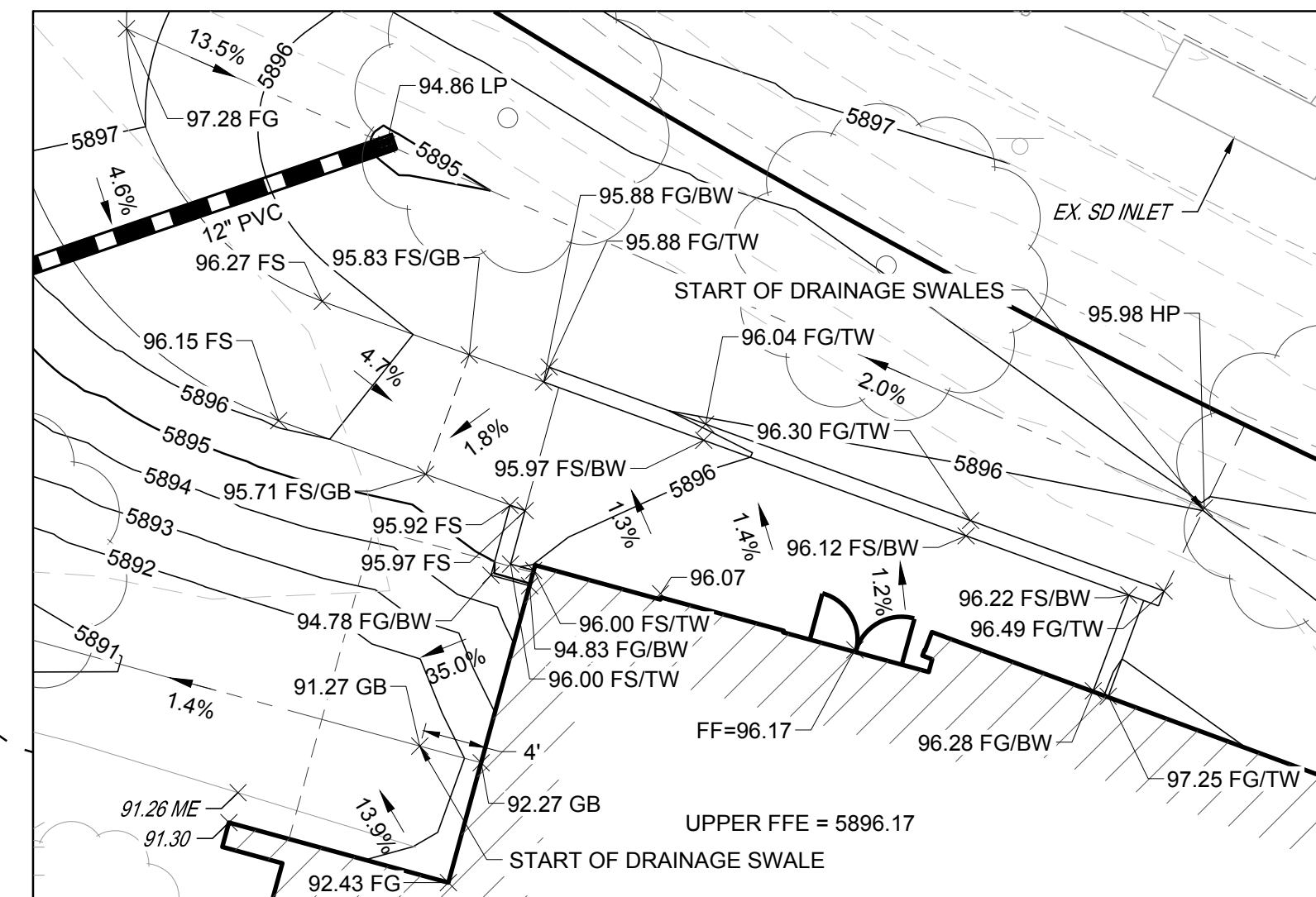
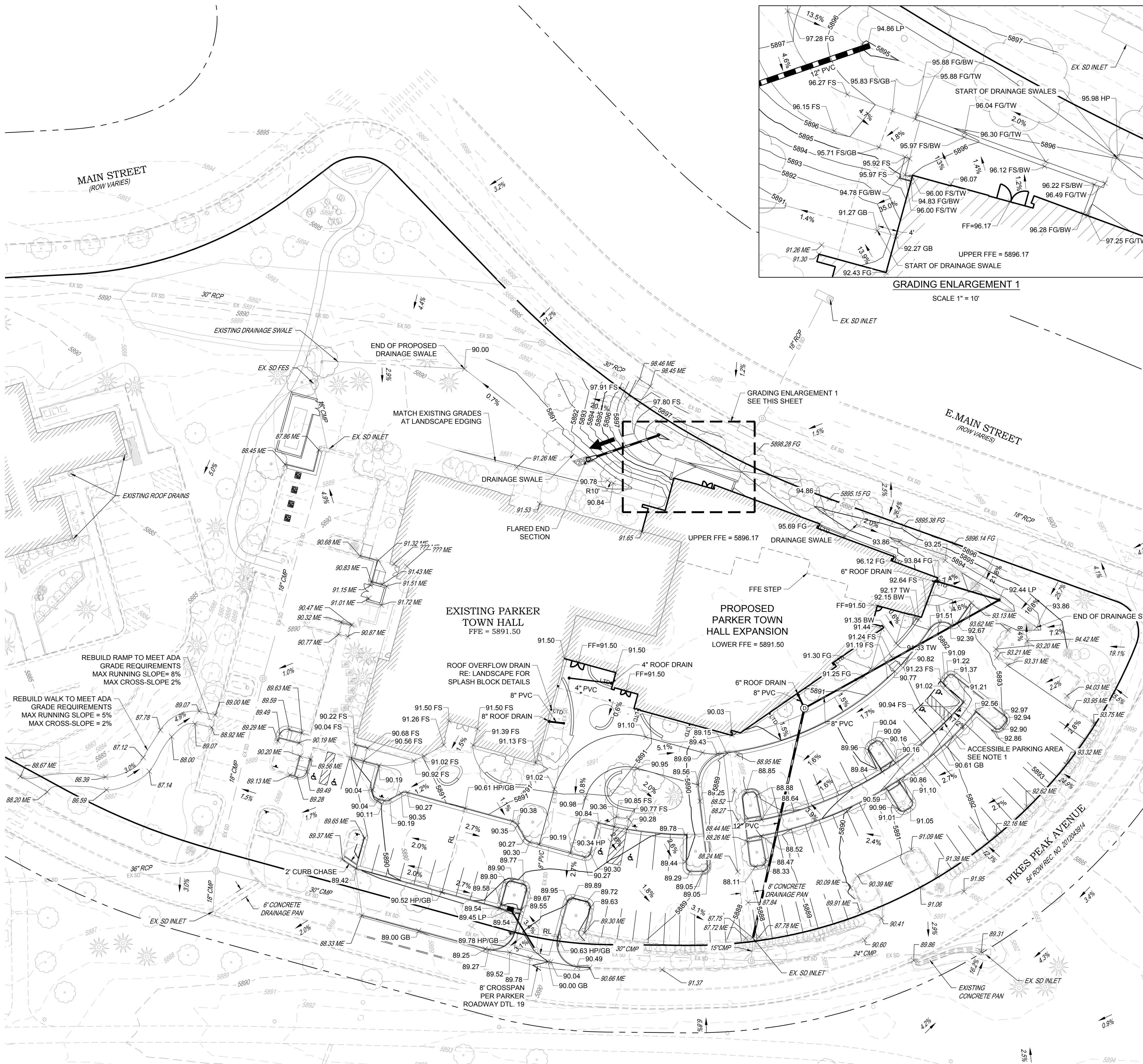
DRAWING TITLE: **HORIZONTAL CONTROL DATA**

FILE PATH: C:\Users\sthd\Documents\S.A. Miro, Inc.\Civil\Jobs\21139 Parker Town Hall\05 CAD\Plans and Details\C121-HORIZ-PLAN.dwg C122 - 2/5/2024



DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139

DRAWING NUMBER:  
**C-122**



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
  - MAXIMUM GRADE SHALL BE 2% IN ALL DIRECTIONS AT EVERY ACCESSIBLE PARKING STALL AND ACCESS AISLE.
  - SPOT ELEVATIONS ARE TO THE FLOWLINE UNLESS OTHERWISE NOTED.
  - SPOT ELEVATIONS HAVE BEEN ABBREVIATED FOR CLARITY. REFER TO THE ADJACENT CONTOUR LABELS FOR FULL ELEVATIONS.



DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

NO.	DESCRIPTION
1	
2	

**PROJECT:** PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

**DRAWING TITLE:** GRADING PLAN

FILE PATH: C:\Users\jph\OneDrive\A. Miro, Inc\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C211-GRAD-PLAN.dwg C211 - 2/5/2024

**DESIGNED BY:** RJH  
**DRAWN BY:** DEI  
**CHECKED BY:** MHV  
**MIRO JOB NO.:** 22139  
**DRAWING NUMBER:** C-211

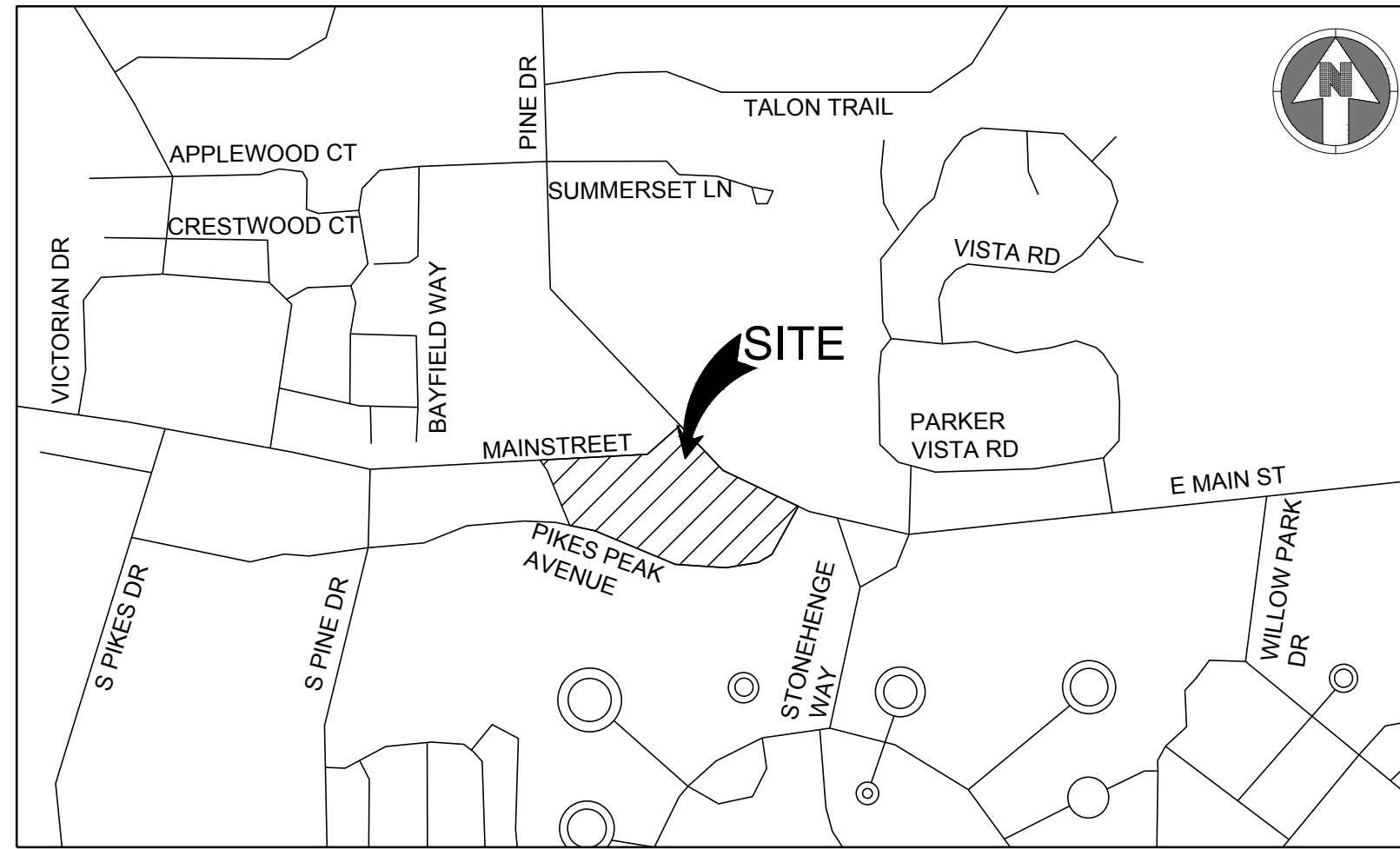
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EROSION CONTROL CONSTRUCTION PLANS  
FOR

# PARKER TOWN HALL

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED  
FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



VICINITY MAP  
NTS

Sheet Number	Sheet Title
C-240	EROSION CONTROL COVER SHEET
C-241	INITIAL EROSION CONTROL PLAN
C-242	INTERIM EROSION CONTROL PLAN
C-243	FINAL EROSION CONTROL PLAN
C-244	OFF-SITE EROSION CONTROL PLAN
C-251	EROSION CONTROL DETAILS
C-252	EROSION CONTROL DETAILS
C-253	EROSION CONTROL DETAILS
C-254	EROSION CONTROL DETAILS
C-255	EROSION CONTROL DETAILS
C-256	EROSION CONTROL DETAILS
C-257	EROSION CONTROL DETAILS
C-258	EROSION CONTROL DETAILS
C-259	EROSION CONTROL DETAILS

### ENGINEER'S STATEMENT

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.



MEGAN HUERTTER VOGT  
REGISTERED PROFESSIONAL ENGINEER  
COLORADO NO. 47666

**S.A. MIRO INC.**  
CONSULTING ENGINEERS  
4582 South Ulster Street  
Suite 750, Denver, CO 80237  
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2	SECOND TOWN SUBMITTAL	02/05/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **EROSION CONTROL COVER SHEET**

FILE PATH:



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-240**

### CONTACTS:

**OWNER'S REPRESENTATIVE**  
TOWN OF PARKER  
20120 E. MAIN STREET  
PARKER, CO 80138  
P: (303) 840-9546

**ENGINEER**  
S.A. MIRO INC.  
MEGAN H. VOGT  
4582 SOUTH ULSTER STREET  
SUITE 750 DENVER, CO 80237  
303-741-3737

**SURVEYOR**  
AZTEC CONSULTANTS, INC.  
DANIEL E. DAVIS  
300 EAST MINERAL AVE, SUITE 1  
LITTLETON, CO 80122  
P: (303)-713-1898

**PUBLIC UTILITIES:**  
**SOUTH METRO FIRE DISTRICT**  
9195 E. MINERAL AVENUE  
CENTENNIAL, CO 80112  
P: (720) 989-2244

**PARKER WATER & SANITATION DISTRICT**  
18100 WOODMAN ROAD  
PARKER, CO 80134  
P: (303) 841-4627

**TOWN OF PARKER**  
20120 E. MAIN STREET  
PARKER, CO 80138  
P: (303) 840-9546

### BASIS OF BEARING:

BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17866 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.

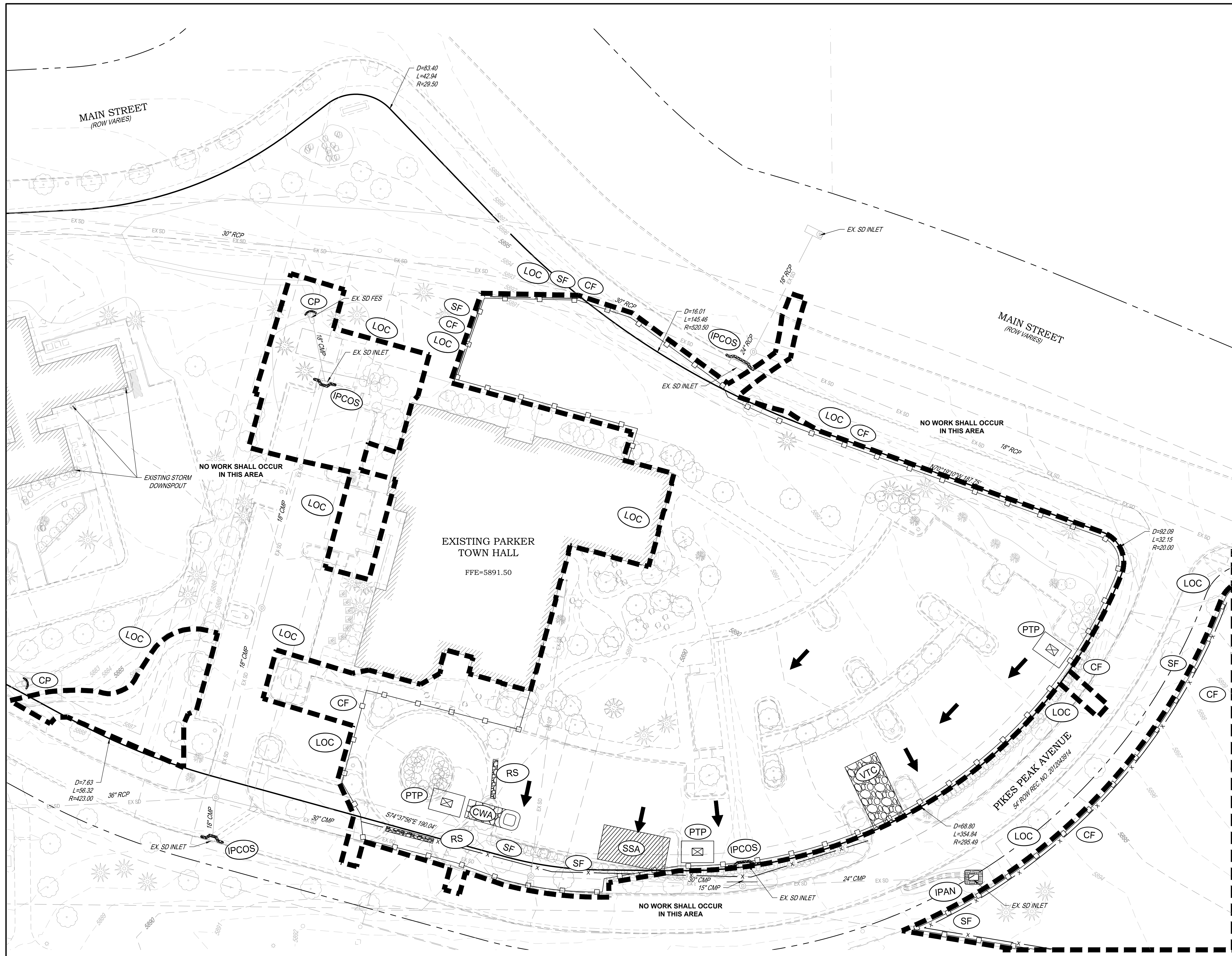
### BENCHMARK:

1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTELTON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL  
ELEVATION=5399.27 (NAVD 88).

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Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_



**NOTES:**  
 1. SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.

**LEGEND:**

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	IPCOG	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	IPCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	SEEDING, MULCHING AND CRIMPING
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION
	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL
	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW

**TOWN OF PARKER NOTES:**

- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADE AFTER TOPSOIL, SOD, PAVING ETC., EXCEPT IN THE AREA OF SOIL STOCK PILE.
- SEE TOWN OF PARKER CONSTRUCTION BEST MANAGEMENT PRACTICES.
- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ECS.
- THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
- THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
- ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
- LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
- PROVIDE SEDIMENT CONTROL LOGS (SCL) ALONG BACK OF ALL CURBS ADJACENT TO LANDSCAPE/PERVIOUS AREAS INCLUDING LANDSCAPE ISLANDS AND ALL ATTACHED PEDESTRIAN WALKS.
- ANY ONSITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION. CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.
- MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
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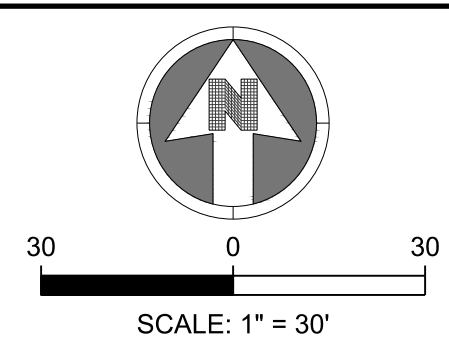
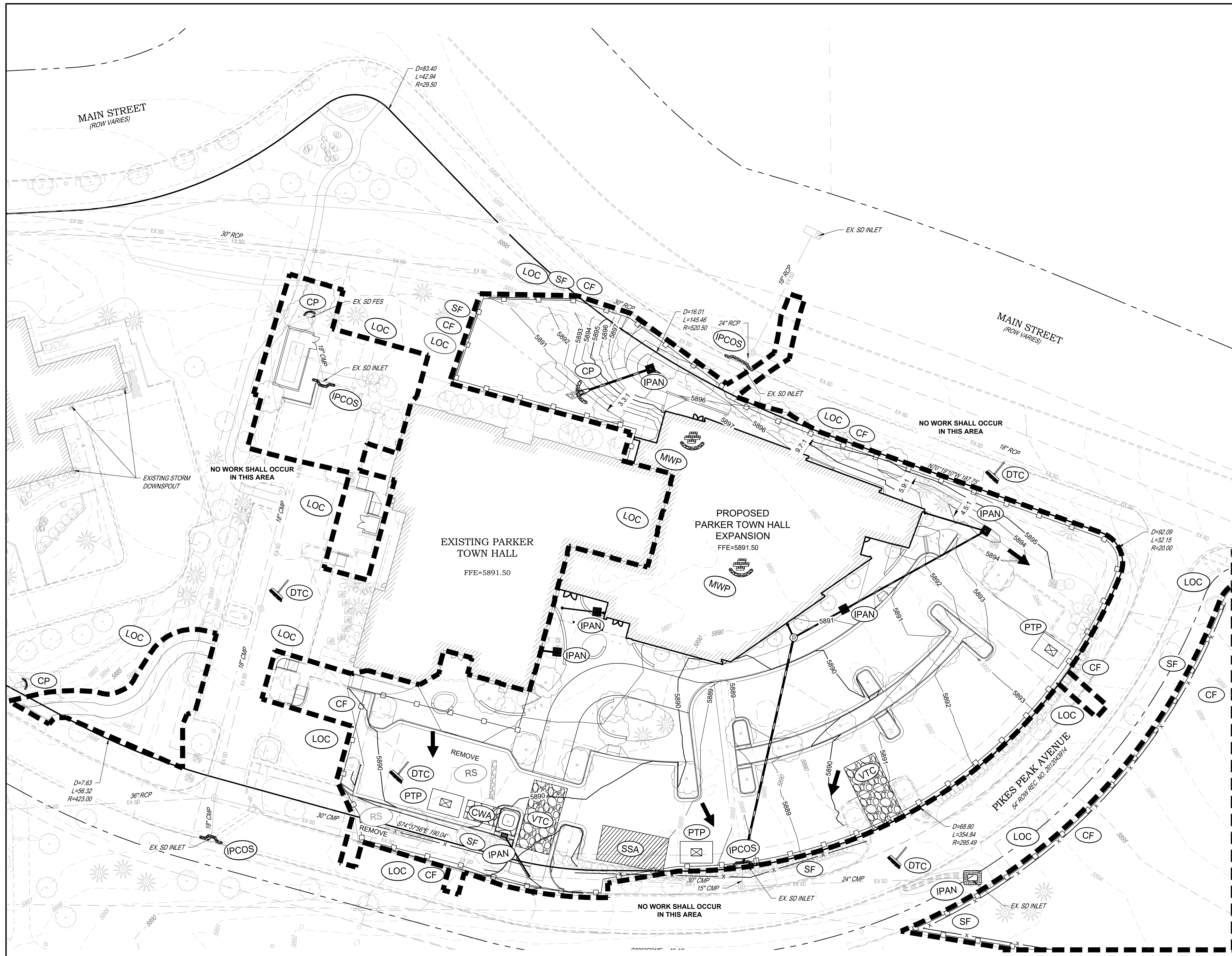
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PROJECT: PARKER TOWN HALL EXPANSION  
 20120 MAIN STREET  
 PARKER, CO 80138

DRAWING TITLE: INITIAL EROSION CONTROL PLAN

DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139

DRAWING NUMBER:  
**C-241**



**NOTES:**

- SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.
- ADDITIONAL PTP'S MAY BE NEEDED IN THE INTERIM AND FINAL PHASE.
- EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL SLOPES STEEPER THAN 4:1.

**LEGEND:**

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
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	PCOC	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	PCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
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	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
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	LOC	LIMITS OF CONSTRUCTION
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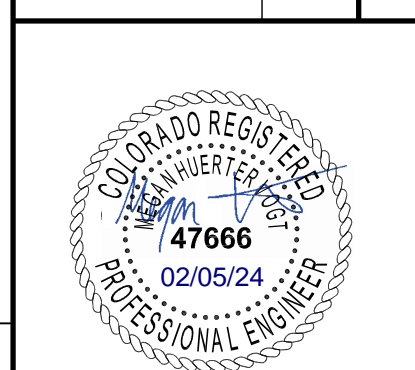
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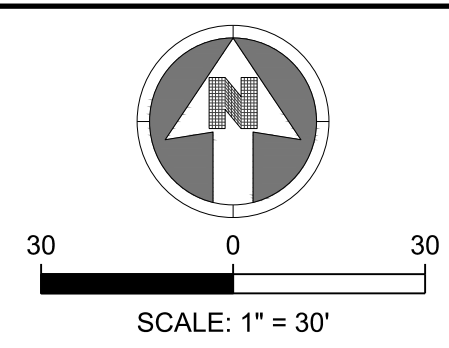
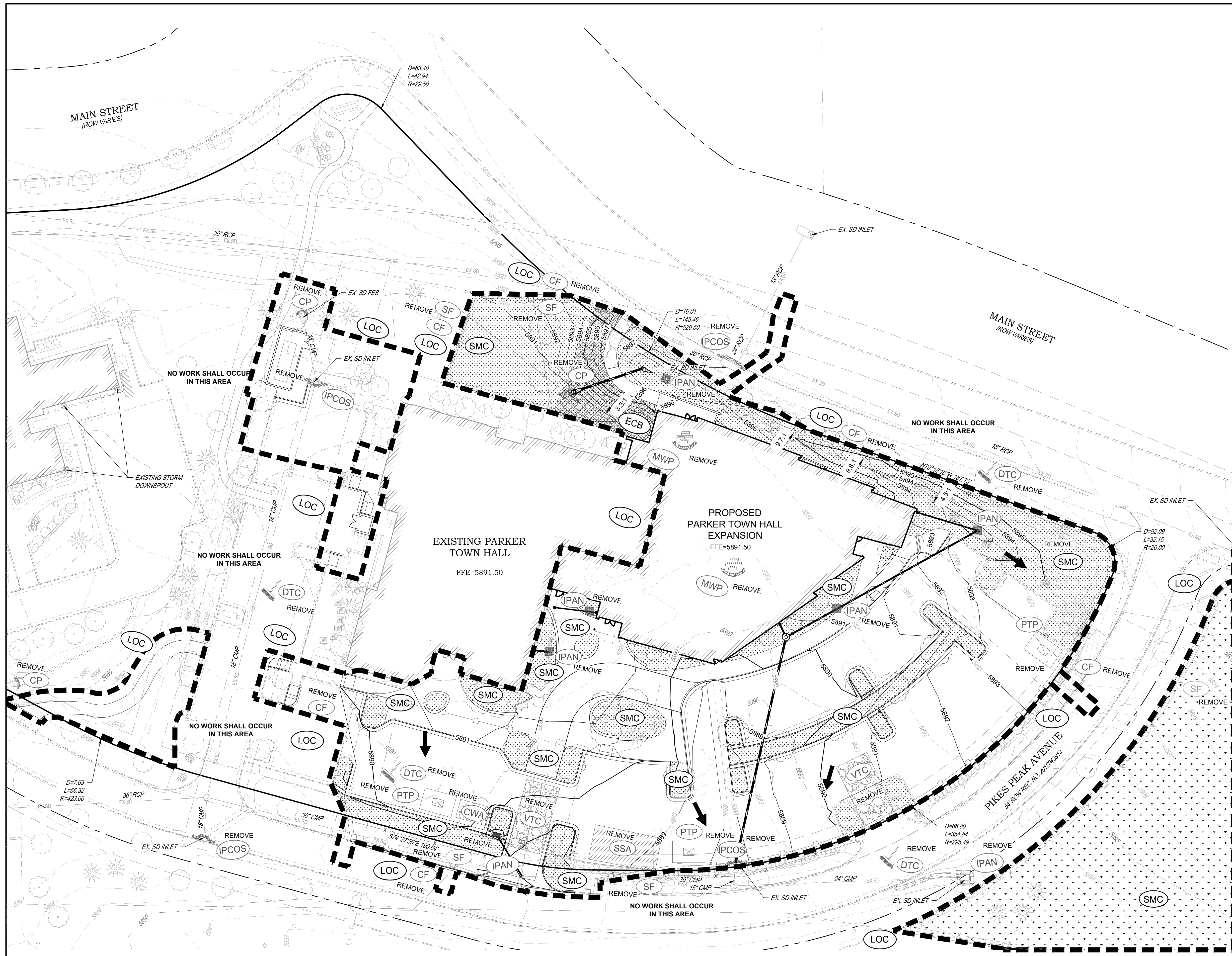
DATE	DESCRIPTION
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DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139

DRAWING NUMBER:  
**C-242**



- NOTES:**
- SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.
  - ADDITIONAL PTP'S MAY BE NEEDED IN THE INTERIM AND FINAL PHASE.
  - EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL SLOPES STEEPER THAN 4:1.

**LEGEND:**

- |  |      |  |
|--|------|--|
|  | CD   | CHECK DAM  |
|  | CF   | CONSTRUCTION FENCE                               |
|  | CP   | CULVERT PROTECTION                               |
|  | CWA  | CONCRETE WASHOUT AREA                            |
|  | D    | DEWATERING                                       |
|  | DD   | DIVERSION DITCH                                  |
|  | DP   | DETENTION POND PROTECTION                        |
|  | DTC  | DEBRIS TRASH CONTROL                             |
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|  | PCOC | INLET PROTECTION, CURB ON-GRADE, TYPE R INLET    |
|  | PCOS | INLET PROTECTION, CURB ON SUMP, TYPE R INLET     |
|  | LP   | LOT PROTECTION                                   |
|  | MWP  | MASONRY WORK PROTECTION                          |
|  | PTP  | PORTABLE TOILET PROTECTION                       |
|  | RCSC | ROUGH CUT STREET CONTROL                         |
|  | RS   | ROCK SOCK  |
|  | RSS  | ROCK SOCK IN SWALE                               |
|  | SB   | STRAW BALE                                       |
|  | SCL  | SEDIMENT CONTROL LOGS                            |
|  | SF   | SILT FENCE                                       |
|  | SMC  | SEEDING, MULCHING AND CRIMPING                   |
|  | SR   | SURFACE ROUGHING                                 |
|  | SSA  | STABILIZED STAGING AREA                          |
|  | STP  | SIDEWALK TRANSITION PROTECTION                   |
|  | TI   | TEMPORARY IRRIGATION                             |
|  | TSB  | TEMPORARY SEDIMENT BASIN                         |
|  | VTC  | VEHICLE TRACKING CONTROL                         |
|  | LOC  | LIMITS OF CONSTRUCTION                           |
|  |      | FLOW DIRECTION ARROW                             |

**TOWN OF PARKER NOTES:**

- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADE AFTER TOPSOIL, SOD, PAVING ETC., EXCEPT IN THE AREA OF SOIL STOCK PILE.
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- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ECS.
- THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
- THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
- ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
- LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
- PROVIDE SEDIMENT CONTROL LOGS (SCL) ALONG BACK OF ALL CURBS ADJACENT TO LANDSCAPE/PREVIOUS AREAS INCLUDING LANDSCAPE ISLANDS AND ALL ATTACHED PEDESTRIAN WALKS.
- ANY ONSITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION. CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.
- MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
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Town of Parker, Director of Engineering/Public Works Date

**S.A. MIRO INC.**  
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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

NO.	NO.
1	2

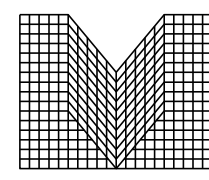
**PROJECT:** PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

**DRAWING TITLE:** FINAL EROSION CONTROL PLAN

**FILE PATH:**

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-243**



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PROJECT: PARKER TOWN HALL EXPANSION  
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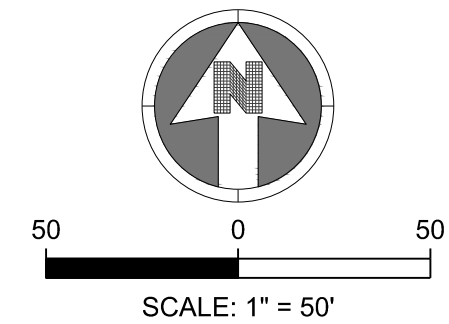
DRAWING TITLE: OFF-SITE EROSION CONTROL PLAN

FILE PATH:



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

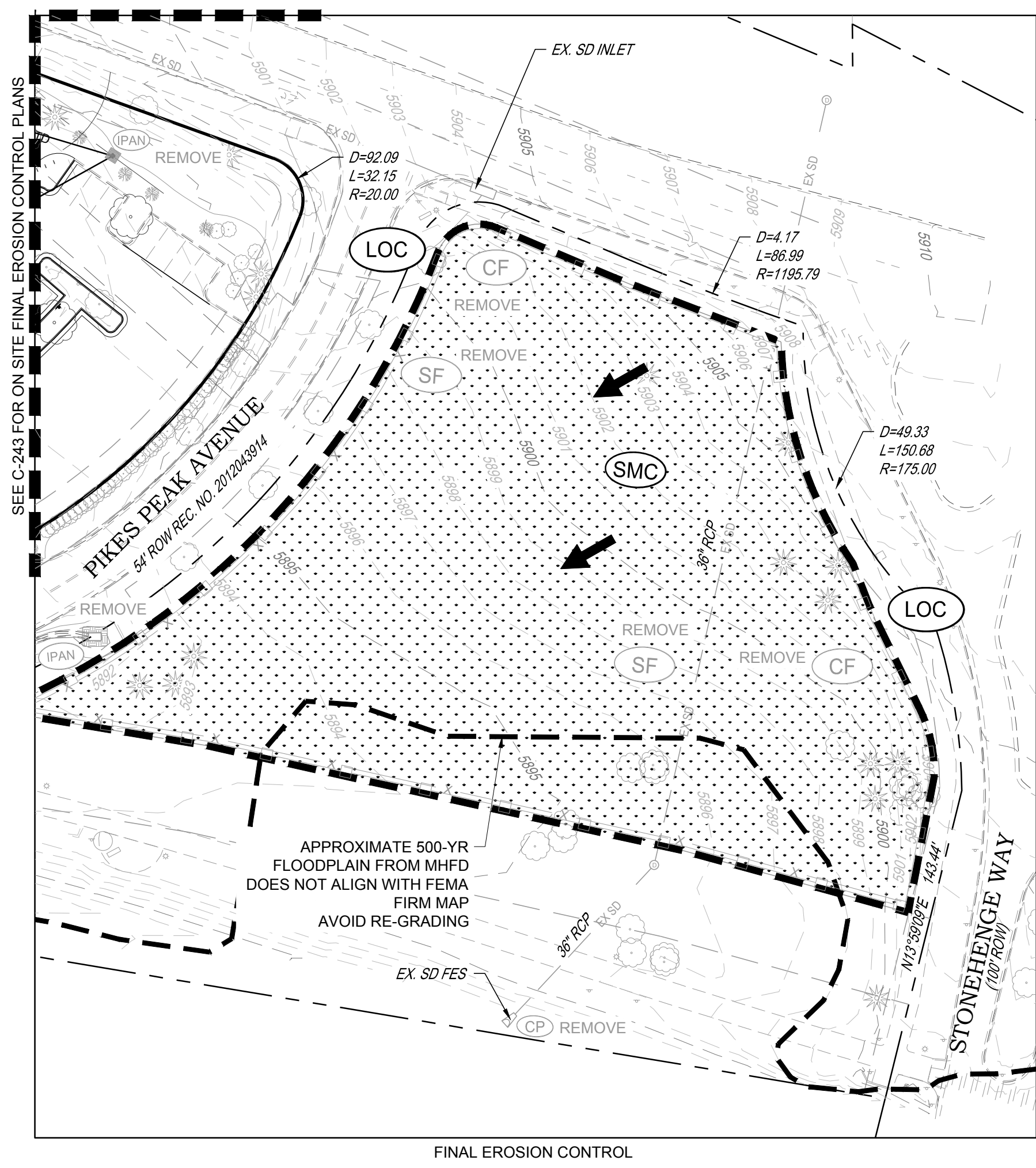
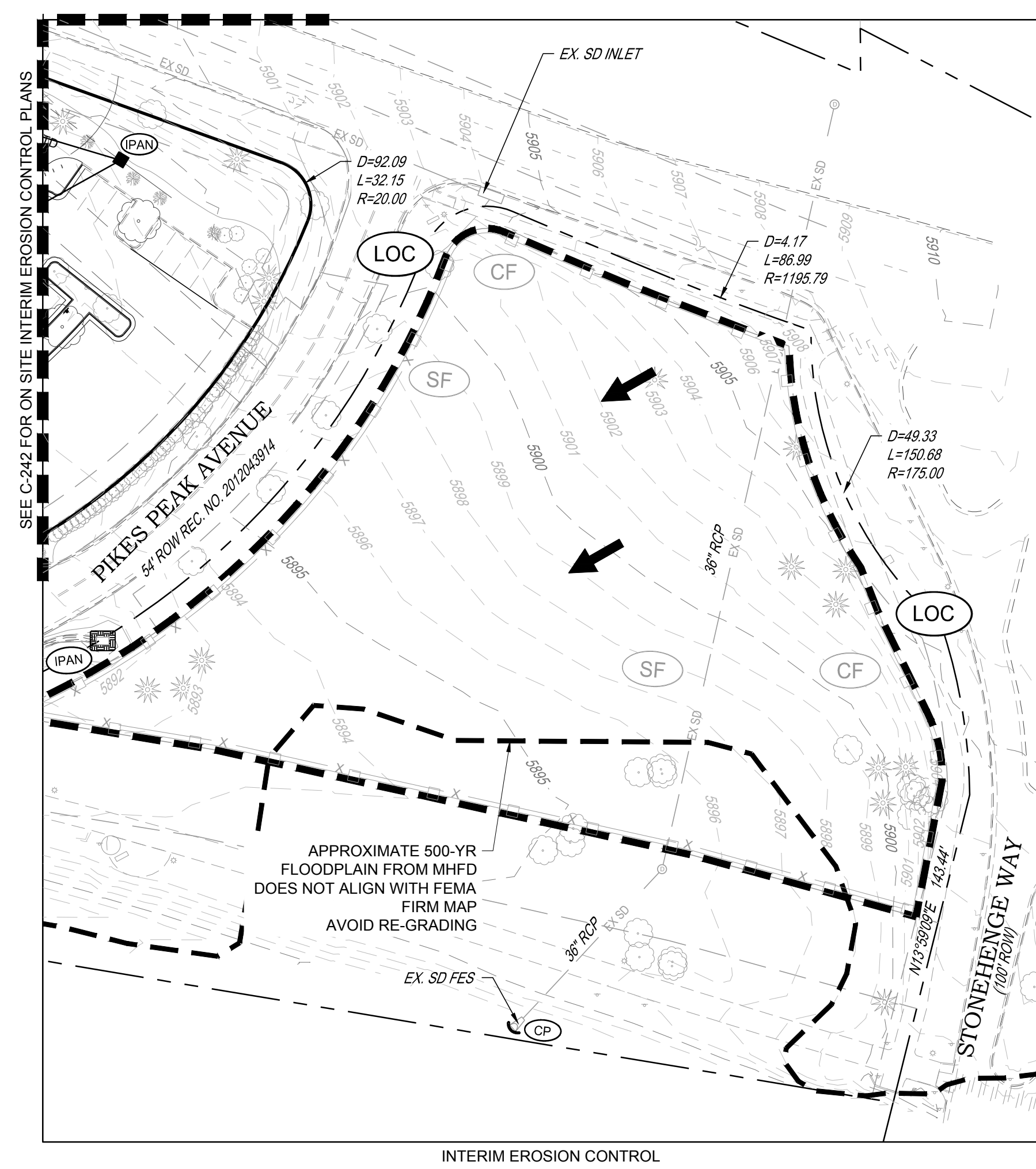
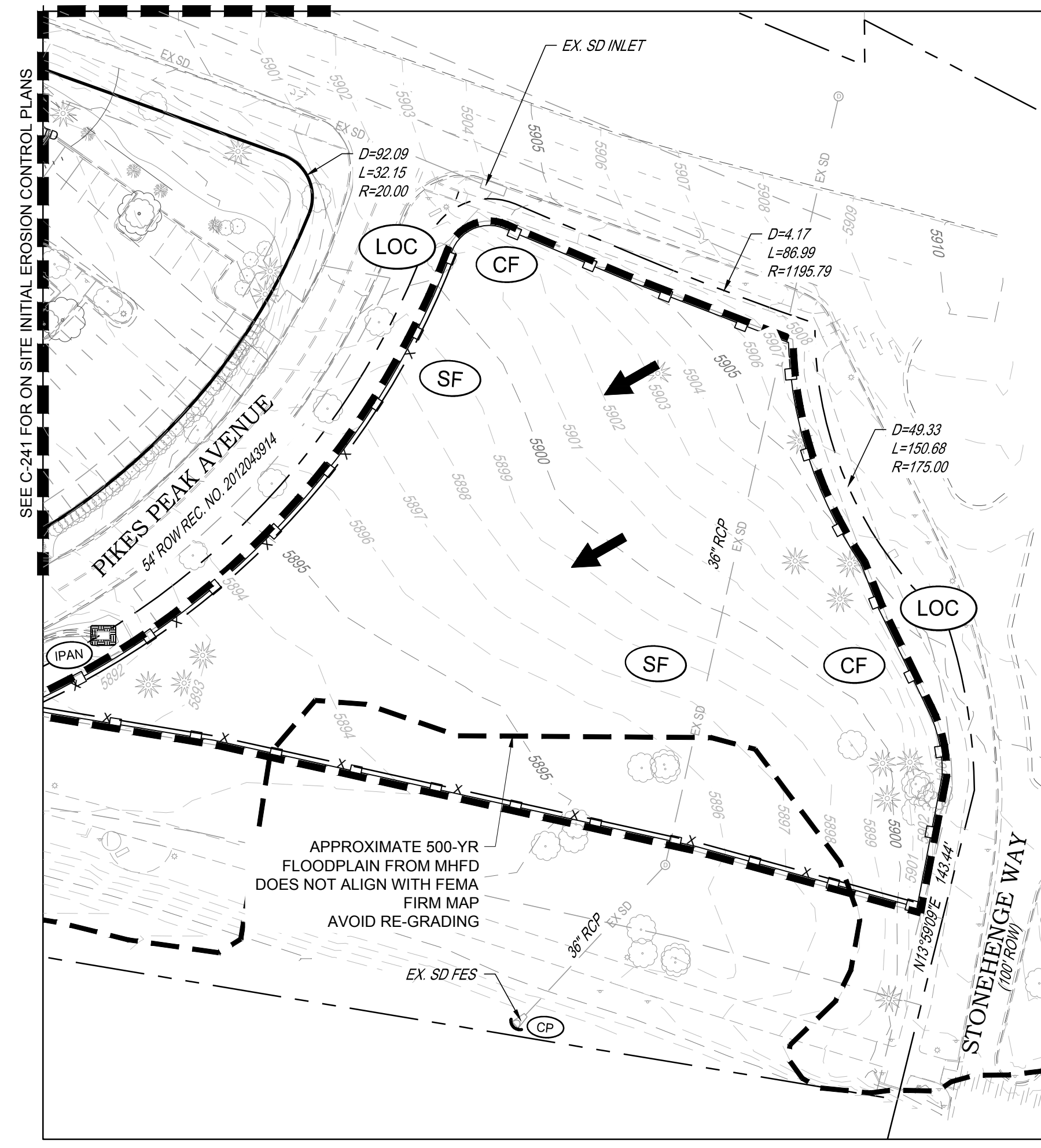
DRAWING NUMBER: C-244



NOTES:  
1. SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.

LEGEND:

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	PCOG	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	PCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	SEEDING, MULCHING AND CRIMPING
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION
	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL
	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW



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Town of Parker, Director of Engineering/Public Works Date

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**TI** TEMPORARY IRRIGATION  
**TSB** TEMPORARY SEDIMENT BASIN  
**VTC** VEHICLE TRACKING CONTROL

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
 LEGEND  
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 Oct. 2013

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**CD** CHECK DAM  
**CF** CONSTRUCTION FENCE  
**CP** CULVERT PROTECTION  
**CWA** CONCRETE WASHOUT AREA  
**D** DEWATERING  
**DD** DIVERSION DITCH  
**DP** DETENTION POND PROTECTION  
**DTC** DEBRIS TRASH CONTROL  
**ECB** EROSION CONTROL BLANKET  
**IPAN** INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT  
**IPAP** INLET PROTECTION FOR AREA INLETS IN PAVEMENT  
**IPCOG** INLET PROTECTION, CURB ON-GRADE, TYPE R INLET  
**IPCOS** INLET PROTECTION, CURB ON SUMP, TYPE R INLET

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
 LEGEND  
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25. ALL CHEMICAL OR HAZARDOUS MATERIAL SPILLS, INCLUDING CONCRETE WASHOUT WATER, WHICH MAY ENTER WATERS OF THE STATE OF COLORADO, WHICH INCLUDES BUT ARE NOT LIMITED TO, SURFACE WATER, GROUND WATER, DRY GULLIES OR STORM SEWERS LEADING TO SURFACE WATER, SHALL BE IMMEDIATELY REPORTED TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) PER 24-B-601, AND THE TOWN OF PARKER. RELEASES OF PETROLEUM PRODUCTS AND CERTAIN HAZARDOUS SUBSTANCES LISTED UNDER THE FEDERAL CLEAN WATER ACT (40 CFR PART 116) MUST BE REPORTED TO THE NATIONAL RESPONSE CENTER AND THE CDPHE. SPILLS THAT POSE AN IMMEDIATE SAFETY HAZARD SHALL BE REPORTED TO 911.

26. THE CLEANING OF CONCRETE TRUCKS AND EQUIPMENT IS RESTRICTED TO THE APPROVED CONCRETE WASHOUT LOCATION ON THE JOB SITE. CONCRETE WASH WATER SHALL NOT BE DISCHARGED TO STATE WATERS OR STORM SEWER SYSTEMS.

27. VEHICLE AND EQUIPMENT DEGREASING IS PROHIBITED ON THE JOB SITE.

28. ALL DEWATERING ON SITE SHALL BE COORDINATED WITH THE TOWN'S INSPECTOR. A STATE PERMIT MAY BE REQUIRED FOR DEWATERING. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR OBTAINING AND ADHERING TO ALL APPLICABLE PERMITS.

29. HYDRAULIC SEEDING AND/OR HYDRAULIC MULCHING ARE ONLY ALLOWED IN AREAS UNDER TEMPORARY OR PERMANENT IRRIGATION OR FOR THE PURPOSE OF TEMPORARY SOIL STABILIZATION.

30. APPLICABLE CONSTRUCTION BMPs SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL ALL LANDSCAPING HAS BEEN INSTALLED AND THE DESIRABLE VEGETATION HAS REACHED A POINT IN WHICH EROSION AND SEDIMENTATION IS NO LONGER A CONCERN AS DETERMINED BY THE TOWN'S INSPECTOR.

31. GRADING SECURITY RELEASE REQUIREMENTS:

1) DEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H OR ITEM I (BELOW).

A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.

B. UNIFORM PERENNIAL VEGETATION COVER HAS BEEN ESTABLISHED WITH AN INDIVIDUAL PLANT DENSITY OF AT LEAST SEVENTY PERCENT (70%) OF PRE-DISTURBANCE LEVELS.

C. ALL BMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.

D. IF ANY EROSION IS PRESENT, IT IS INSIGNIFICANT AND IS NOT LEAVING THE SITE AND/OR LEADING INTO ANY ON-SITE DRAINAGE INFRASTRUCTURE THAT MAY CONVEY SURFACE WATER OFF SITE.

E. WEEDS REPRESENT NO MORE THAN FIFTY PERCENT (50%) OF THE TOTAL VEGETATION ON THE SITE.

F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.

G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.

H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.

I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR ASSIGNMENT AS PROVIDED BY SECTION 11.10.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION AT THE TIME OF CLOSING TO ENSURE THAT THE NEW SITE OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
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1. GRADING, DRILLING, CLEARING, EXCAVATING, BACK-FILLING, SOIL STRIPPING, SOIL IMPORTING, EXPORTING OR ANY OTHER FORM OF SOIL DISTURBANCE SHALL NOT COMMENCE UNTIL A TOWN OF PARKER GRADING/EXCAVATION PERMIT HAS BEEN ISSUED.

2. THE TOWN OF PARKER GRADING/EXCAVATION PERMIT IS VALID FOR A PERIOD OF 2 YEARS FROM THE DATE OF ISSUANCE. ANY LAPSE IN PERMIT COVERAGE MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER AND/OR FINES.

3. ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE CBMP PLAN INSTALLATION NOTES AND DETAILS AND/OR PERMITTED VARIANCES. ALL MATERIALS, WORKMANSHIP AND INSTALLATION METHODS ARE SUBJECT TO INSPECTION BY THE TOWN'S INSPECTOR. THE TOWN OF PARKER RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS, WORKMANSHIP AND/OR INSTALLATION METHODS THAT DO NOT CONFORM TO THE CBMP PLAN AND/OR PERMITTED VARIANCES.

4. THE EROSION CONTROL SUPERVISOR SHALL REVISE OR MODIFY THE EROSION AND SEDIMENT CONTROL MEASURES IF IT BECOMES APPARENT THAT THE ORIGINAL PLAN IS INADEQUATE, OR AS A RESULT OF DEFICIENCIES IDENTIFIED DURING INSPECTIONS PERFORMED BY THE TOWN'S INSPECTOR.

