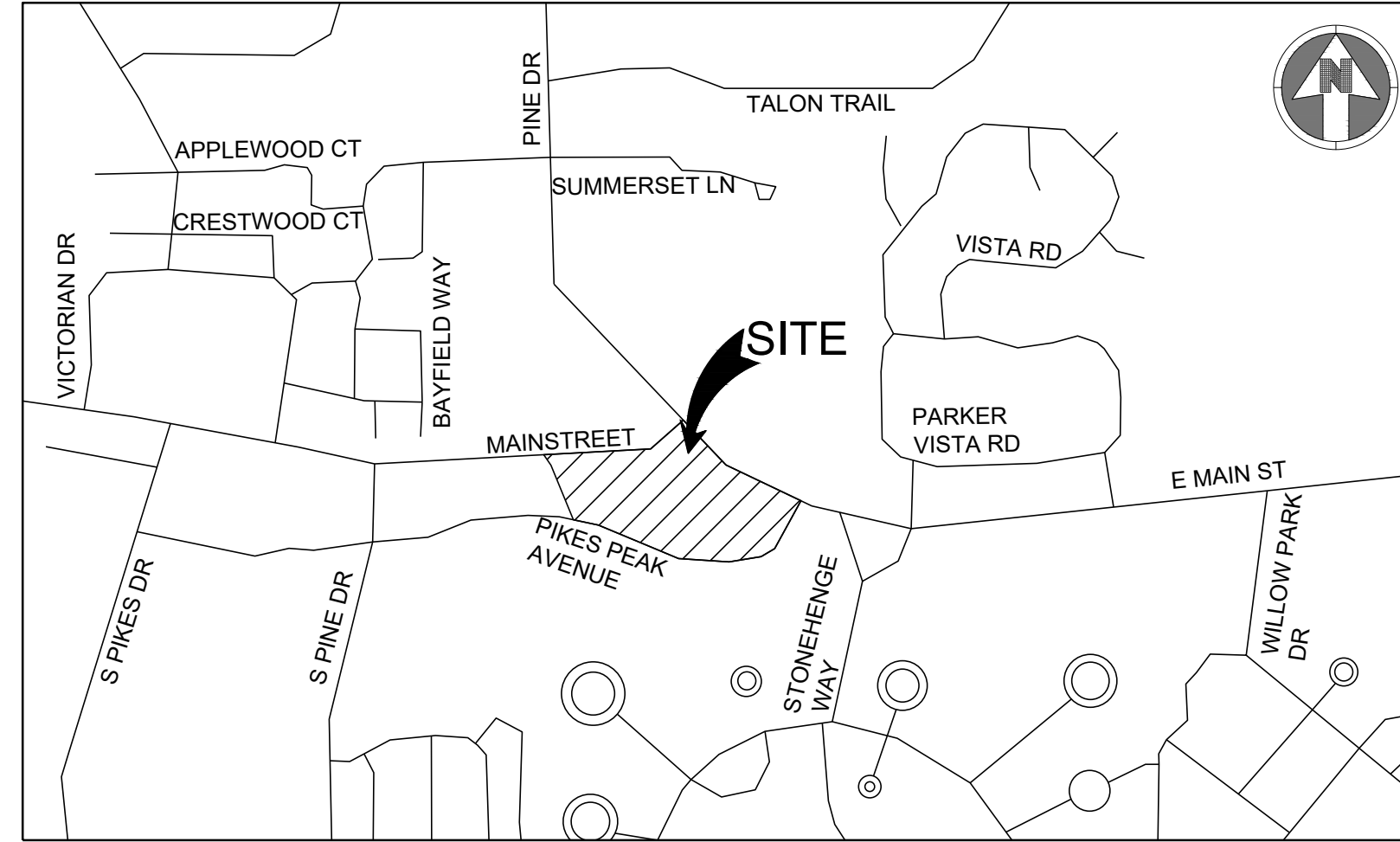


CONSTRUCTION PLANS  
FOR  
**PARKER TOWN HALL**

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED  
FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



VICINITY MAP  
NTS

Drawing Number	Sheet Title
C-000	COVER SHEET
C-001	CIVIL NOTES AND LEGEND
C-021	SITE PLAN
C-111	DEMOLITION PLAN
C-121	HORIZONTAL CONTROL PLAN
C-122	HORIZONTAL CONTROL DATA
C-211	GRADING PLAN
C-240	EROSION CONTROL COVER SHEET
C-241	INITIAL EROSION CONTROL PLAN
C-242	INTERIM EROSION CONTROL PLAN
C-243	FINAL EROSION CONTROL PLAN
C-244	OFF-SITE EROSION CONTROL PLAN
C-251	EROSION CONTROL DETAILS
C-252	EROSION CONTROL DETAILS
C-253	EROSION CONTROL DETAILS
C-254	EROSION CONTROL DETAILS
C-255	EROSION CONTROL DETAILS
C-256	EROSION CONTROL DETAILS
C-257	EROSION CONTROL DETAILS
C-258	EROSION CONTROL DETAILS
C-259	EROSION CONTROL DETAILS
C-311	UTILITY PLAN
C-331	STORM PLAN AND PROFILES
C-332	STORM PLAN AND PROFILES
C-333	STORM PLAN AND PROFILES
C-411	PAVING PLAN
C-421	SIGNAGE AND STRIPING PLAN
C-451	SITE DETAILS
C-452	SITE DETAILS
C-453	SITE DETAILS
C-454	SITE DETAILS

**ENGINEER'S STATEMENT**

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.



MEGAN HUERTTER VOGT  
REGISTERED PROFESSIONAL ENGINEER  
COLORADO NO. 47666

**M I R O**  
S.A. MIRO INC.  
CONSULTING ENGINEERS  
4582 South Ulster Street  
Suite 750, Denver, CO 80237  
303-741-3737  
www.samiro.com

**811**  
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NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024
3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **COVER SHEET**

FILE PATH: C:\Users\jgh\OneDrive\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C000-COVR.dwg C000 - 4/10/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-000**

**CONTACTS:**

<b>OWNER'S REPRESENTATIVE</b> TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546	<b>CIVIL ENGINEER</b> S.A. MIRO INC. MEGAN H. VOGT 4582 SOUTH ULSTER STREET SUITE 750 DENVER, CO 80237 303-741-3737	<b>SURVEYOR</b> AZTEC CONSULTANTS, INC. DANIEL E. DAVIS 300 EAST MINERAL AVE, SUITE 1 LITTLETON, CO 80122 P: (303)-713-1898
<b>PUBLIC UTILITIES:</b> <b>SOUTH METRO FIRE DISTRICT</b> 9195 E. MINERAL AVENUE CENTENNIAL, CO 80112 P: (720) 989-2244	<b>PARKER WATER &amp; SANITATION DISTRICT</b> 18100 WOODMAN ROAD PARKER, CO 80134 P: (303) 841-4627	<b>TOWN OF PARKER</b> 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546

**BASIS OF BEARING:**  
BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17866 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.

**BENCHMARK:**  
1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTELTON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL  
ELEVATION=5399.27 (NAVD 88).

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER

Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_

**MASTER LEGEND:**

PROPOSED		EXISTING	
	PROPERTY LINE		CURB AND GUTTER
	SECTION LINE		SIDEWALK
	RIGHT OF WAY LINE		WALL
	CENTER LINE		FENCE
	CURB AND GUTTER		SIGN
	SIDEWALK		LIGHT
	WALL		TREE
	FENCE		MINOR CONTOURS
	SIGN		MAJOR CONTOURS
	LIGHT		SPOT ELEVATION
	TREE		CABLE TV LINE
	MINOR CONTOURS		COMMUNICATION LINE
	MAJOR CONTOURS		ELECTRICAL LINE
	RIDGE LINE		OVERHEAD ELECTRICAL LINE
	SPOT ELEVATION		FIBER OPTIC LINE
	CABLE TV LINE		GAS LINE
	COMMUNICATION LINE		TELEPHONE LINE
	ELECTRICAL LINE		IRRIGATION LINE
	OVERHEAD ELECTRICAL LINE		RAW WATER LINE
	FIBER OPTIC LINE		SANITARY SEWER LINE
	GAS LINE		STEAM LINE
	TELEPHONE LINE		STORM SEWER LINE
	IRRIGATION LINE		WATER LINE
	RAW WATER LINE		EASEMENT
	SANITARY SEWER LINE		INLET
	STEAM LINE		FLARED END SECTIONS
	STORM SEWER LINE		CLEANOUT
	WATER LINE		FIRE HYDRANT
	EASEMENT		VALVE
	INLET		PEDESTAL
	FLARED END SECTIONS		UTILITY CROSSING
	CLEANOUT		MANHOLES
	FIRE HYDRANT		LAMBS TONGUE ROOF DRAIN OVERFLOW
	VALVE		
	WATER FITTINGS		
	PEDESTAL		
	UTILITY CROSSING		
	MANHOLES		
	LAMBS TONGUE ROOF DRAIN OVERFLOW		

**ABBREVIATIONS:**

BFV	BUTTERFLY VALVE	PCC	POINT OF COMPOUND CURVATURE
BOV	BLOW OFF VALVE	PCR	POINT OF CURB RETURN
BW	BOTTOM OF WALL @ FINISHED GRADE	PI	POINT ON INTERSECTION
CL	CENTERLINE	PRC	POINT OF REVERSE CURVATURE
CMP	CORRUGATED METAL PIPE	PT	POINT OF TANGENCY
EJ	EXPANSION JOINT	PVC	POLYVINYL CHLORIDE
ELEC.	ELECTRIC	RCP	REINFORCED CONCRETE PIPE
ELEV.	ELEVATION	RCB	REINFORCED CONCRETE BOX
EX	EXISTING	RL	RIDGE LINE
FES	FLARED END SECTION	ROW	RIGHT OF WAY
FG	FINISHED GRADE	RT	RIGHT
FS	FINISHED SURFACE	SD	STORM DRAINAGE SEWER
FL	FLOWLINE	SS	SANITARY SEWER
GB	GRADE BREAK	STA	STATION
HP	HIGH POINT	STRM	STORM SEWER
FH	FIRE HYDRANT	TOF	TOP OF FLANGE
INV.	INVERT	TOP	TOP OF PIPE
IRR	IRRIGATION	TW	TOP OF WALL @ FINISHED GRADE
KB	KICK BLOCK	TYP	TYPICAL
LF	LINEAR FEET	VCP	VITRIFIED CLAY PIPE
LP	LOW POINT	VPC	VERTICAL POINT OF CURVATURE
LT	LEFT	VPI	VERTICAL POINT OF INTERSECTION
ME	MATCH EXISTING	VPT	VERTICAL POINT OF TANGENCY
MH	MANHOLE	WATR	WATER LINE
PC	POINT OF CURVATURE		

**TOWN OF PARKER STORM DRAINAGE INFRASTRUCTURE NOTES**

- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72" (5 COR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUTED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
- STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
- ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:
  - 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC)
  - 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
- EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS AND STRUCTURES. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
- PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

**TOWN OF PARKER DEPARTMENT OF ENGINEERING/ PUBLIC WORKS STANDARD CONSTRUCTION NOTES**

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- THE TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE TOWN INSPECTOR'S ABSENCE. HOWEVER, THE TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER DIRECTOR OF ENGINEERING/PUBLIC WORKS, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY USE PERMIT APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION."
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR INCLUDING AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR. SUCH RESTORATION MAY BE REQUIRED TO BE COMPLETED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

**GENERAL NOTES:**

- THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS AT AND ADJACENT TO THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THE WORK SHOWN ON THE PLANS OR DESCRIBED IN THE SPECIFICATIONS IN A SATISFACTORY MANNER. THE CONTRACTOR SHALL FURNISH ALL MATERIALS, EQUIPMENT, TOOLS, LABOR, AND INCIDENTALS TO COMPLETE THE WORK.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AT THEIR EXPENSE TO COMPLETE ALL GRADING, UTILITY WORK AND INSTALLATION OF EROSION CONTROL MEASURES.
- THE CONTRACTOR SHALL COORDINATE WORK WITH ALL ADJACENT PROJECTS.
- DURING CONSTRUCTION IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING UTILITY LINES SHOWN ON THESE PLANS AND THOSE UTILITY LINES WHICH MAY NOT BE SHOWN ON THESE PLANS.
- EXISTING SITE INFORMATION SHOWN ON THESE DRAWINGS IS BASED UPON AVAILABLE SURVEY BY AZTEC CONSULTANTS, INC DATED 08/11/2022. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF THE COMPLETENESS OR ACCURACY OF THE EXISTING CONDITIONS PRESENTED. THE CONTRACTOR SHALL INSPECT THE SITE TO ASSESS EXISTING CONDITIONS, AND SATISFY THEMSELVES THAT THE DOCUMENTS ACCURATELY REPRESENT THE SITE CONDITIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES FOUND DURING INITIAL INSPECTIONS, OR SUBSEQUENTLY, SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY FOR RESOLUTION RELATIVE TO THE INTENDED CONSTRUCTION.
- THE EXISTING UTILITIES SHOWN ON THESE PLANS HAVE BEEN LOCATED USING FIELD SURVEY DATA LOCATING MANHOLES, WATER VALVES, ELECTRICAL EQUIPMENT, AND OTHER FEATURES FOUND AT THE SURFACE. NO SUBSURFACE EXPLORATION HAS BEEN DONE TO VERIFY EXACT LOCATION OF EXISTING UTILITIES. THE CONTRACTOR SHALL POTHOLE EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICT PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT AND DELAY RELATED TO UNVERIFIED EXISTING CONDITIONS. WHERE THE CONTRACTOR FINDS CONFLICTS OR DISCREPANCIES, THEY SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY APPROPRIATE ENTITIES FOR UTILITY LOCATES AND START OF CONSTRUCTION PRIOR TO ANY WORK BEING STARTED.
- THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH ALL AFFECTED PARTIES PRIOR TO ANY OUTAGES OR SHUTDOWN OF SERVICE.
- ALL QUANTITIES SHOWN ON THESE PLANS ARE SHOWN FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL QUANTITIES PRIOR TO CONSTRUCTION AND ORDERING OF ANY MATERIALS.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL FOLLOW/VERIFY THE INTENT OF THE DESIGN AT ALL LOCATIONS OF TIE-IN TO EXISTING FEATURES.

**DEMOLITION NOTES:**

- UNLESS OTHERWISE NOTED, EXISTING UTILITIES AND STRUCTURES SHALL REMAIN AND BE PROTECTED TO REMAIN.
- THE CONTRACTOR SHALL PROTECT ALL ADJACENT IMPROVEMENTS (BUILDINGS, ROADWAYS, FENCES, PARKING LOTS, UTILITIES, ETC.) FROM DAMAGE AND EROSION. ALL DISTURBED AREAS OUTSIDE OF THE LIMITS OF WORK SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.

**HORIZONTAL CONTROL NOTES:**

- BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.
- THE CONTRACTOR SHALL VERIFY THAT BUILDING GRID LOCATIONS ARE CONSISTENT WITH LATEST ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.

**PAVING NOTES:**

- THE CONTRACTOR SHALL ADJUST EXISTING AND PROPOSED MANHOLES, WATER VALVES AND OTHER SURFACE APPURTENANCES WITHIN THE LIMITS OF WORK TO FINISHED GRADE.
- THE CONTRACTOR SHALL REFER TO ARCHITECTURAL/LANDSCAPE PLANS FOR CONCRETE SCORING PATTERNS, COLOR AND FINISHING.
- PROPOSED PAVEMENT DESIGN IS BASED UPON THE GEOTECHNICAL REPORT BY KUMAR & ASSOCIATES DATED 01/17/2023. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF PAVEMENT PERFORMANCE.

**UTILITY NOTES:**

- LOCATION OF ALL EXISTING UTILITIES (PRIVATE AND PUBLIC) SHALL BE IDENTIFIED AND VERIFIED BY THE CONTRACTOR PRIOR TO MOBILIZATION. CONSTRUCTION AND ORDERING OF MATERIALS. IF ANY CONFLICTS OR DISCREPANCIES ARE FOUND THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY. THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT, REPAIR AND DELAYS RELATED TO ANY AND ALL UNVERIFIED EXISTING CONDITIONS.
- ALL UTILITY LENGTHS AND QUANTITIES SHOWN ON THESE PLANS ARE FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND QUANTITIES PRIOR TO ORDERING OF ANY MATERIALS.
- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
- WHERE FLARED END SECTIONS ARE REQUIRED, THE LENGTH OF PIPE INCLUDES THE FLARED END SECTION.
- ALL UTILITY WORK SHALL BE COMPLETED PRIOR TO ANY PAVING.
- CONTRACTOR SHALL SURVEY THE LOCATION AND ELEVATION OF ALL INSTALLED UTILITIES PRIOR TO TRENCH BACKFILL, AND PROVIDE SURVEY INFORMATION TO THE ENGINEER UPON COMPLETION OF THE PROJECT FOR AS-BUILT DOCUMENTATION.

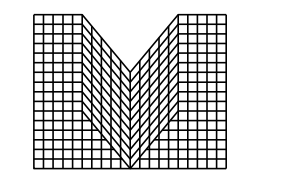
**GRADING NOTES:**

- SITE GRADING (EXCAVATION, EMBANKMENTS, AND COMPACTION) SHALL CONFORM TO THE RECOMMENDATIONS OF THE LATEST GEOTECHNICAL REPORT FOR THIS PROPERTY.
- SPOT ELEVATIONS MAY BE ABBREVIATED FOR CLARITY. REFER TO THE ADJACENT CONTOUR LABELS FOR FULL ELEVATIONS.
- MAXIMUM PROPOSED SLOPES SHALL BE 4:1 UNLESS NOTED OTHERWISE.
- SPOT ELEVATIONS ARE TO FLOWLINE UNLESS NOTED OTHERWISE.
- PROPOSED GRADING SHALL MEET AND MATCH EXISTING GRADE AT LIMITS OF DISTURBANCE.
- PROPOSED GRADING AS SHOWN REFLECTS PLANNED FINISHED GRADE. THE CONTRACTOR SHALL ACCOUNT FOR PAVEMENT DEPTH, SUBGRADE REQUIREMENTS, OVEREXCAVATION, TOPSOIL, AND ANY PLANNED SURFACING CONSTRUCTION.
- MAXIMUM TEMPORARY SLOPES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEER'S RECOMMENDATIONS AND IN COMPLIANCE WITH OSHA REQUIREMENTS.
- THE CONTRACTOR SHALL CONFORM TO OVEREXCAVATION REQUIREMENTS DETAILED WITHIN THE PROJECT SPECIFICATIONS AND GEOTECHNICAL REPORT. OVEREXCAVATION REQUIREMENTS ARE NOT DEPICTED ON THE CIVIL PLANS.

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Town of Parker, Director of Engineering/Public Works Date



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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

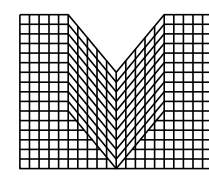
NO.	DESCRIPTION
1	
2	
3	

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138  
DRAWING TITLE: CIVIL NOTES AND LEGEND  
FILE PATH: C:\Users\ghDACC\Documents\A. Miros\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C000-COVR.dwg C001 - 4/10/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-001**

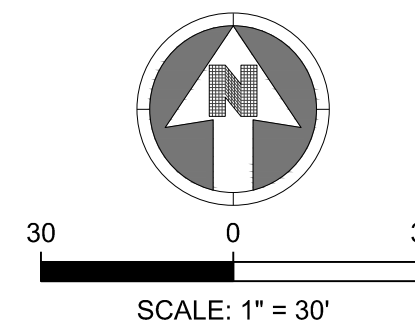


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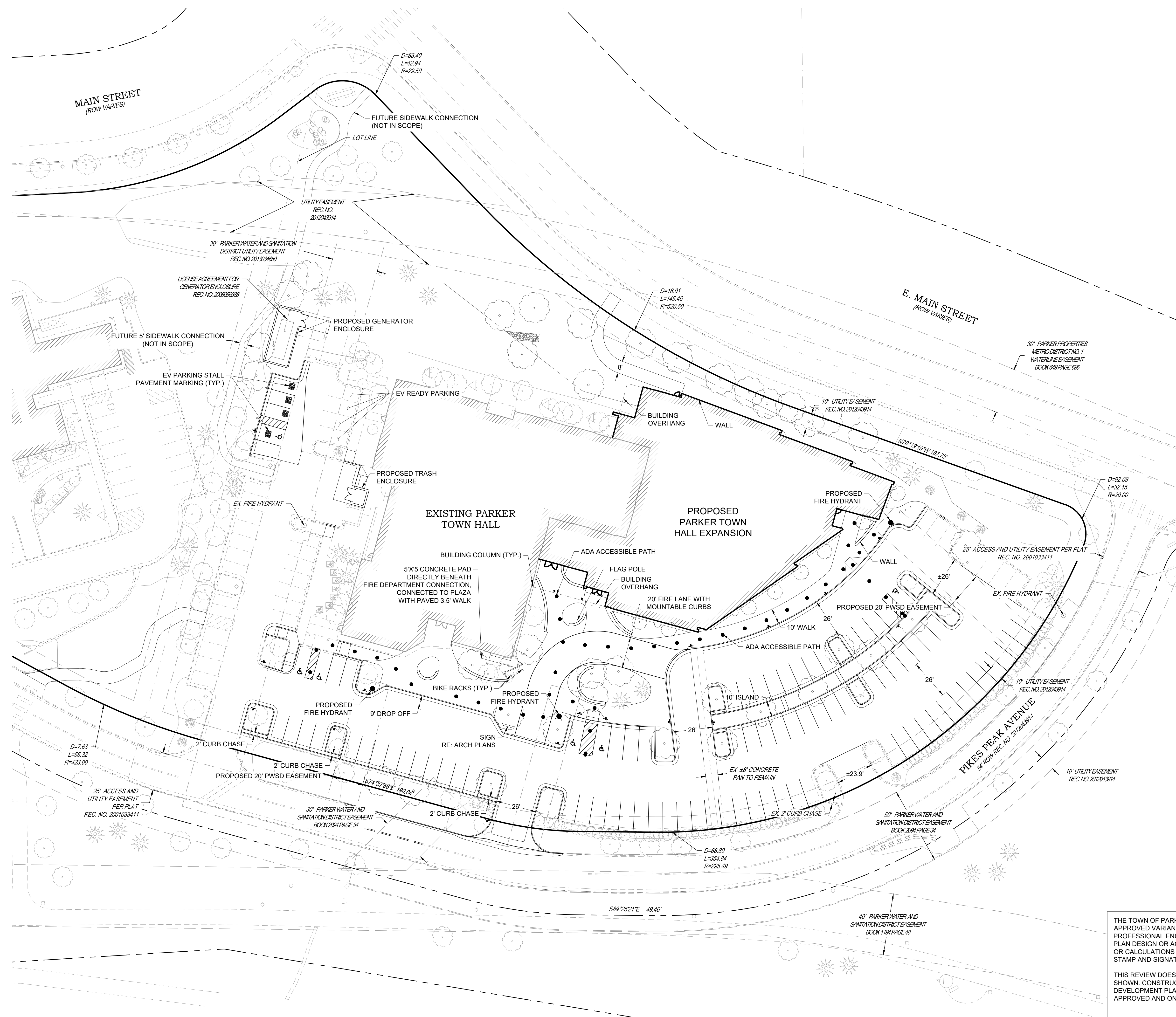


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**NOTES:**

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- ADD ALTERNATE 1 - SEAL COAT EXISTING ASPHALT AND RESTRIPE PARKING LOT.
- ADD ALTERNATE 2 - ADD ADDITIONAL PEDESTRIAN CROSSING INCLUDING PUSH BUTTON SIGNAL AND RAISED CROSSWALK ON PIKES PEAK AVENUE.
- A SITE INSPECTION IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION.
- NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THE CURB GUTTER AND FIRST LIFT OF ASPHALT ARE INSTALLED



NO.	DATE	DESCRIPTION
1	11/03/2023	FIRST TOWN SUBMITTAL
2	02/05/2024	SECOND TOWN SUBMITTAL
3	04/04/2024	THIRD TOWN SUBMITTAL

**PROJECT:** PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

**DRAWING TITLE:** SITE PLAN

FILE PATH: C:\Users\jgh\OneDrive\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C021-SITE-PLAN.dwg C021 - 4/10/2024

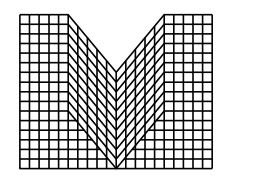


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-021**

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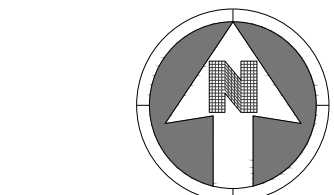


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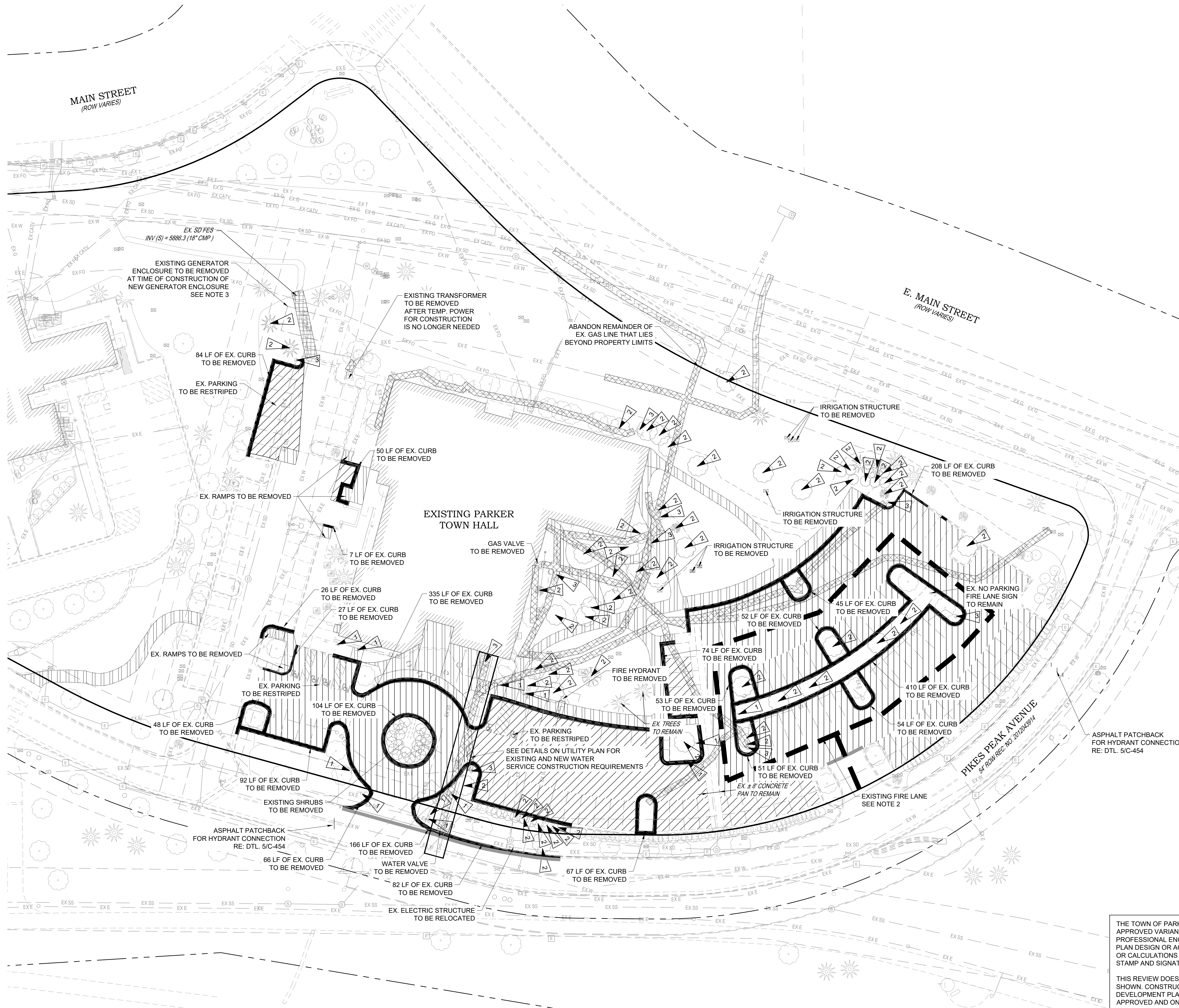
30 0 30  
SCALE: 1" = 30'

NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- A SITE INSPECTION BY A REPRESENTATIVE OF THE SOUTH METRO FIRE DEPARTMENT IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION. NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THAT CURB, GUTTER, AND FIRST LIFT OF ASPHALT ARE INSTALLED FOR THE FULL LENGTH OF THE FIRE LANE.
- THE EXISTING GENERATOR SHALL REMAIN IN PLACE AS LONG AS POSSIBLE UNTIL REMOVAL IS NECESSARY TO CONSTRUCT THE NEW GENERATOR ENCLOSURE. AT THIS TIME, THE CONTRACTOR SHALL LOAD, TRANSPORT, AND OFFLOAD THE EXISTING GENERATOR TO TOWN OF PARKER PUBLIC WORKS OPERATIONS CENTER (PWOC) LOCATED AT 9045 TAMMY LANE, PARKER, CO 80134. COORDINATE DELIVERY WITH ADRAIN ORZESCU 303.434.8279.

LEGEND:

- UTILITY TO BE REMOVED
- CURB AND GUTTER REMOVAL
- CONCRETE SIDEWALK REMOVAL
- ASPHALT PAVEMENT REMOVAL SEE NOTE 2
- UTILITY TO BE ABANDONED
- SAWCUT LINE
- REMOVE/ RELOCATE SIGN
- REMOVE/ RELOCATE TREE
- REMOVE/ RELOCATE LIGHT POLE



DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

NO.	DESCRIPTION
1	
2	
3	

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138

DRAWING TITLE: DEMOLITION PLAN

FILE PATH: C:\Users\jnh\Documents\S.A. Miro, Inc.\Civil\libs\22139 Parker Town Hall\05 CAD\Plans and Details\C111-DEMOL-PLAN.dwg C111 - 4/10/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER: C-111

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY OR CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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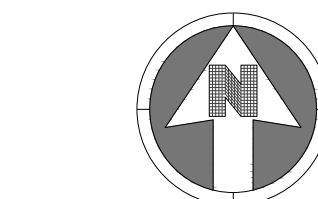


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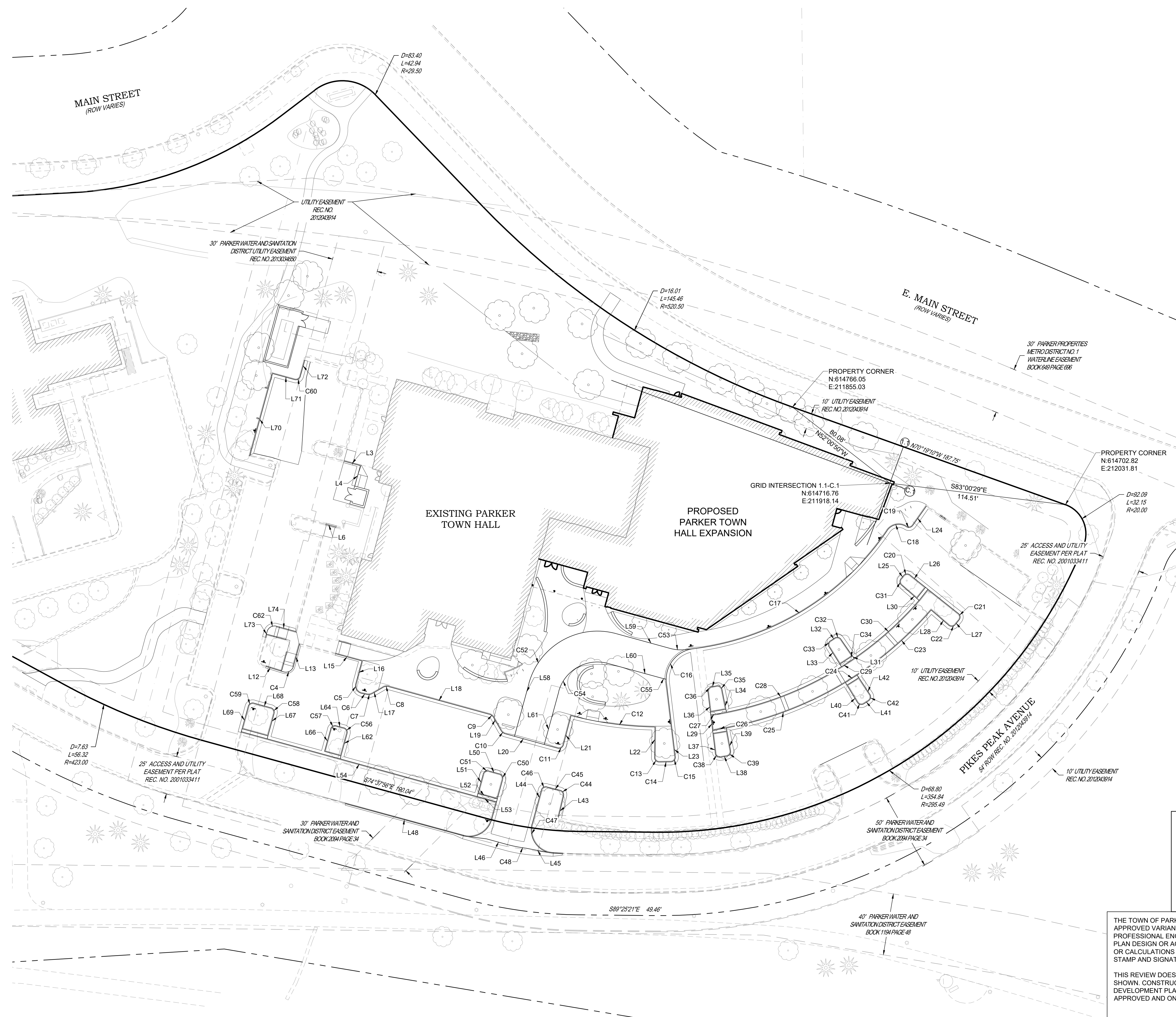


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SCALE: 1" = 30'

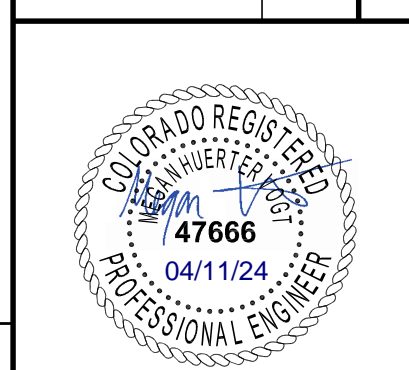
NOTES:

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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

NO.	PROJECT	DRAWING TITLE:
1	PARKER TOWN HALL EXPANSION 20120 MAIN STREET PARKER, CO 80138	HORIZONTAL CONTROL PLAN
2		
3		



DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139
DRAWING NUMBER:	C-121

**BASIS OF BEARING:**  
BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.

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FILE PATH: C:\Users\jgh\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C121-HORZ-PLAN.dwg C121 - 4/10/2024

CURVE TABLE						
SEGMENT	LENGTH	RADIUS	CHORD DIRECTION	CHORD LENGTH	START N.E	END N.E
C1	7.82'	5.00'	S60°12'50"W	7.05'	614623.91, 211520.91	614620.41, 211514.79
C2	1.56'	1.00'	S60°12'50"W	1.41'	614618.76, 211515.36	614618.06, 211514.14
C3	1.60'	1.00'	N61°19'20"E	1.44'	614595.30, 211523.22	614595.99, 211524.48
C4	8.00'	5.00'	N61°19'20"E	7.18'	614594.59, 211525.54	614598.03, 211531.83
C5	8.11'	5.00'	S31°22'20"E	7.25'	614586.87, 211572.28	614580.68, 211576.05
C6	3.54'	250.00'	S78°13'50"E	3.54'	614580.68, 211576.05	614579.96, 211579.51
C7	3.58'	5.00'	N80°50'10"E	3.51'	614579.96, 211579.51	614580.52, 211582.98
C8	3.93'	5.00'	S82°50'20"W	3.83'	614584.97, 211593.75	614584.49, 211589.95
C9	3.92'	5.00'	N52°09'40"W	3.82'	614564.79, 211661.66	614567.14, 211658.64
C10	3.92'	5.00'	S52°09'40"E	3.82'	614556.75, 211666.24	614554.41, 211669.26
C11	4.75'	3.00'	N60°01'00"E	4.27'	614545.13, 211703.02	614547.26, 211706.71
C12	55.00'	227.36'	S82°16'00"E	54.87'	614566.23, 211711.68	614558.84, 211766.05
C13	4.75'	3.00'	S44°32'40"E	4.27'	614539.23, 211765.77	614536.18, 211768.77
C14	7.54'	250.00'	N89°14'40"E	7.54'	614536.18, 211768.77	614536.28, 211776.30
C15	4.99'	3.00'	N40°45'40"E	4.43'	614536.28, 211776.30	614539.64, 211779.20
C16	31.37'	20.00'	S38°04'40"W	28.25'	614609.51, 211790.89	614587.27, 211773.47
C17	157.40'	178.00'	N59°32'30"E	152.32'	614608.90, 211785.14	614686.11, 211916.44
C18	13.70'	9.00'	S77°49'50"W	12.42'	614688.72, 211928.58	614686.11, 211916.44
C19	4.52'	3.00'	N78°18'30"E	4.10'	614688.52, 211928.91	614689.35, 211932.92
C20	4.67'	3.00'	S83°55'40"W	4.21'	614657.30, 211931.19	614656.85, 211927.00
C21	4.75'	3.00'	N6°08'20"W	4.27'	614628.14, 211962.95	614632.39, 211962.49
C22	4.75'	3.00'	N85°26'50"E	4.27'	614624.81, 211956.21	614625.15, 211960.47
C23	64.00'	232.00'	N48°42'00"E	63.80'	614592.49, 211896.94	614634.60, 211944.87
C24	9.00'	232.00'	N57°42'50"E	9.00'	614587.69, 211889.33	614592.49, 211896.94
C25	83.00'	232.00'	N69°04'20"E	82.56'	614558.20, 211812.22	614587.69, 211889.33
C26	9.01'	232.00'	N80°26'10"E	9.01'	614556.70, 211803.33	614558.20, 211812.22
C27	10.36'	222.00'	N80°08'30"E	10.36'	614566.63, 211802.14	614568.41, 211812.34
C28	80.53'	222.00'	N68°24'40"E	80.09'	614568.41, 211812.34	614597.88, 211886.81
C29	9.79'	222.00'	N56°45'20"E	9.79'	614597.88, 211886.81	614603.24, 211895.00
C30	56.59'	222.00'	N48°11'20"E	56.44'	614603.24, 211895.00	614640.87, 211937.07
C31	4.67'	3.00'	S4°31'40"E	4.21'	614654.90, 211925.38	614650.70, 211925.71
C32	4.67'	3.00'	N79°05'30"W	4.21'	614615.62, 211886.49	614616.42, 211882.36
C33	4.67'	3.00'	S12°36'20"W	4.21'	614614.73, 211879.78	614610.62, 211878.86
C34	9.71'	220.00'	N56°45'20"E	9.71'	614599.57, 211885.75	614604.89, 211893.87
C35	4.67'	3.00'	N55°46'50"W	4.21'	614583.14, 211809.42	614585.51, 211805.94
C36	4.61'	3.00'	S37°07'20"W	4.17'	614584.97, 211802.85	614581.65, 211800.33
C37	10.21'	220.00'	N80°08'00"E	10.21'	614568.62, 211801.90	614570.37, 211811.95
C38	4.88'	3.00'	S53°25'50"E	4.36'	614541.90, 211805.11	614539.30, 211808.61
C39	4.75'	3.00'	N34°40'10"E	4.27'	614539.97, 211812.57	614543.48, 211815.00
C40	9.15'	234.00'	N80°26'30"E	9.15'	614554.72, 211803.57	614556.23, 211812.59
C41	4.75'	3.00'	S76°31'30"E	4.27'	614574.87, 211897.09	614573.87, 211901.24
C42	4.75'	3.00'	N11°57'00"E	4.27'	614575.81, 211904.31	614579.99, 211905.19
C43	9.08'	234.00'	N57°42'50"E	9.08'	614585.98, 211890.37	614590.83, 211898.04
C44	4.70'	3.00'	N31°34'20"W	4.23'	614514.24, 211706.84	614517.85, 211704.63
C45	8.76'	276.00'	S75°33'10"E	8.76'	614520.04, 211696.14	614517.85, 211704.63
C46	4.72'	3.00'	S60°20'00"W	4.24'	614520.04, 211696.14	614517.94, 211692.45
C47	26.79'	15.00'	S35°51'00"E	23.37'	614496.08, 211686.47	614477.14, 211700.16
C48	12.97'	98.00'	S78°34'10"E	12.96'	614482.98, 211669.48	614480.41, 211682.18
C50	4.71'	3.00'	N29°38'00"W	4.24'	614524.83, 211667.38	614528.52, 211665.29
C51	4.71'	3.00'	S60°22'00"W	4.24'	614530.39, 211658.48	614528.29, 211654.79
C52	72.25'	46.00'	S60°18'10"W	65.05'	614619.29, 211740.92	614587.07, 211684.42
C53	9.27'	26.00'	S84°54'50"E	9.22'	614609.71, 211775.96	614608.90, 211785.14
C54	40.84'	26.00'	S60°18'10"W	36.77'	614600.00, 211735.65	614581.79, 211703.71
C55	2.72'	46.00'	S76°24'00"E	2.72'	614590.42, 211770.68	614589.78, 211773.33

LINE TABLE				
SEGMENT	LENGTH	DIRECTION	STARTING N.E	ENDING N.E
L1	13.62'	S14°00'39"W	614796.36, 211539.22	614783.15, 211535.92
L2	18.21'	N75°59'21"W	614783.15, 211535.92	614787.55, 211518.26
L3	11.07'	S74°42'16"E	614731.06, 211564.58	614728.14, 211575.27
L4	13.72'	N15°37'16"E	614714.92, 211571.57	614728.14, 211575.27
L5	6.93'	S74°42'16"E	614715.40, 211571.71	614713.58, 211578.38
L6	6.83'	N74°49'40"W	614688.37, 211559.51	614690.16, 211552.92
L7	8.00'	N74°58'26"W	614621.84, 211528.63	614623.91, 211520.91
L8	1.45'	S15°24'02"W	614620.41, 211514.79	614619.01, 211514.41
L9	2.15'	S15°24'02"W	614619.01, 211514.41	614616.94, 211513.84
L10	12.02'	N74°58'26"W	614615.64, 211526.97	614618.76, 211515.36
L11	11.93'	S15°28'28"W	614607.48, 211527.67	614595.99, 211524.48
L12	4.24'	N72°49'40"W	614594.59, 211525.54	614595.84, 211521.48
L13	8.00'	S15°28'28"W	614605.74, 211533.97	614598.03, 211531.83
L14	8.00'	N74°46'21"W	614612.38, 211569.11	614614.48, 211561.91
L15	17.72'	N74°55'09"W	614601.68, 211576.27	614606.29, 211559.16
L16	15.34'	S15°04'51"W	614601.68, 211576.27	614586.87, 211572.28
L17	8.03'	N60°18'26"E	614580.52, 211582.98	614584.49, 211589.95
L18	67.29'	S74°37'56"E	614584.97, 211593.75	614567.14, 211658.64
L19	9.25'	S29°41'34"E	614564.79, 211661.66	614556.75, 211666.24
L20	35.01'	N74°37'56"W	614545.13, 211703.02	614554.41, 211669.26
L21	19.60'	S14°39'52"W	614566.23, 211711.68	614547.26, 211706.71
L22	19.62'	N0°48'13"E	614539.23, 211765.77	614558.84, 211766.05
L23	47.97'	N6°51'27"W	614539.64, 211779.20	614587.27, 211773.47
L24	15.44'	N35°09'56"E	614689.35, 211932.92	614701.97, 211941.81
L25	2.54'	N39°42'02"E	614654.90, 211925.38	614656.85, 211927.00
L26	40.00'	N51°29'12"W	614632.39, 211962.49	614657.30, 211931.19
L27	3.89'	N39°39'17"E	614625.15, 211960.47	614628.14, 211962.95
L28	14.98'	S49°12'15"E	614634.60, 211944.87	614624.81, 211956.21
L29	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L30	15.02'	S49°06'45"E	614650.70, 211925.71	614640.87, 211937.07
L31	15.02'	N34°30'28"W	614603.24, 211895.00	614615.62, 211886.49
L32	3.09'	N56°45'22"E	614614.73, 211879.78	614616.42, 211882.36
L33	15.02'	S31°58'48"E	614610.62, 211878.86	614597.88, 211886.81
L34	15.02'	N11°11'46"W	614568.41, 211812.34	614583.14, 211809.42
L35	3.14'	N80°04'12"E	614584.97, 211802.85	614585.51, 211805.94
L36	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L37	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L38	4.01'	N80°29'03"E	614539.30, 211808.61	614539.97, 211812.57
L39	14.98'	N10°40'38"W	614543.48, 211815.00	614558.20, 211812.22
L40	14.98'	S31°10'33"E	614587.69, 211889.33	614574.87, 211897.09
L41	3.63'	N57°42'47"E	614573.87, 211901.24	614575.81, 211904.31
L42	14.98'	N33°23'54"W	614579.99, 211905.19	614592.49, 211896.94
L43	16.32'	S13°19'10"W	614514.24, 211706.84	614498.36, 211703.08
L44	22.66'	N15°18'26"E	614496.08, 211686.47	614517.94, 211692.45
L45	16.25'	S79°16'45"E	614480.41, 211682.18	614477.38, 211698.15
L46	13.07'	N74°46'42"W	614482.98, 211669.48	614486.41, 211656.87
L48	79.01'	S74°46'42"E	614509.16, 211565.62	614488.42, 211641.86
L50	7.06'	S74°37'56"E	614530.39, 211658.48	614528.52, 211665.29
L51	14.98'	N15°22'04"E	614513.84, 211650.82	614528.29, 211654.79
L52	13.06'	S74°37'56"E	614515.77, 211651.35	614512.31, 211663.94
L53	13.06'	S74°37'56"E	614513.84, 211650.82	614510.38, 211663.41
L54	92.00'	N74°37'56"W	614513.84, 211650.82	614538.23, 211562.11
L55	10.15'	N55°50'28"W	614538.23, 211562.11	614543.92, 211553.71
L56	19.51'	N15°22'04"E	614538.23, 211562.11	614557.03, 211567.28
L57	9.47'	N55°50'28"W	614540.26, 211562.67	614545.58, 211554.84
L58	35.51'	S15°18'26"W	614587.07, 211684.42	614552.82, 211675.04
L59	36.32'	N74°42'16"W	614609.71, 211775.96	614619.29, 211740.92
L60	36.32'	S74°42'16"E	614600.00, 211735.65	614590.42, 211770.68
L61	35.53'	S15°18'26"W	614581.79, 211703.71	614547.52, 211694.33

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Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_



NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024
3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **HORIZONTAL CONTROL DATA**

FILE PATH: C:\Users\jgh\Documents\S. A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C121-HORIZ-PLAN.dwg C122 - 4/10/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

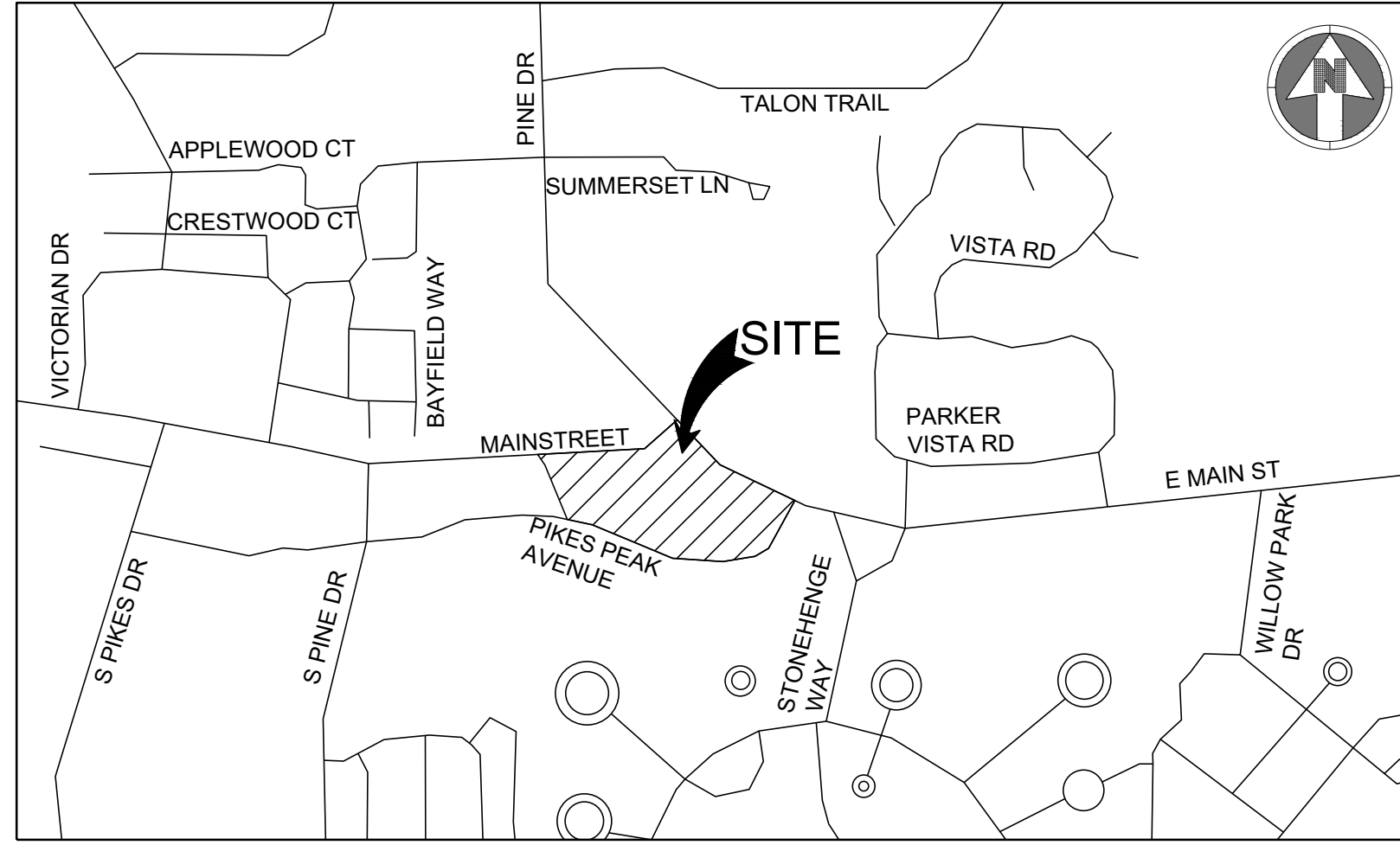
DRAWING NUMBER: **C-122**



EROSION CONTROL CONSTRUCTION PLANS  
FOR

# PARKER TOWN HALL

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED  
FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



VICINITY MAP  
NTS

Sheet Number	Sheet Title
C-240	EROSION CONTROL COVER SHEET
C-241	INITIAL EROSION CONTROL PLAN
C-242	INTERIM EROSION CONTROL PLAN
C-243	FINAL EROSION CONTROL PLAN
C-244	OFF-SITE EROSION CONTROL PLAN
C-251	EROSION CONTROL DETAILS
C-252	EROSION CONTROL DETAILS
C-253	EROSION CONTROL DETAILS
C-254	EROSION CONTROL DETAILS
C-255	EROSION CONTROL DETAILS
C-256	EROSION CONTROL DETAILS
C-257	EROSION CONTROL DETAILS
C-258	EROSION CONTROL DETAILS
C-259	EROSION CONTROL DETAILS

### ENGINEER'S STATEMENT

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.



