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Structural Engineer

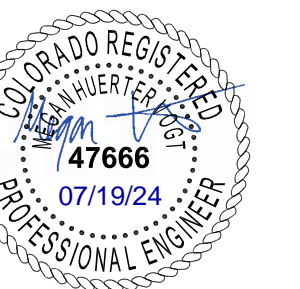
KL&A, Inc.
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Telephone: 303-384-9910

Mechanical + Plumbing Engineer

The Ballard Group, Inc.
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Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
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Issue _____ Date _____

SCHEMATIC DESIGN 01 MAY 2023
DESIGN DEVELOPMENT 20 OCT 2023
PERMIT / GMP SET 17 MAY 2024

COB-001 19 JULY 2024

Project Number: 22-139

Drawn By:

Reviewed By:

Approved By:

COVER SHEET

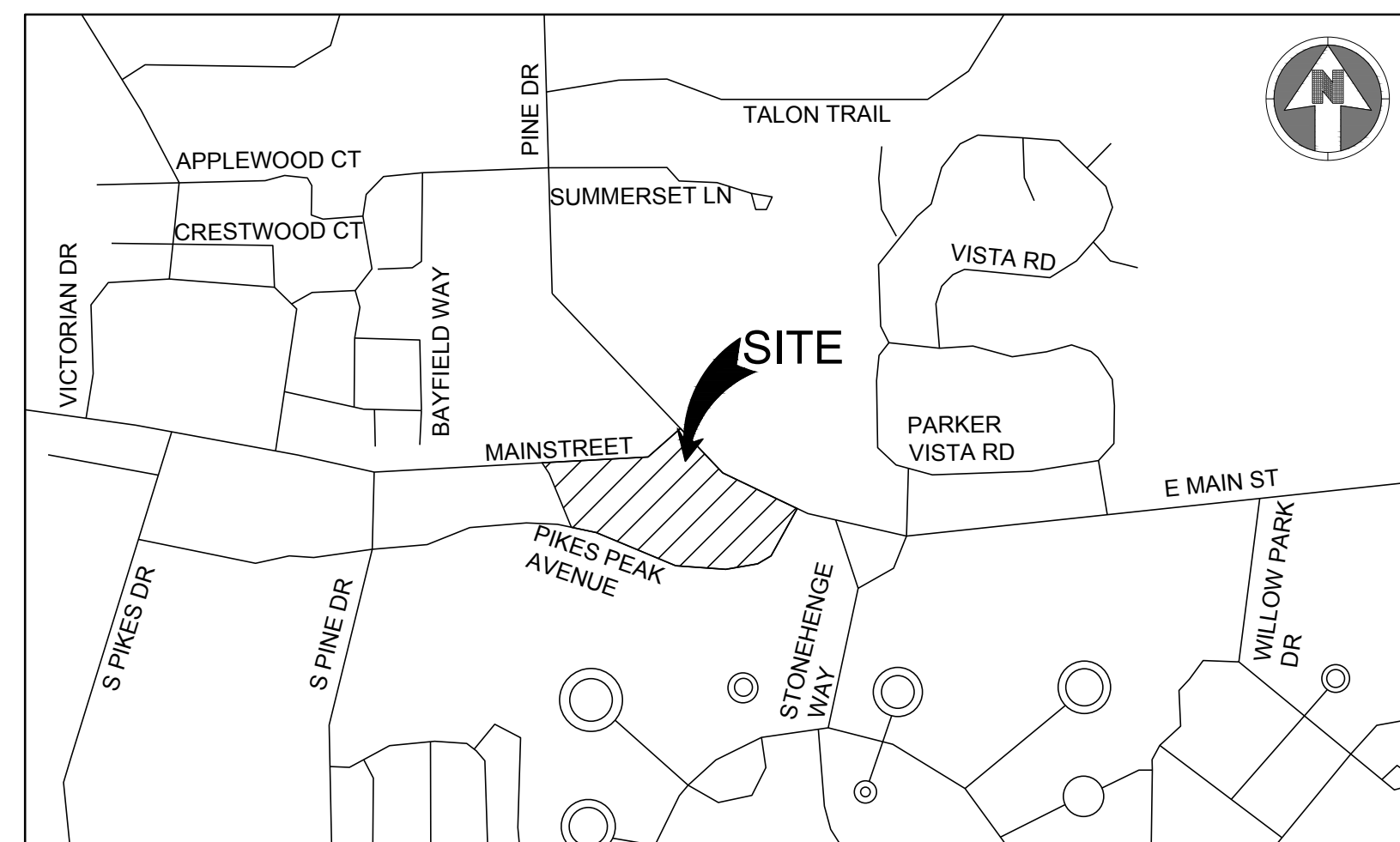
C-000

CONSTRUCTION PLANS

FOR

PARKER TOWN HALL

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



VICINITY MAP
NTS

Sheet Number	Sheet Title
C-000	COVER SHEET
C-001	CIVIL NOTES AND LEGEND
C-021	SITE PLAN
C-111	DEMOLITION PLAN
C-121	HORIZONTAL CONTROL PLAN
C-122	HORIZONTAL CONTROL DATA
C-211	GRADING PLAN
C-240	EROSION CONTROL COVER SHEET
C-241	INITIAL EROSION CONTROL PLAN
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C-243	FINAL EROSION CONTROL PLAN
C-244	OFF-SITE EROSION CONTROL PLAN
C-251	EROSION CONTROL DETAILS
C-252	EROSION CONTROL DETAILS
C-253	EROSION CONTROL DETAILS
C-254	EROSION CONTROL DETAILS
C-255	EROSION CONTROL DETAILS
C-256	EROSION CONTROL DETAILS
C-257	EROSION CONTROL DETAILS
C-258	EROSION CONTROL DETAILS
C-259	EROSION CONTROL DETAILS
C-311	UTILITY PLAN
C-331	STORM PLAN AND PROFILES
C-332	STORM PLAN AND PROFILES
C-333	STORM PLAN AND PROFILES
C-411	PAVING PLAN
C-421	SIGNAGE AND STRIPING PLAN
C-451	SITE DETAILS
C-452	SITE DETAILS
C-453	SITE DETAILS
C-454	SITE DETAILS
C-500	WATER AND SANITARY COVER SHEET
C-501	WATER AND SANITARY NOTES AND LEGEND
C-511	UTILITY PLAN
C-531	SANITARY PLAN AND PROFILE
C-541	WATER AND SANITARY DETAILS
C-552	WATER AND SANITARY DETAILS

ENGINEER'S STATEMENT

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.

MEGAN HUERTER VOGT
REGISTERED PROFESSIONAL ENGINEER
COLORADO NO. 47666

ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO WATER UTILITY STANDARDS. THE NUMBER AND LOCATIONS OF THE FIRE HYDRANTS AS SHOWN ON THE OVERALL UTILITY PLAN ARE CORRECT AS SPECIFIED BY THE TOWN OF PARKER COMMUNITY DEVELOPMENT DEPARTMENT

[Signature] 07/29/2024