5. THE INSTALLATION OF ADDITIONAL EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs) MAY BE REQUIRED OF THE EROSION CONTROL SUPERVISOR, PROPERTY OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS AT ANY TIME THROUGHOUT THE DURATION OF THE PROJECT CONSTRUCTION AND/OR SUBSEQUENT REVEGETATION PERIOD.

6. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE SITE REMAINS IN COMPLIANCE WITH THE NOTICE OF NUISANCE REPORTS (a.k.a., CONSTRUCTION SITE RUNOFF CONTROL INSPECTION REPORTS), APPROVED CBMP PLAN(S) AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.

7. THE EROSION CONTROL SUPERVISOR SHALL BE READILY AVAILABLE TO DISCUSS AND CORRECT ANY PROBLEMS THAT MAY ARISE RELATING TO GRADING, EROSION AND SEDIMENT CONTROL.

8. IF IT IS ANTICIPATED THAT ALL OR A PORTION OF THE PROJECT WILL OCCUR DURING SEASONS SUSCEPTIBLE TO SNOWFALL, THE USE OF CERTAIN EROSION AND SEDIMENT CONTROL BMPs ADJACENT TO PUBLIC ROADWAYS MAY NEED TO BE RECONSIDERED DUE TO THE NATURE OF SNOW REMOVAL OPERATIONS. AN APPROVED ALTERNATIVE MAY BE NECESSARY TO MINIMIZE DAMAGE FROM THESE OPERATIONS. THE TOWN OF PARKER ASSUMES NO RESPONSIBILITY FOR DAMAGE TO ANY BMPs AS A RESULT OF SNOW FLOWING AND SNOW REMOVAL.

9. AREAS OF LAND DISTURBANCE EQUAL TO 40 ACRES OR GREATER SHALL NOT BE EXPOSED FOR MORE THAN 30 CONSECUTIVE DAYS WITHOUT TEMPORARY OR PERMANENT STABILIZATION.

10. AUTHORIZED EXEMPTIONS MAY BE ALLOWED TO THE 40-ACRE LIMIT FOR REMOVAL AND STORAGE OF CUT MATERIAL WHERE (A) GEOTECHNICAL LIMITATIONS RESTRICT THE USE OF TEMPORARY OR PERMANENT STABILIZATION OF THE STORED MATERIAL (E.G., SWELLING SOILS, ROCK) AND (B) WHEN THE OWNER CAN DEMONSTRATE THAT THE 40-ACRE LIMIT IS PHYSICALLY AND/OR FINANCIALLY IMPRACTICABLE. FOR SITES GRANTED THIS EXEMPTION, A PHASING AND EARTHWORK QUANTITIES PLAN SHALL BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT AND APPROVED PRIOR TO THE COMMENCEMENT OF LAND DISTURBANCE ACTIVITIES. SUBMITTAL REQUIREMENTS INCLUDE:

(i) PHASING PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.

(ii) EARTHWORK QUANTITY PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.

(iii) BMP PLAN SHOWING SPECIFIC EROSION AND SEDIMENT CONTROLS FOR EACH PHASE.

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 GEN NOTES  
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**LP** LOT PROTECTION  
**MWP** MASONRY WORK PROTECTION  
**PTP** PORTABLE TOILET PROTECTION  
**RCSC** ROUGH CUT STREET CONTROL  
**RS** ROCK SOCK  
**RSS** ROCK SOCK IN SWALE  
**SB** STRAW BALE  
**SCL** SEDIMENT CONTROL LOGS  
**SF** SILTY FENCE  
**SMC** SEEDING, MULCHING AND CRIMPING  
**SR** SURFACE ROUGHING  
**SSA** STABILIZED STAGING AREA  
**STP** SIDEWALK TRANSITION PROTECTION

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
 LEGEND  
 2 OF 3  
 Oct. 2013

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2) NONDEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H AND J, OR ITEMS I AND J (BELOW).

A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.

B. ALL BMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.

C. EROSION IS NEGLIGIBLE, IF EVEN PRESENT.

D. THE VEGETATION REPRESENTS A PERENNIAL STAND OF A DENSE, UNIFORM SURFACE OF GRASSES WITH NO AREA GREATER THAN ONE (1) SQUARE FOOT THAT IS BARRIERS OF DESIRABLE VEGETATION, INFREQUENT, WIDELY SCATTERED AREAS WHERE NATIVE VEGETATION HAS NOT YET TAKEN HOLD MAY QUALIFY FOR ACCEPTANCE AT THE DISCRETION OF THE TOWN.

E. WEEDS REPRESENT NO MORE THAN TEN PERCENT (10%) OF THE TOTAL VEGETATION ON SITE.

F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.

G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.

H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.

I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR THE GRADING PERMIT HAS BEEN ASSIGNED AS PROVIDED BY SECTION 11.10.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION, AT THE TIME OF CLOSING ON THE SALE OF A SITE THAT IS SUBJECT TO A GRADING PERMIT, TO ENSURE THAT THE NEW PROPERTY OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.

J. ALL KNOWN DRAINAGE ISSUES ASSOCIATED WITH THE PROJECT HAVE BEEN MITIGATED AND A SUFFICIENT AMOUNT OF TIME HAS PASSED TO ENSURE THAT SUCH ISSUES HAVE BEEN CORRECTED. THIS REQUIREMENT DOES NOT INCLUDE THOSE DRAINAGE ISSUES ORIGINATING ON RESIDENTIAL LOTS.

(D) NOXIOUS WEEDS MUST BE CONTROLLED AS PROVIDED UNDER STATE LAW AND SECTION 6.01.260 OF THE TOWN OF PARKER MUNICIPAL CODE. FAILURE TO CONTROL NOXIOUS WEEDS ON THE SITE MAY CONSTITUTE A NUISANCE, SUBJECT TO THE PENALTIES CONTAINED IN THE CODE.

**DEFINITIONS:**

DEVELOPABLE PROPERTY MEANS ANY LAND THAT HAS BEEN GRADED AND IS PART OF A PLATTED LOT OR PLATTED TRACT OF RECORD, THAT WAS PLATTED FOR FUTURE DEVELOPMENT, INCLUDING RESIDENTIAL HOME CONSTRUCTION OR PUBLIC IMPROVEMENTS.

NONDEVELOPABLE PROPERTY MEANS LAND THAT HAS BEEN GRADED AND WILL NOT BE FURTHER DISTURBED AS PART OF ANY FUTURE DEVELOPMENT. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO, PARKS, OPEN SPACE, HOMEOWNER ASSOCIATION OR BUSINESS ASSOCIATION PLATTED TRACTS, DETENTION PONDS AND DRAINAGEWAYS.

32. FAILURE TO COMPLY WITH ANY OF THE REQUIREMENTS DESCRIBED IN THIS SECTION MAY RESULT IN THE ISSUANCE OF: A NOTICE OF INTENT TO ISSUE A STOP WORK ORDER; A STOP WORK ORDER AND/OR THE REMEDIES/PENALTIES DESCRIBED IN CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE.

33. ANY PERSON CONVICTED OF VIOLATING ANY PROVISION OF THE TOWN OF PARKER, GRADING & EARTH MOVEMENT SECTION OF THE MUNICIPAL CODE SHALL BE GUILTY OF A MISDEMEANOR AND, UPON CONVICTION, BE PUNISHED BY A FINE OF NOT MORE THAN FOUR HUNDRED NINETY NINE DOLLARS (\$499.00) FOR EACH SEPARATE OFFENSE. EACH DAY A VIOLATION CONTINUES SHALL CONSTITUTE A SEPARATE OFFENSE. THE TOWN ALSO MAY SEEK IN MUNICIPAL COURT AN INJUNCTION, ABATEMENT, RESTITUTION OR ANY OTHER REMEDY TO PREVENT, ENJOIN, ABATE OR REMOVE THE VIOLATION. A PERSON CONVICTED OF VIOLATING CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE SHALL BE LIABLE FOR THE ACTUAL COST OF REHABILITATING THE PROPERTY. THE COSTS MAY BE RECOVERED AS RESTITUTION IN MUNICIPAL COURT PROCEEDINGS OR IN A SEPARATE CIVIL ACTION.

34. THE TOWN OF PARKER RESERVES THE RIGHT TO ALLOW MODIFICATIONS AND SUBSTITUTIONS TO THE CBMP NOTES AND DETAILS WHEN SUCH MODIFICATIONS AND SUBSTITUTIONS OFFER THE SAME LEVEL OF PROTECTION AS THE STANDARD REQUIREMENTS BASED UPON THE SPECIFIC SITUATION, AS DETERMINED BY TOWN STAFF. DUE TO THE INSIGNIFICANCE AND REGULARITY OF SUCH MODIFICATIONS AND SUBSTITUTIONS, THE APPROVAL OF SUCH VARIATIONS MAY NOT BE DOCUMENTED BY TOWN STAFF.

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
 LEGEND  
 4 OF 4  
 Oct. 2013

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11. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

12. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.

13. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.

14. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.

15. ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.

16. ALL TRASH RECEPTACLES ON SITE SHALL BE EMPTIED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONFINED TO THE RECEPTACLE.

17. ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

18. ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REBAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS, CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.

19. THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT. FAILURE TO ABIDE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.

20. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PAVED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.

21. NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.

22. ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL BLANKET(S). TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS.

23. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.

24. A WATER SOURCE SHALL BE AVAILABLE ON-SITE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FUGITIVE DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
 GEN NOTES  
 2 OF 4  
 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

11. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

12. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.

13. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.

14. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.

15. ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.

16. ALL TRASH RECEPTACLES ON SITE SHALL BE EMPTIED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONFINED TO THE RECEPTACLE.

17. ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

18. ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REBAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS, CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.

19. THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT. FAILURE TO ABIDE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.

20. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PAVED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.

21. NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.

22. ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL BLANKET(S). TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS.

23. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.

24. A WATER SOURCE SHALL BE AVAILABLE ON-SITE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FUGITIVE DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.

**CBMP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES  
 GEN NOTES  
 2 OF 4  
 Oct. 2013

**S.A. MIRO INC.**  
 CONSULTING ENGINEERS  
 4582 South Ulster Street  
 Suite 750, Denver, CO 80237  
 303-741-3737  
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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

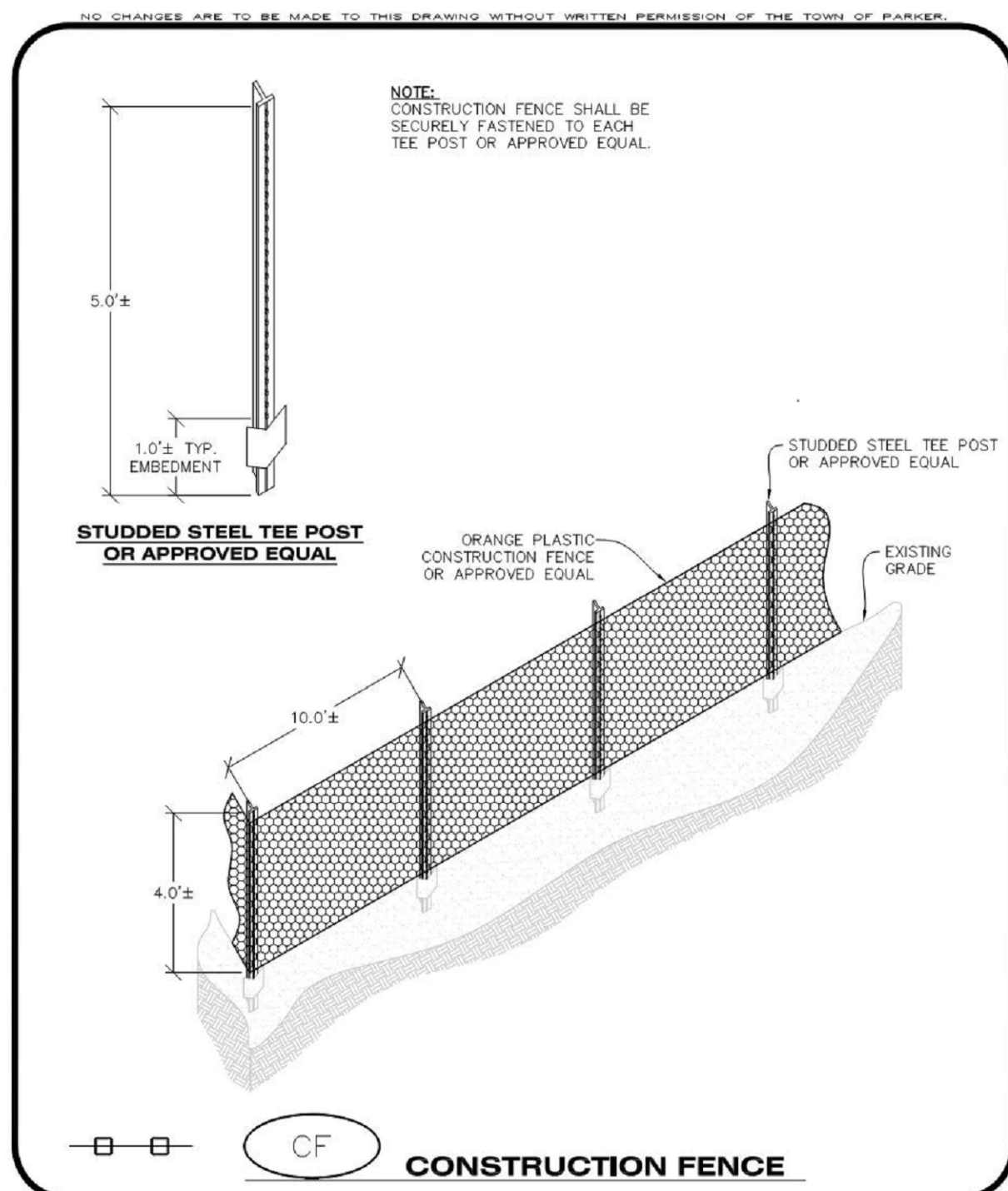
PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

DRAWING TITLE: **EROSION CONTROL DETAILS**

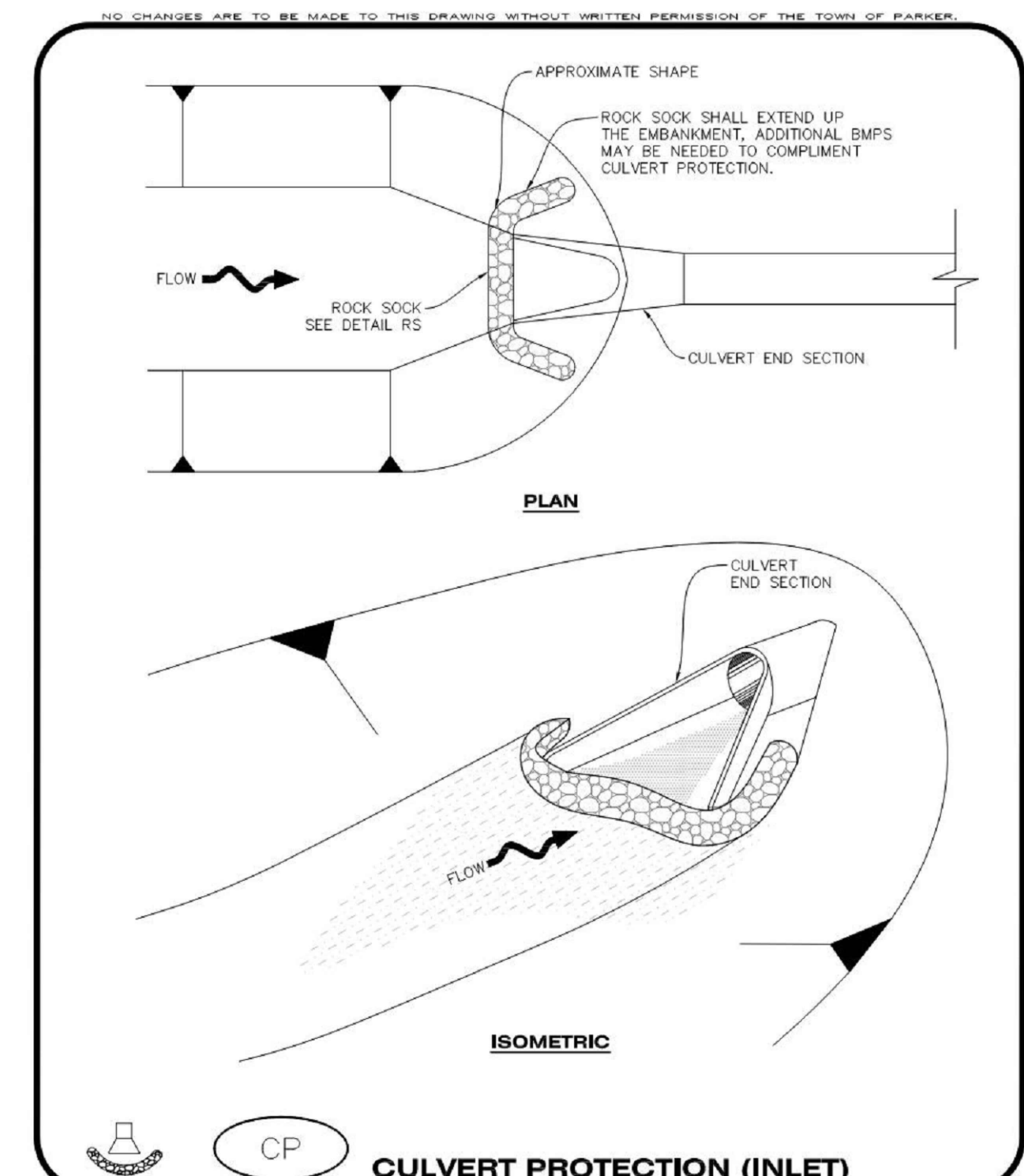
FILE PATH:

COLORADO REGISTERED PROFESSIONAL ENGINEER  
 47666  
 02/05/24

DESIGNED BY: RJH  
 DRAWN BY: DEI  
 CHECKED BY: MHV  
 MIRO JOB NO. 22139  
 DRAWING NUMBER: **C-251**



**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CF** | 1 OF 2 | Oct. 2013



**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CP** | 1 OF 2 | Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

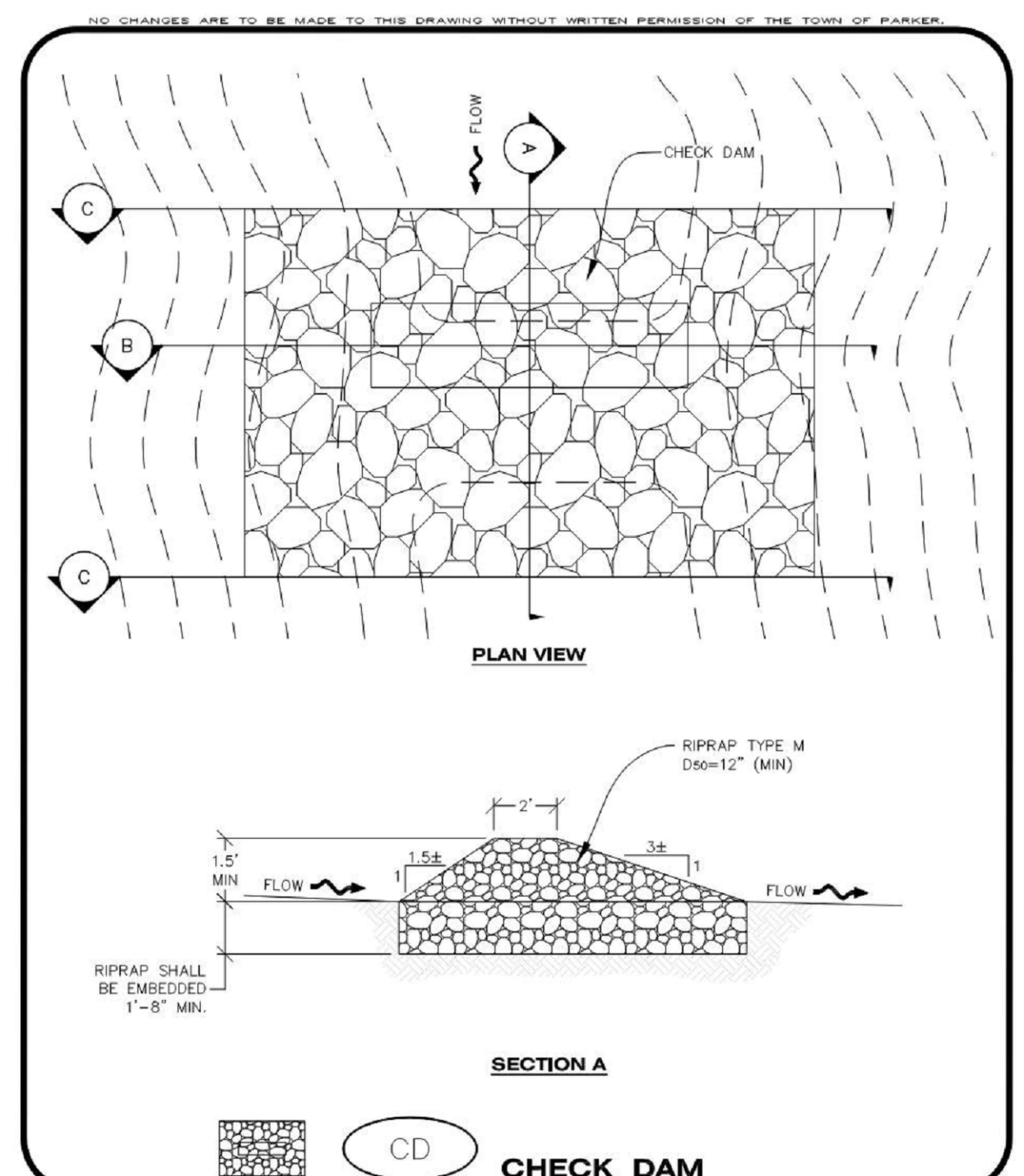
**CHECK DAM INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF CHECK DAMS.
- CHECK DAMS SHOWN ON CBMP PLAN SHALL BE INSTALLED WHEN DIRECTED BY THE TOWN'S INSPECTOR.
- RIPRAP UTILIZED FOR CHECK DAMS SHALL HAVE A D50 MEDIAN STONE SIZE OF 12".
- RIPRAP PAD SHALL BE TRENCHED INTO THE GROUND A MINIMUM OF 1'-8".
- THE MAXIMUM SPACING BETWEEN CHECK DAMS SHOULD BE SUCH THAT THE BOTTOM OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM AS SHOWN IN THE DETAIL.

**CHECK DAM INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CHECK DAMS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE CREST OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- CHECK DAMS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN CHECK DAMS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED TOPSOIL AND ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CHECK DAMS BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- IN SOME INSTANCES, CHECK DAMS MAY REMAIN IN PLACE PERMANENTLY.

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CD** | 3 OF 3 | Oct. 2013



**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CD** | 1 OF 3 | Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CONSTRUCTION FENCE INSTALLATION NOTES**

- THE CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO EACH POST OR APPROVED EQUAL.

**CONSTRUCTION FENCE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONSTRUCTION FENCE AND MAKE ANY NECESSARY REPAIRS.
- CONSTRUCTION FENCE SHALL BE REPAIRED WHEN THE FENCING MATERIAL FALLS OUT OF COMPLIANCE WITH THE NOTES AND DETAILS.

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CF** | 2 OF 2 | Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

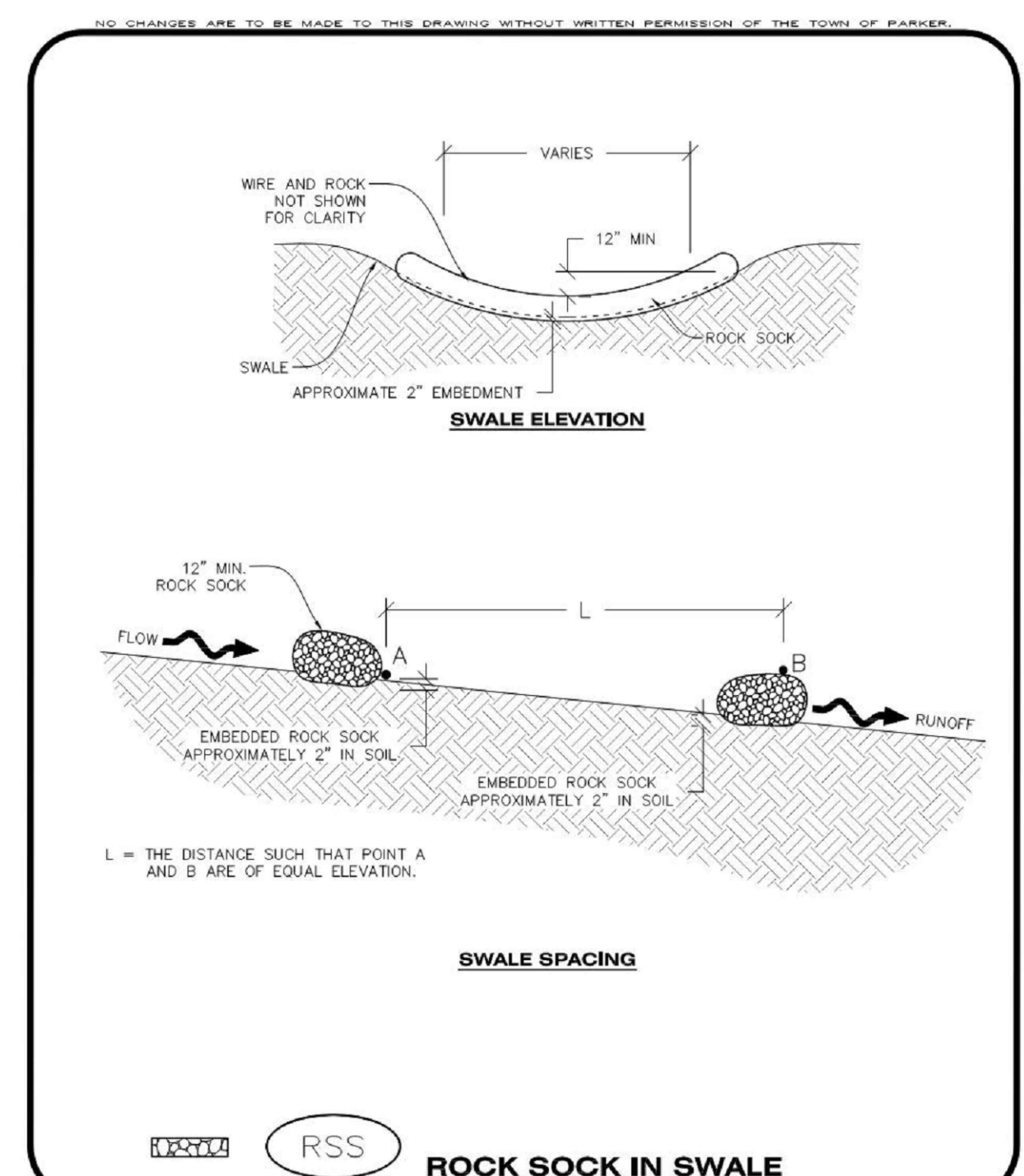
**CULVERT PROTECTION (INLET) INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF CULVERT PROTECTION (INLET).
- ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
- ROCK SOCK SHALL BE APPROXIMATELY 12" IN DIAMETER.
- ROCK SOCK SHALL EXTEND ABOVE THE FLOW LINE ELEVATION ON BOTH SIDES OF THE CULVERT END SECTION.

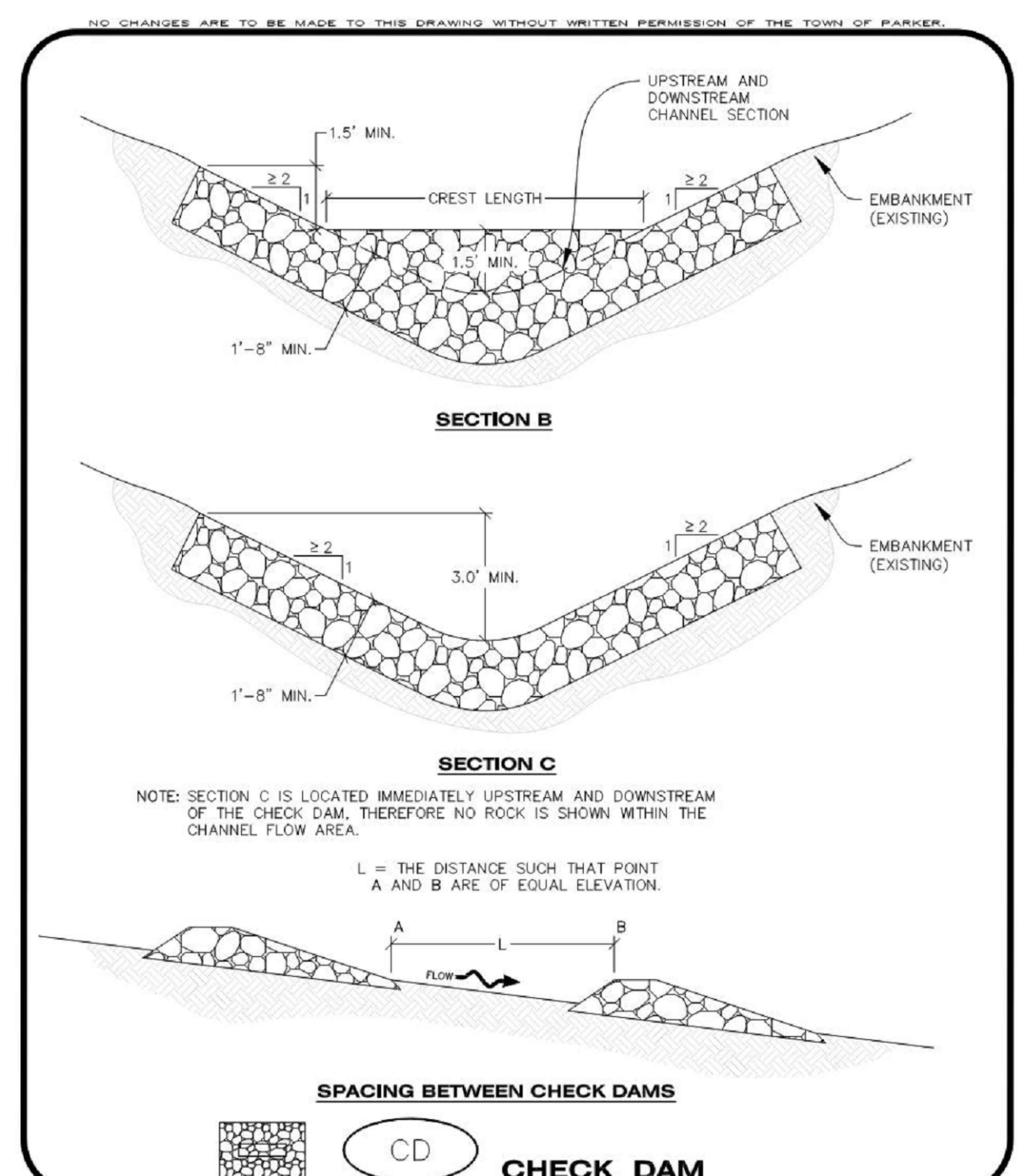
**CULVERT PROTECTION (INLET) INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CULVERT PROTECTION (INLET).
- AT A MINIMUM, ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE ROCK SOCK.
- CULVERT PROTECTION (INLET) IS TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE CULVERT INLET PROTECTION IS REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CULVERT INLET PROTECTION TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CP** | 2 OF 2 | Oct. 2013



**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **RSS** | 1 OF 1 | Oct. 2013



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**M I R O**

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**811**

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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:

PROFESSIONAL ENGINEER

47666

02/05/24

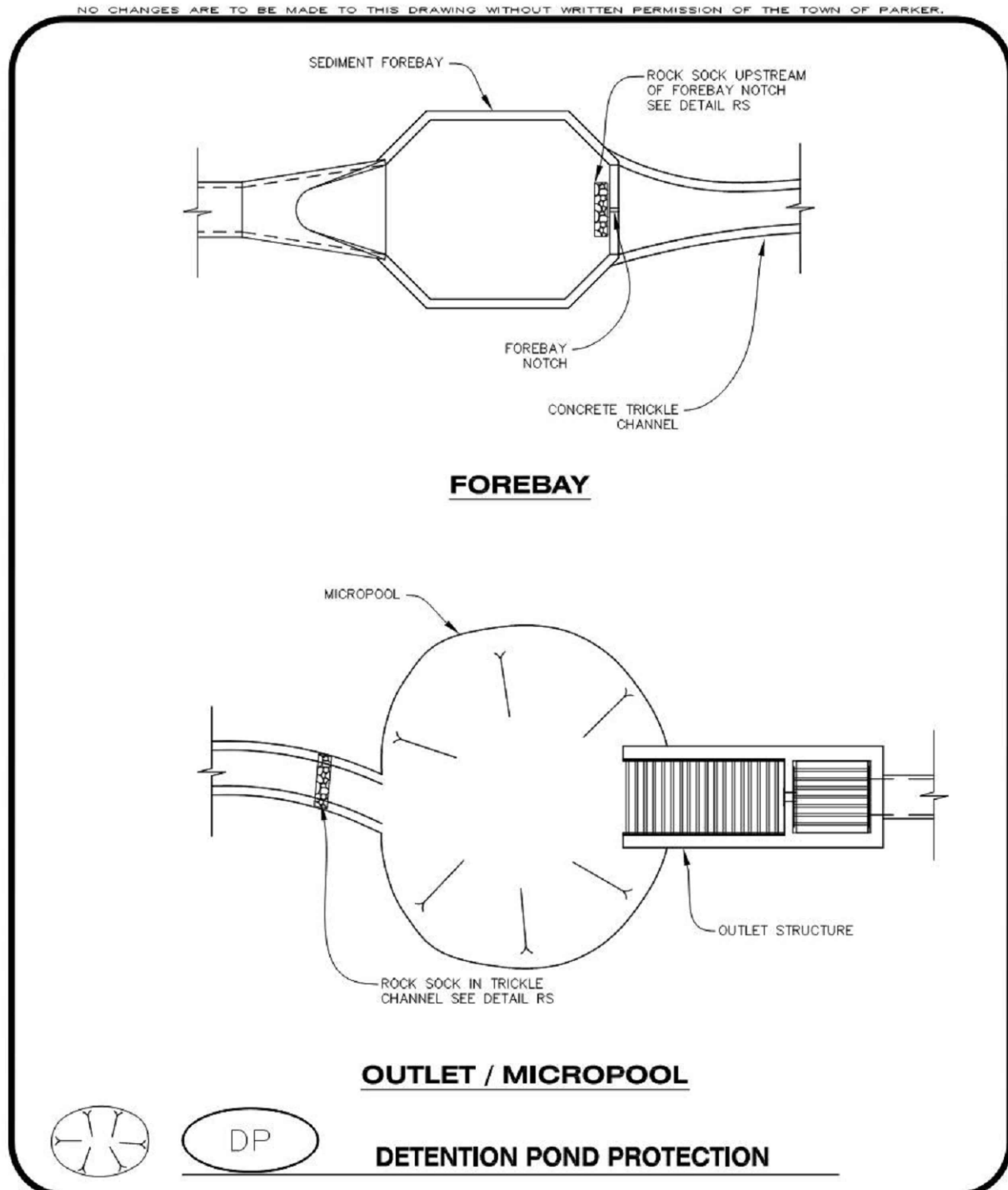
DESIGNED BY: R/JH

DRAWN BY: DEI

CHECKED BY: MHV

MIRO JOB NO. 22139

DRAWING NUMBER: **C-252**



**DP**  
DETENTION POND PROTECTION

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

1 OF 2  
Oct. 2013

**DETENTION POND PROTECTION INSTALLATION NOTES**

- DETENTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).

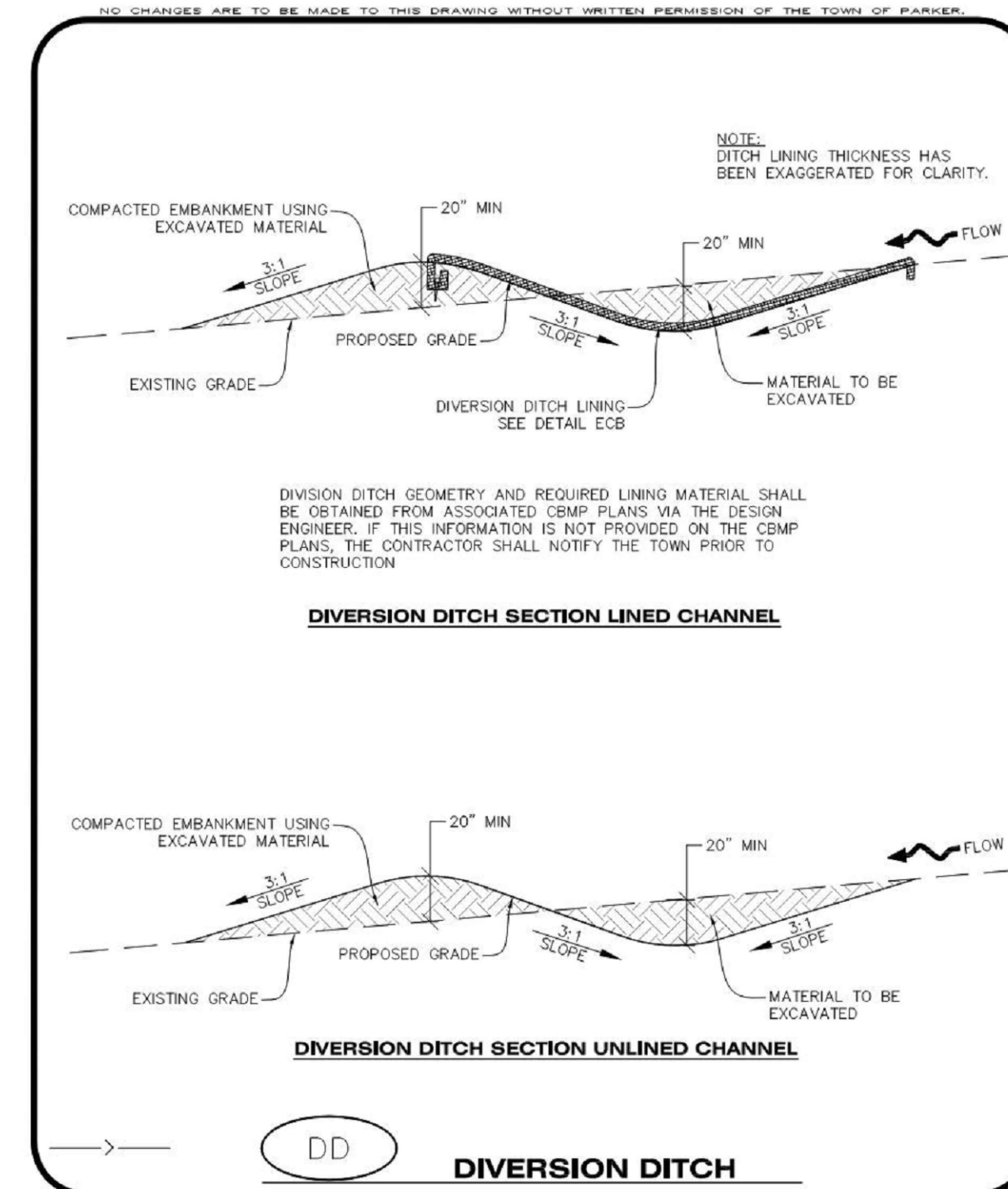
**DETENTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
- OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**DP**  
DETENTION POND PROTECTION

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

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**DD**  
DIVERSION DITCH

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

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Oct. 2013

**DIVERSION DITCH INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
- A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
- ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
- THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
- THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

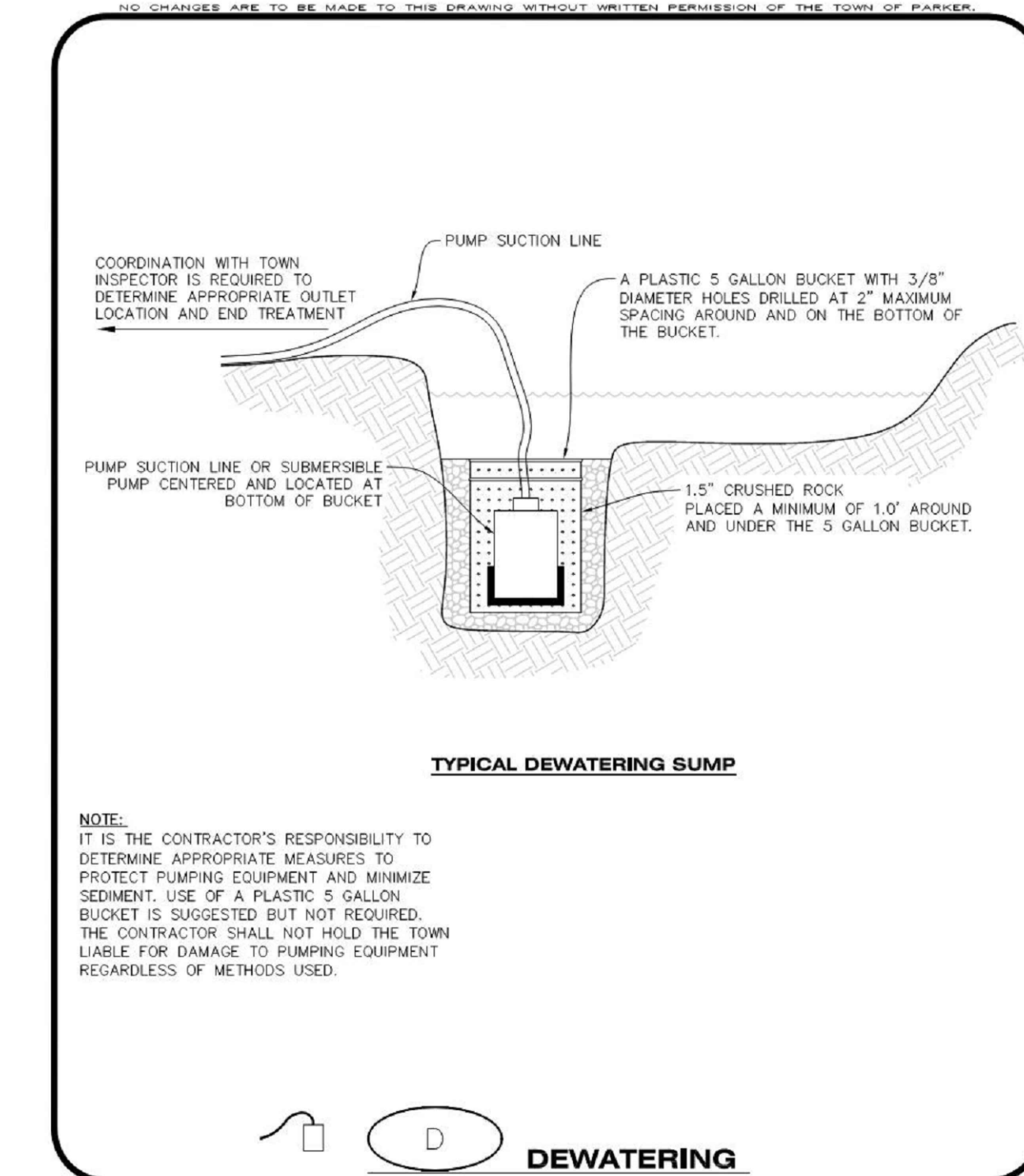
**DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
- DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
- DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

**DD**  
DIVERSION DITCH

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

2 OF 2  
Oct. 2013



**D**  
DEWATERING

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

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Oct. 2013

**DEWATERING INSTALLATION NOTES**

- IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

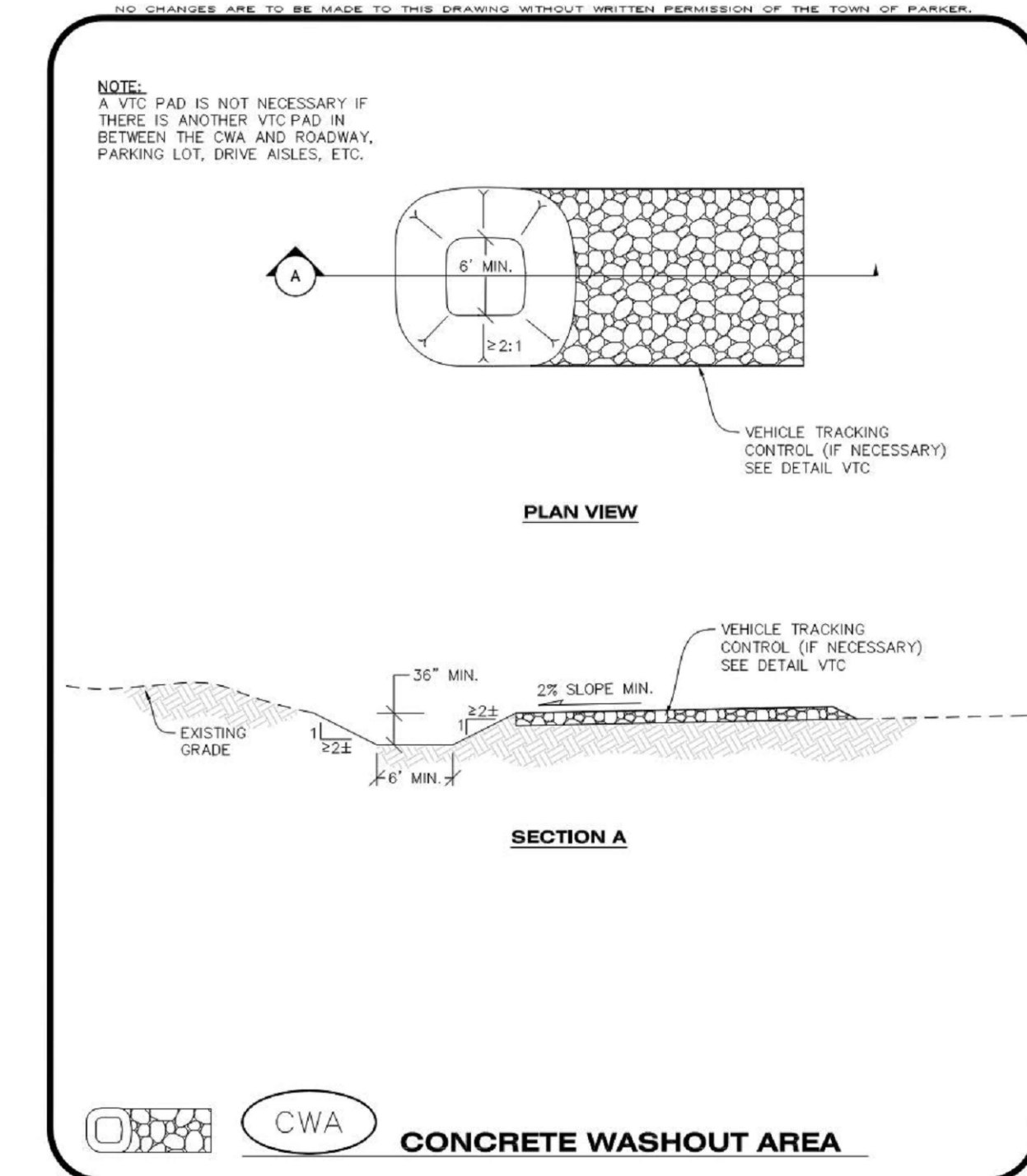
**DEWATERING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

**D**  
DEWATERING

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

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Oct. 2013



**CWA**  
CONCRETE WASHOUT AREA

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

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Oct. 2013

**CONCRETE WASHOUT AREA INSTALLATION NOTES**

- CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
- A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
- IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.

**CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
- CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
- CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
- CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
- CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
- WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**CWA**  
CONCRETE WASHOUT AREA

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

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Oct. 2013

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

**PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

**EROSION CONTROL DETAILS**

PROJECT: PARKER TOWN HALL EXPANSION  
DRAWING TITLE: EROSION CONTROL DETAILS  
FILE PATH:

**PROFESSIONAL ENGINEER**  
47666  
02/05/24

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139  
DRAWING NUMBER: **C-253**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**AREA INLET PROTECTION INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
- THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
- AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

**Parker COLORADO** | **CBMP** | **IPA**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**EROSION CONTROL BLANKET INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
- EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THESE CALCULATIONS, TURF REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.
- IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH, AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.
- EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.
- ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
- ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELISOR.

MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN	SI150
APPROVED EQUAL	APPROVED EQUAL

7. IN MOST CASES, EROSION CONTROL BLANKETS FOR SLOPE PROTECTION ARE TO REMAIN IN PLACE PERMANENTLY.

**EROSION CONTROL BLANKET MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

**Parker COLORADO** | **CBMP** | **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

START EROSION BLANKET AT THE TOP OF SLOPE AND EXTEND PAST THE TOE OF SLOPE. BLANKET SHALL BE PLACED PERPENDICULAR TO THE TOP AND BOTTOM OF THE SLOPE. COMPLETE SUBSEQUENT ROWS IN THE SAME MANNER, OVERLAPPING THE BLANKETS A MINIMUM OF 1'0".

**STAPLE PATTERN**  
NOTE: STAPLES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATION. IF MANUFACTURER'S INFO IS NOT AVAILABLE THEN ABOVE STAPLE PATTERN SHALL BE USED. WOODEN STAKES SHALL NOT BE USED FOR EROSION CONTROL BLANKET ON SLOPES.

**NOTE:**  
WHEN NECESSARY, ALL SEEDING SHALL BE COMPLETED PRIOR TO THE PLACEMENT OF EROSION CONTROL BLANKETS. PLEASE SEE DETAIL SMC FOR SEEDING, MULCHING, AND CRIMPING REQUIREMENTS.