MEGAN HUERTER VOGT  
REGISTERED PROFESSIONAL ENGINEER  
COLORADO NO. 47666

**S.A. MIRO INC.**  
CONSULTING ENGINEERS  
4582 South Ulster Street  
Suite 750, Denver, CO 80237  
303-741-3737  
www.samiro.com

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3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **EROSION CONTROL COVER SHEET**

FILE PATH:



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-240**

### CONTACTS:

**OWNER'S REPRESENTATIVE**  
TOWN OF PARKER  
20120 E. MAIN STREET  
PARKER, CO 80138  
P: (303) 840-9546

**CIVIL ENGINEER**  
S.A. MIRO INC.  
MEGAN H. VOGT  
4582 SOUTH ULSTER STREET  
SUITE 750 DENVER, CO 80237  
303-741-3737

**SURVEYOR**  
AZTEC CONSULTANTS, INC.  
DANIEL E. DAVIS  
300 EAST MINERAL AVE, SUITE 1  
LITTLETON, CO 80122  
P: (303)-713-1898

### PUBLIC UTILITIES:

**SOUTH METRO FIRE DISTRICT**  
9195 E. MINERAL AVENUE  
CENTENNIAL, CO 80112  
P: (720) 989-2244

**PARKER WATER & SANITATION DISTRICT**  
18100 WOODMAN ROAD  
PARKER, CO 80134  
P: (303) 841-4627

**TOWN OF PARKER**  
20120 E. MAIN STREET  
PARKER, CO 80138  
P: (303) 840-9546

### BASIS OF BEARING:

BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17866 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.

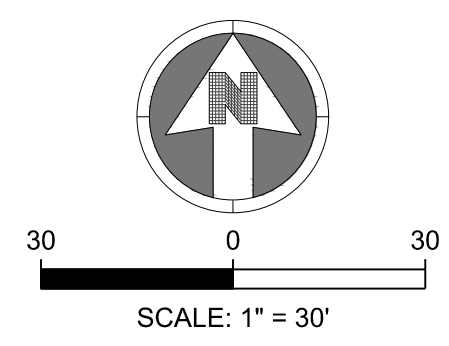
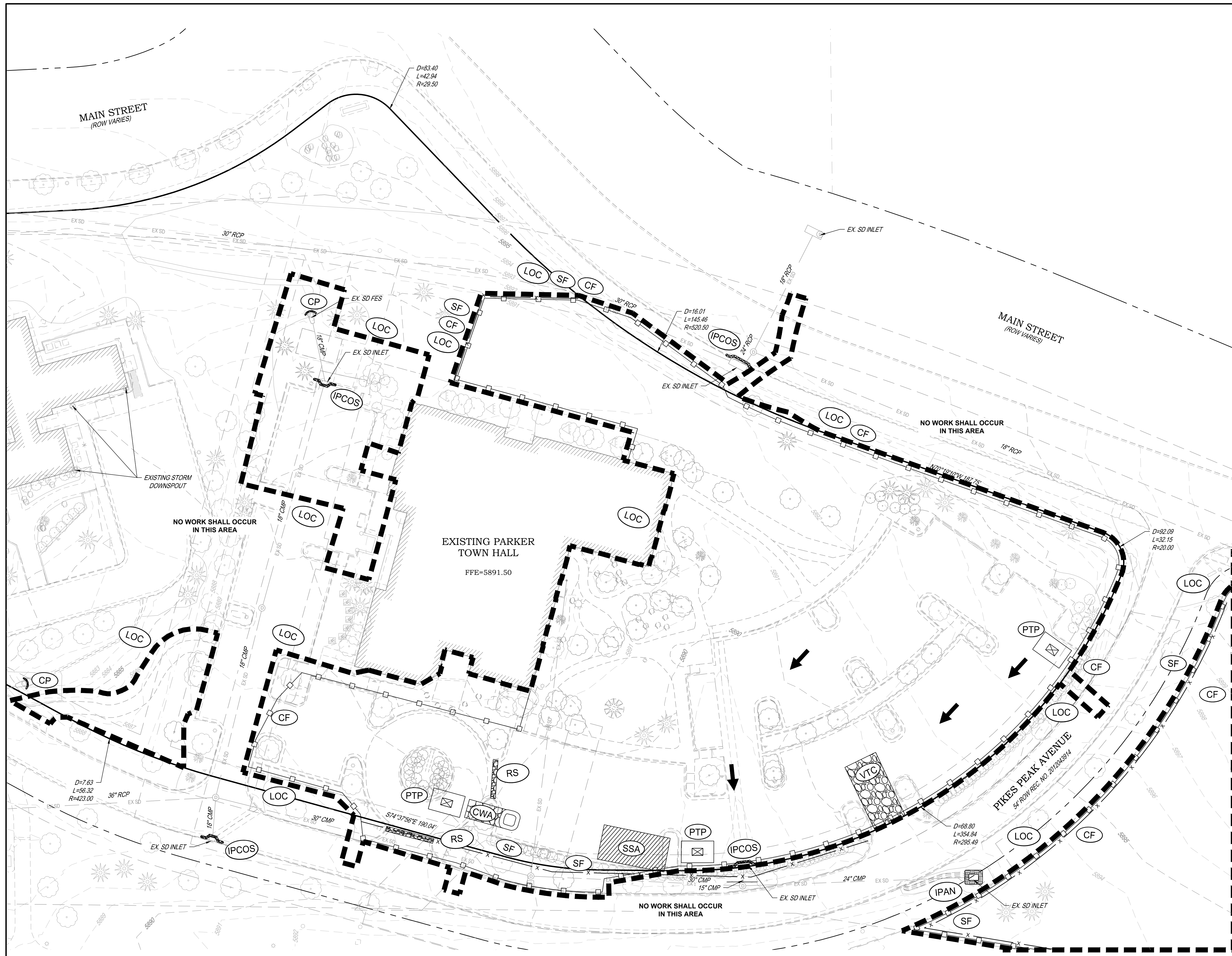
### BENCHMARK:

1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTELTON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL  
ELEVATION=5399.27 (NAVD 88).

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Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_



**NOTES:**  
 1. SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.

**LEGEND:**

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	IPCOG	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	IPCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	SEEDING, MULCHING AND CRIMPING
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION
	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL
	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW

**TOWN OF PARKER NOTES:**

- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADE AFTER TOPSOIL, SOD, PAVING ETC., EXCEPT IN THE AREA OF SOIL STOCK PILE.
- SEE TOWN OF PARKER CONSTRUCTION BEST MANAGEMENT PRACTICES.
- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ECS.
- THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
- THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
- ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
- LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
- PROVIDE SEDIMENT CONTROL LOGS (SCL) ALONG BACK OF ALL CURBS ADJACENT TO LANDSCAPE/PERVIOUS AREAS INCLUDING LANDSCAPE ISLANDS AND ALL ATTACHED PEDESTRIAN WALKS.
- ANY ONSITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION. CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.
- MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
- THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND THE ECS.

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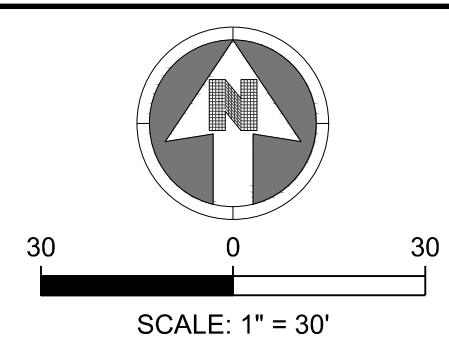
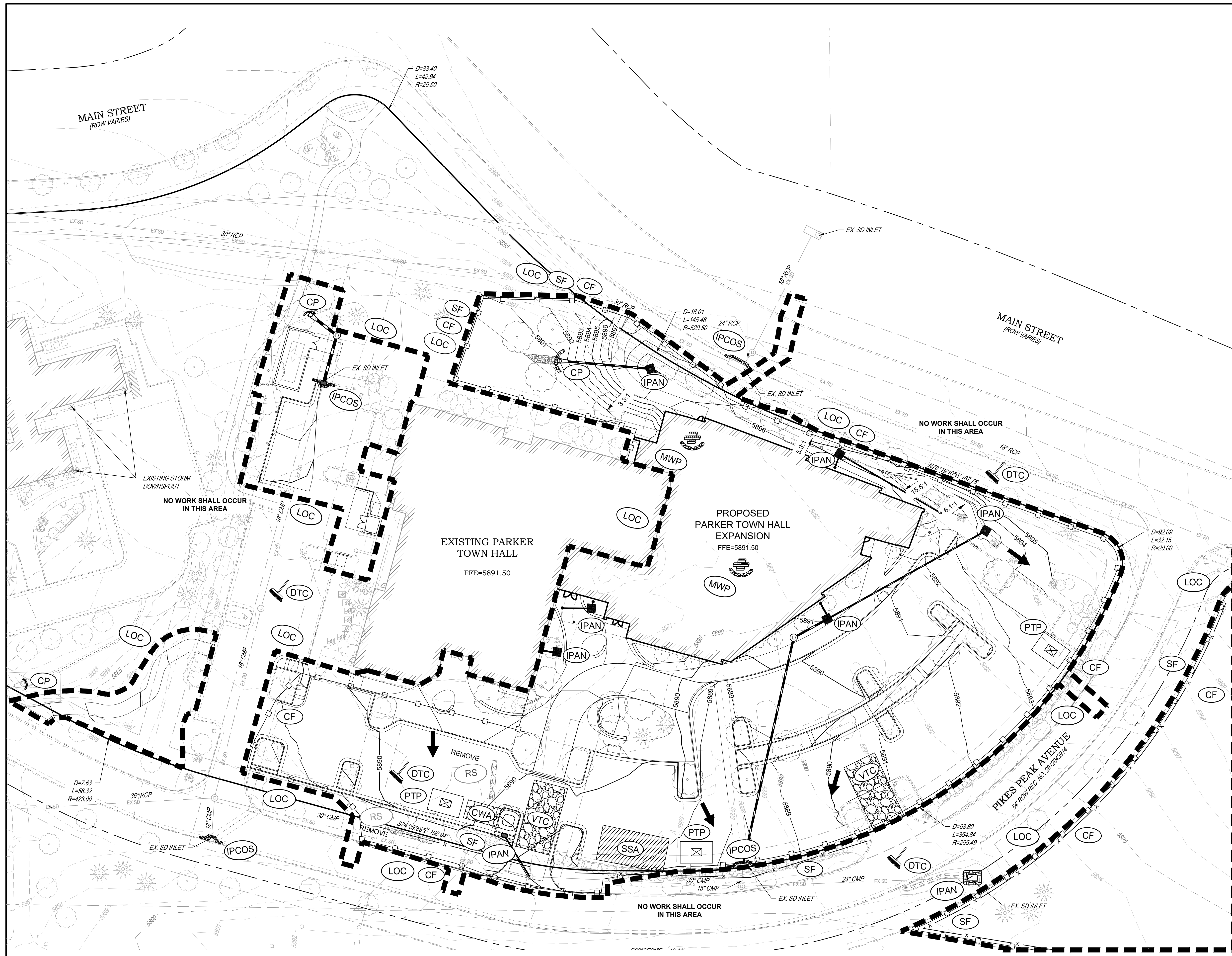
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11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

NO.	PROJECT	DRAWING TITLE	FILE PATH
1	PARKER TOWN HALL EXPANSION 20120 MAIN STREET PARKER, CO 80138	INITIAL EROSION CONTROL PLAN	
2			
3			

DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139

DRAWING NUMBER:  
**C-241**



**NOTES:**

- SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.
- ADDITIONAL PTP'S MAY BE NEEDED IN THE INTERIM AND FINAL PHASE.
- EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL SLOPES STEEPER THAN 4:1.

**LEGEND:**

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	PCOC	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	PCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
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	STP	SIDEWALK TRANSITION PROTECTION
	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL
	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW

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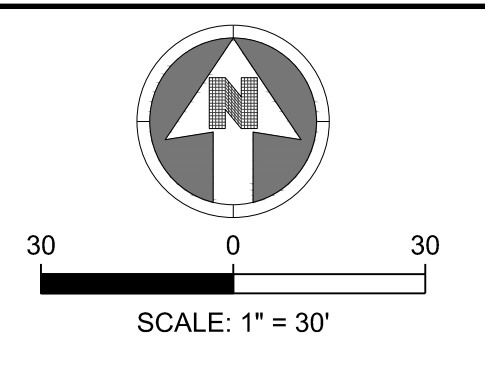
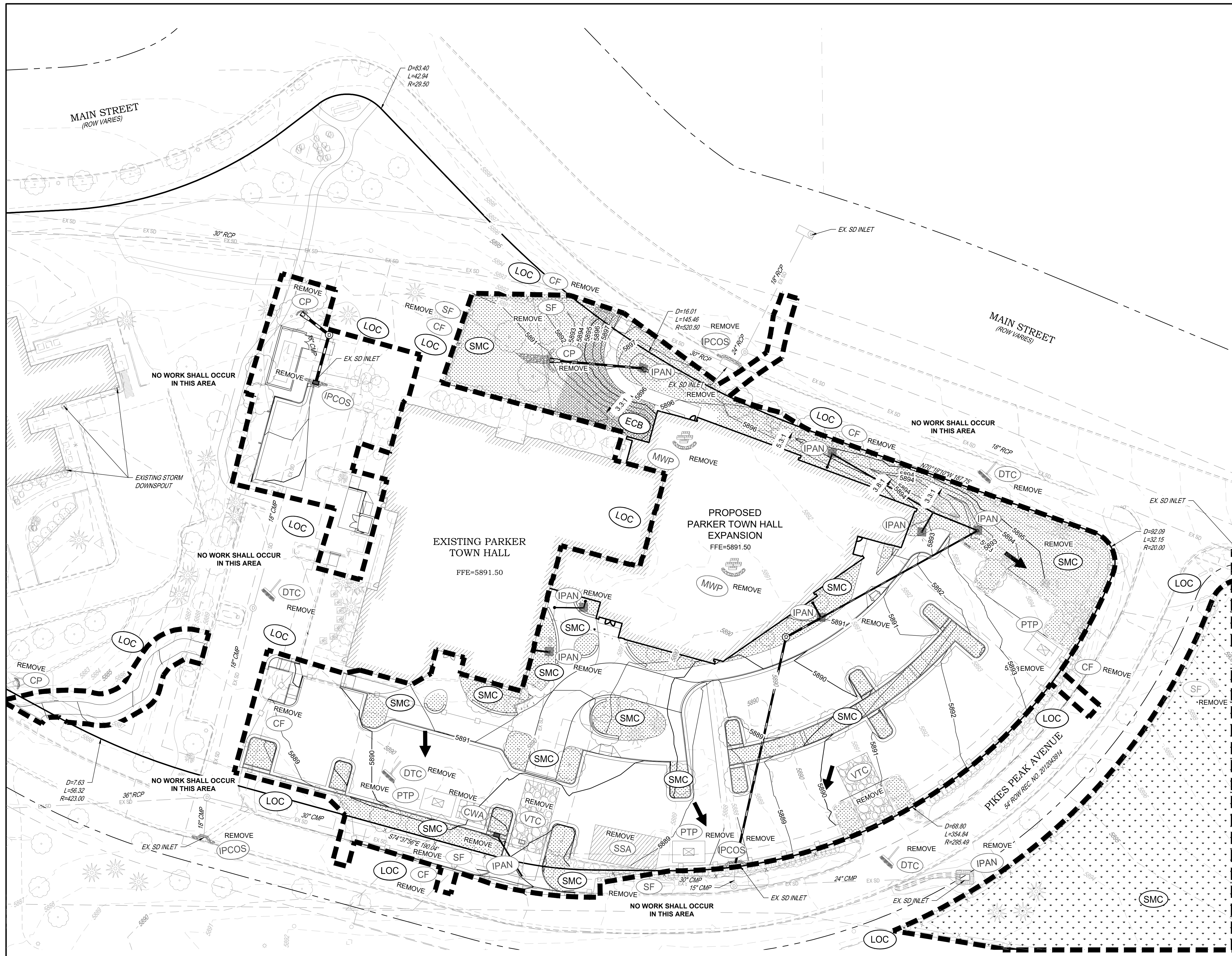
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NO.	PROJECT	DRAWING TITLE	FILE PATH
1	PARKER TOWN HALL EXPANSION 20120 MAIN STREET PARKER, CO 80138	INTERIM EROSION CONTROL PLAN	
2			
3			

DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139

DRAWING NUMBER:

**C-242**



- NOTES:**
- SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.
  - ADDITIONAL PTP'S MAY BE NEEDED IN THE INTERIM AND FINAL PHASE.
  - EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL SLOPES STEEPER THAN 4:1.

- LEGEND:**
- CD CHECK DAM
  - CF CONSTRUCTION FENCE
  - CP CULVERT PROTECTION
  - CWA CONCRETE WASHOUT AREA
  - D DEWATERING
  - DD DIVERSION DITCH
  - DP DETENTION POND PROTECTION
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  - PCOS INLET PROTECTION, CURB ON SUMP, TYPE R INLET
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  - PTP PORTABLE TOILET PROTECTION
  - RCSC ROUGH CUT STREET CONTROL
  - RS ROCK SOCK
  - RSS ROCK SOCK IN SWALE
  - SB STRAW BALE
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  - TI TEMPORARY IRRIGATION
  - TSB TEMPORARY SEDIMENT BASIN
  - VTC VEHICLE TRACKING CONTROL
  - LOC LIMITS OF CONSTRUCTION
  - FLOW DIRECTION ARROW

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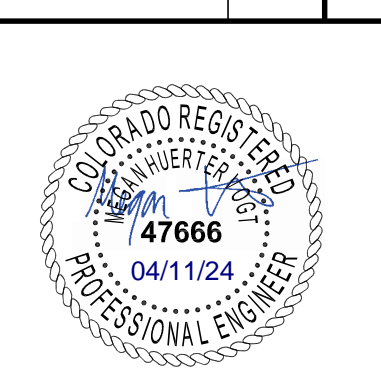
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PROJECT: **PARKER TOWN HALL EXPANSION  
 20120 MAIN STREET  
 PARKER, CO 80138**

DRAWING TITLE: **FINAL EROSION CONTROL PLAN**

FILE PATH:



DESIGNED BY: RJH  
 DRAWN BY: DEI  
 CHECKED BY: MHV  
 MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-243**

**TOWN OF PARKER NOTES:**

- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADE AFTER TOPSOIL, SOD, PAVING ETC., EXCEPT IN THE AREA OF SOIL STOCK PILE.
- SEE TOWN OF PARKER CONSTRUCTION BEST MANAGEMENT PRACTICES.
- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
- THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
- ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
- LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
- PROVIDE SEDIMENT CONTROL LOGS (SCL) ALONG BACK OF ALL CURBS ADJACENT TO LANDSCAPE/PERVIOUS AREAS INCLUDING LANDSCAPE ISLANDS AND ALL ATTACHED PEDESTRIAN WALKS.
- ANY ONSITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION. CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.
- MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
- THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND THE ECS.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works Date



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02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

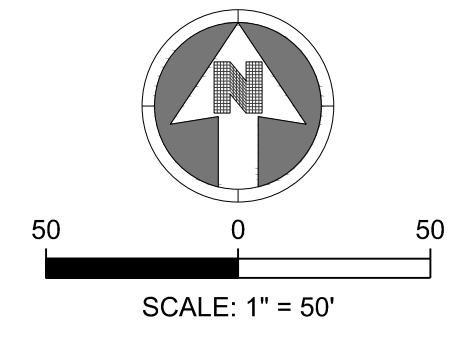
NO.	DESCRIPTION
1	
2	
3	



DESIGNED BY:	RJH
DRAWN BY:	DEI
CHECKED BY:	MHV
MIRO JOB NO.	22139

DRAWING NUMBER:

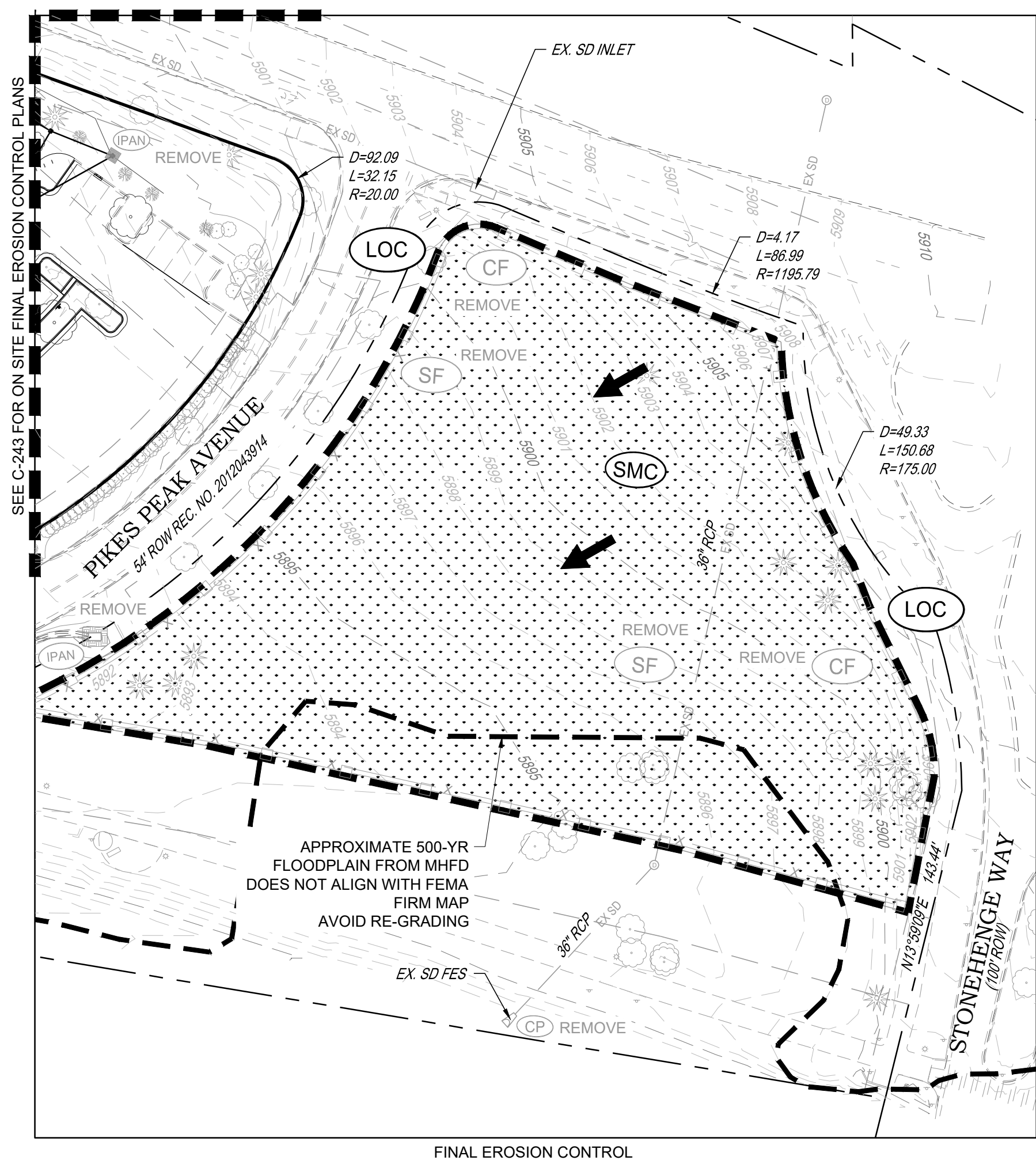
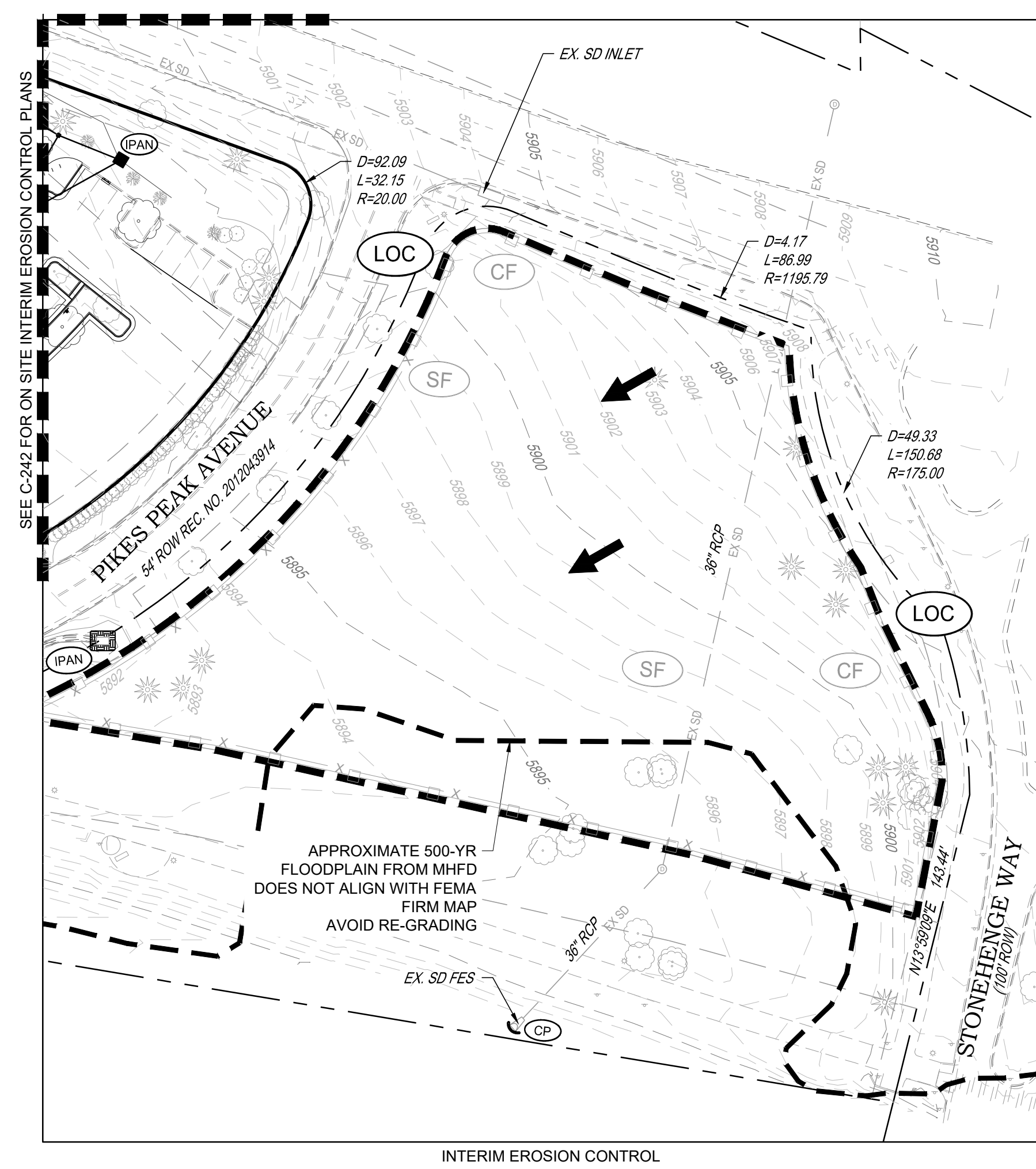
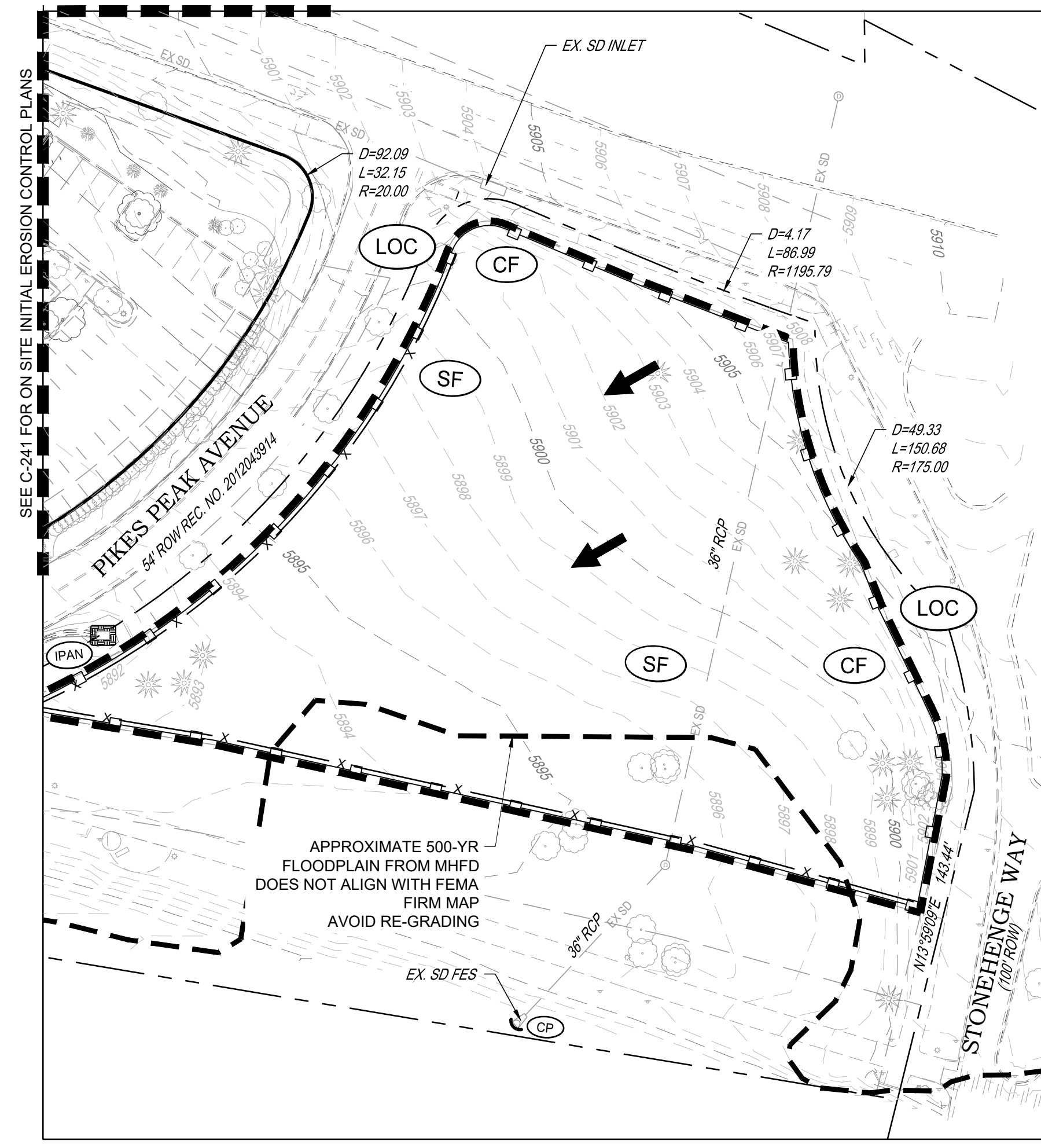
**C-244**



NOTES:  
1. SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.

LEGEND:

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	PCOG	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	PCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	SEEDING, MULCHING AND CRIMPING
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION
	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL
	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW



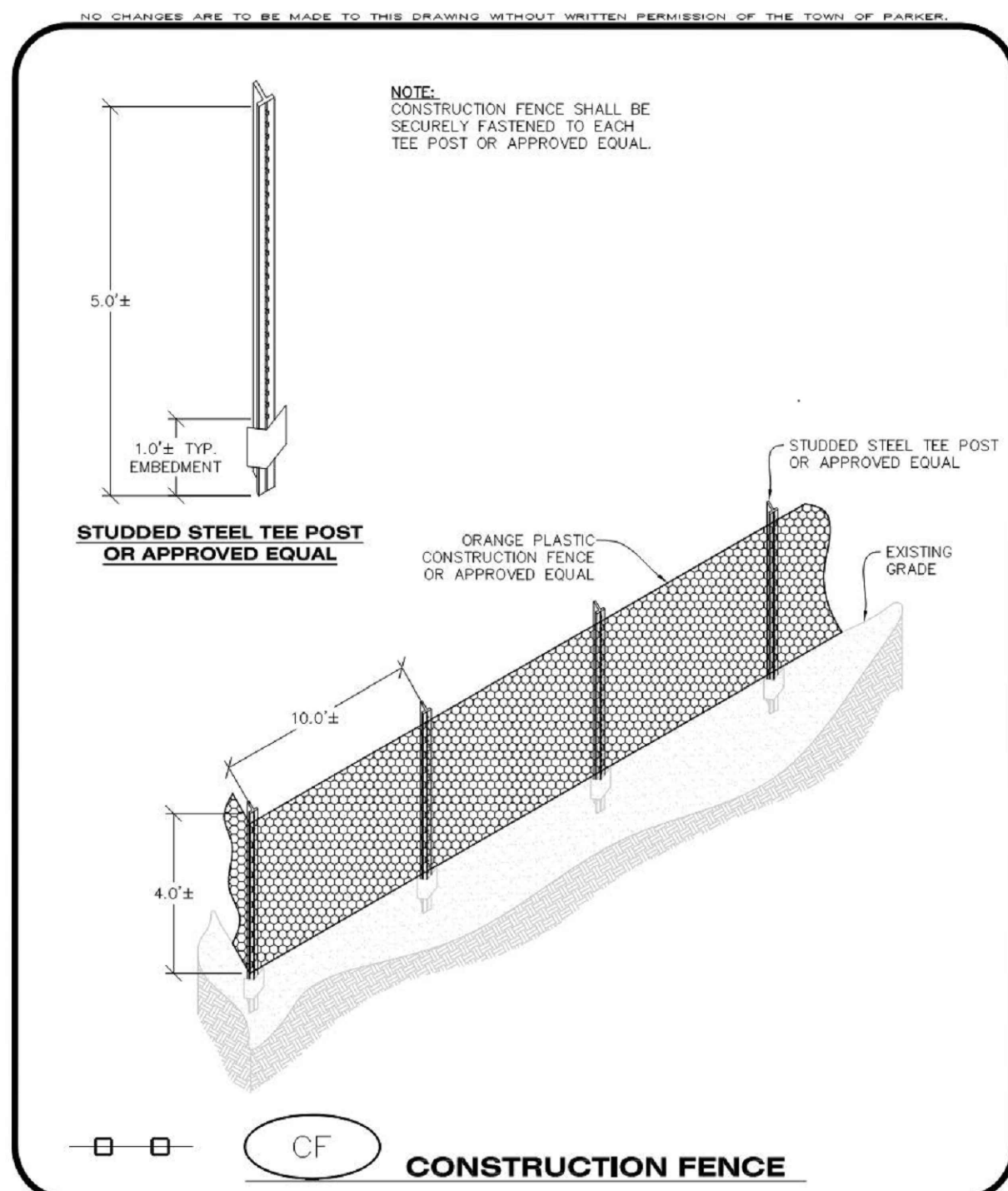
TOWN OF PARKER NOTES:

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- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ECS.
- THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
- THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
- ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
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**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CF** | 1 OF 2 | Oct. 2013

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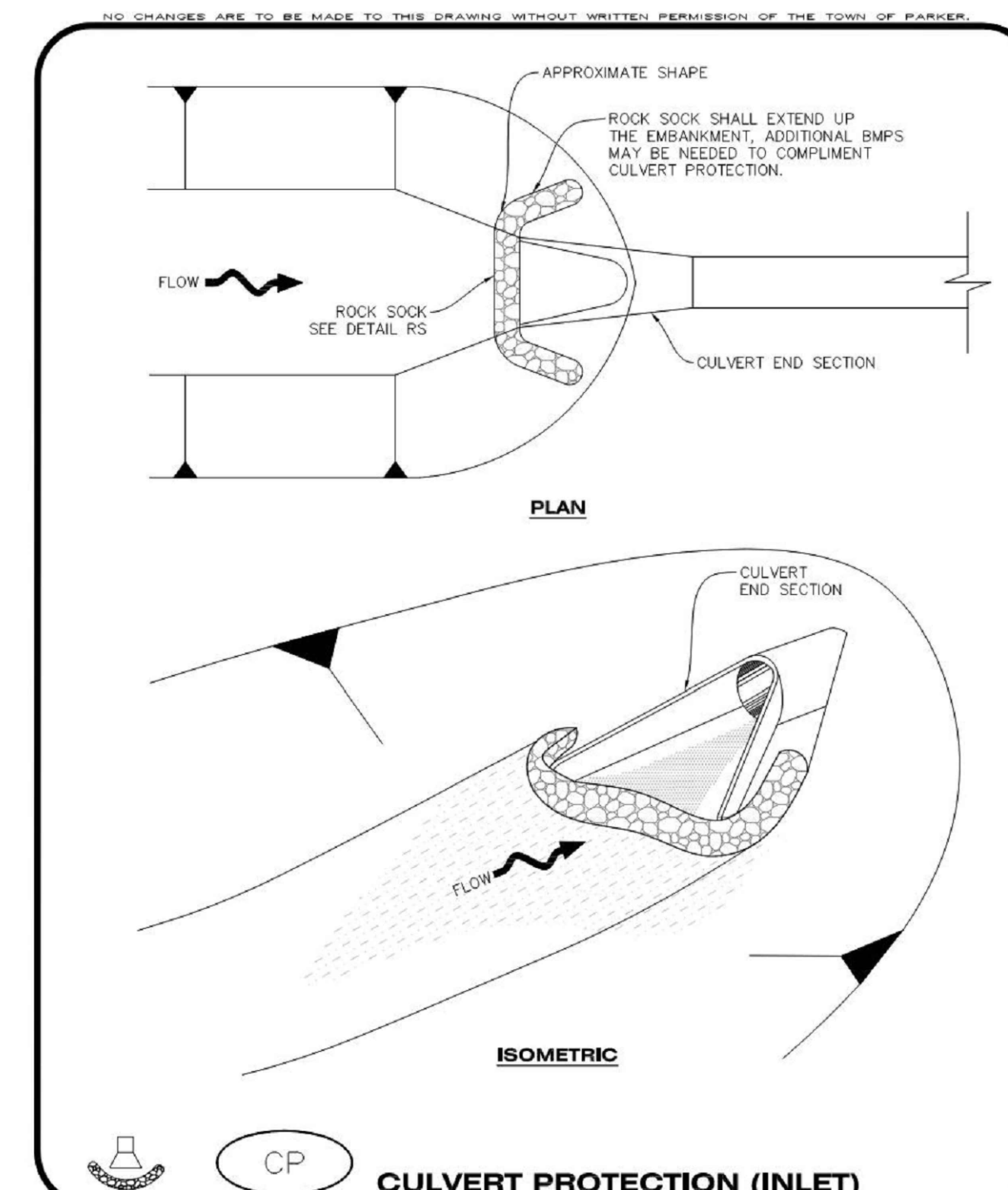
**CONSTRUCTION FENCE INSTALLATION NOTES**

1. THE CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO EACH POST OR APPROVED EQUAL.

**CONSTRUCTION FENCE INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONSTRUCTION FENCE AND MAKE ANY NECESSARY REPAIRS.
2. CONSTRUCTION FENCE SHALL BE REPAIRED WHEN THE FENCING MATERIAL FALLS OUT OF COMPLIANCE WITH THE NOTES AND DETAILS.

**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CF** | 2 OF 2 | Oct. 2013



**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CP** | 1 OF 2 | Oct. 2013

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**CULVERT PROTECTION (INLET) INSTALLATION NOTES**

1. SEE PLAN VIEW FOR LOCATION(S) OF CULVERT PROTECTION (INLET).
2. ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
3. ROCK SOCK SHALL BE APPROXIMATELY 12" IN DIAMETER.
4. ROCK SOCK SHALL EXTEND ABOVE THE FLOW LINE ELEVATION ON BOTH SIDES OF THE CULVERT END SECTION.

**CULVERT PROTECTION (INLET) INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CULVERT PROTECTION (INLET).
2. AT A MINIMUM, ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE ROCK SOCK.
3. CULVERT PROTECTION (INLET) IS TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. WHEN THE CULVERT INLET PROTECTION IS REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CULVERT INLET PROTECTION TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CP** | 2 OF 2 | Oct. 2013

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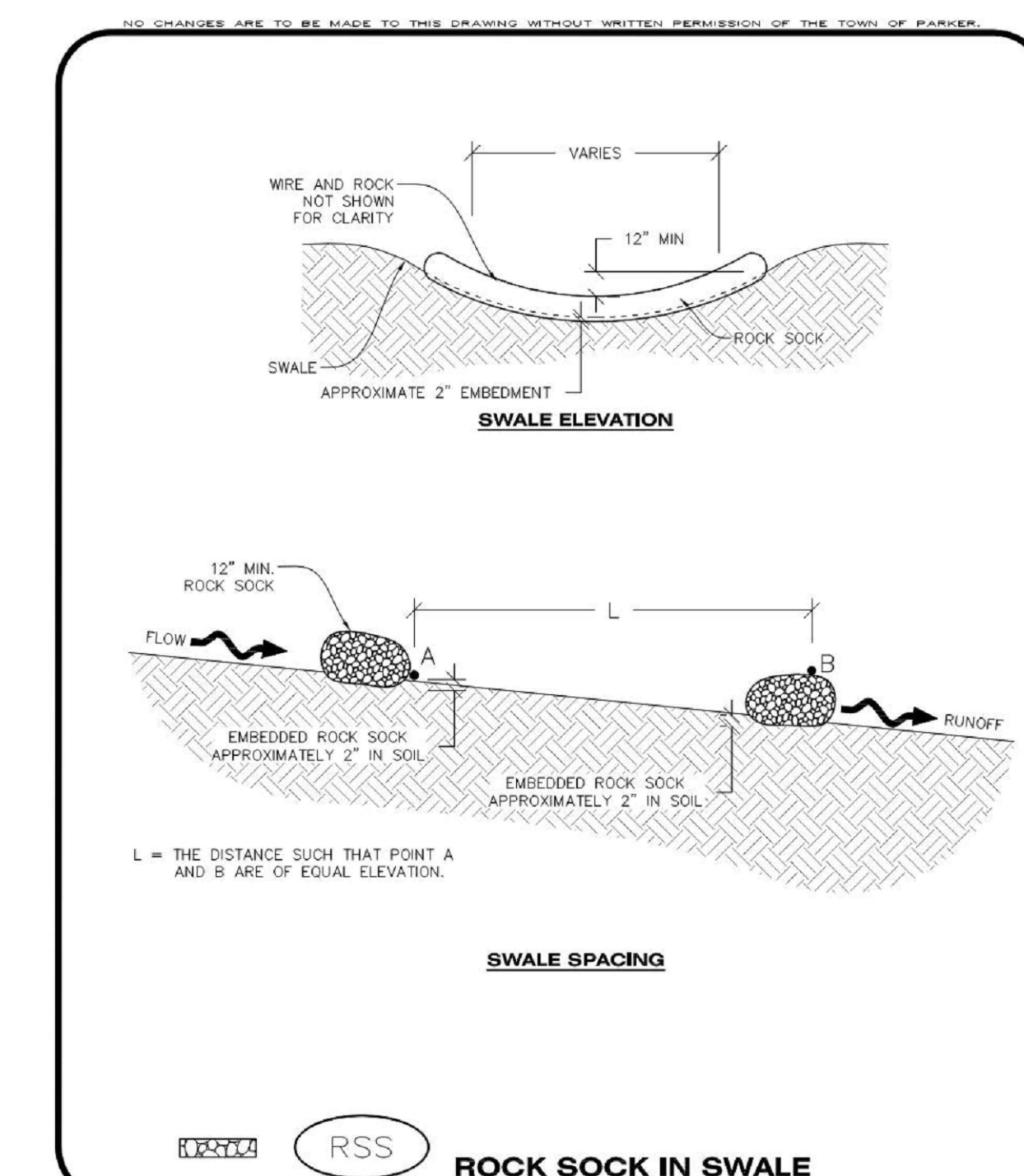
**CHECK DAM INSTALLATION NOTES**

1. SEE PLAN VIEW FOR LOCATION(S) OF CHECK DAMS.
2. CHECK DAMS SHOWN ON CBMP PLAN SHALL BE INSTALLED WHEN DIRECTED BY THE TOWN'S INSPECTOR.
3. RIPRAP UTILIZED FOR CHECK DAMS SHALL HAVE A D50 MEDIAN STONE SIZE OF 12".
4. RIPRAP PAD SHALL BE TRENCHED INTO THE GROUND A MINIMUM OF 1'-8".
5. THE MAXIMUM SPACING BETWEEN CHECK DAMS SHOULD BE SUCH THAT THE BOTTOM OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM AS SHOWN IN THE DETAIL.

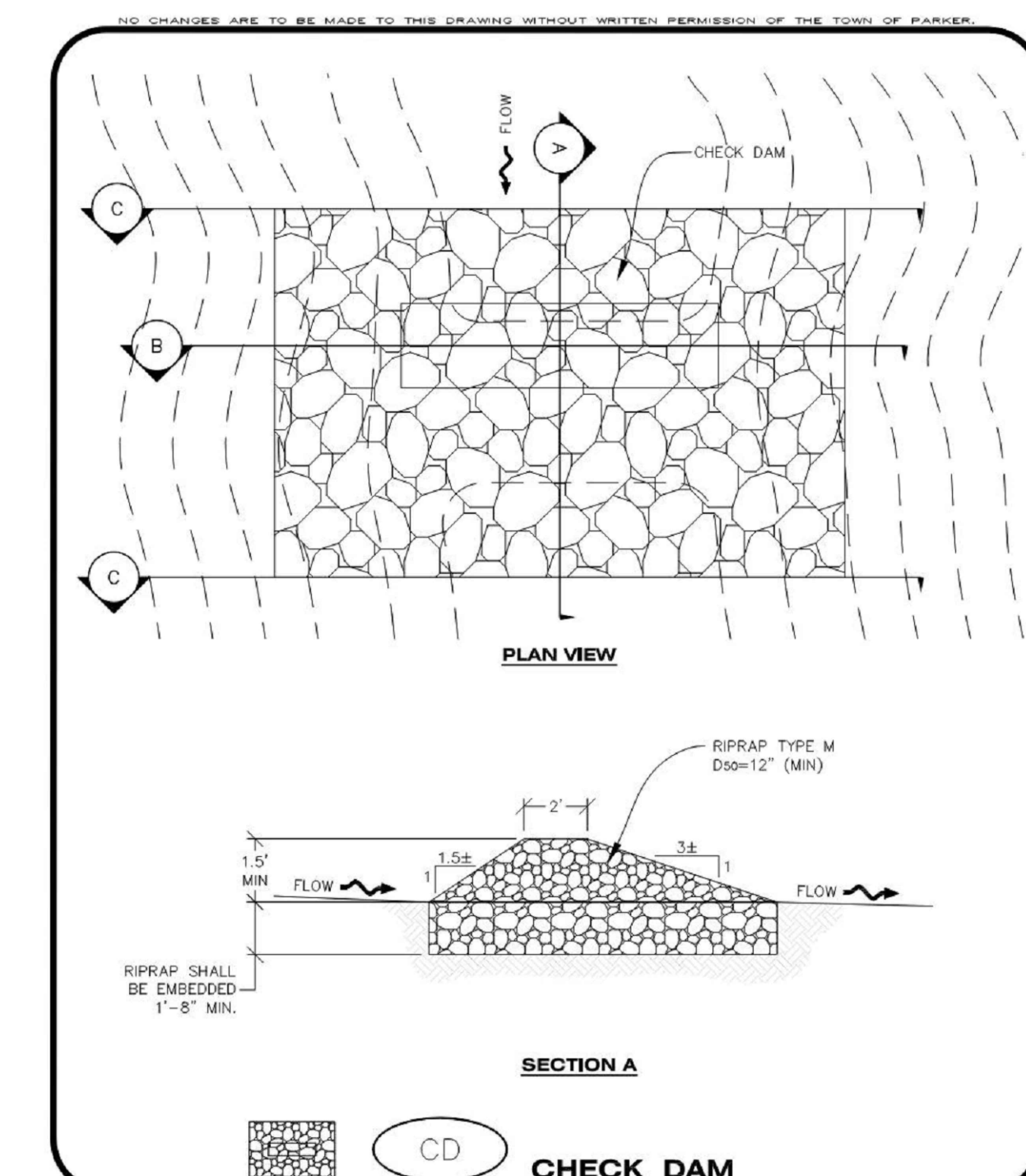
**CHECK DAM INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CHECK DAMS.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE CREST OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
3. CHECK DAMS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. WHEN CHECK DAMS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED TOPSOIL AND ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CHECK DAMS BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
5. IN SOME INSTANCES, CHECK DAMS MAY REMAIN IN PLACE PERMANENTLY.