**ECB** | **EROSION CONTROL BLANKET (SLOPE)**

**Parker COLORADO** | **CBMP** | **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**NOTE:**

- THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY. FURTHERMORE, ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

**DTC** | **DEBRIS AND TRASH CONTROL**

**Parker COLORADO** | **CBMP** | **DTC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**AREA INLET WITH CONCRETE APRON**  
**AREA INLET PLAN VIEW**  
**SECTION A**  
**SECTION B**  
**ISOMETRIC**

**IPAP** | **INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

**Parker COLORADO** | **CBMP** | **IPAP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

THIS METHOD OF INLET PROTECTION SHALL NOT BE USED ON INLETS RECEIVING SIGNIFICANT FLOWS, SUCH AS IN CERTAIN SWALES/CHANNELS OR HIGHWAY MEDIANS. INLET PROTECTION FOR AREA INLETS IN PAVEMENT (SEE DETAIL IPAP) SHALL BE USED IN THESE CONDITIONS.

**AREA INLET WITH CONCRETE APRON**  
**AREA INLET PLAN VIEW**  
**SECTION A**  
**SECTION B**  
**ISOMETRIC**

**IPAN** | **INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT**

**Parker COLORADO** | **CBMP** | **IPAN**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**TOE OF SLOPE ANCHOR TRENCH**  
**TOP OF SLOPE ANCHOR TRENCH**  
**JOINT ANCHOR TRENCH**  
**WOOD STAKE DETAIL**  
**STAKING PATTERN**

**NOTE:**  
1. WOOD STAKES SHALL BE INSTALLED SUCH THAT ONLY 1" IS EXPOSED ABOVE THE GROUND.  
2. WHEN NECESSARY, ALL SEEDING SHALL BE COMPLETED PRIOR TO THE PLACEMENT OF EROSION CONTROL BLANKETS. PLEASE SEE DETAIL SMC FOR SEEDING, MULCHING, AND CRIMPING REQUIREMENTS.

**ECB** | **EROSION CONTROL BLANKET (CHANNEL)**

**Parker COLORADO** | **CBMP** | **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**DEBRIS CONTROL NOTES:**

- A COMBINATION OF SURFACE SCRAPING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THESE AREAS.
- ALL CHEMICAL SPILLS AND/OR STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EXTENT PRACTICABLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VAC-TRUCK.
- ON-SITE PERSONNEL, DELIVERY DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUAL DEBRIS AND TRASH CONTROL.

**Parker COLORADO** | **CBMP** | **DTC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

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NO.	1
	2

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

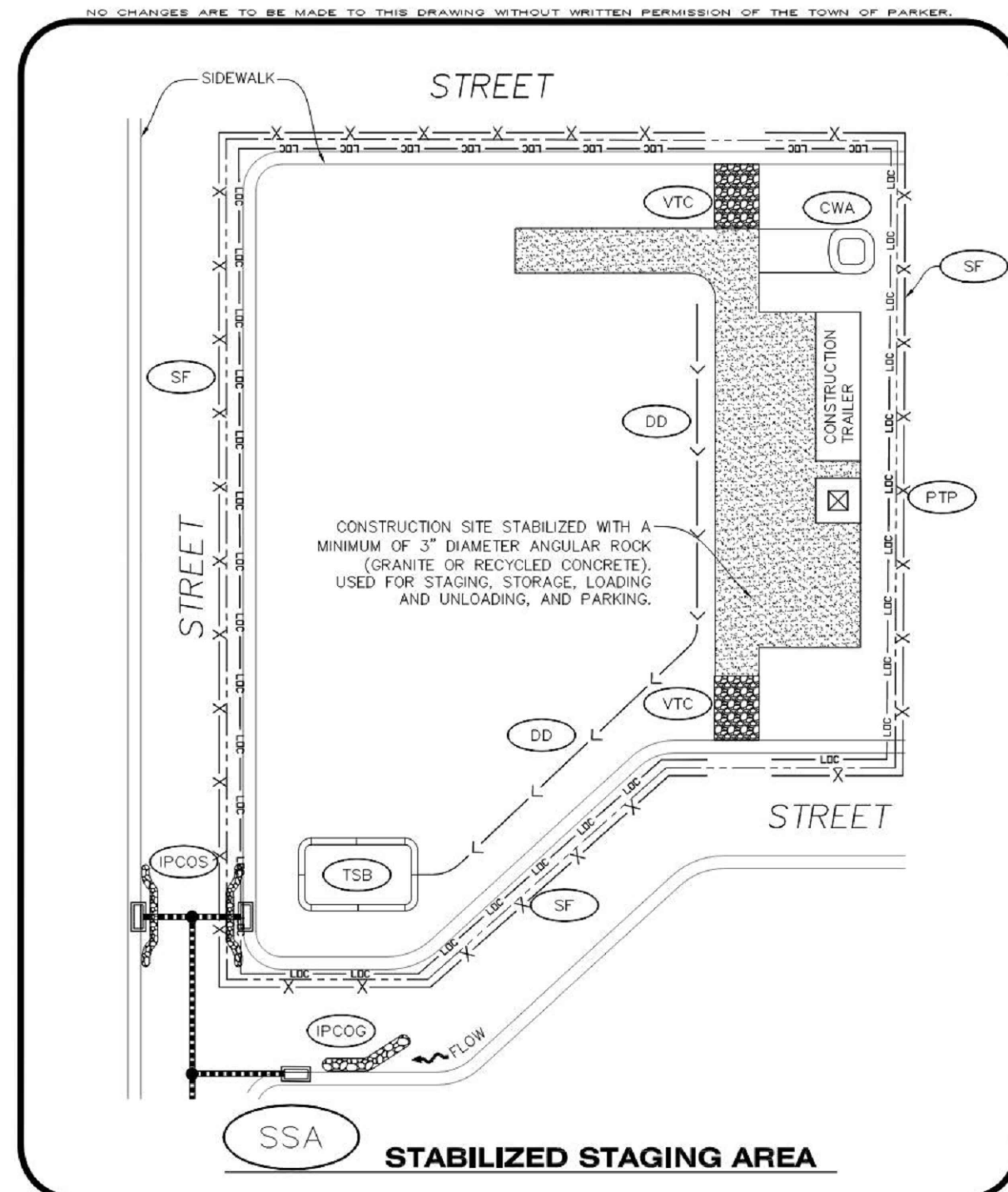
DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:

PROFESSIONAL ENGINEER  
47666  
02/05/24

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-254**



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**CBMP** | **SSA**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

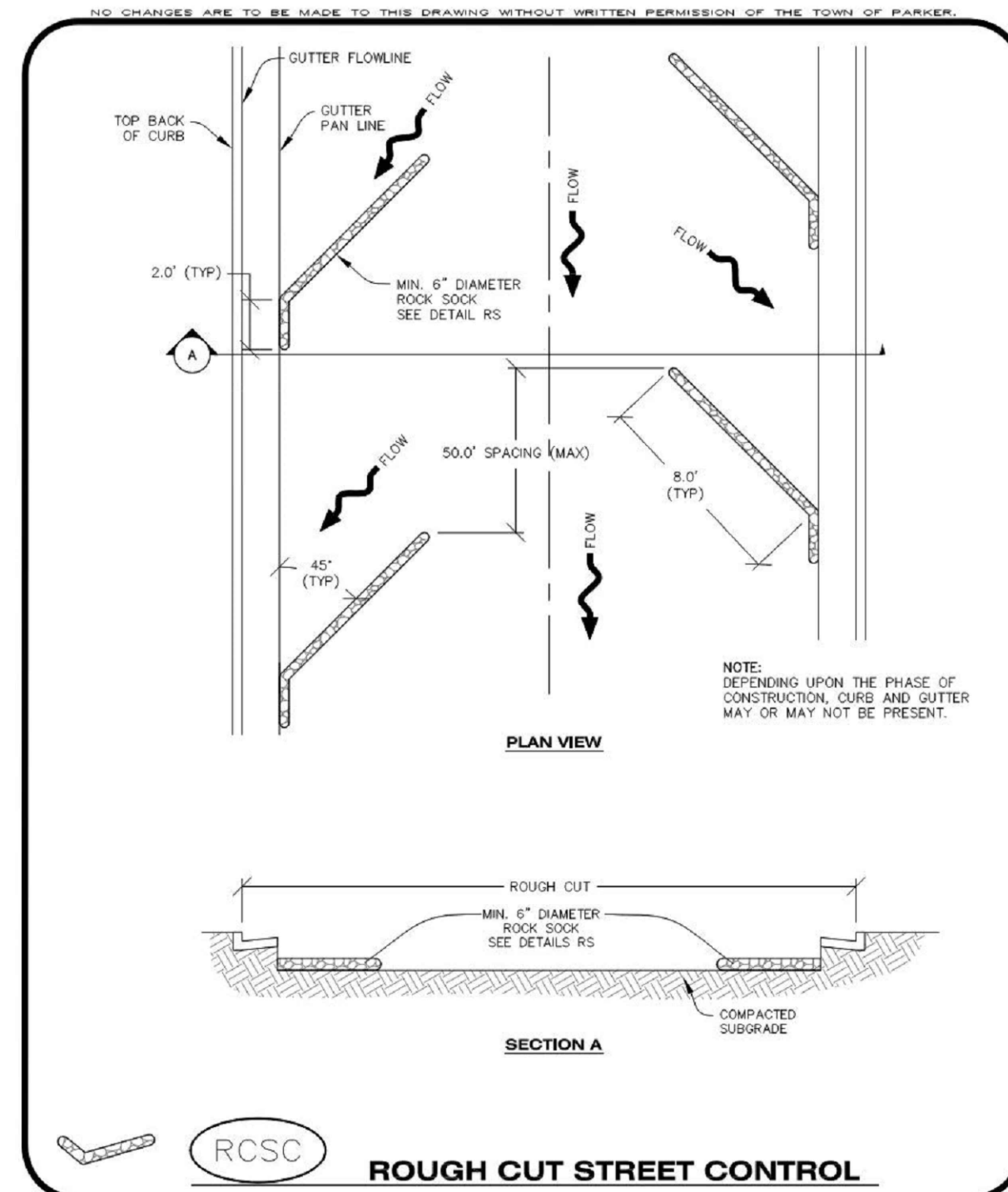
**STABILIZED STAGING AREA INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION OF STAGING AREA. CONTRACTOR MAY MODIFY LOCATION AND SIZE OF STABILIZED STAGING AREA WITH TOWN APPROVAL.
- STABILIZED STAGING AREA SHALL BE LARGE ENOUGH TO FULLY CONTAIN PARKING, STORAGE, AND LOADING OPERATIONS.
- THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM OF 3" DIAMETER OF ANGULAR ROCK (GRANITE OR RECYCLED CONCRETE).
- SSA FOR SMALLER SITES MAY NOT BE PRACTICAL. IN THESE AND SIMILAR SITUATIONS, VARIANCES MAY BE PERMITTED BY THE TOWN.

**STABILIZED STAGING AREA INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STAGING AREA.
- STABILIZED STAGING AREA SHALL BE ENLARGED AS NECESSARY TO CONTAIN PARKING, STORAGE, LOADING, AND UNLOADING.

**CBMP** | **SSA**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
 Oct. 2013



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CBMP** | **RCSC**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

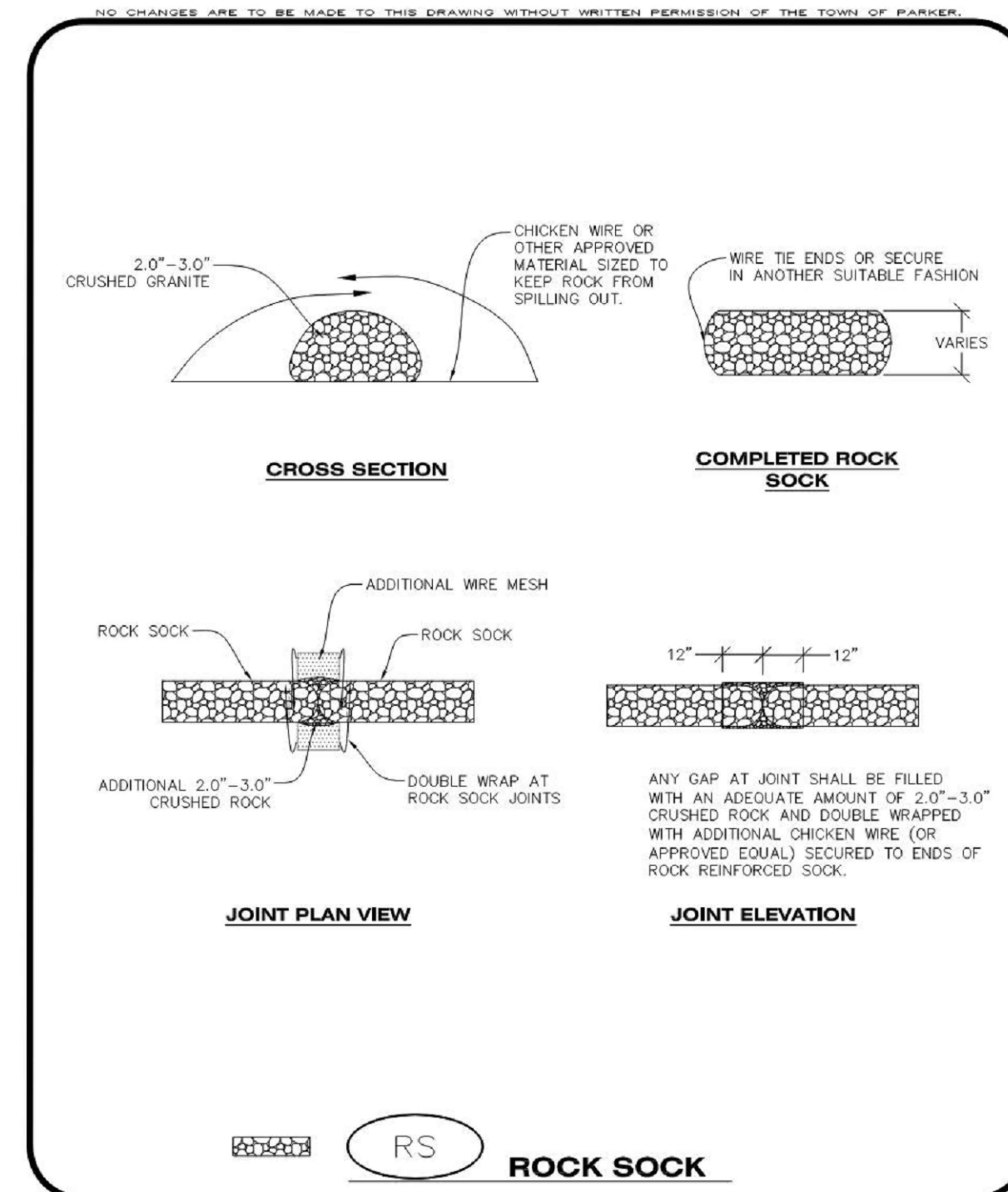
**ROUGH CUT STREET CONTROL INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
- THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.

**ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
- ROUGH CUT STREET CONTROL SHALL BE REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
- ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUB-GRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE RCSC SHOULD BE REMOVED IN INCREMENTS BASED ON SUBGRADE PREPARATION.

**CBMP** | **RCSC**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CBMP** | **RS**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

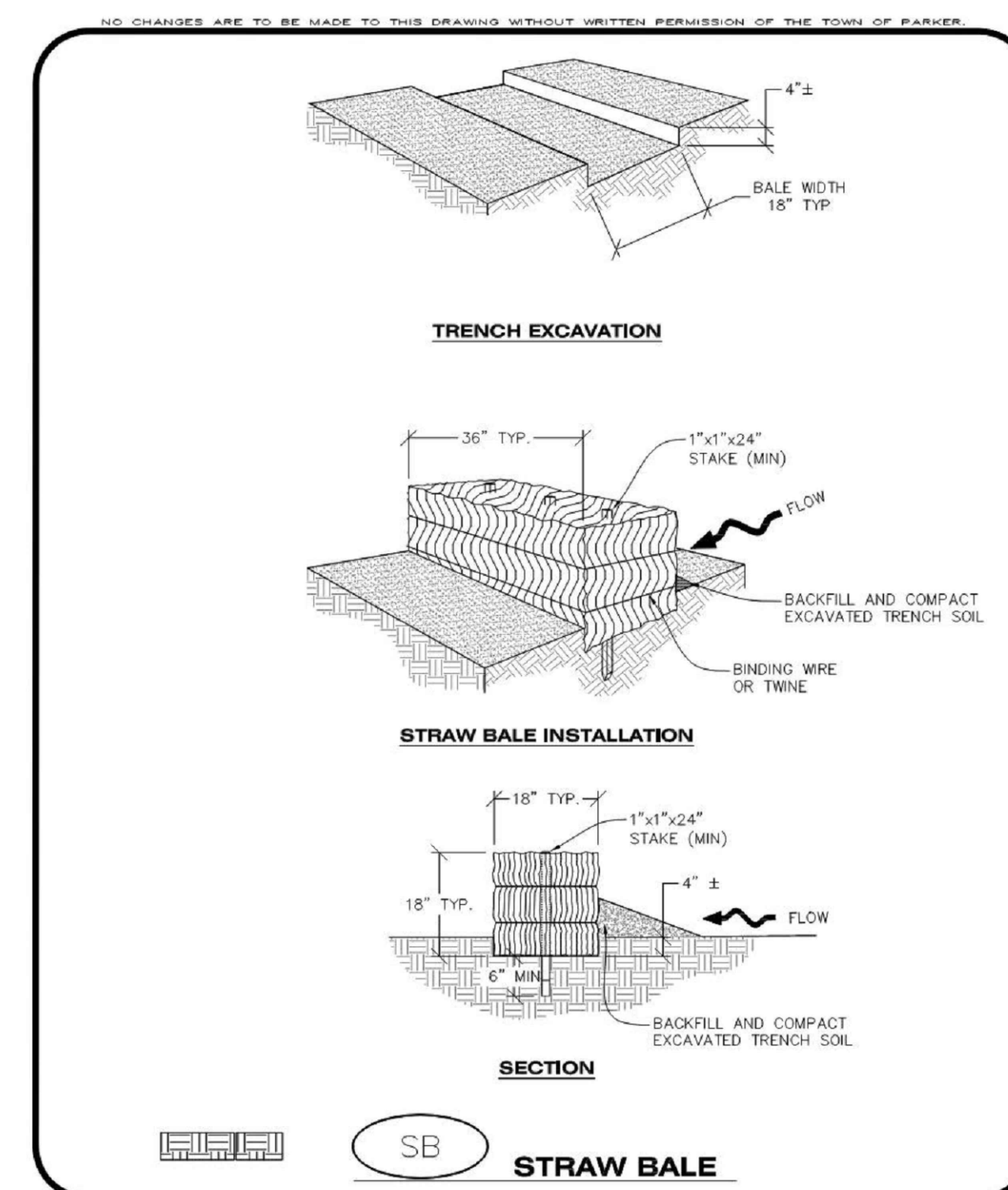
**ROCK SOCK INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
- CRUSHED ROCK SHALL BE APPROXIMATELY 2.0"-3.0" GRANITE IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
- TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCATION SUSCEPTIBLE TO VEHICLE OR PEDESTRIAN TRAFFIC. TUBULAR MARKERS SHALL CONFORM TO THE TUBULAR MARKER DETAIL.

**ROCK SOCK INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** | **RS**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
 Oct. 2013



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CBMP** | **SB**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

**STRAW BALE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
- TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36"x18"x18".
- TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1"x1"x24".
- WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
- STRAW BALES SHALL BE SPACED AND POSITIONED ACCORDING TO DETAILS.

**STRAW BALE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
- STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
- STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**CBMP** | **SB**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
 Oct. 2013

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

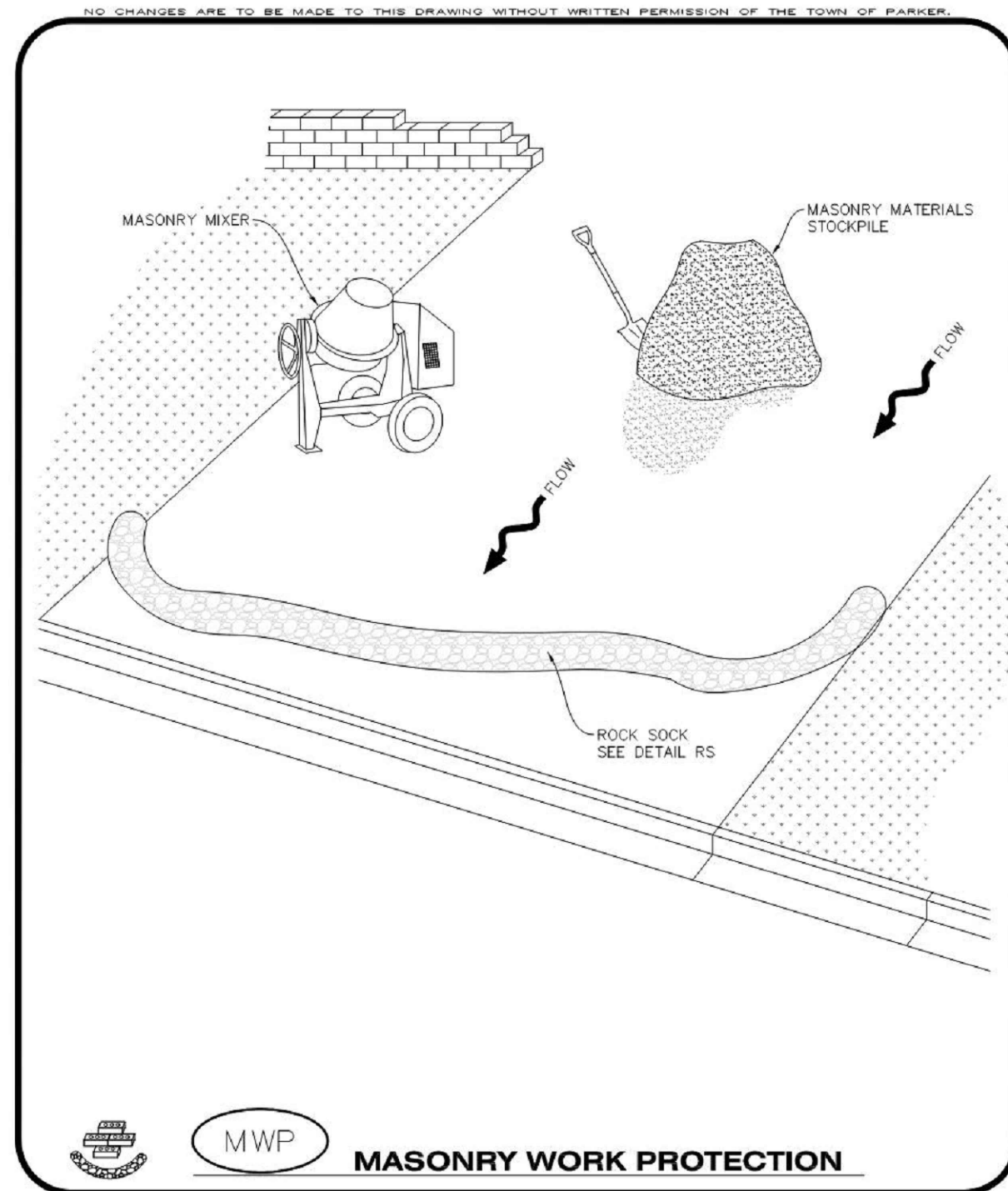
NO.	DATE	DESCRIPTION
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PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138**

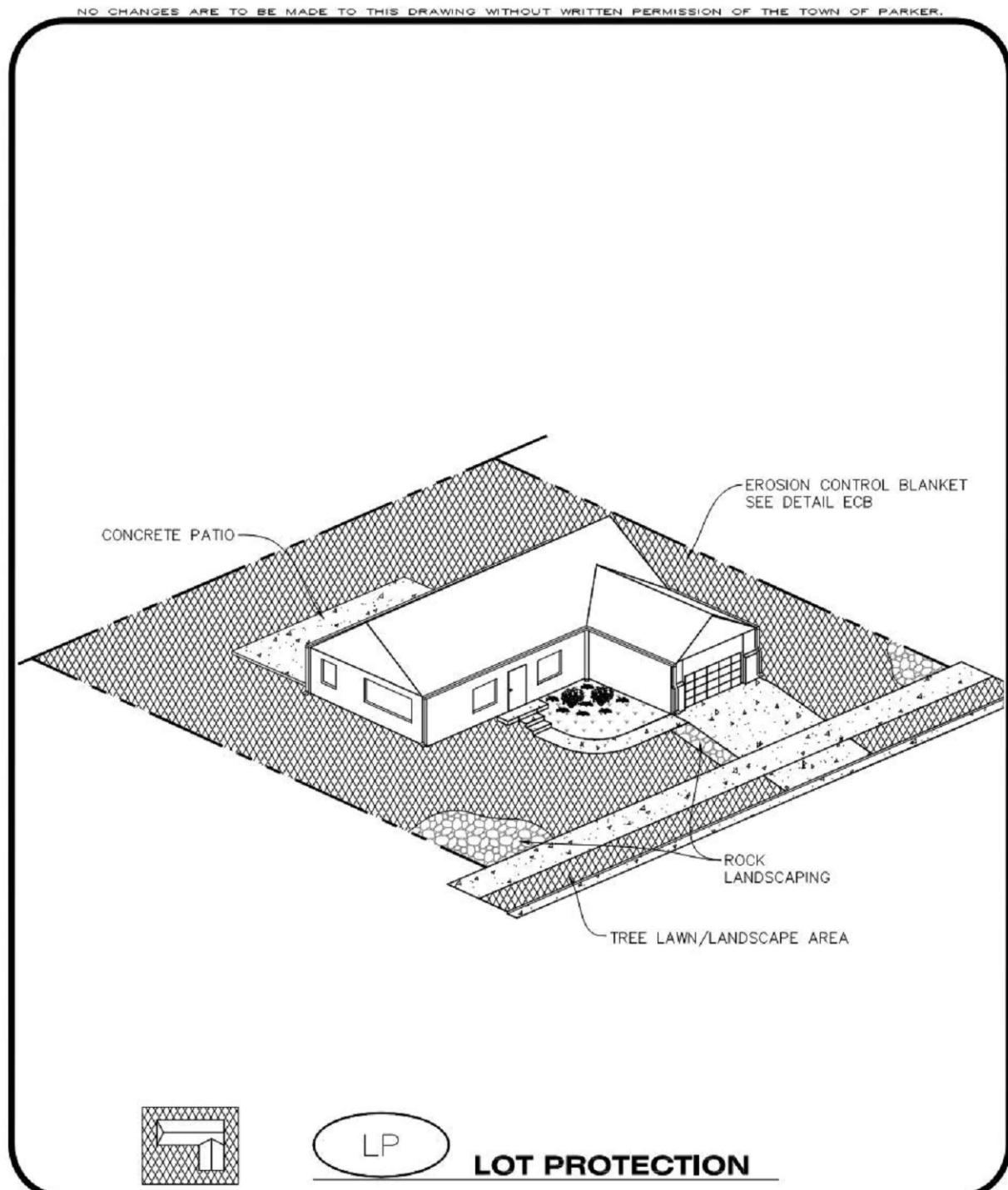
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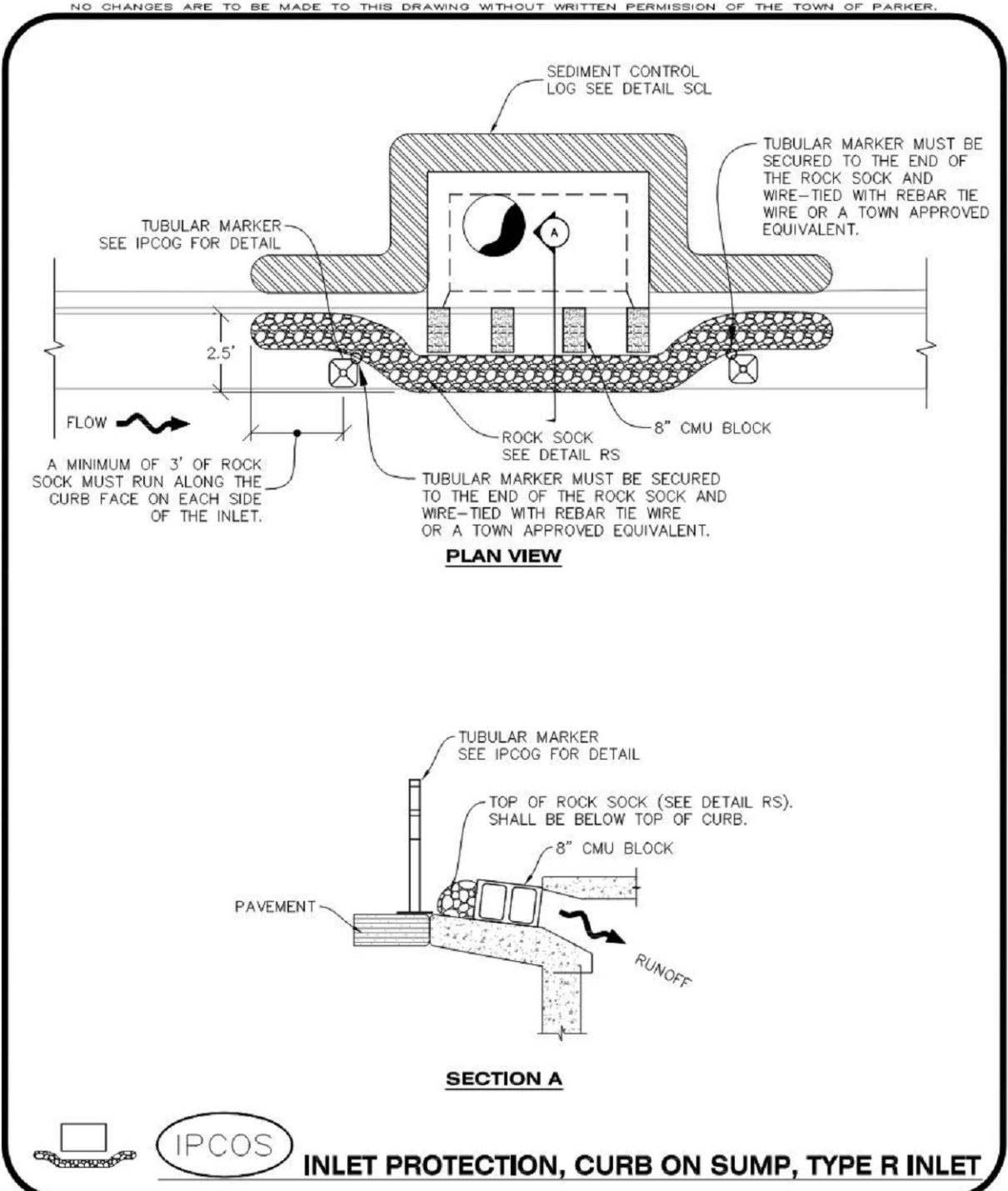
DESIGNED BY: R/JH  
 DRAWN BY: D/EI  
 CHECKED BY: M/HV  
 MIRO JOB NO. 22139  
 DRAWING NUMBER: **C-255**



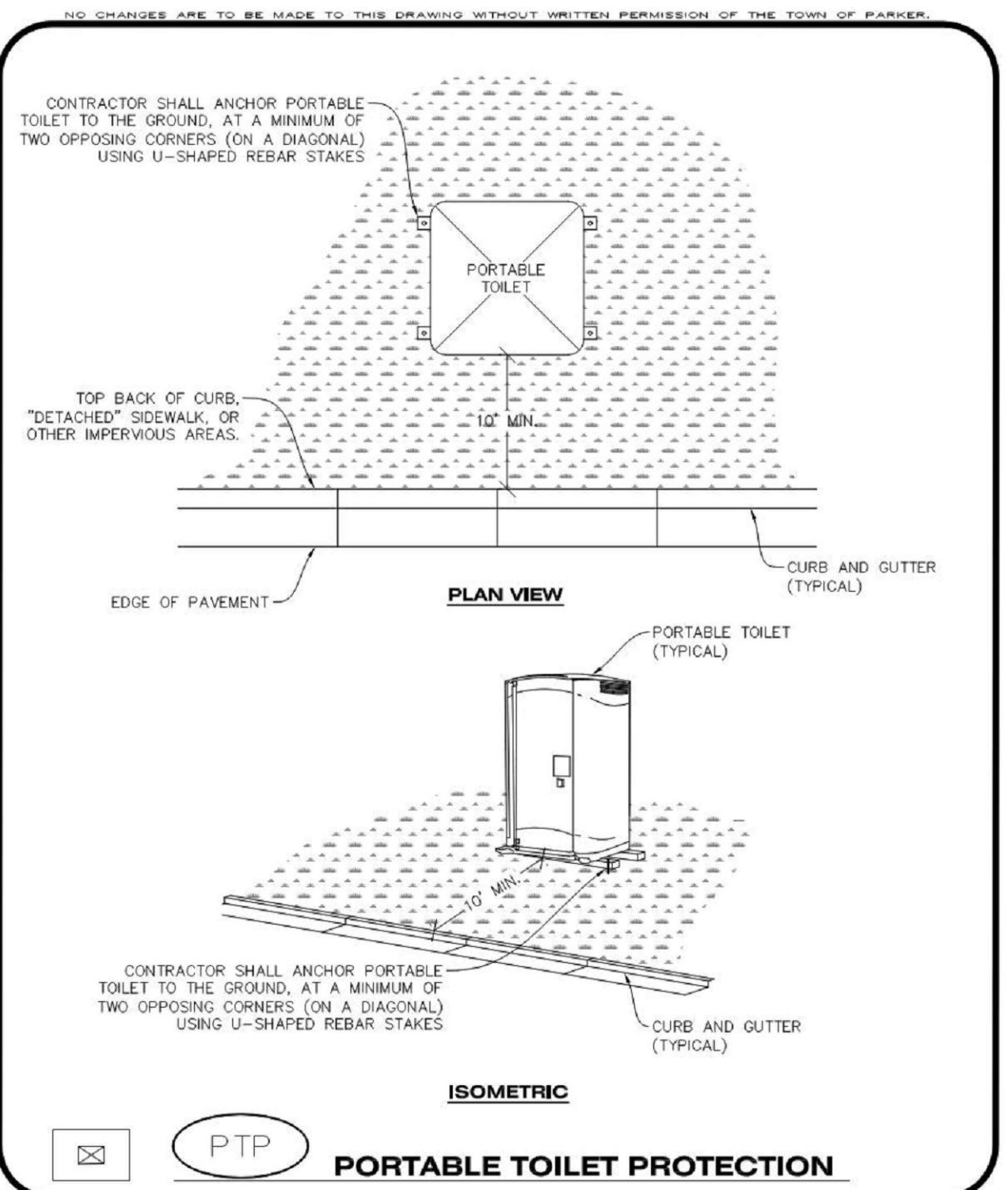
**Parker COLORADO** | **CBMP** | **MWP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013



**Parker COLORADO** | **CBMP** | **LP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
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**Parker COLORADO** | **CBMP** | **IPCOS**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013



**Parker COLORADO** | **CBMP** | **PTP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**MASONRY WORK PROTECTION INSTALLATION NOTES**

- MASONRY WORK PROTECTION MAY NEED TO BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
- A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.

**MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
- ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASH OUT AREA.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.

**Parker COLORADO** | **CBMP** | **MWP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**EROSION CONTROL BLANKET FOR LOT PROTECTION INSTALLATION NOTES**

- ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION INSTALLED IN THE TOWN SHALL MEET THE FOLLOWING MINIMUM REQUIREMENTS:
 

MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN	SISO BN
APPROVED EQUAL	APPROVED EQUAL
- ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION SHALL BE INSTALLED ACCORDING TO THE DETAIL DRAWINGS, WITH THE FOLLOWING ALLOWANCES AND ADDITIONS:
  - THE TOWN WILL ALLOW THE USE OF BIODEGRADABLE, EROSION CONTROL BLANKET-SPECIFIC STAPLES, IN LIEU OF TRADITIONAL METAL STAPLES.
  - ALL EROSION CONTROL BLANKET EDGES (SIDES AND ENDS) MUST OVERLAP THE ADJACENT BLANKET BY A MINIMUM OF 6-INCHES WITH THE UP-GRADIENT EDGES BEING PLACED ON TOP OF THE DOWN-GRADIENT EDGE OF THE ADJACENT BLANKET.
  - THE EDGES (SIDES AND ENDS) OF THE EROSION CONTROL BLANKETS DO NOT NEED TO BE TRENCHED INTO THE GROUND ASSUMING THE SITE CONDITIONS WILL NOT CAUSE EROSION BENEATH THE BLANKETS. THESE ASSUMPTIONS WILL BE THE RESPONSIBILITY OF THE EROSION CONTROL SUPERVISOR. ON OCCASION, THE TOWN'S INSPECTOR MAY REQUEST TRENCHING BASED UPON SITE CONDITIONS.
  - TOPSOIL PLACEMENT, SEEDING, AND MULCHING WILL NOT BE REQUIRED ON THE LOTS FOLLOWING INSTALLATION.
- ANY DAMAGED OR EXCESS STAPLES SHALL BE REMOVED FROM THE SITE FOLLOWING INSTALLATION.
- THE EROSION CONTROL BLANKETING REQUIRED AS PART OF THE LOT PROTECTION (LP) REQUIREMENT MUST BE INSTALLED OVER ALL UN-LANDSCAPED AREAS WITHIN EACH RESIDENTIAL, MULTI-FAMILY, AND COMMERCIAL LOT PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY / TEMPORARY CERTIFICATE OF OCCUPANCY.

**EROSION CONTROL BLANKET FOR LOT PROTECTION MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKET FOR LOT PROTECTION.
- EROSION CONTROL BLANKETS FOR LOT PROTECTION ARE INTENDED TO REMAIN IN PLACE AND MAINTAINED UNTIL LANDSCAPING IS INSTALLED.

**Parker COLORADO** | **CBMP** | **LP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CURB INLET PROTECTION INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF CURB INLET PROTECTION.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- ROCK SOCK SHALL EXTEND 3.0' ALONG THE CURB BEYOND LOCATIONS WHERE IT RETURNS TO CONTACT CURB FACE.
- TUBULAR TRAFFIC MARKERS SHALL BE A MINIMUM OF 36" IN HEIGHT WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
- THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE CURB INLET. CMU BLOCKS OR THE ROCK SOCK SHALL BE USED AS INTERIM PROTECTION UNTIL THE FIRST LIFT OF ASPHALT IS INSTALLED.

**CURB INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CURB INLET PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- CURB INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**Parker COLORADO** | **CBMP** | **IPCOS**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**PORTABLE TOILET PROTECTION INSTALLATION NOTES**

- PORTABLE TOILETS SHALL BE PLACED A MINIMUM OF 10.0' BEHIND ALL CURBS, SIDEWALKS, AND OTHER IMPERVIOUS AREAS.
- ALL PORTABLE TOILETS MUST BE GROUPED TOGETHER.
- PORTABLE TOILETS SHALL BE SECURELY ANCHORED TO THE GROUND USING U-SHAPED REBAR STAKES.
- U-SHAPED REBAR STAKES SHALL BE POSITIONED ON AT LEAST 2 OPPOSING (DIGITAL) CORNERS.

**PORTABLE TOILET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE PORTABLE TOILET PROTECTION.
- PORTABLE TOILETS SHALL BE SERVICED AT THE NECESSARY INTERVALS TO ELIMINATE THE POSSIBILITY OF OVERFLOW.
- WHEN THE PORTABLE TOILETS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE TOILETS MAY NEED TO BE LANDSCAPED OR ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- PORTABLE TOILETS THAT ARE NOT CONSISTENTLY MAINTAINED IN ACCORDANCE WITH THESE REQUIREMENTS MAY NEED TO BE CLUSTERED TOGETHER, IN ONE CENTRALIZED LOCATION IN ORDER TO INCREASE COMPLIANCE AND REDUCE THE CHANCE OF A SPILL.

**Parker COLORADO** | **CBMP** | **PTP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
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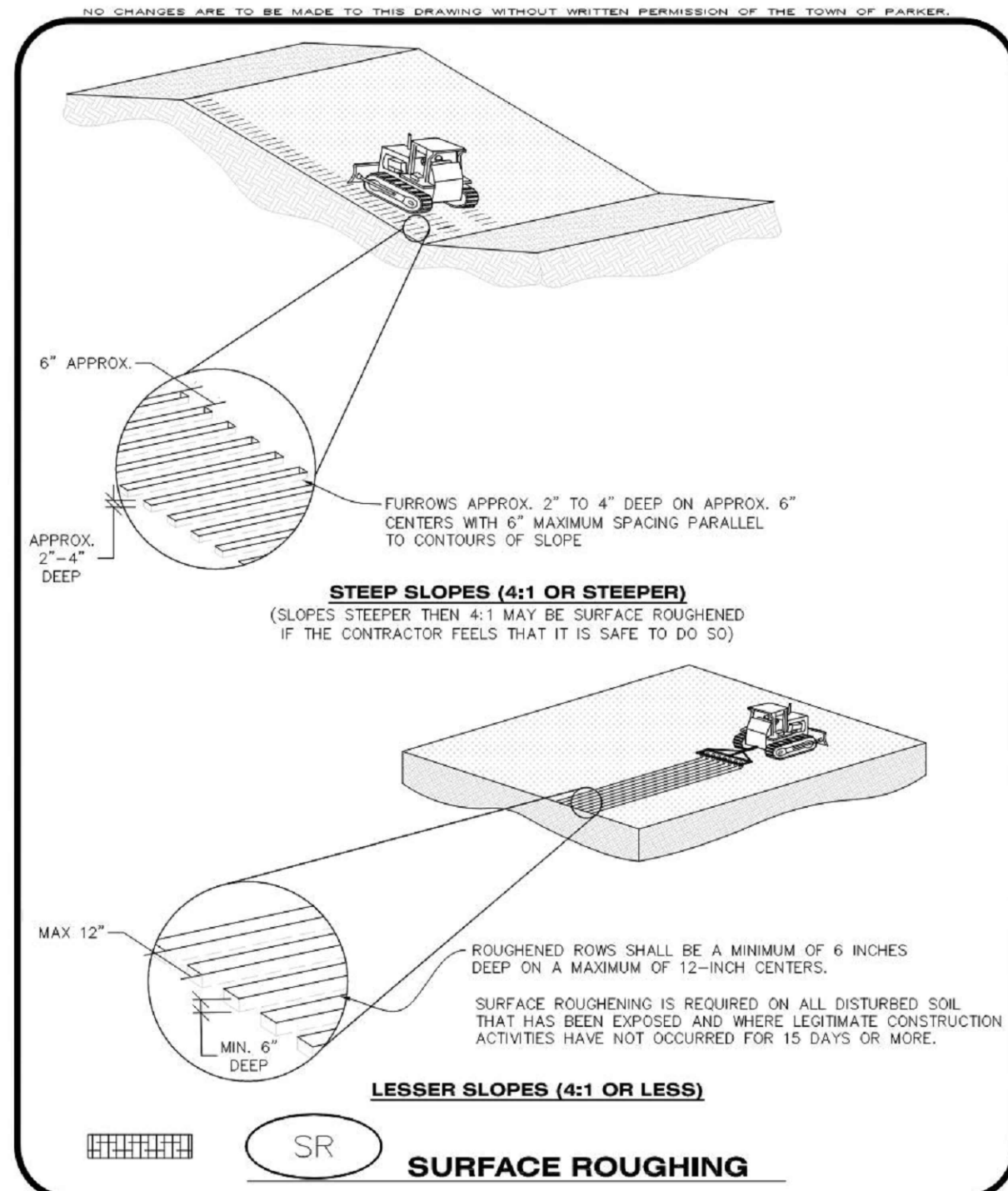
PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

DRAWING TITLE: **EROSION CONTROL DETAILS**

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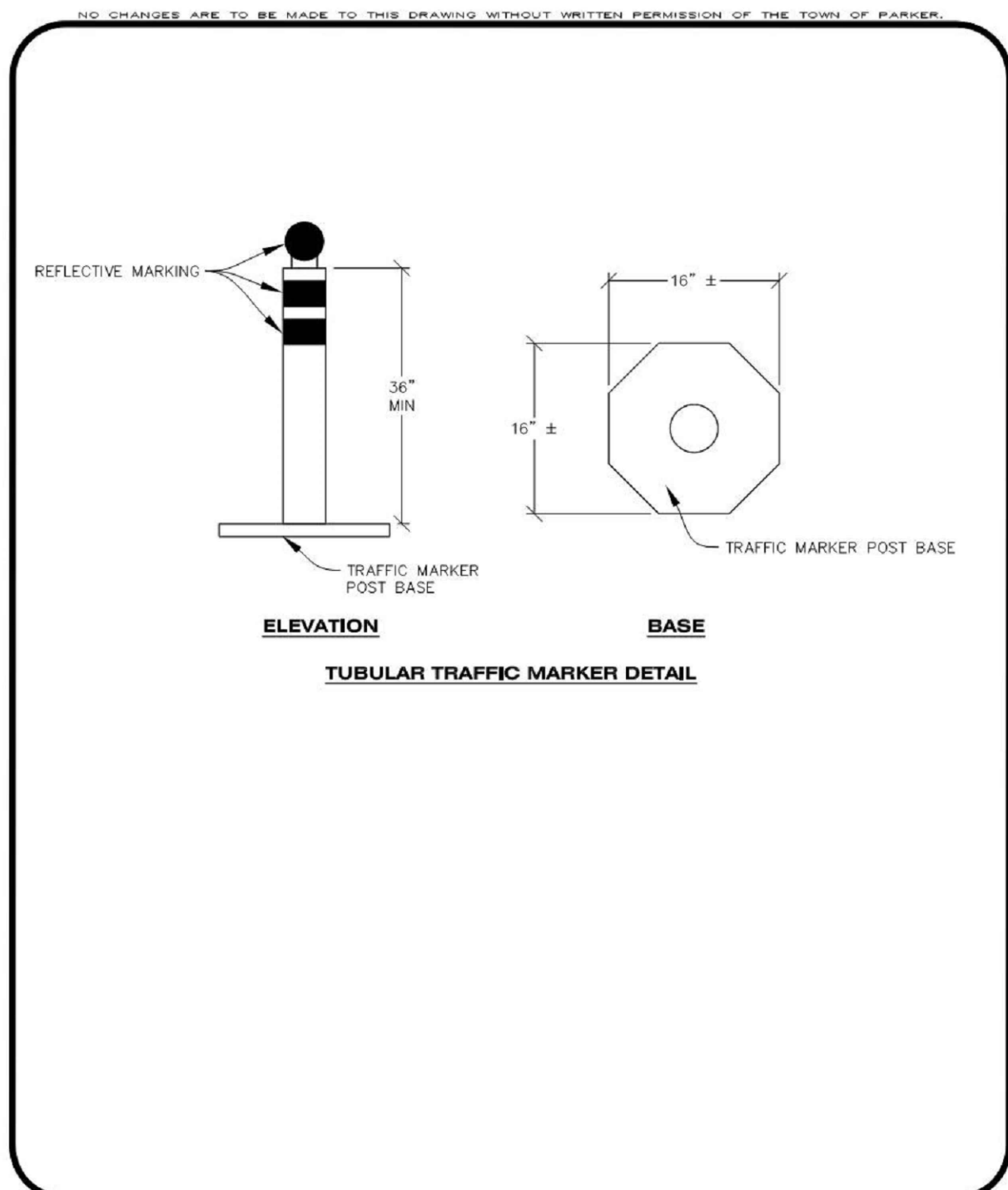
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DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139  
DRAWING NUMBER: **C-256**

PROFESSIONAL ENGINEER  
47666  
02/05/24



**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

SR  
1 OF 2  
Oct. 2013



**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

IPCOG  
2 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**TOWN OF PARKER, SEED MIX 1**  
20% CANADA WILDMIE  
15% CRESTED WHEATGRASS  
15% SLENDER WHEATGRASS  
10% ANNUAL RYEGRASS  
10% SHEEP FESCUE  
10% BIG BLUESTEM  
10% SIBEOATS GRAMA  
5% CANADA BLUEGRASS  
5% BLUE GRAMA

**SEEDING RATE:**  
DRILLED: 25 LBS/ACRE  
BROADCAST: 50 LBS/ACRE

**TOWN OF PARKER, SEED MIX 2**  
22% SLENDER WHEATGRASS  
18% SODAR STREAMBANK WHEATGRASS  
13% ARIZONA FESCUE  
13% BLUE GRAMA  
12% BUFFALOGRASS  
12% BARLEY OR OATS  
5% SPIKE MUHLY  
5% INDIAN RICEGRASS

**SEEDING RATE:**  
DRILLED: 25 LBS/ACRE  
BROADCAST: 50 LBS/ACRE

**TOWN OF PARKER, SEED MIX 3 (LOW-GROWTH MIX)**  
25% EPHRAIM CRESTED WHEATGRASS  
23% SHEEP FESCUE  
18% PERENNIAL RYEGRASS  
13% CANADA BLUEGRASS  
12% BARLEY OR OATS  
9% BLUE FESCUE

**SEEDING RATE:**  
DRILLED: 25 LBS/ACRE  
BROADCAST: 50 LBS/ACRE

**SEED MIX 4:**  
OTHER SEED MIXES APPROVED BY THE TOWN OF PARKER

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

SMC  
3 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SEEDING AND MULCHING SHALL BE PERFORMED ACCORDING TO THE ACCOMPANYING DETAIL(S) AND TEXT. NO EXCEPTIONS SHALL BE MADE**

- SEE PLAN VIEW FOR:
  - LOCATION(S) OF SEEDING AND MULCHING
  - TYPE OF SEED MIX
- SEED MIXES MAY CONFORM TO THE TABLE PROVIDED WITH THE SMC NOTES OR ALTERNATIVES MAY BE ALLOWED WITH PRIOR PERMISSION BY THE TOWN'S INSPECTOR.
- SEEDING MAY BE PERFORMED YEAR ROUND ASSUMING THE SOIL IS NOT FROZEN. SEEDING DURING TIMES OF EXTREME TEMPERATURES SHOULD BE AVOIDED IF POSSIBLE.
- AT THE BEGINNING OF THE LAND DISTURBANCE ACTIVITIES, IT IS HIGHLY RECOMMENDED THAT AN APPROPRIATE AMOUNT OF NATIVE TOPSOIL BE STRIPPED FROM THE SITE AND STOCKPILED. ALL AREAS PRIOR TO PERMANENT SEEDING AND MULCHING WILL LIKELY NEED TO BE COVERED WITH AN APPROPRIATE LAYER OF TOPSOIL. THIS REQUIREMENT APPLIES TO ALL AREAS WHERE NATIVE SEEDING IS SPECIFIED ON THE CBMP PLAN AND/OR LANDSCAPING PLANS.
- IT IS STRONGLY RECOMMENDED THAT SAMPLES FROM THE STRIPPED TOPSOIL BE PROPERLY COLLECTED AND TESTED BY A QUALIFIED LABORATORY TO ENSURE ADEQUATE NUTRIENT CONTENT PRIOR TO SEEDING AND MULCHING. IF IT IS DISCOVERED THAT THE TOPSOIL IS VOID OF THE NUTRIENTS NECESSARY TO SUCCESSFULLY ESTABLISH THE REQUIRED VEGETATION, THEN THE APPROPRIATE AMENDMENTS SHALL BE ADDED.
- ALL AREAS TO BE SEEDING AND MULCHED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING DETAILS AND NOTES. SURFACE ROUGHENING SHALL OCCUR AFTER PLACEMENT OF THE TOPSOIL.
- WHEN INSTALLED WITH A DRILL SEEDER, SEED SHALL BE PLACED AT A DEPTH OF 1/4 - 1/2 INCH. ROW SPACING SHALL BE NO MORE THAN 6-INCHES.
- ALL AREAS INCAPABLE OF BEING DRILL SEEDER SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING NOTES OR EFFECTIVELY ROUGHENED USING A HARROW OR OTHER SUCH IMPLEMENT. ALL AREAS SHALL BE UNIFORMLY HAND BROADCASTED WITH THE PROPER SEED MIX APPLIED AT TWO TIMES THE DRILL SEEDER RATE. BROADCASTED AREAS SHALL THEN BE RE-HARROWED OR RE-RAKED USING A HARD-TIPPED RAKE TO ENSURE THAT SEEDS ARE BURIED TO AN APPROXIMATE DEPTH OF 1/4 - 1/2 INCH.
- AFTER SEEDING HAS BEEN COMPLETED, MULCH SHALL BE UNIFORMLY APPLIED AT A RATE OF 2 TONS/ACRE (4,000 LBS/ACRE). MULCH SHALL BE MECHANICALLY CRIMPED TO A DEPTH OF 2 INCHES USING A CRIMPER. MULCH SHALL BE HAND CRIMPED AND COVERED WITH A TACKIFIER IN AREAS WHERE MECHANICAL CRIMPING IS NOT POSSIBLE, WHEN SOILS PERMIT. ALL MULCH SHALL BE CRIMPED SUCH THAT THE INDIVIDUAL PIECES OF STRAW OR HAY FORM EXAGGERATED V-SHAPES PROTRUDING OUT OF THE GROUND SEVERAL INCHES.
- IN CERTAIN INSTANCES, IT MAY BE NECESSARY TO APPLY A TACKIFIER IN ORDER TO HELP WITH STRAW DISPLACEMENT. TACKIFIER SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

SMC  
1 OF 3  
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**SURFACE ROUGHENING INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SURFACE ROUGHENING.
- DISTURBED AREAS THAT REMAIN INACTIVE FOR 15 DAYS OR MORE MUST RECEIVE SURFACE ROUGHENING OR ANOTHER APPROVED BMP FROM THE SDCM. DETERMINATION OF JOB SITE INACTIVITY IS AT THE DISCRETION OF THE TOWN'S INSPECTOR.
- FOR STEEP SLOPES (3:1 OR STEEPER), IT IS ACCEPTABLE TO "TRACK" THE SLOPES, ACCORDING TO THE CBMP DETAILS.
- SCHEDULES FOR REQUIRING STABILIZATION MAY BE MODIFIED BY THE PERMITTEE TO ALLOW FOR SPECIAL CONSIDERATIONS SUCH AS STABILIZING ACCESS AREAS AND AREAS IN CLOSE PROXIMITY TO CONTINUING CONSTRUCTION.

**SURFACE ROUGHENING INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL PROACTIVELY INSPECT THE SURFACE ROUGHENING.

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

SR  
2 OF 2  
Oct. 2013

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**INLET PROTECTION, CURB ON-GRADE INSTALLATION NOTES**

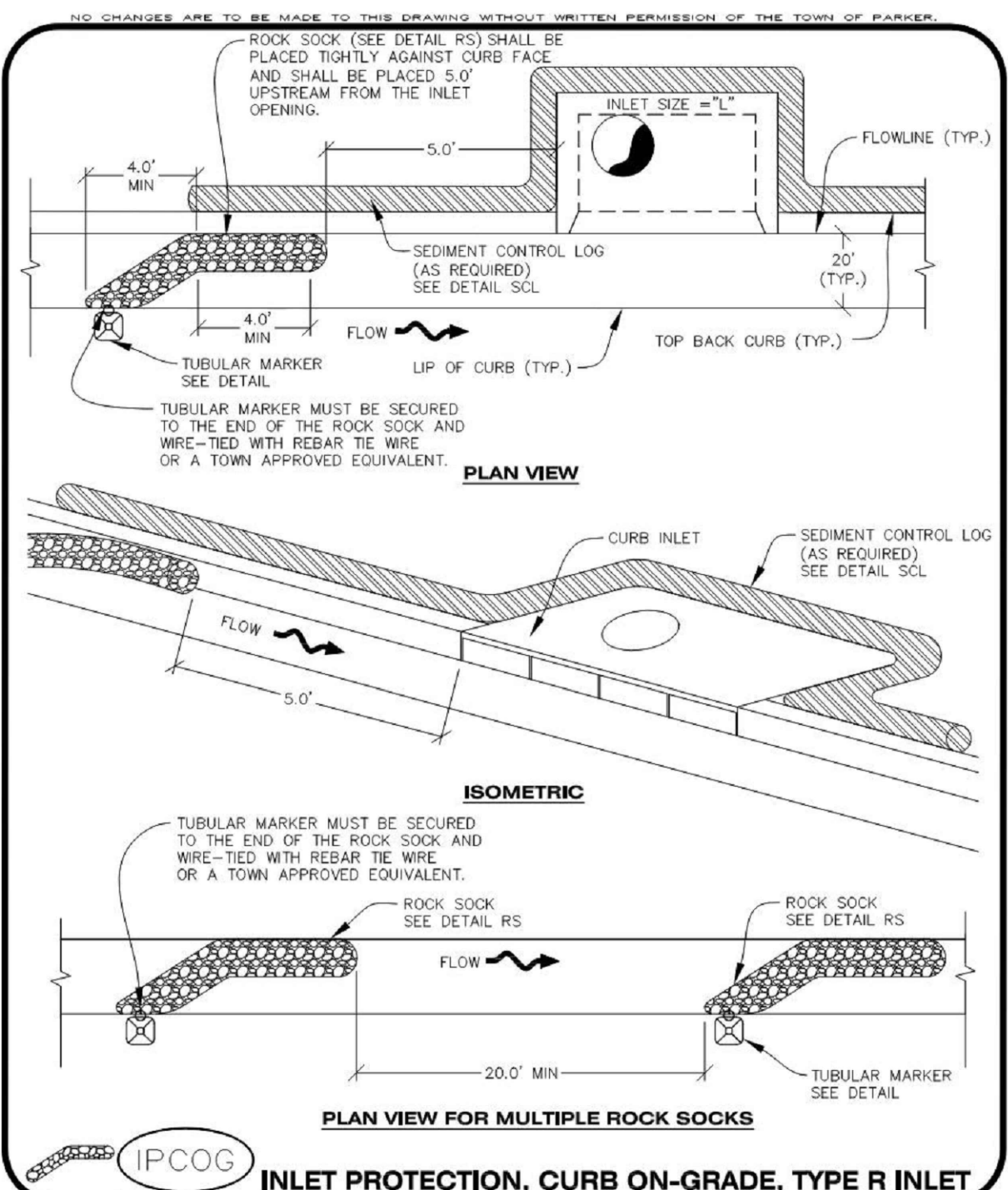
- SEE CBMP PLAN FOR LOCATION(S) OF ON-GRADE INLET PROTECTION.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR ON-GRADE INLET PROTECTION SHALL BE ONE CONTINUOUS PIECE.
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.
- ROCK SOCK SHALL BE PLACED 5.0' UPHILL OF THE INLET OPENING.
- TUBULAR MARKER SHALL BE A MINIMUM OF 3.0' HIGH WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
- THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE APPLICABLE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE INLET.

**ON-GRADE INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ON-GRADE INLET PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ON-GRADE INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

IPCOG  
3 OF 3  
Oct. 2013



**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

IPCOG  
1 OF 3  
Oct. 2013

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**SEEDING AND MULCHING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEEDING AND MULCHING.
- ANY SEEDED AND MULCHED AREAS THAT BECOME DAMAGED SHALL BE REPAIRED WITHIN THE TIME FRAME SPECIFIED BY THE TOWN'S INSPECTOR.

**WEED MANAGEMENT**

- ALL HERBICIDES SHALL BE APPLIED BY COMMERCIAL PESTICIDE APPLICATORS LICENSED BY THE COLORADO DEPARTMENT OF AGRICULTURE AS QUALIFIED APPLICATORS. THE CONTRACTOR SHALL FURNISH DOCUMENTATION OF SUCH LICENSING PRIOR TO HERBICIDE APPLICATION.
- HERBICIDE APPLICATION METHOD SHALL BE SUCH THAT PLANT GROWTH OUTSIDE THE DESIGNATED TREATMENT AREAS WILL NOT BE DAMAGED. ALL DAMAGE CAUSED BY IMPROPER HERBICIDE APPLICATION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- HERBICIDES SHALL BE APPLIED DURING THE APPROPRIATE SEASONS, WHEN TARGET PLANTS ARE ACTIVELY GROWING.
- AFTER THE GRASS SEED IS ESTABLISHED, APPROPRIATE HERBICIDES SHALL BE APPLIED TO CONTROL THE REMAINING WEEDS TO ENSURE A TIMELY RETURN OF THE FINANCIAL SECURITY. PROPER TIMING OF HERBICIDE APPLICATIONS ARE NECESSARY TO ACHIEVE THE SUPPRESSION OF WEED SEED PRODUCTION AND DEPLETION OF WEED ROOT MASS. ULTIMATELY, THE HERBICIDES USED SHALL BE BASED UPON THE TARGET WEEDS.
- HERBICIDE TREATMENTS SHALL CONTINUE AT AN APPROPRIATE RATE UNTIL IT IS EVIDENT THAT WEED GROWTH PRESENCE AND GROWTH IS MINIMAL AND MAY BE CONTROLLED THROUGH MOWING AND/OR ANNUAL HERBICIDE TREATMENT.

**CBMP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES

SMC  
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02/05/2024 <td>SECOND TOWN SUBMITTAL</td>	SECOND TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

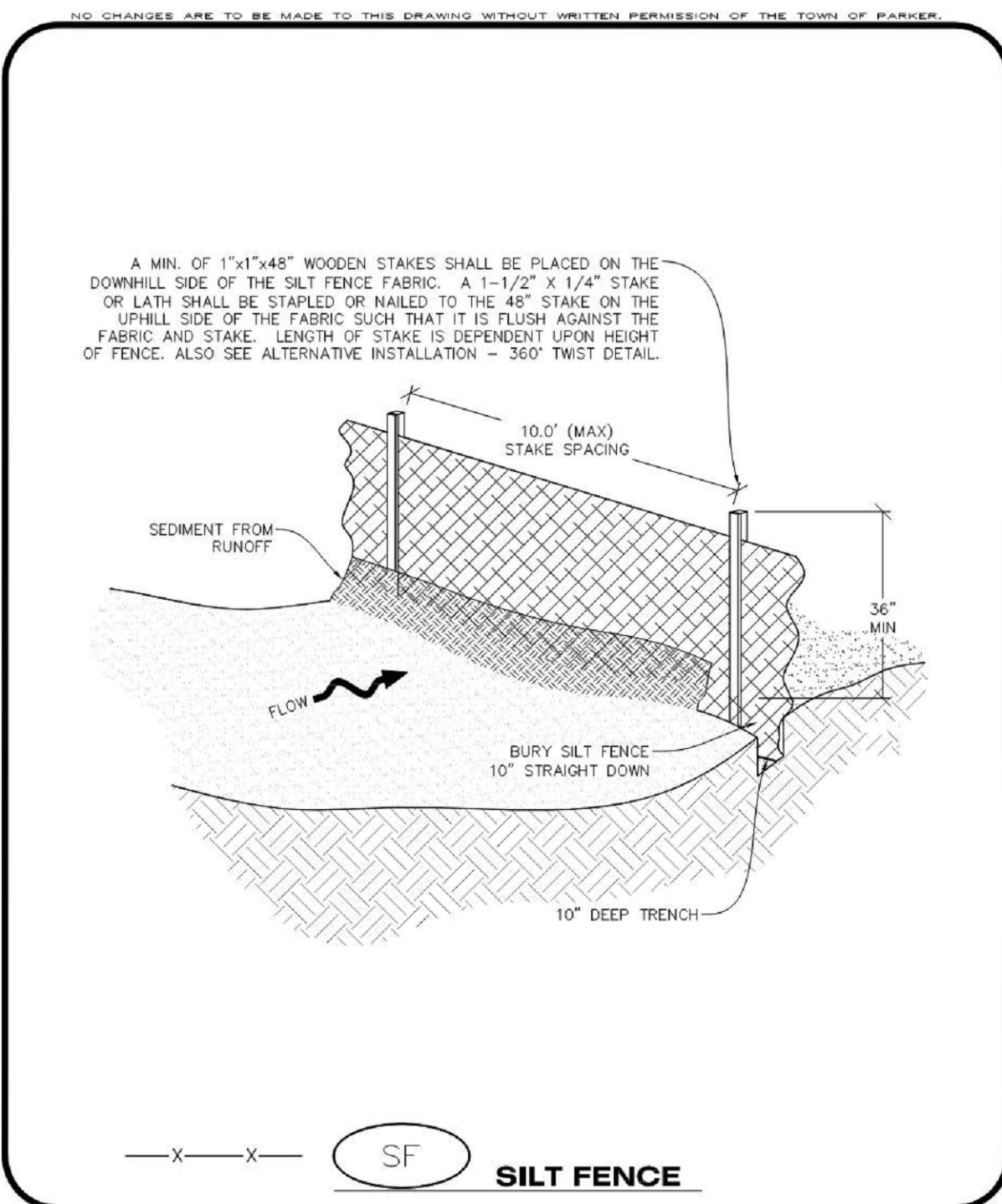
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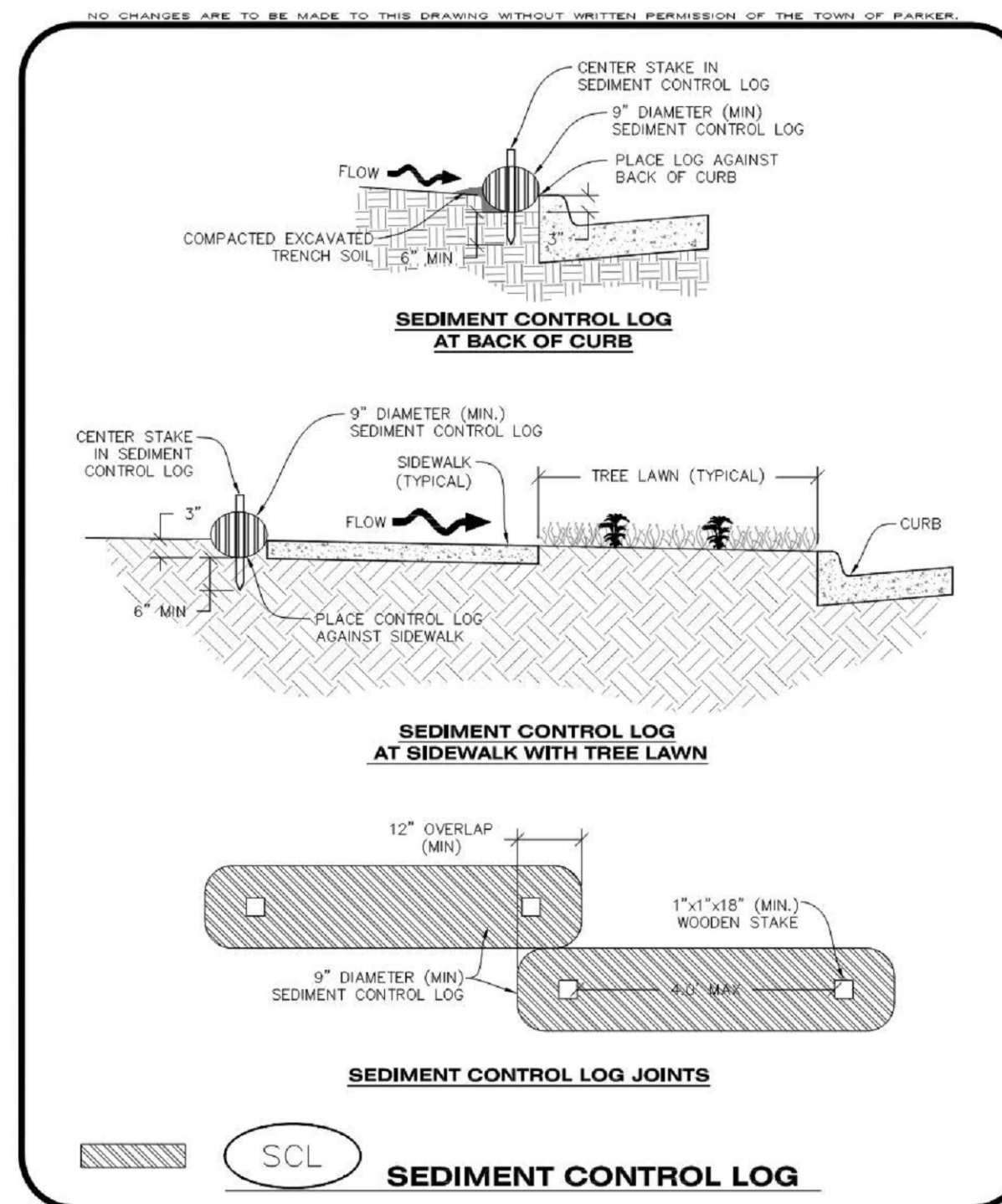
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02/05/24

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

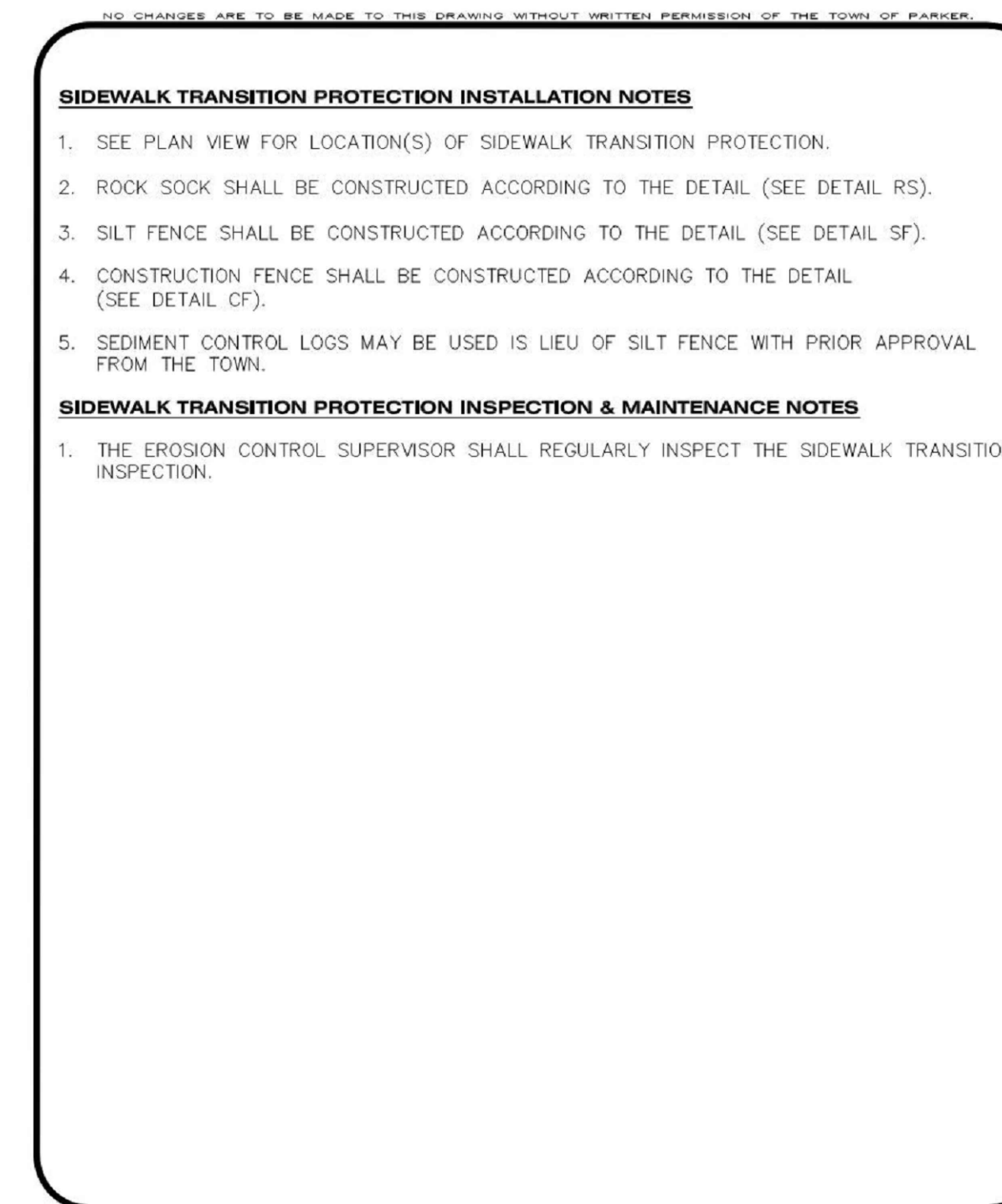
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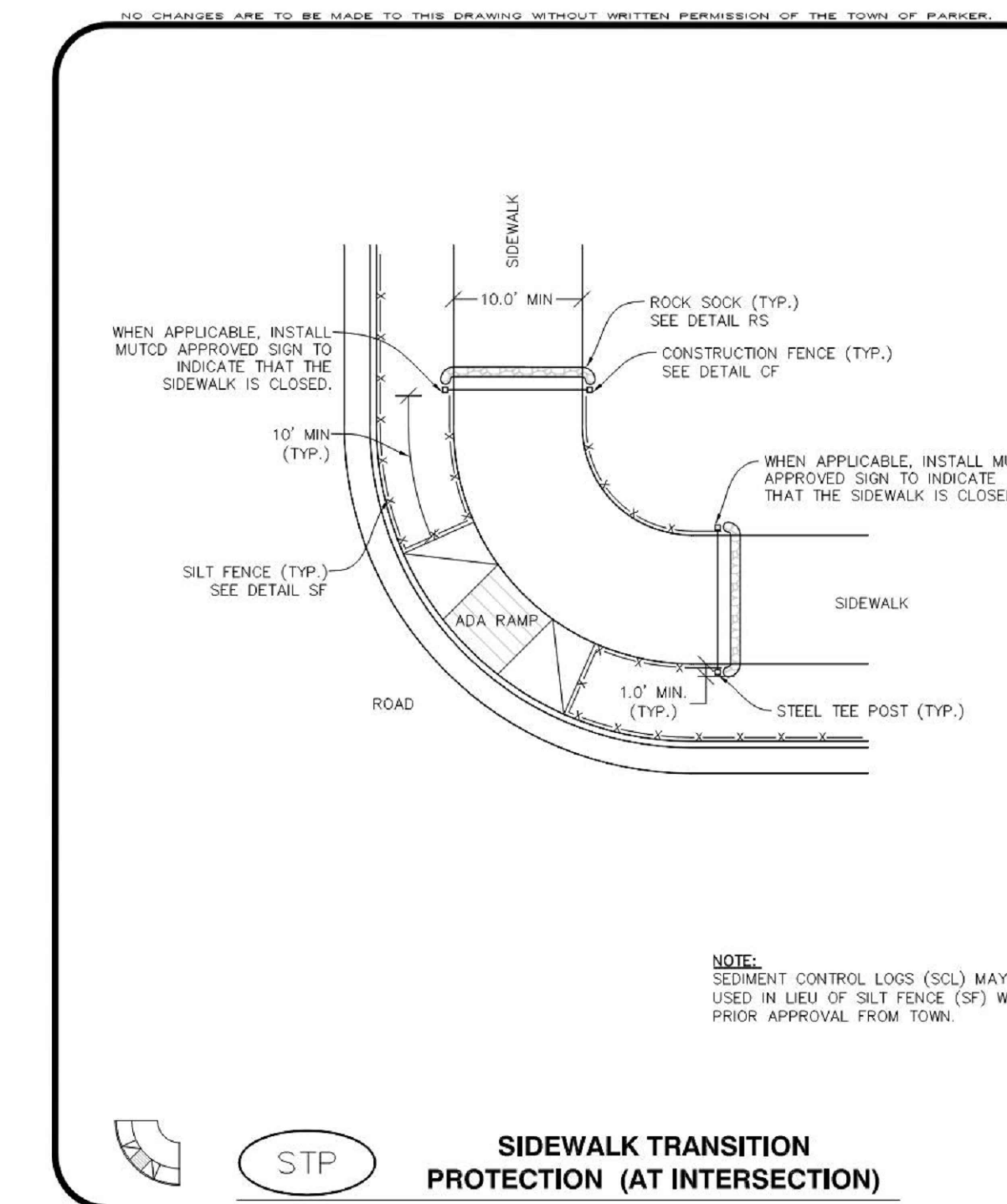
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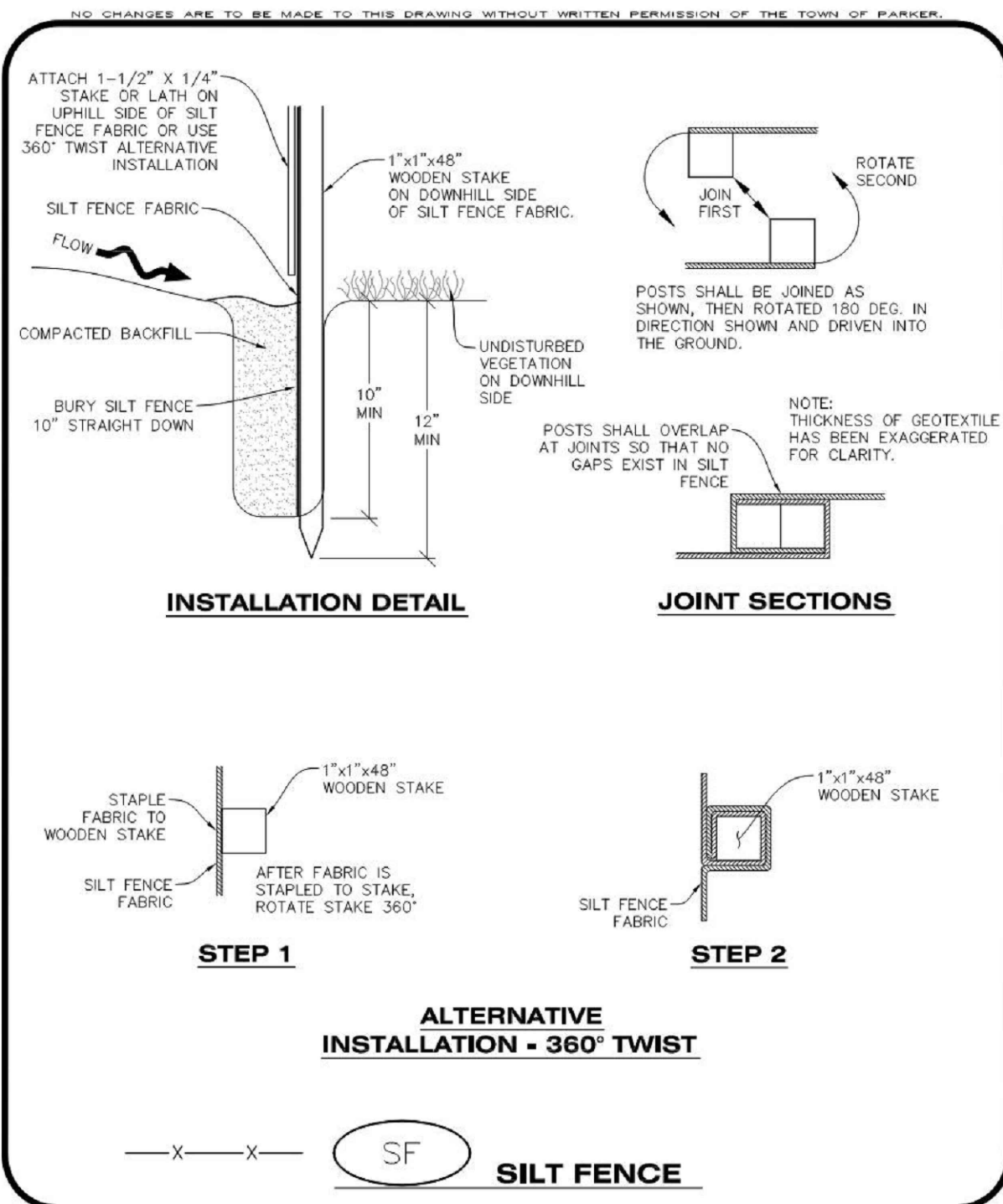
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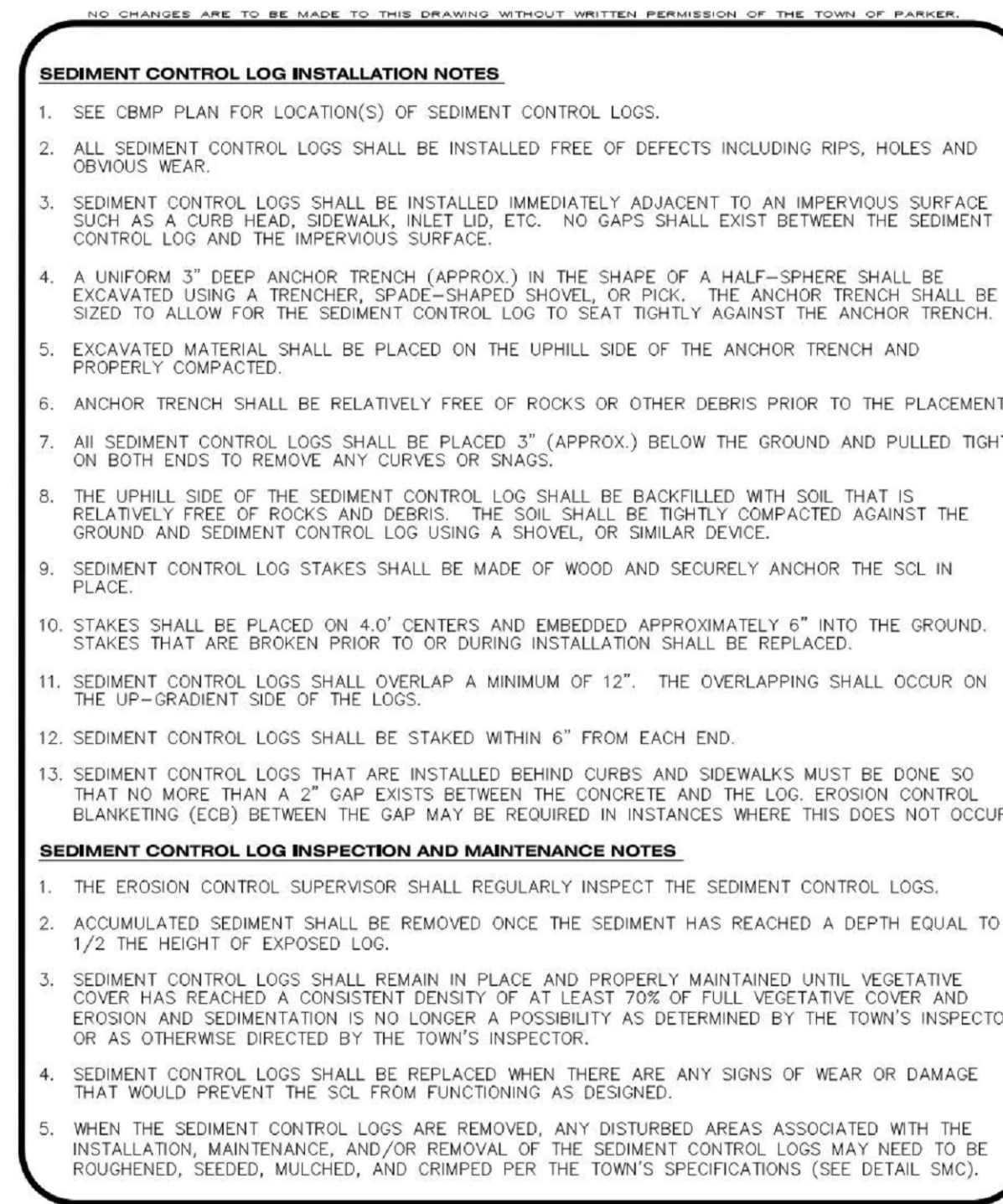
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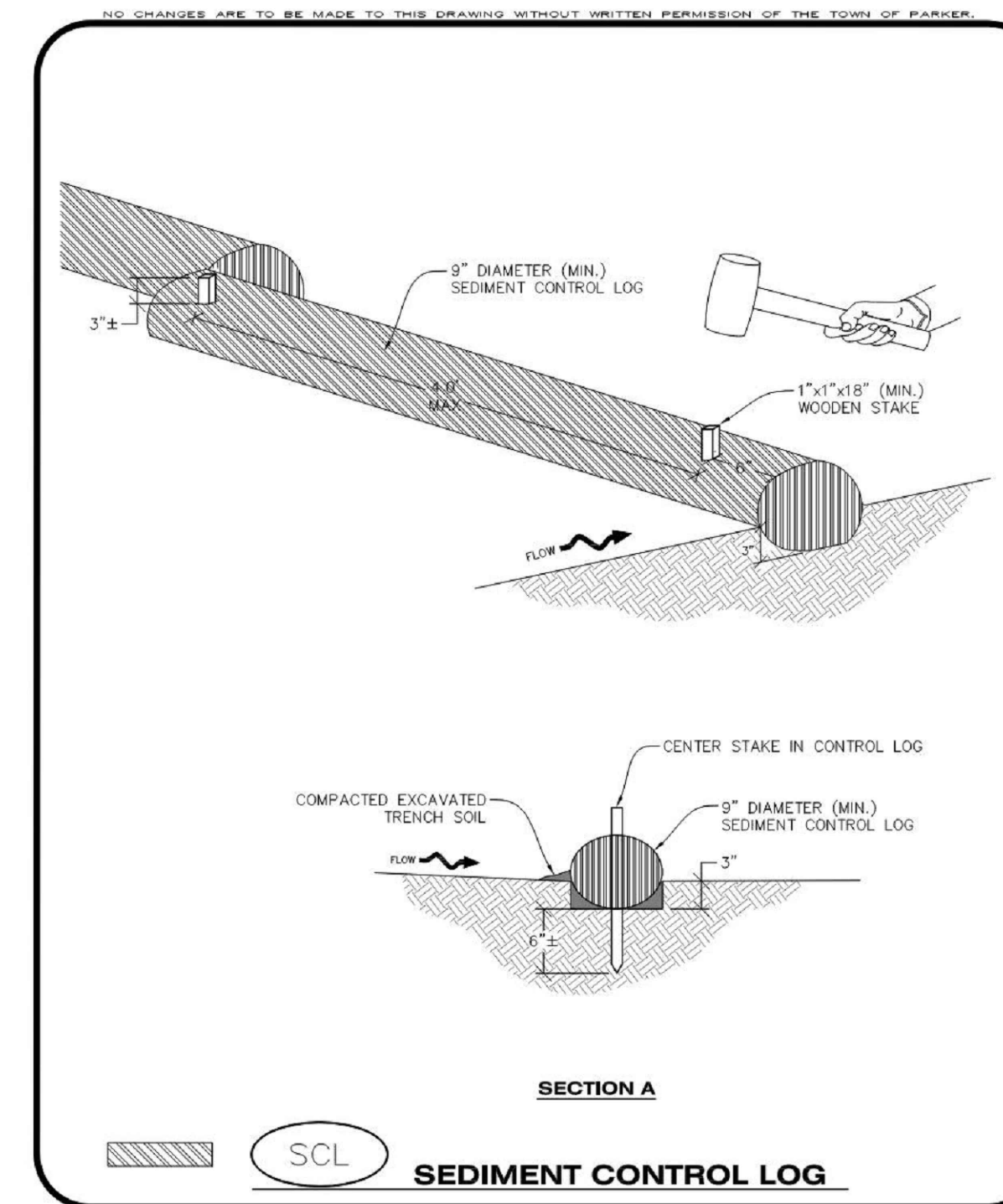
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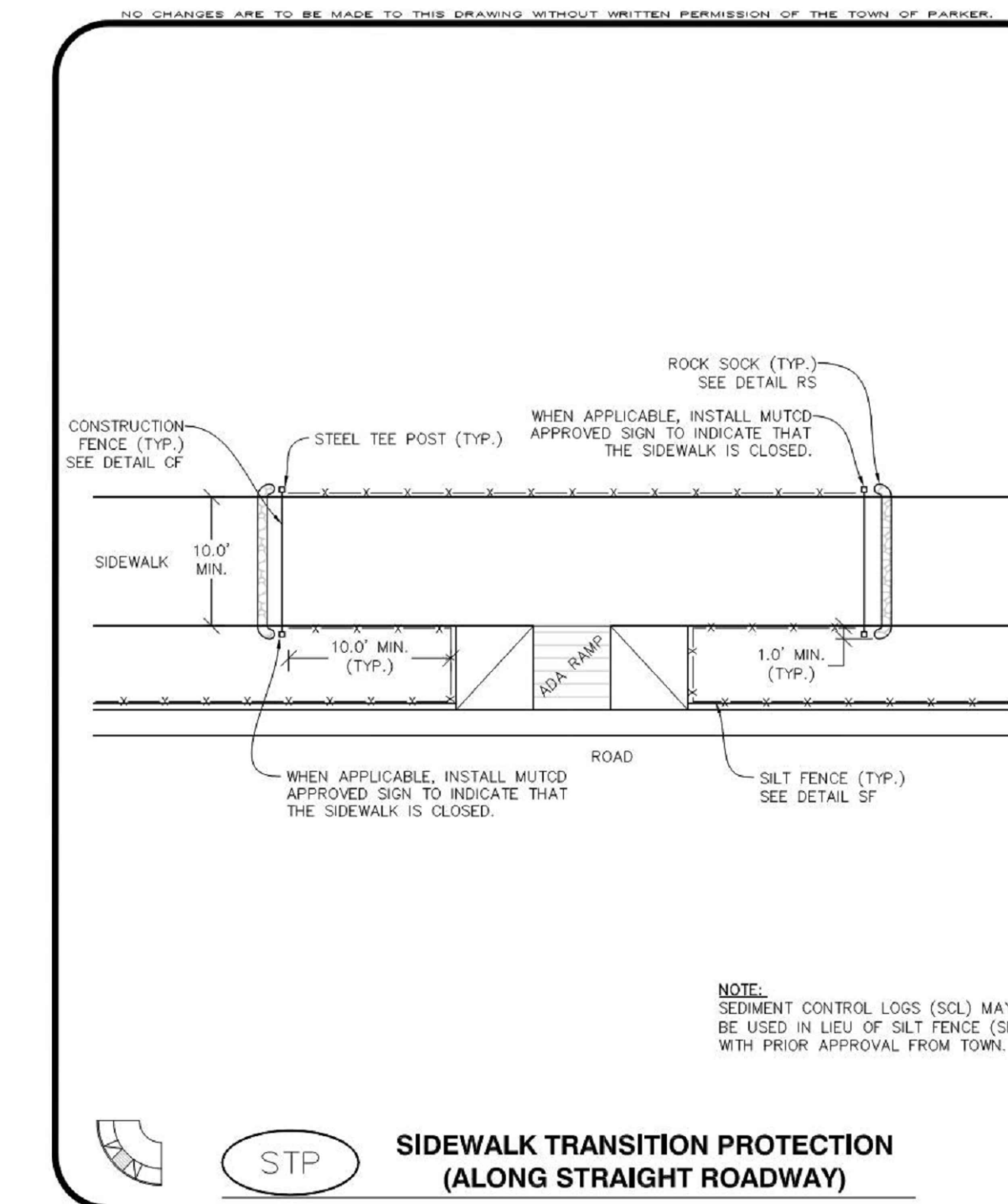
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**Parker** COLORADO **CONSTRUCTION BEST MANAGEMENT PRACTICES** **CBMP** **SCL** 3 OF 3 Oct. 2013



**Parker** COLORADO **CONSTRUCTION BEST MANAGEMENT PRACTICES** **CBMP** **SCL** 1 OF 3 Oct. 2013



**Parker** COLORADO **CONSTRUCTION BEST MANAGEMENT PRACTICES** **CBMP** **STP** 2 OF 3 Oct. 2013

NO.	DATE	DESCRIPTION
1	11/03/2023	FIRST TOWN SUBMITTAL
2	02/05/2024	SECOND TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAIN STREET**  
**PARKER, CO 80138**

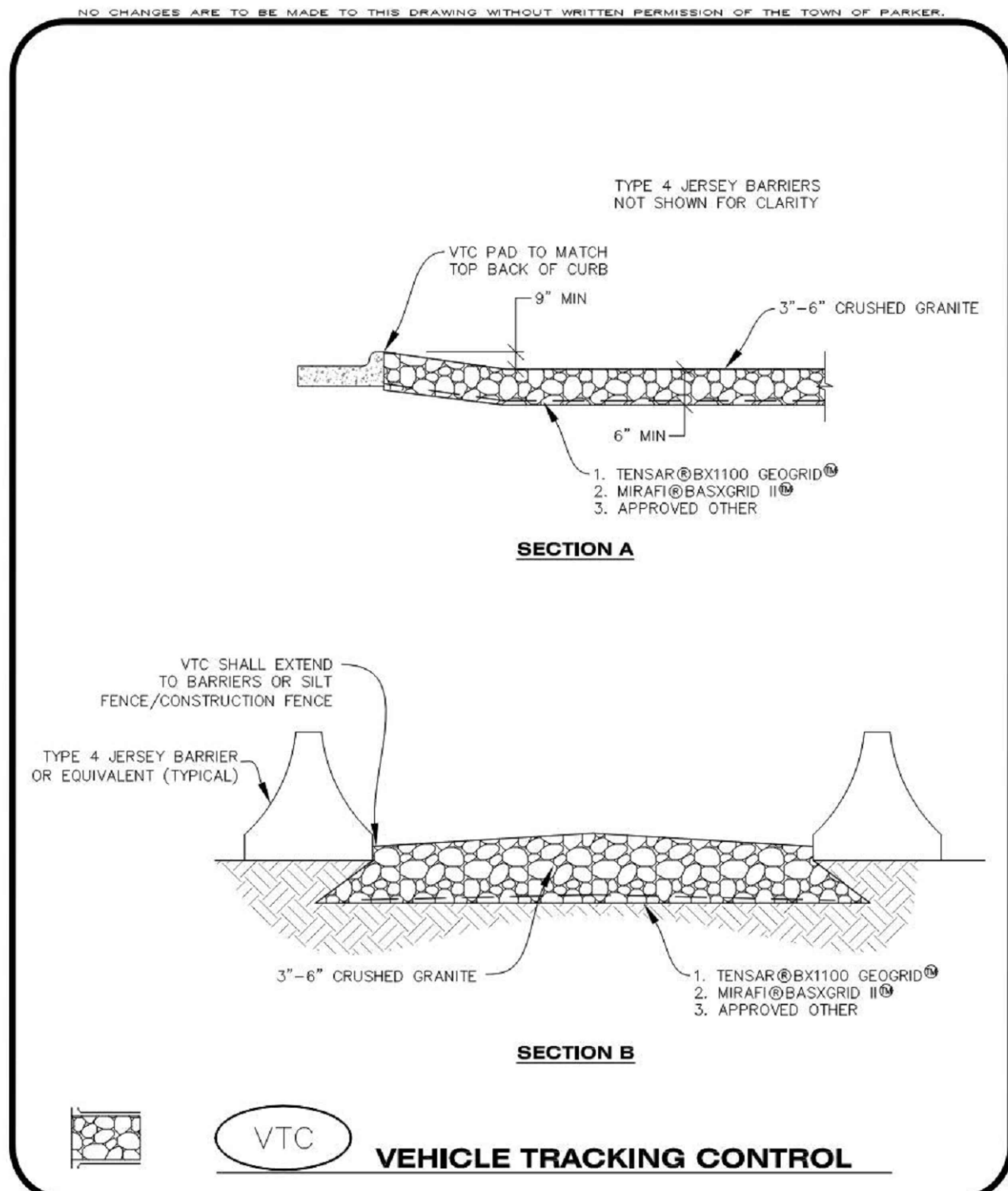
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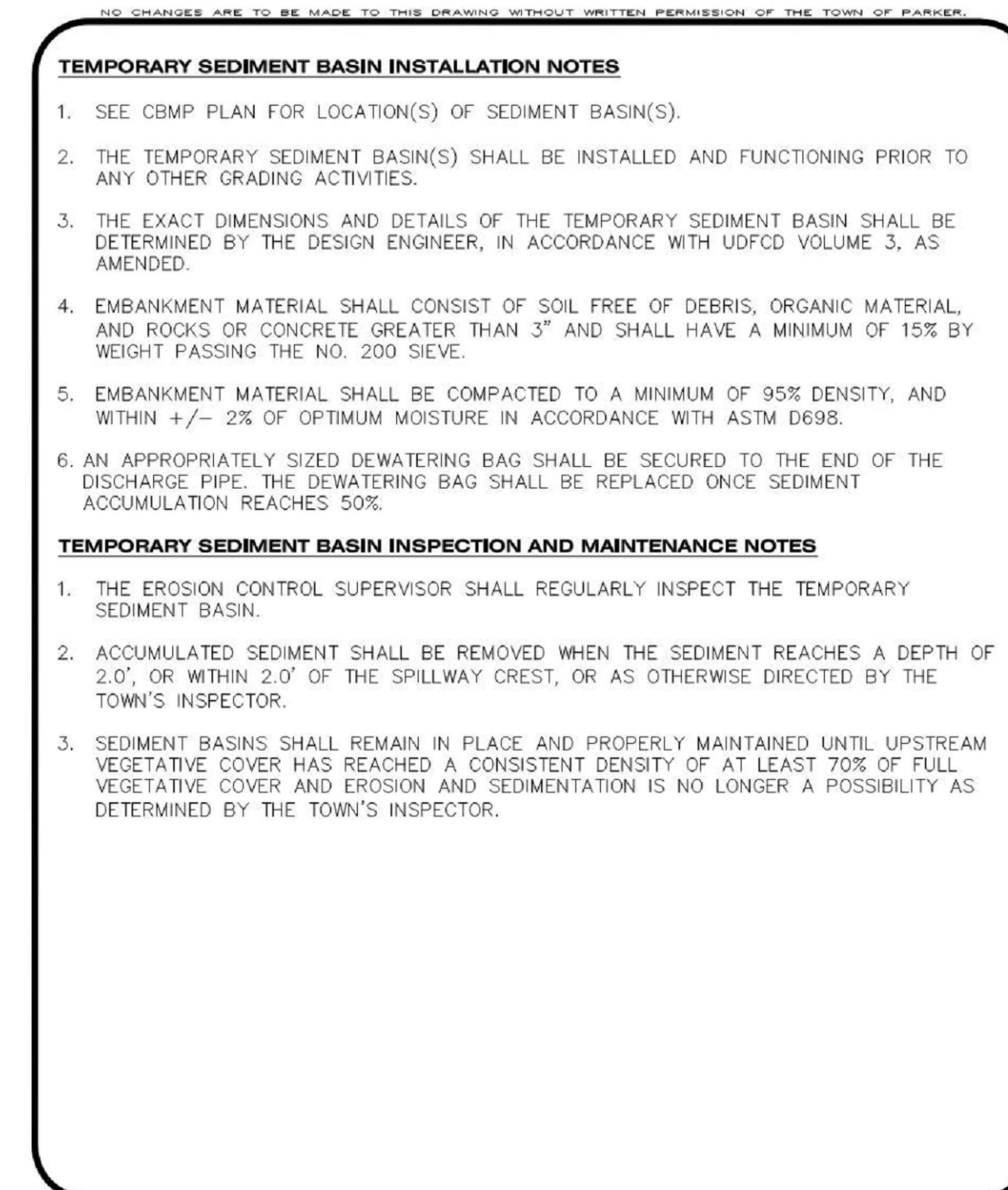