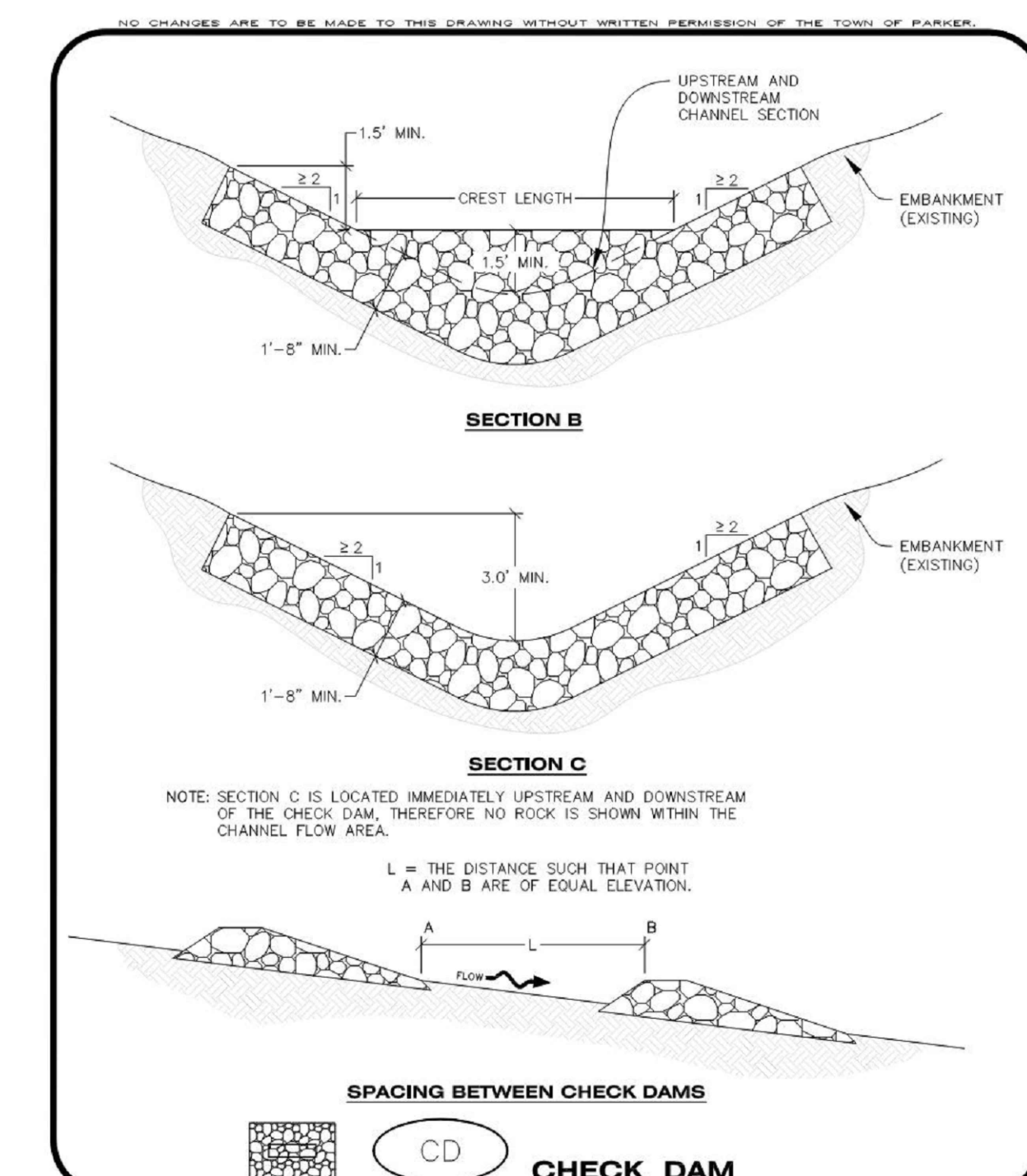
**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CD** | 3 OF 3 | Oct. 2013



**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **RSS** | 1 OF 1 | Oct. 2013



**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CD** | 1 OF 3 | Oct. 2013



**Parker COLORADO** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **CBMP** | **CD** | 2 OF 3 | Oct. 2013

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DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
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04/04/2024	THIRD TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

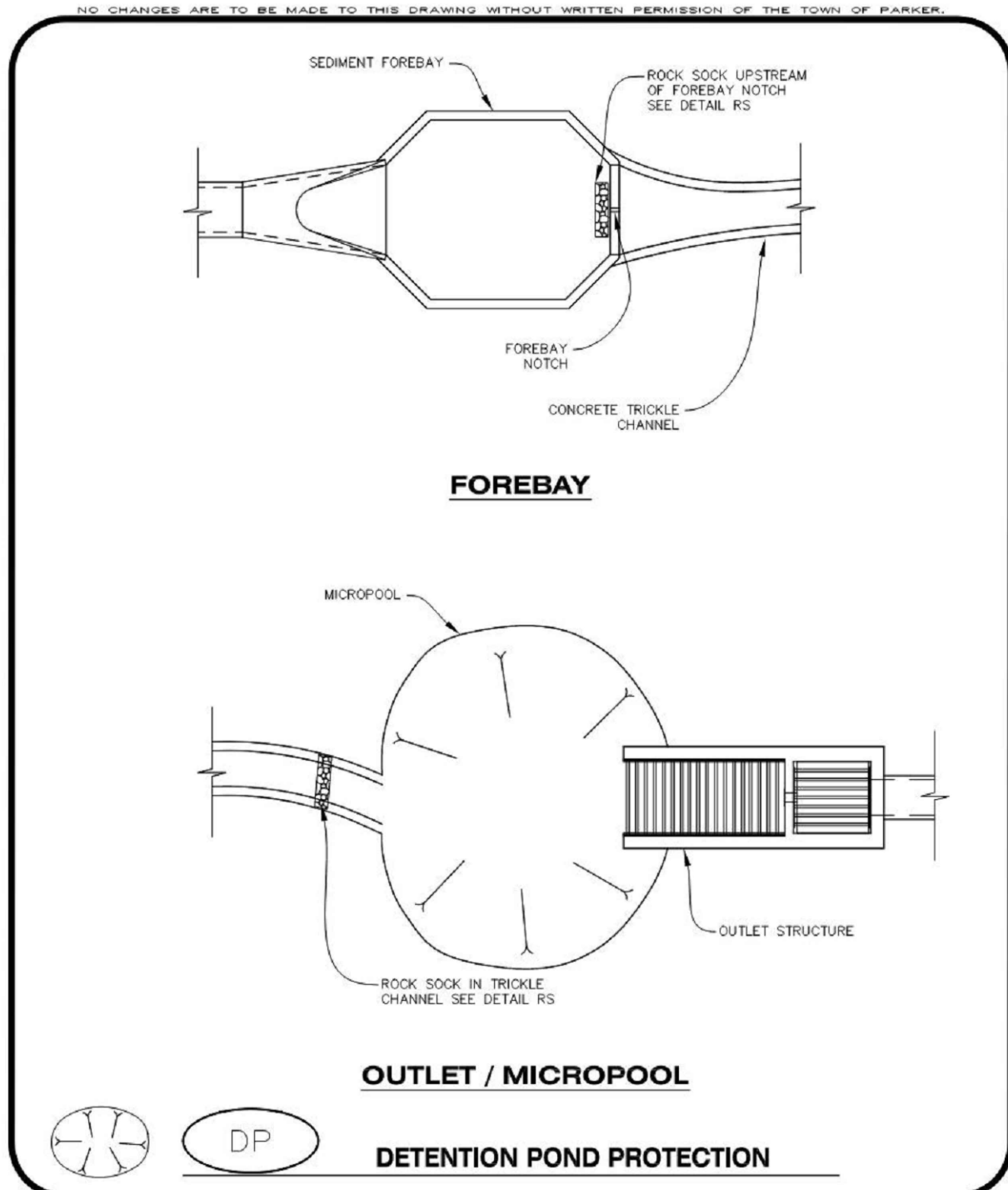
DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:

**COLORADO REGISTERED PROFESSIONAL ENGINEER**  
47666  
04/11/24

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-252**



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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DP** 1 OF 2 Oct. 2013

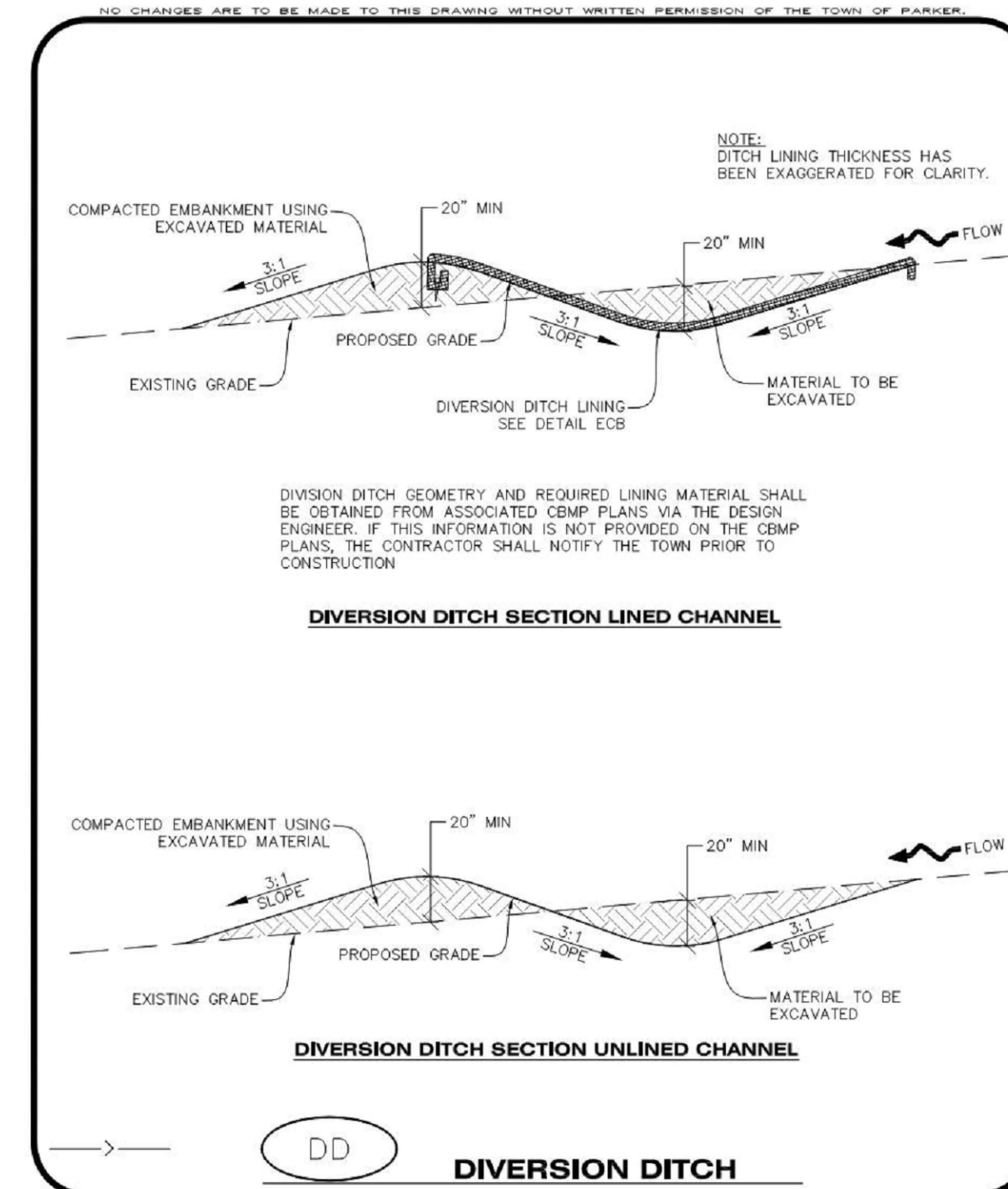
**DETENTION POND PROTECTION INSTALLATION NOTES**

- DETENTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).

**DETENTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
- OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DP** 2 OF 2 Oct. 2013



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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DD** 1 OF 2 Oct. 2013

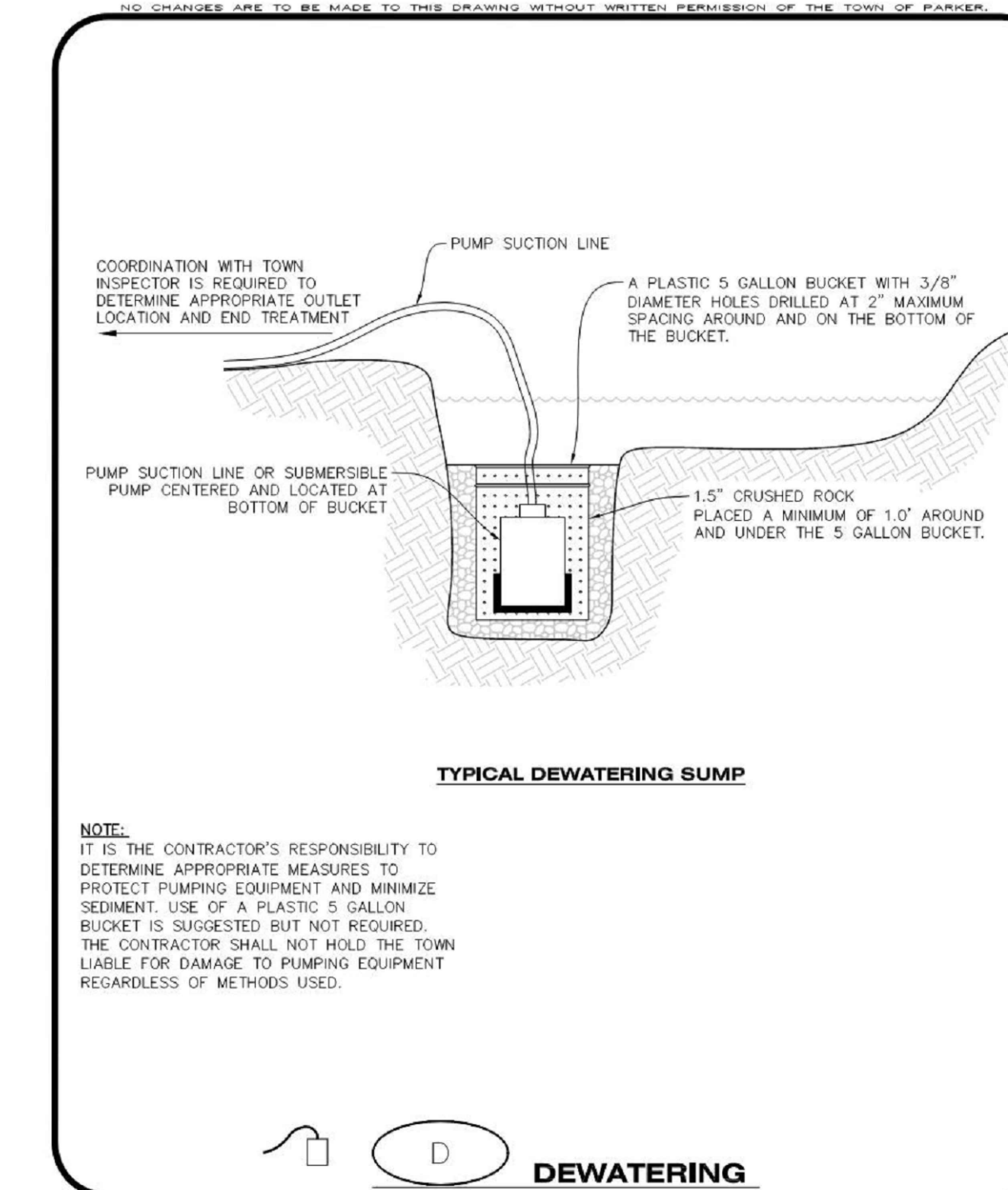
**DIVERSION DITCH INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
- A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
- ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
- THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
- THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

**DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
- DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
- DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DD** 2 OF 2 Oct. 2013



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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **D** 1 OF 2 Oct. 2013

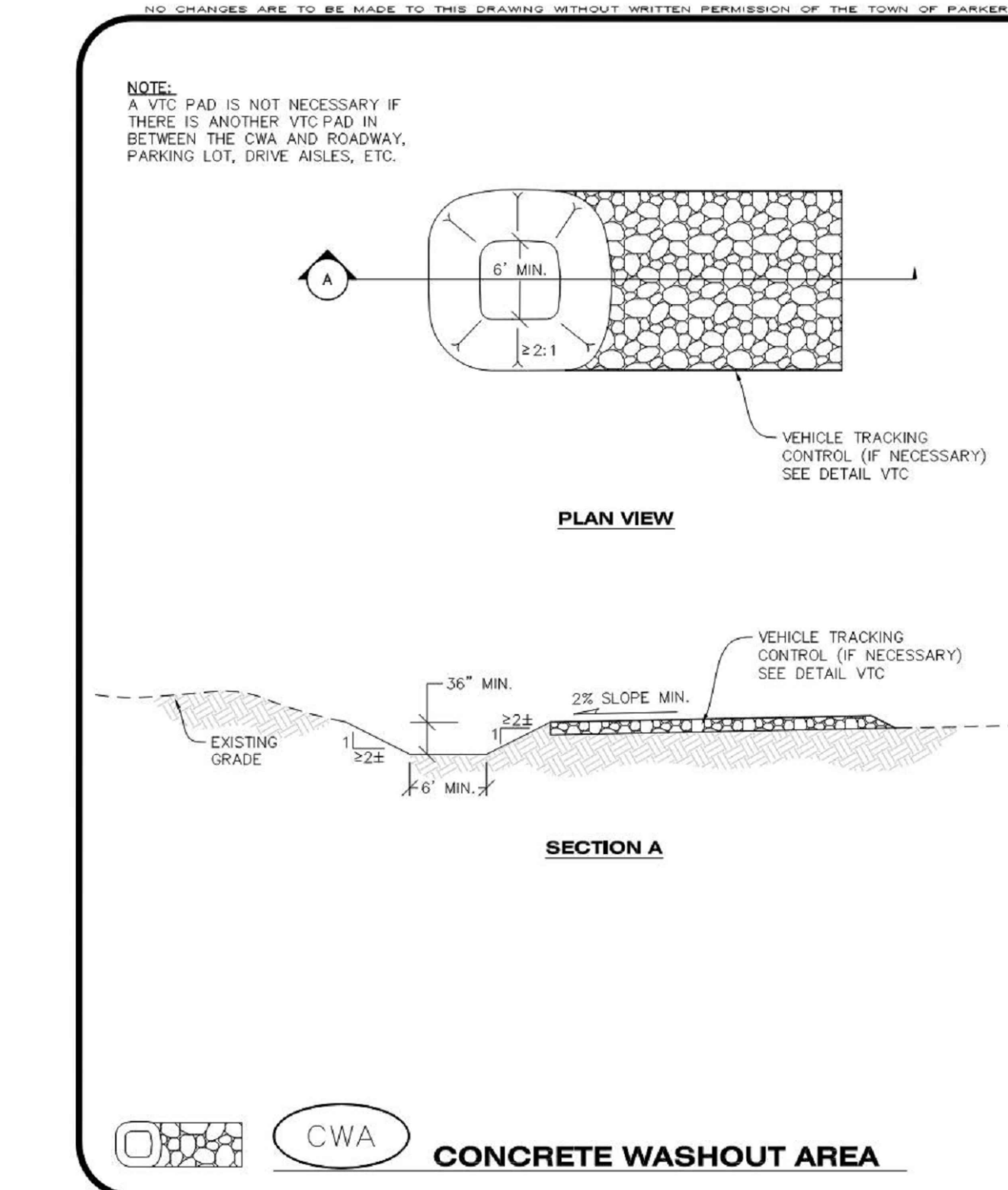
**DEWATERING INSTALLATION NOTES**

- IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

**DEWATERING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **D** 2 OF 2 Oct. 2013



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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CWA** 1 OF 2 Oct. 2013

**CONCRETE WASHOUT AREA INSTALLATION NOTES**

- CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
- A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
- IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.

**CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
- CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
- CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
- CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
- CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
- WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL. ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CWA** 2 OF 2 Oct. 2013

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

NO.	NO.
1	
2	
3	

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-253**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**AREA INLET PROTECTION INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
- THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
- AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

**Parker COLORADO** | **CBMP** | **IPA**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

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**EROSION CONTROL BLANKET INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
- EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THESE CALCULATIONS, TURF REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.
- IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH, AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.
- EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.
- ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
- ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELSIOR.

MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN	IS150
APPROVED EQUAL	APPROVED EQUAL

- IN MOST CASES, EROSION CONTROL BLANKETS FOR SLOPE PROTECTION ARE TO REMAIN IN PLACE PERMANENTLY.

**EROSION CONTROL BLANKET MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

**Parker COLORADO** | **CBMP** | **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

START EROSION BLANKET AT THE TOP OF SLOPE AND EXTEND PAST THE TOE OF SLOPE. BLANKET SHALL BE PLACED PERPENDICULAR TO THE TOP AND BOTTOM OF THE SLOPE. COMPLETE SUBSEQUENT ROWS IN THE SAME MANNER, OVERLAPPING THE BLANKETS A MINIMUM OF 1'0".

**STAPLE PATTERN**  
NOTE: STAPLES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATION. IF MANUFACTURER'S INFO IS NOT AVAILABLE, THEN ABOVE STAPLE PATTERN SHALL BE USED. WOODEN STAPLES SHALL NOT BE USED FOR EROSION CONTROL BLANKET ON SLOPES.

NOTE: WHEN NECESSARY, ALL SEEDING SHALL BE COMPLETED PRIOR TO THE PLACEMENT OF EROSION CONTROL BLANKETS. PLEASE SEE DETAIL SMC FOR SEEDING, MULCHING, AND CRIMPING REQUIREMENTS.

**ECB** | **EROSION CONTROL BLANKET (SLOPE)**

**Parker COLORADO** | **CBMP** | **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

THOROUGHLY REMOVE ALL DIRT & DEBRIS FROM CURB & GUTTER, STREET(S), AND SIDEWALK(S), ETC. ALSO REMOVE ALL LOOSE TRASH AND LITTER FROM THE CONSTRUCTION SITE, AS DESCRIBED BELOW.

**DTC** | **DEBRIS AND TRASH CONTROL**

**NOTE:**

- THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY. FURTHERMORE, ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

**Parker COLORADO** | **CBMP** | **DTC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**AREA INLET WITH CONCRETE APRON**  
**AREA INLET PLAN VIEW**  
**SECTION A**  
**SECTION B**  
**ISOMETRIC**

**IPAP** | **INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

**Parker COLORADO** | **CBMP** | **IPAP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

THIS METHOD OF INLET PROTECTION SHALL NOT BE USED ON INLETS RECEIVING SIGNIFICANT FLOWS, SUCH AS IN CERTAIN SWALES/CHANNELS OR HIGHWAY MEDIANS. INLET PROTECTION FOR AREA INLETS IN PAVEMENT (SEE DETAIL IPAP) SHALL BE USED IN THESE CONDITIONS.

**AREA INLET WITH CONCRETE APRON**  
**AREA INLET PLAN VIEW**  
**SECTION A**  
**SECTION B**  
**ISOMETRIC**

**IPAN** | **INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT**

**Parker COLORADO** | **CBMP** | **IPAN**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**TOE OF SLOPE ANCHOR TRENCH**  
**TOP OF SLOPE ANCHOR TRENCH**  
**JOINT ANCHOR TRENCH**  
**WOOD STAKE DETAIL**  
**STAKING PATTERN**

**ECB** | **EROSION CONTROL BLANKET (CHANNEL)**

**Parker COLORADO** | **CBMP** | **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**DEBRIS CONTROL NOTES:**

- A COMBINATION OF SURFACE SCRAPING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THESE AREAS.
- ALL CHEMICAL SPILLS AND/OR STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EXTENT PRACTICABLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VAC-TRUCK.
- ON-SITE PERSONNEL, DELIVERY DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUAL DEBRIS AND TRASH CONTROL.

**Parker COLORADO** | **CBMP** | **DTC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

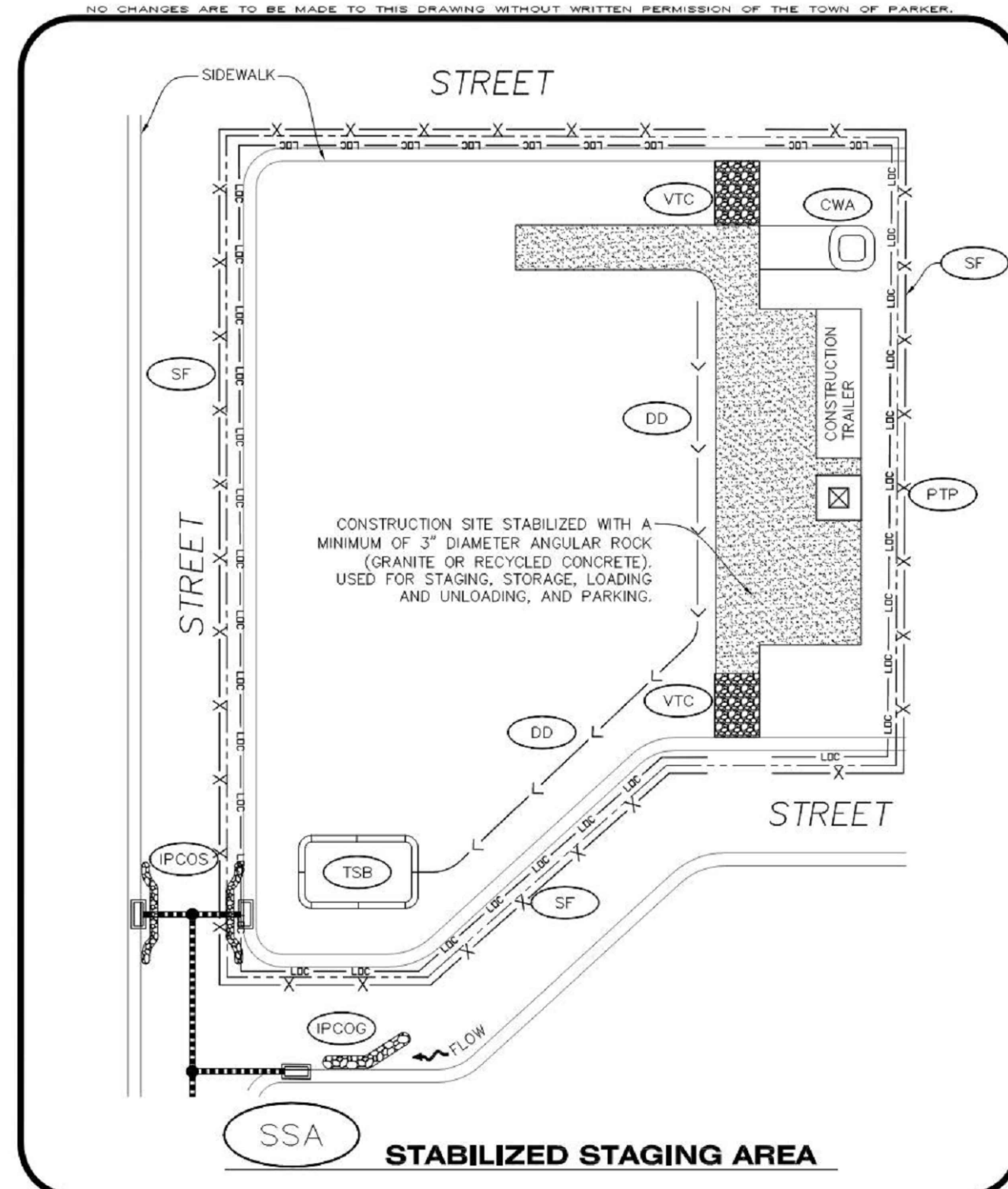
DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:



DESIGNED BY: **RJH**  
DRAWN BY: **DEI**  
CHECKED BY: **MHV**  
MIRO JOB NO. **22139**

DRAWING NUMBER:  
**C-254**



**Parker COLORADO** **CBMP** **SSA**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

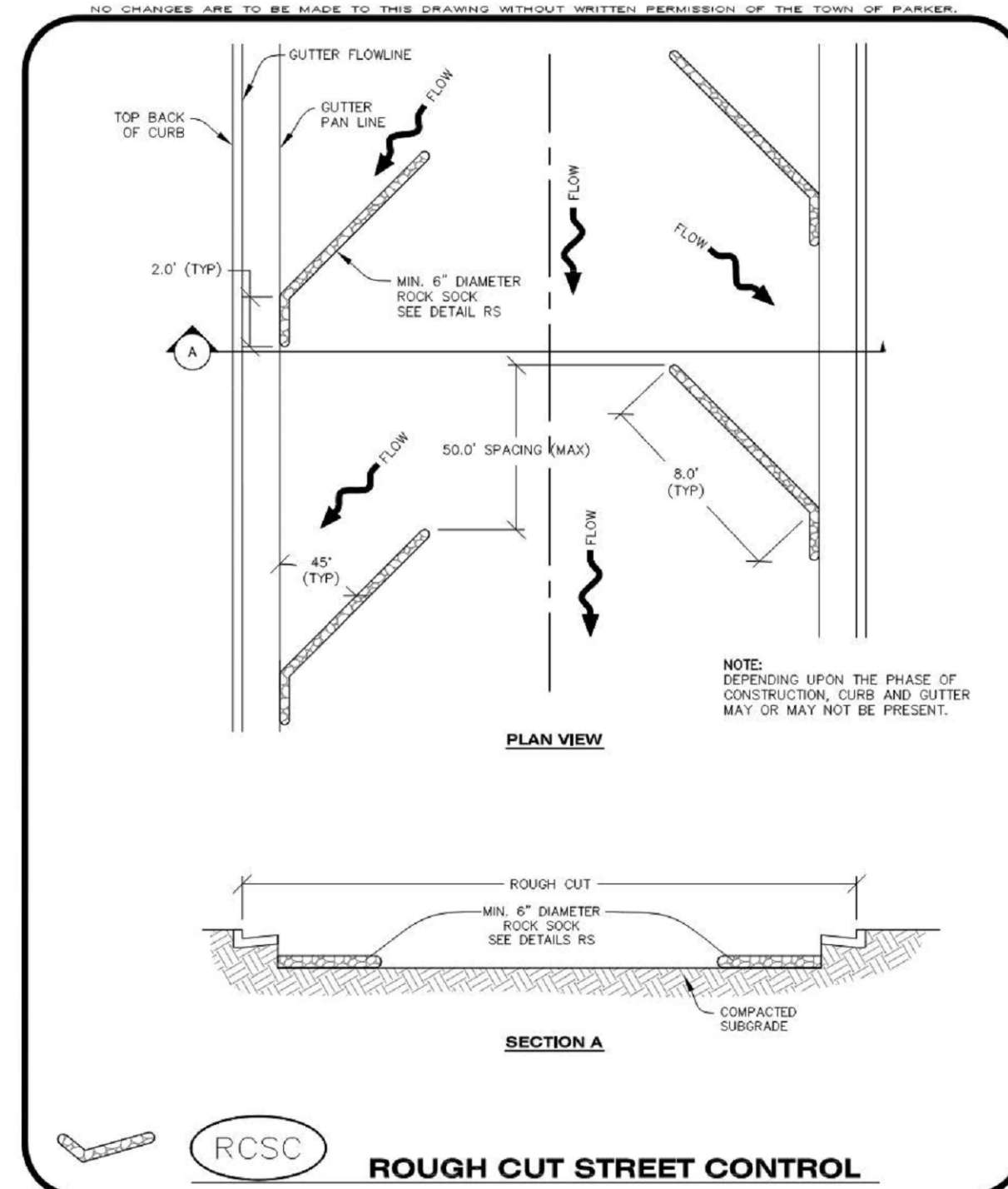
**STABILIZED STAGING AREA INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION OF STAGING AREA. CONTRACTOR MAY MODIFY LOCATION AND SIZE OF STABILIZED STAGING AREA WITH TOWN APPROVAL.
- STABILIZED STAGING AREA SHALL BE LARGE ENOUGH TO FULLY CONTAIN PARKING, STORAGE, AND LOADING OPERATIONS.
- THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM OF 3" DIAMETER OF ANGULAR ROCK (GRANITE OR RECYCLED CONCRETE).
- SSA FOR SMALLER SITES MAY NOT BE PRACTICAL. IN THESE AND SIMILAR SITUATIONS, VARIANCES MAY BE PERMITTED BY THE TOWN.

**STABILIZED STAGING AREA INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STAGING AREA.
- STABILIZED STAGING AREA SHALL BE ENLARGED AS NECESSARY TO CONTAIN PARKING, STORAGE, LOADING, AND UNLOADING.

**Parker COLORADO** **CBMP** **SSA**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
Oct. 2013



**Parker COLORADO** **CBMP** **RCSC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

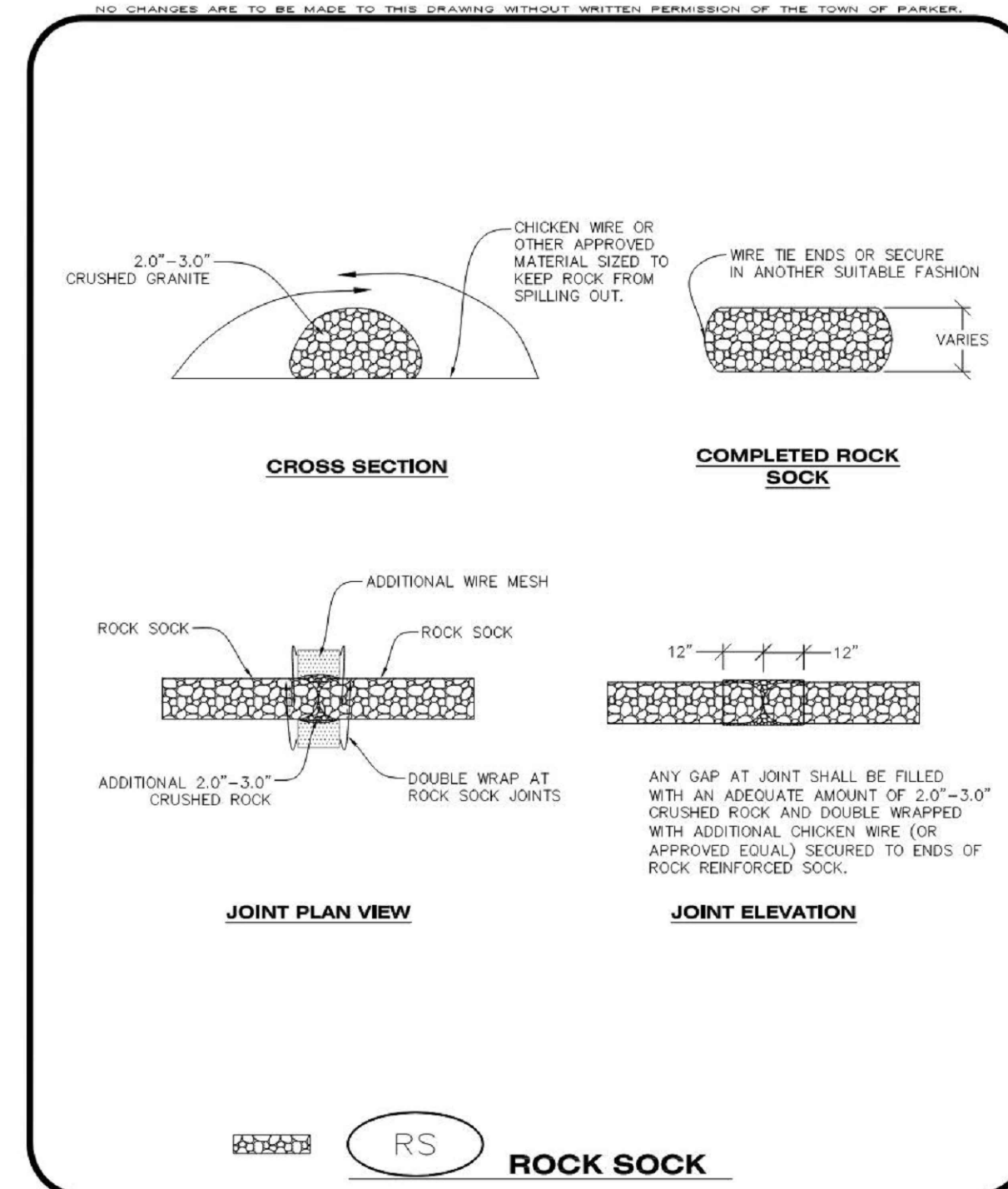
**ROUGH CUT STREET CONTROL INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
- THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.

**ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
- ROUGH CUT STREET CONTROL SHALL BE REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
- ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUB-GRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE RCSC SHOULD BE REMOVED IN INCREMENTS BASED ON SUBGRADE PREPARATION.

**Parker COLORADO** **CBMP** **RCSC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
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**Parker COLORADO** **CBMP** **RS**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

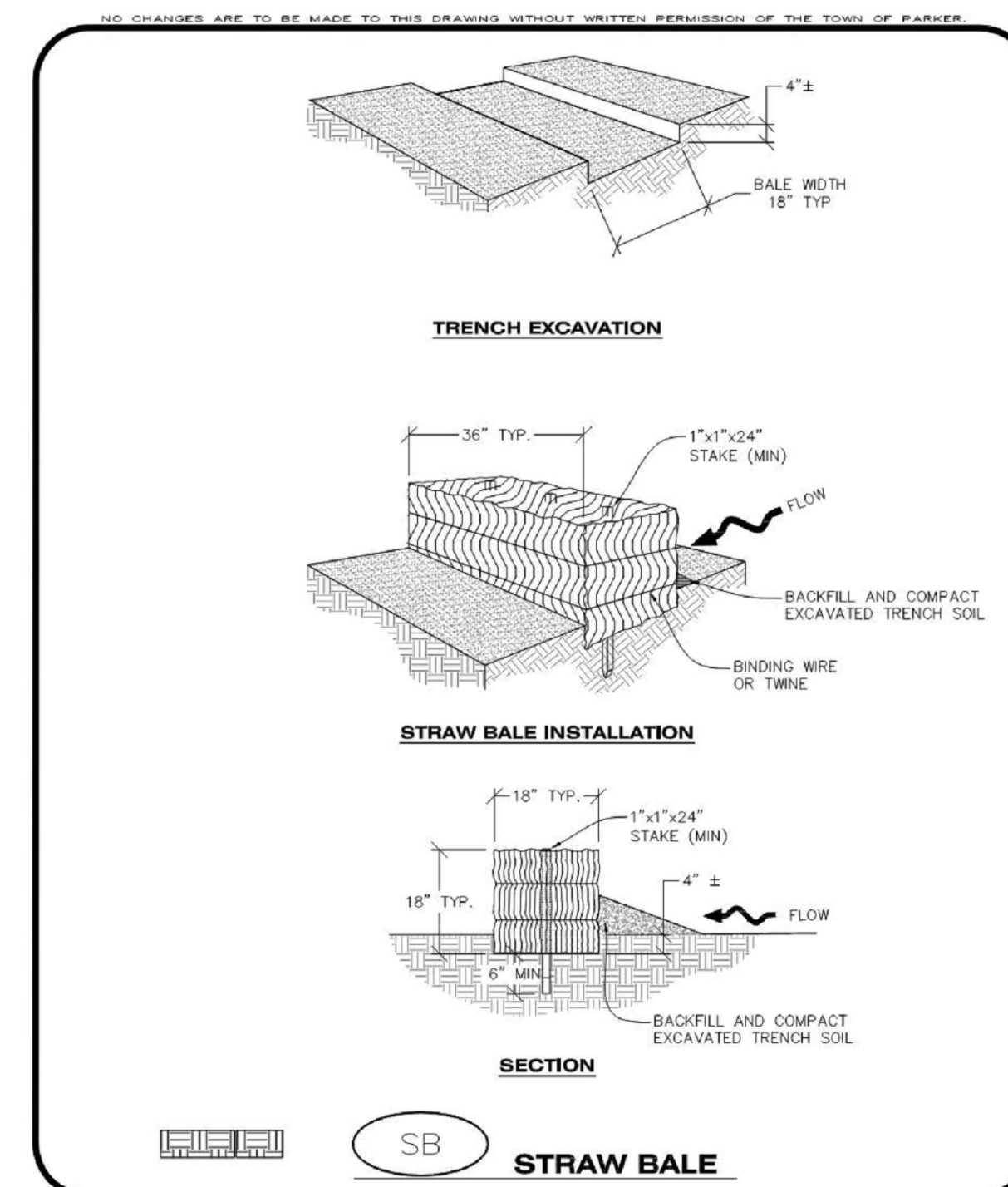
**ROCK SOCK INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
- CRUSHED ROCK SHALL BE APPROXIMATELY 2.0"-3.0" GRANITE IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
- TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCATION SUSCEPTIBLE TO VEHICLE OR PEDESTRIAN TRAFFIC. TUBULAR MARKERS SHALL CONFORM TO THE TUBULAR MARKER DETAIL.

**ROCK SOCK INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**Parker COLORADO** **CBMP** **RS**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
Oct. 2013



**Parker COLORADO** **CBMP** **SB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**STRAW BALE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
- TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36"x18"x18".
- TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1"x1"x24".
- WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
- STRAW BALES SHALL BE SPACED AND POSITIONED ACCORDING TO DETAILS.

**STRAW BALE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
- STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
- STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN INSPECTOR.
- WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**Parker COLORADO** **CBMP** **SB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
Oct. 2013



DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
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PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

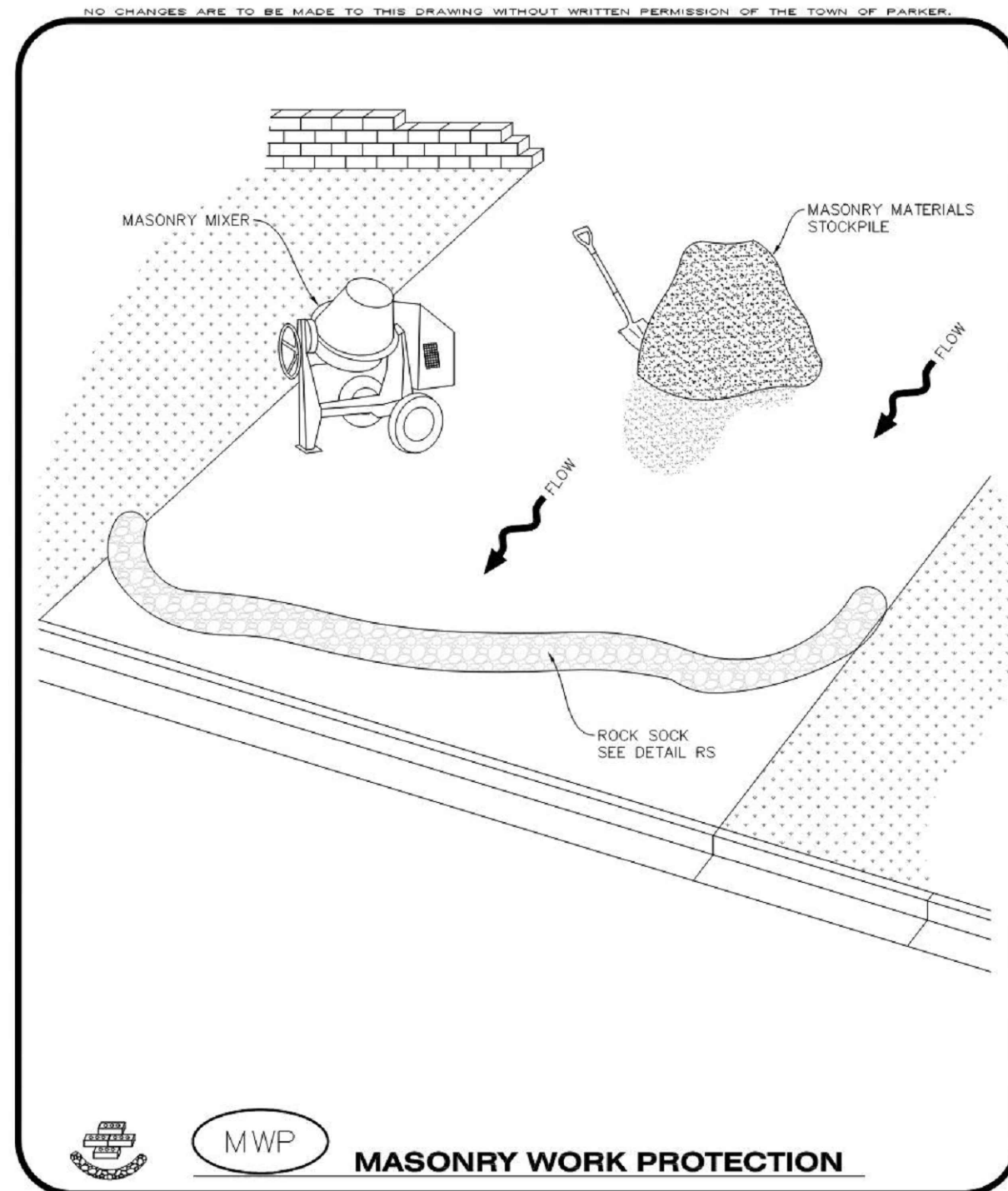
DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:



DESIGNED BY: R/JH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-255**



**MWP**  
**MASONRY WORK PROTECTION**

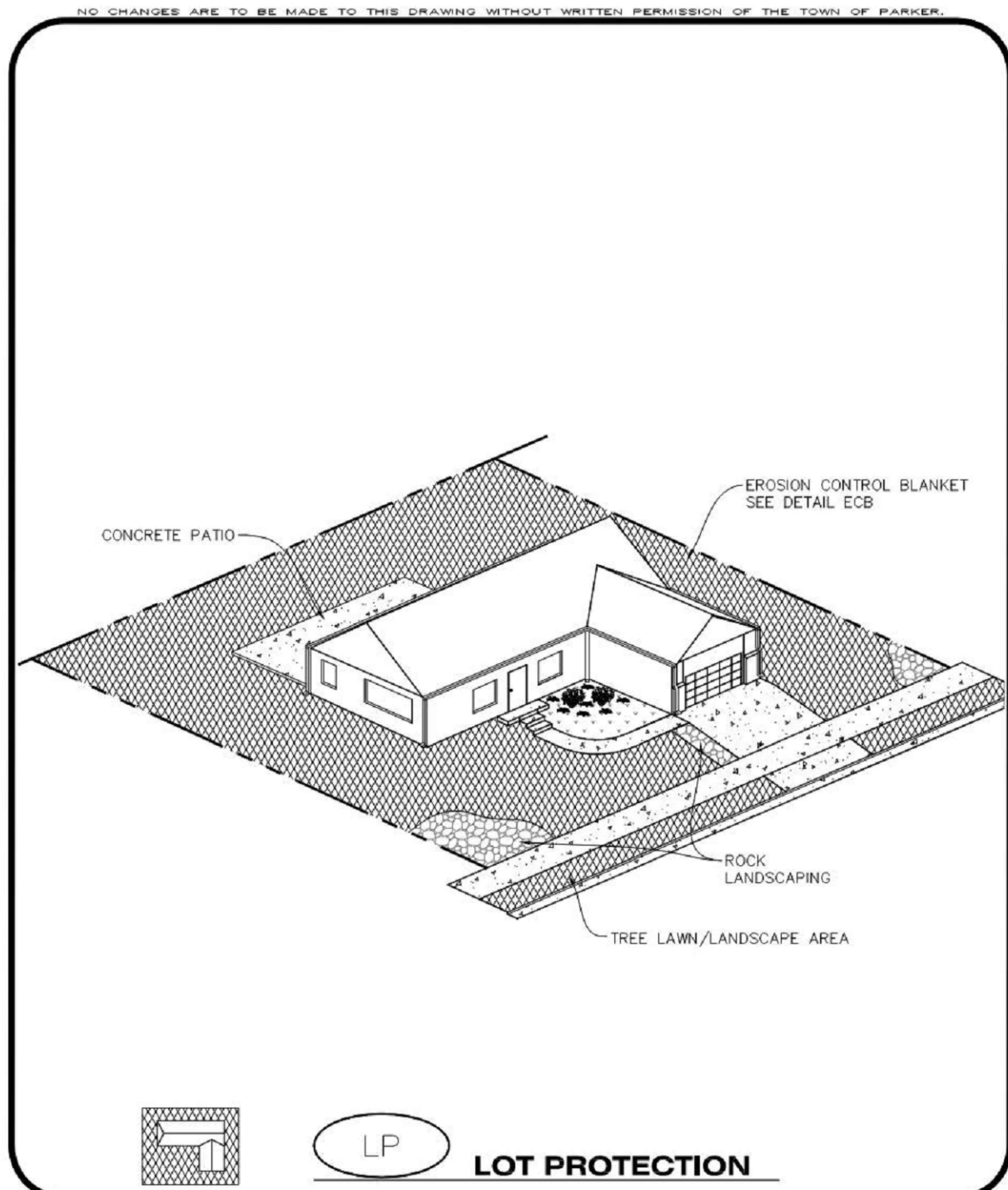
**MASONRY WORK PROTECTION INSTALLATION NOTES**

- MASONRY WORK PROTECTION MAY NEED TO BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
- A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.

**MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
- ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASH OUT AREA.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.

**CBMP**  
**CONSTRUCTION BEST MANAGEMENT PRACTICES**  
**MWP**  
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**LP**  
**LOT PROTECTION**

**EROSION CONTROL BLANKET FOR LOT PROTECTION INSTALLATION NOTES**

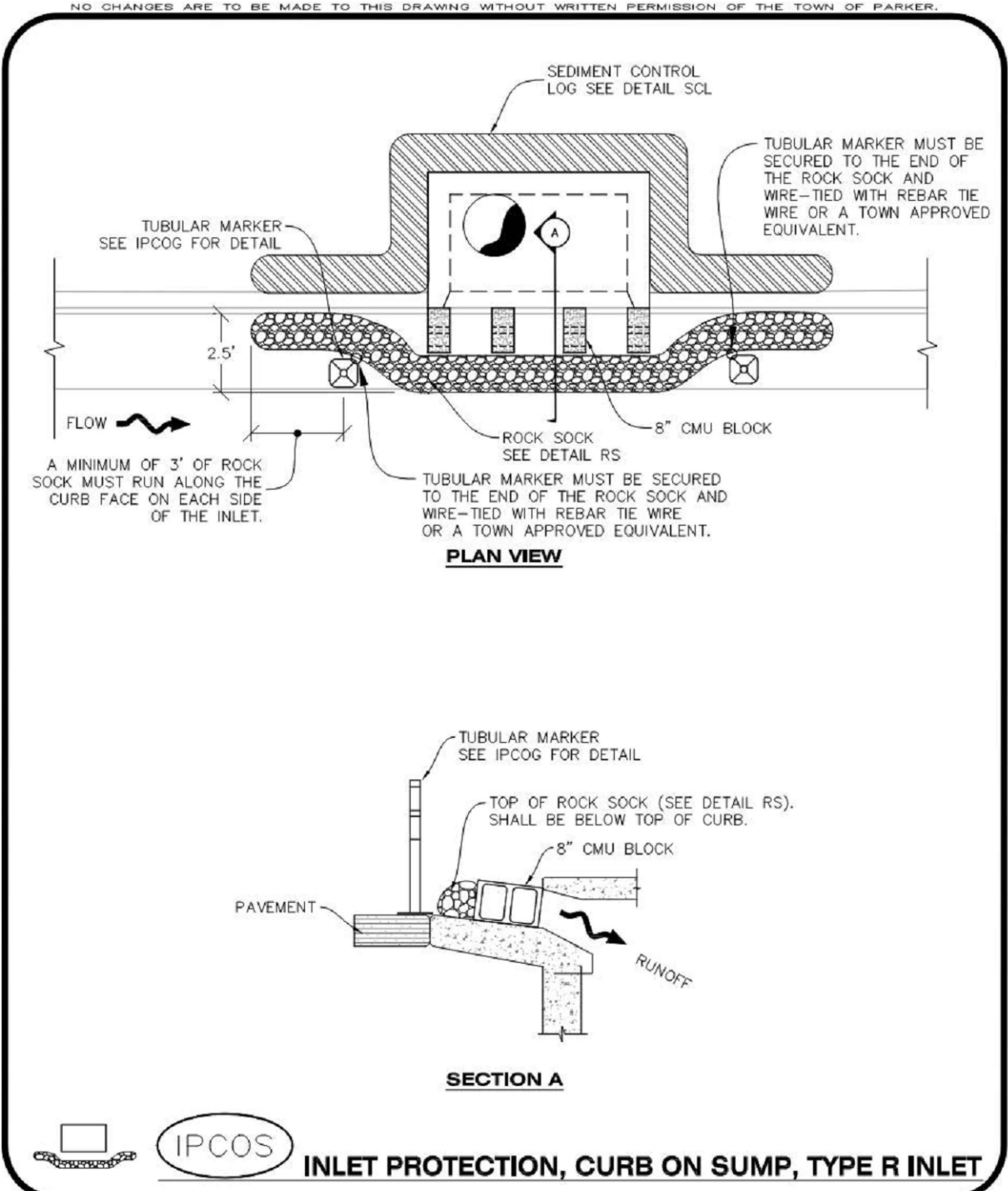
- ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION INSTALLED IN THE TOWN SHALL MEET THE FOLLOWING MINIMUM REQUIREMENTS:
 

MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN APPROVED EQUAL	S150 EN APPROVED EQUAL
- ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION SHALL BE INSTALLED ACCORDING TO THE DETAIL DRAWINGS, WITH THE FOLLOWING ALLOWANCES AND ADDITIONS:
  - THE TOWN WILL ALLOW THE USE OF BIODEGRADABLE, EROSION CONTROL BLANKET-SPECIFIC STAPLES, IN LIEU OF TRADITIONAL METAL STAPLES.
  - ALL EROSION CONTROL BLANKET EDGES (SIDES AND ENDS) MUST OVERLAP THE ADJACENT BLANKET BY A MINIMUM OF 6-INCHES WITH THE UP-GRADIENT EDGES BEING PLACED ON TOP OF THE DOWN-GRADIENT EDGE OF THE ADJACENT BLANKET.
  - THE EDGES (SIDES AND ENDS) OF THE EROSION CONTROL BLANKETS DO NOT NEED TO BE TRENCHED INTO THE GROUND ASSUMING THE SITE CONDITIONS WILL NOT CAUSE EROSION BENEATH THE BLANKETS. THESE ASSUMPTIONS WILL BE THE RESPONSIBILITY OF THE EROSION CONTROL SUPERVISOR. ON OCCASION, THE TOWN'S INSPECTOR MAY REQUEST TRENCHING BASED UPON SITE CONDITIONS.
  - TOPSOIL PLACEMENT, SEEDING, AND MULCHING WILL NOT BE REQUIRED ON THE LOTS PRIOR TO THE INSTALLATION OF THE EROSION CONTROL BLANKET.
- ANY DAMAGED OR EXCESS STAPLES SHALL BE REMOVED FROM THE SITE FOLLOWING INSTALLATION.
- THE EROSION CONTROL BLANKETING REQUIRED AS PART OF THE LOT PROTECTION (LP) REQUIREMENT MUST BE INSTALLED OVER ALL UN-LANDSCAPED AREAS WITHIN EACH RESIDENTIAL, MULTI-FAMILY, AND COMMERCIAL LOT PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY / TEMPORARY CERTIFICATE OF OCCUPANCY.