DESIGNED BY: R/JH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

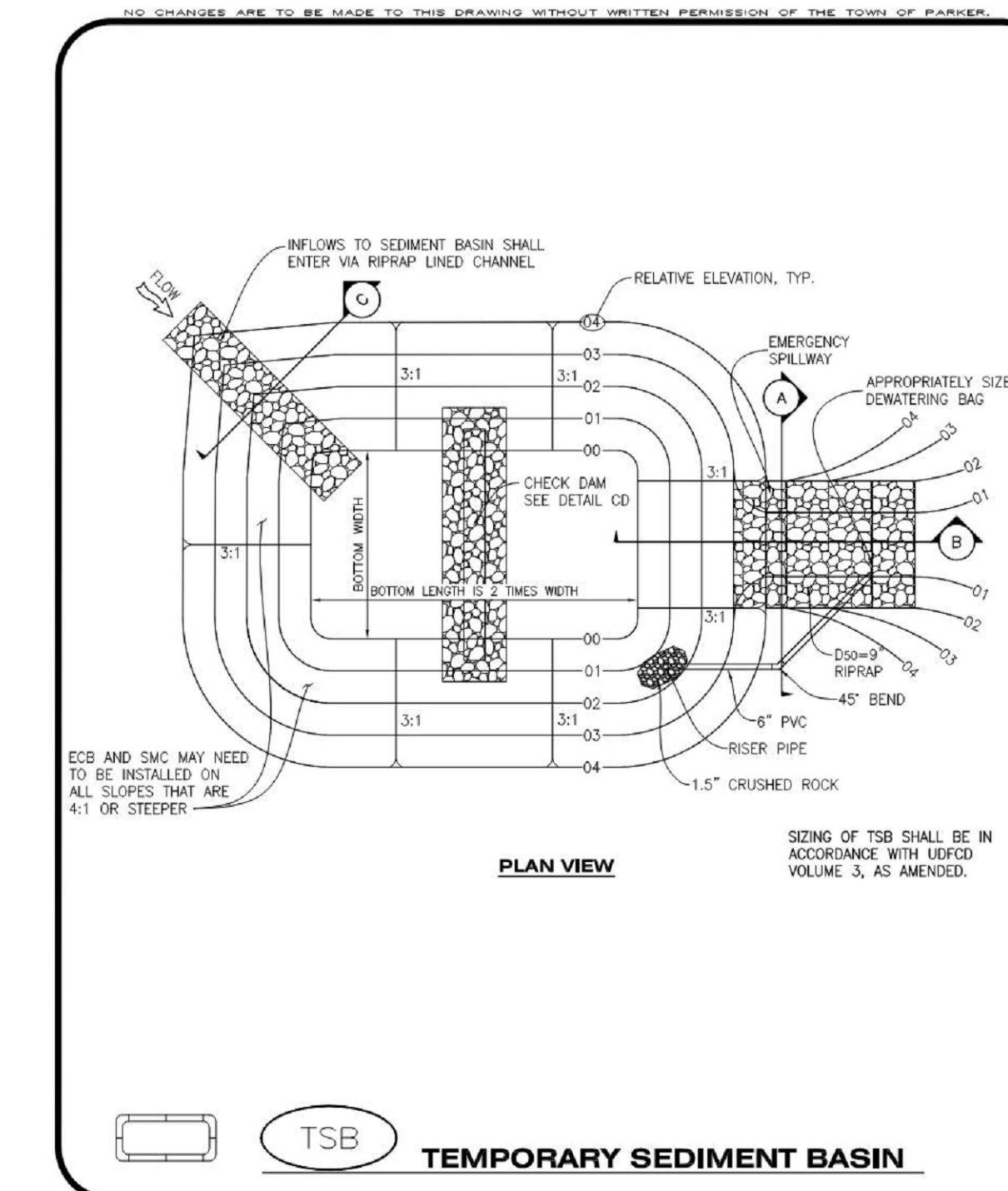
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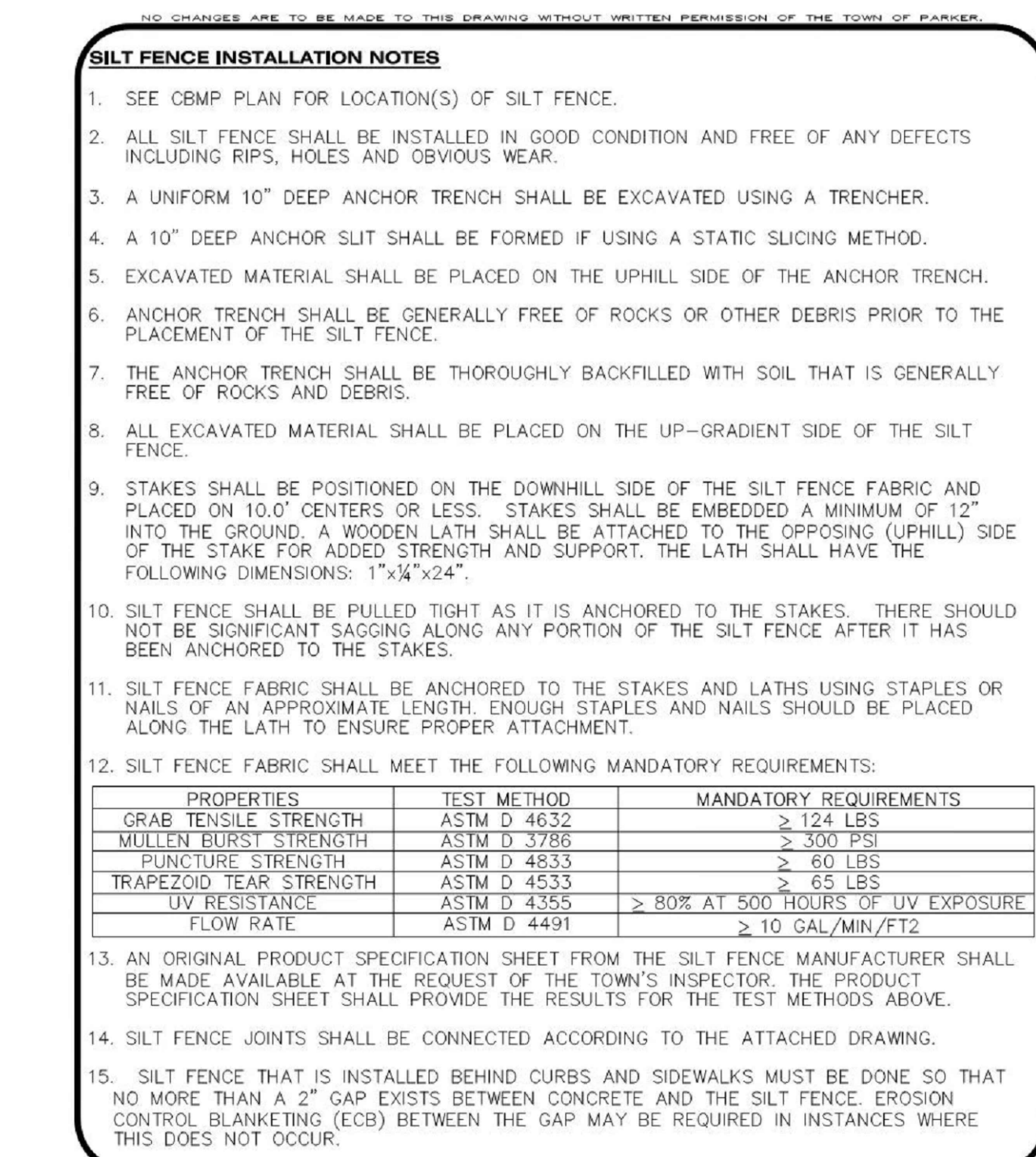
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
Oct. 2013



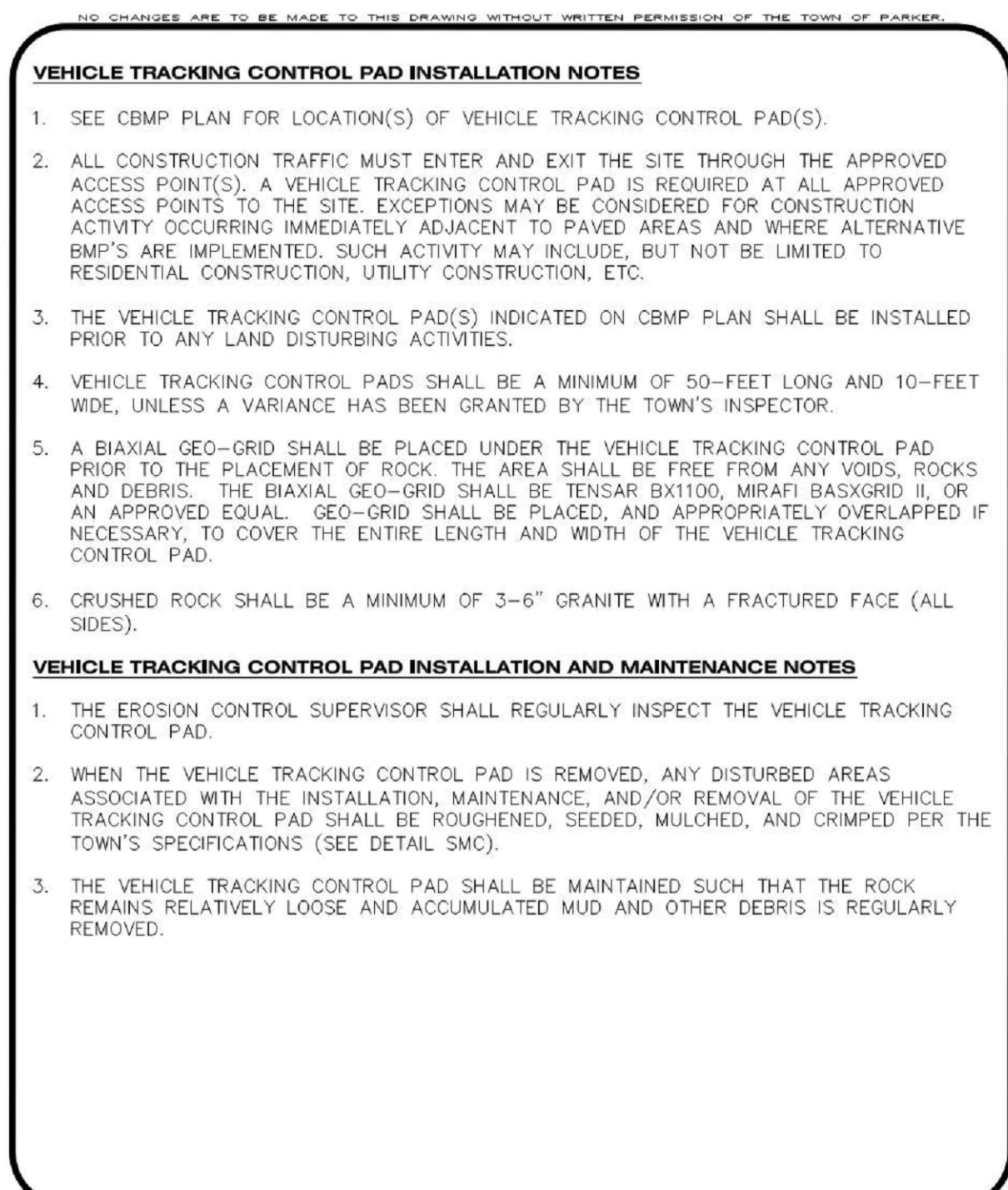
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
Oct. 2013



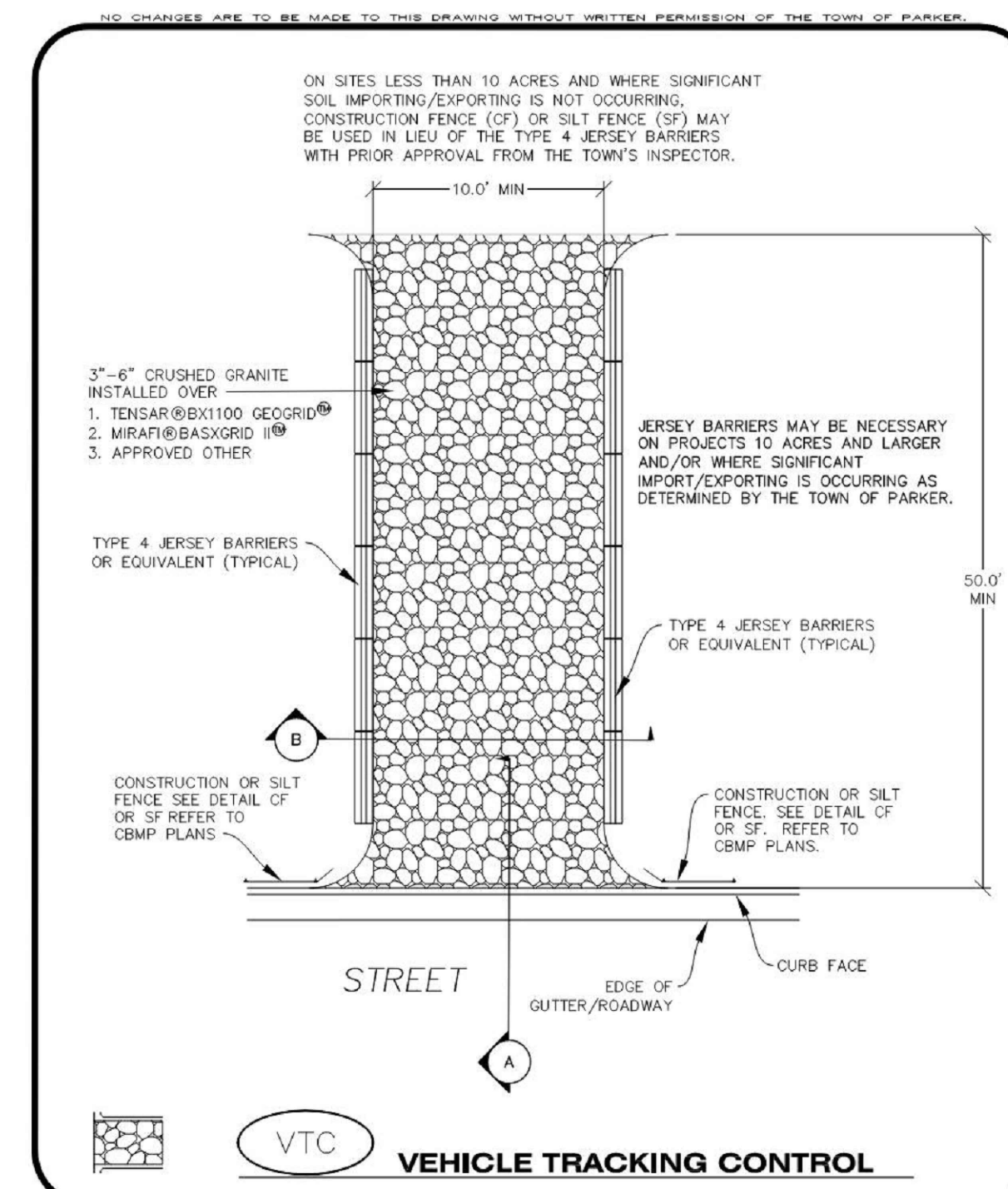
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
Oct. 2013



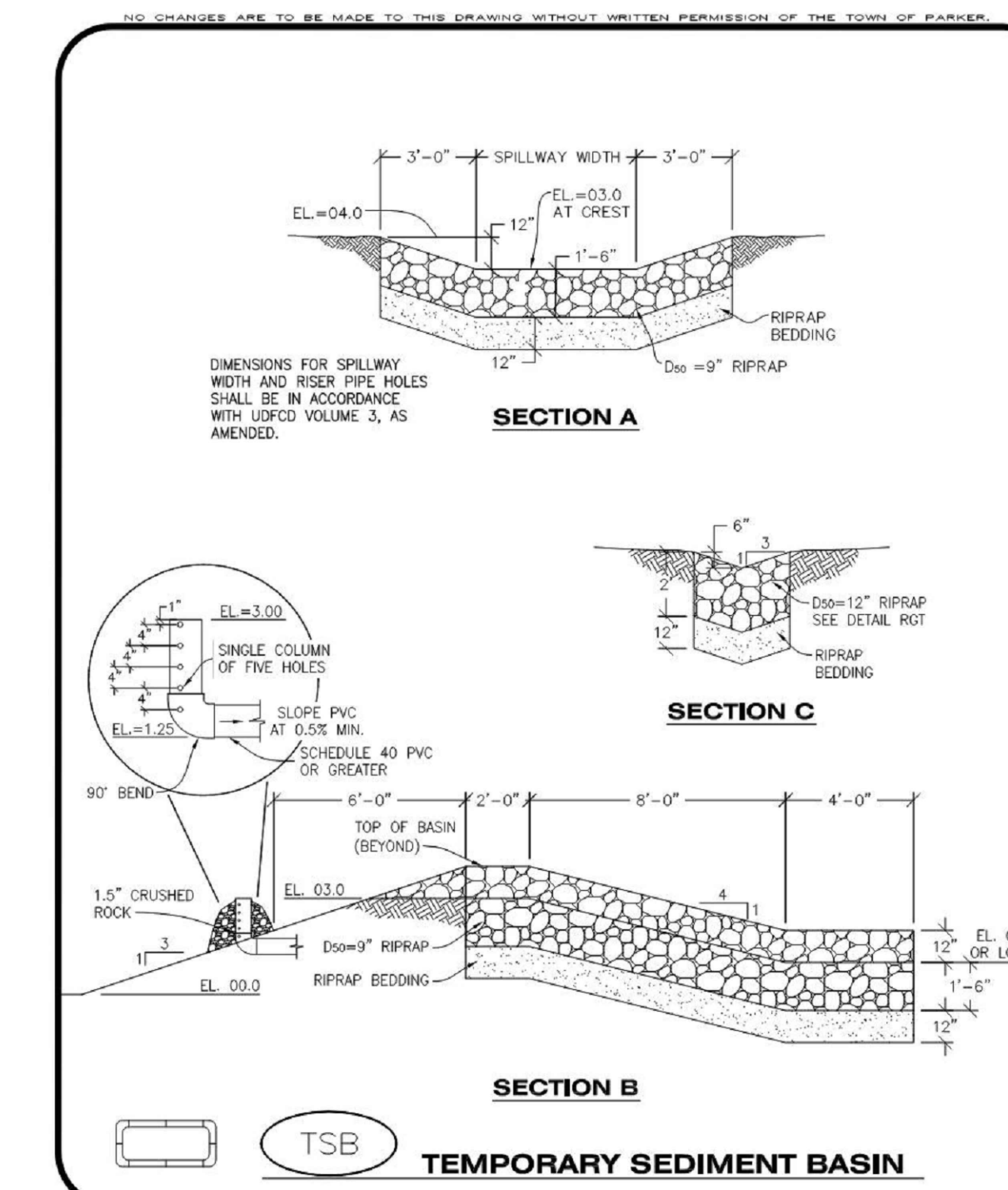
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 4  
Oct. 2013



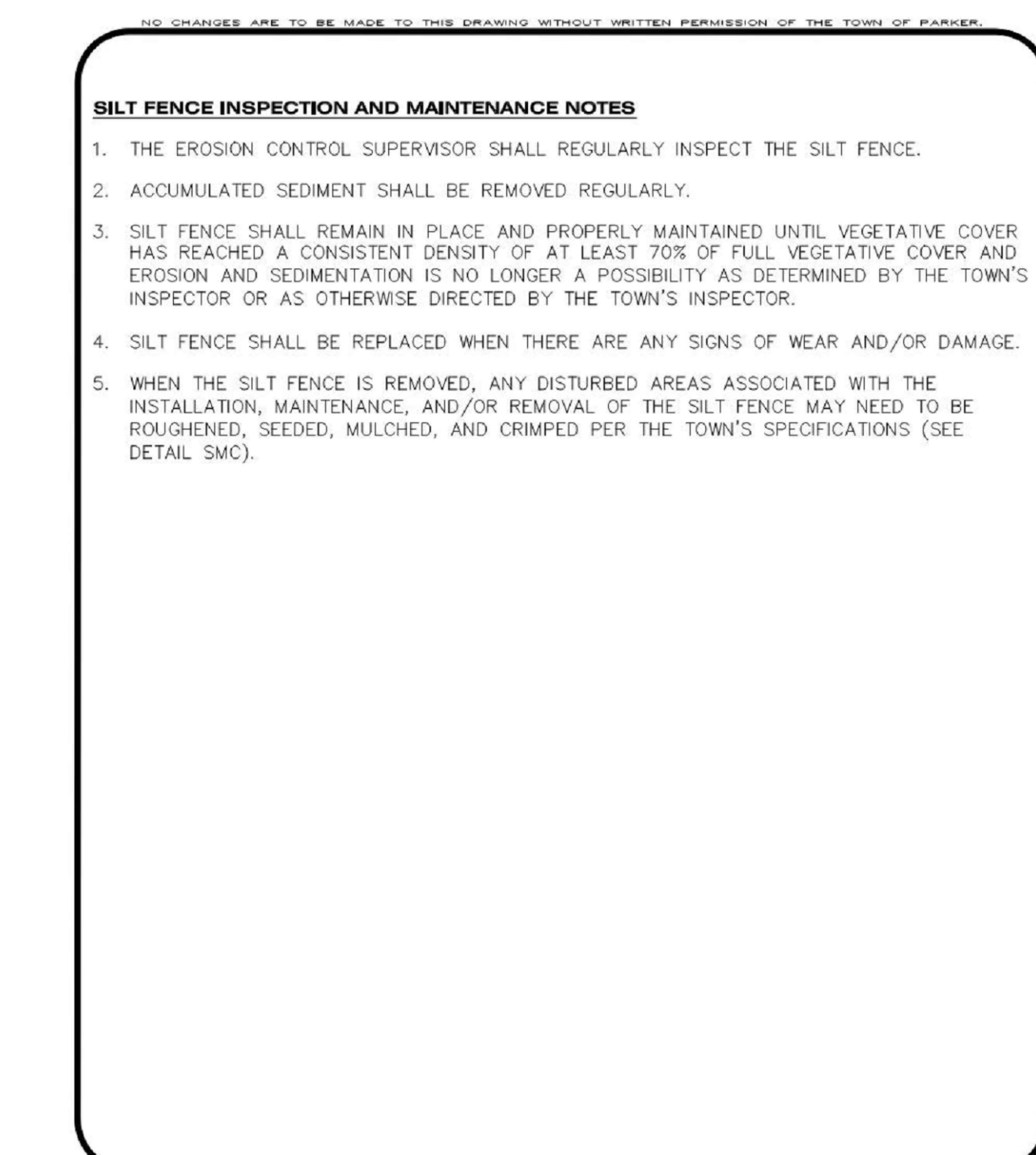
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
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**Parker COLORADO** | **CBMP** | **SF**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 4 OF 4  
Oct. 2013

DATE	DESCRIPTION
11/03/2023 <td>FIRST TOWN SUBMITTAL</td>	FIRST TOWN SUBMITTAL
02/05/2024 <td>SECOND TOWN SUBMITTAL</td>	SECOND TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

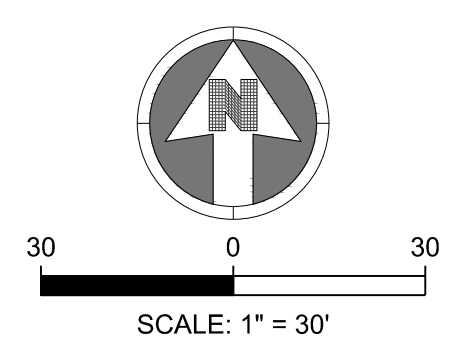
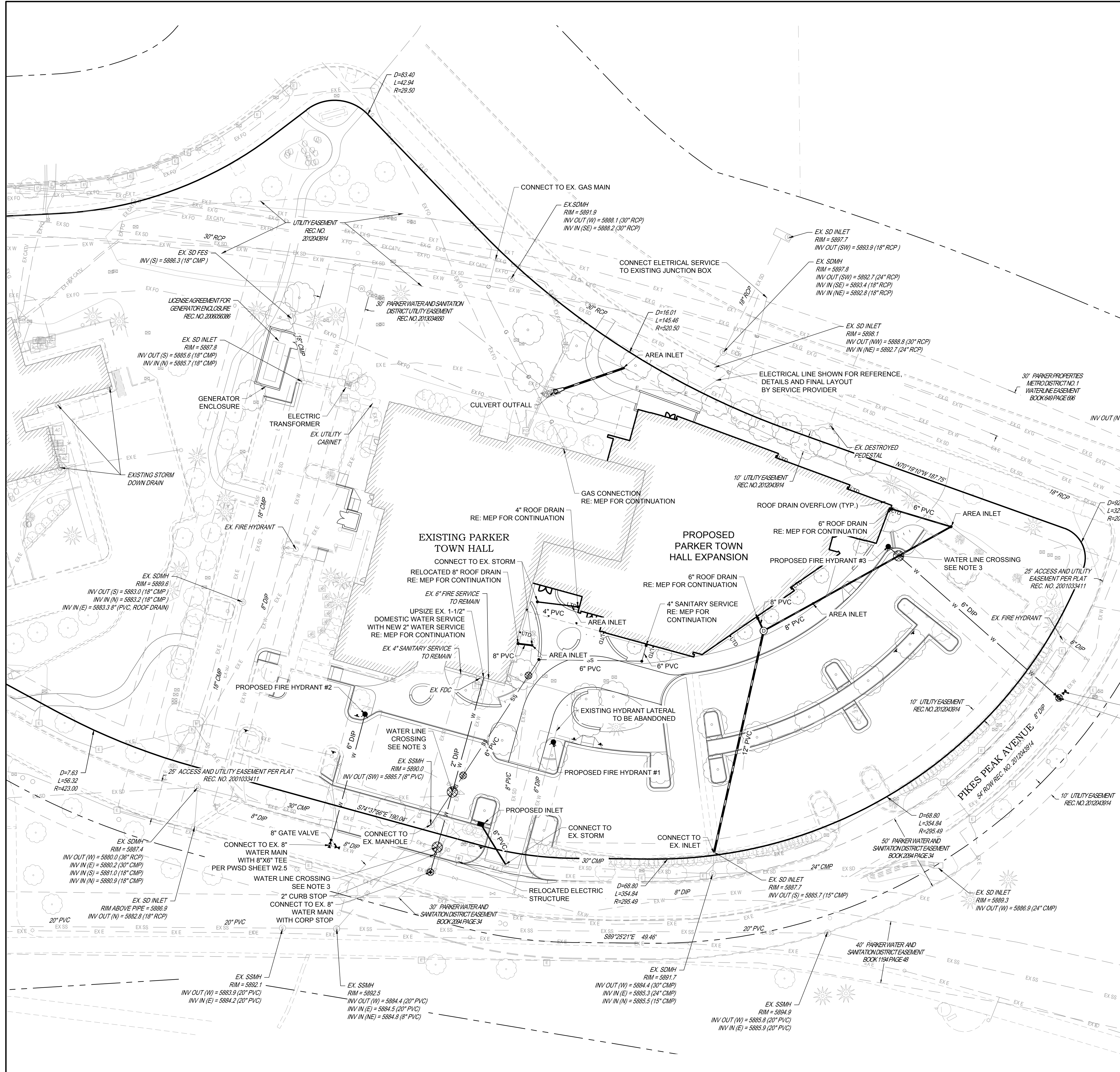
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FILE PATH:



DESIGNED BY: **RJH**  
DRAWN BY: **DEI**  
CHECKED BY: **MHV**  
MIRO JOB NO. **22139**

DRAWING NUMBER: **C-259**



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND
  - DRY UTILITIES ARE SHOWN FOR REFERENCE ONLY. FINAL DESIGN TO BE COORDINATED WITH UTILITY PROVIDER
  - CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.



DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

NO.	DESCRIPTION
1	
2	

**PARKER TOWN HALL EXPANSION**  
**20120 MAIN STREET**  
**PARKER, CO 80138**

UTILITY PLAN

PROJECT: PARKER TOWN HALL EXPANSION  
DRAWING TITLE: UTILITY PLAN

FILE PATH: C:\Users\sthd\OneDrive\A. Miros\Projects\20120 Main Street\20120 Main Street\20120 Main Street.dwg

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139  
DRAWING NUMBER: **C-311**

ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO WATER UTILITY STANDARDS. THE NUMBER AND LOCATIONS OF THE FIRE HYDRANTS AS SHOWN ON THE OVERALL UTILITY PLAN ARE CORRECT AS SPECIFIED BY THE TOWN OF PARKER, COMMUNITY DEVELOPMENT DEPARTMENT.

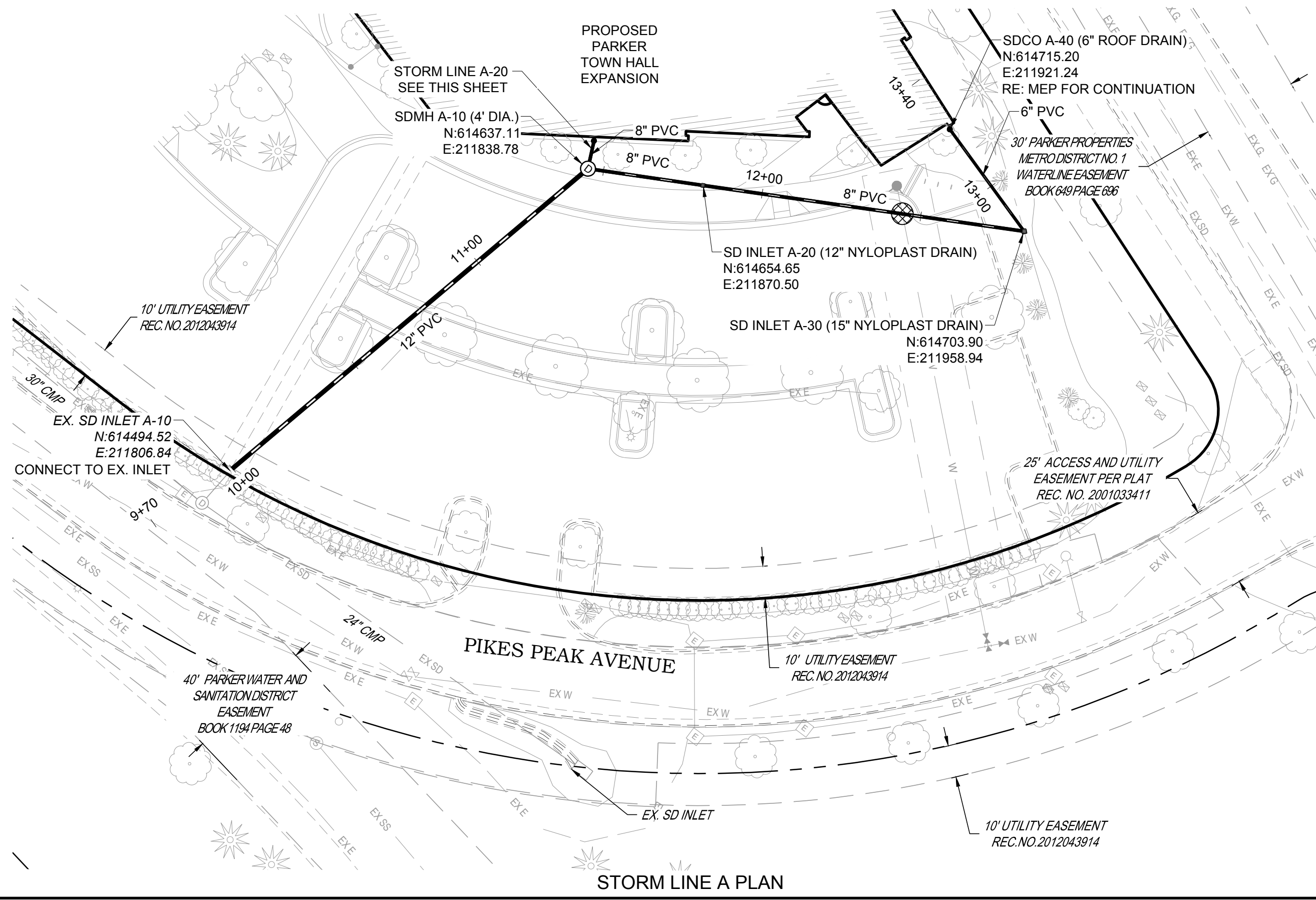
Fire Code Official or Designated Representative \_\_\_\_\_ Date \_\_\_\_\_

(NOTE - UNDERGROUND FIRE LINE (UFL) SUBMITTAL DOCUMENTS MUST MEET THE REQUIREMENTS OF NFPA 24 WHEN SUBMITTING FOR REVIEW.)

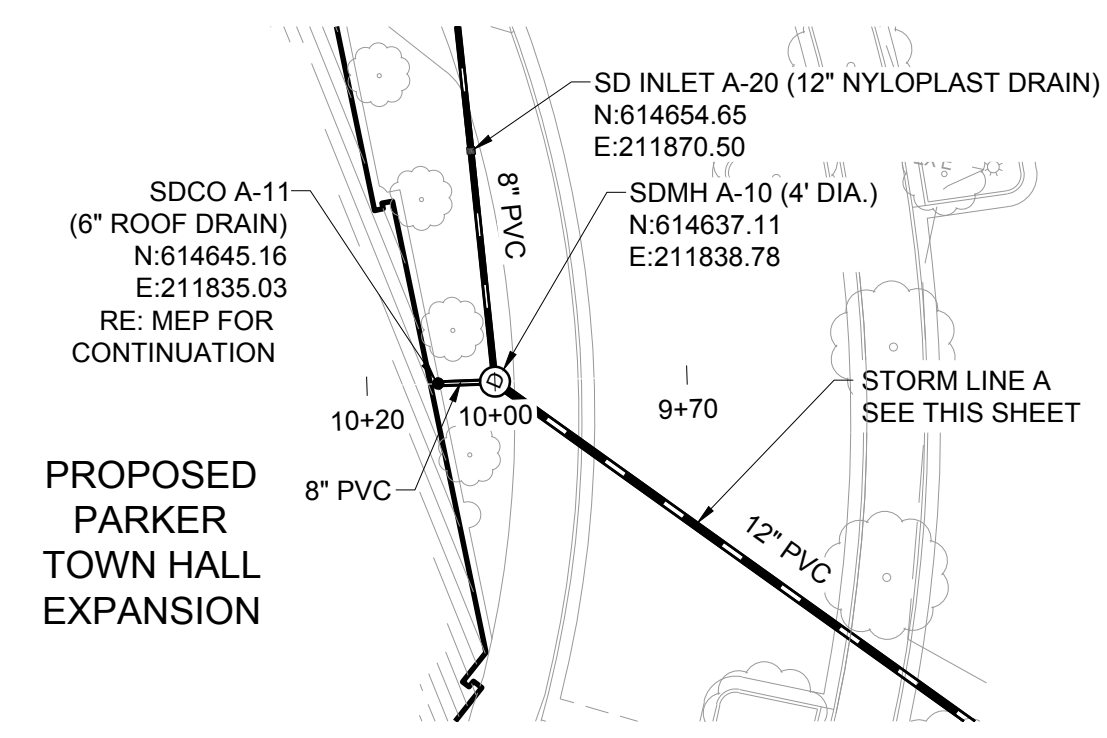
THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER

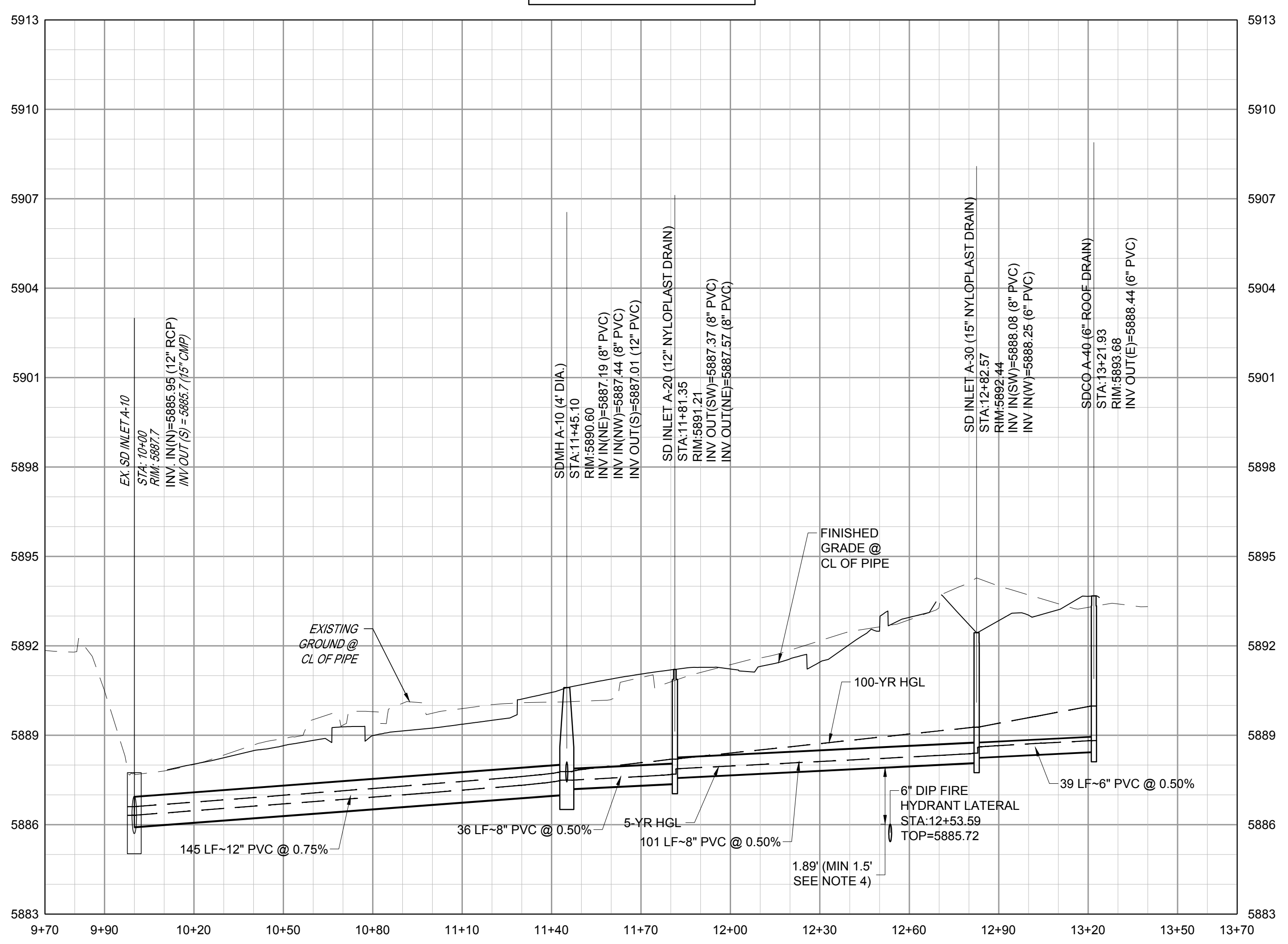
Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_



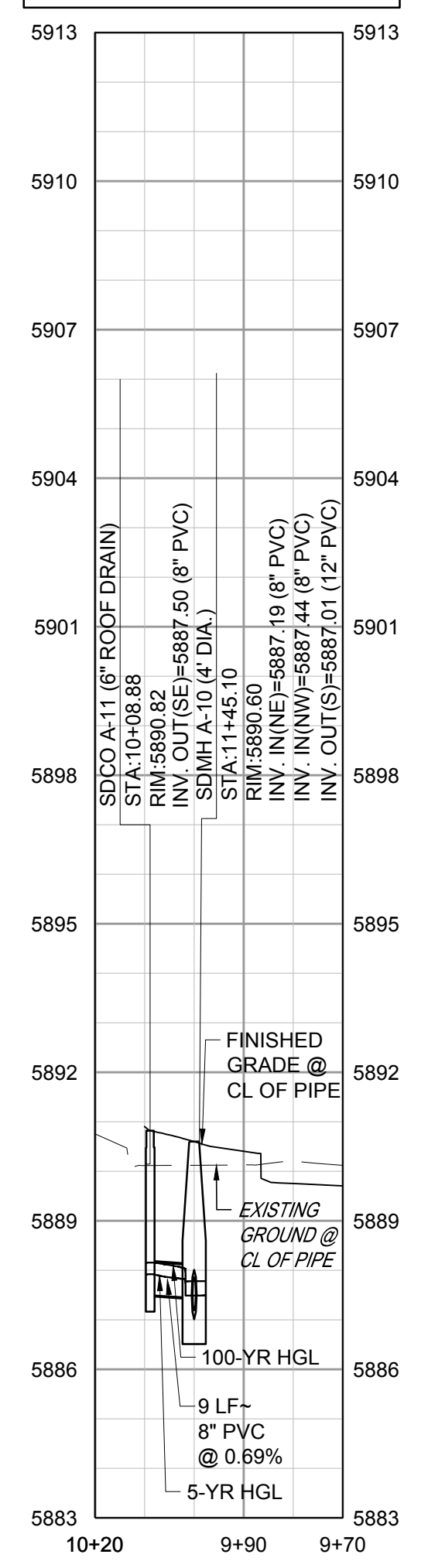
STORM LINE A PLAN



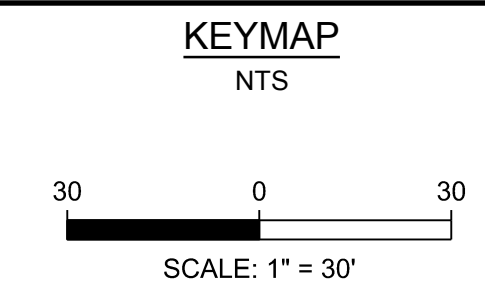
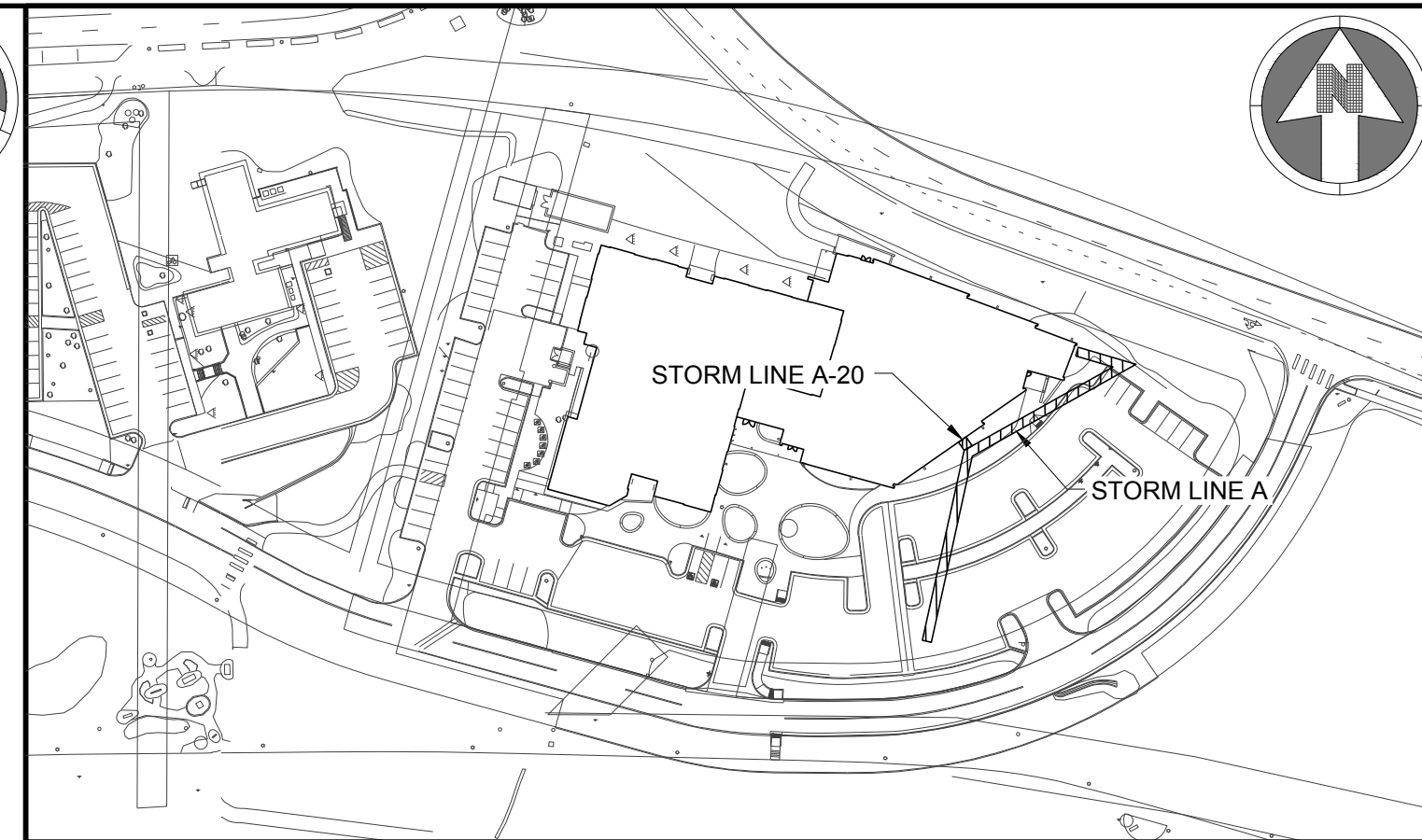
STORM LINE A-20 PLAN



STORM LINE A PROFILE



STORM LINE A-20 PROFILE



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
  - ALL EXISTING DRY UTILITIES ARE ASSUMED TO BE 3' DEEP TO TOP OF PIPE.
  - ALL EXISTING WATER LINES ARE ASSUMED TO BE 4.5' DEEP TO TOP OF PIPE, UNLESS OTHERWISE NOTED.
  - CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
  - THE CONTRACTOR SHALL POTHOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY.

- LEGEND:**
- UTILITY CROSSING

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**PROJECT:** PARKER TOWN HALL EXPANSION  
 20120 MAIN STREET  
 PARKER, CO 80138

**DRAWING TITLE:** STORM PLAN AND PROFILES

FILE PATH: C:\Users\rdh\OneDrive\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C331-STRM-PROF.dwg C331 - 2/5/2024



DESIGNED BY: RJH  
 DRAWN BY: DEI  
 CHECKED BY: MHV  
 MIRO JOB NO. 22139  
 DRAWING NUMBER:

**C-331**

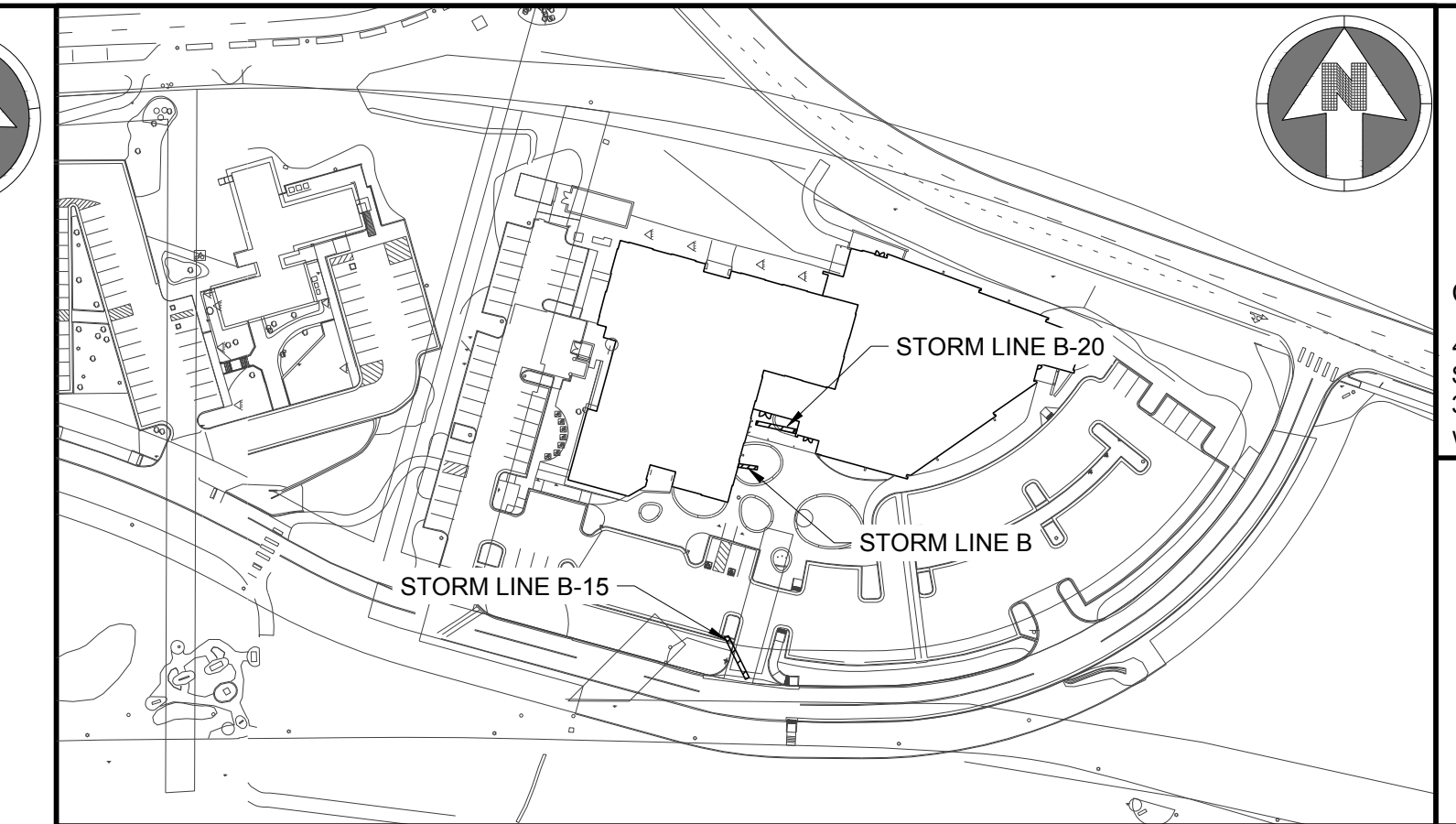


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NTS

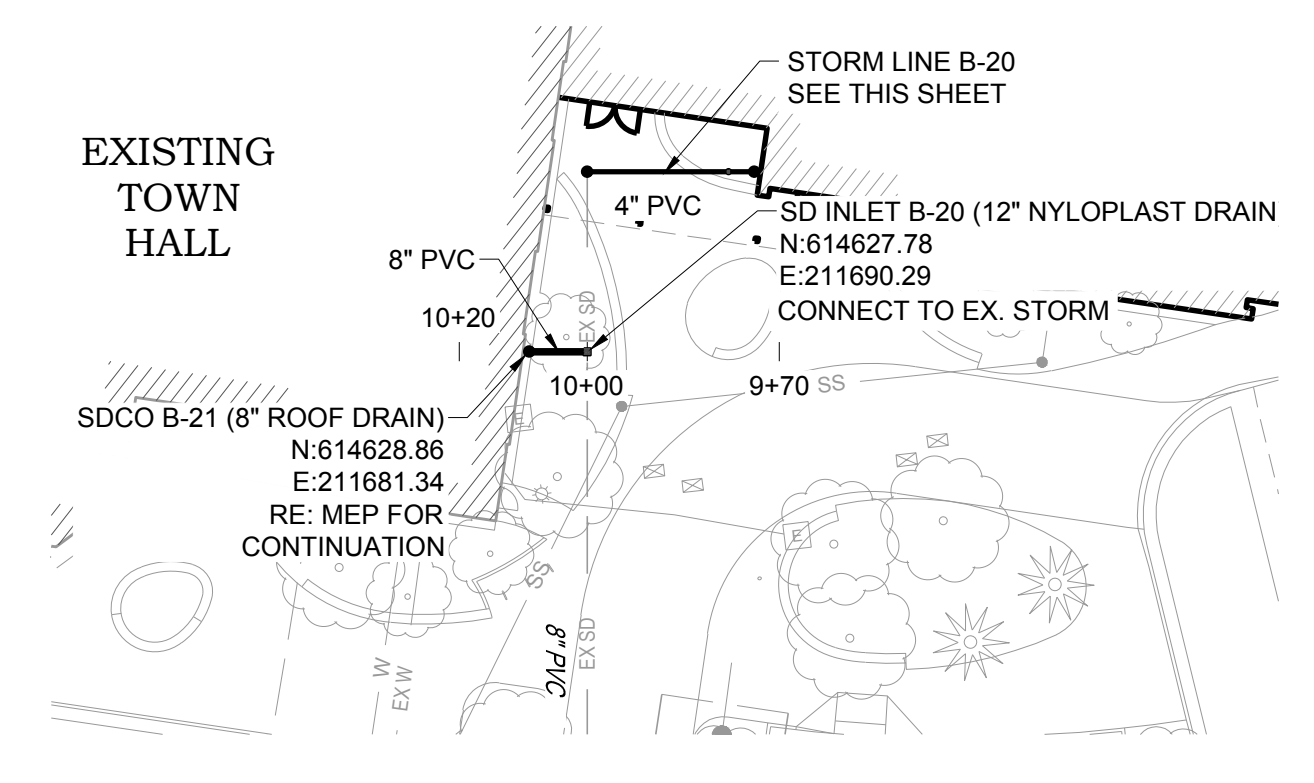


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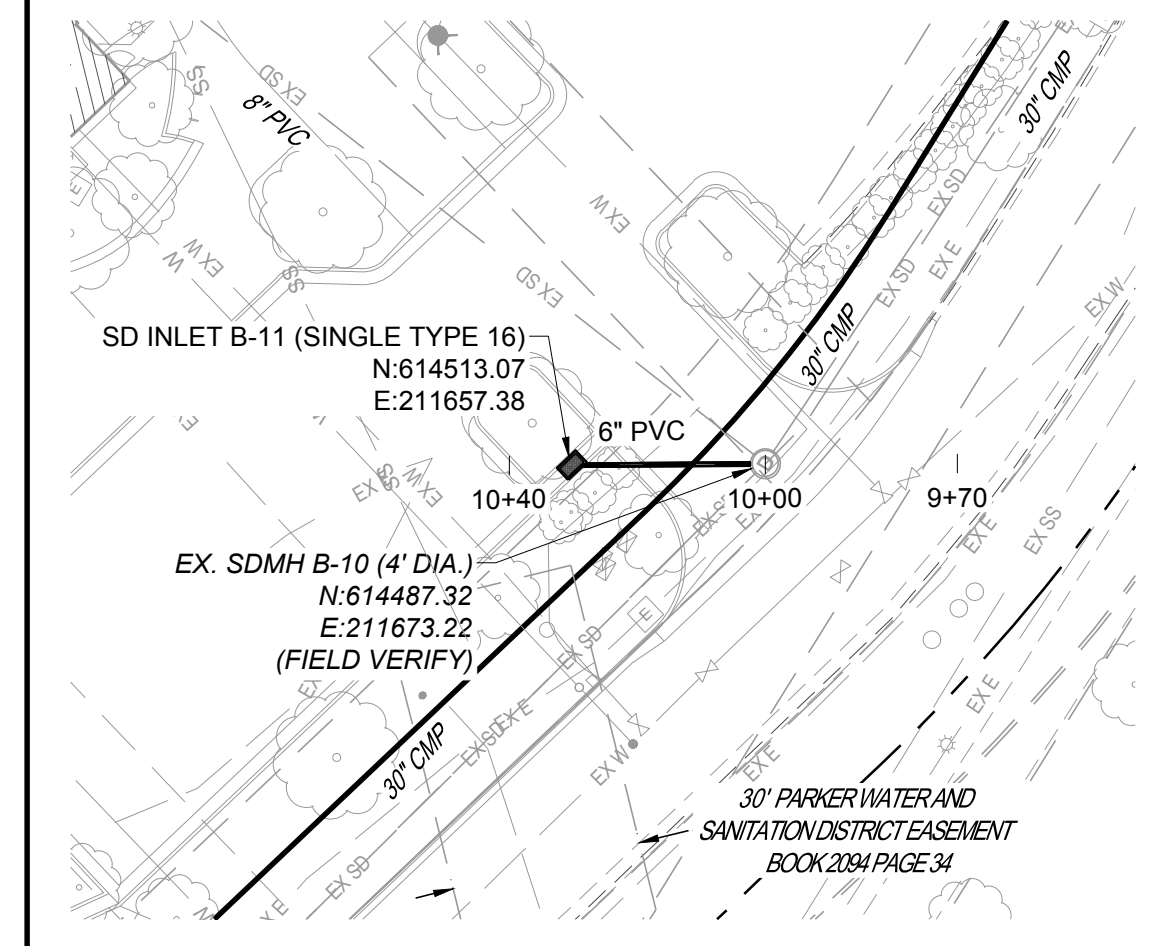
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
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LEGEND:

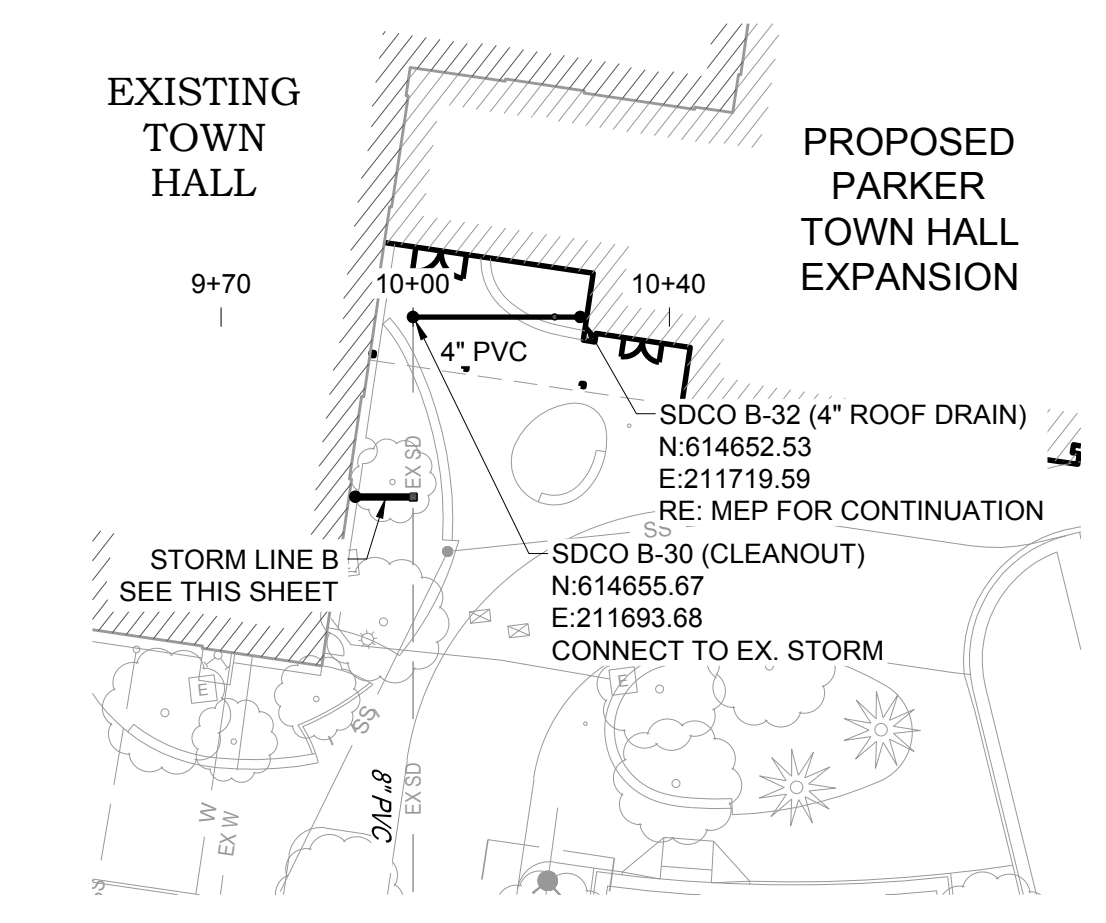
UTILITY CROSSING



STORM LINE B PLAN

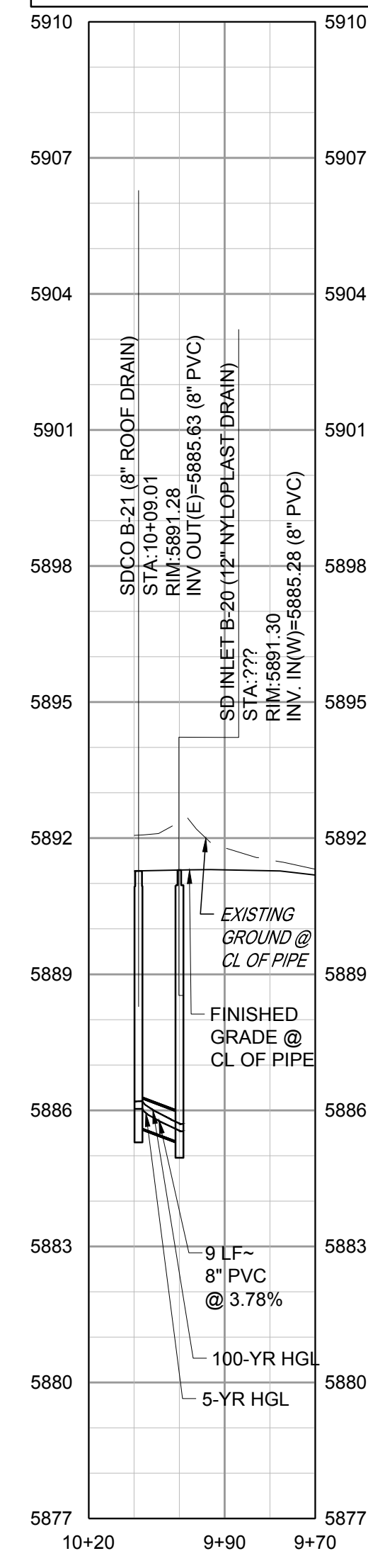


STORM LINE B-15 PLAN



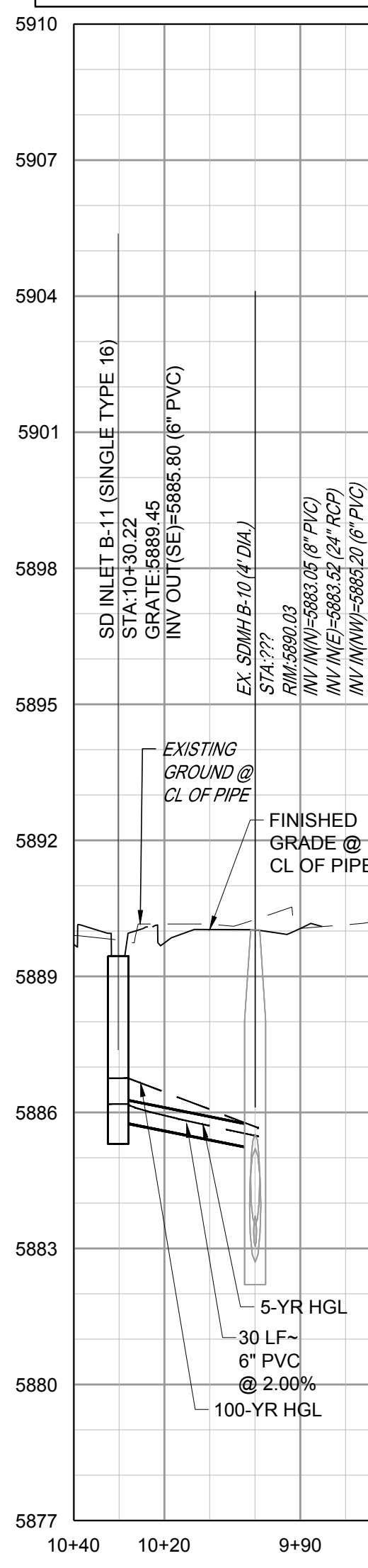
STORM LINE B-20 PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



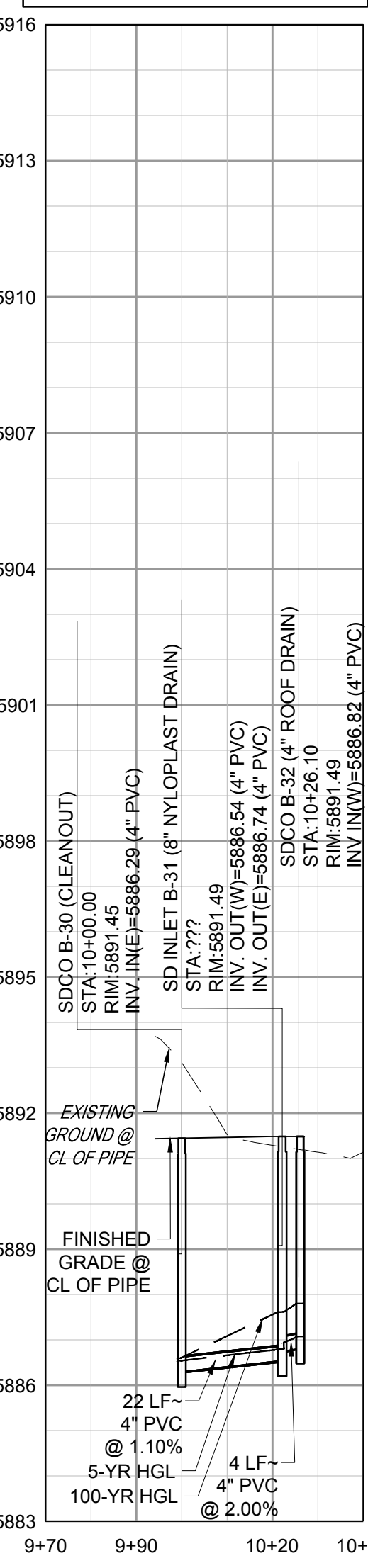
STORM LINE B PROFILE

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE B-15 PROFILE

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE B-20 PROFILE

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Town of Parker, Director of Engineering/Public Works Date

NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **STORM PLAN AND PROFILES**

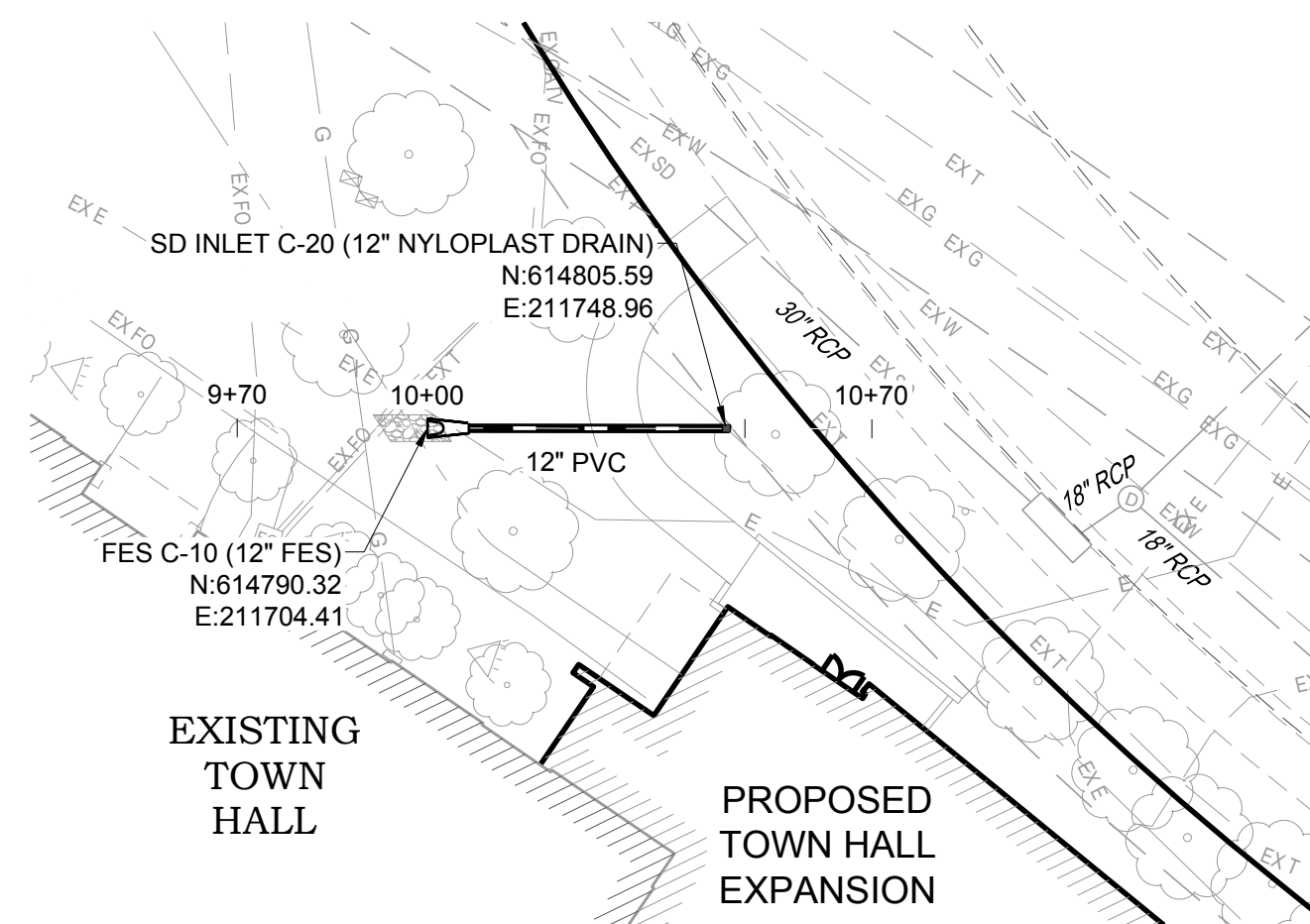
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DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

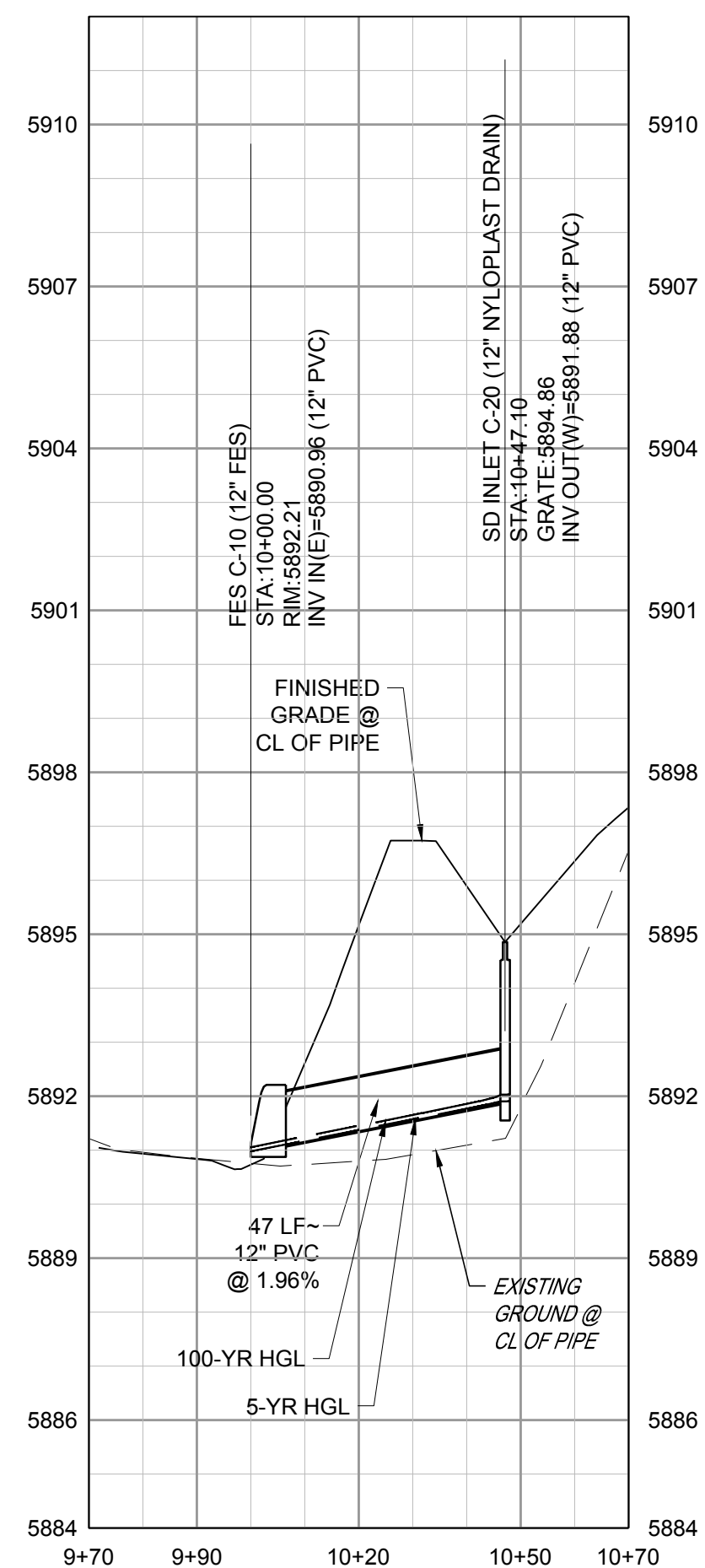
DRAWING NUMBER:

**C-332**

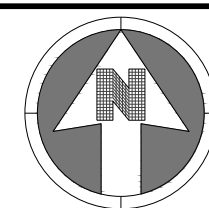
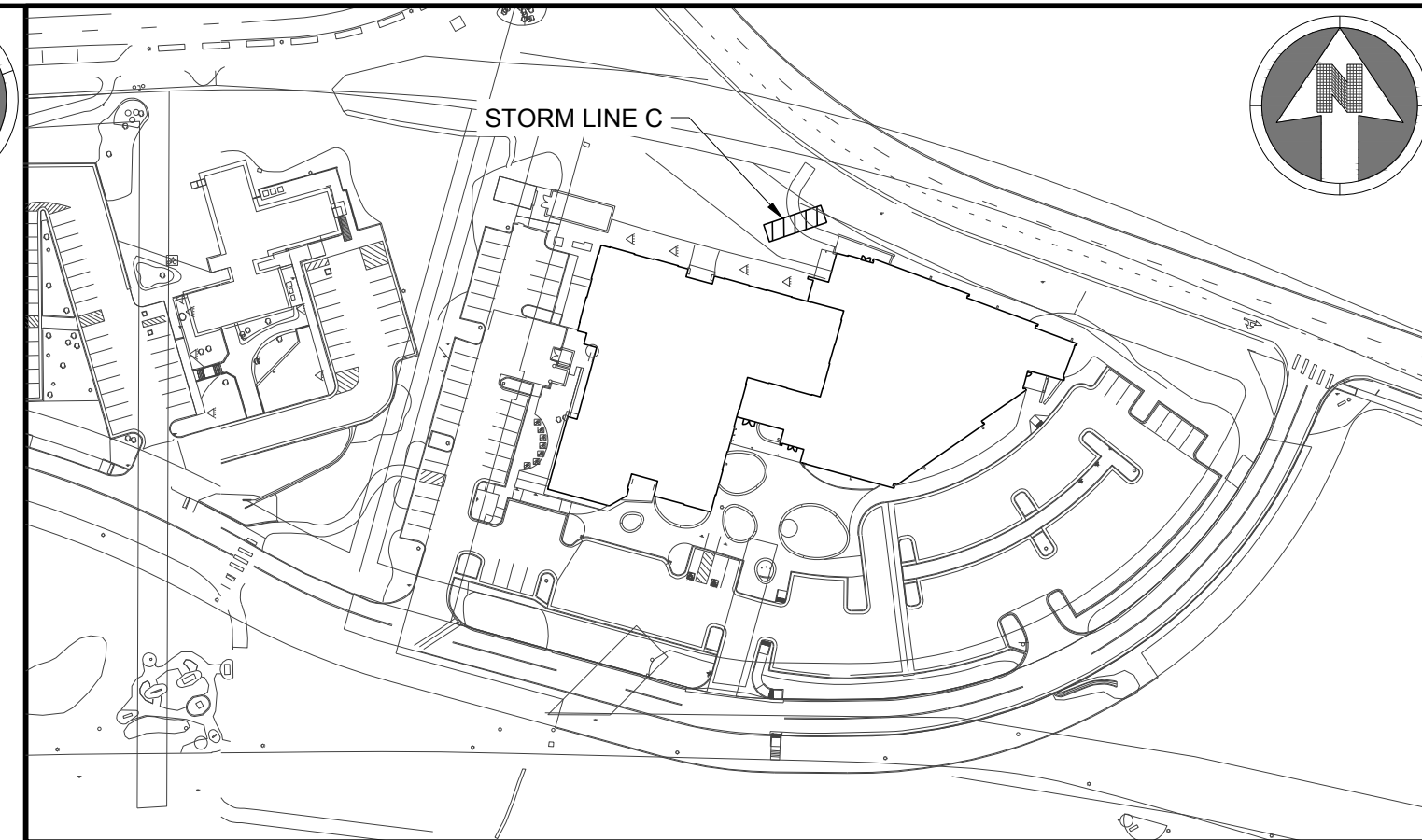
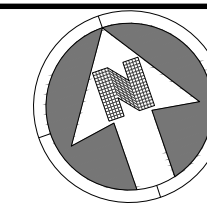


STORM LINE C PLAN

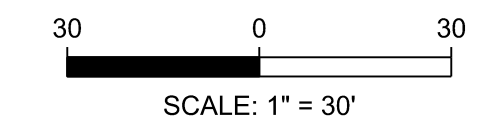
VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE C PROFILE



KEYMAP  
NTS



NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- ALL EXISTING DRY UTILITIES ARE ASSUMED TO BE 3' DEEP TO TOP OF PIPE.
- ALL EXISTING WATER LINES ARE ASSUMED TO BE 4.5' DEEP TO TOP OF PIPE, UNLESS OTHERWISE NOTED.
- CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
- THE CONTRACTOR SHALL POTHOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY.

LEGEND:

UTILITY CROSSING

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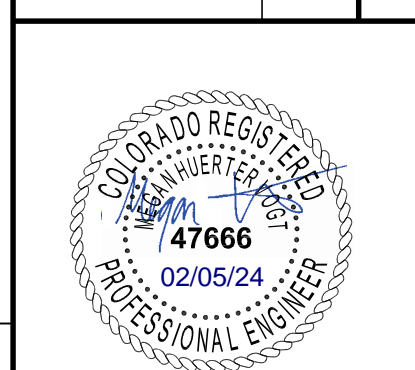
**811**  
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NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **STORM PLAN AND PROFILES**

FILE PATH: C:\Users\rhDCACC\Documents\A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C331-STRM-PROF.dwg C333 - 2/5/2024

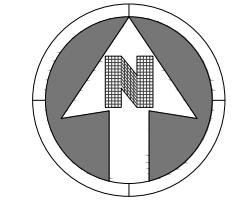
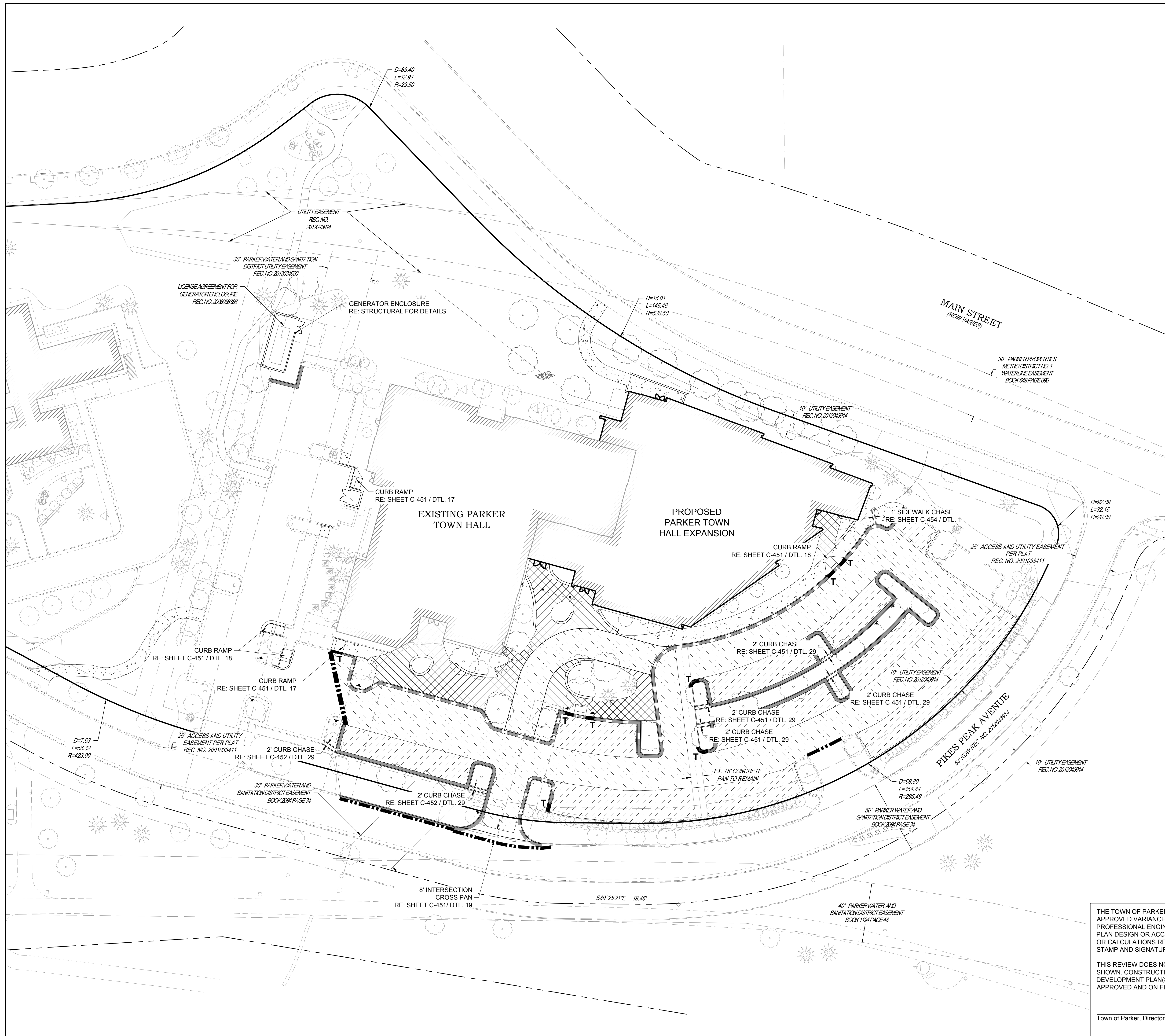


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DESIGNED BY: R/JH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-333**



SCALE: 1" = 30'

- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
  - A SITE INSPECTION IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION. NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THAT CURB, GUTTER, AND FIRST LIFT OF ASPHALT ARE INSTALLED.

- LEGEND:**
- PROPOSED 6" CATCH CURB AND GUTTER  
RE: SHEET C-451 / DTL. 3
  - PROPOSED 6" SPILL CURB AND GUTTER  
RE: SHEET C-451 / DTL. 3
  - PROPOSED FLUSH CURB AND GUTTER  
RE: SHEET C-453 / DTL. 2
  - PROPOSED PATCHBACK  
RE: SHEET C-453 / DTL. 3
  - PROPOSED CURB TRANSITION  
RE: SHEET C-454 / DTL. 3 & 4
  - PROPOSED 5" COMPOSITE ASPHALT OVER 6" BASE COURSE  
RE: SHEET C-453 / DTL. 1-A
  - PROPOSED FIRE TRUCK RATED PAVEMENT  
DESIGN TBD  
RE: SHEET C-453 / DTL. 1-A
  - PROPOSED 6" CONCRETE PAVEMENT  
RE: SHEET C-451 / DTL. 4
  - REFER TO LANDSCAPE PLANS

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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

**PROJECT:** PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

**DRAWING TITLE:** PAVING PLAN

FILE PATH: C:\Users\jph\ACCDocs\S.A. Miro, Inc\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C411-PAVE-PLAN.dwg C411 - 2/5/2024



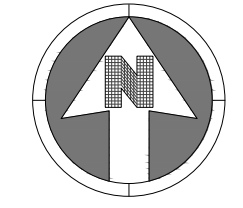
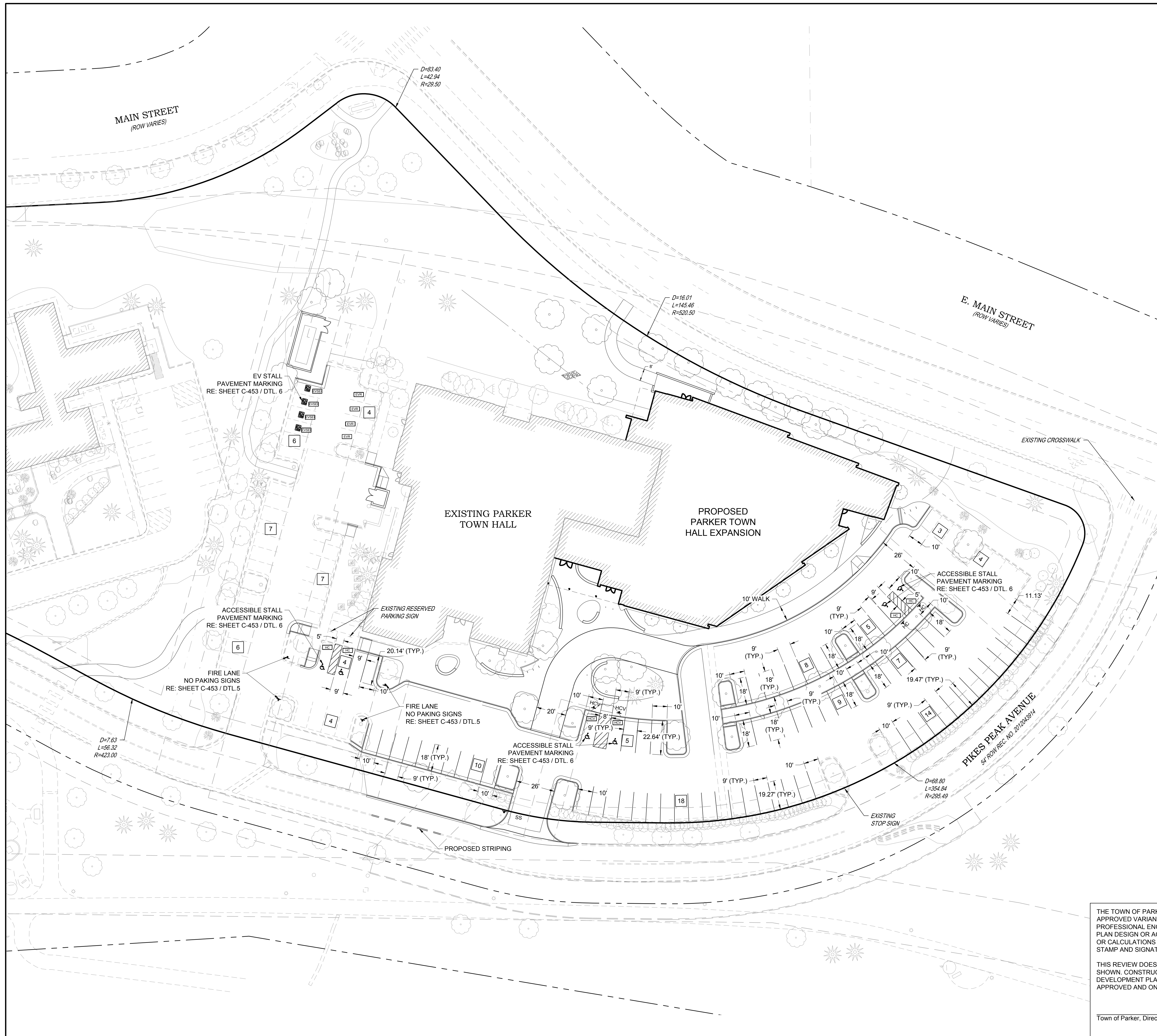
DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-411**

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works Date



SCALE: 1" = 30'

- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
  - PARKING STALLS ADJACENT TO LANDSCAPE ISLANDS SHALL BE A MINIMUM OF 10' IN WIDTH.
  - PARKING STALLS WITHOUT AN ASSOCIATED PARKING CLASSIFICATION ARE STANDARD PARKING STALLS.

- LEGEND:**
- ▲-HC PROPOSED "HANDICAP PARKING" SIGN  
RE: SHEET C-453 / DTL. 5
  - ▲-HCV PROPOSED "VAN ACCESSIBLE HANDICAP PARKING" SIGN  
RE: SHEET C-453 / DTL. 5
  - ▲-SS PROPOSED "STOP" SIGN  
RE: SHEET C-453 / DTL. 5
  - 9 PARKING STALL COUNT  
9'X18' MINIMUM DIMENSIONS  
UNLESS OTHERWISE NOTED
  - C COMPACT  
PARKING CLASSIFICATION
  - HC STANDARD ACCESSIBLE  
PARKING CLASSIFICATION
  - HCV ACCESSIBLE VAN  
PARKING CLASSIFICATION
  - HCEV ACCESSIBLE ELECTRIC VEHICLE  
PARKING CLASSIFICATION
  - UNV UNIVERSAL VEHICLE  
PARKING CLASSIFICATION
  - EVSE ELECTRIC VEHICLE SUPPLY EQUIPMENT INSTALLED  
PARKING CLASSIFICATION
  - EVR ELECTRIC VEHICLE READY  
PARKING CLASSIFICATION
  - EVC ELECTRIC VEHICLE CAPABLE  
PARKING CLASSIFICATION

PARKING COUNT TABLE			
PARKING TYPE	# OF PROPOSED SPACES	# OF EXISTING SPACES TO REMAIN	TOTAL
STANDARD	74	33	107
COMPACT	0	0	0
STANDARD ACCESSIBLE	4	0	4
ACCESSIBLE VAN	2	0	2
ACCESSIBLE EV	0	0	0
UNIVERSAL	0	0	0
EVSE INSTALLED	4	0	4
EV READY	4	0	4
EV CAPABLE	0	0	0
<b>TOTAL</b>	<b>88</b>	<b>33</b>	<b>121</b>



DATE	DESCRIPTION	NO.
11/03/2023 <td>FIRST TOWN SUBMITTAL <td>1</td> </td>	FIRST TOWN SUBMITTAL <td>1</td>	1
02/05/2024 <td>SECOND TOWN SUBMITTAL <td>2</td> </td>	SECOND TOWN SUBMITTAL <td>2</td>	2

**PROJECT:** PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

**DRAWING TITLE:** SIGNAGE AND STRIPING PLAN

FILE PATH: C:\Users\sthd\OneDrive\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C421-STRIP-PLAN.dwg C421 - 2/5/2024

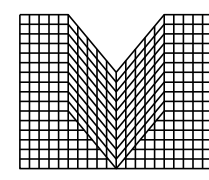


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-421**

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DATE	11/03/2023
FIRST TOWN SUBMITTAL	02/05/2024
SECOND TOWN SUBMITTAL	
DESCRIPTION	
NO.	2

NO.	1
NO.	2

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138**

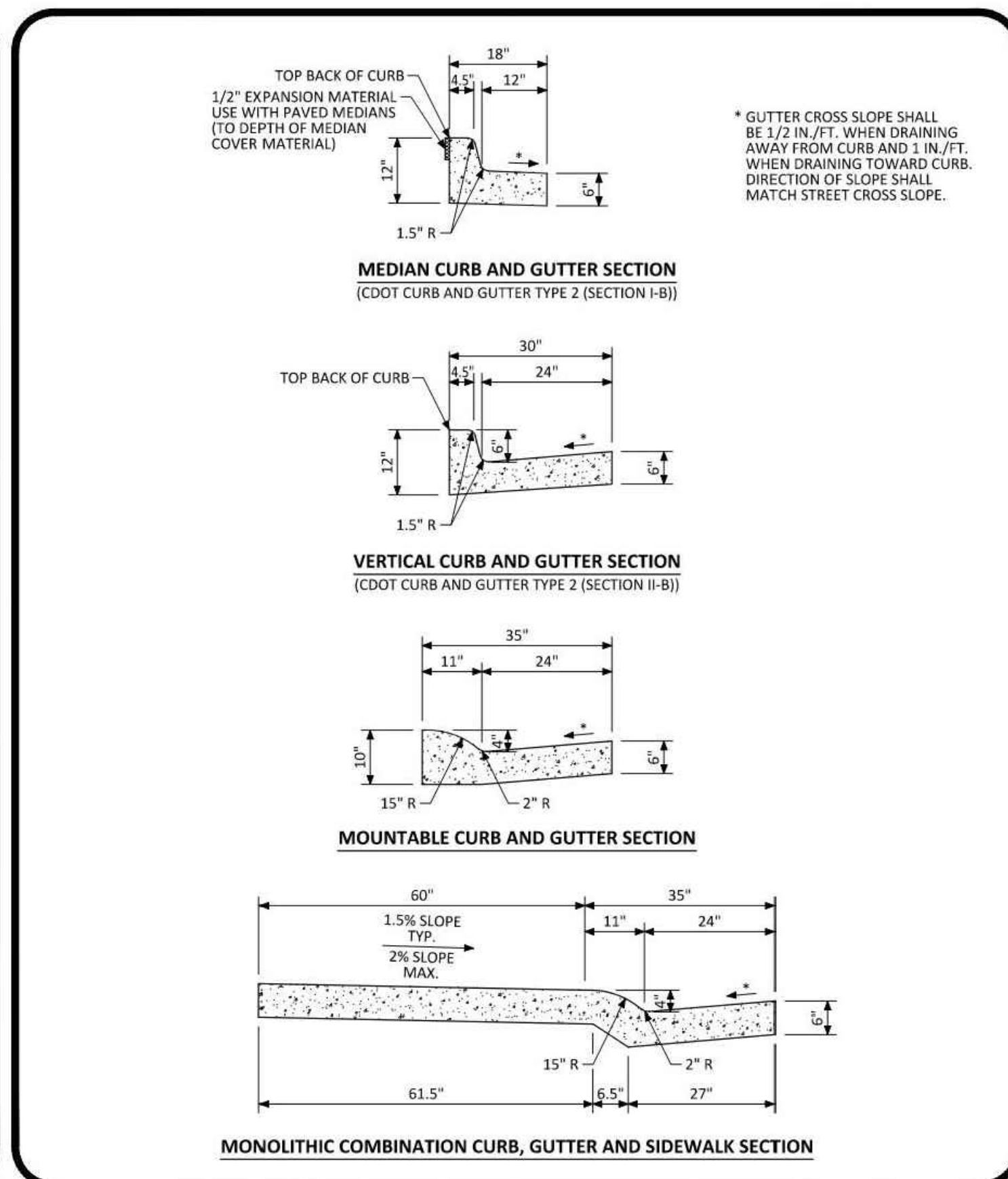
DRAWING TITLE: **SITE DETAILS**

FILE PATH: C:\Users\sthd\CAAC\Docs\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C451-SITE-DTLS.dwg C451 - 2/5/2024

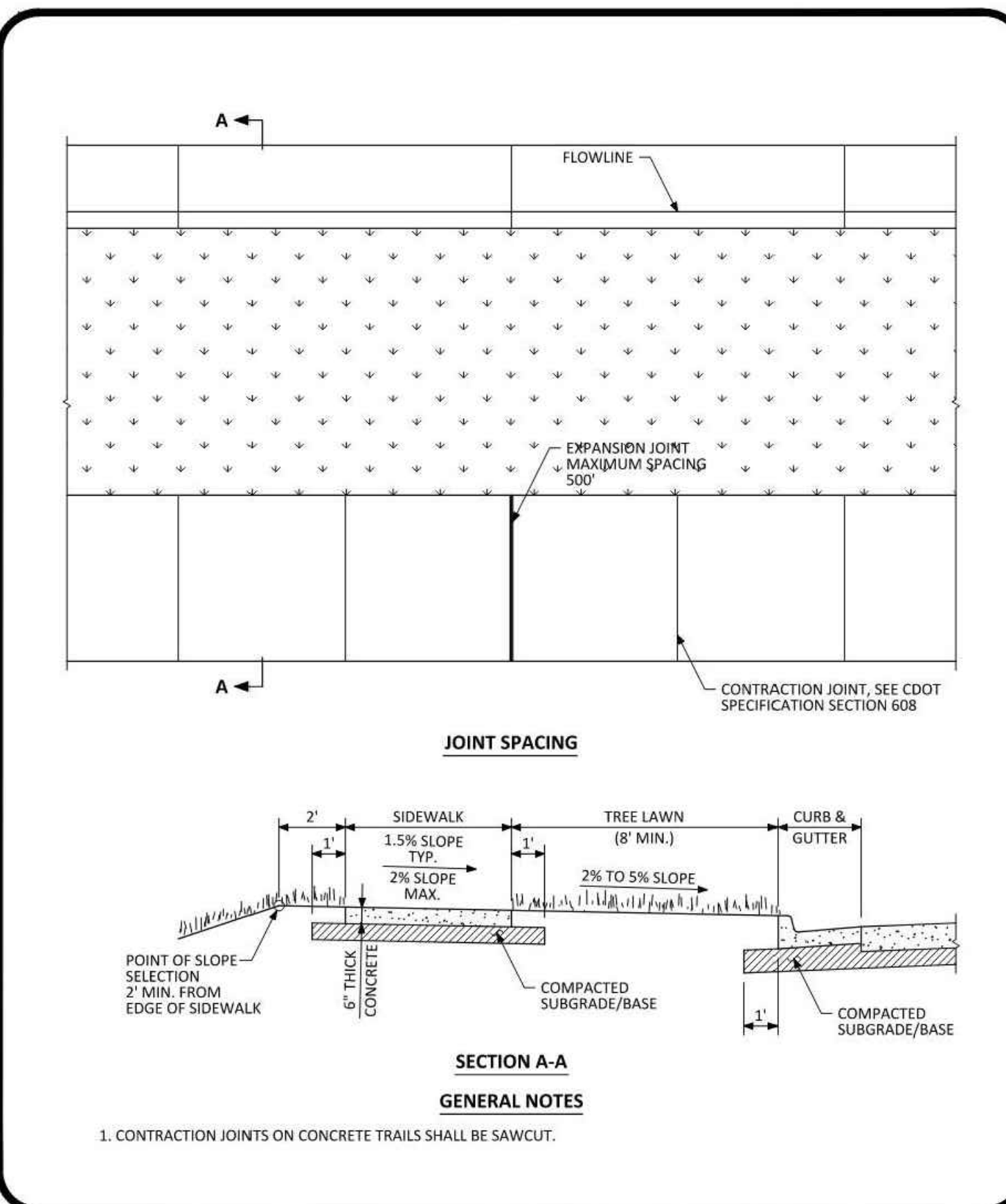


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CHECKED BY: MHV  
MIRO JOB NO. 22139