**EROSION CONTROL BLANKET FOR LOT PROTECTION MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKET FOR LOT PROTECTION.
- EROSION CONTROL BLANKETS FOR LOT PROTECTION ARE INTENDED TO REMAIN IN PLACE AND MAINTAINED UNTIL LANDSCAPING IS INSTALLED.

**CBMP**  
**CONSTRUCTION BEST MANAGEMENT PRACTICES**  
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**IPCOS**  
**INLET PROTECTION, CURB ON SUMP, TYPE R INLET**

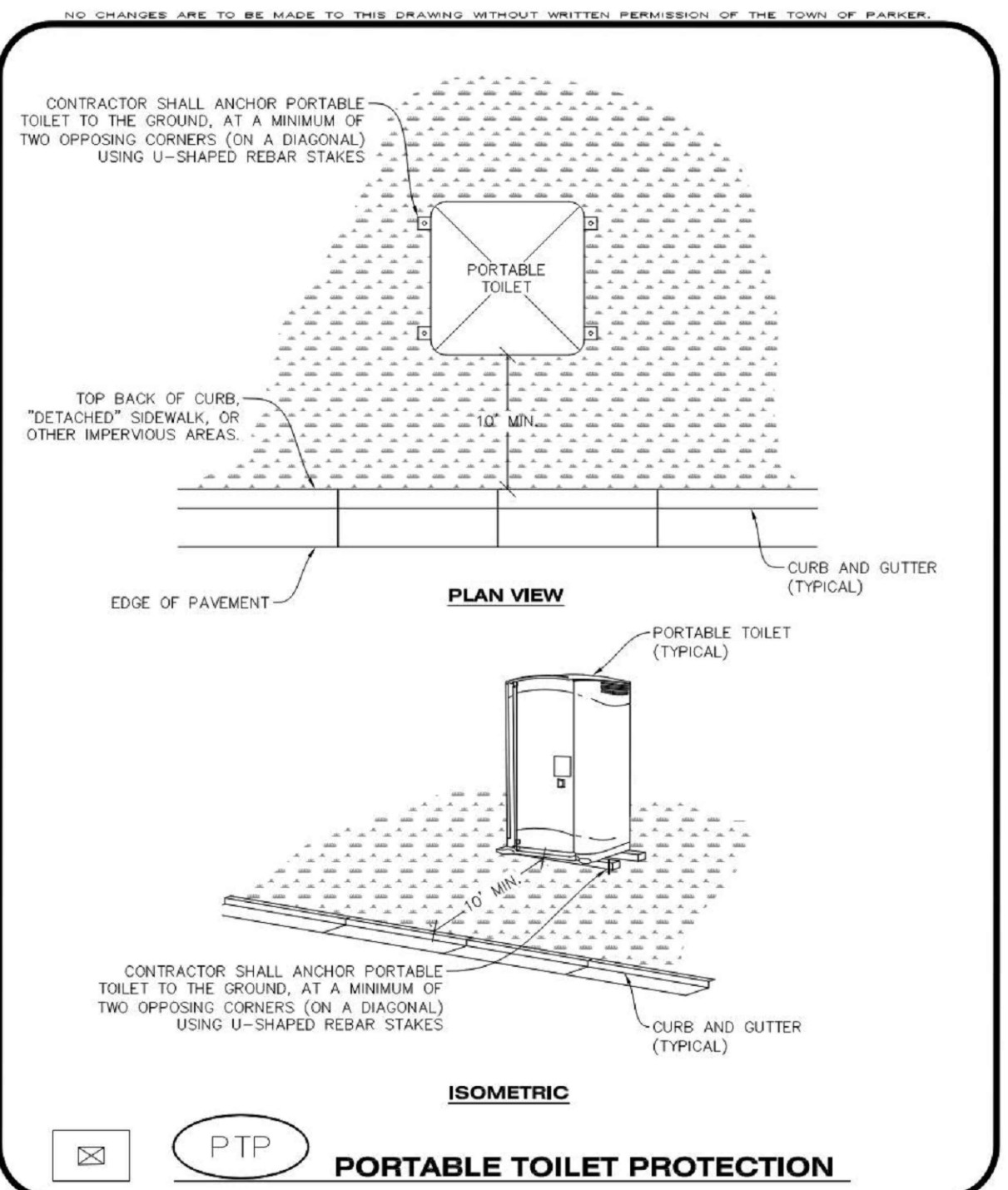
**CURB INLET PROTECTION INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF CURB INLET PROTECTION.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- ROCK SOCK SHALL EXTEND 3.0' ALONG THE CURB BEYOND LOCATIONS WHERE IT RETURNS TO CONTACT CURB FACE.
- TUBULAR TRAFFIC MARKERS SHALL BE A MINIMUM OF 36" IN HEIGHT WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
- THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE CURB INLET. CMU BLOCKS OR THE ROCK SOCK SHALL BE USED AS INTERIM PROTECTION UNTIL THE FIRST LIFT OF ASPHALT IS INSTALLED.

**CURB INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CURB INLET PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- CURB INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP**  
**CONSTRUCTION BEST MANAGEMENT PRACTICES**  
**IPCOS**  
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**PTP**  
**PORTABLE TOILET PROTECTION**

**PORTABLE TOILET PROTECTION INSTALLATION NOTES**

- PORTABLE TOILETS SHALL BE PLACED A MINIMUM OF 10.0' BEHIND ALL CURBS, SIDEWALKS, AND OTHER IMPERVIOUS AREAS.
- ALL PORTABLE TOILETS MUST BE GROUPED TOGETHER.
- PORTABLE TOILETS SHALL BE SECURELY ANCHORED TO THE GROUND USING U-SHAPED REBAR STAKES.
- U-SHAPED REBAR STAKES SHALL BE POSITIONED ON AT LEAST 2 OPPOSING (DIGITAL) CORNERS.

**PORTABLE TOILET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE PORTABLE TOILET PROTECTION.
- PORTABLE TOILETS SHALL BE SERVICED AT THE NECESSARY INTERVALS TO ELIMINATE THE POSSIBILITY OF OVERFLOW.
- WHEN THE PORTABLE TOILETS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE TOILETS MAY NEED TO BE LANDSCAPED OR ROUGHENED, SEEDED, MULCHED, AND GRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- PORTABLE TOILETS THAT ARE NOT CONSISTENTLY MAINTAINED IN ACCORDANCE WITH THESE REQUIREMENTS MAY NEED TO BE CLUSTERED TOGETHER, IN ONE CENTRALIZED LOCATION IN ORDER TO INCREASE COMPLIANCE AND REDUCE THE CHANCE OF A SPILL.

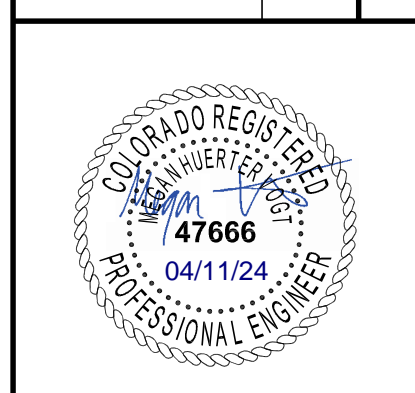
**CBMP**  
**CONSTRUCTION BEST MANAGEMENT PRACTICES**  
**PTP**  
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Oct. 2013

DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
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PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

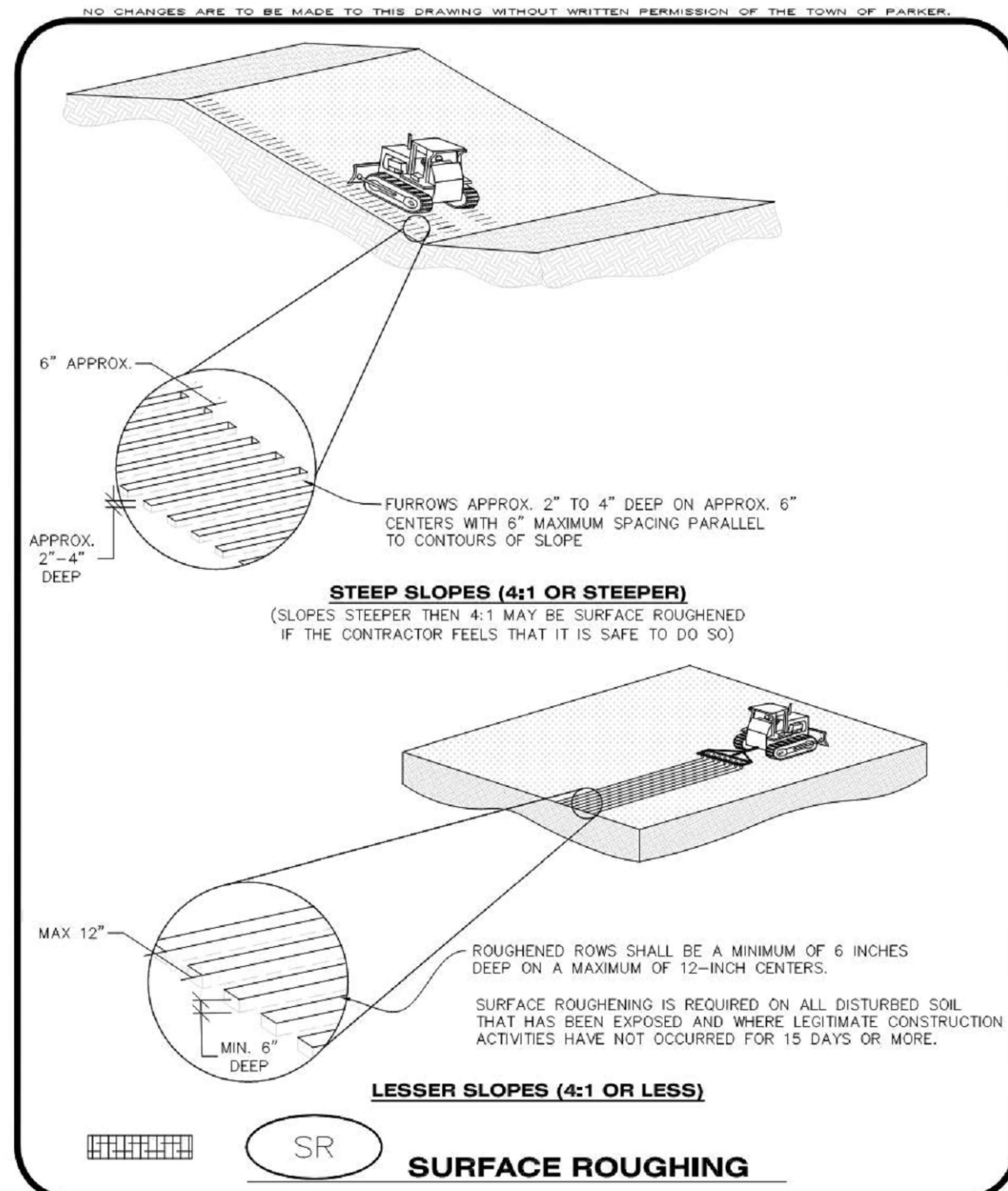
DRAWING TITLE: **EROSION CONTROL DETAILS**

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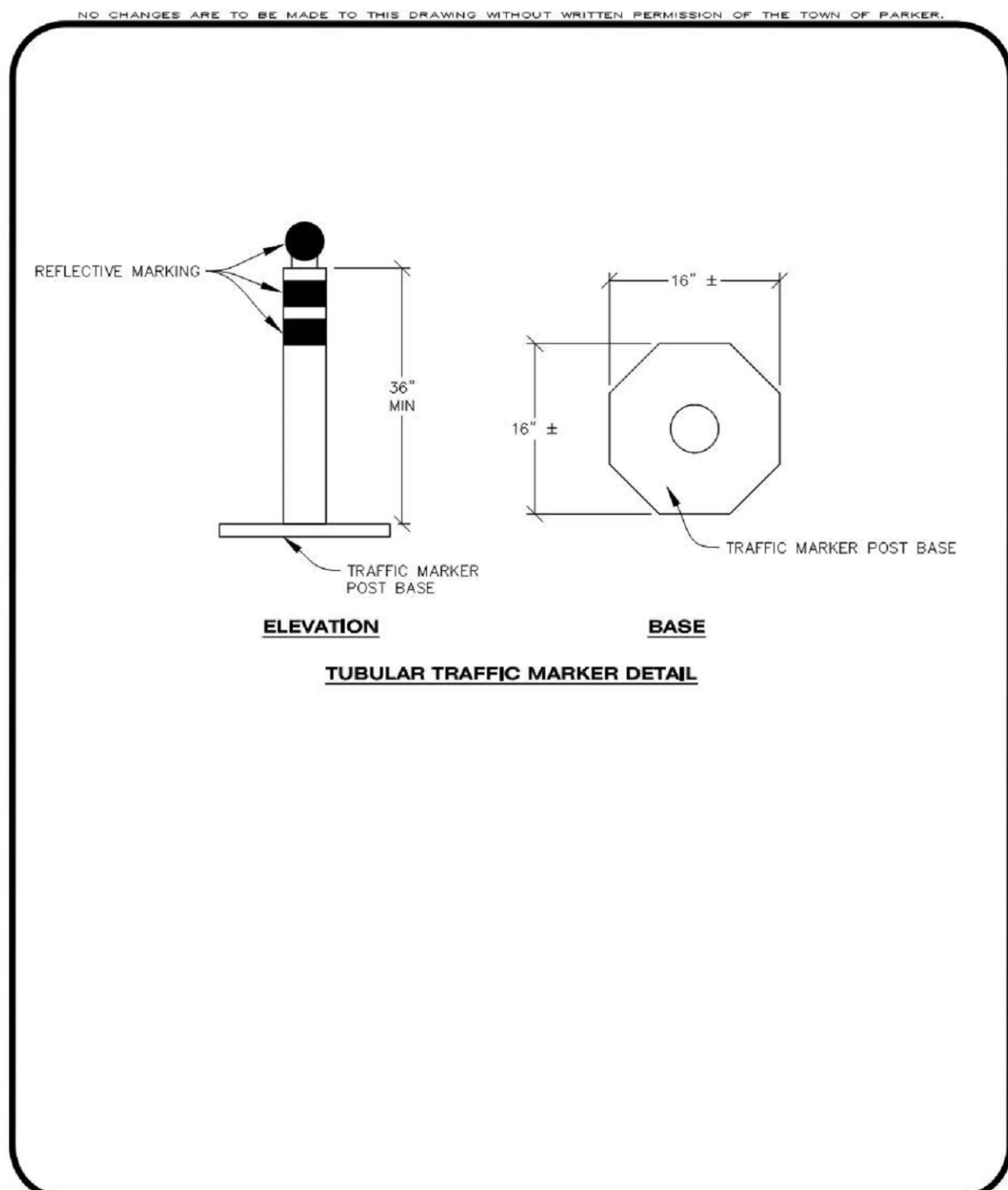


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-256**



**Parker COLORADO** | **CBMP** | **SR**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013



**Parker COLORADO** | **CBMP** | **IPCOG**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**TOWN OF PARKER, SEED MIX 1**  
20% CANADA BLUEGRASS  
15% CRESTED WHEATGRASS  
15% SLENDER WHEATGRASS  
10% ANNUAL RYEGRASS  
10% SHEEP FESCUE  
10% BIG BLUESTEM  
10% SIBEOATS GRAMA  
5% CANADA BLUEGRASS  
5% BLUE GRAMA

**SEEDING RATE:**  
DRILLED: 25 LBS./ACRE  
BROADCAST: 50 LBS./ACRE

**TOWN OF PARKER, SEED MIX 2**  
22% SLENDER WHEATGRASS  
18% SODAR STREAMBANK WHEATGRASS  
13% ARIZONA FESCUE  
13% BLUE GRAMA  
12% BUFFALOGRASS  
12% BARLEY OR OATS  
5% SPIKE MUHLY  
5% INDIAN RICEGRASS

**SEEDING RATE:**  
DRILLED: 25 LBS./ACRE  
BROADCAST: 50 LBS./ACRE

**TOWN OF PARKER, SEED MIX 3 (LOW-GROWTH MIX)**  
25% EPHRAIM CRESTED WHEATGRASS  
23% SHEEP FESCUE  
18% PERENNIAL RYEGRASS  
13% CANADA BLUEGRASS  
12% BARLEY OR OATS  
9% BLUE FESCUE

**SEEDING RATE:**  
DRILLED: 25 LBS./ACRE  
BROADCAST: 50 LBS./ACRE

**SEED MIX 4:**  
OTHER SEED MIXES APPROVED BY THE TOWN OF PARKER

**Parker COLORADO** | **CBMP** | **SMC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SEEDING AND MULCHING SHALL BE PERFORMED ACCORDING TO THE ACCOMPANYING DETAIL(S) AND TEXT. NO EXCEPTIONS SHALL BE MADE**

- SEE PLAN VIEW FOR:
  - LOCATION(S) OF SEEDING AND MULCHING
  - TYPE OF SEED MIX
- SEED MIXES MAY CONFORM TO THE TABLE PROVIDED WITH THE SMC NOTES OR ALTERNATIVES MAY BE ALLOWED WITH PRIOR PERMISSION BY THE TOWN'S INSPECTOR.
- SEEDING MAY BE PERFORMED YEAR ROUND ASSUMING THE SOIL IS NOT FROZEN. SEEDING DURING TIMES OF EXTREME TEMPERATURES SHOULD BE AVOIDED IF POSSIBLE.
- AT THE BEGINNING OF THE LAND DISTURBANCE ACTIVITIES, IT IS HIGHLY RECOMMENDED THAT AN APPROPRIATE AMOUNT OF NATIVE TOPSOIL BE STRIPPED FROM THE SITE AND STOCKPILED. ALL AREAS, PRIOR TO PERMANENT SEEDING AND MULCHING, WILL LIKELY NEED TO BE COVERED WITH AN APPROPRIATE LAYER OF TOPSOIL. THIS REQUIREMENT APPLIES TO ALL AREAS WHERE NATIVE SEEDING IS SPECIFIED ON THE CBMP PLAN AND/OR LANDSCAPING PLANS.
- IT IS STRONGLY RECOMMENDED THAT SAMPLES FROM THE STRIPPED TOPSOIL BE PROPERLY COLLECTED AND TESTED BY A QUALIFIED LABORATORY TO ENSURE ADEQUATE NUTRIENT CONTENT PRIOR TO SEEDING AND MULCHING. IF IT IS DISCOVERED THAT THE TOPSOIL IS VOID OF THE NUTRIENTS NECESSARY TO SUCCESSFULLY ESTABLISH THE REQUIRED VEGETATION, THEN THE APPROPRIATE AMENDMENTS SHALL BE ADDED.
- ALL AREAS TO BE SEEDING AND MULCHED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING DETAILS AND NOTES. SURFACE ROUGHENING SHALL OCCUR AFTER PLACEMENT OF THE TOPSOIL.
- WHEN INSTALLED WITH A DRILL SEEDER, SEED SHALL BE PLACED AT A DEPTH OF 1/4 - 1/2 INCH. ROW SPACING SHALL BE NO MORE THAN 6-INCHES.
- ALL AREAS INCAPABLE OF BEING DRILL SEEDER SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING NOTES OR EFFECTIVELY ROUGHENED USING A HARROW OR OTHER SUCH IMPLEMENT. ALL AREAS SHALL BE UNIFORMLY HAND BROADCASTED WITH THE PROPER SEED MIX APPLIED AT TWO TIMES THE DRILL SEEDER RATE. BROADCASTED AREAS SHALL THEN BE RE-HARROWED OR RE-RAKED USING A HARD-TIPPED RAKE TO ENSURE THAT SEEDS ARE BURIED TO AN APPROXIMATE DEPTH OF 1/4 - 1/2 INCH.
- AFTER SEEDING HAS BEEN COMPLETED, MULCH SHALL BE UNIFORMLY APPLIED AT A RATE OF 2 TONS/ACRE (4,000 LBS./ACRE). MULCH SHALL BE MECHANICALLY CRIMPED TO A DEPTH OF 2 INCHES USING A CRIMPER. MULCH SHALL BE HAND CRIMPED AND COVERED WITH A TACKIFIER IN AREAS WHERE MECHANICAL CRIMPING IS NOT POSSIBLE. WHEN SOILS PERMIT, ALL MULCH SHALL BE CRIMPED SUCH THAT THE INDIVIDUAL PIECES OF STRAW OR HAY FORM EXAGGERATED V-SHAPES PROTRUDING OUT OF THE GROUND SEVERAL INCHES.
- IN CERTAIN INSTANCES, IT MAY BE NECESSARY TO APPLY A TACKIFIER IN ORDER TO HELP WITH STRAW DISPLACEMENT. TACKIFIER SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.

**Parker COLORADO** | **CBMP** | **SMC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SURFACE ROUGHENING INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SURFACE ROUGHENING.
- DISTURBED AREAS THAT REMAIN INACTIVE FOR 15 DAYS OR MORE MUST RECEIVE SURFACE ROUGHENING OR ANOTHER APPROVED BMP FROM THE SDCM. DETERMINATION OF JOB SITE INACTIVITY IS AT THE DISCRETION OF THE TOWN'S INSPECTOR.
- FOR STEEP SLOPES (3:1 OR STEEPER), IT IS ACCEPTABLE TO "TRACK" THE SLOPES, ACCORDING TO THE CBMP DETAILS.
- SCHEDULES FOR REQUIRING STABILIZATION MAY BE MODIFIED BY THE PERMITTEE TO ALLOW FOR SPECIAL CONSIDERATIONS SUCH AS STABILIZING ACCESS AREAS AND AREAS IN CLOSE PROXIMITY TO CONTINUING CONSTRUCTION.

**SURFACE ROUGHENING INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL PROACTIVELY INSPECT THE SURFACE ROUGHENING.

**Parker COLORADO** | **CBMP** | **SR**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

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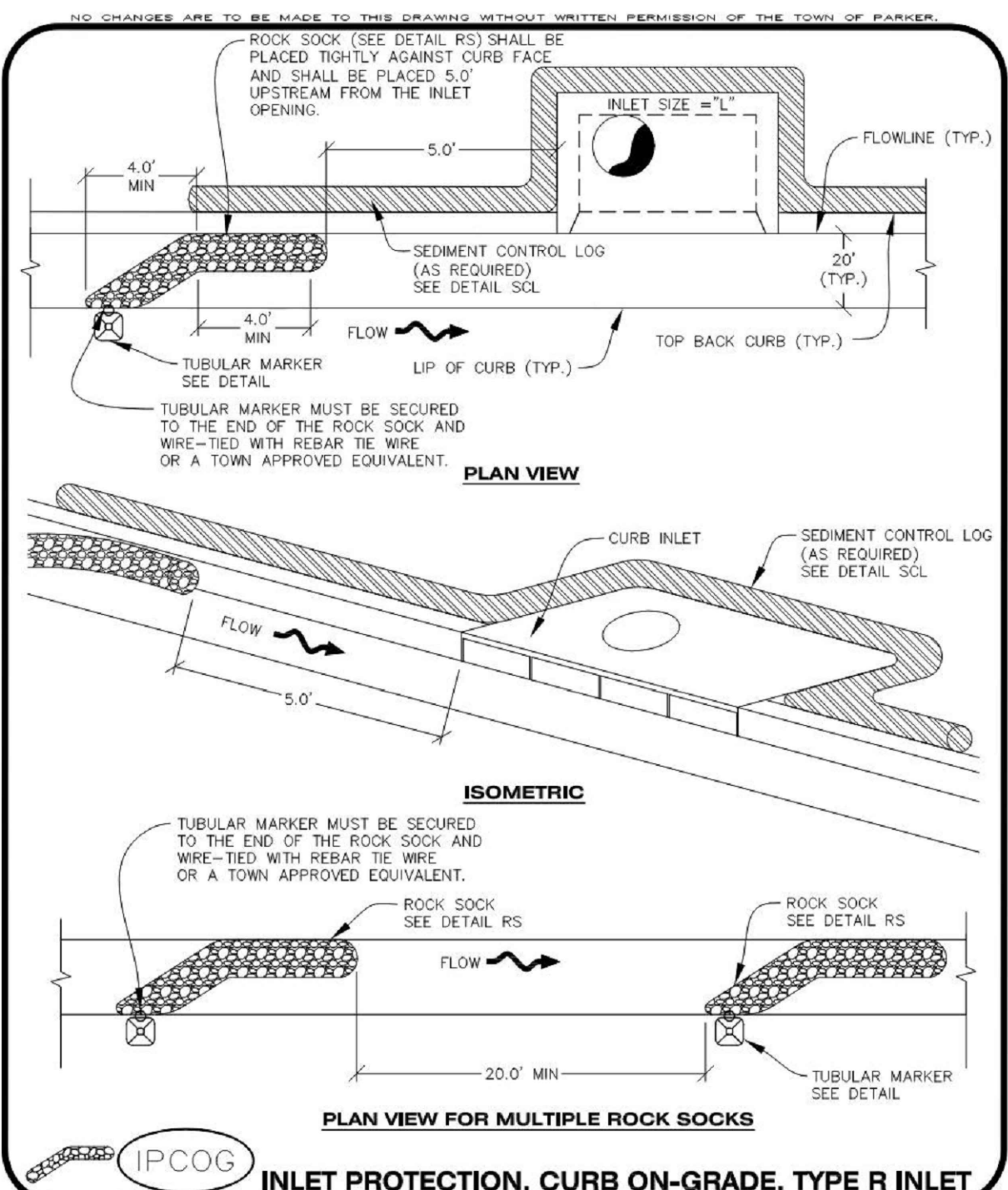
**INLET PROTECTION, CURB ON-GRADE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ON-GRADE INLET PROTECTION.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR ON-GRADE INLET PROTECTION SHALL BE ONE CONTINUOUS PIECE.
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.
- ROCK SOCK SHALL BE PLACED 5.0' UPHILL OF THE INLET OPENING.
- TUBULAR MARKER SHALL BE A MINIMUM OF 3.0' HIGH WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
- THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE APPLICABLE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE INLET.

**ON-GRADE INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ON-GRADE INLET PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ON-GRADE INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**Parker COLORADO** | **CBMP** | **IPCOG**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
Oct. 2013



**Parker COLORADO** | **CBMP** | **IPCOG**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SEEDING AND MULCHING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEEDING AND MULCHING.
- ANY SEEDED AND MULCHED AREAS THAT BECOME DAMAGED SHALL BE REPAIRED WITHIN THE TIME FRAME SPECIFIED BY THE TOWN'S INSPECTOR.

**WEED MANAGEMENT**

- ALL HERBICIDES SHALL BE APPLIED BY COMMERCIAL PESTICIDE APPLICATORS LICENSED BY THE COLORADO DEPARTMENT OF AGRICULTURE AS QUALIFIED APPLICATORS. THE CONTRACTOR SHALL FURNISH DOCUMENTATION OF SUCH LICENSING PRIOR TO HERBICIDE APPLICATION.
- HERBICIDE APPLICATION METHOD SHALL BE SUCH THAT PLANT GROWTH OUTSIDE THE DESIGNATED TREATMENT AREAS WILL NOT BE DAMAGED. ALL DAMAGE CAUSED BY IMPROPER HERBICIDE APPLICATION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- HERBICIDES SHALL BE APPLIED DURING THE APPROPRIATE SEASONS, WHEN TARGET PLANTS ARE ACTIVELY GROWING.
- AFTER THE GRASS SEED IS ESTABLISHED, APPROPRIATE HERBICIDES SHALL BE APPLIED TO CONTROL THE REMAINING WEEDS TO ENSURE A TIMELY RETURN OF THE FINANCIAL SECURITY. PROPER TIMING OF HERBICIDE APPLICATIONS ARE NECESSARY TO ACHIEVE THE SUPPRESSION OF WEED SEED PRODUCTION AND DEPLETION OF WEED ROOT MASS. ULTIMATELY, THE HERBICIDES USED SHALL BE BASED UPON THE TARGET WEEDS.
- HERBICIDE TREATMENTS SHALL CONTINUE AT AN APPROPRIATE RATE UNTIL IT IS EVIDENT THAT WEED GROWTH PRESENCE AND GROWTH IS MINIMAL AND MAY BE CONTROLLED THROUGH MOWING AND/OR ANNUAL HERBICIDE TREATMENT.

**Parker COLORADO** | **CBMP** | **SMC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
Oct. 2013

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02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

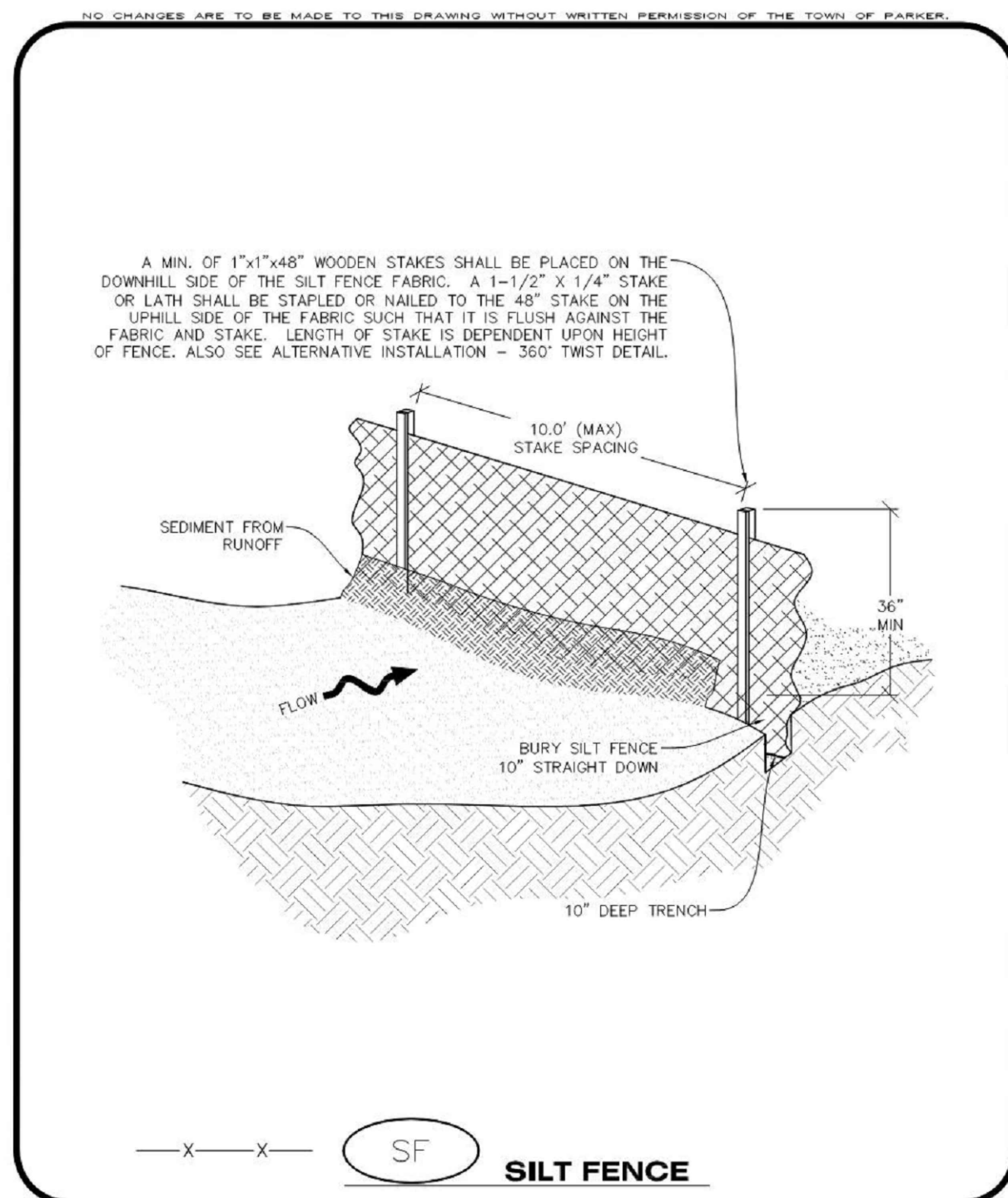
DRAWING TITLE: **EROSION CONTROL DETAILS**

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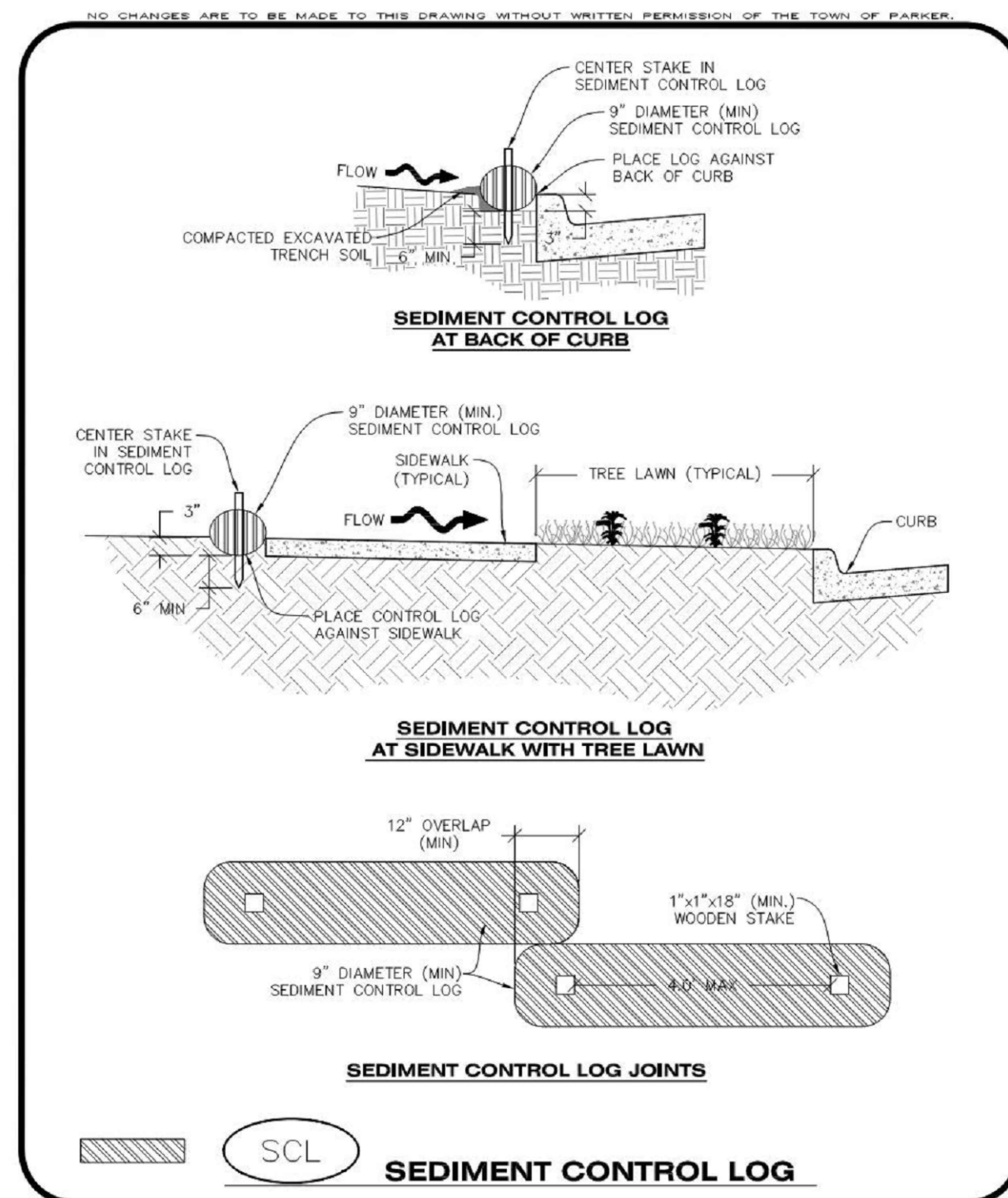
REGISTERED PROFESSIONAL ENGINEER  
47666  
04/11/24

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

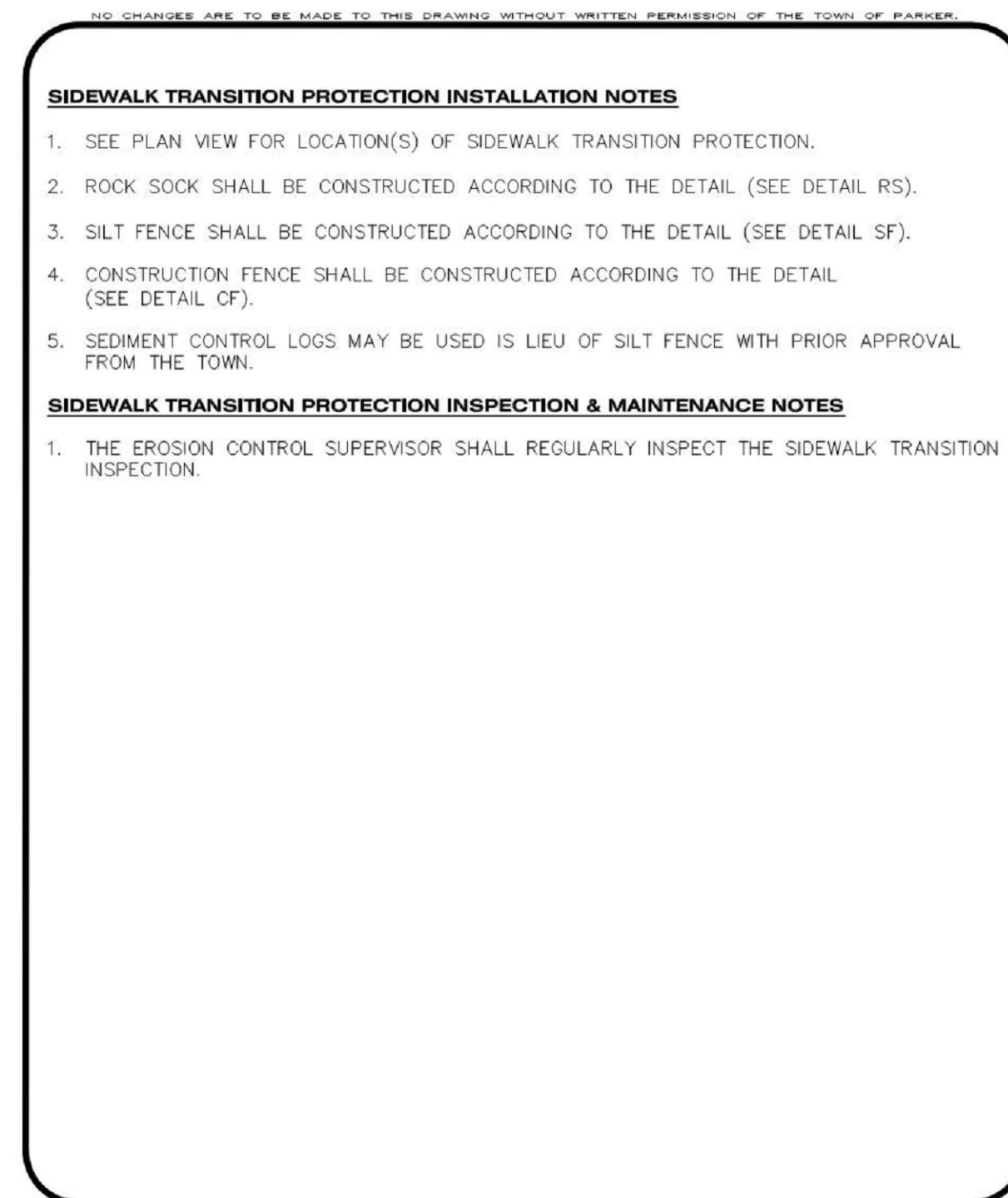
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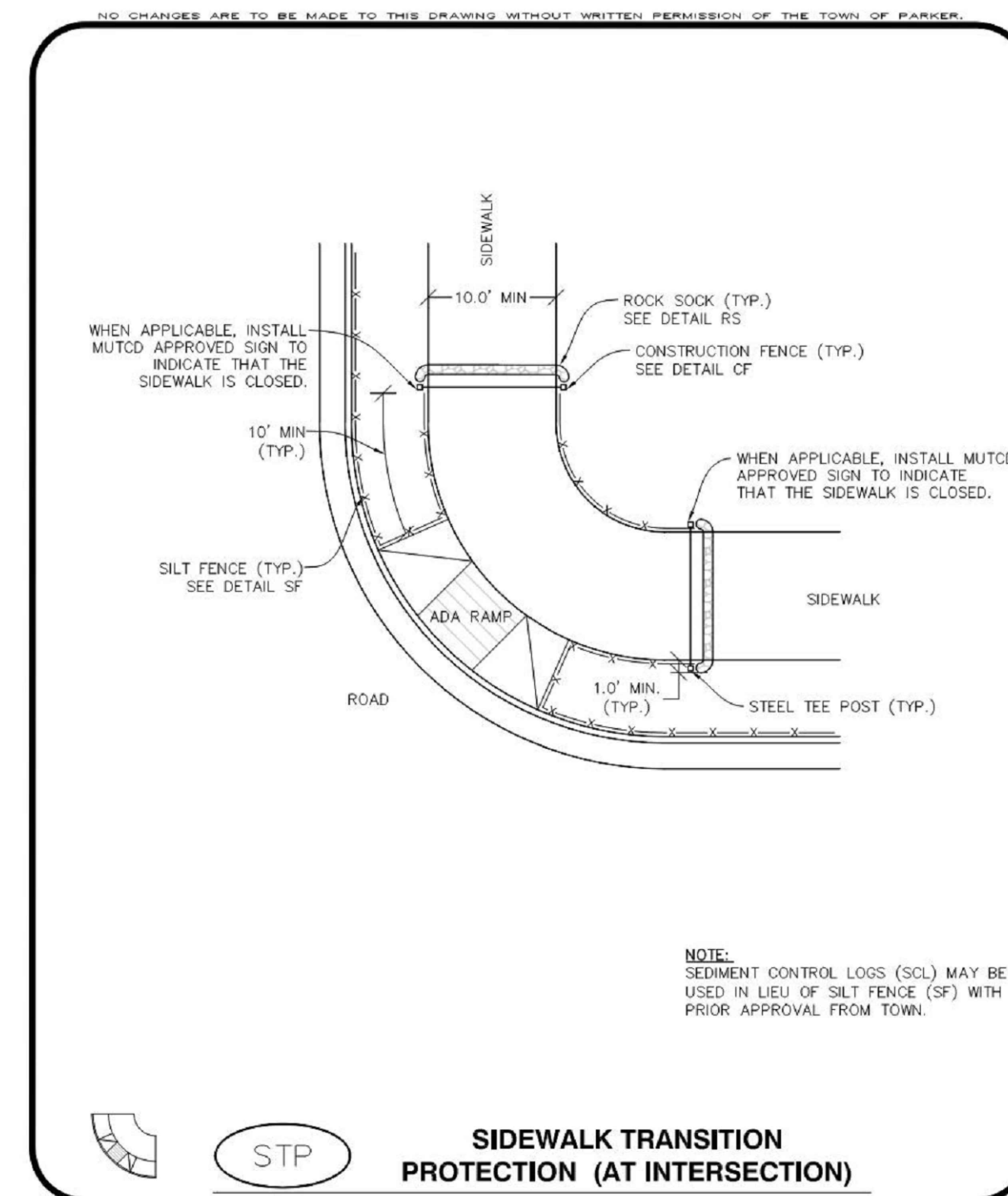
**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **SF** 1 OF 4 Oct. 2013



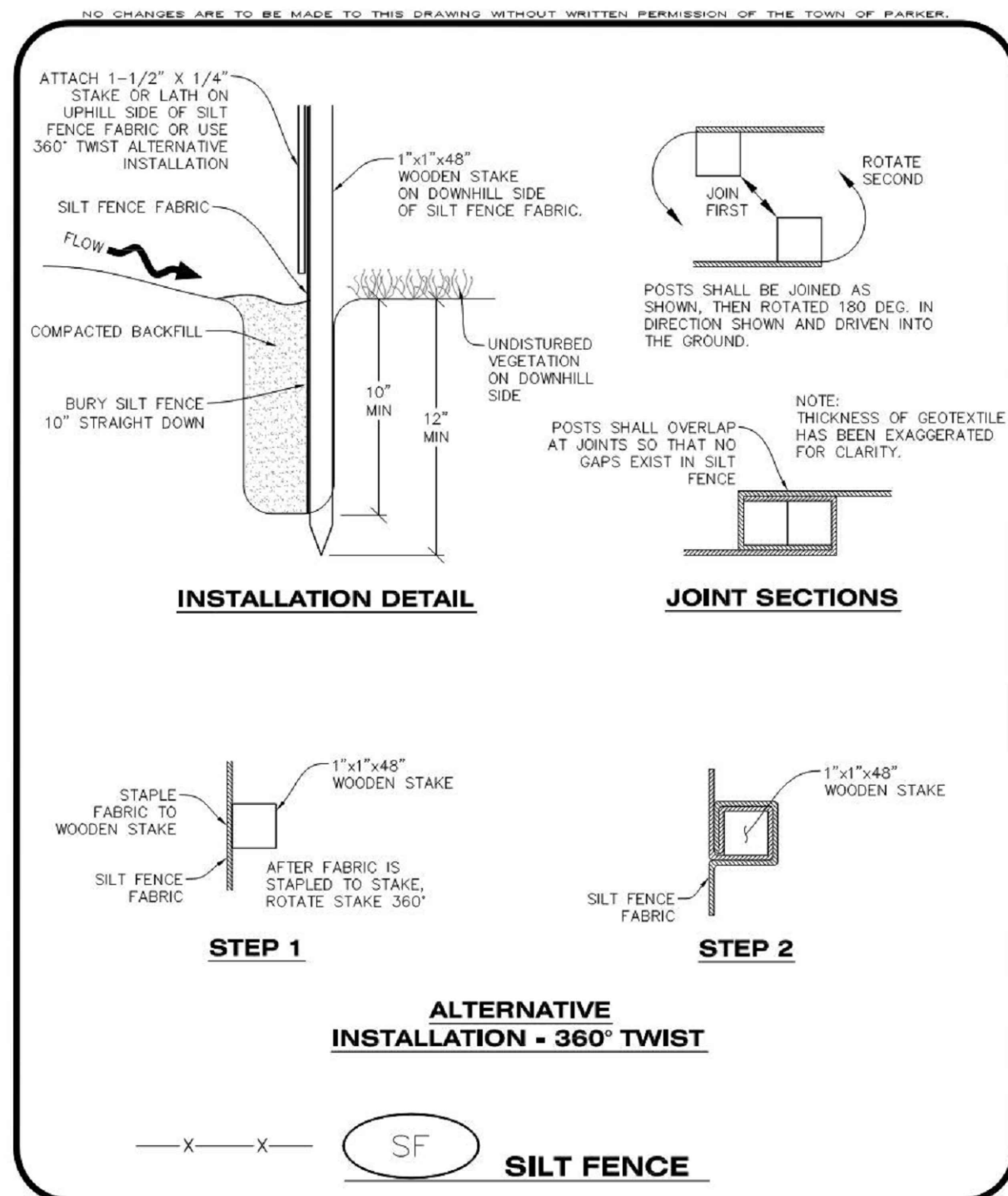
**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **SCL** 2 OF 3 Oct. 2013