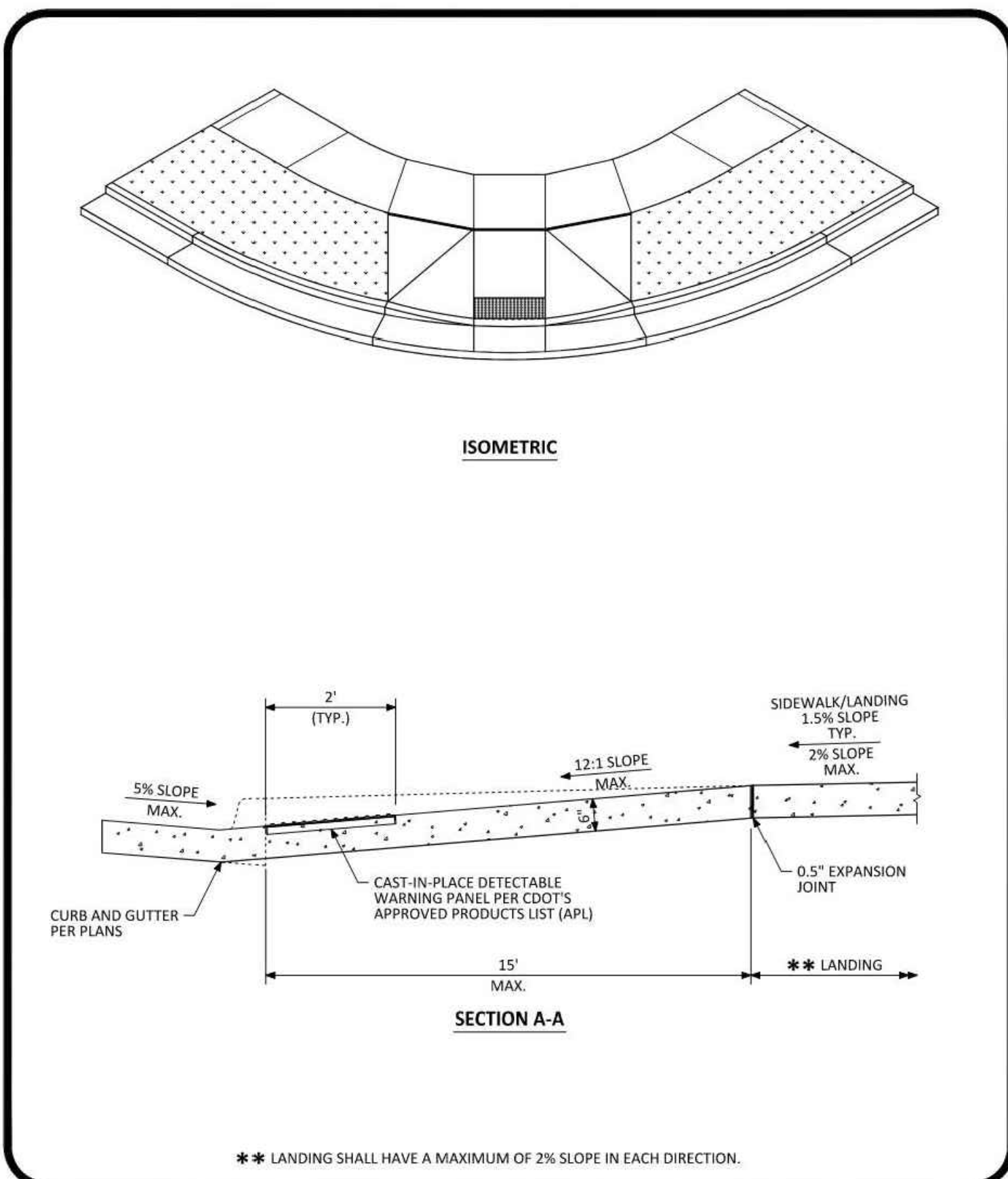
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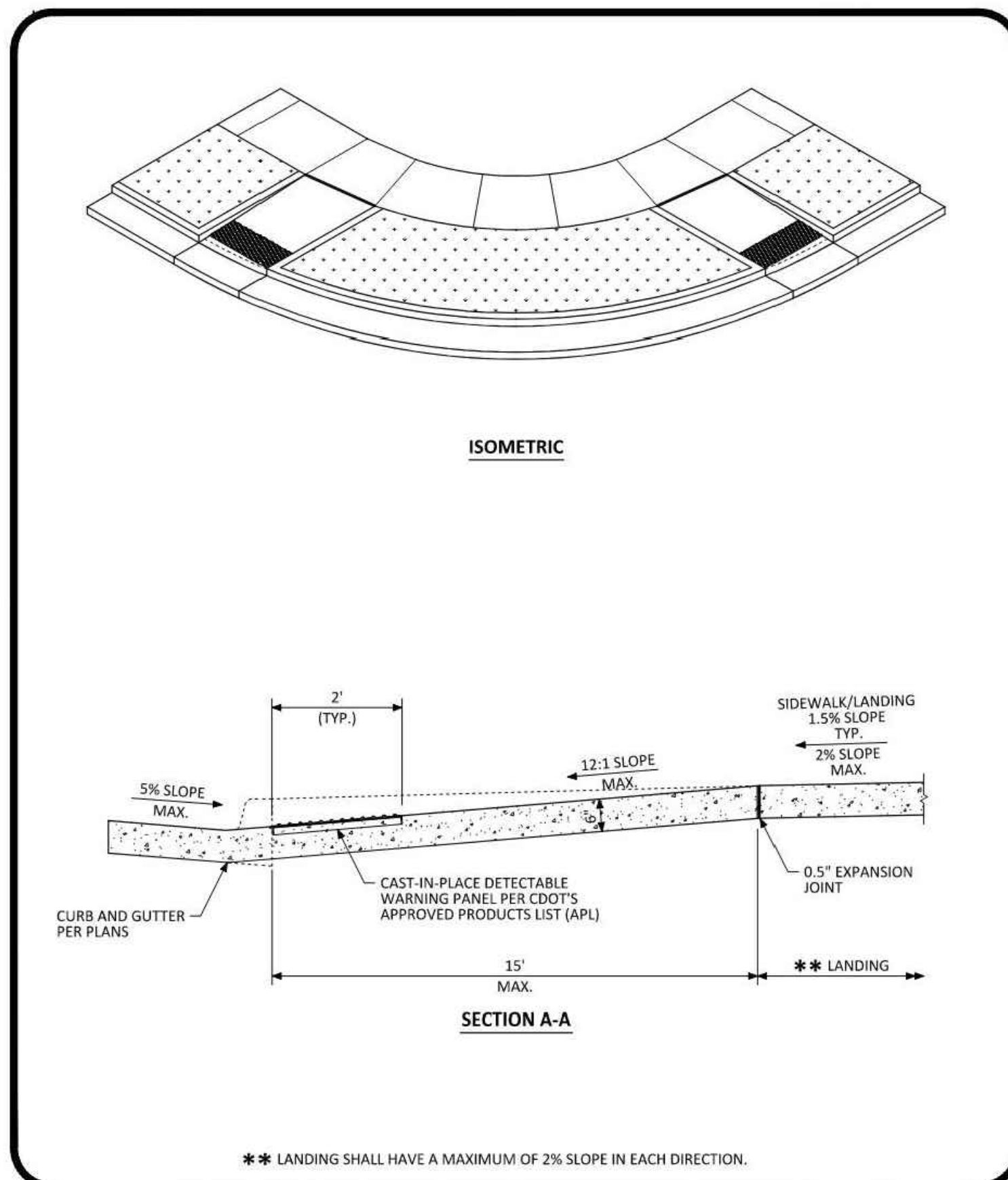
**PARKER COLORADO** | **CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 3 | 1 OF 1



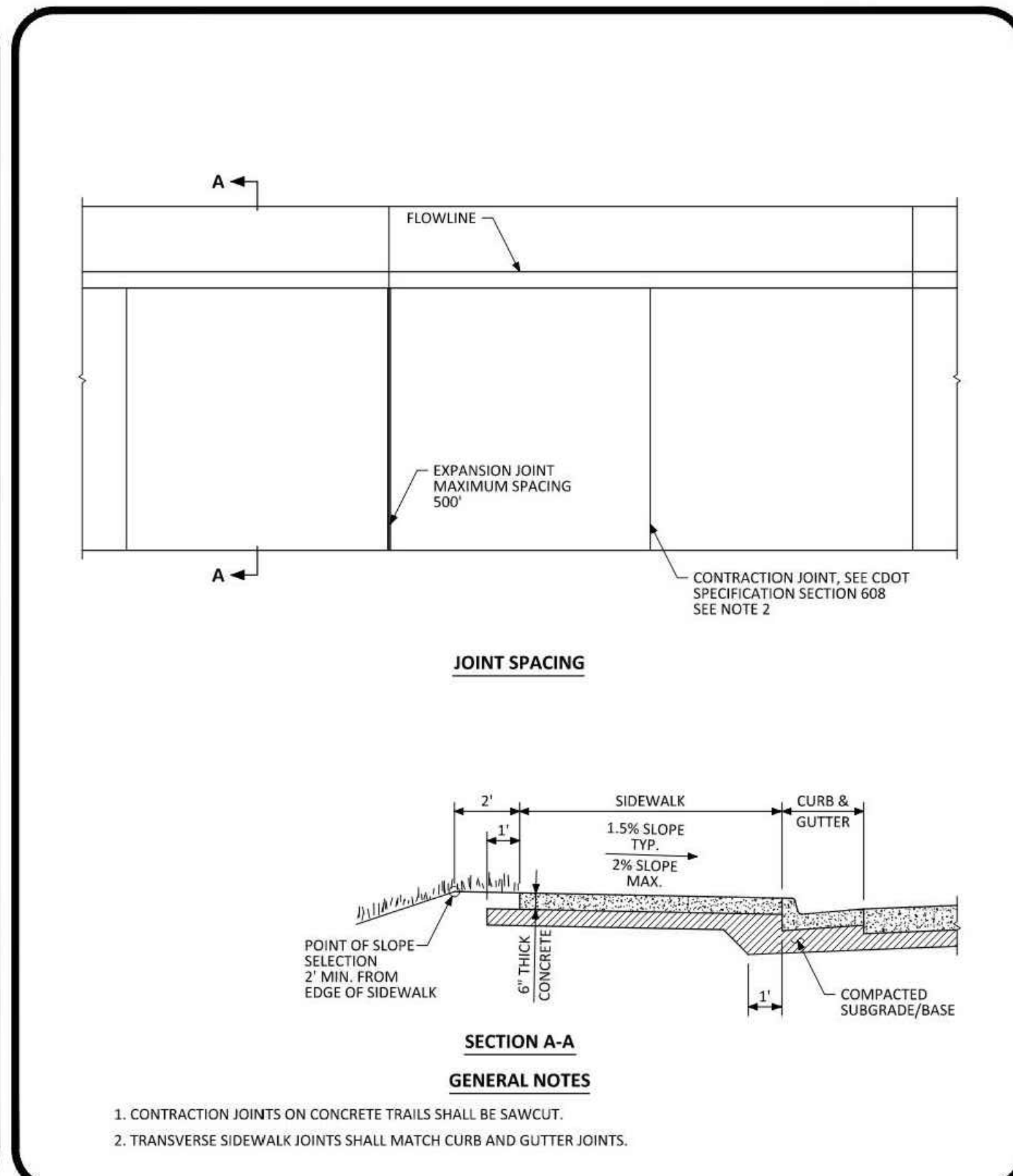
**PARKER COLORADO** | **DETACHED SIDEWALK STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 4 | 1 OF 2



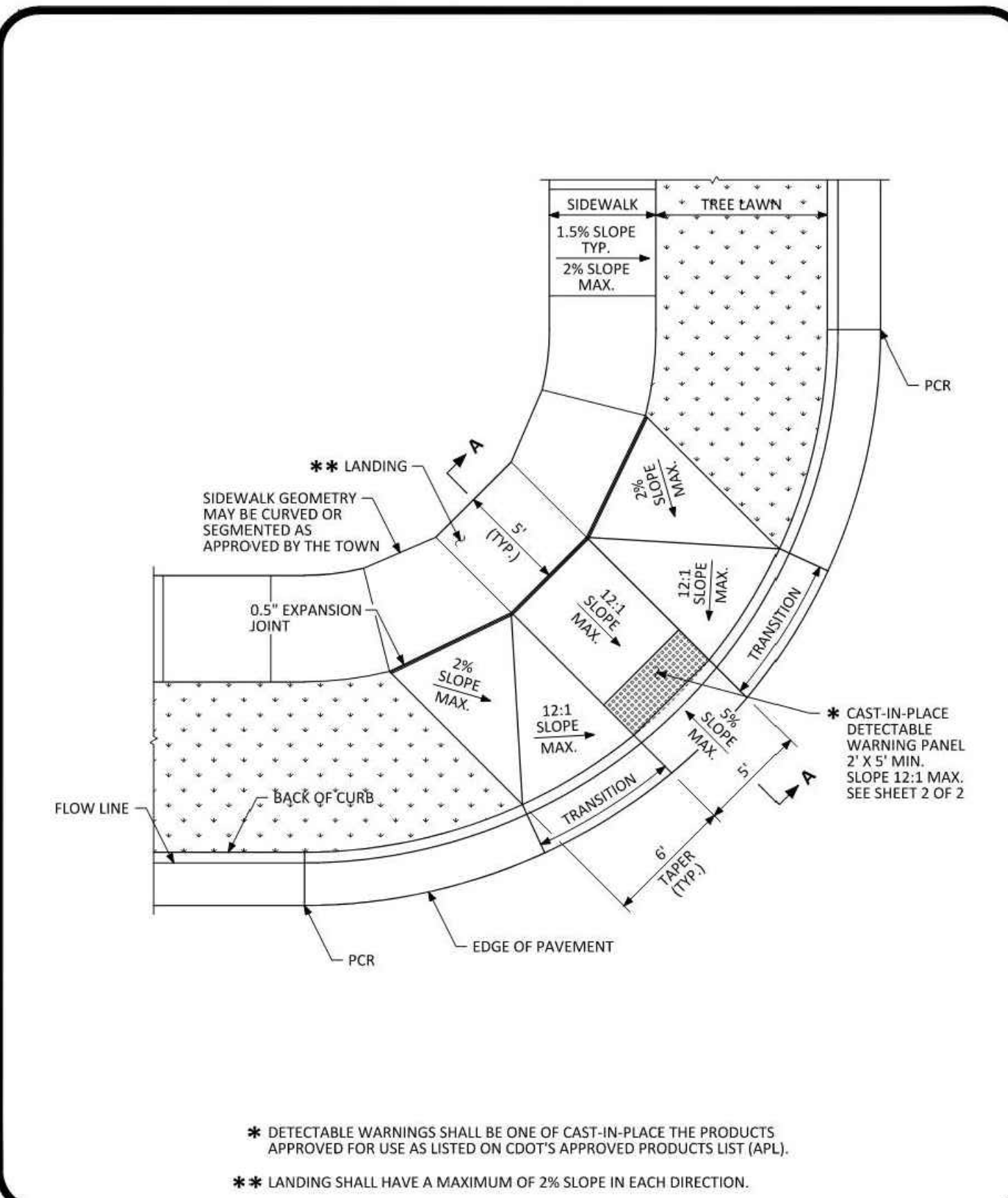
**PARKER COLORADO** | **DIAGONAL CURB RAMP LAYOUT STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 15 | 2 OF 4



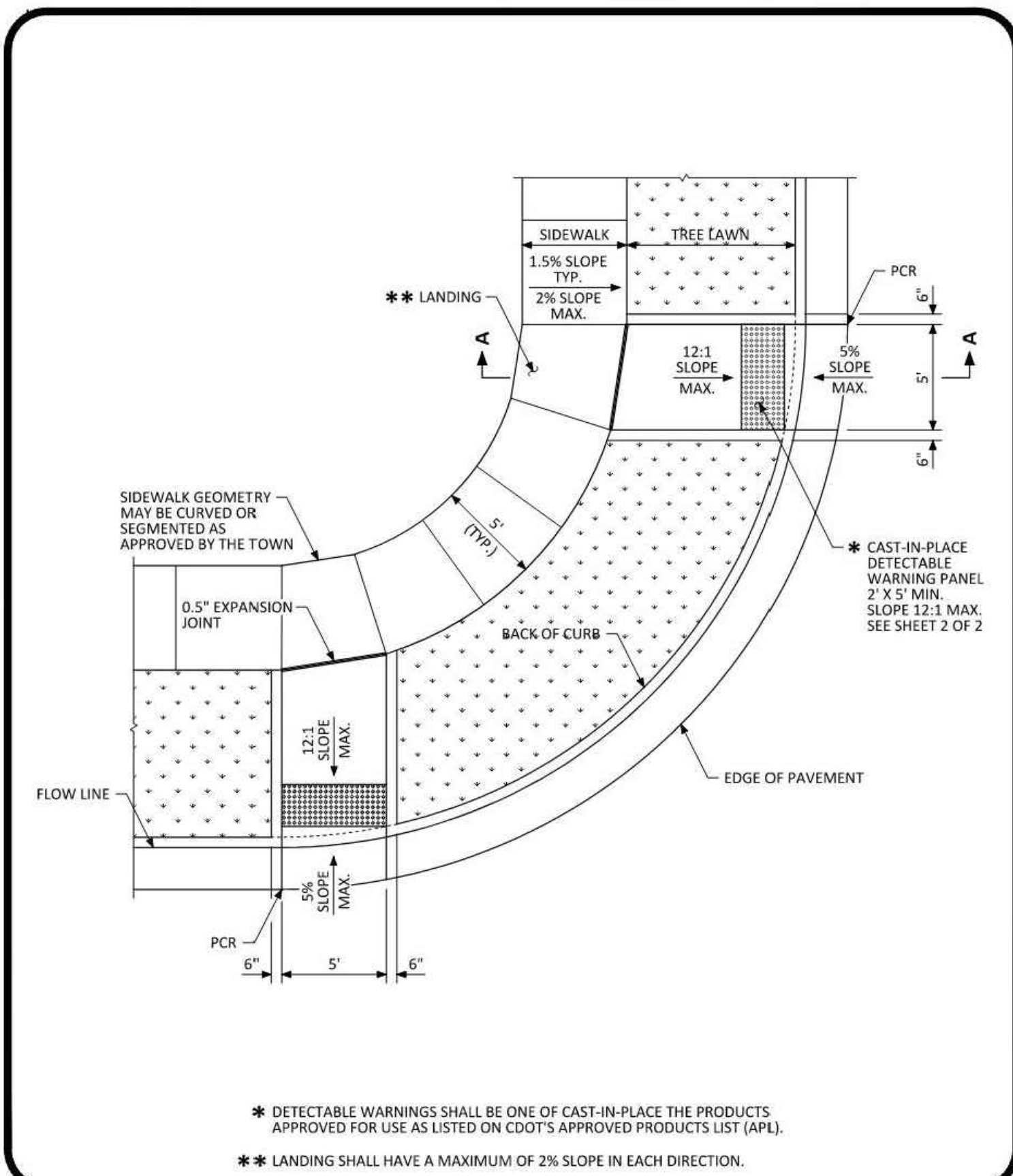
**PARKER COLORADO** | **DIRECTIONAL CURB RAMP LAYOUT STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 15 | 4 OF 4



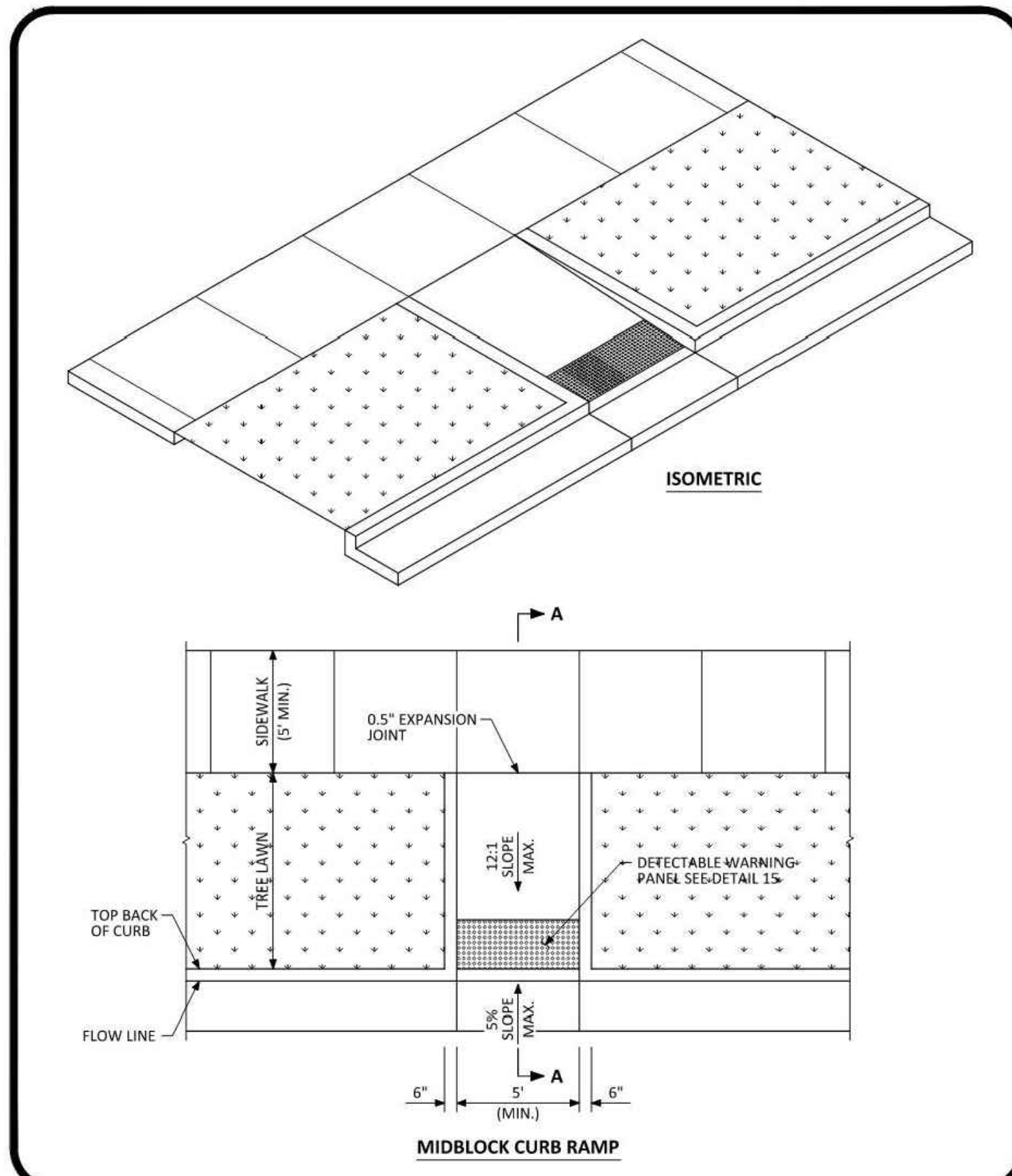
**PARKER COLORADO** | **ATTACHED SIDEWALK STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 4 | 2 OF 2



**PARKER COLORADO** | **DIAGONAL CURB RAMP LAYOUT STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 15 | 1 OF 4



**PARKER COLORADO** | **DIRECTIONAL CURB RAMP LAYOUT STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 15 | 3 OF 4



**PARKER COLORADO** | **MIDBLOCK CURB RAMP LAYOUT STANDARD DETAIL** | DATE NOVEMBER 2020 | DETAIL 17 | 1 OF 2



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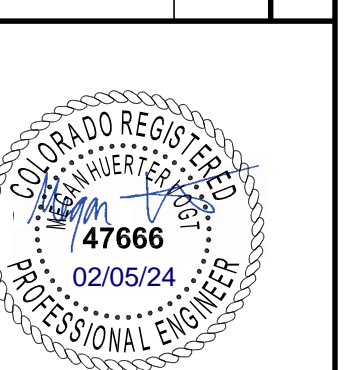
NO.	DATE	DESCRIPTION
1	11/03/2023	FIRST TOWN SUBMITTAL
2	02/05/2024	SECOND TOWN SUBMITTAL

NO.	DATE	DESCRIPTION
1	NOVEMBER 2020	29
2	NOVEMBER 2020	25
3	NOVEMBER 2020	19
4	NOVEMBER 2020	18

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

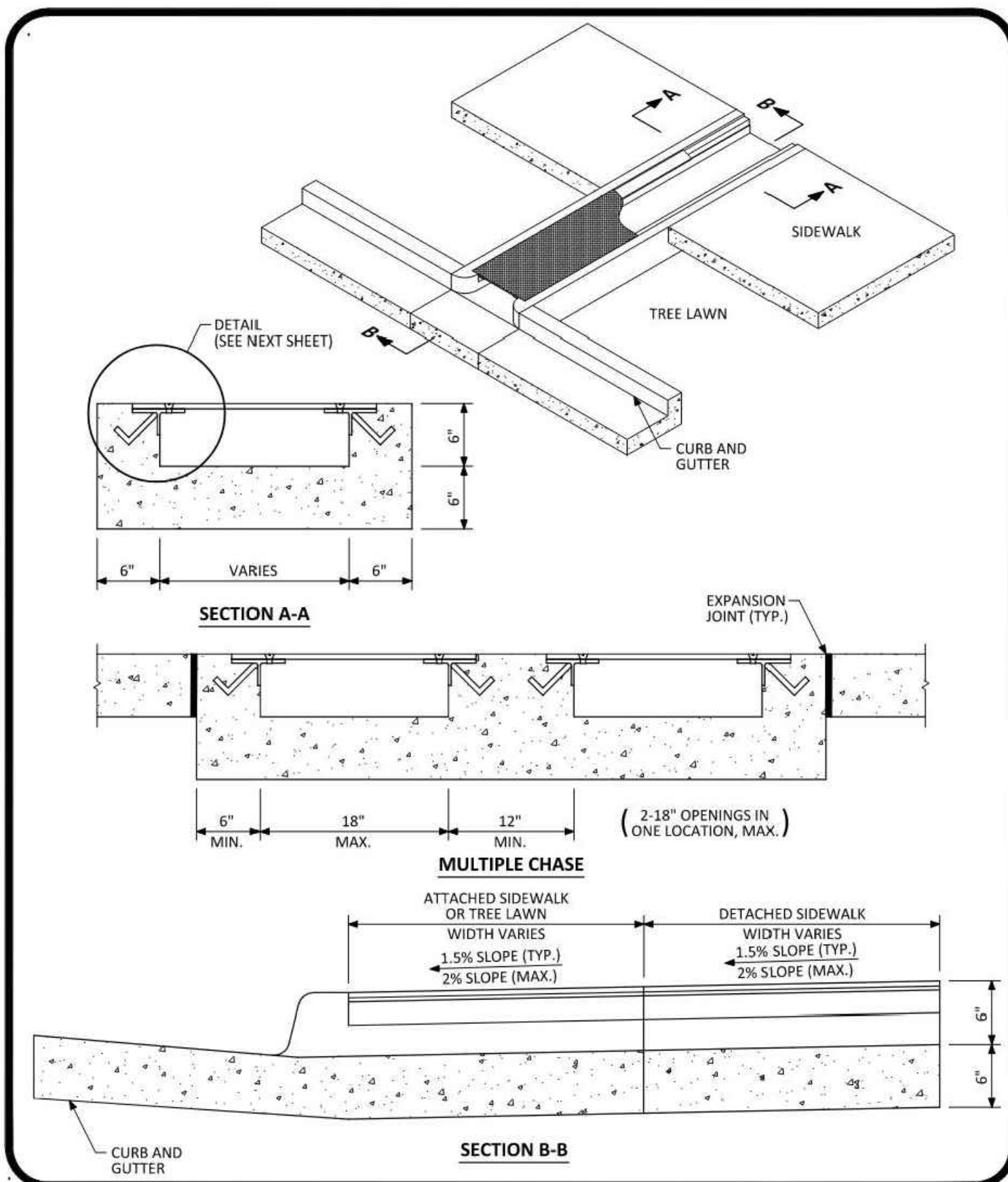
DRAWING TITLE: SITE DETAILS

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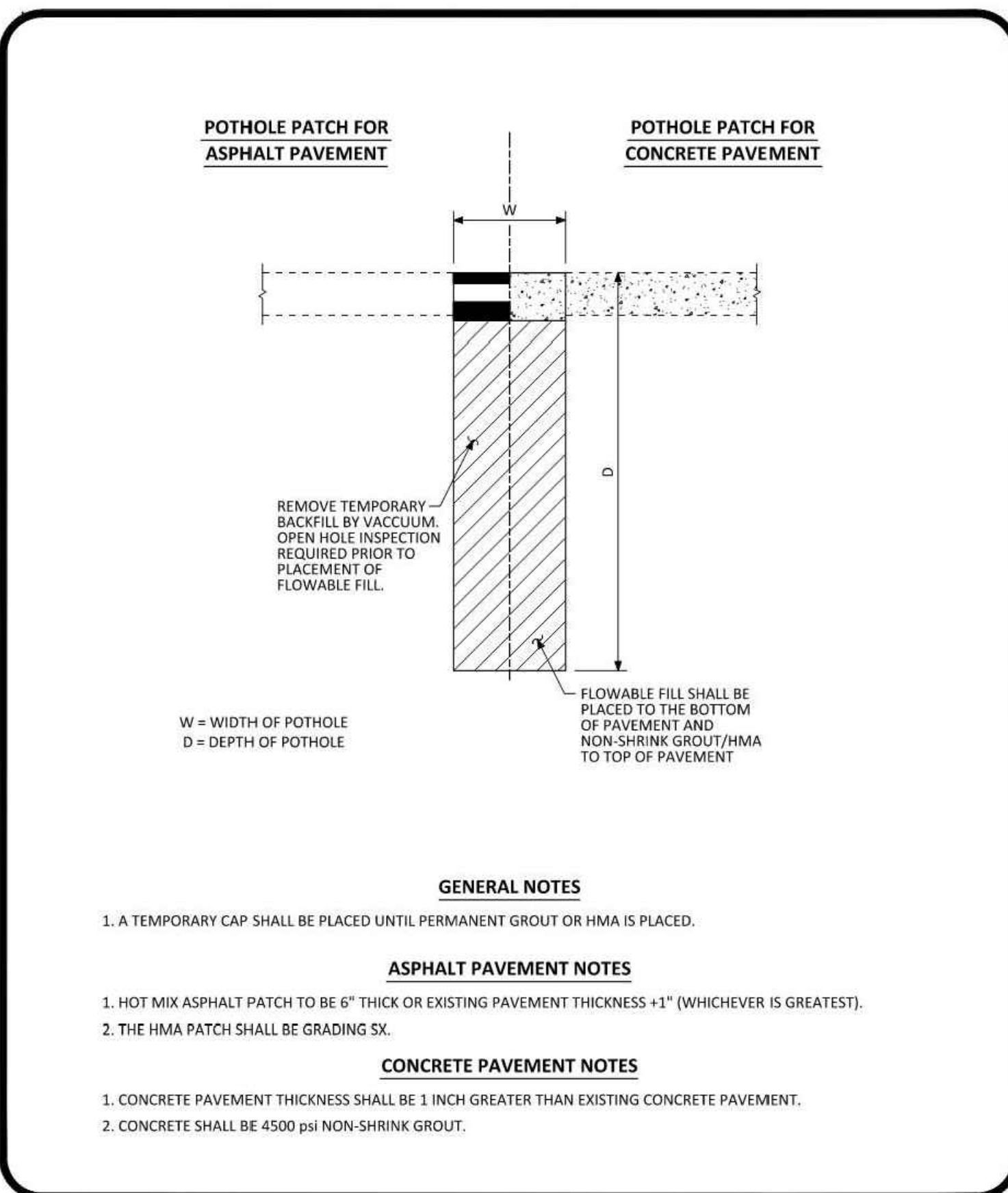


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139  
DRAWING NUMBER:

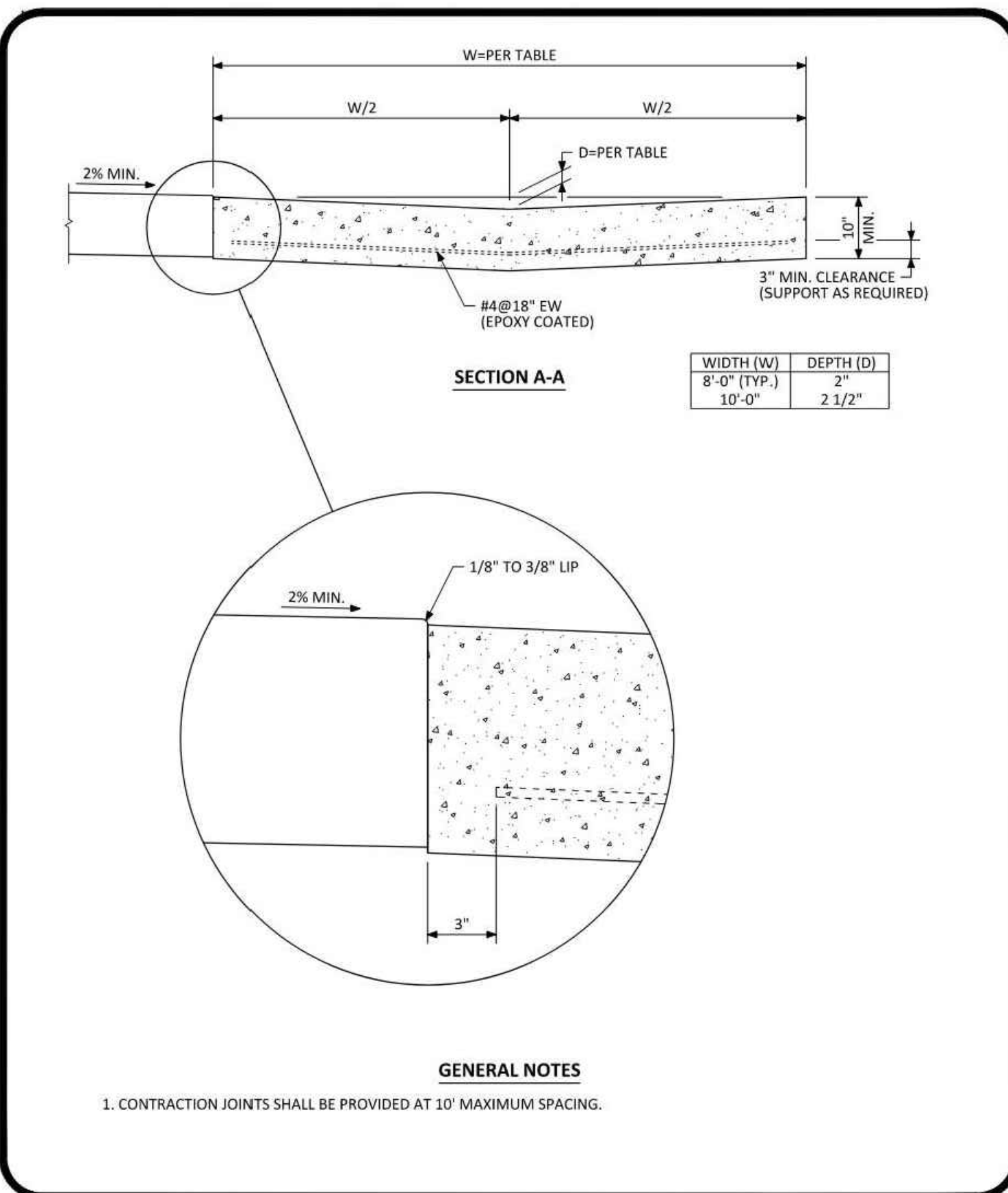
C-452



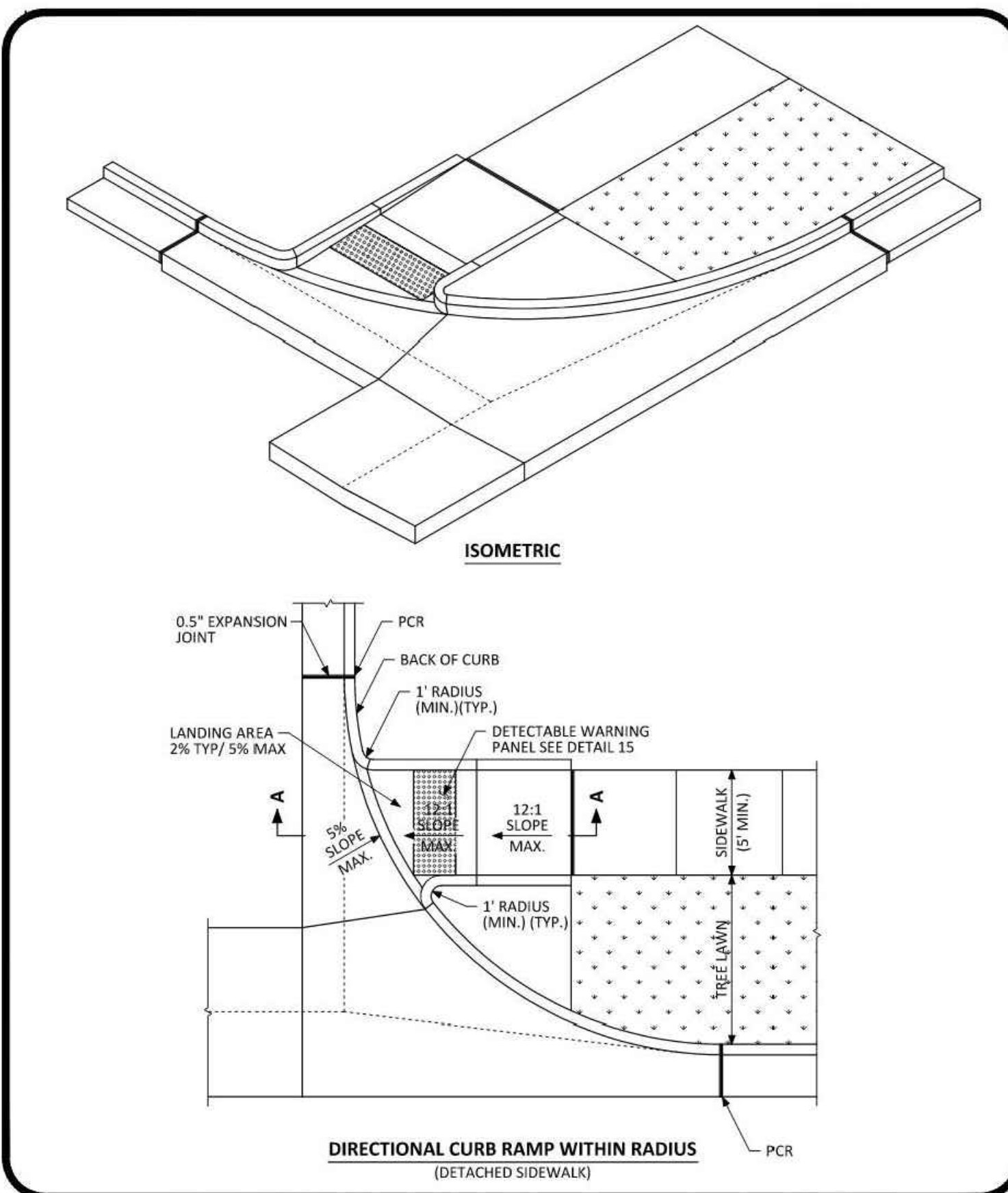
**PARKER COLORADO** CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 29 1 OF 2



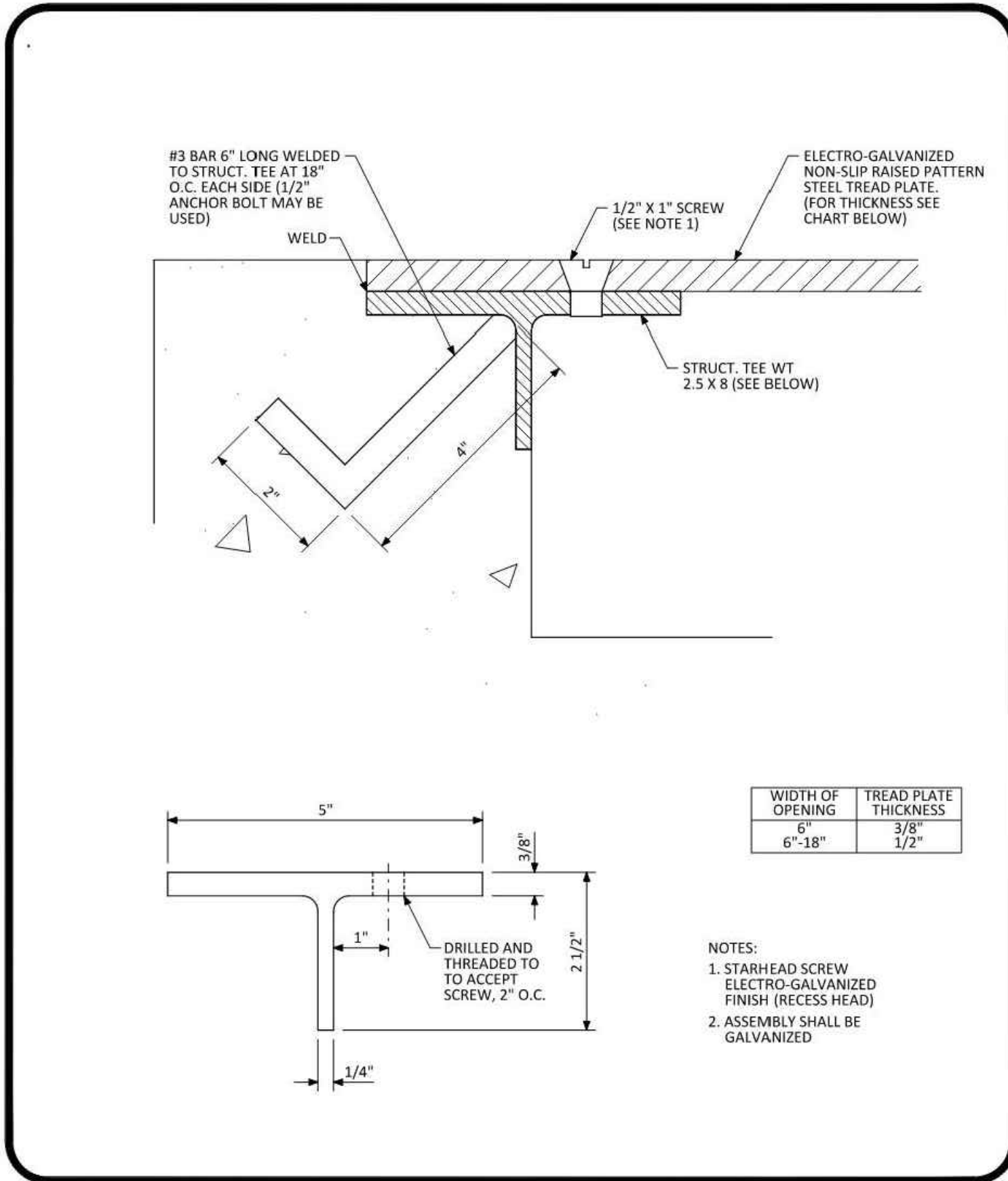
**PARKER COLORADO** TEST HOLE RESTORATION STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 25 2 OF 2



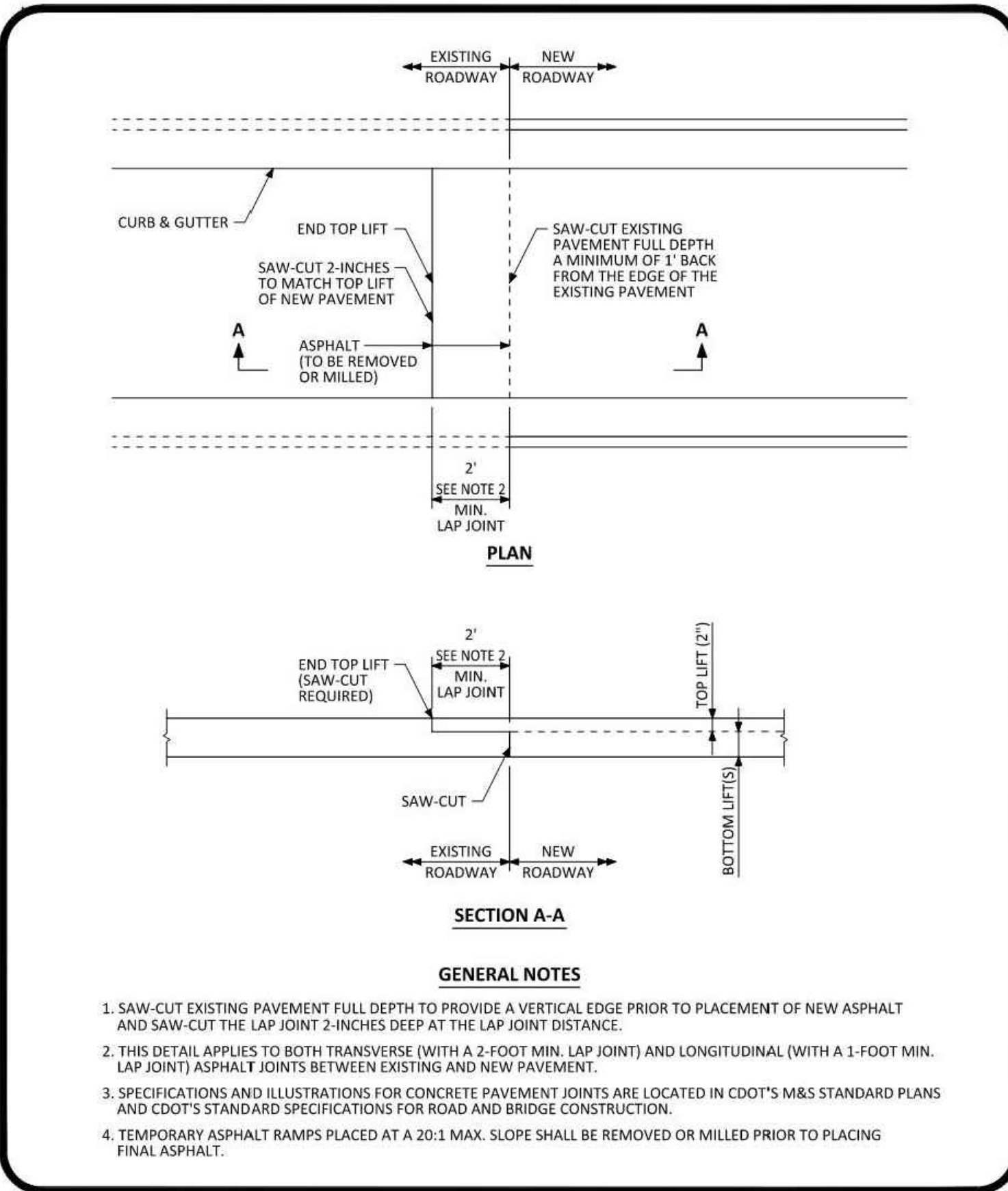
**PARKER COLORADO** INTERSECTION CROSS PAN LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 19 1 OF 2



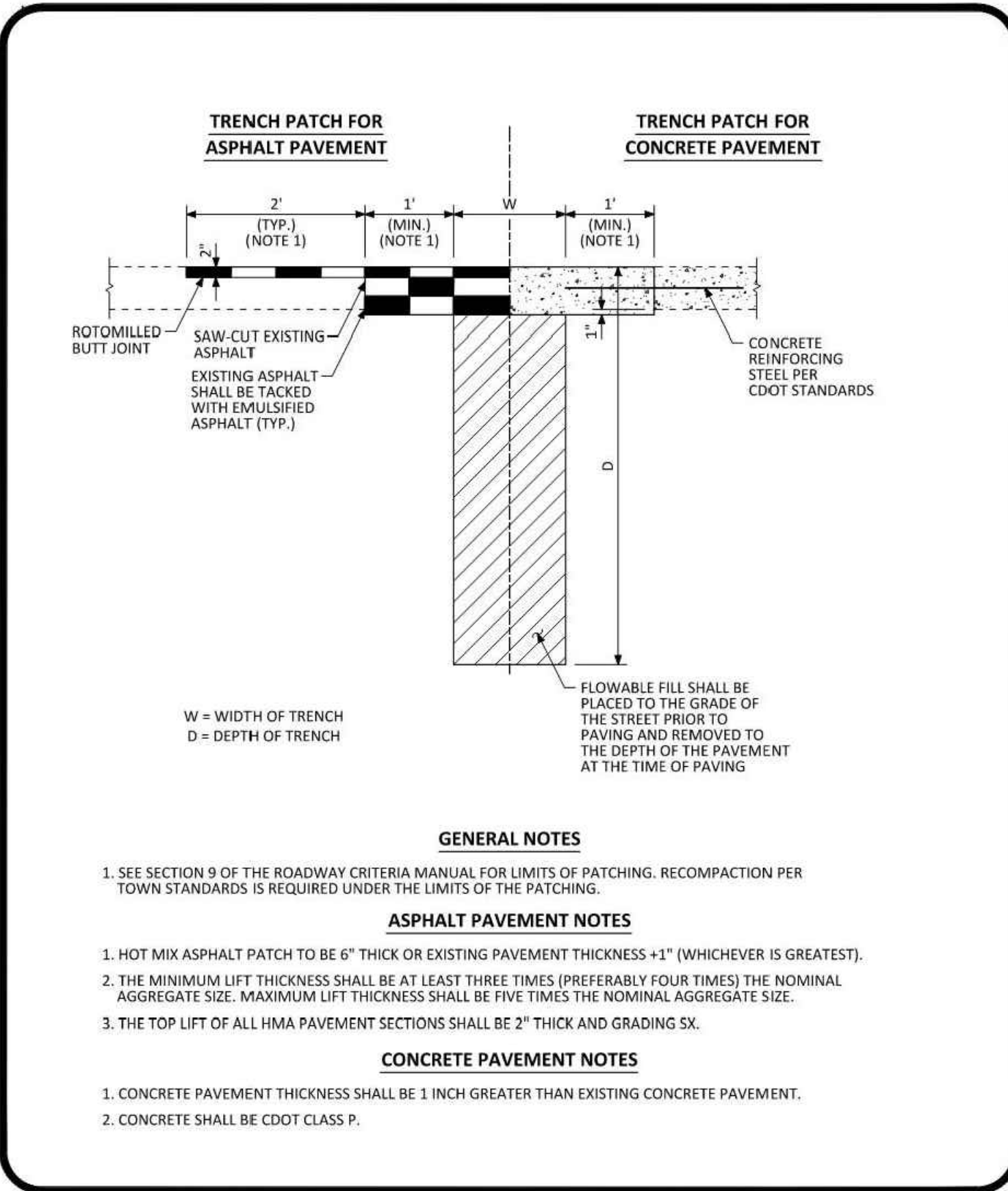
**PARKER COLORADO** DIRECTIONAL CURB RAMP WITHIN RADIUS LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 18 2 OF 2



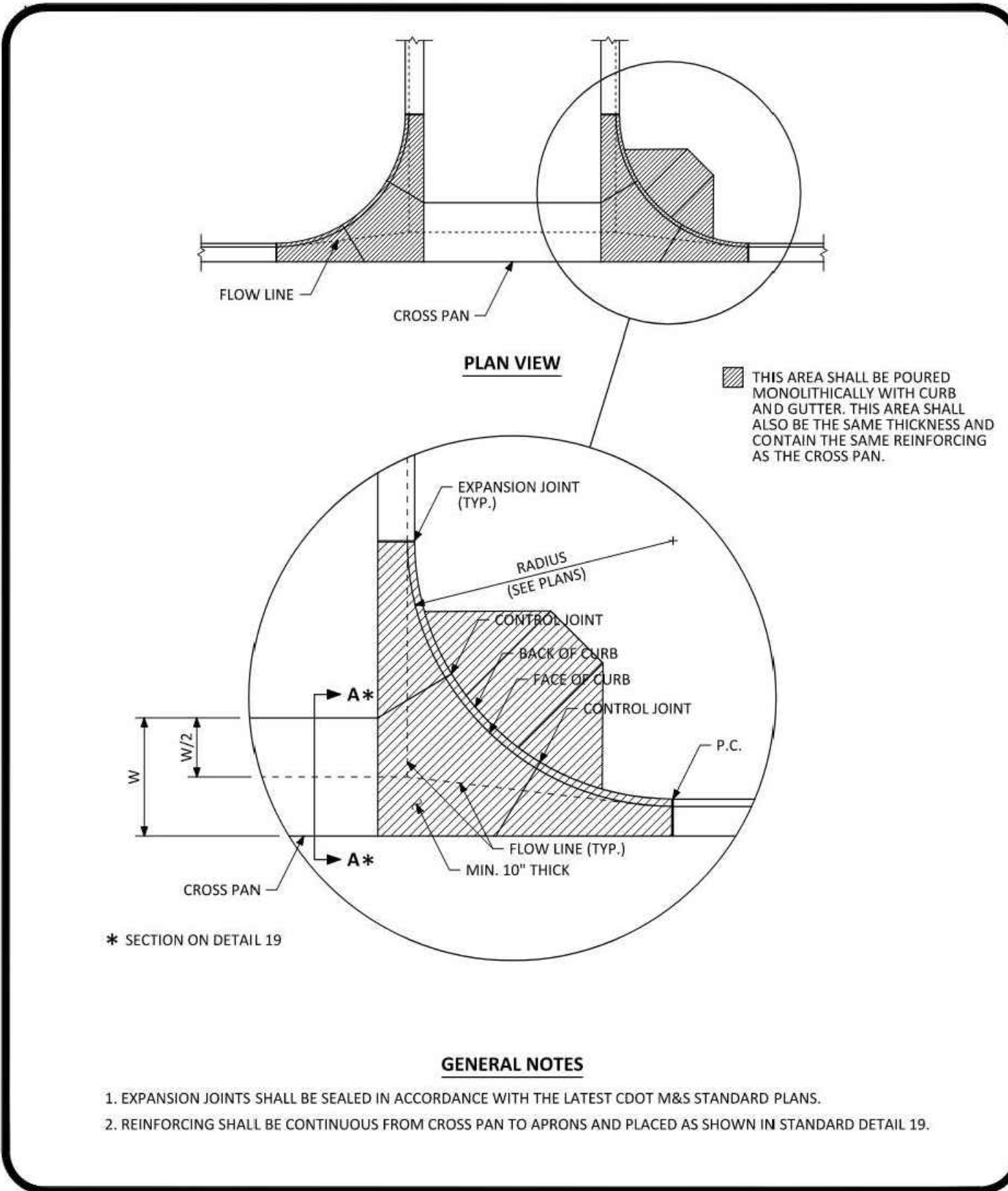
**PARKER COLORADO** CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 29 2 OF 2