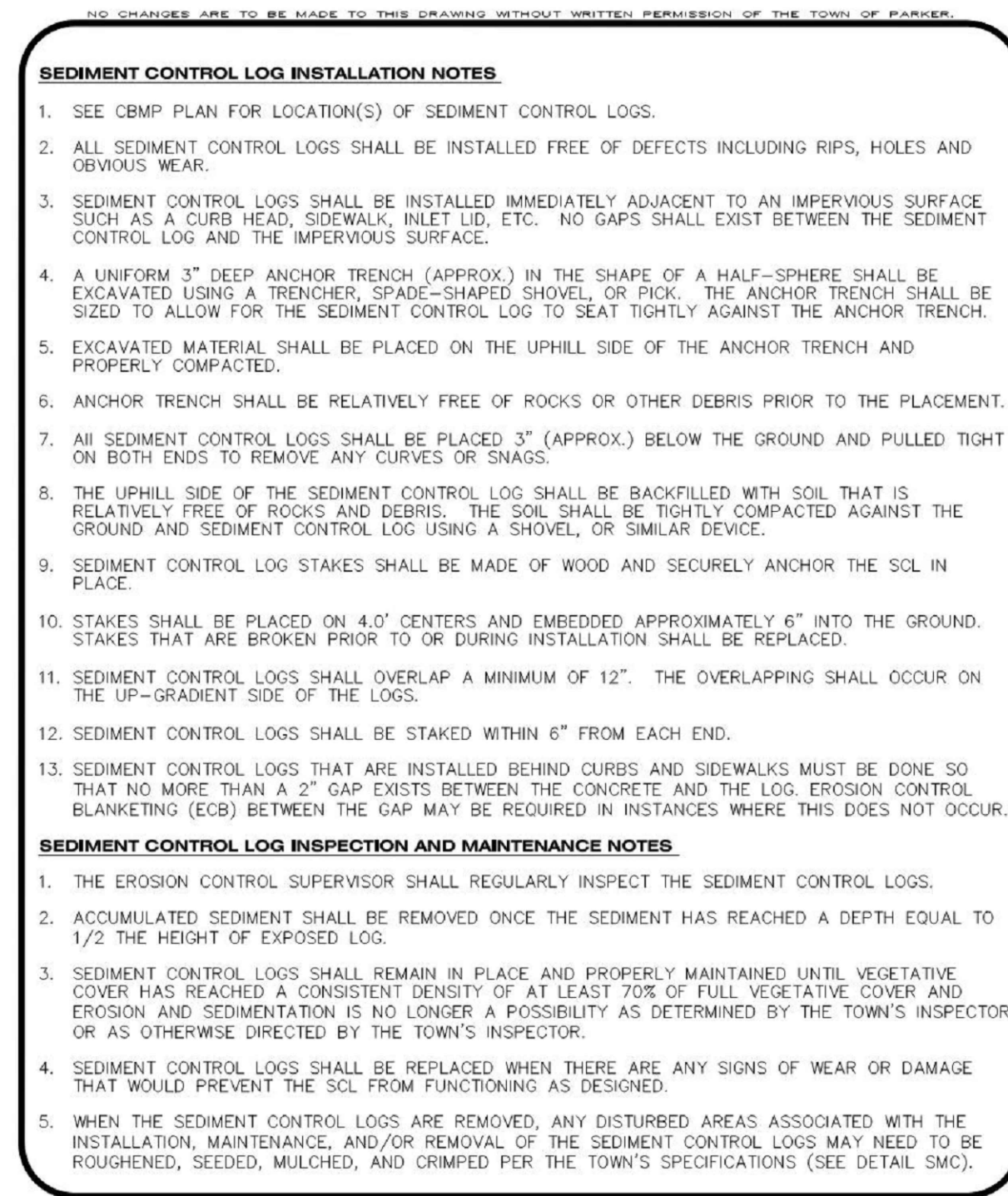
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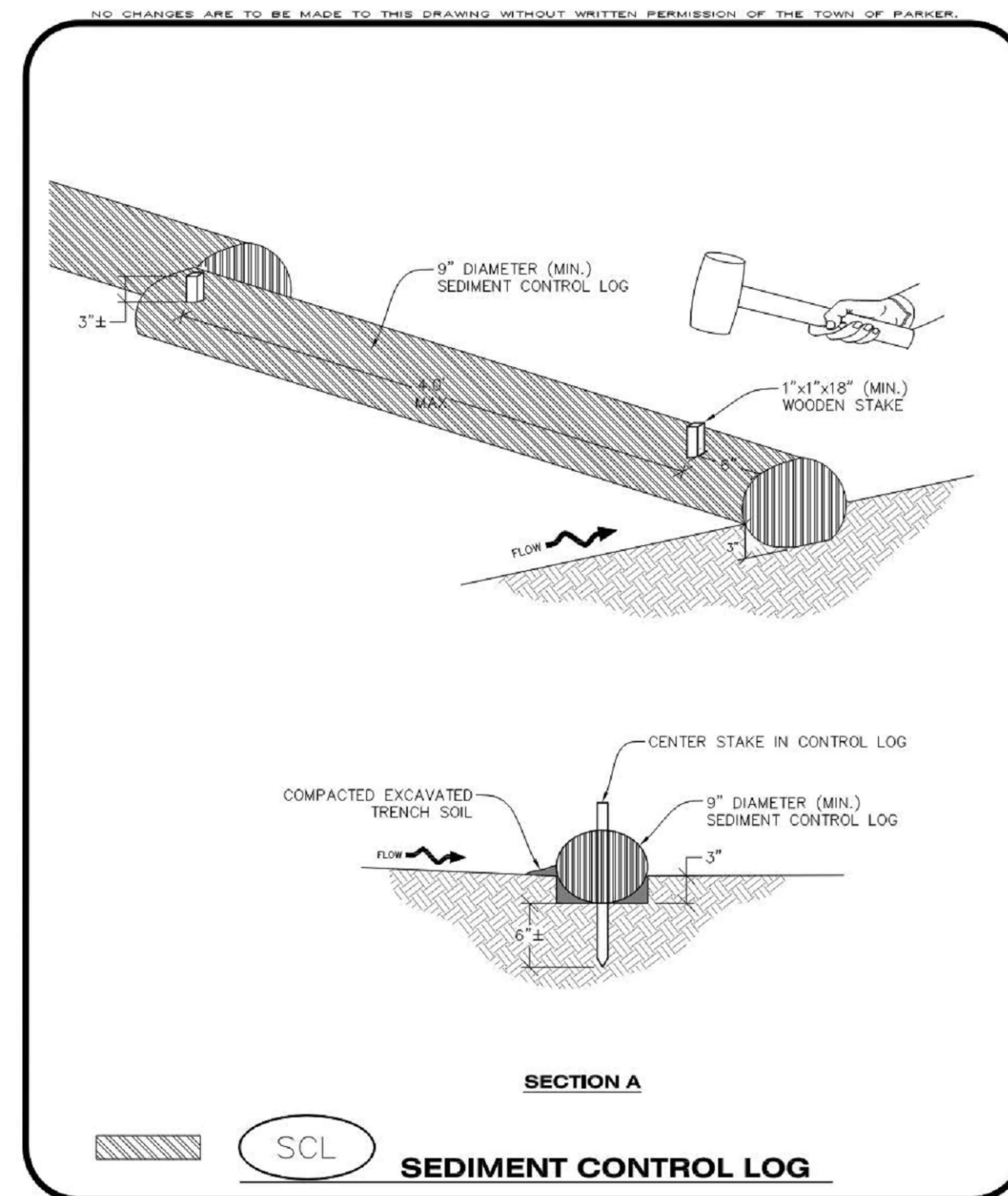
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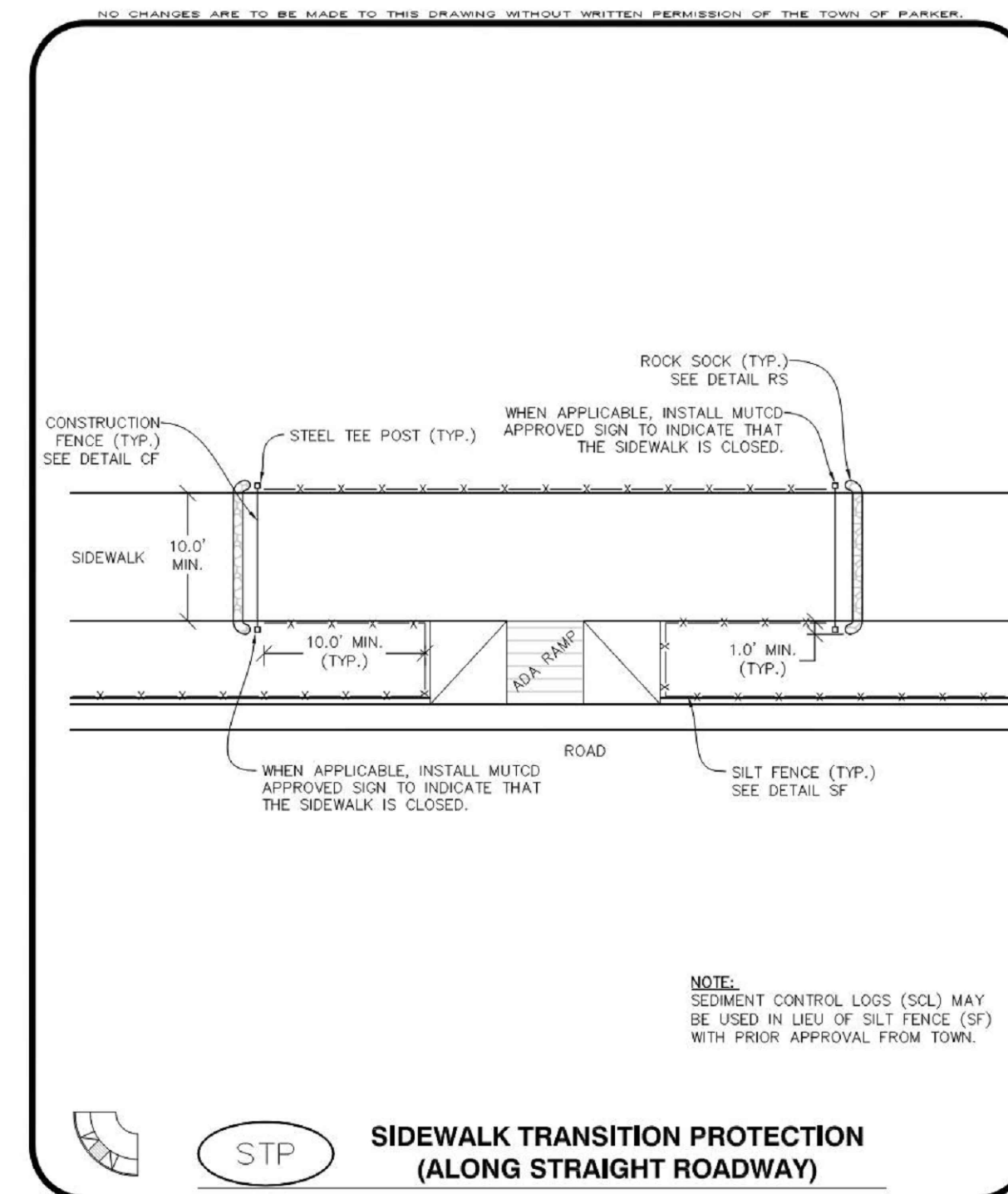
**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **SF** 2 OF 4 Oct. 2013



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **SCL** 3 OF 3 Oct. 2013



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **SCL** 1 OF 3 Oct. 2013



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **STP** 2 OF 3 Oct. 2013

NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024
3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

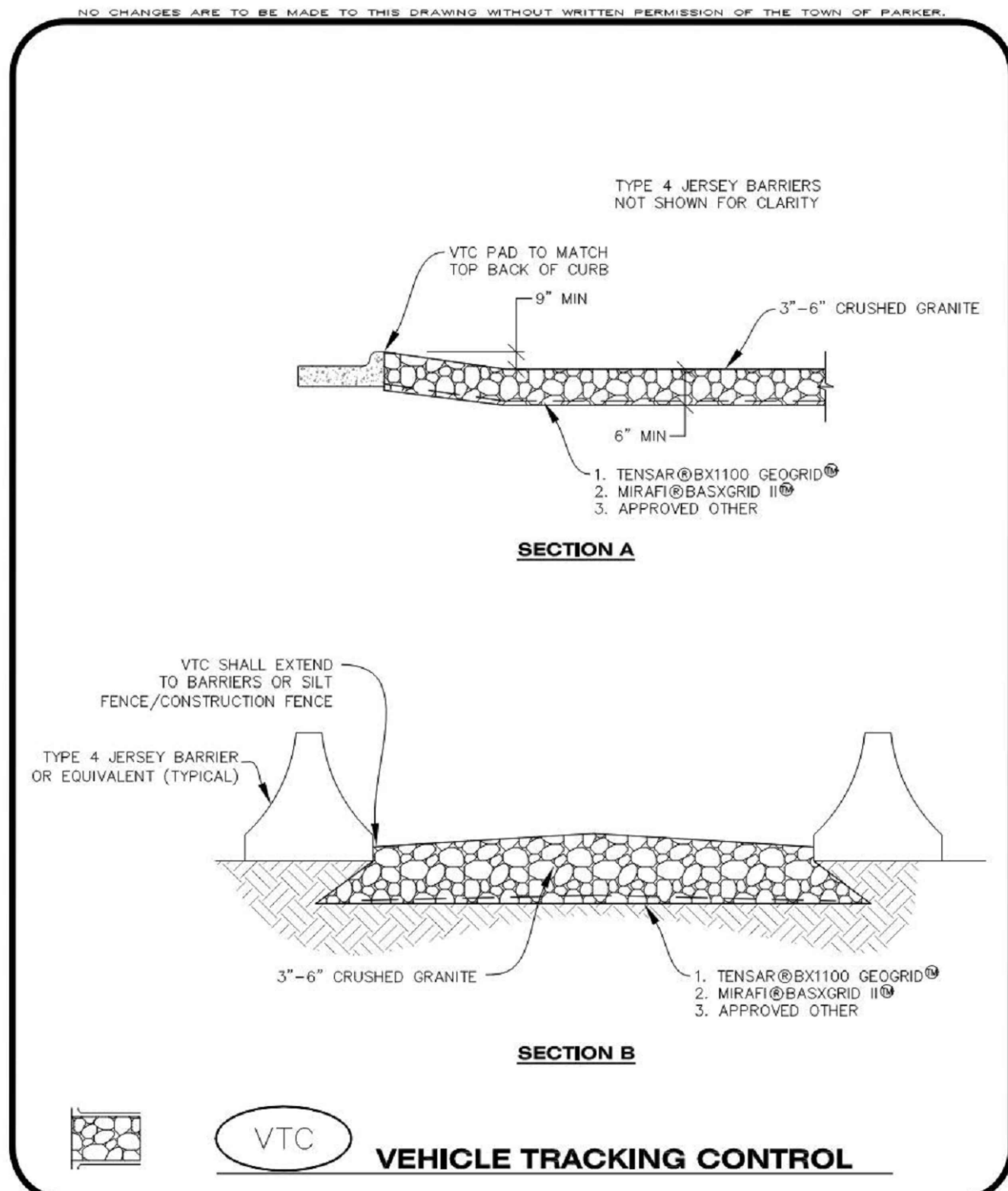
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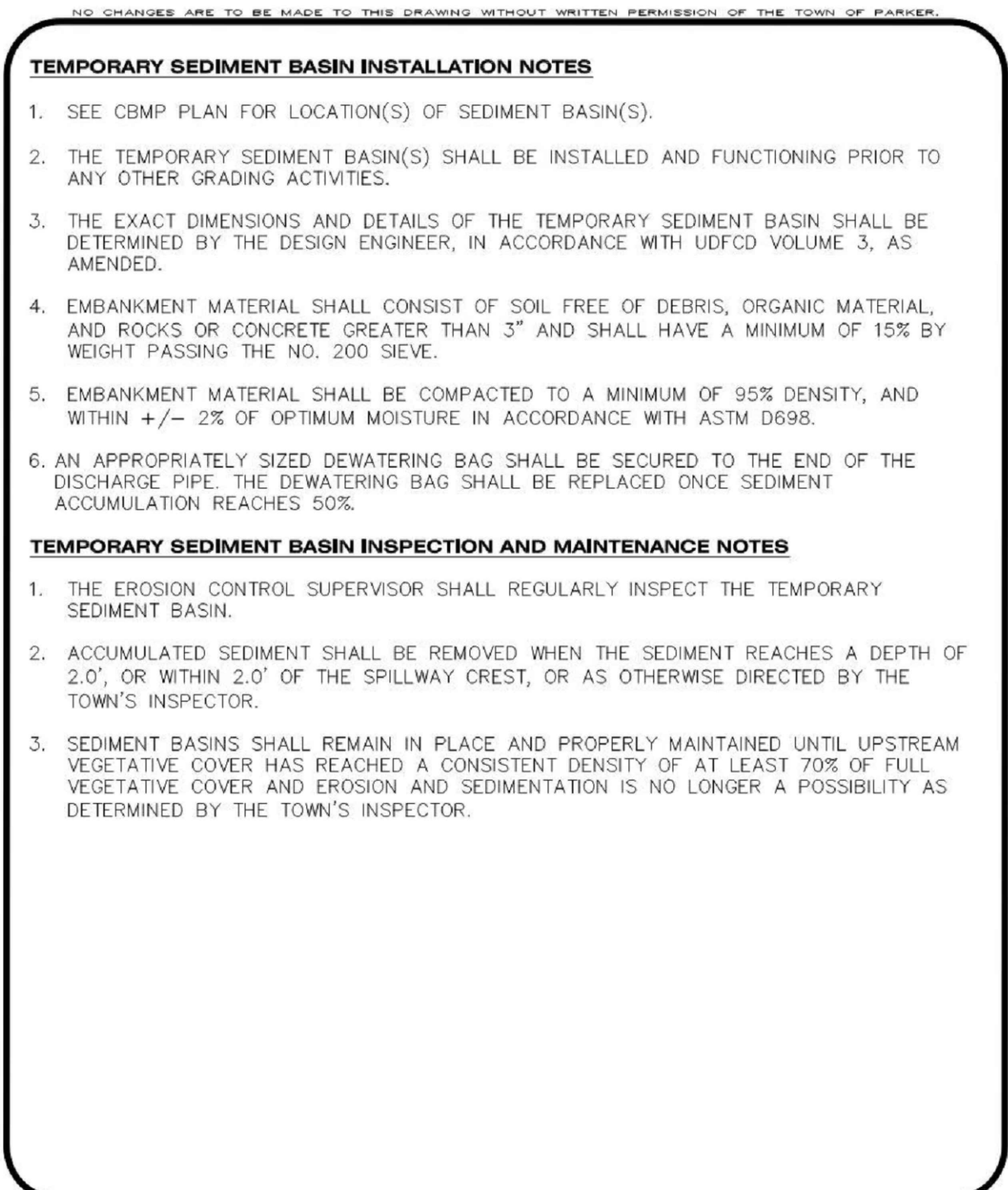
DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER: **C-258**



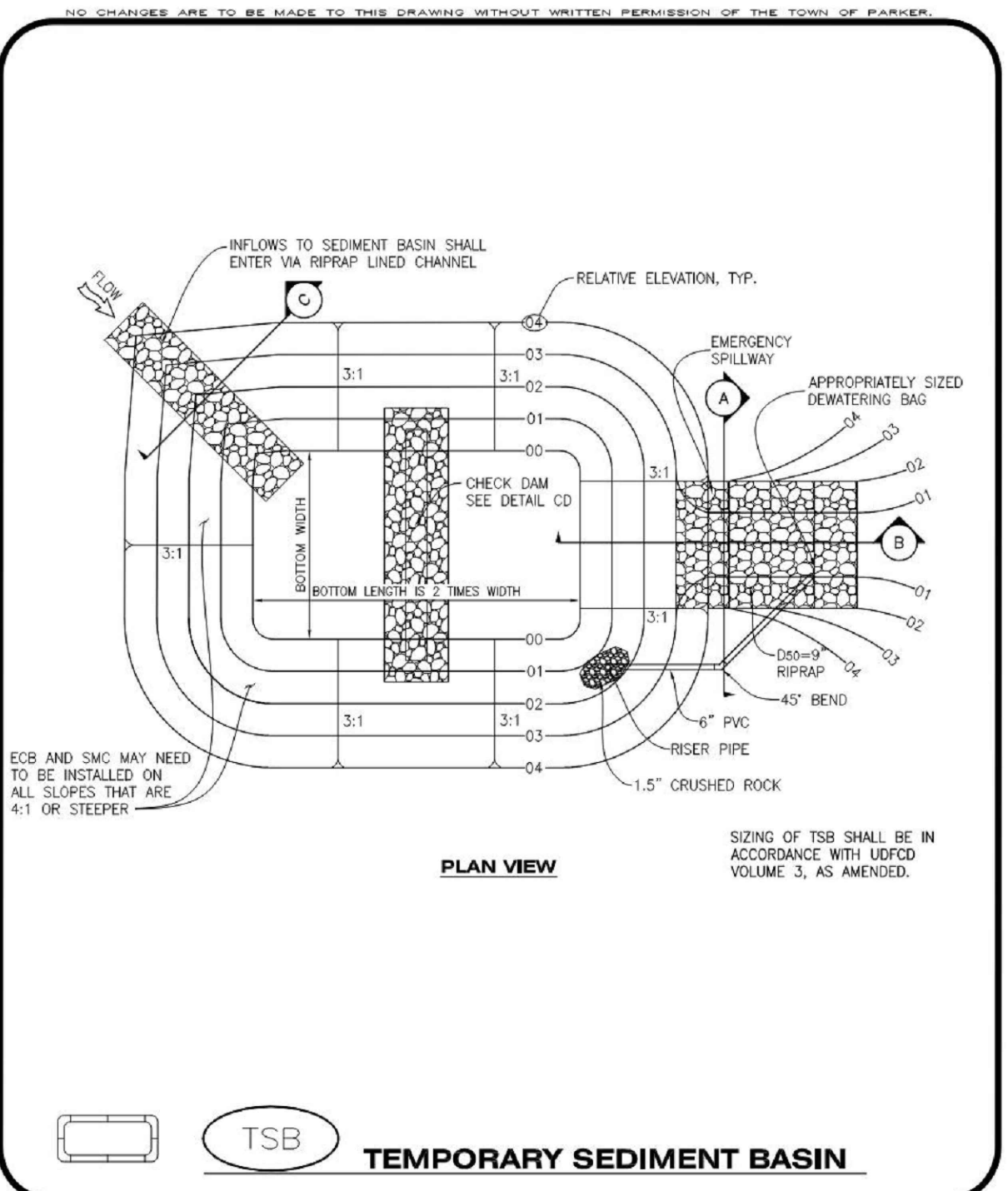
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**Parker COLORADO** | **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES | **VTC** 2 OF 3 Oct. 2013



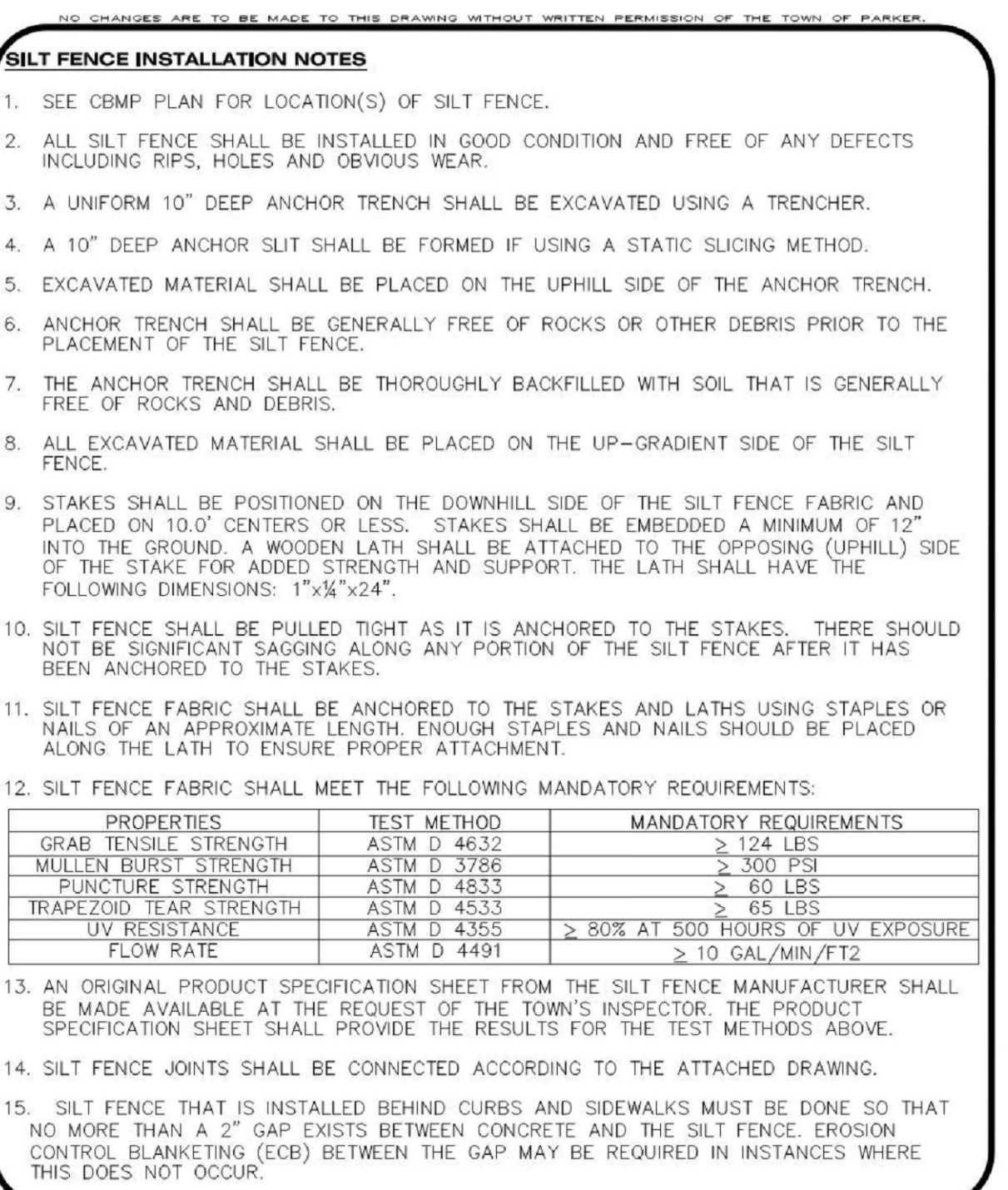
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**Parker COLORADO** | **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES | **TSB** 3 OF 3 Oct. 2013



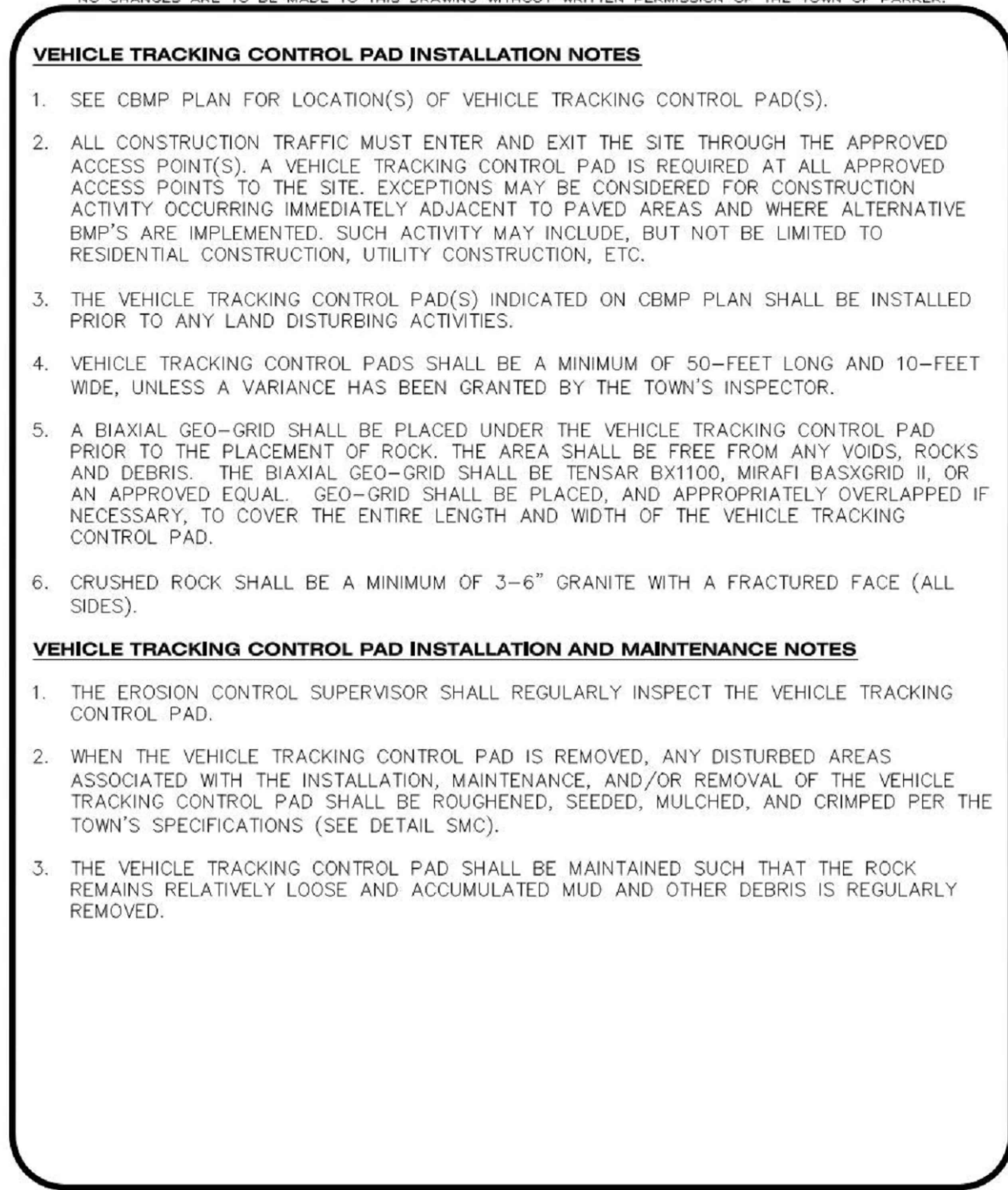
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**Parker COLORADO** | **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES | **TSB** 1 OF 3 Oct. 2013



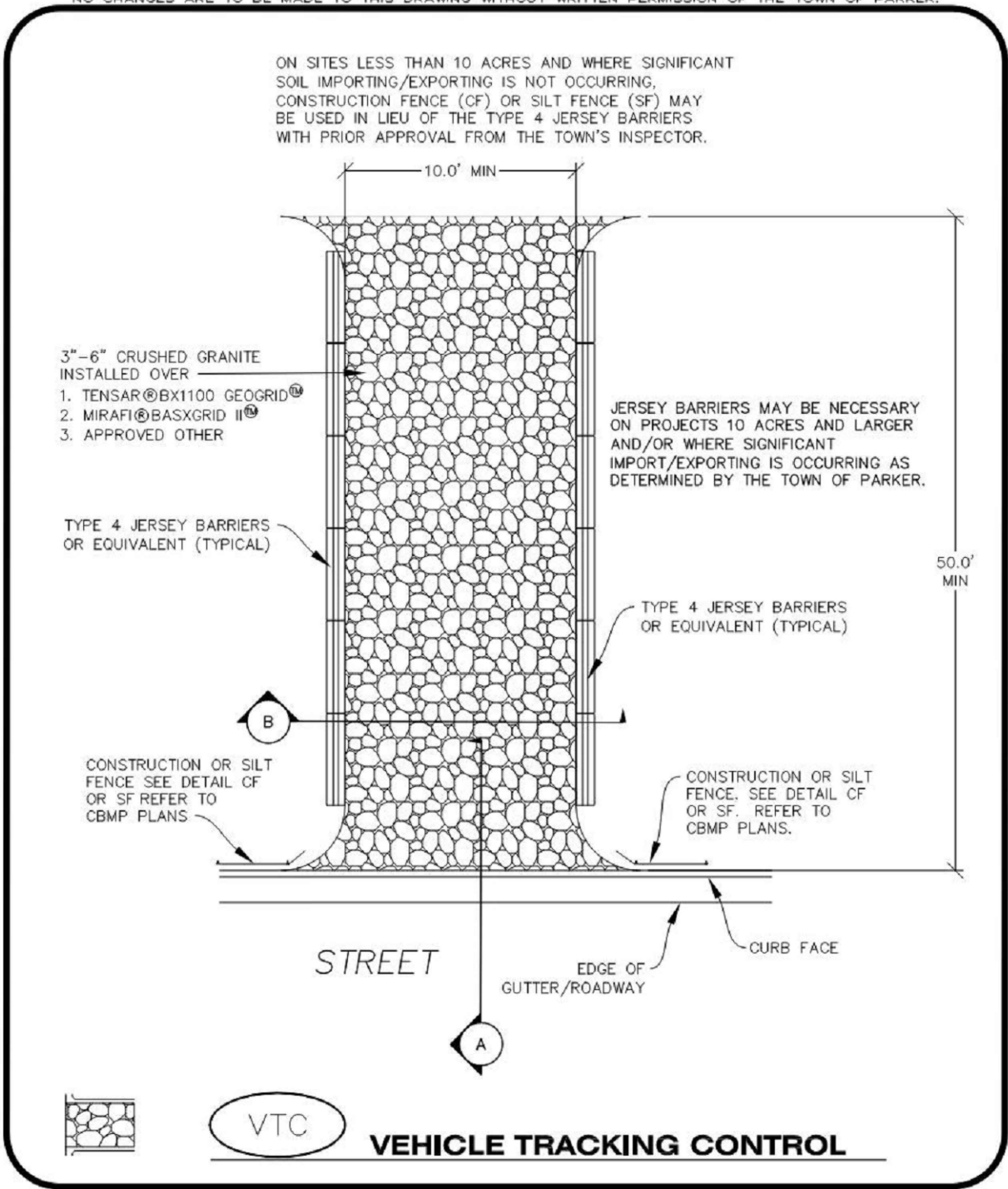
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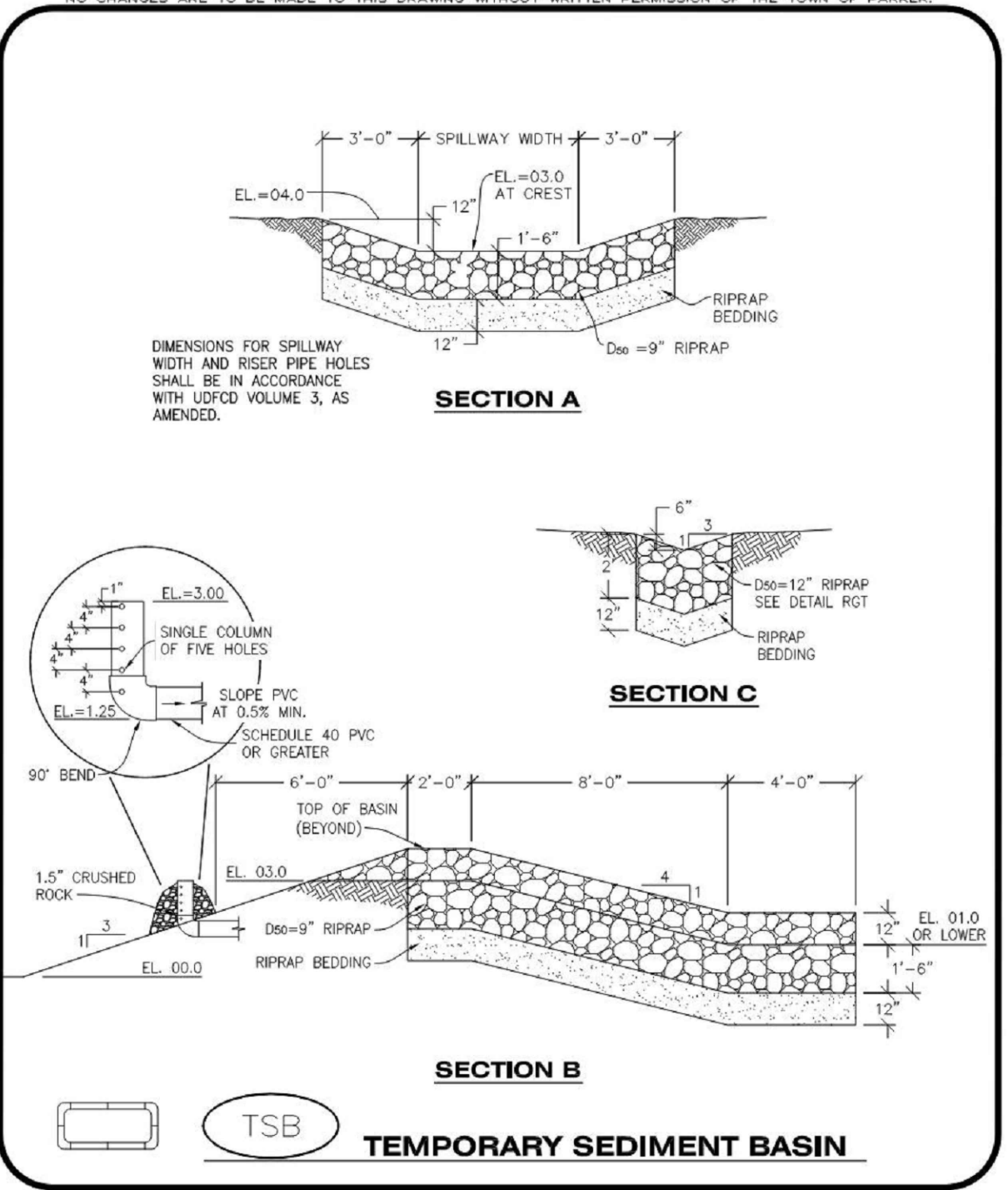
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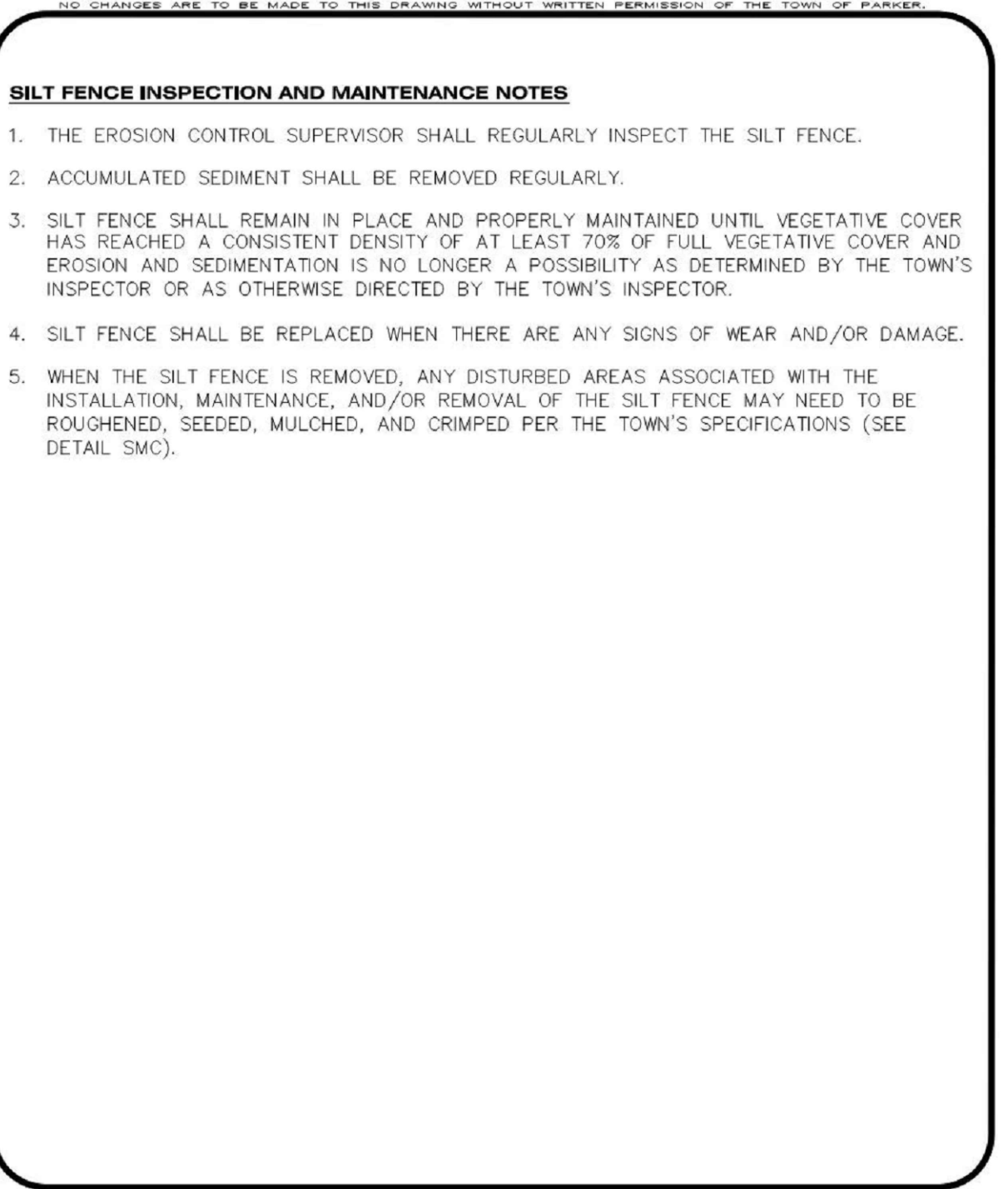
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DATE	DESCRIPTION
11/03/2023 <td>FIRST TOWN SUBMITTAL</td>	FIRST TOWN SUBMITTAL
02/05/2024 <td>SECOND TOWN SUBMITTAL</td>	SECOND TOWN SUBMITTAL
04/04/2024 <td>THIRD TOWN SUBMITTAL</td>	THIRD TOWN SUBMITTAL

NO.	NO.
1	
2	
3	

PROJECT: **PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**

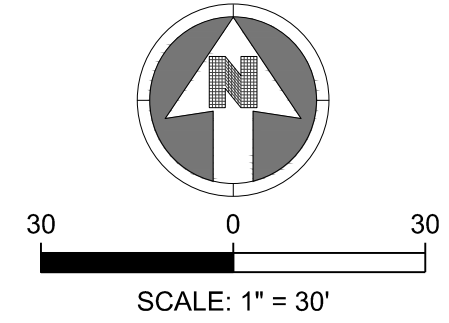
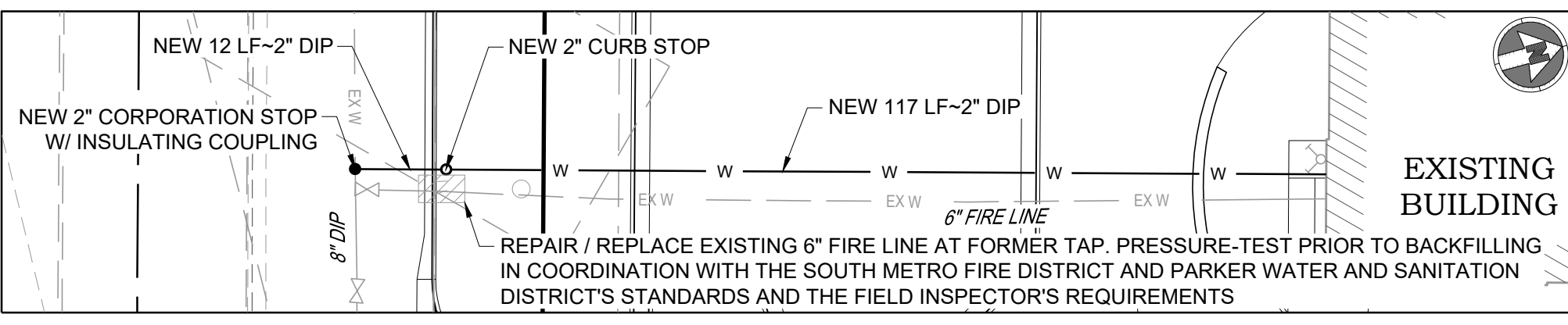
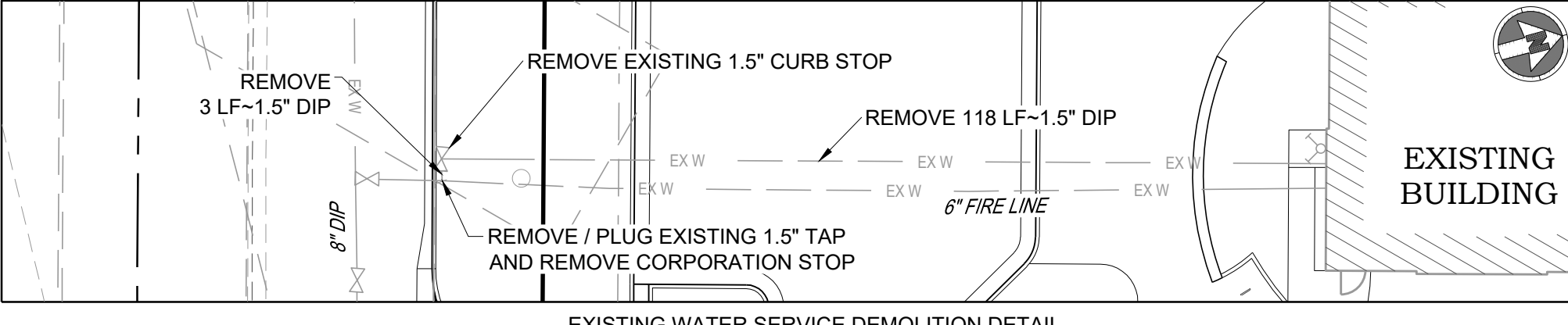
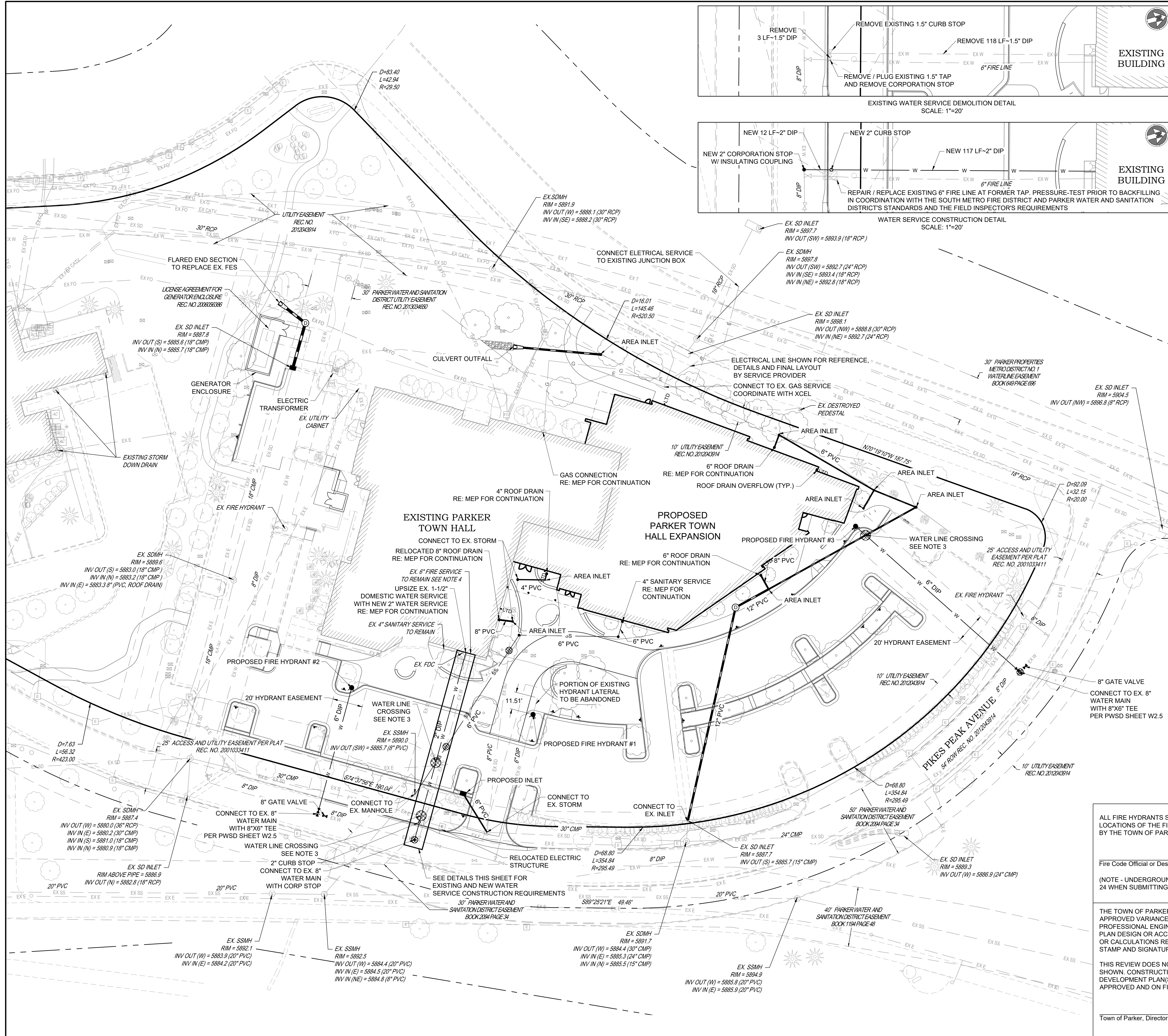
DRAWING TITLE: **EROSION CONTROL DETAILS**

FILE PATH:

PROFESSIONAL ENGINEER  
47666  
04/11/24

DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-259**



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND
  - DRY UTILITIES ARE SHOWN FOR REFERENCE ONLY. FINAL DESIGN TO BE COORDINATED WITH UTILITY PROVIDER
  - CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
  - THE SPRINKLER RISER IS ALLOWED TO ENTER NO FURTHER THAN 24-INCHES INTO THE BUILDING (AND ANY LESS THAN 12-INCHES INTO THE BUILDING).
  - TWO SEPARATE PERMITS WILL BE REQUIRED FOR THE PROJECT: ONE FOR THE BUILDING EXPANSION, AND ONE FOR THE REMODEL OF THE EXISTING BUILDING. PERMITS FOR THE FIRE SUPPRESSION, FIRE ALARM NOTIFICATION, AND ACCESS CONTROL WILL BE REQUIRED TO BE SUBMITTED FOR EACH PERMIT TYPE.

ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO WATER UTILITY STANDARDS. THE NUMBER AND LOCATIONS OF THE FIRE HYDRANTS AS SHOWN ON THE OVERALL UTILITY PLAN ARE CORRECT AS SPECIFIED BY THE TOWN OF PARKER, COMMUNITY DEVELOPMENT DEPARTMENT.

Fire Code Official or Designated Representative \_\_\_\_\_ Date \_\_\_\_\_

(NOTE - UNDERGROUND FIRE LINE (UFL) SUBMITTAL DOCUMENTS MUST MEET THE REQUIREMENTS OF NFPA 24 WHEN SUBMITTING FOR REVIEW.)