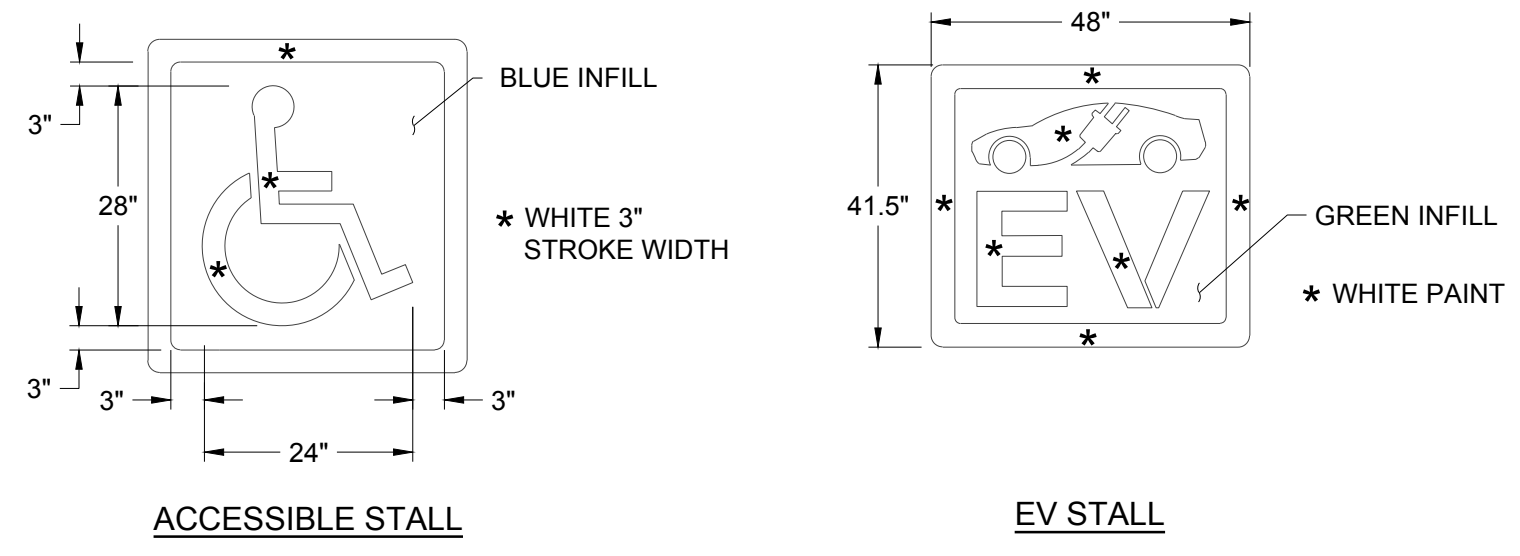
**PARKER COLORADO** ASPHALT CONNECTION BETWEEN EXISTING AND NEW PAVEMENT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 26 1 OF 1



**PARKER COLORADO** MINIMUM STREET PATCH DETERMINATION STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 25 1 OF 2

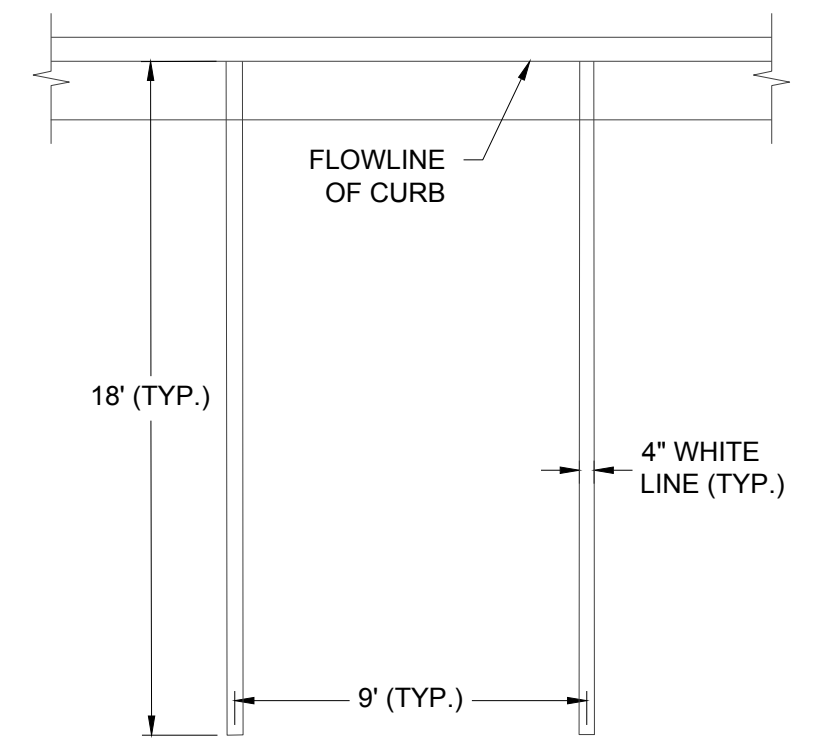


**PARKER COLORADO** INTERSECTION CROSS PAN LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 19 2 OF 2



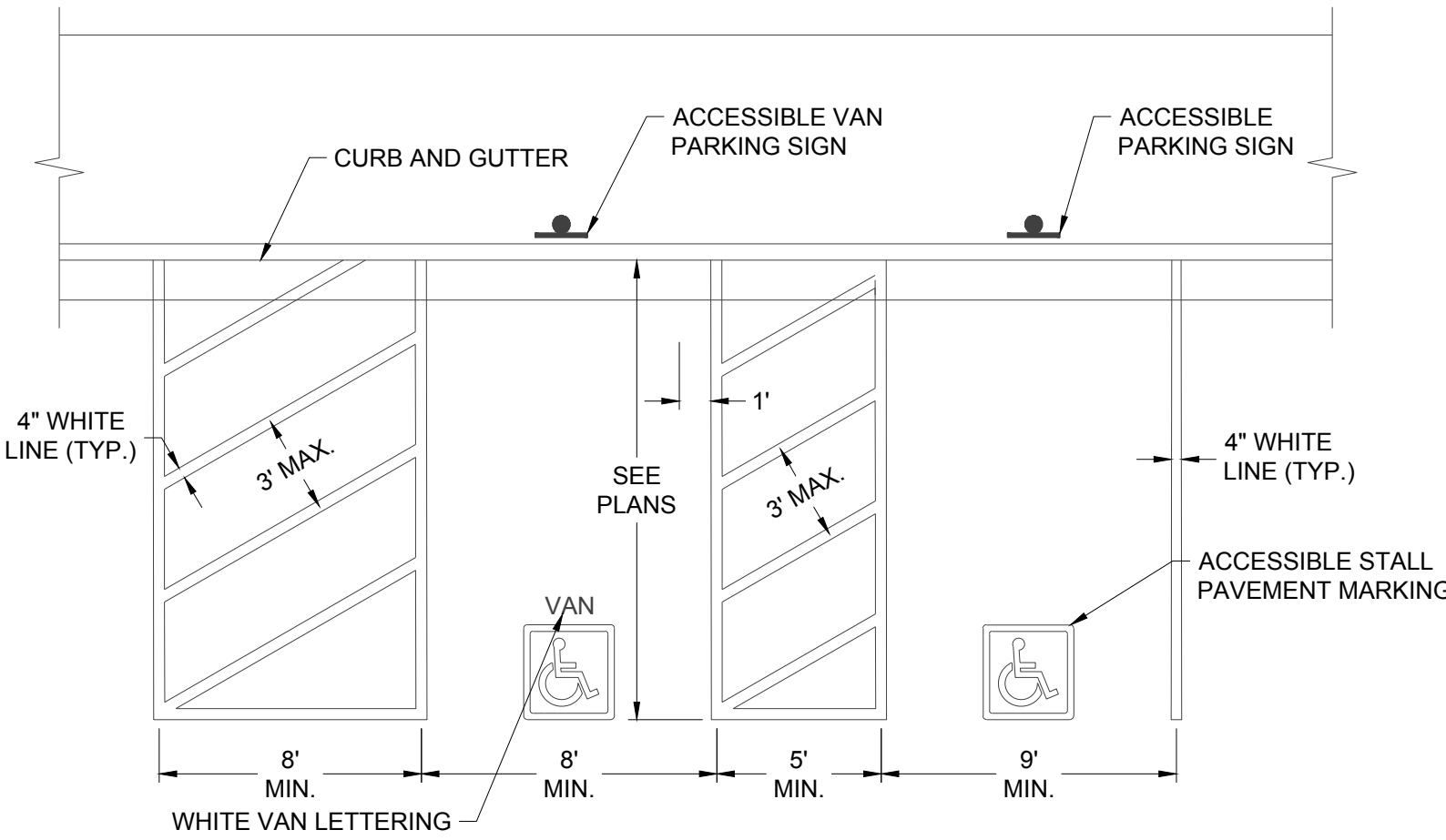
**NOTES:**  
 1. WHITE PAINT OR THERMOPLASTIC MARKINGS SHALL BE USED UNLESS OTHERWISE SPECIFIED BY AUTHORITY HAVING JURISDICTION. ALSO COMPARE AGAINST PROJECT SPECIFIC CIVIL SPECS TO ENSURE THEY MATCH.

**6 PAVEMENT MARKINGS DETAIL** NO SCALE



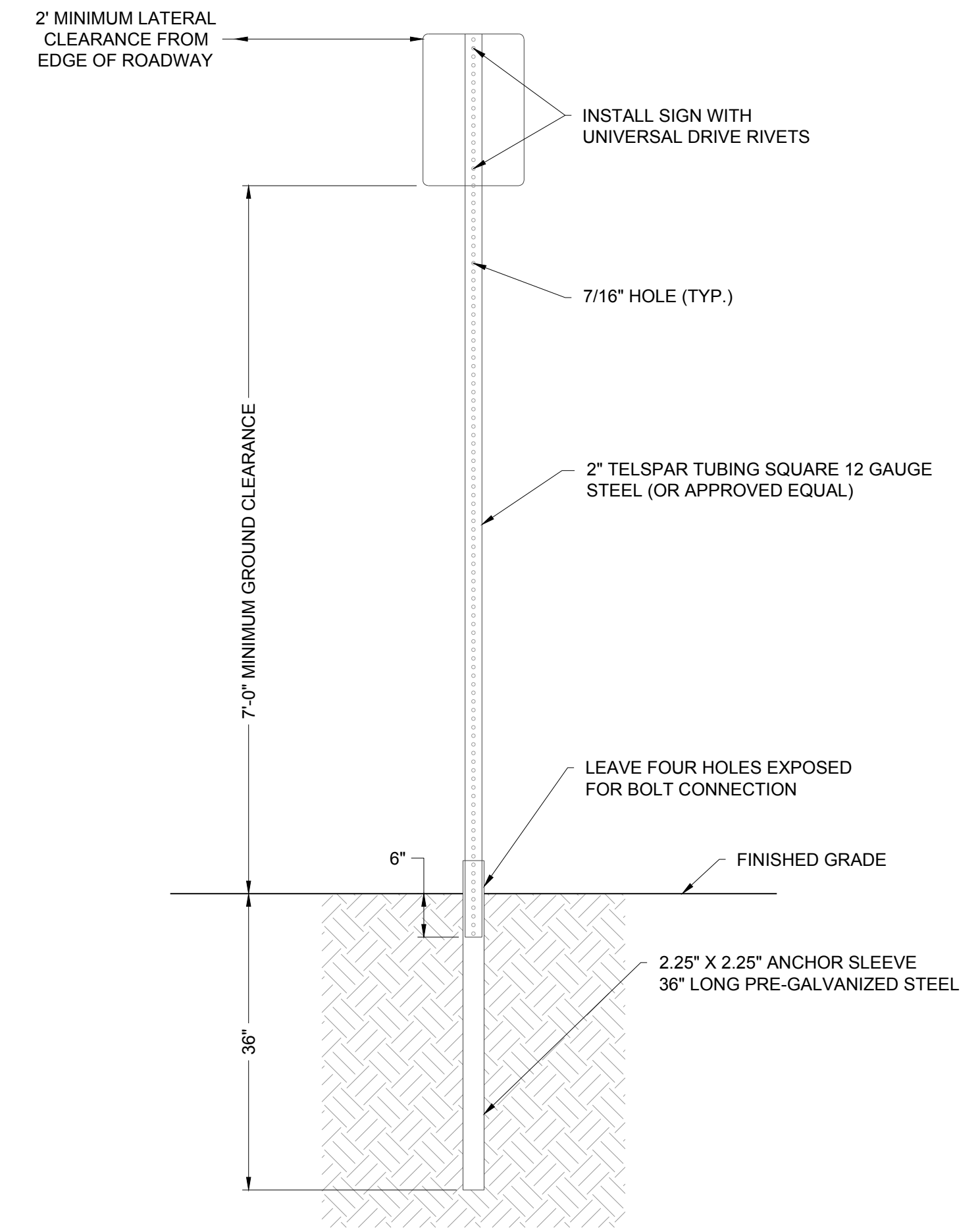
**NOTES:**  
 1. PARKING STALLS ADJACENT TO LANDSCAPE ISLANDS SHALL BE MINIMUM OF 10' IN WIDTH.

**7 TYPICAL PARKING STALL DETAIL** NO SCALE



**NOTE:**  
 THE ACCESSIBLE STALLS AND ASSOCIATED AISLES SHALL BE SLOPED AT A MAXIMUM OF 2% IN ANY DIRECTION. REFER TO GRADING PLANS FOR SPECIFIC ELEVATIONS AND CONTACT THE ENGINEER IF THERE ARE ANY CONCERNS

**8 TYPICAL ACCESSIBLE STALLS DETAIL** NO SCALE

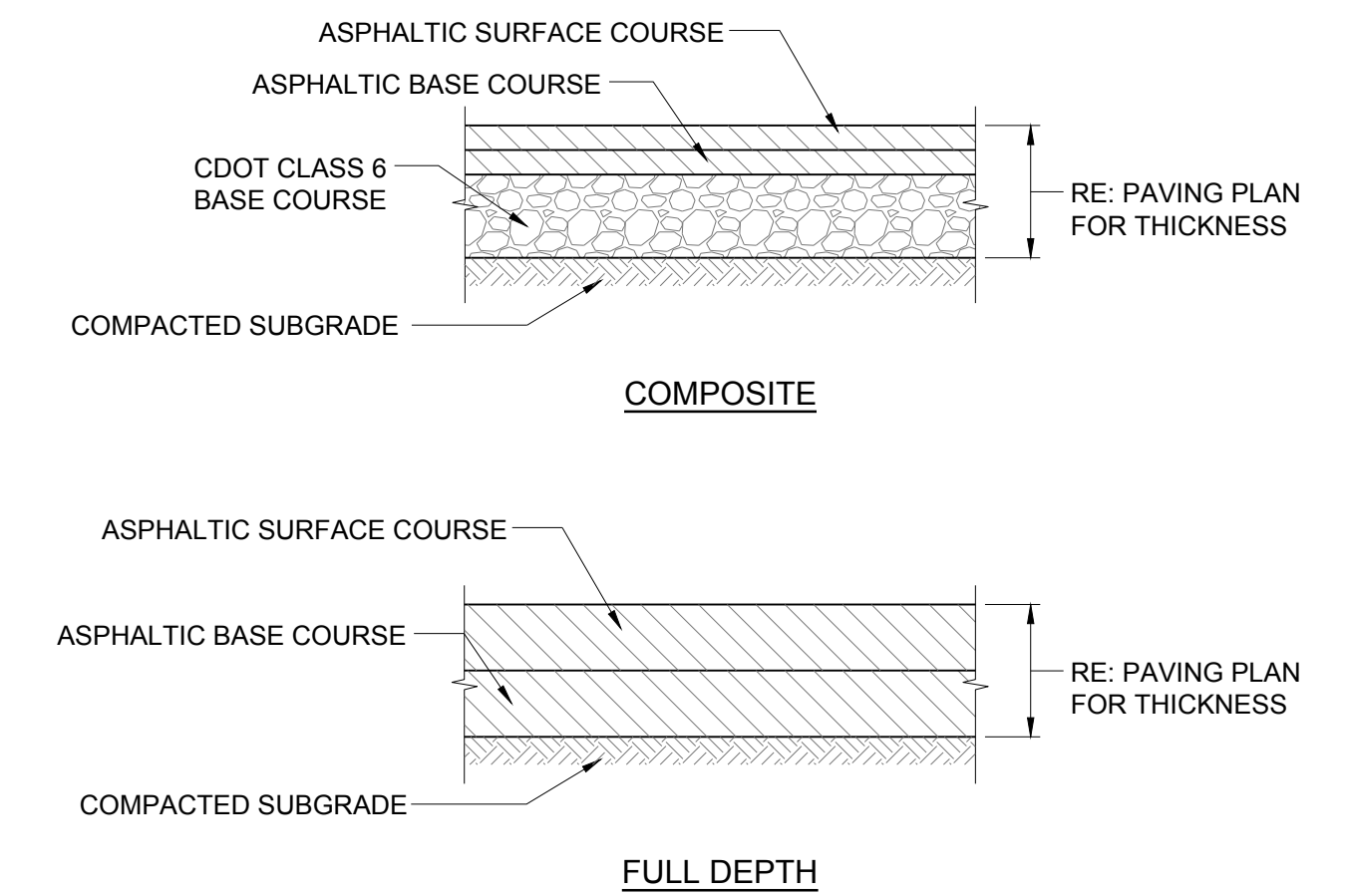


**NOTES:**  
 1. SIGN POST INSTALLATION SHALL FOLLOW THE MANUFACTURE'S SPECIFICATIONS.  
 2. ALL TUBING SHALL BE #12 U.S. STANDARD GAUGE WALL THICKNESS.  
 3. SIGN AND POST SHALL BE PLUMB AND FREE OF VERTICAL AND LATERAL MOVEMENT.  
 4. SIGN SHALL BE PARALLEL TO FLOWLINE FOR PARKING STALL INSTALLATION AND 90° FOR ROADWAY INSTALLATION.  
 5. DRIVE ANCHOR POST INTO THE GROUND TOGETHER, LEAVING FOUR HOLES EXPOSED FOR BOLT CONNECTION. FOR HAND INSTALLATION, DRIVE CAP OF PROPER SIZE SHALL BE USED TO PREVENT DAMAGE TO TUBING. SIDEWALK SHALL BE POURED AROUND A PVC SLEEVE. EXISTING SIDEWALK SHALL BE CORE DRILLED FOR A MINIMUM OF A 4" PVC SLEEVE.  
 6. INSERT SIGN POST INTO ANCHOR SLEEVE APPROXIMATELY 6" AND BOLT IN PLACE.  
 7. A 10' POST WILL WORK FOR SIGNS OF 30" OR LESS. ANY SIGNS LARGER WILL REQUIRE A 11' OR 12' POST DEPENDING ON SIGN SIZE TO MAINTAIN 7' OF GROUND CLEARANCE.

**4 SIGN SUPPORT DETAIL** NO SCALE

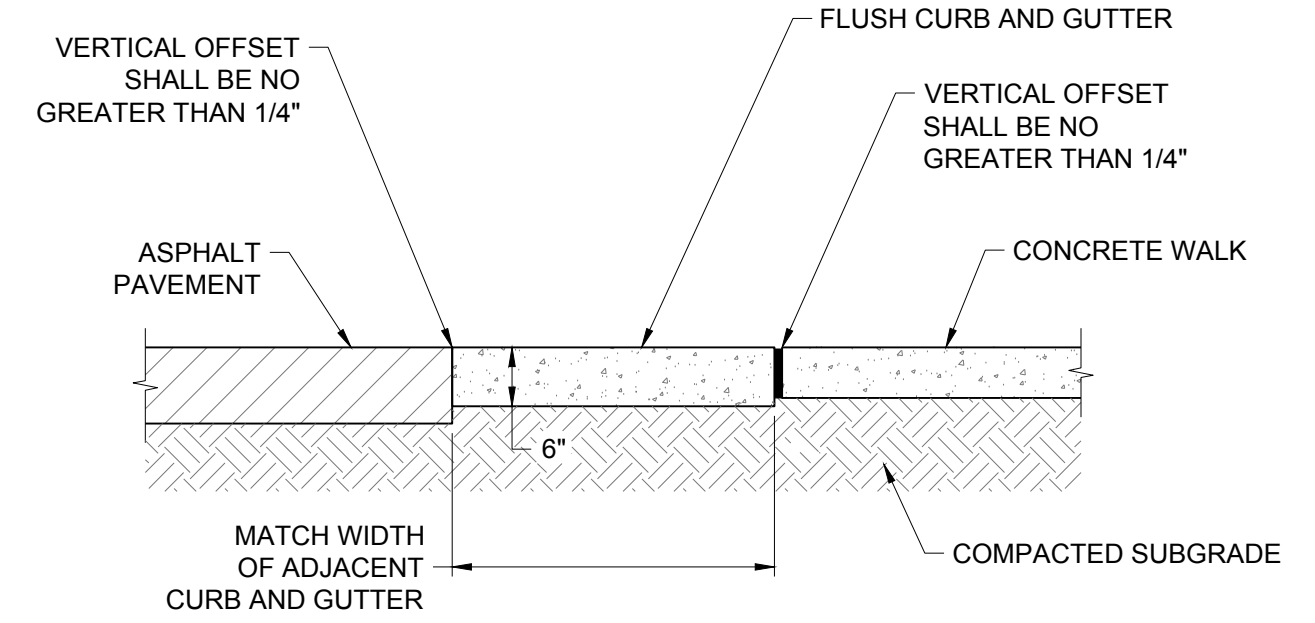


**5 SIGN DETAILS** NO SCALE

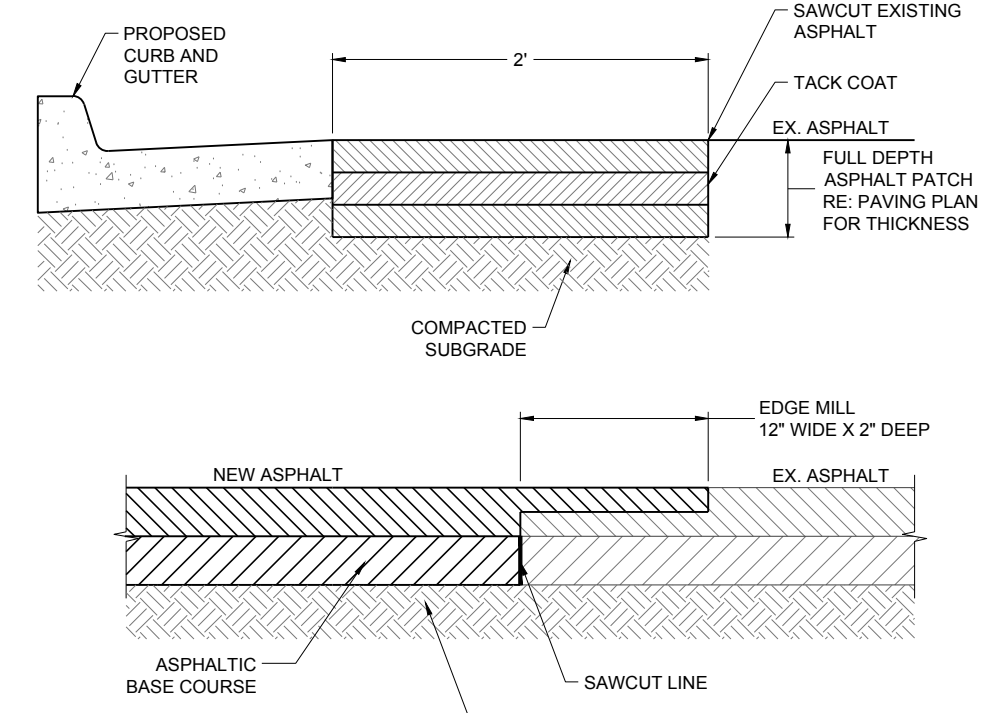


**A ASPHALT PAVEMENT SECTIONS** NO SCALE

**1 PAVEMENT SECTIONS** NO SCALE



**2 FLUSH CURB AND GUTTER DETAIL** NO SCALE



**3 ASPHALT PATCHBACK DETAIL** NO SCALE

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Town of Parker, Director of Engineering/Public Works Date

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL

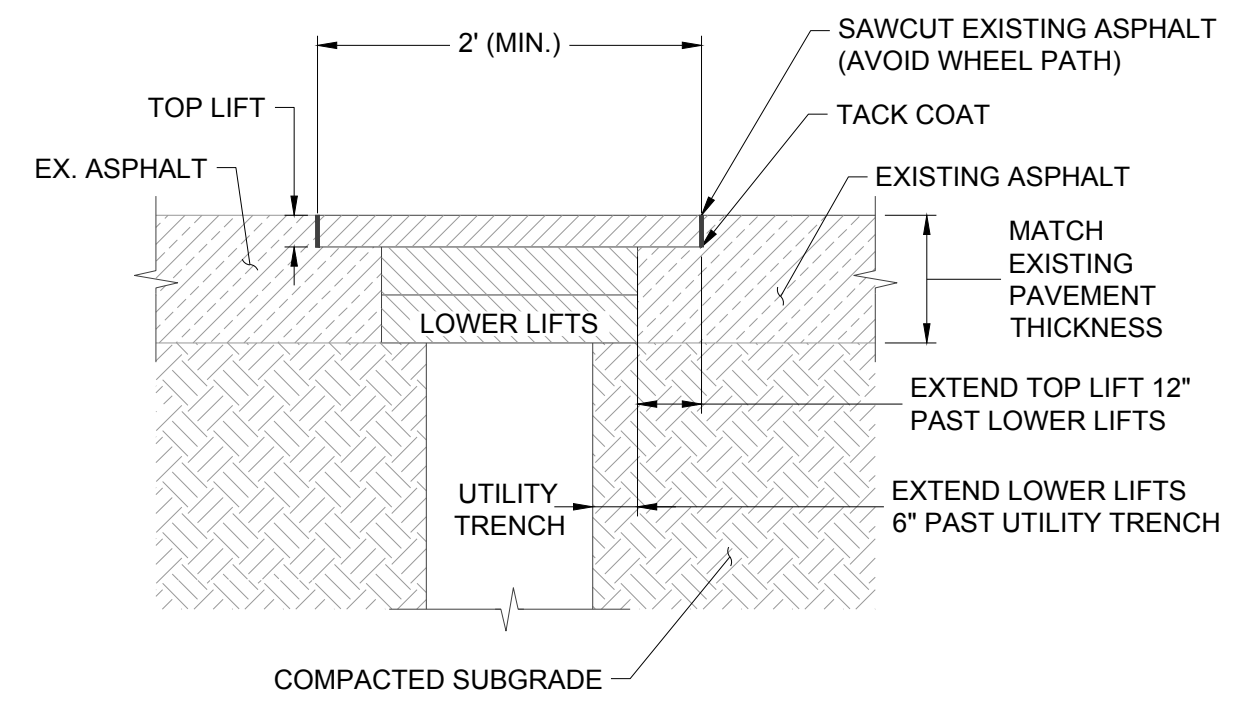
**PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**  
 PROJECT:  
 DRAWING TITLE:  
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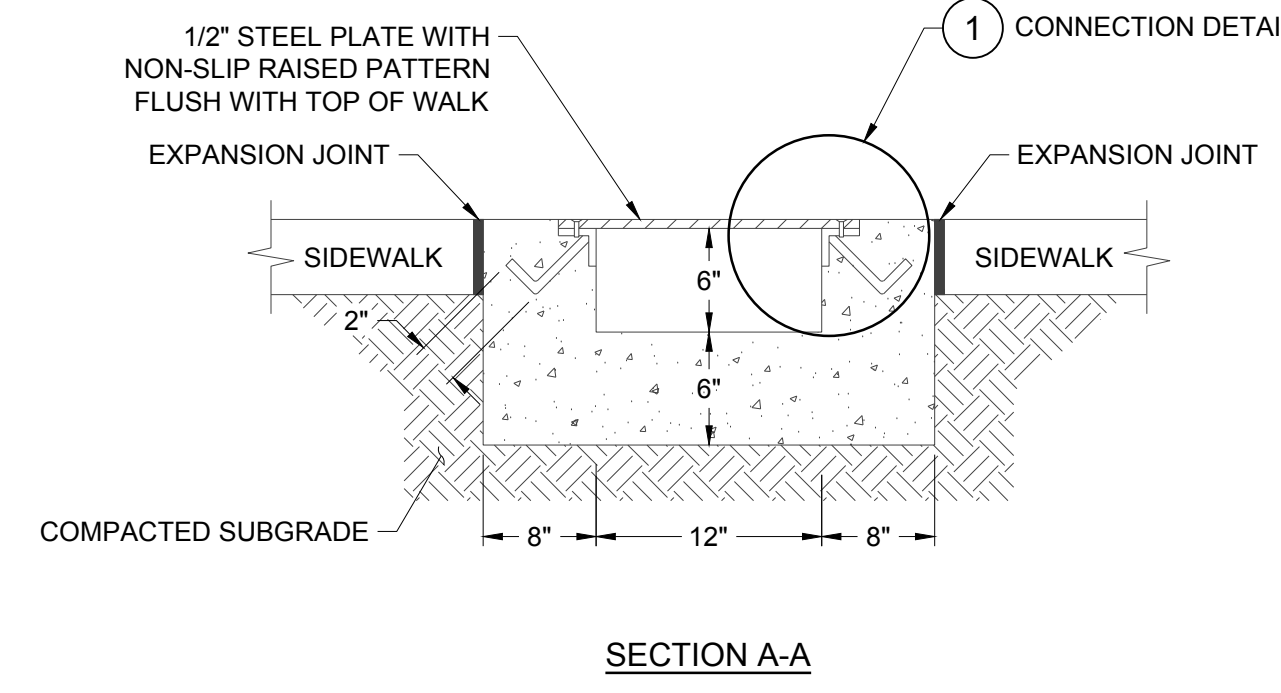
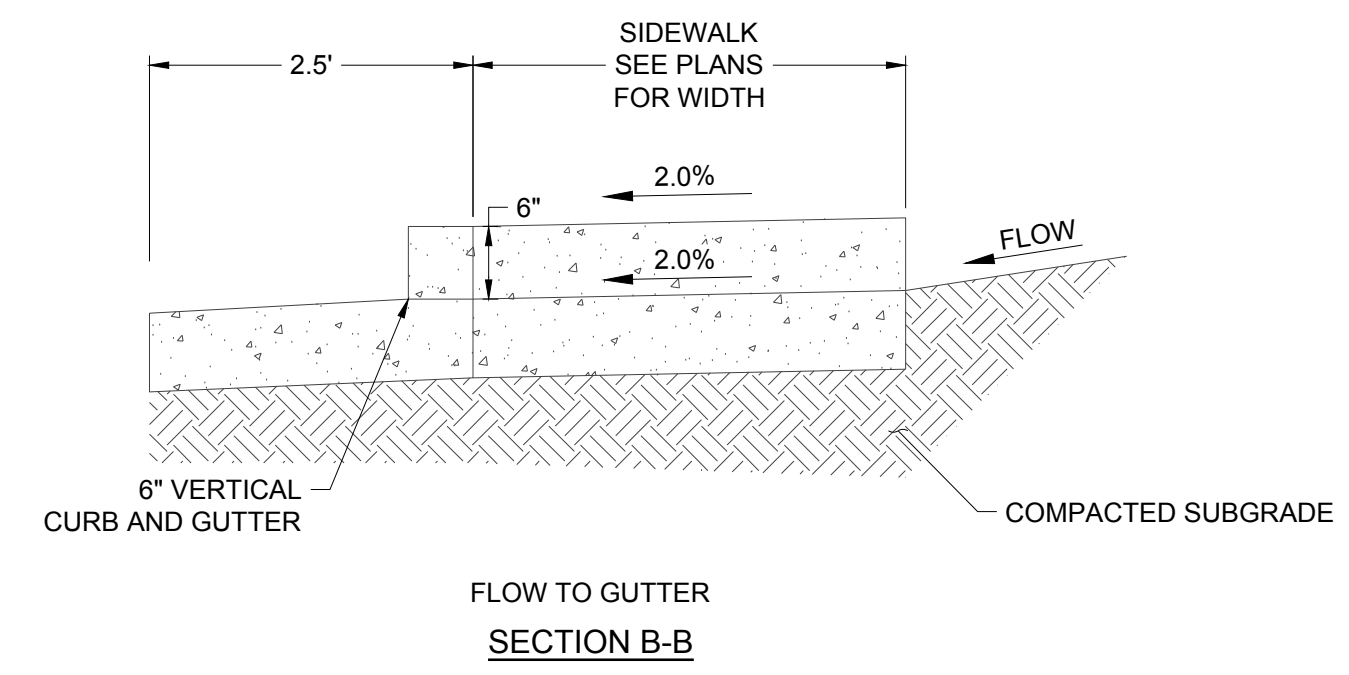
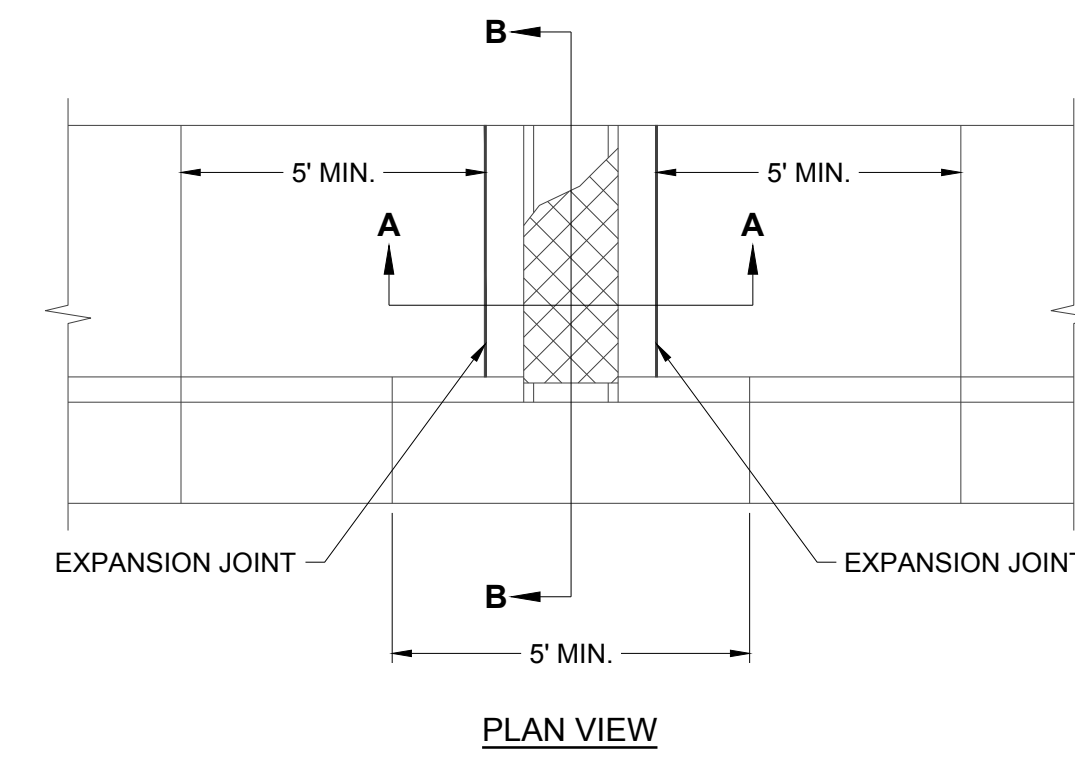
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 DRAWN BY: DEI  
 CHECKED BY: MHV  
 MIRO JOB NO. 22139

DRAWING NUMBER:

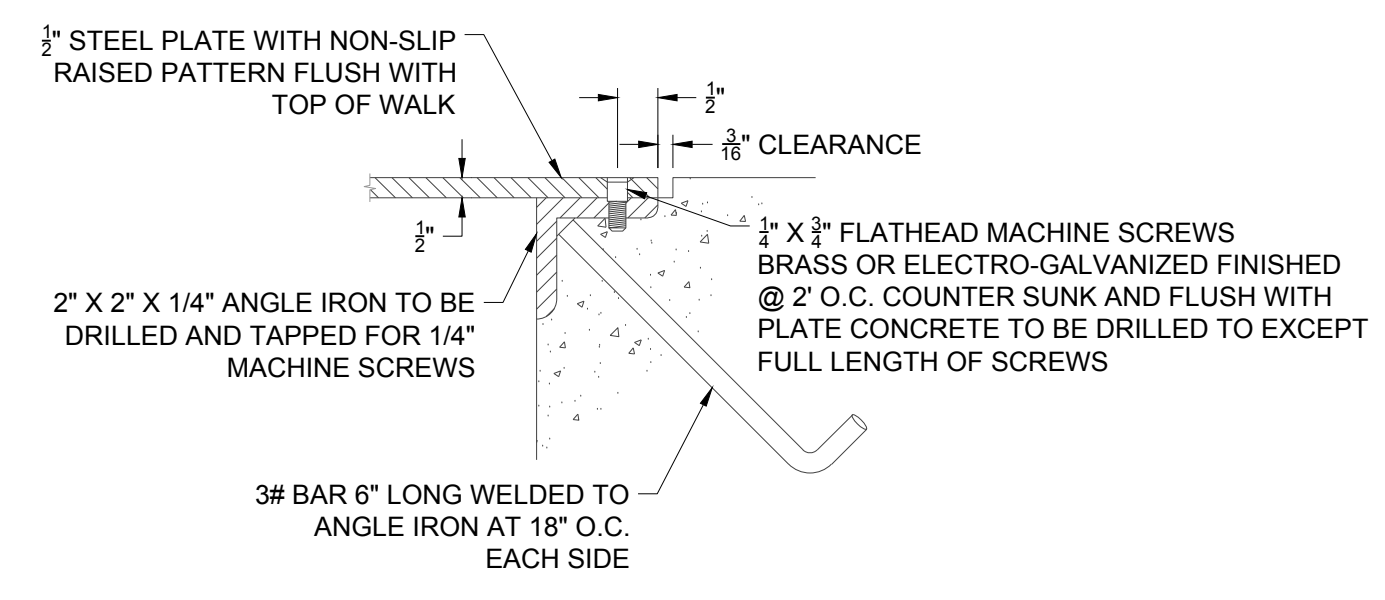
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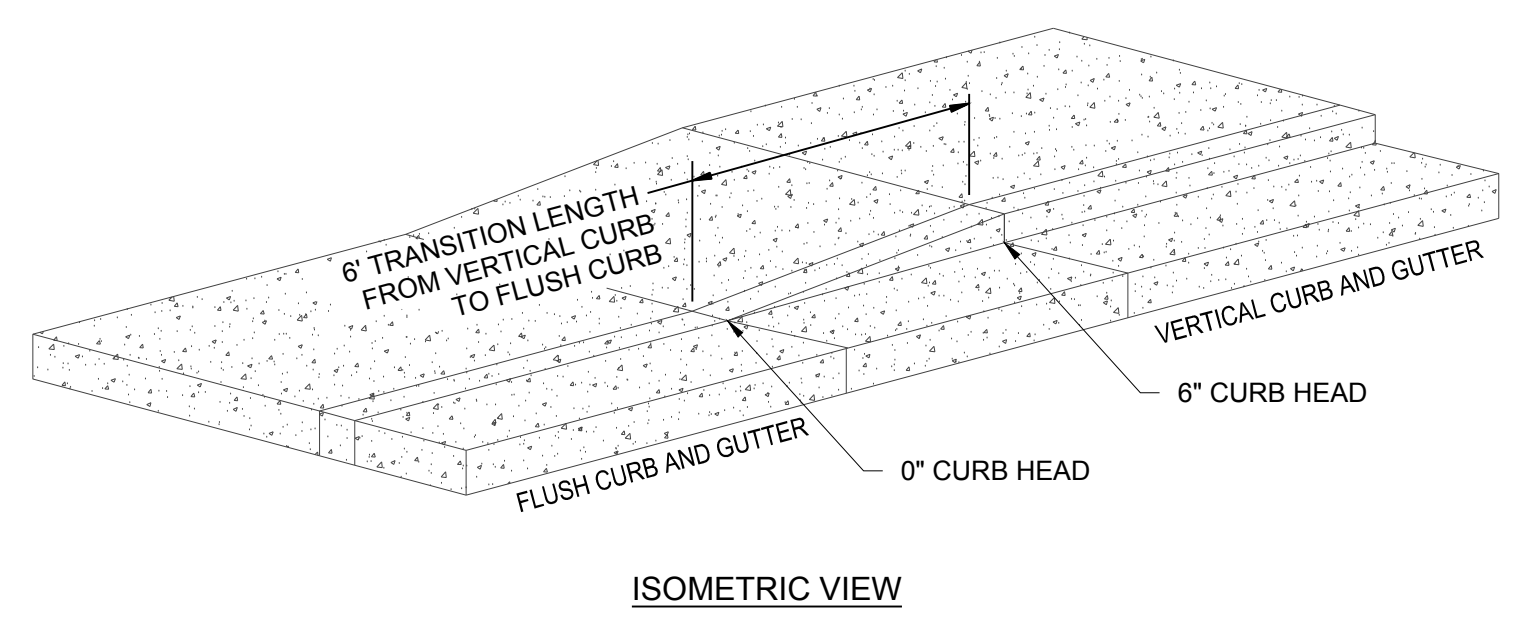
5 UTILITY CUT ASPHALT PATCHBACK DETAIL  
C-454 3" MAXIMUM COMPACTED LIFTS NO SCALE



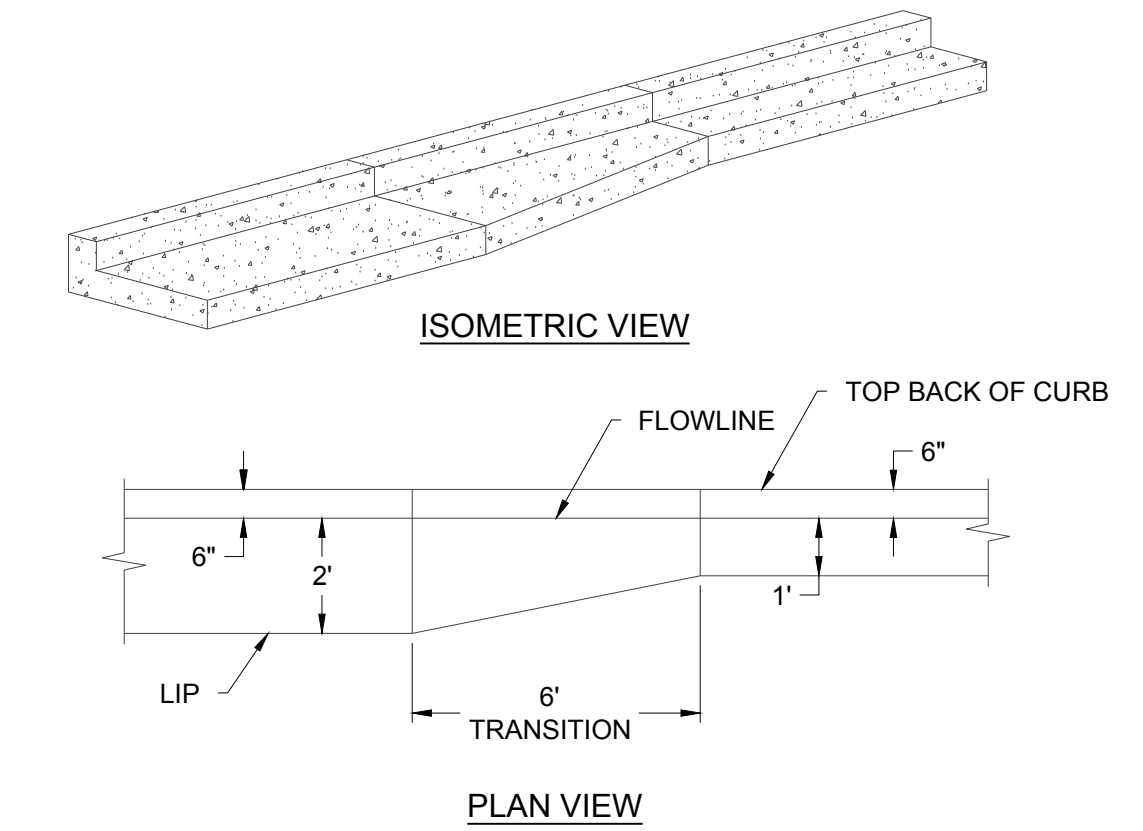
1 ATTACHED SIDEWALK CHASE DETAIL  
C-454 NO SCALE



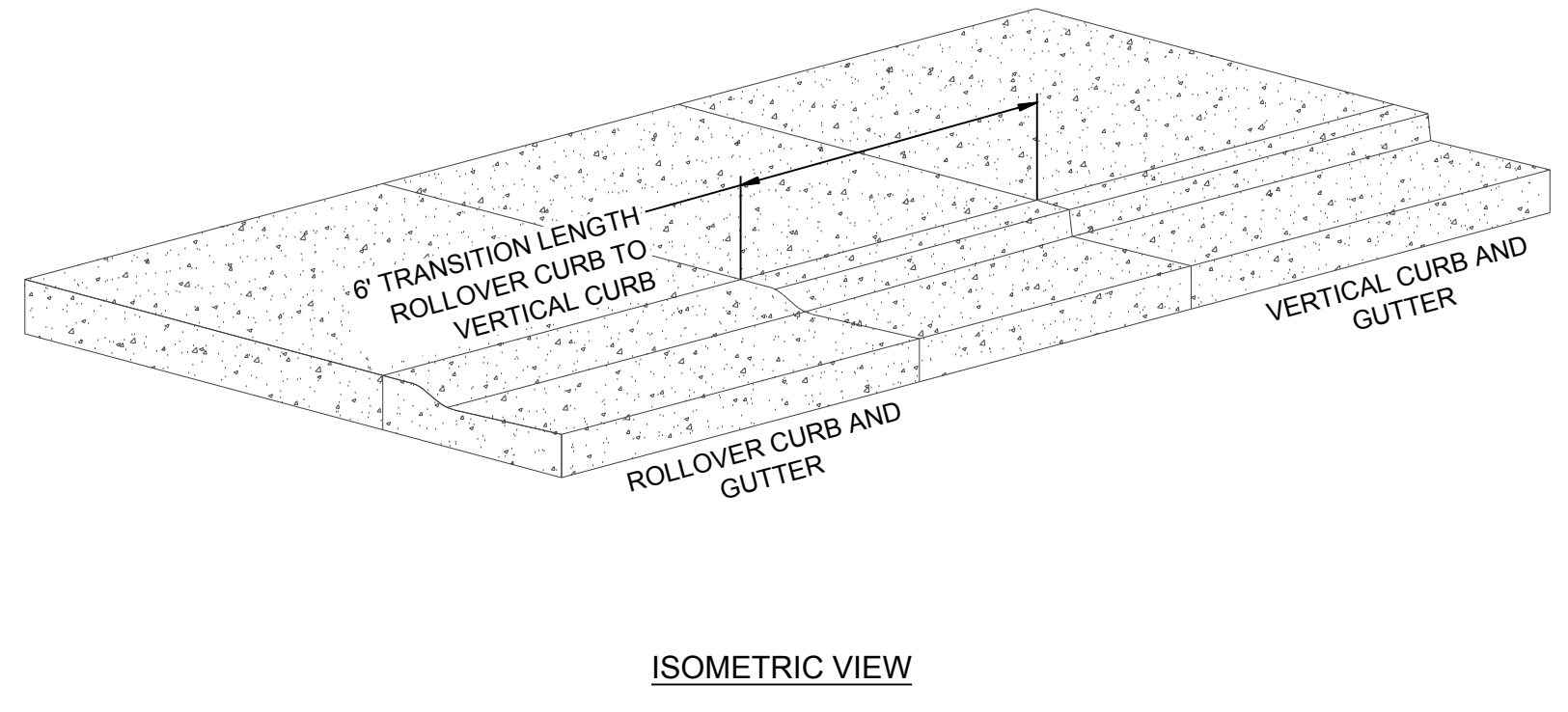
1 CONNECTION DETAIL NO SCALE



3 TRANSITION FROM 6\"/>



2 TRANSITION FROM 2\"/>



4 TRANSITION FROM ROLLOVER CURB TO 6\"/>

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NO.	DESCRIPTION	DATE
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**PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**  
SITE DETAILS



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:

**C-454**