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER

Town of Parker, Director of Engineering/Public Works \_\_\_\_\_ Date \_\_\_\_\_



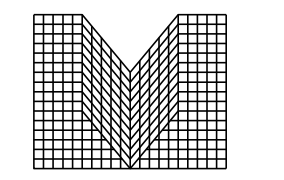
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DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-311**

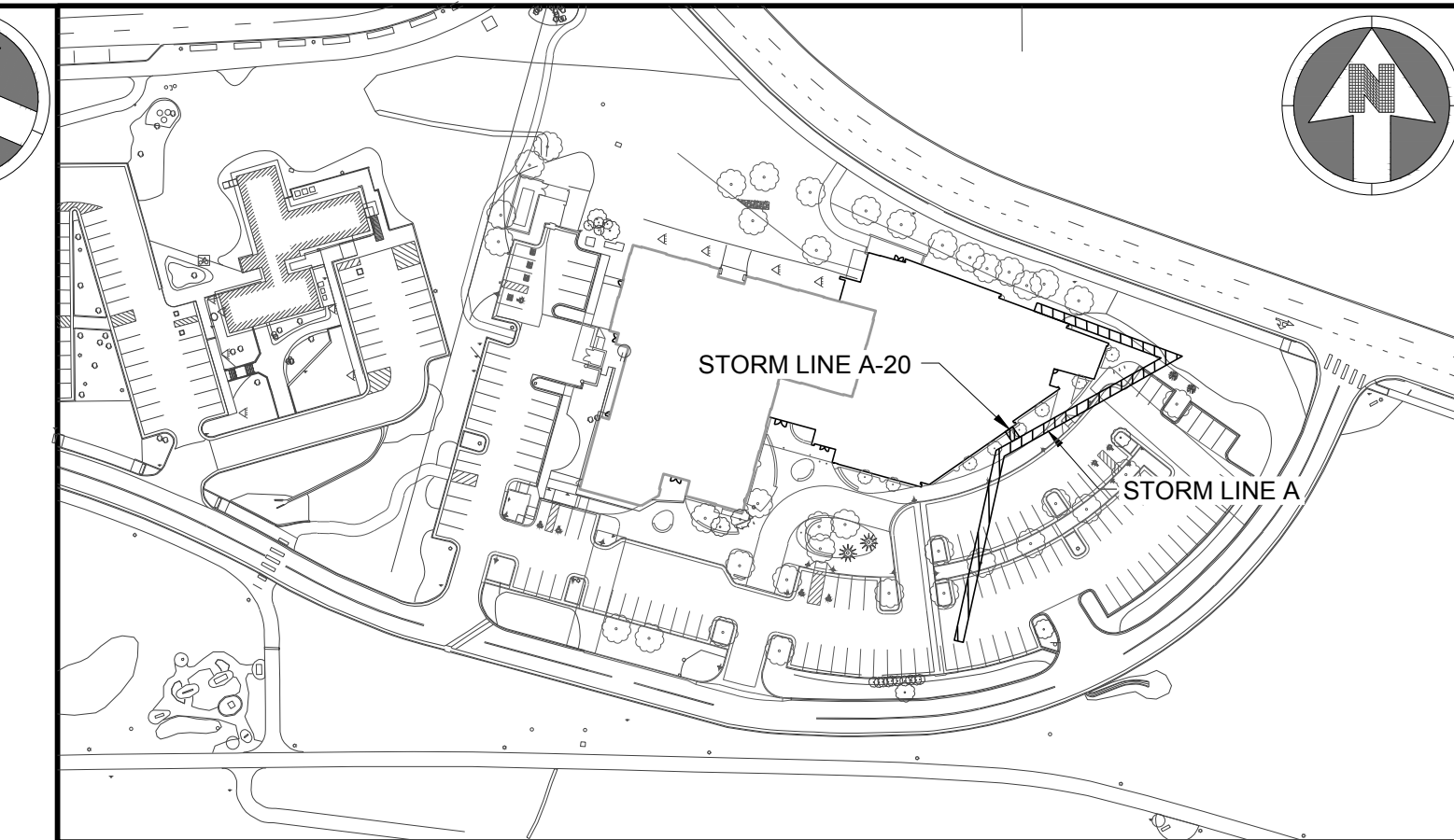


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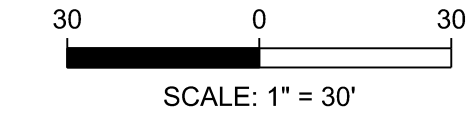
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NTS



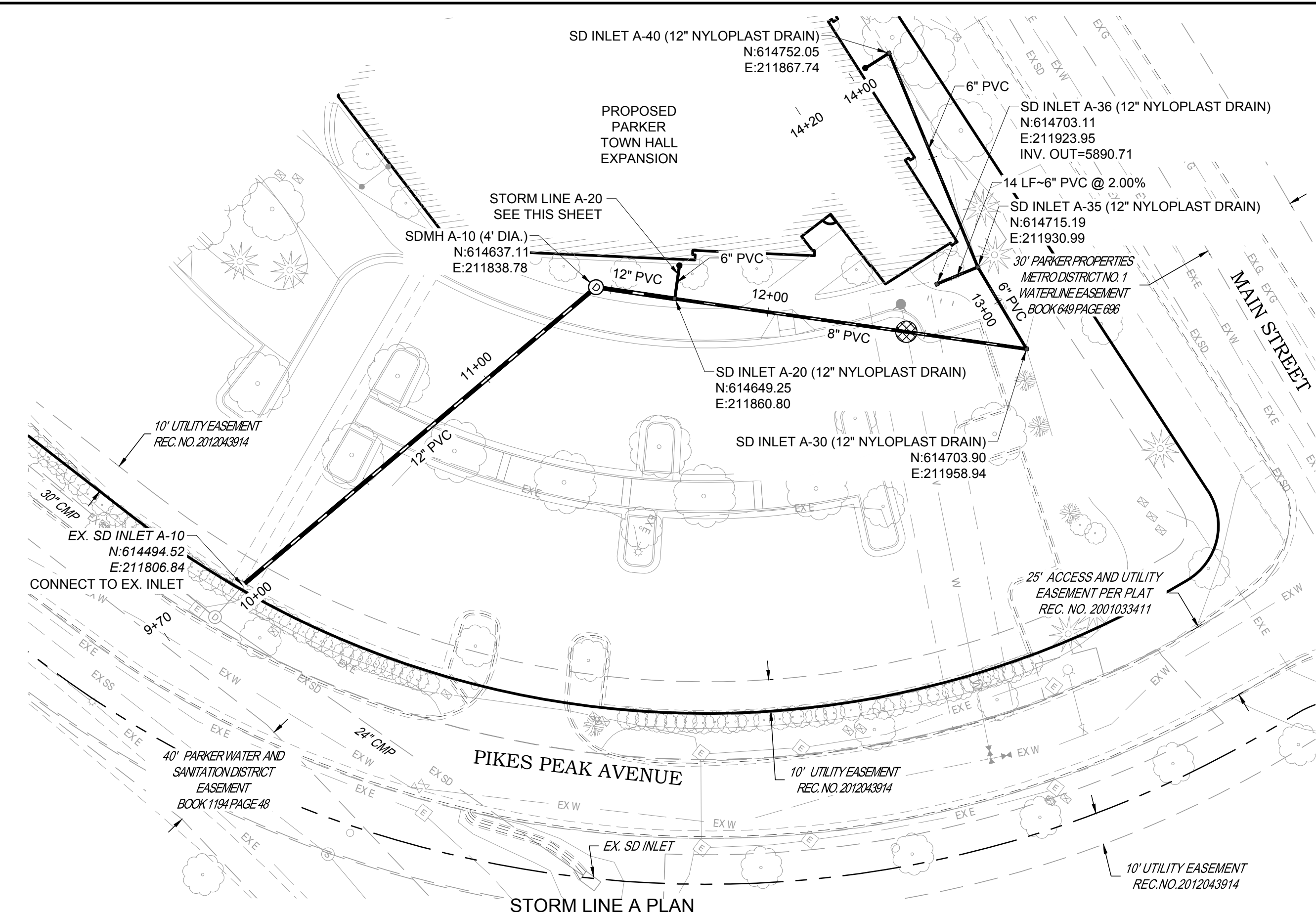
NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- ALL EXISTING DRY UTILITIES ARE ASSUMED TO BE 3' DEEP TO TOP OF PIPE.
- ALL EXISTING WATER LINES ARE ASSUMED TO BE 4.5' DEEP TO TOP OF PIPE, UNLESS OTHERWISE NOTED.
- CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
- THE CONTRACTOR SHALL POT-HOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY.
- MATCH EXISTING FES (FLARED END SECTION) INVERT, EXISTING GROUND AND FES PIPE INVERT DIFFER. FIELD VERIFY INVERT AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY. ENSURE FES REMAINS AS LOW POINT FOR EXISTING DRAINAGE SWALES.

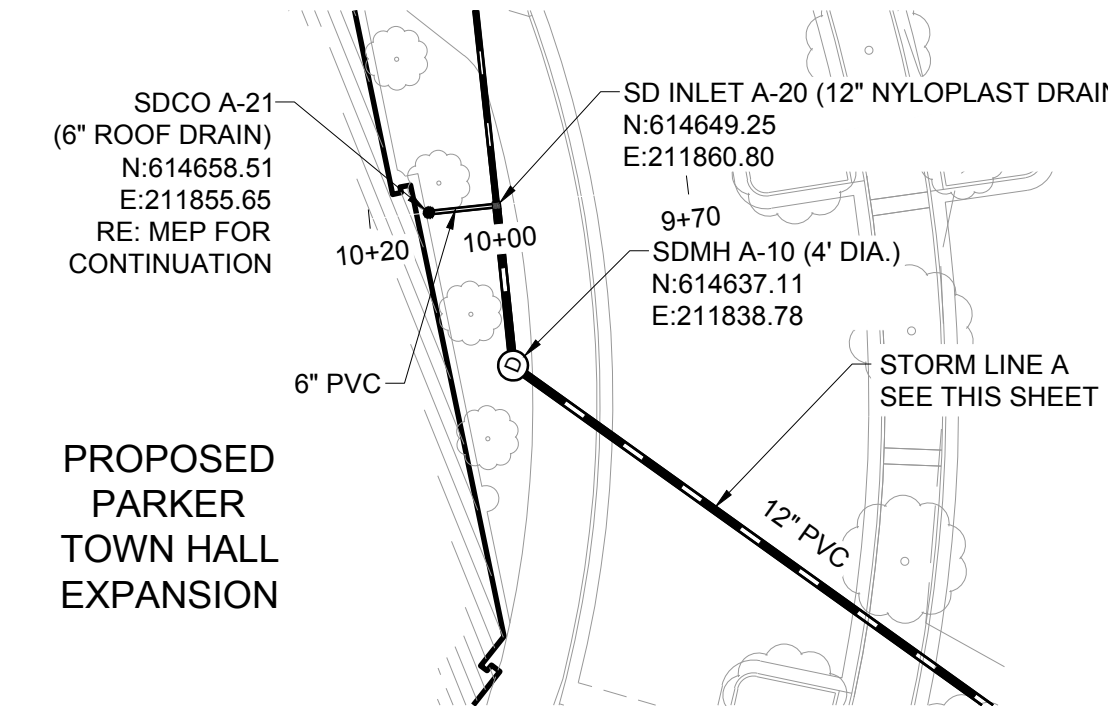
LEGEND:



UTILITY CROSSING

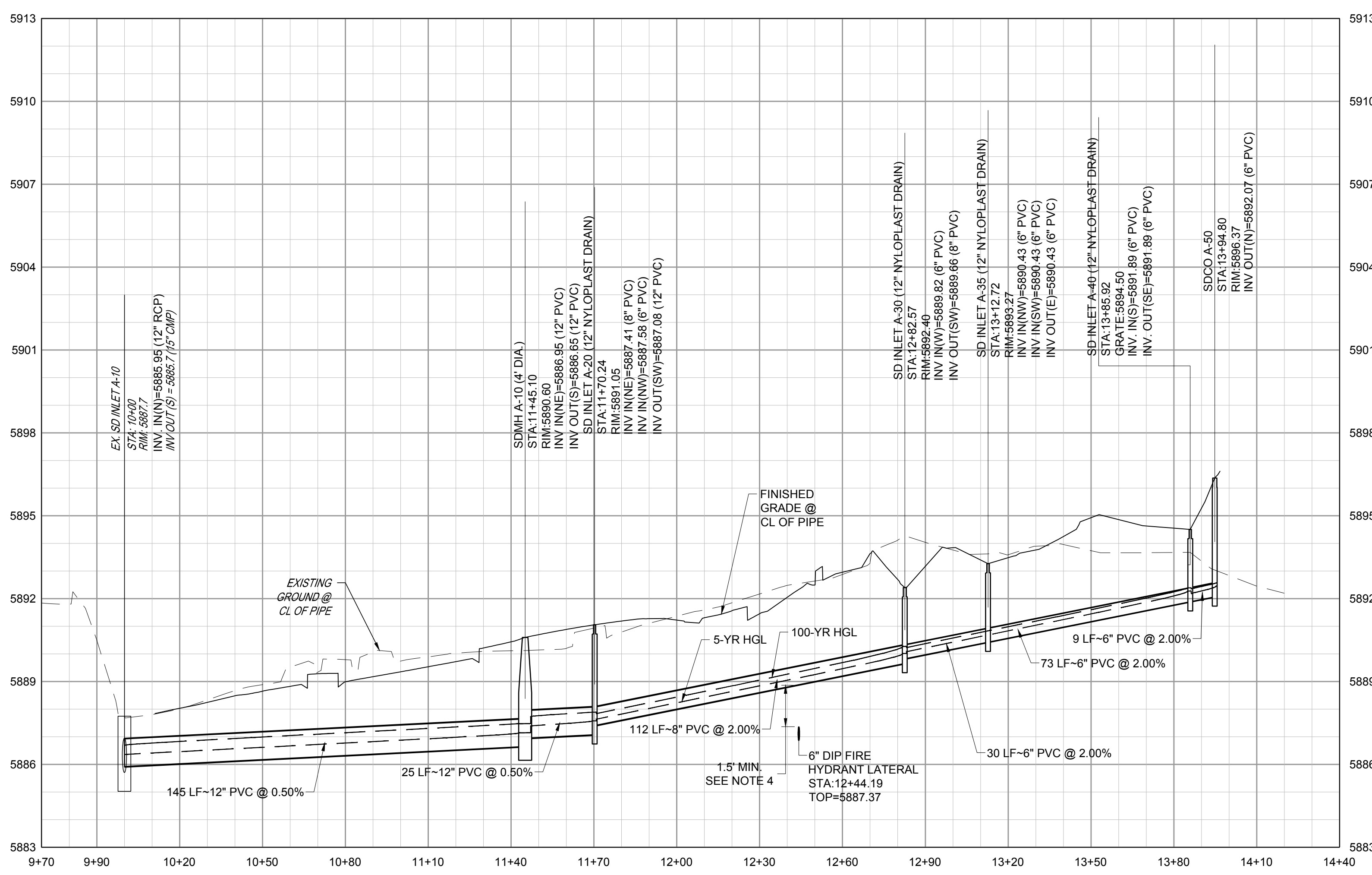


STORM LINE A PLAN



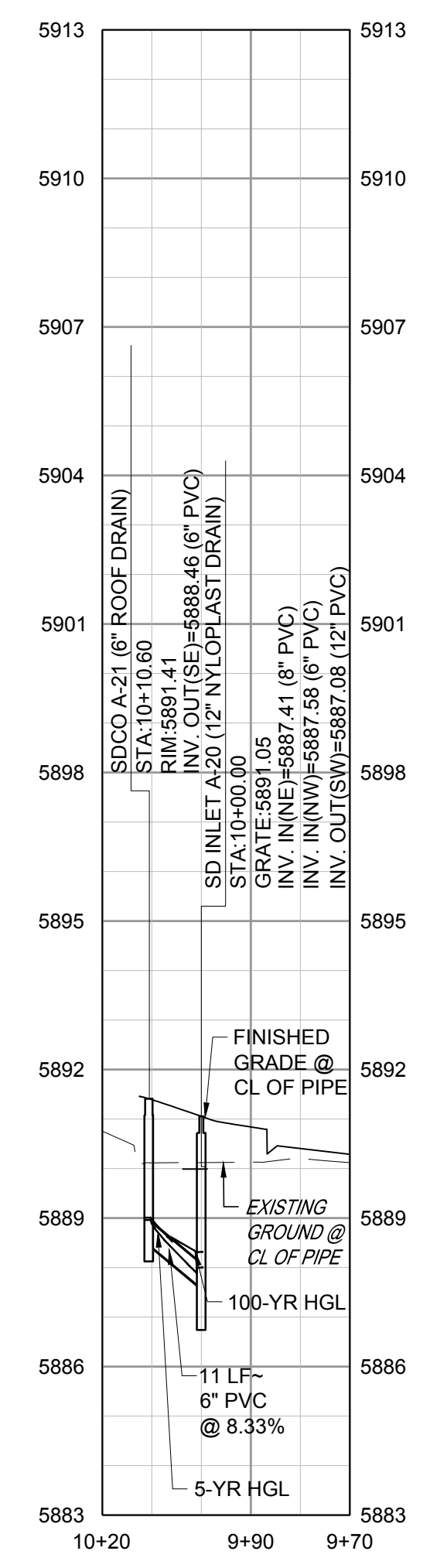
STORM LINE A-20 PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE A PROFILE

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE A-20 PROFILE

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Town of Parker, Director of Engineering/Public Works Date

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

DRAWING TITLE: STORM PLAN AND PROFILES

NO. 1 2 3

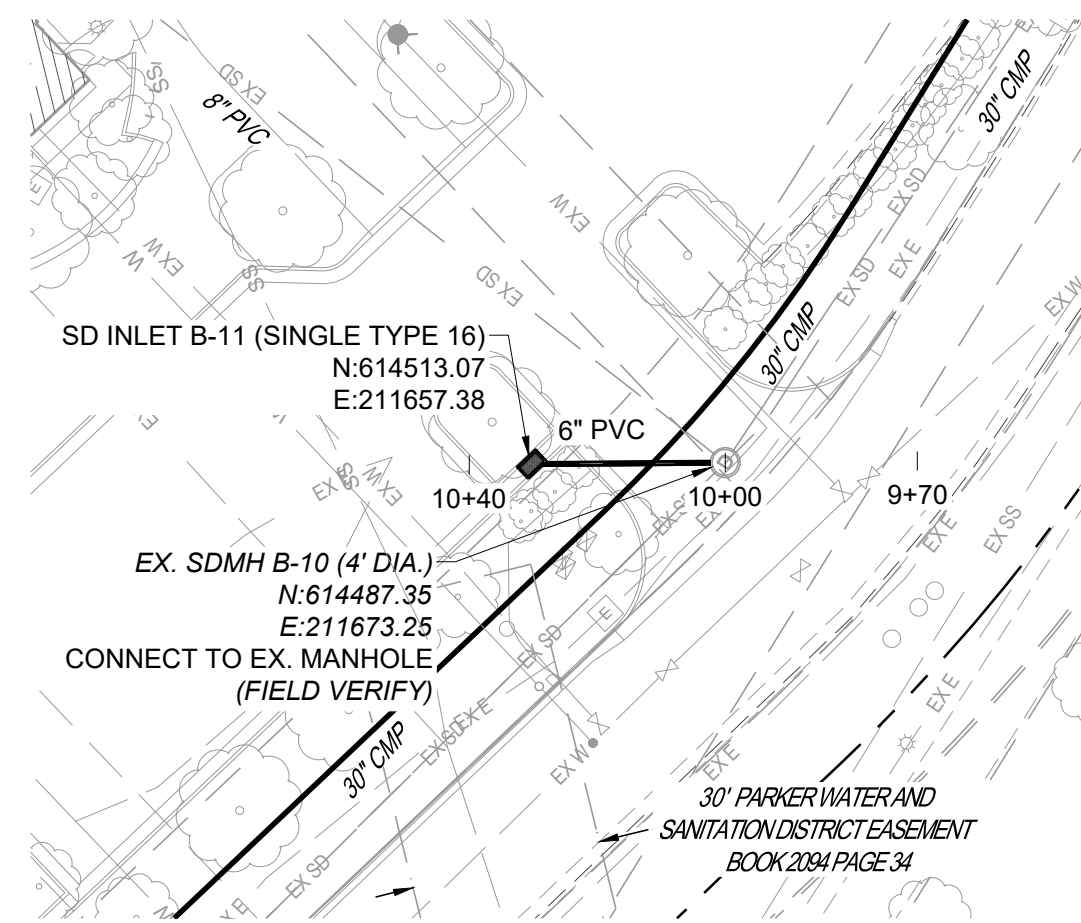


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:

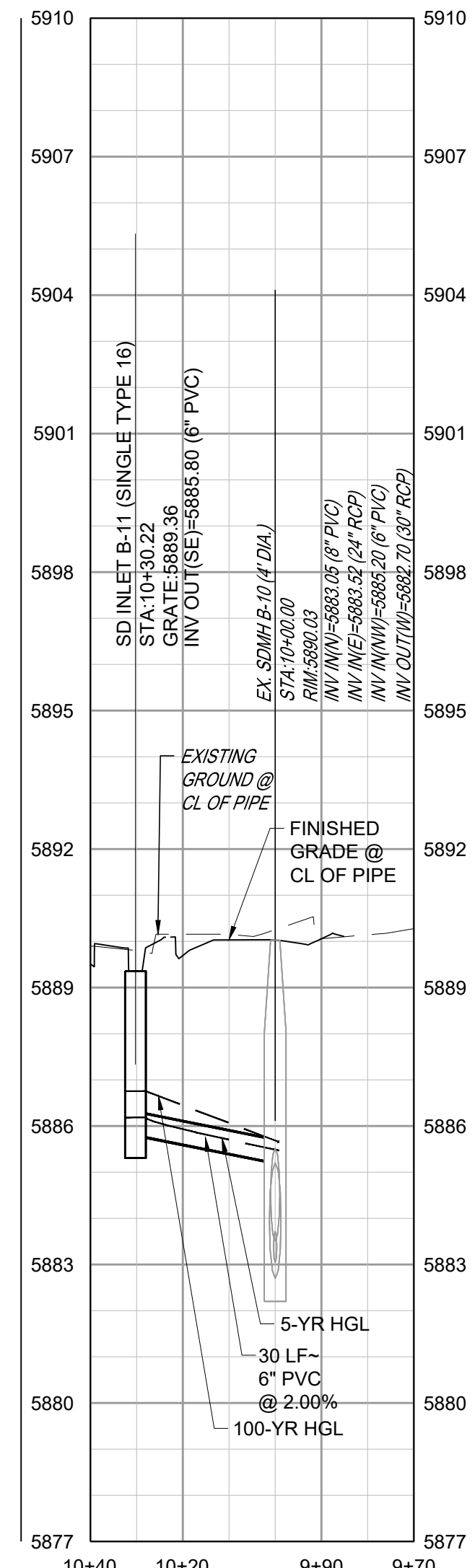
C-331

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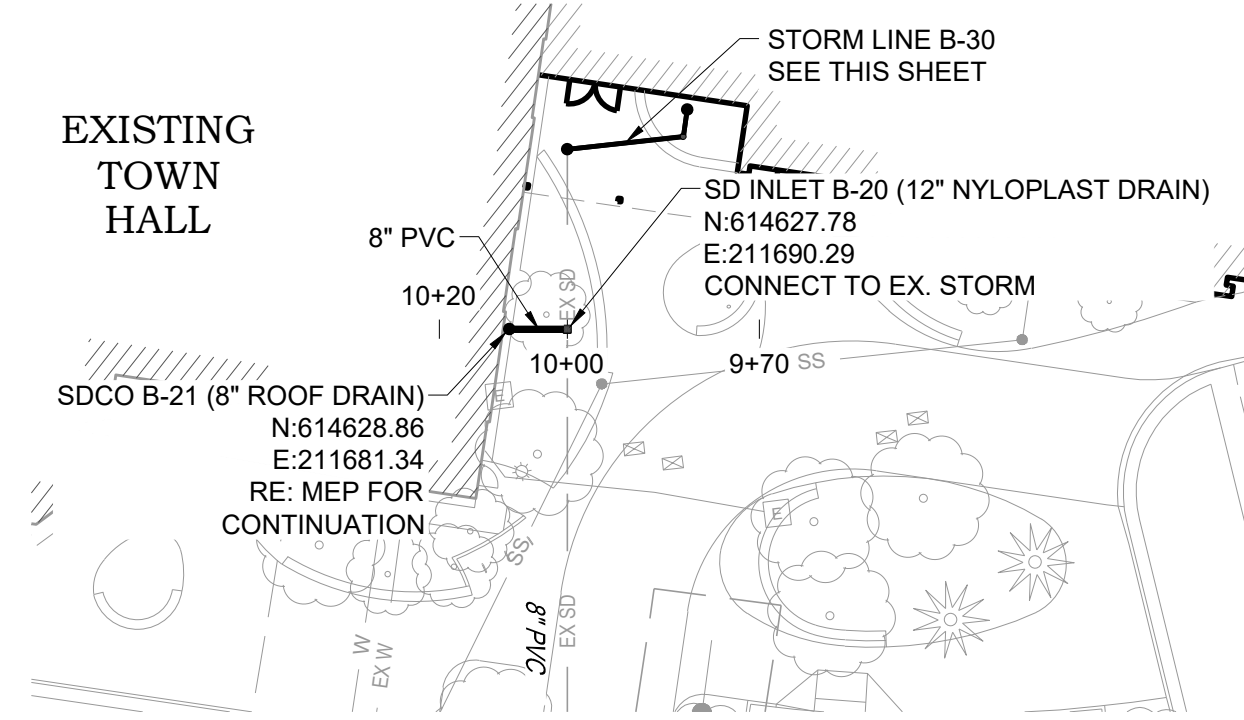


STORM LINE B-10 PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'

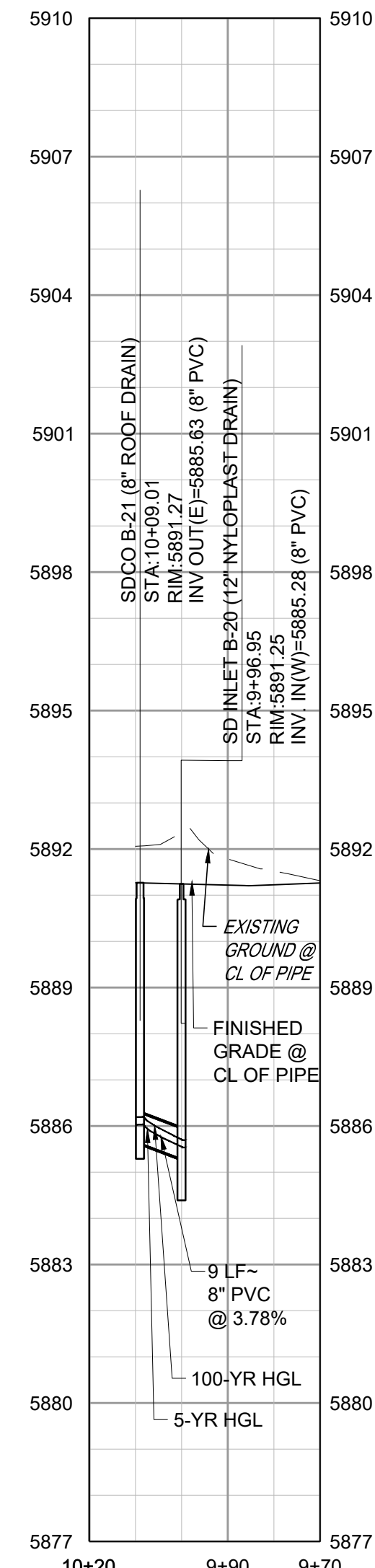


STORM LINE B-10 PROFILE

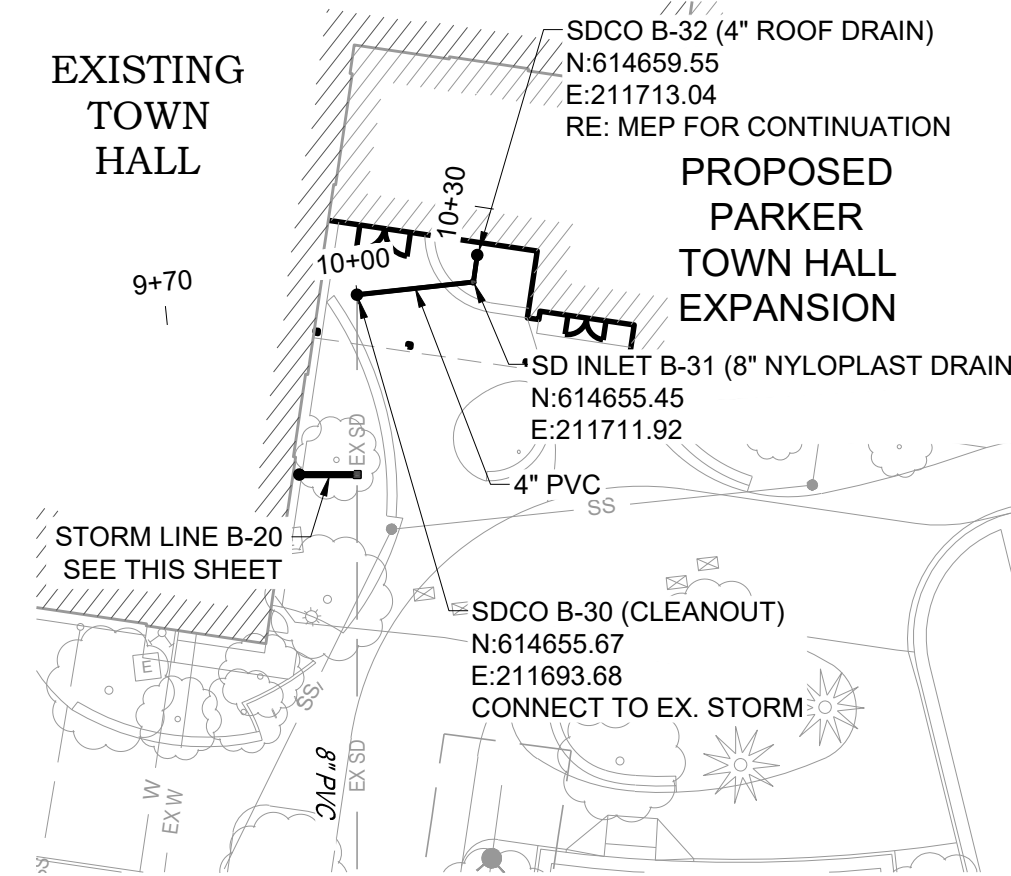


STORM LINE B-20 PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'

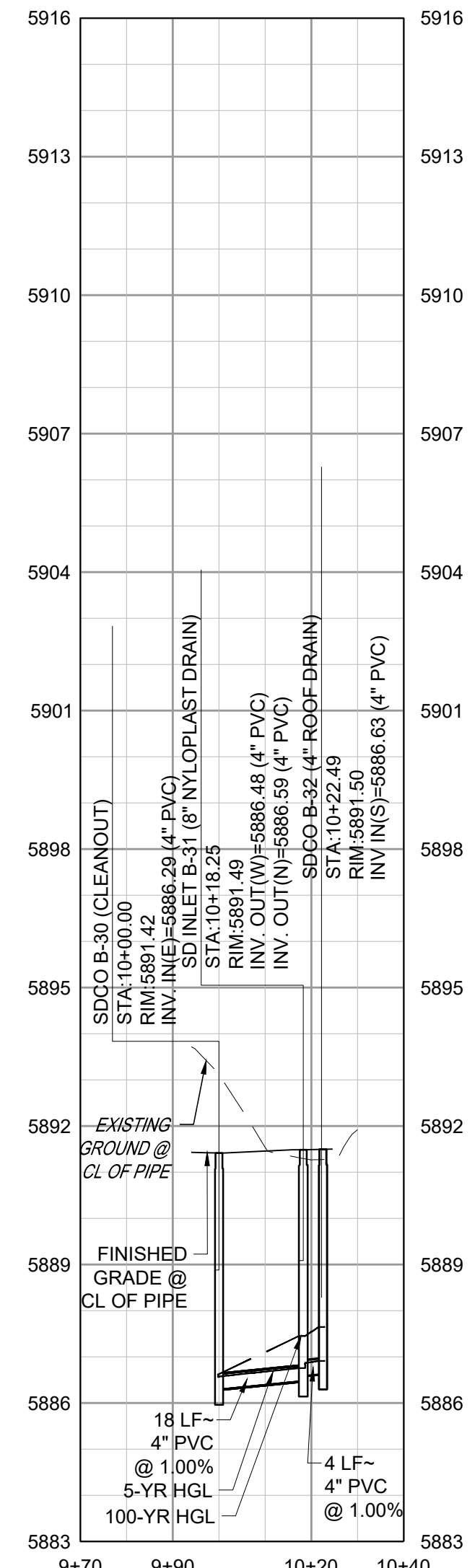


STORM LINE B-20 PROFILE

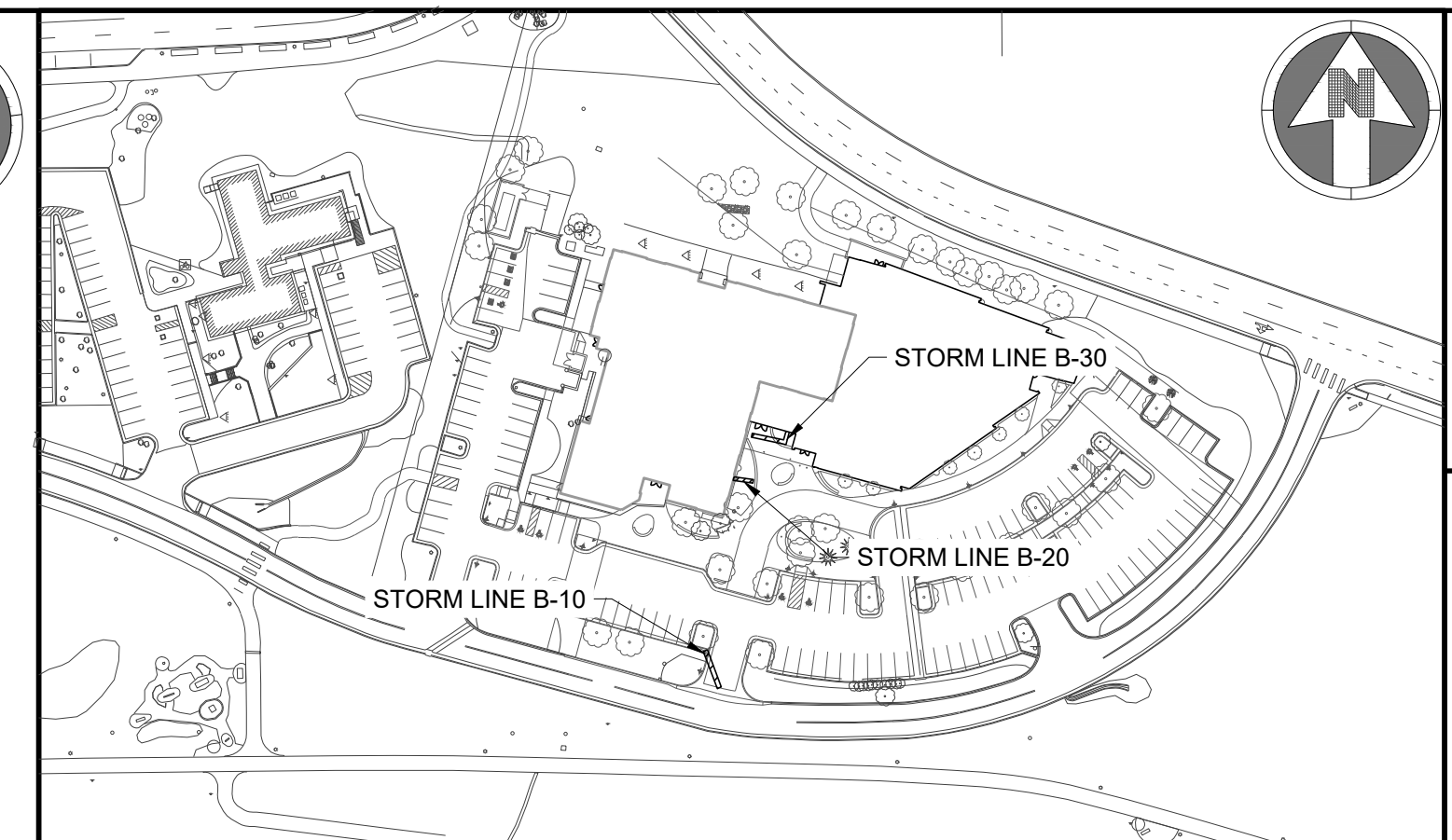


STORM LINE B-30 PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE B-30 PROFILE



KEYMAP  
NTS



NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- ALL EXISTING DRY UTILITIES ARE ASSUMED TO BE 3' DEEP TO TOP OF PIPE.
- ALL EXISTING WATER LINES ARE ASSUMED TO BE 4.5' DEEP TO TOP OF PIPE, UNLESS OTHERWISE NOTED.
- CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
- THE CONTRACTOR SHALL POTHOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY.
- MATCH EXISTING FES (FLARED END SECTION) INVERT. EXISTING GROUND AND FES PIPE INVERT DIFFER. FIELD VERIFY INVERT AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY. ENSURE FES REMAINS AS LOW POINT FOR EXISTING DRAINAGE SWALES.

LEGEND:

UTILITY CROSSING

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works Date

NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024
3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138**

DRAWING TITLE: **STORM PLAN AND PROFILES**

FILE PATH: C:\Users\jgh\Documents\S.A. Miros, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C331-STRM-PROF.dwg C332 - 4/10/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:

**C-332**

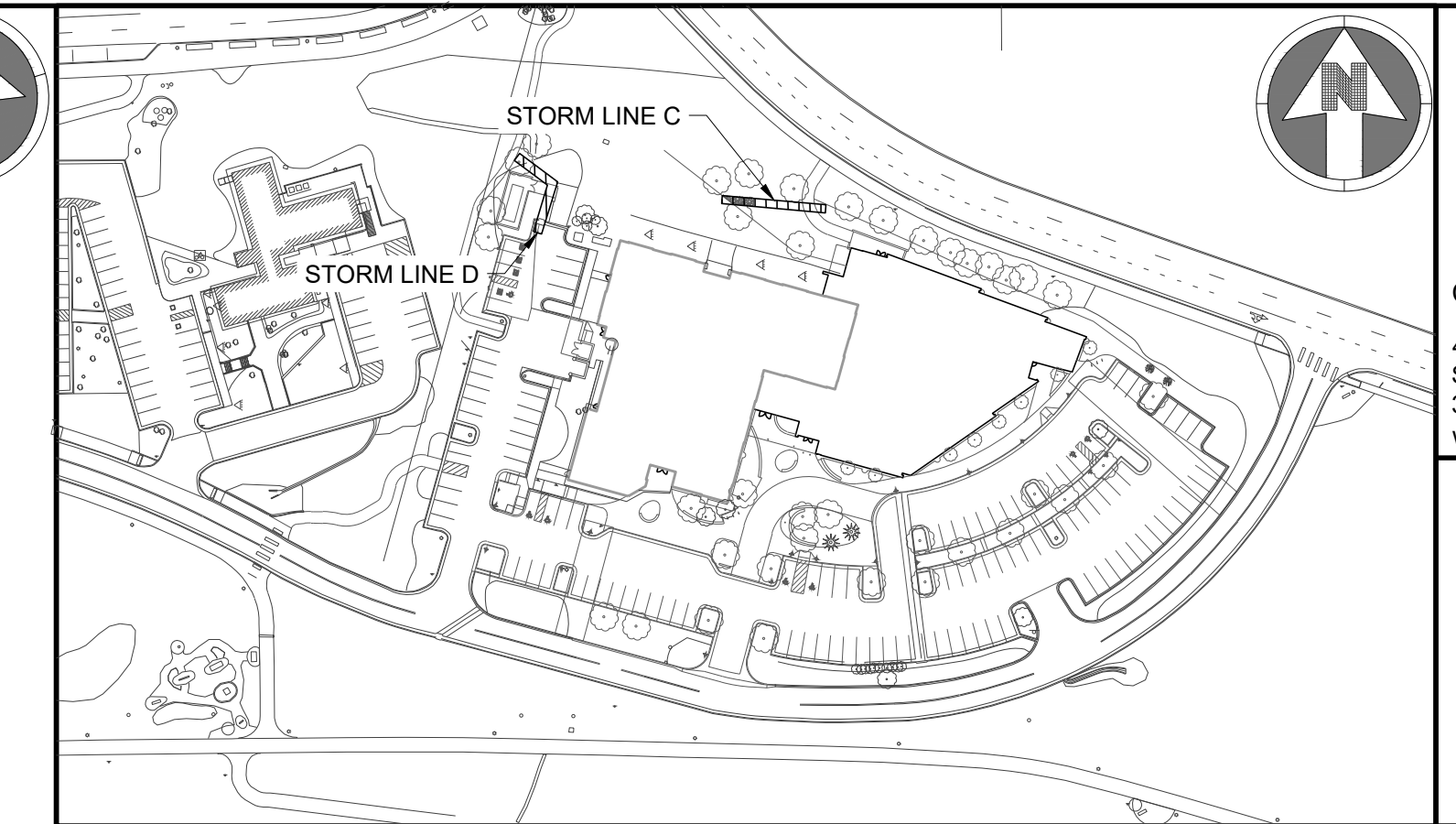


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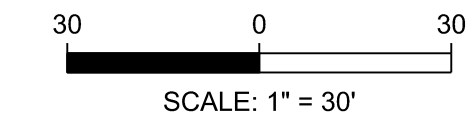
4582 South Ulster Street  
Suite 750, Denver, CO 80237  
303-741-3737  
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KEYMAP  
NTS



NOTES:

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LEGEND:

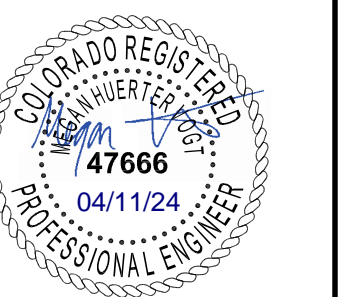
UTILITY CROSSING

NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024
3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: **PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138**

DRAWING TITLE: **STORM PLAN AND PROFILES**

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DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

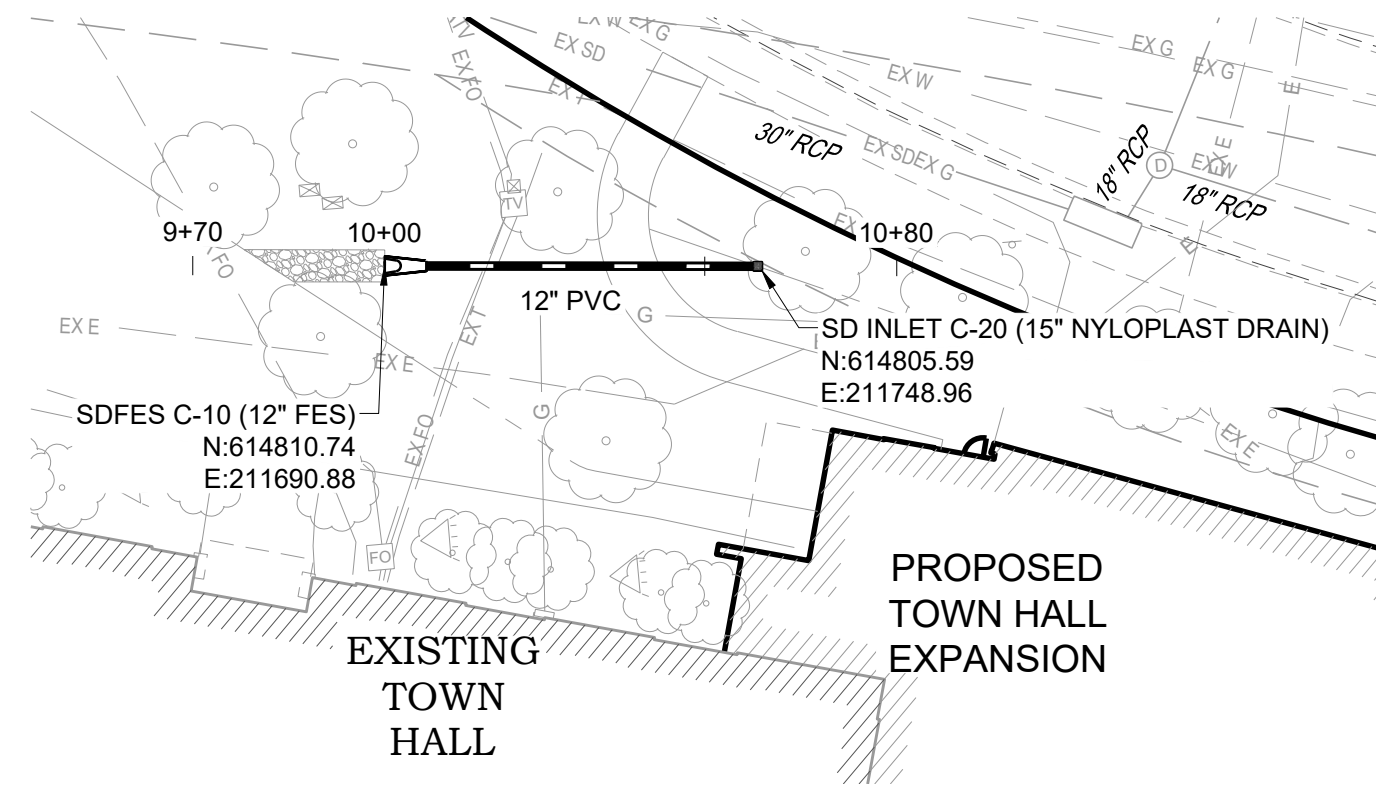
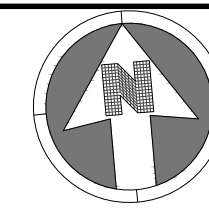
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**C-333**

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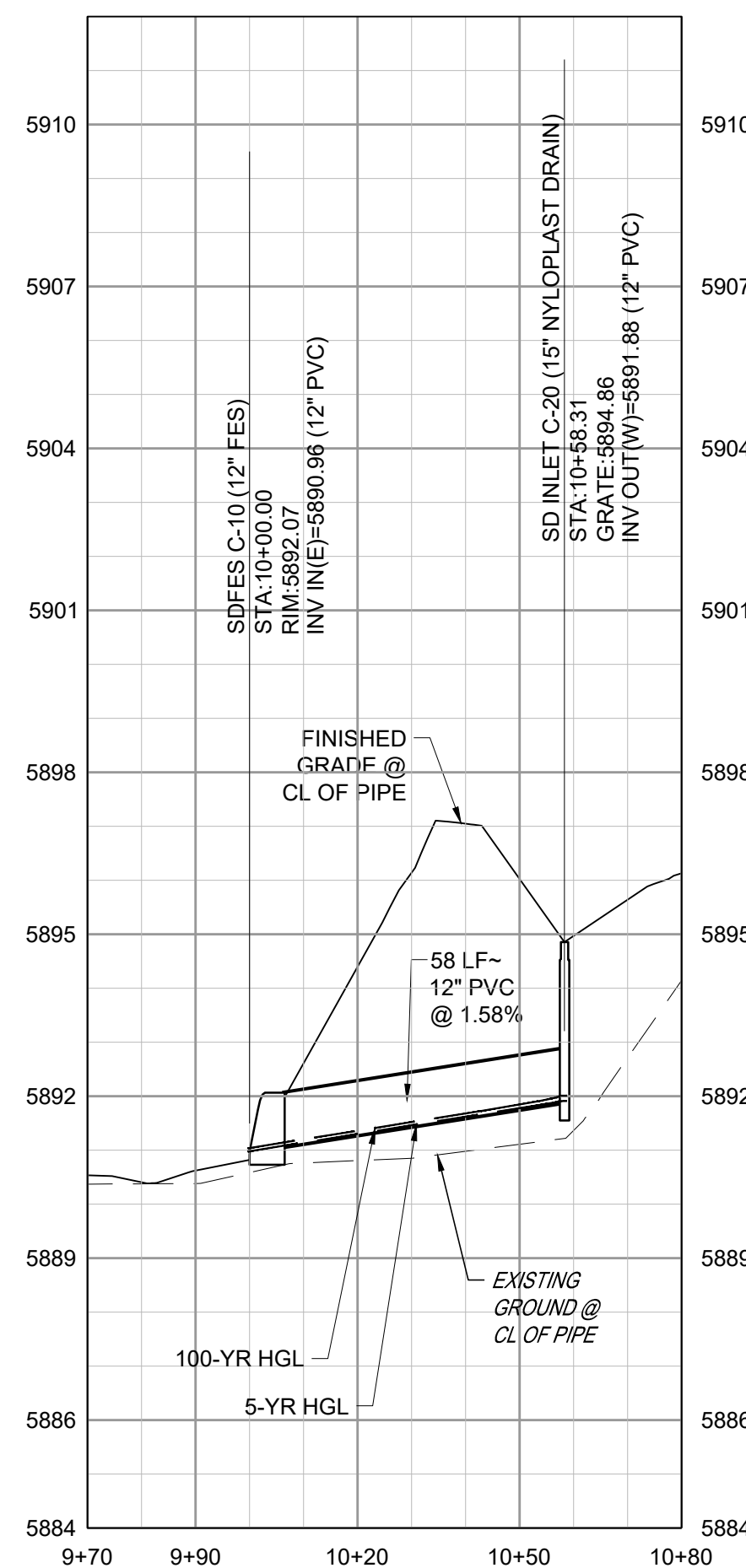
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Town of Parker, Director of Engineering/Public Works Date

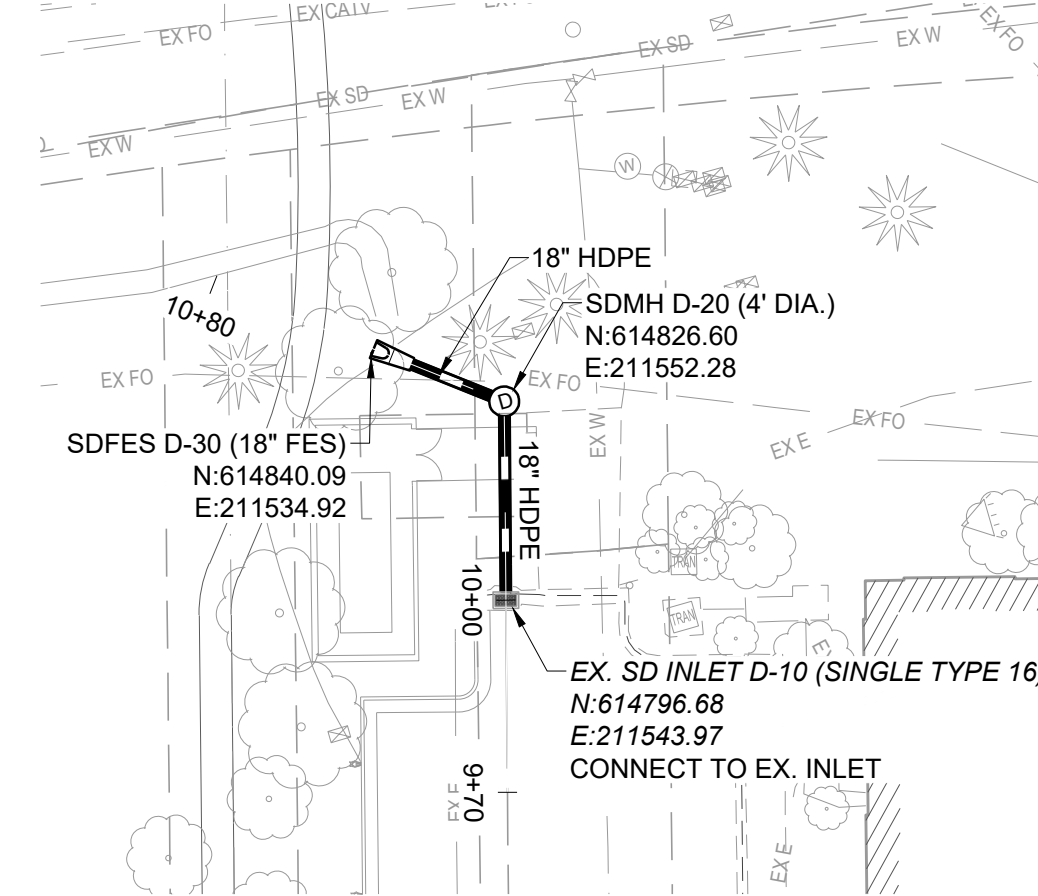
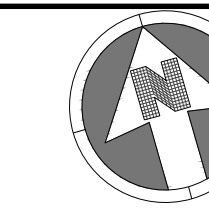


STORM LINE C PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'

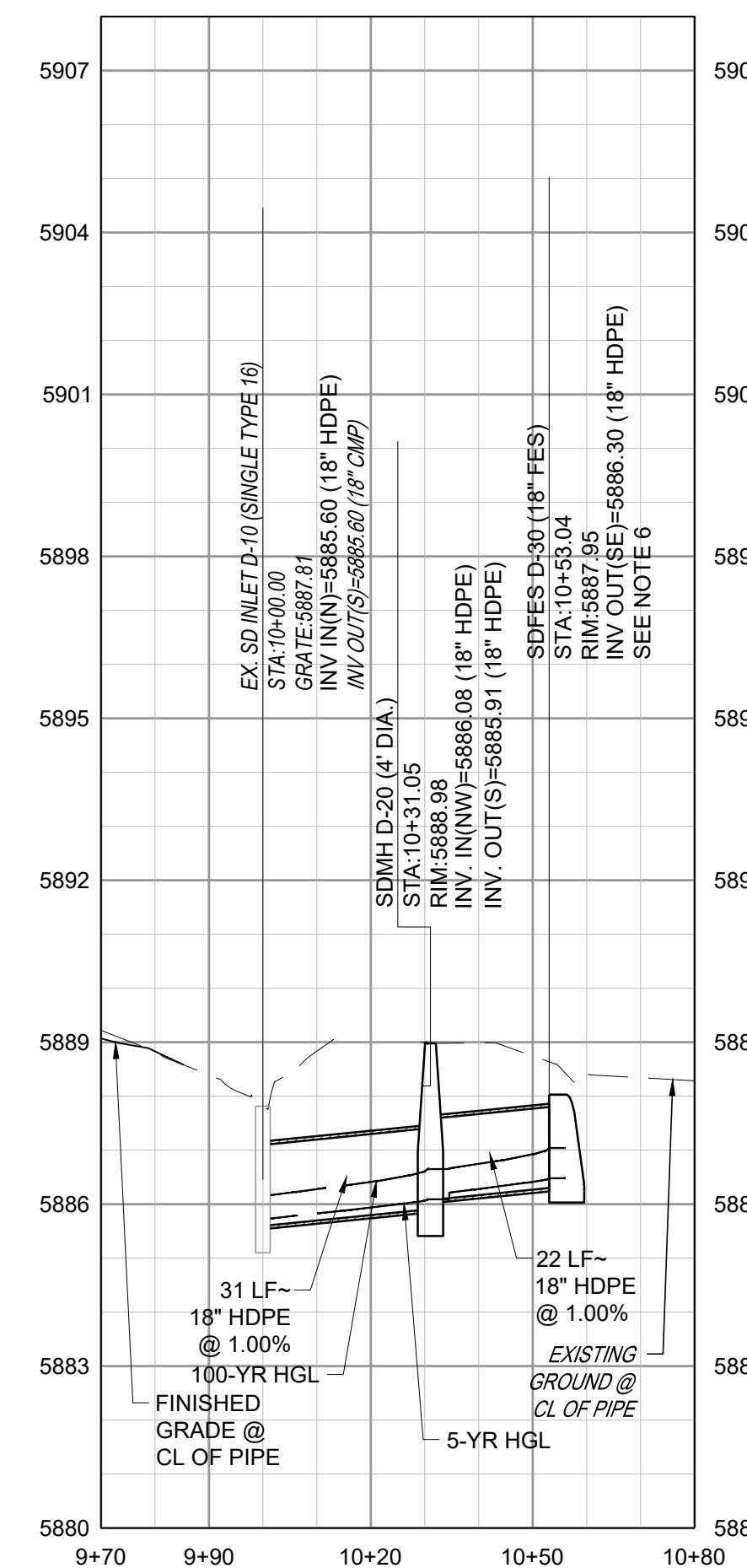


STORM LINE C PROFILE

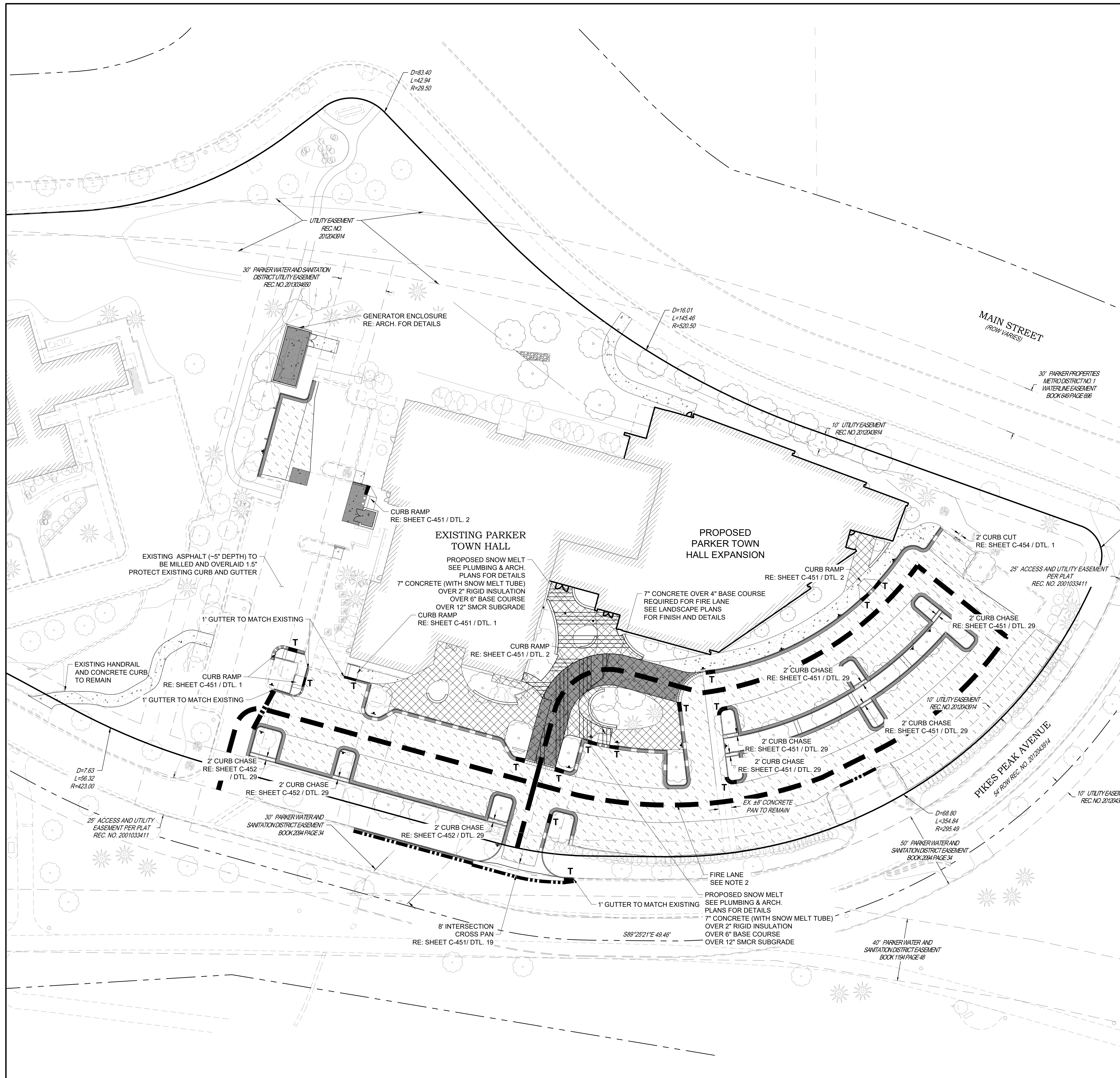


STORM LINE D PLAN

VERTICAL SCALE: 1"=3'  
HORIZONTAL SCALE: 1"=30'



STORM LINE D PROFILE



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
  - A SITE INSPECTION BY A REPRESENTATIVE OF THE SOUTH METRO FIRE DEPARTMENT IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION. NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THAT CURB, GUTTER, AND FIRST LIFT OF ASPHALT ARE INSTALLED FOR THE FULL LENGTH OF THE FIRE LANE.
  - SMCR = SCARIFY, MOISTURE CONTENT, AND RECOMPACT PER SPECIFICATIONS, SEE SECTION 31 20 00 FOR DETAILS.

- LEGEND:**
- PROPOSED 6" CATCH CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-451 / DTL. 3
  - PROPOSED 6" SPILL CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-451 / DTL. 3
  - PROPOSED FLUSH CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 2
  - PROPOSED MOUNTABLE CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-451 / DTL. 3
  - PROPOSED PATCHBACK RE: SHEET C-453 / DTL. 3
  - PROPOSED CURB TRANSITION OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-454 / DTL. 3 & 4
  - PROPOSED 6" COMPOSITE ASPHALT OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 1-A
  - PROPOSED 6" CONCRETE PAVEMENT OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 1-B
  - PROPOSED 7" CONCRETE PAVEMENT OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 1-B
  - REFER TO LANDSCAPE PLANS
  - SNOW MELT SYSTEM RE: ARCH & PLUMBING
  - ADD ALTERNATE SNOW MELT SYSTEM RE: ARCH & PLUMBING



DATE	DESCRIPTION
11/03/2023	FIRST TOWN SUBMITTAL
02/05/2024	SECOND TOWN SUBMITTAL
04/04/2024	THIRD TOWN SUBMITTAL

NO.	PROJECT	DRAWING TITLE:
1	<b>PARKER TOWN HALL EXPANSION</b> <b>20120 MAIN STREET</b> <b>PARKER, CO 80138</b>	<b>PAVING PLAN</b>
2		
3		



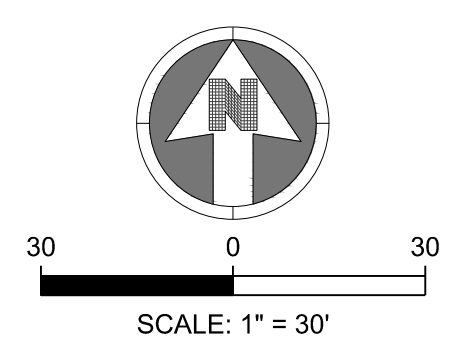
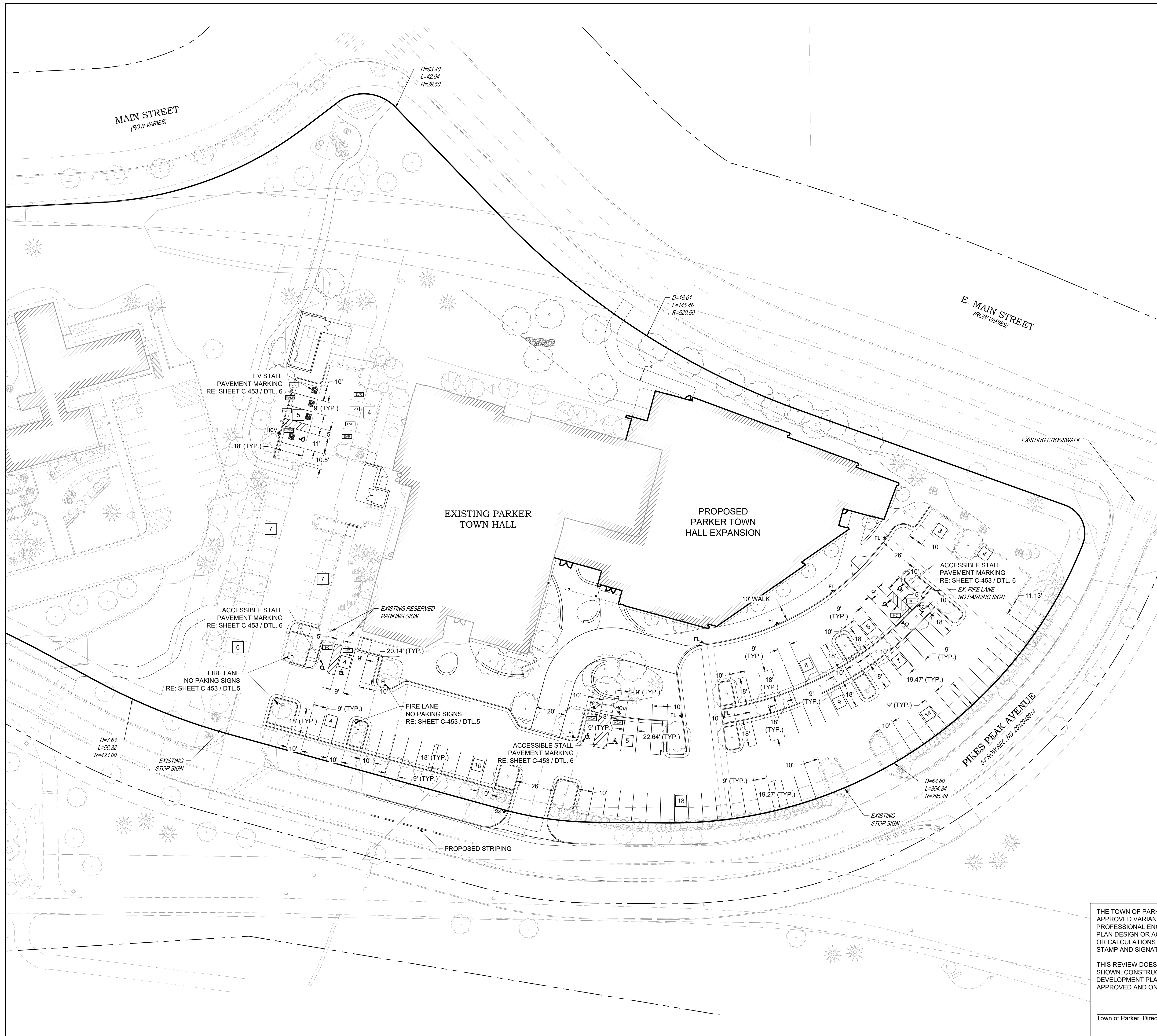
DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO: 22139  
DRAWING NUMBER:

# C-411

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FILE PATH: C:\Users\jgh\OneDrive\Work\A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C411-PAVE-PLAN.dwg C411 - 4/10/2024



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
  - PARKING STALLS ADJACENT TO LANDSCAPE ISLANDS SHALL BE A MINIMUM OF 10' IN WIDTH.
  - PARKING STALLS WITHOUT AN ASSOCIATED PARKING CLASSIFICATION ARE STANDARD PARKING STALLS.

- LEGEND:**
- ▲-HC PROPOSED "HANDICAP PARKING" SIGN  
RE: SHEET C-453 / DTL. 5
  - ▲-HCV PROPOSED "VAN ACCESSIBLE HANDICAP PARKING" SIGN  
RE: SHEET C-453 / DTL. 5
  - ▲-SS PROPOSED "STOP" SIGN  
RE: SHEET C-453 / DTL. 5
  - ▲-FL PROPOSED "FIRE LANE - NO PARKING" SIGN  
RE: SHEET C-453 / DTL. 5
  - 9 PARKING STALL COUNT  
9'X18' MINIMUM DIMENSIONS UNLESS OTHERWISE NOTED
  - C COMPACT PARKING CLASSIFICATION
  - HC STANDARD ACCESSIBLE PARKING CLASSIFICATION
  - HCV ACCESSIBLE VAN PARKING CLASSIFICATION
  - HCEV ACCESSIBLE ELECTRIC VEHICLE PARKING CLASSIFICATION
  - UNV UNIVERSAL VEHICLE PARKING CLASSIFICATION
  - EVSE ELECTRIC VEHICLE SUPPLY EQUIPMENT INSTALLED PARKING CLASSIFICATION
  - EVR ELECTRIC VEHICLE READY PARKING CLASSIFICATION
  - EVC ELECTRIC VEHICLE CAPABLE PARKING CLASSIFICATION

**PARKING COUNT TABLE**

PARKING TYPE	# OF PROPOSED SPACES	# OF EXISTING SPACES TO REMAIN	TOTAL
STANDARD	75	31	106
COMPACT	0	0	0
STANDARD ACCESSIBLE	4	0	4
ACCESSIBLE VAN	2	0	2
ACCESSIBLE EV	1	0	1
UNIVERSAL	0	0	0
EVSE INSTALLED	3	0	3
EV READY	4	0	4
EV CAPABLE	0	0	0
<b>TOTAL</b>	<b>89</b>	<b>31</b>	<b>120</b>

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**PROJECT:** PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

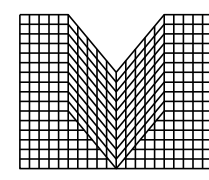
**DRAWING TITLE:** SIGNAGE AND STRIPING PLAN

FILE PATH: C:\Users\jgh2\OneDrive\Work\Projects\2024\20120 Main Street\20120 Main Street Plans and Details\C421-STRIP-PLAN.dwg C421 - 4/10/2024



DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

DRAWING NUMBER:  
**C-421**



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NO.	DESCRIPTION	DATE
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3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAINSTREET  
PARKER, CO 80138

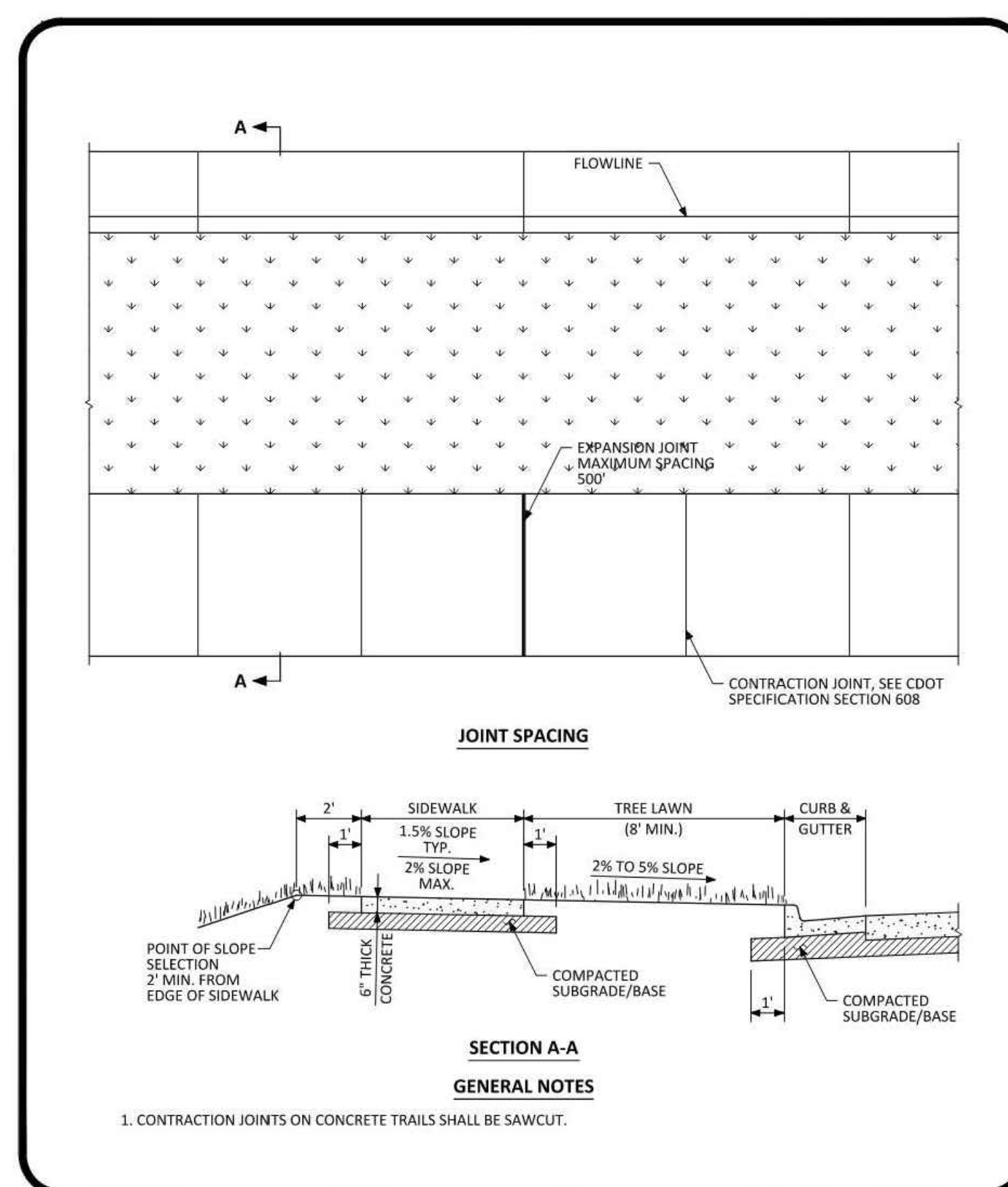
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FILE PATH: C:\Users\ghDCA\Documents\S.A. Miro, Inc.\Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C451-SITE-DTLS.dwg C451 - 4/10/2024

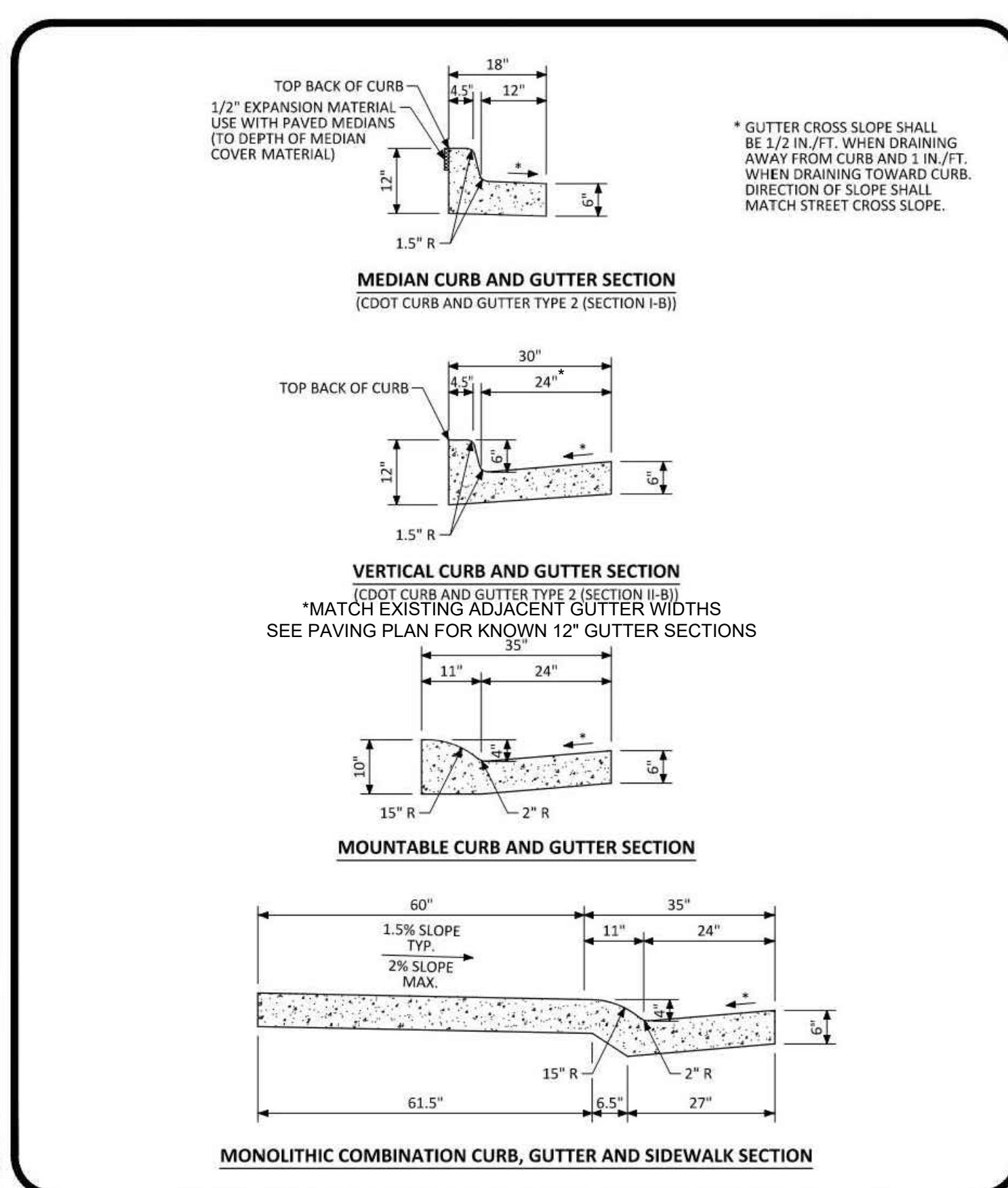


DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139

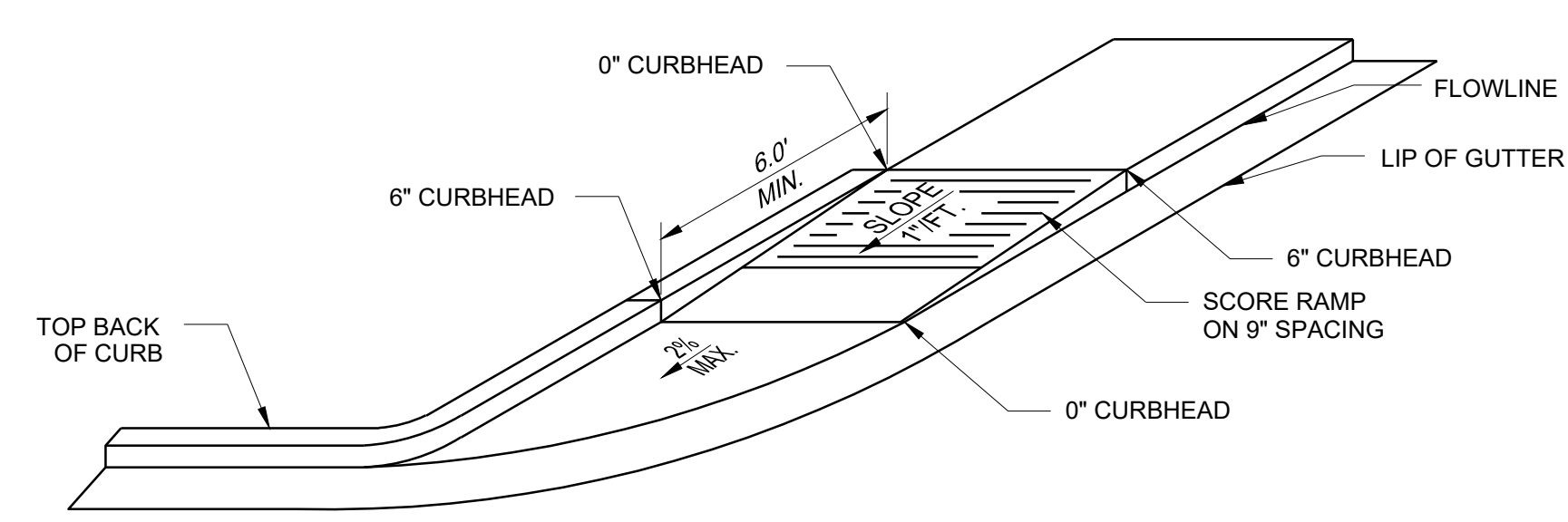
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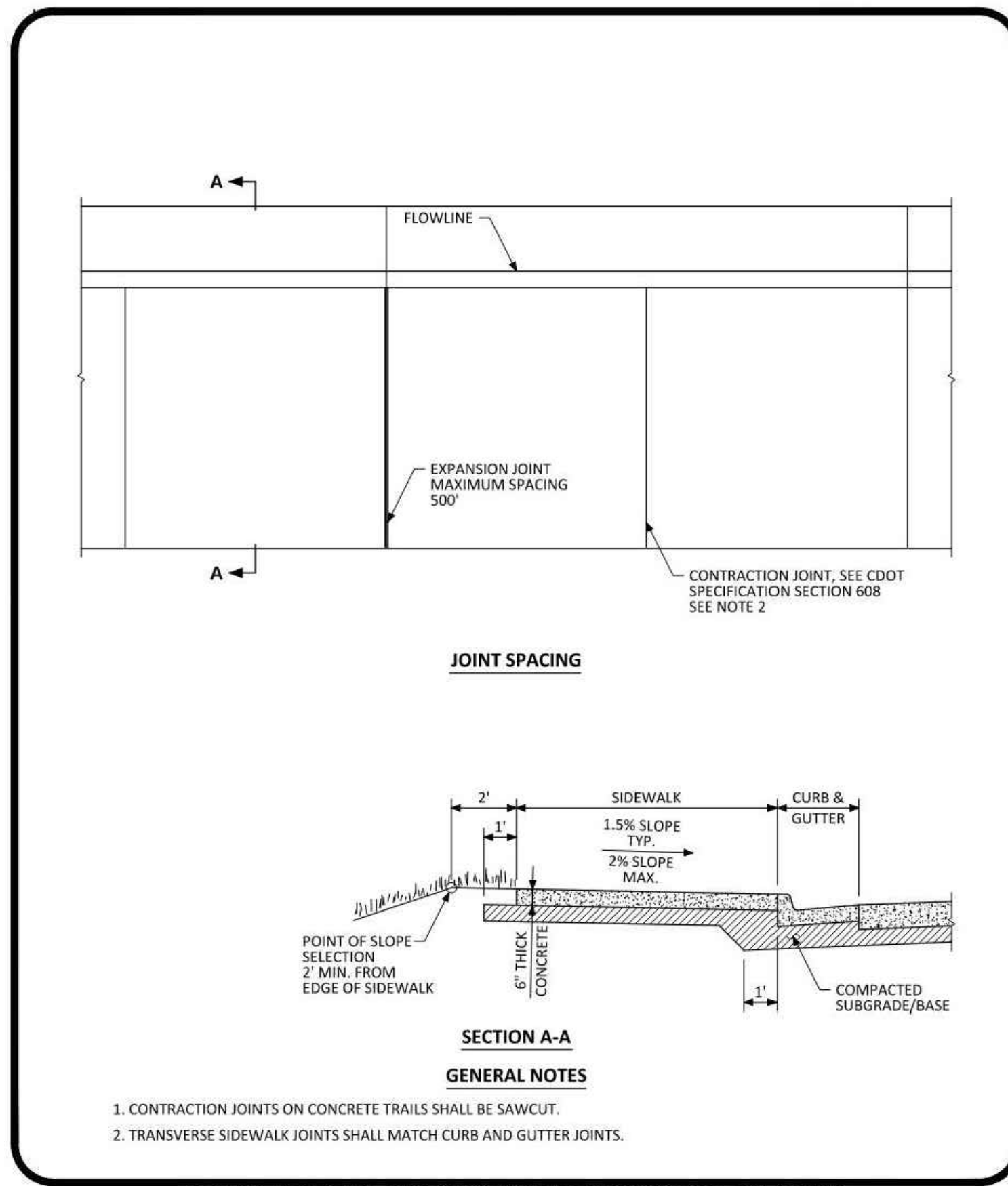
**PARKER COLORADO** DETACHED SIDEWALK STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 4 1 OF 2



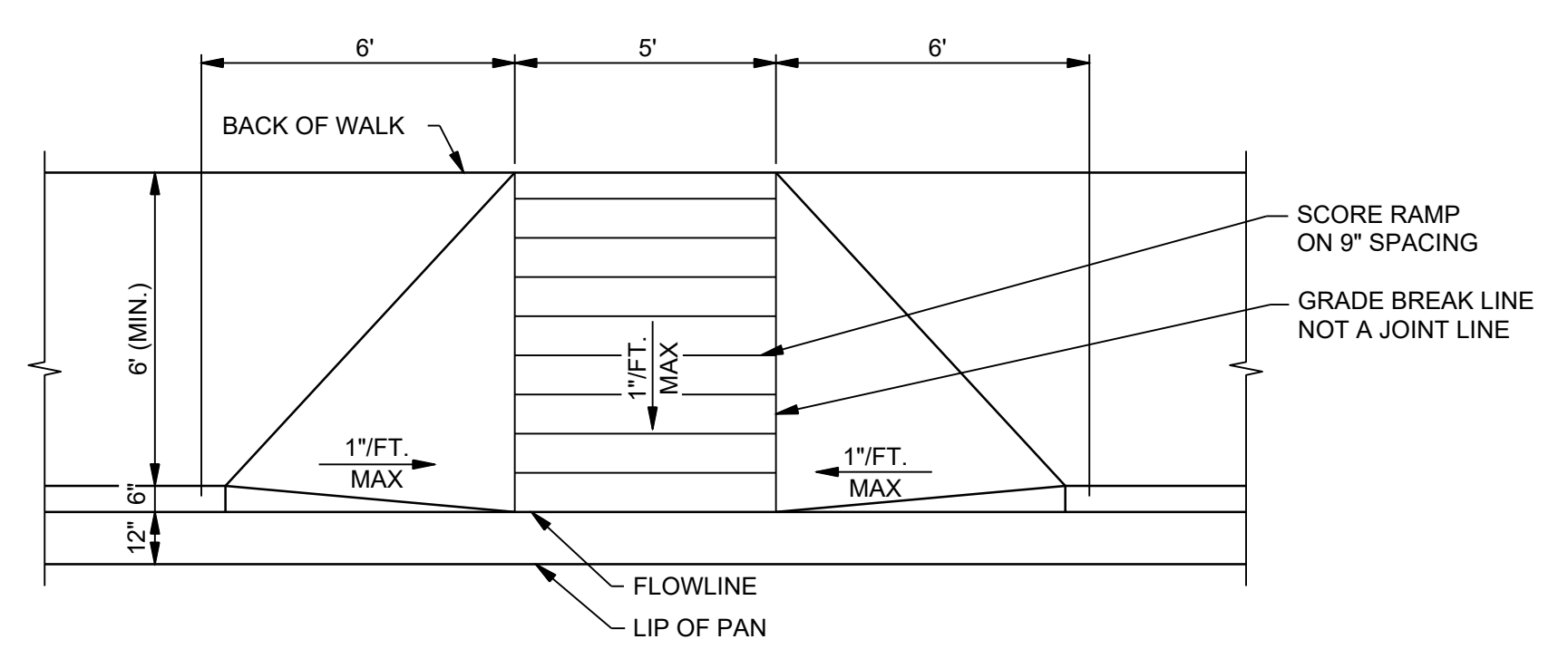
**PARKER COLORADO** CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 3 1 OF 1



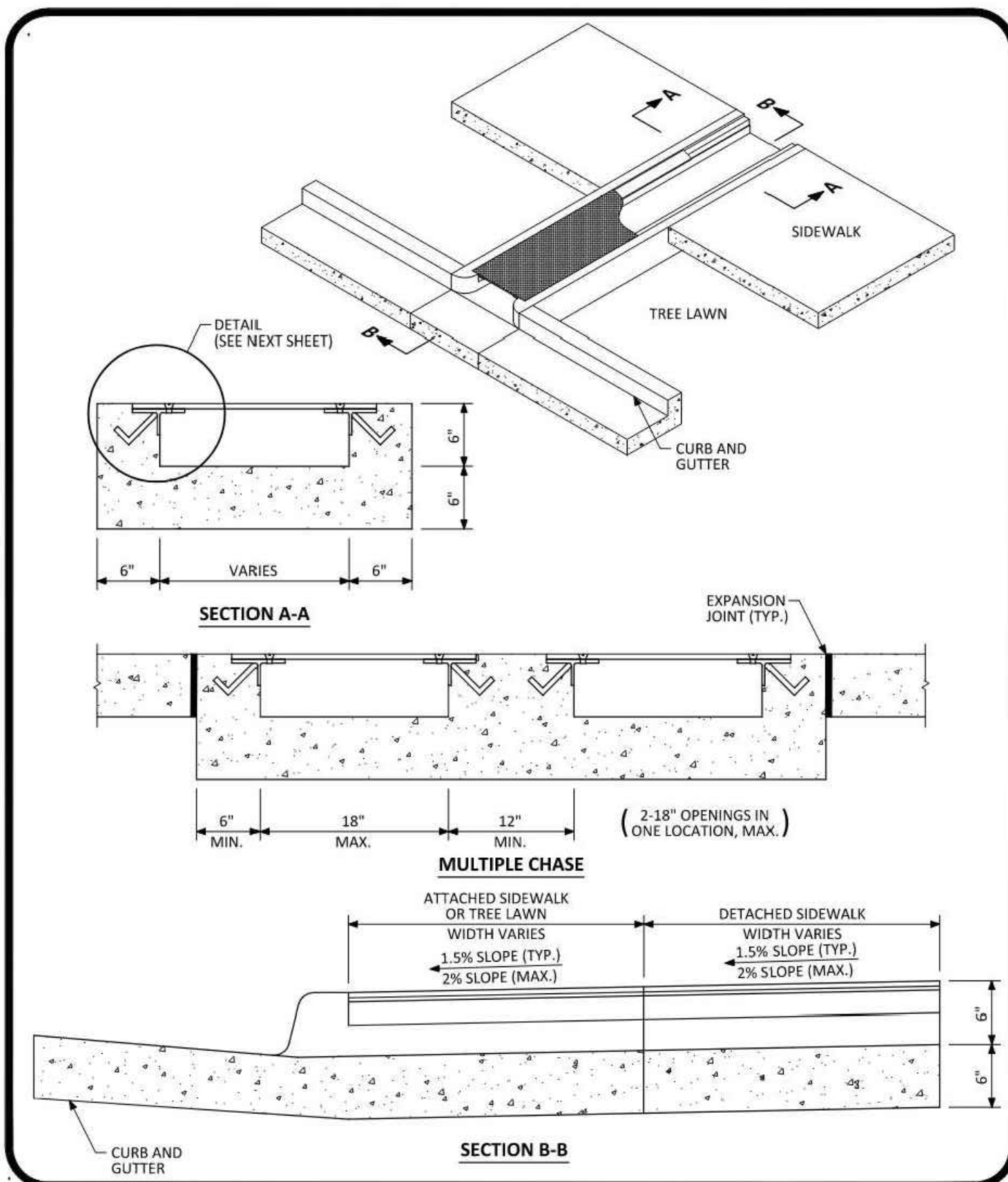
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**PARKER COLORADO** ATTACHED SIDEWALK STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 4 2 OF 2

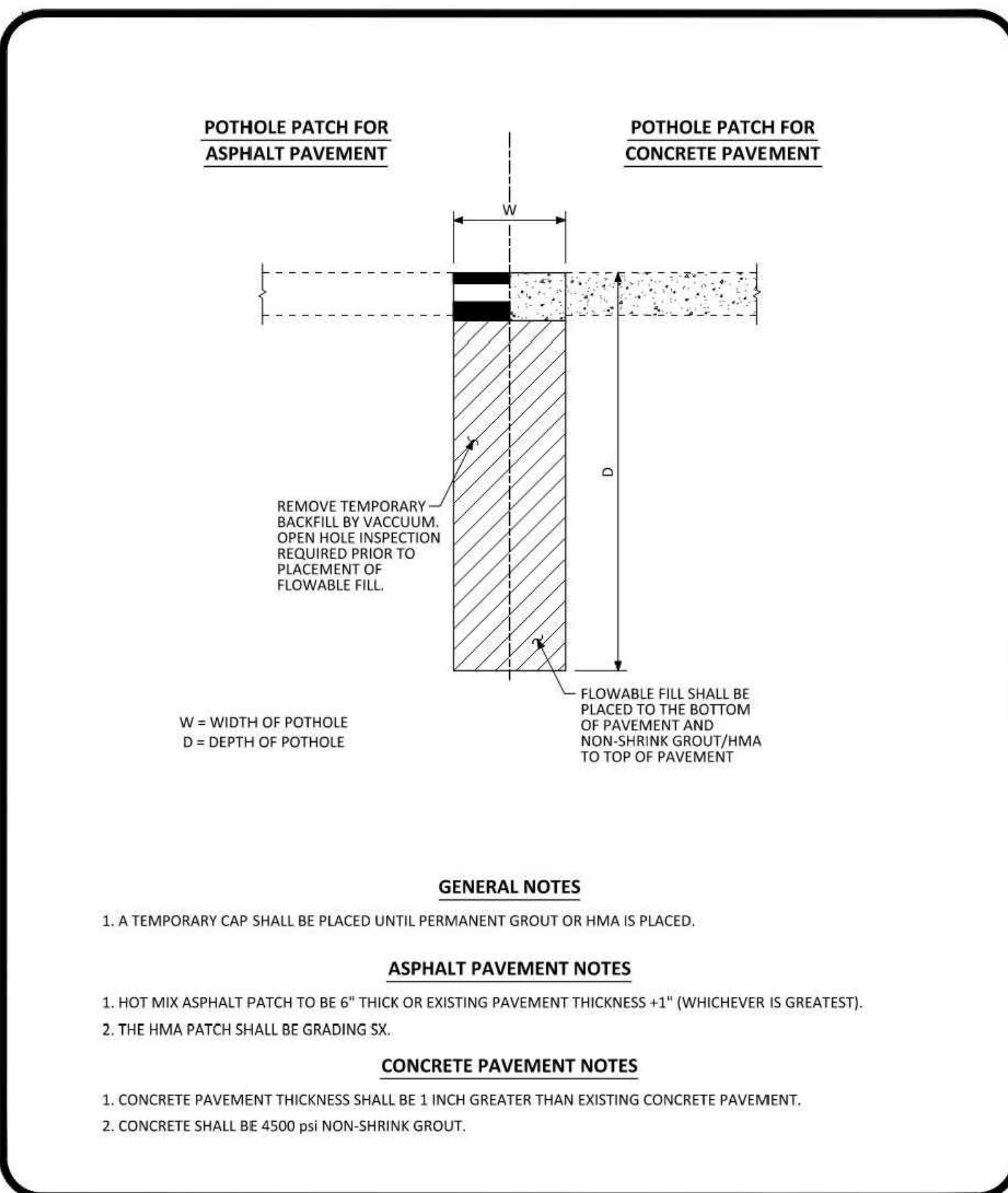


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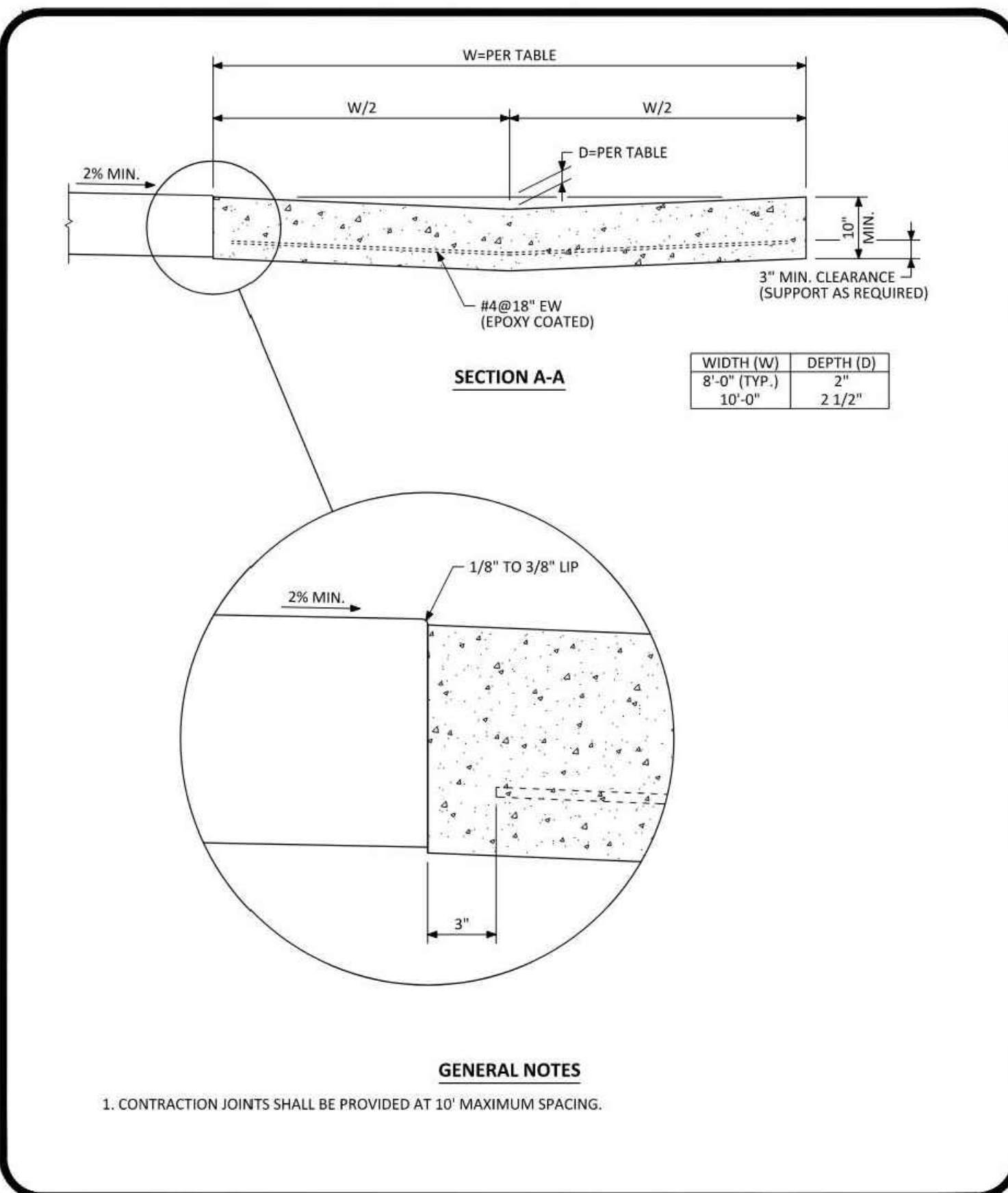
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**PARKER COLORADO** CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 29 1 OF 2



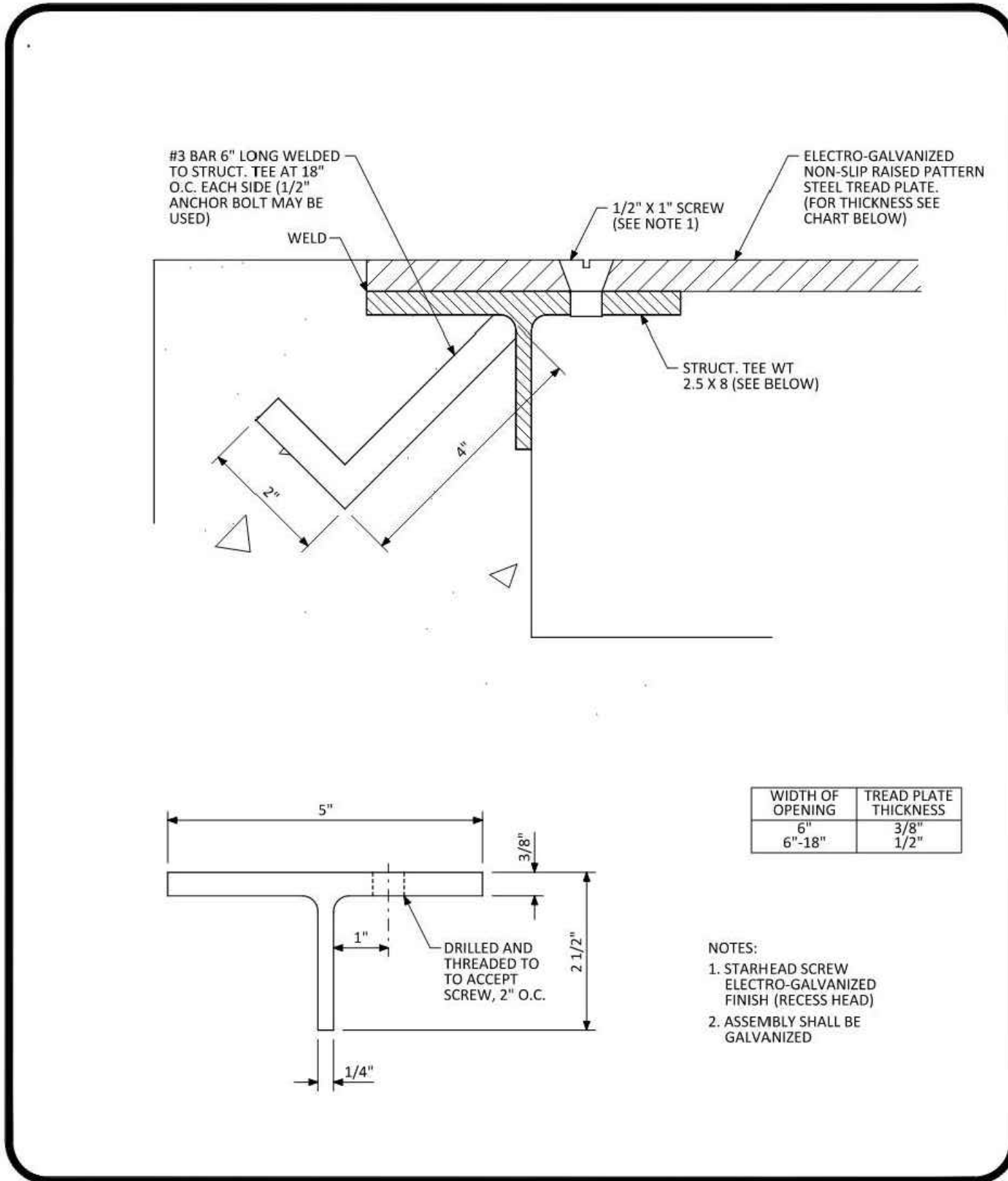
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**PARKER COLORADO** TEST HOLE RESTORATION STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 25 2 OF 2



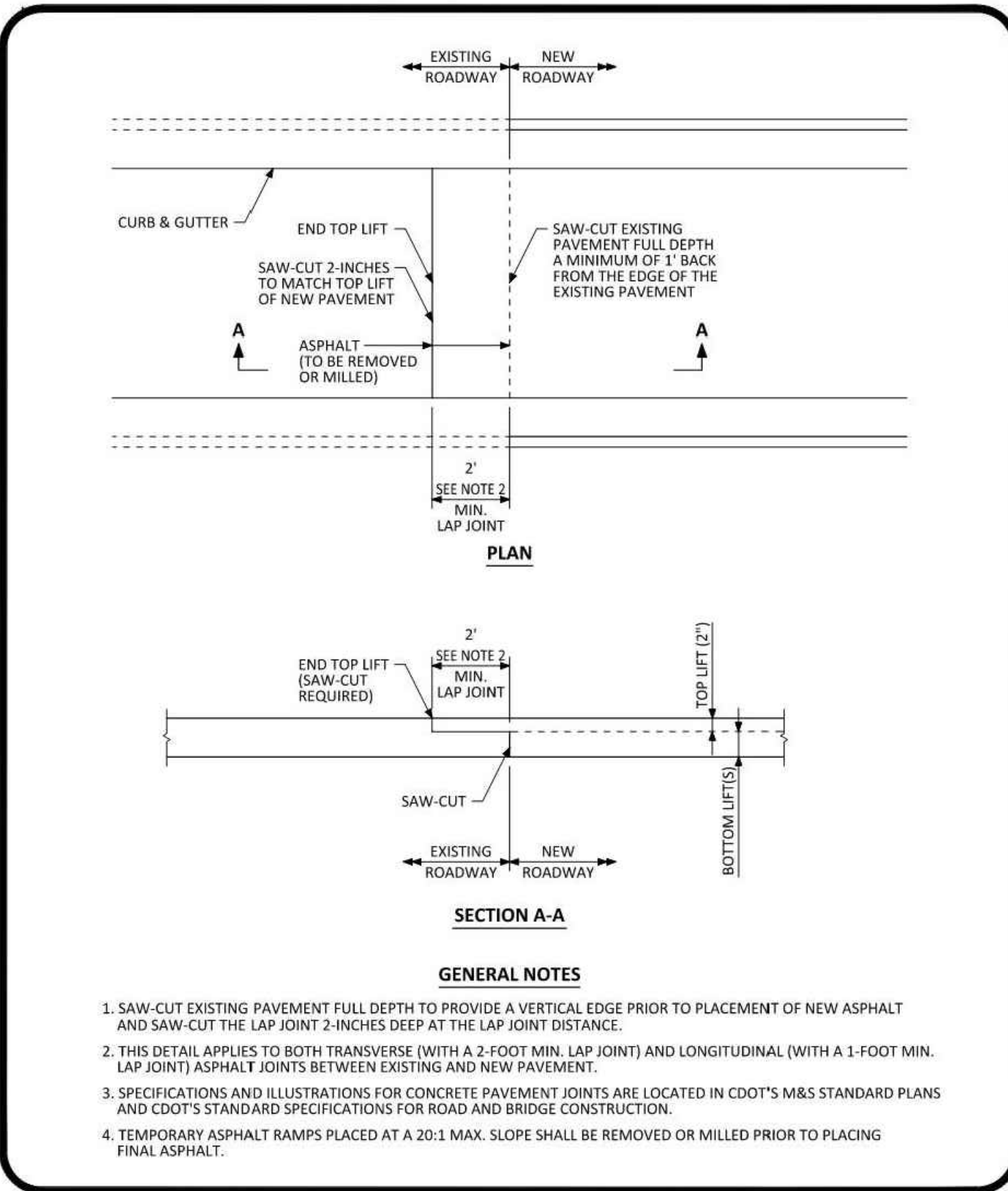
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**PARKER COLORADO** INTERSECTION CROSS PAN LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 19 1 OF 2



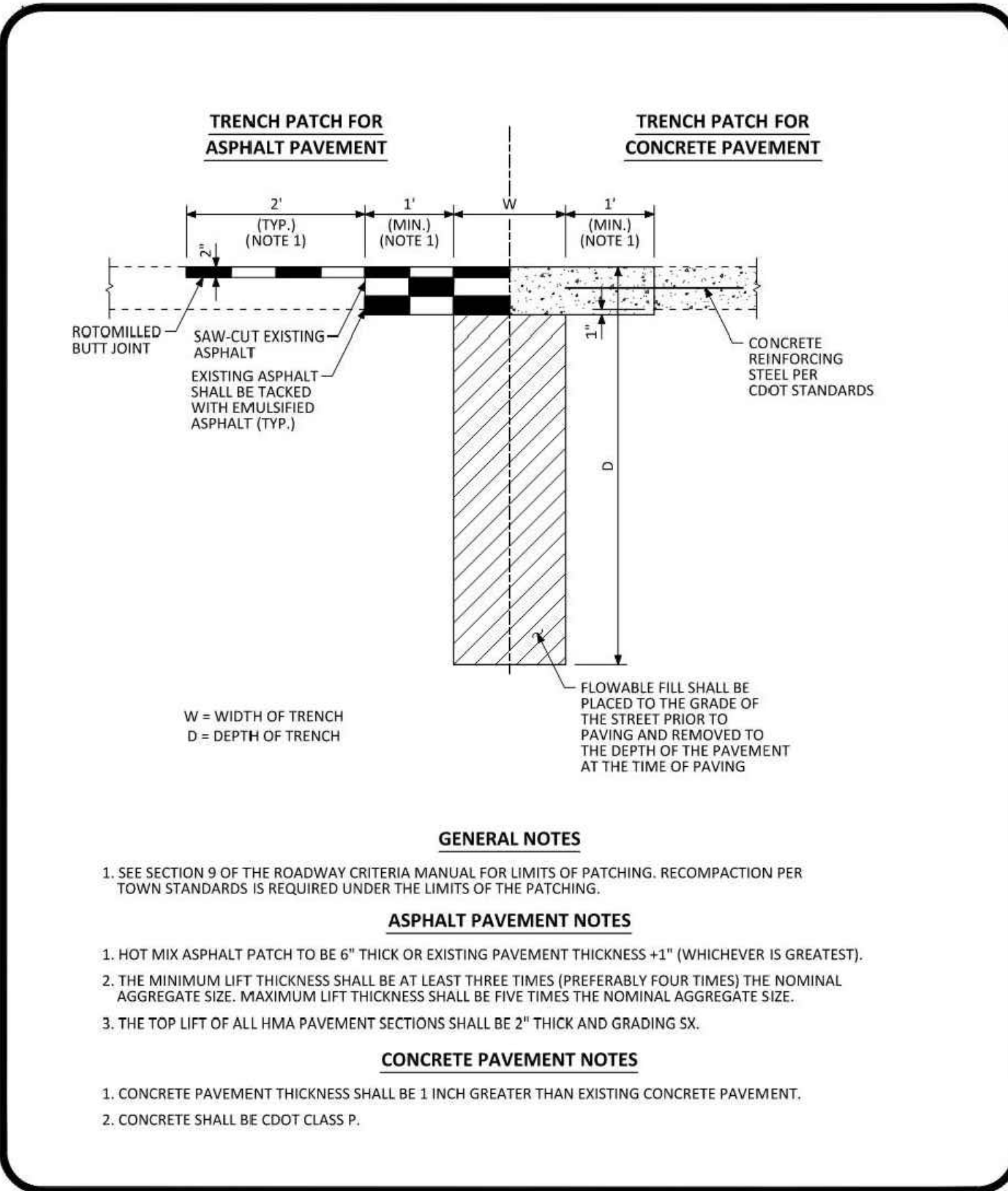
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**PARKER COLORADO** CURB CHASE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 29 2 OF 2



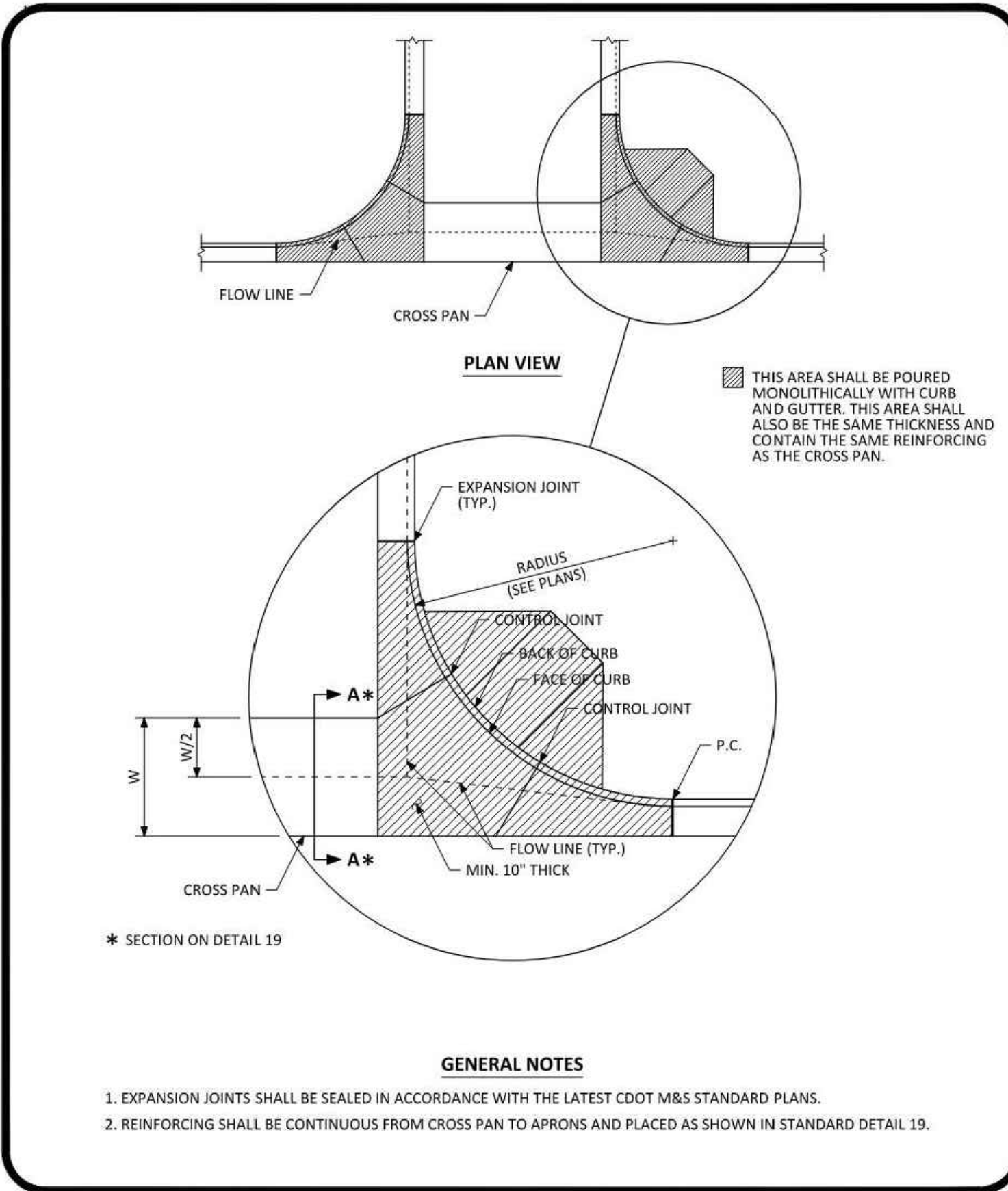
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**PARKER COLORADO** ASPHALT CONNECTION BETWEEN EXISTING AND NEW PAVEMENT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 26 1 OF 1



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**PARKER COLORADO** MINIMUM STREET PATCH DETERMINATION STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 25 1 OF 2



NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

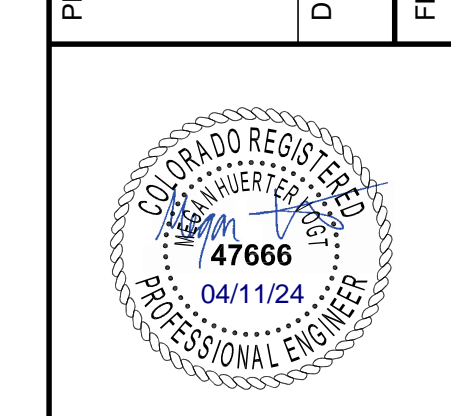
**PARKER COLORADO** INTERSECTION CROSS PAN LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 19 2 OF 2

NO.	DESCRIPTION	DATE
1	FIRST TOWN SUBMITTAL	11/03/2023
2	SECOND TOWN SUBMITTAL	02/05/2024
3	THIRD TOWN SUBMITTAL	04/04/2024

PROJECT: PARKER TOWN HALL EXPANSION  
20120 MAIN STREET  
PARKER, CO 80138

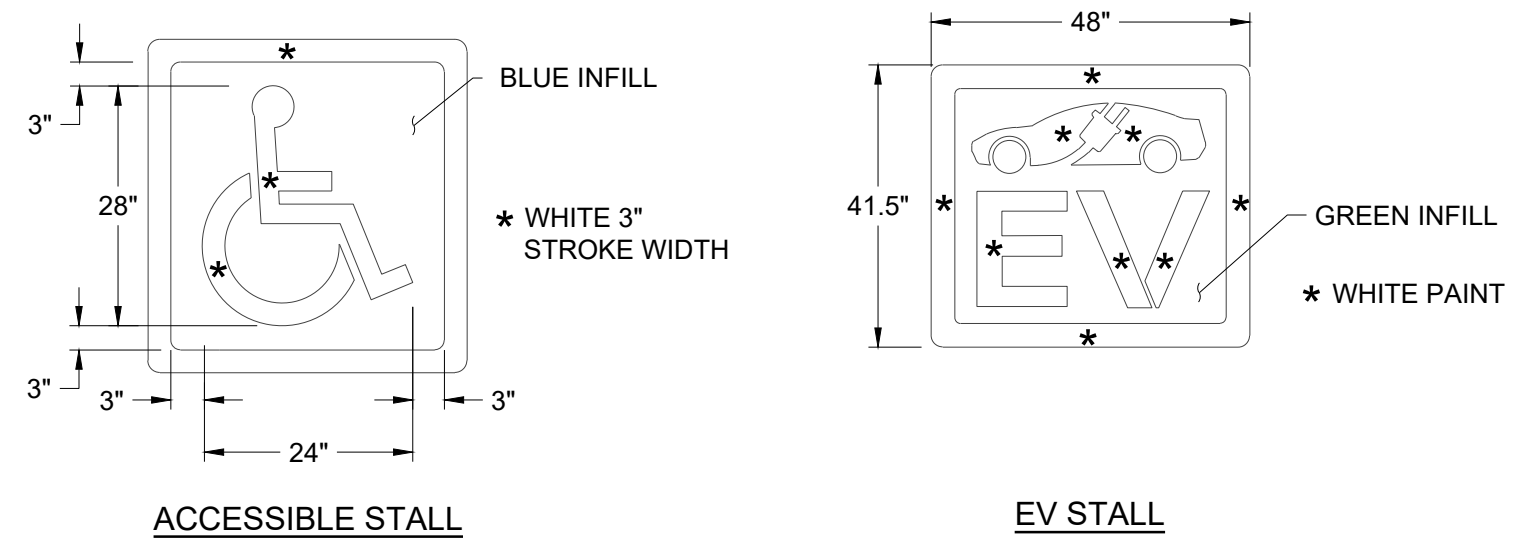
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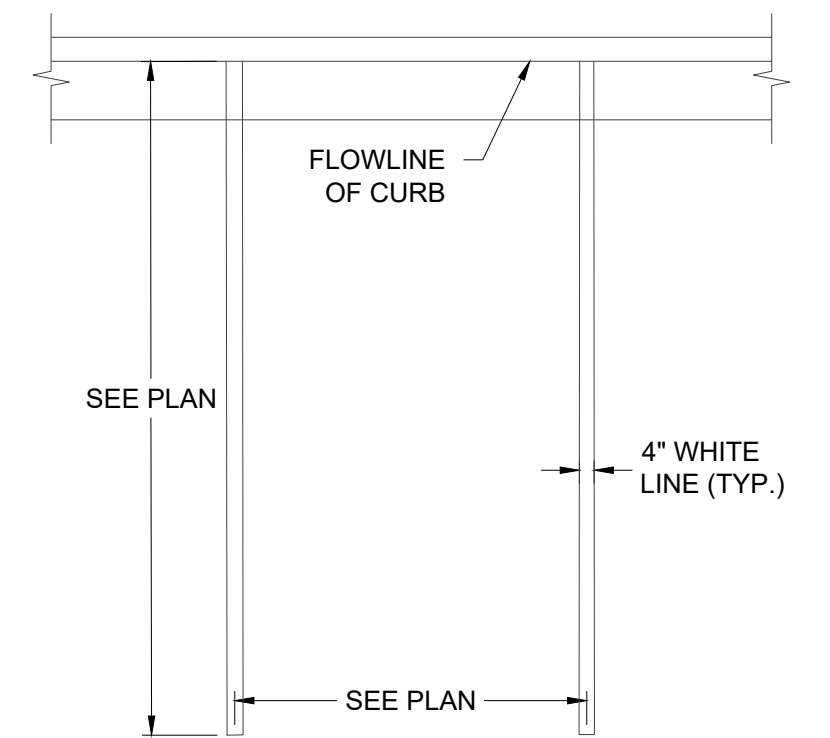
DESIGNED BY: RJH  
DRAWN BY: DEI  
CHECKED BY: MHV  
MIRO JOB NO. 22139  
DRAWING NUMBER:

**C-452**



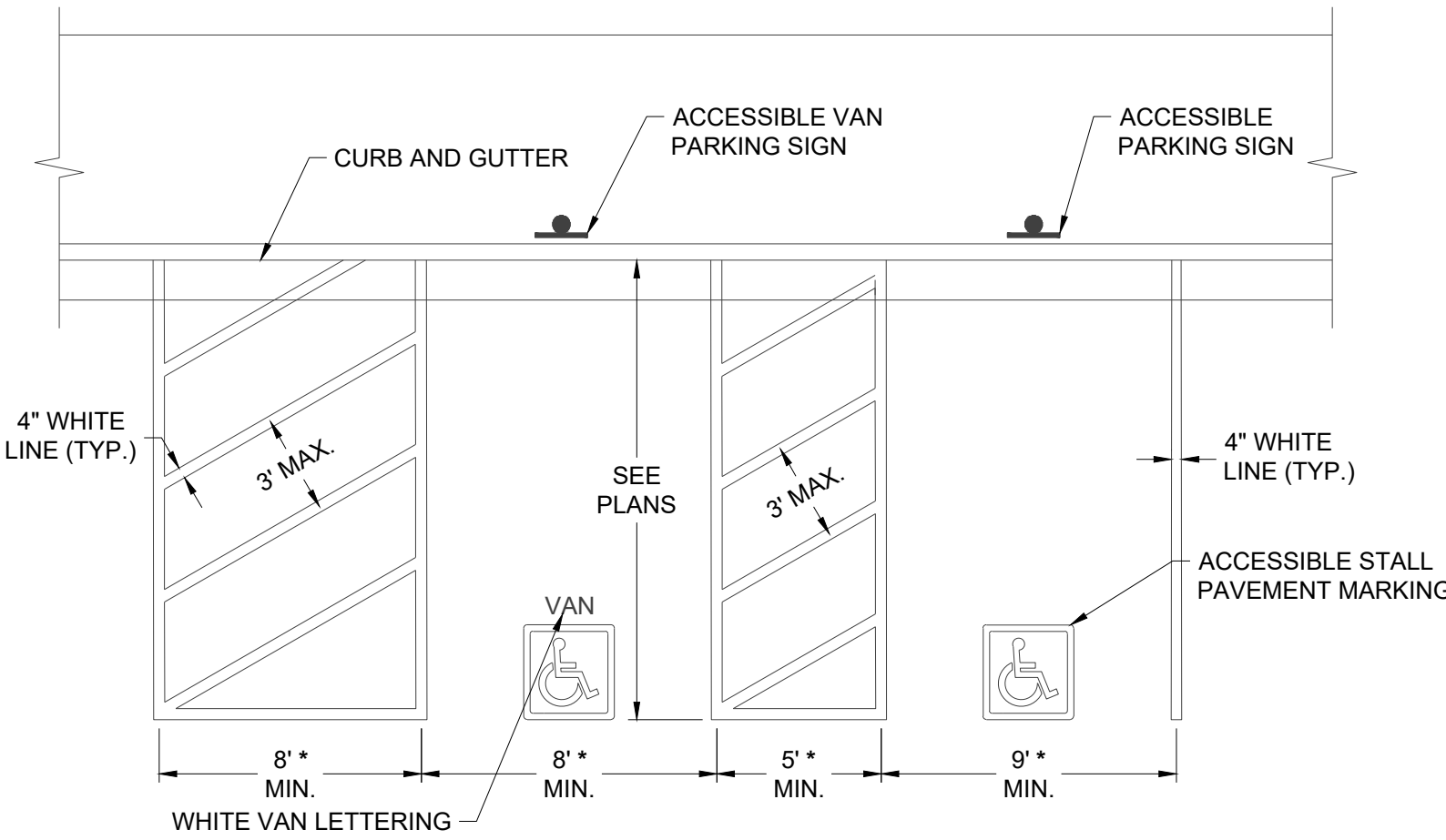
**NOTES:**  
 1. WHITE PAINT OR THERMOPLASTIC MARKINGS SHALL BE USED UNLESS OTHERWISE SPECIFIED BY AUTHORITY HAVING JURISDICTION. ALSO COMPARE AGAINST PROJECT SPECIFIC CIVIL SPECS TO ENSURE THEY MATCH.

**6 PAVEMENT MARKINGS DETAIL** NO SCALE



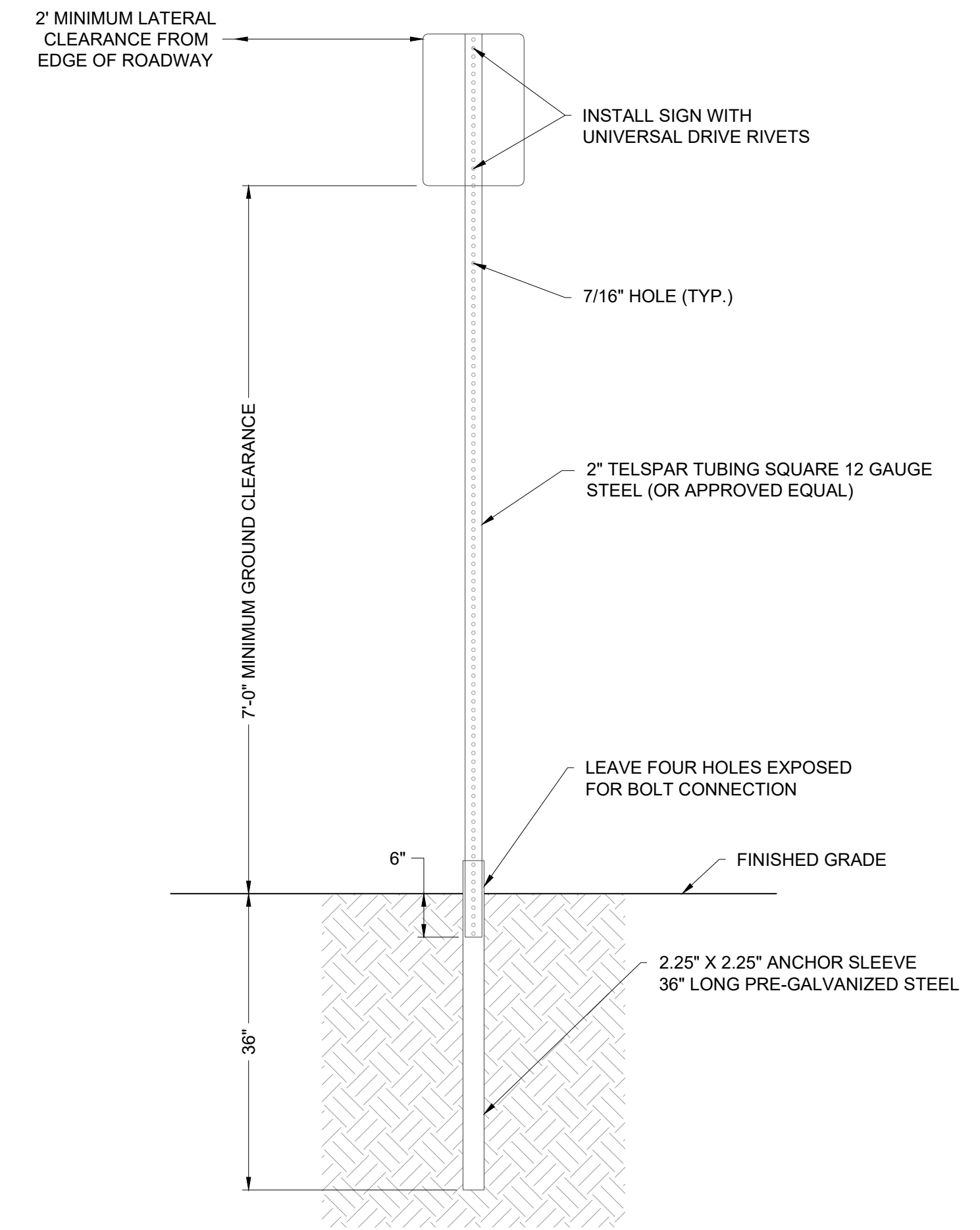
**NOTES:**  
 1. PARKING STALLS ADJACENT TO LANDSCAPE ISLANDS SHALL BE MINIMUM OF 10' IN WIDTH.

**7 TYPICAL PARKING STALL DETAIL** NO SCALE



**NOTE:**  
 THE ACCESSIBLE STALLS AND ASSOCIATED AISLES SHALL BE SLOPED AT A MAXIMUM OF 2% IN ANY DIRECTION. REFER TO GRADING PLANS FOR SPECIFIC ELEVATIONS AND CONTACT THE ENGINEER IF THERE ARE ANY CONCERNS  
 \* SEE PLAN FOR ACTUAL DIMENSIONS

**8 TYPICAL ACCESSIBLE STALLS DETAIL** NO SCALE

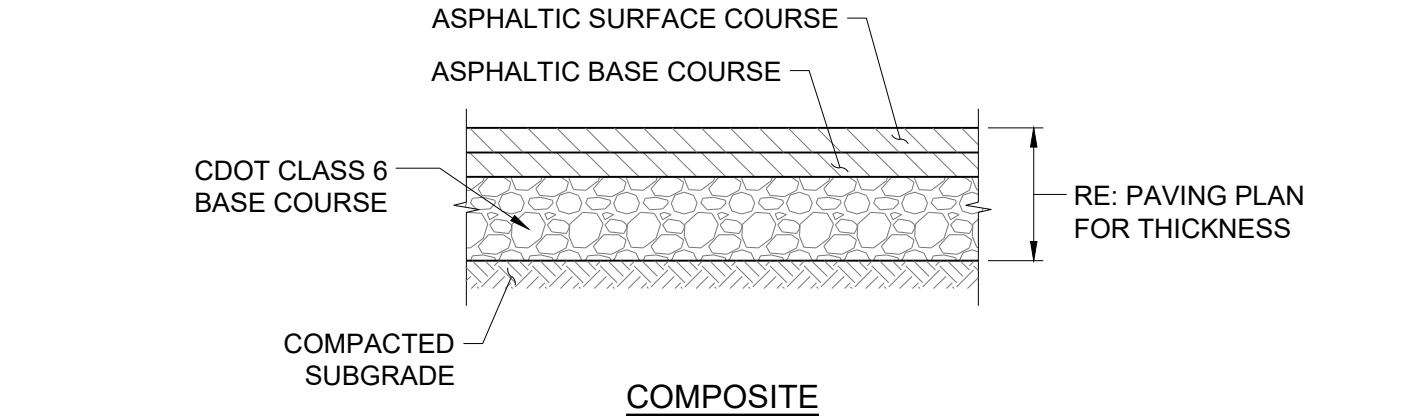


**NOTES:**  
 1. SIGN POST INSTALLATION SHALL FOLLOW THE MANUFACTURE'S SPECIFICATIONS.  
 2. ALL TUBING SHALL BE #12 U.S. STANDARD GAUGE WALL THICKNESS.  
 3. SIGN AND POST SHALL BE PLUMB AND FREE OF VERTICAL AND LATERAL MOVEMENT.  
 4. SIGN SHALL BE PARALLEL TO FLOWLINE FOR PARKING STALL INSTALLATION AND 90° FOR ROADWAY INSTALLATION.  
 5. DRIVE ANCHOR POST INTO THE GROUND TOGETHER, LEAVING FOUR HOLES EXPOSED FOR BOLT CONNECTION. FOR HAND INSTALLATION, DRIVE CAP OF PROPER SIZE SHALL BE USED TO PREVENT DAMAGE TO TUBING. SIDEWALK SHALL BE POURED AROUND A PVC SLEEVE. EXISTING SIDEWALK SHALL BE CORE DRILLED FOR A MINIMUM OF A 4" PVC SLEEVE.  
 6. INSERT SIGN POST INTO ANCHOR SLEEVE APPROXIMATELY 6" AND BOLT IN PLACE.  
 7. A 10" POST WILL WORK FOR SIGNS OF 30" OR LESS. ANY SIGNS LARGER WILL REQUIRE A 11" OR 12" POST DEPENDING ON SIGN SIZE TO MAINTAIN 7' OF GROUND CLEARANCE.

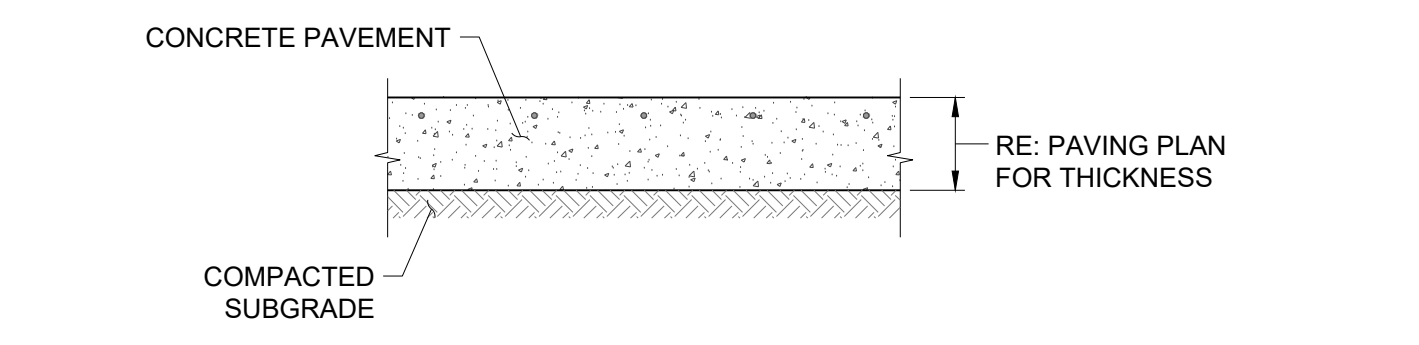
**4 SIGN SUPPORT DETAIL** NO SCALE



**5 SIGN DETAILS** NO SCALE

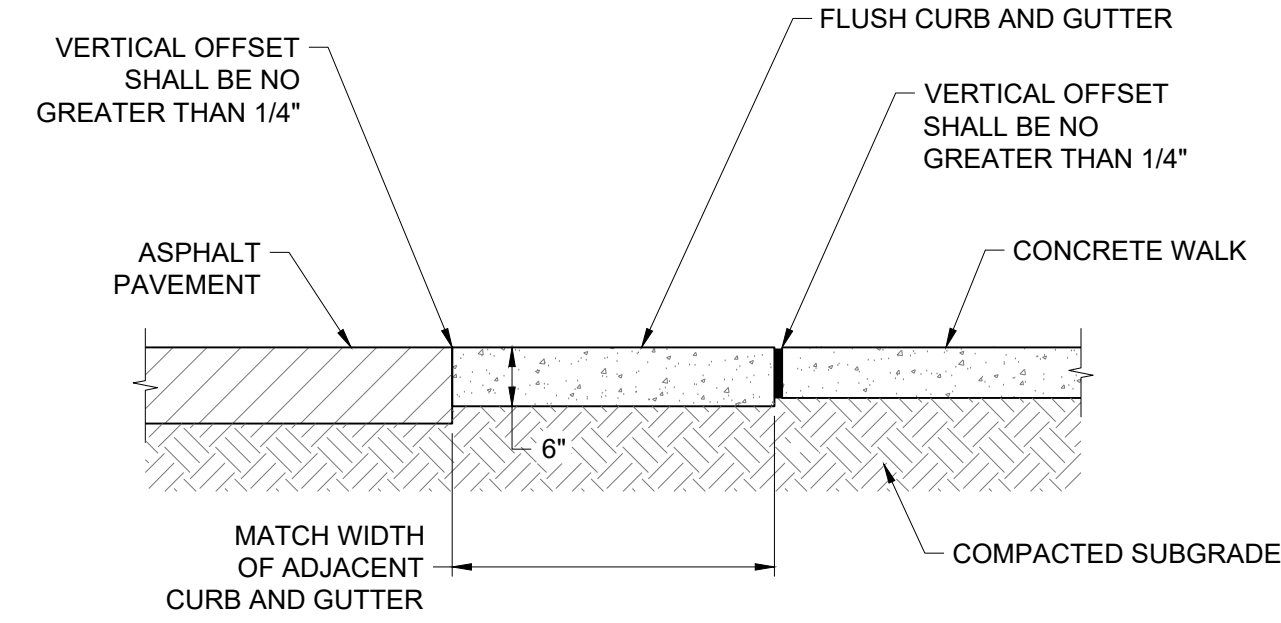


**A ASPHALT PAVEMENT SECTIONS** NO SCALE

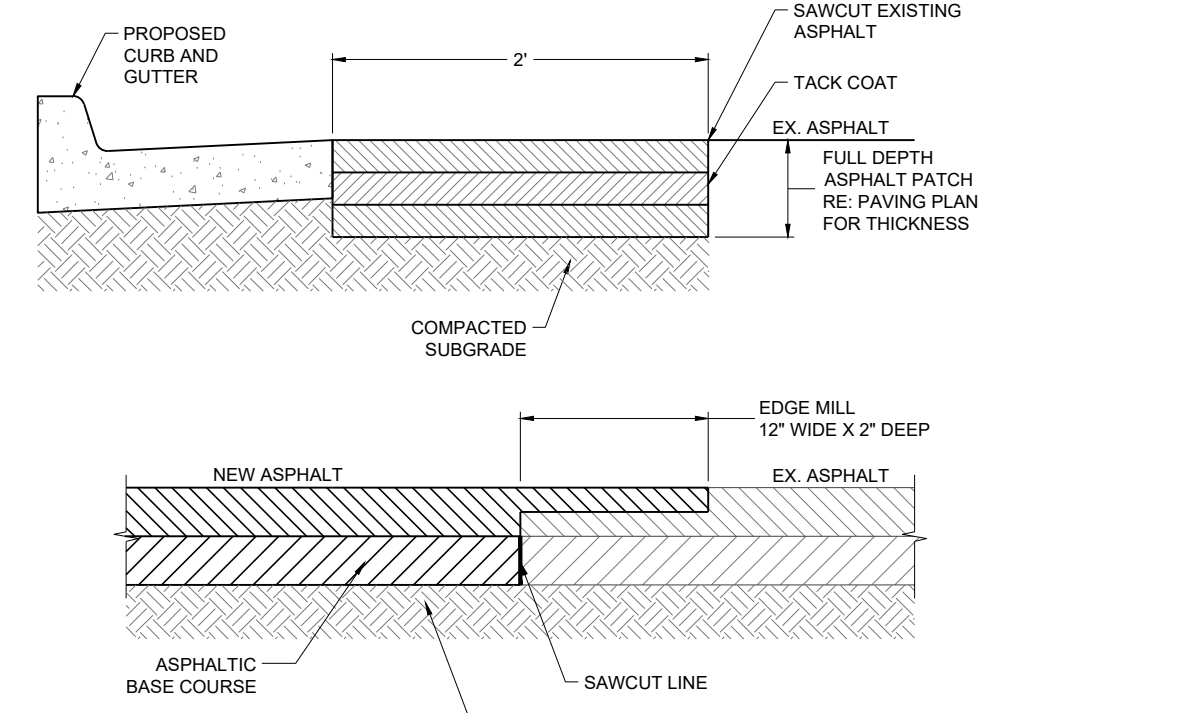


**B CONCRETE PAVEMENT SECTION** NO SCALE

**1 PAVEMENT SECTIONS** NO SCALE



**2 FLUSH CURB AND GUTTER DETAIL** NO SCALE



**3 ASPHALT PATCHBACK DETAIL** NO SCALE

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works Date

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**PARKER TOWN HALL EXPANSION**  
**20120 MAINSTREET**  
**PARKER, CO 80138**  
 PROJECT:  
 DRAWING TITLE:  
 FILE PATH: C:\Users\ghDCA\Documents\A. Miro, Inc.-Civil\Jobs\22139 Parker Town Hall\05 CAD\Plans and Details\C453-SITE-DTLS.dwg C453 - 4/10/2024



DESIGNED BY: RJH  
 DRAWN BY: DEI  
 CHECKED BY: MHV  
 MIRO JOB NO. 22139

DRAWING NUMBER:

**C-453**

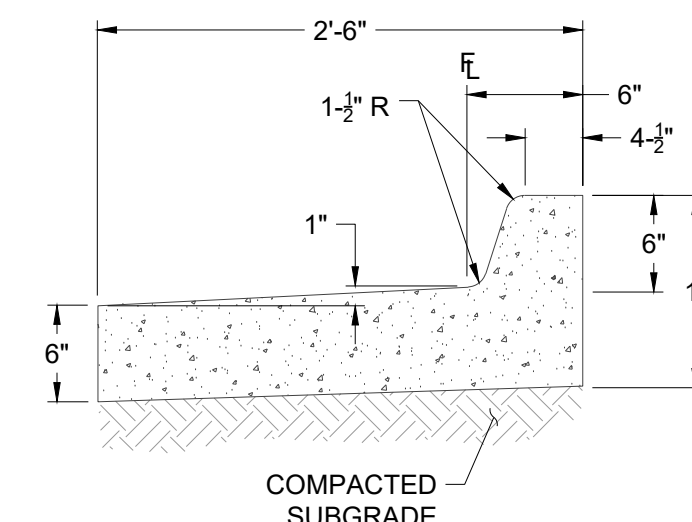


S.A. MIRO INC.  
CONSULTING ENGINEERS

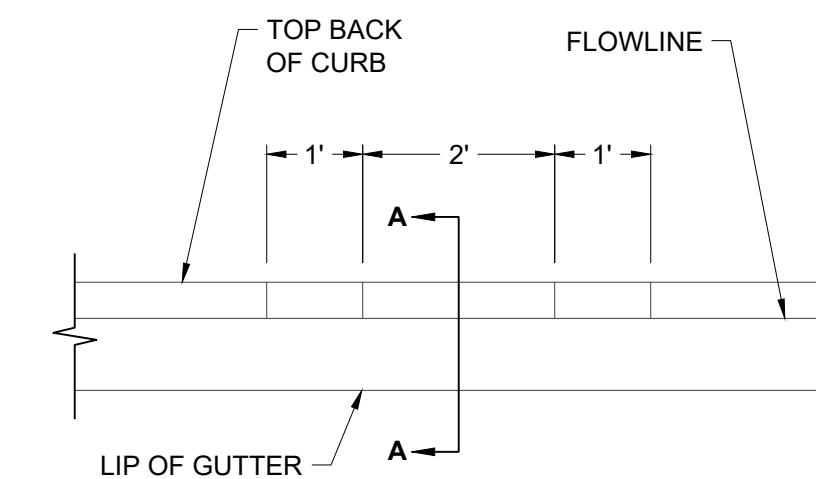
4582 South Ulster Street  
Suite 750, Denver, CO 80237  
303-741-3737  
www.samiro.com



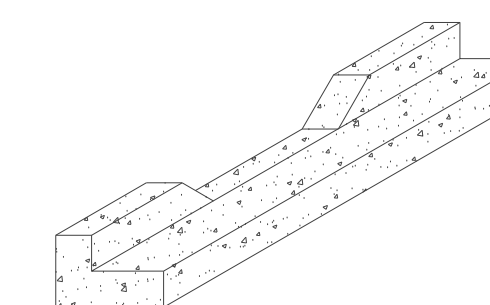
Know what's below.  
Call before you dig.



SECTION A-A



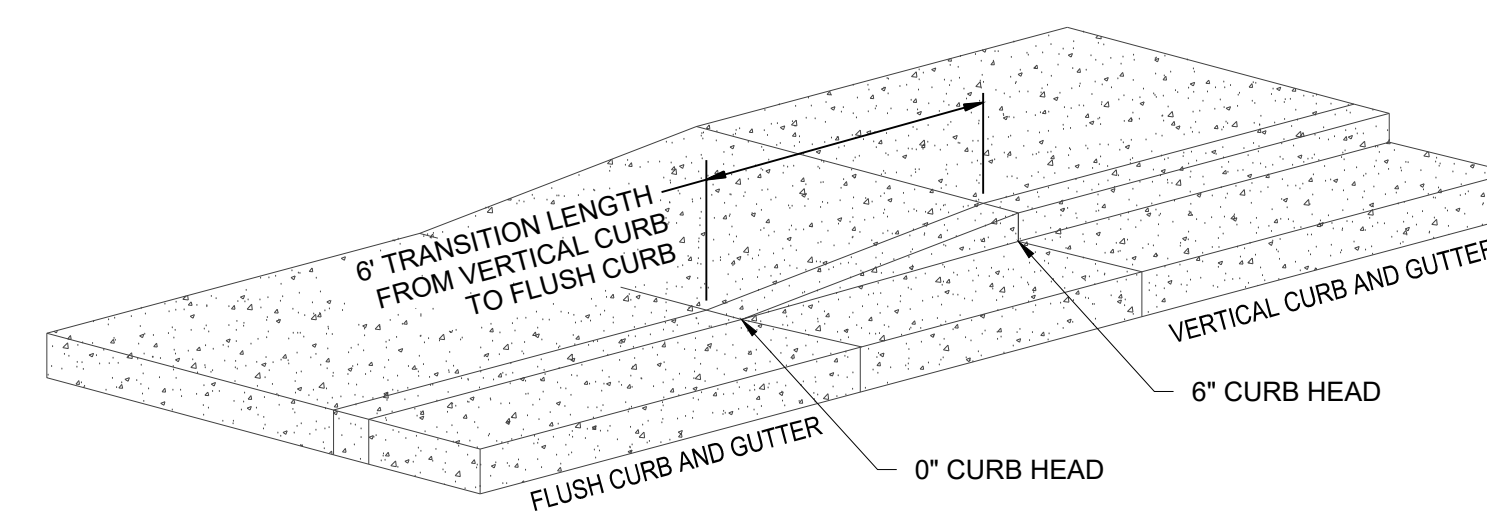
PLAN VIEW



ISOMETRIC VIEW

1 2' CURB CUT DETAIL  
C-454

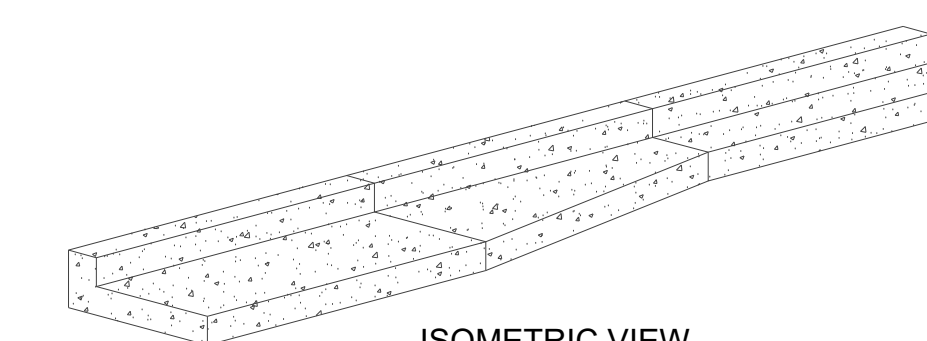
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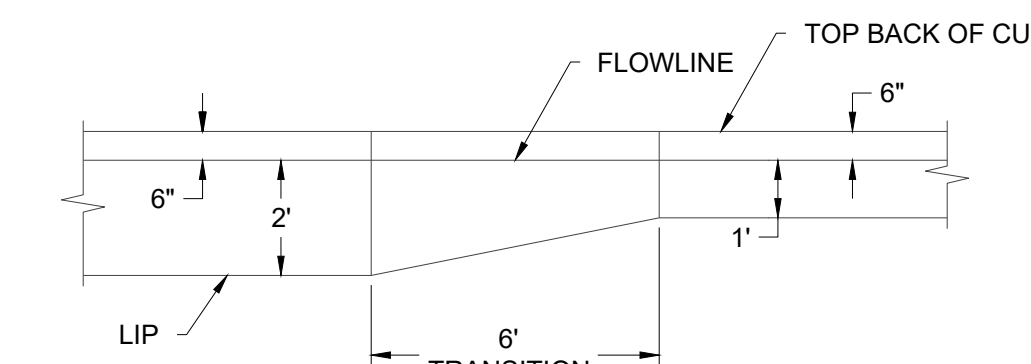
ISOMETRIC VIEW

3 TRANSITION FROM 6" VERTICAL CURB TO FLUSH CURB DETAIL  
C-454

NO SCALE



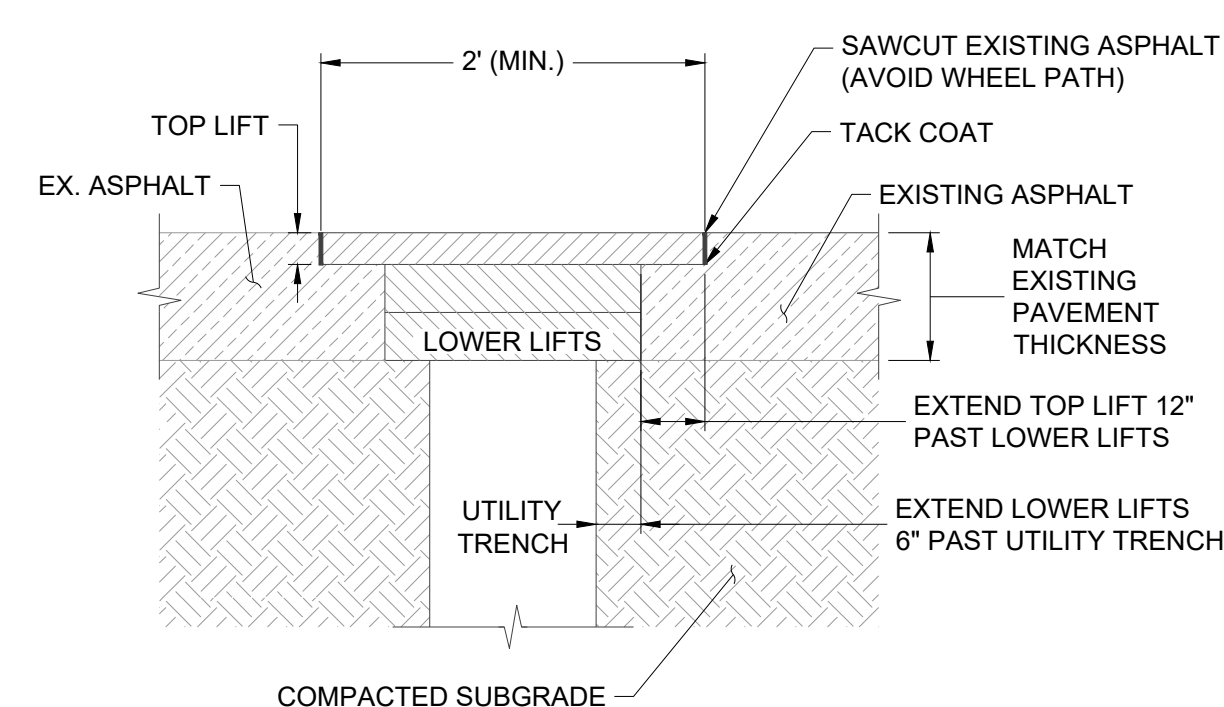
ISOMETRIC VIEW



PLAN VIEW

2 TRANSITION FROM 2' GUTTER TO 1' GUTTER DETAIL  
C-454

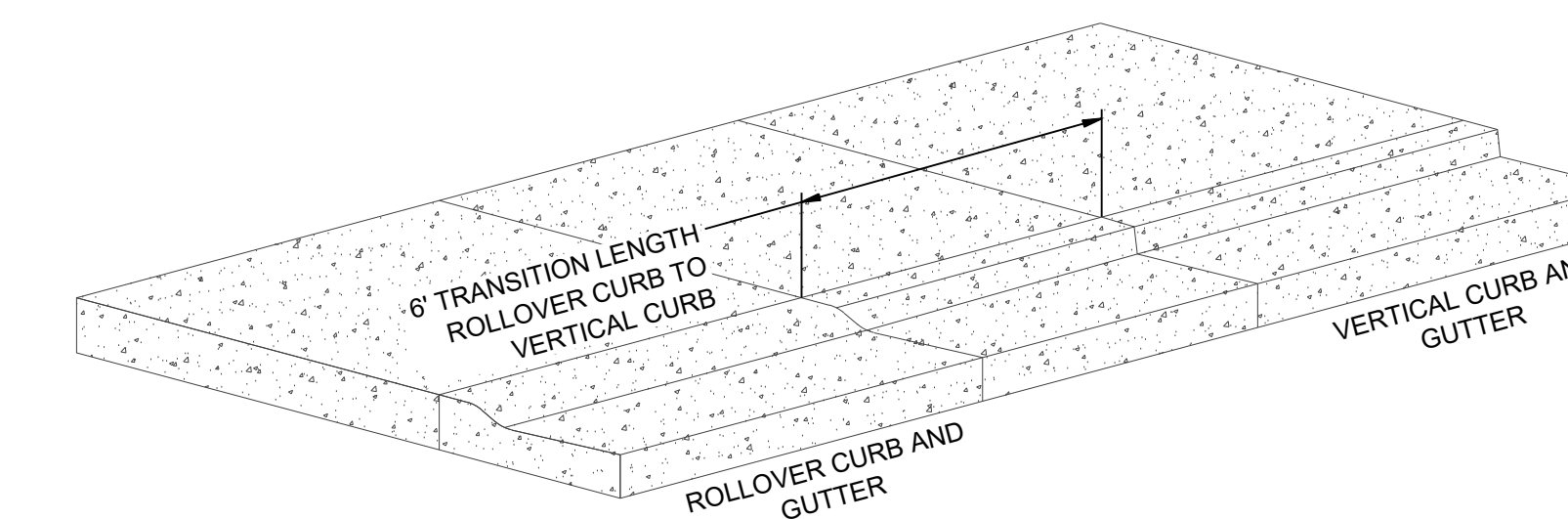
NO SCALE



5 UTILITY CUT ASPHALT PATCHBACK DETAIL  
C-454

NO SCALE

3" MAXIMUM COMPACTED LIFTS



ISOMETRIC VIEW

4 TRANSITION FROM ROLLOVER CURB TO 6" VERTICAL CURB DETAIL  
C-454

NO SCALE

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