DATE

FIRE CODE OFFICIAL OR DESIGNATED REPRESENTATIVE

[NOTE - UNDERGROUND FIRE LINE (ULF) SUBMITTAL DOCUMENTS MUST MEET THE REQUIREMENTS OF NFPA 24 WHEN SUBMITTING FOR REVIEW.]

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER

Town of Parker, Director of Engineering/Public Works _____ Date _____

CONTACTS:

OWNER'S REPRESENTATIVE	CIVIL ENGINEER	SURVEYOR
TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546	S.A. MIRO INC. MEGAN H. VOGT 4582 SOUTH ULSTER STREET SUITE 750 DENVER, CO 80237 303-741-3737	AZTEC CONSULTANTS, INC. DANIEL E. DAVIS 300 EAST MINERAL AVE, SUITE 1 LITTLETON, CO 80122 P: (303) 713-1898

PUBLIC UTILITIES:

SOUTH METRO FIRE DISTRICT	PARKER WATER & SANITATION DISTRICT	TOWN OF PARKER
9195 E. MINERAL AVENUE CENTENNIAL, CO 80112 P: (720) 989-2244	18100 WOODMAN ROAD PARKER, CO 80134 P: (303) 841-4627	20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546

BASIS OF BEARING:

BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°29'07" EAST, A DISTANCE OF 2,648.44 FEET.

BENCHMARK:

1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTLETON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL
ELEVATION=5399.27 (NAVD 88).

Parker Town Hall

Expansion

Parker, CO
22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

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3198 Spaw Boulevard
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Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034

MASTER LEGEND:

PROPOSED	EXISTING

ABBREVIATIONS:

BFV	BUTTERFLY VALVE	PCC	POINT OF COMPOUND CURVATURE
BOV	BLOW OFF VALVE	PCR	POINT OF CURB RETURN
BW	BOTTOM OF WALL @ FINISHED GRADE	PI	POINT ON INTERSECTION
CL	CENTERLINE	PRC	POINT OF REVERSE CURVATURE
CMP	CORRUGATED METAL PIPE	PT	POINT OF TANGENCY
EJ	EXPANSION JOINT	PVC	POLYVINYL CHLORIDE
ELEC.	ELECTRIC	RCB	REINFORCED CONCRETE BOX
ELEV.	ELEVATION	RL	RIDGE LINE
EX	EXISTING	ROW	RIGHT OF WAY
FES	FLARED END SECTION	SD	STORM DRAINAGE SEWER
FG	FINISHED GRADE	SS	SANITARY SEWER
FS	FINISHED SURFACE	STA	STATION
FL	FLOWLINE	STRM	STORM SEWER
GB	GRADE BREAK	TOP	TOP OF FLANGE
HP	HIGH POINT	TOP	TOP OF PIPE
FH	FIRE HYDRANT	TW	TOP OF WALL @ FINISHED GRADE
INV.	INVERT	TYP.	TYPICAL
IRR	IRRIGATION	VCP	VERTICAL CLAY PIPE
KB	KICK BLOCK	VPC	VERTICAL POINT OF CURVATURE
LF	LINEAR FEET	VPI	VERTICAL POINT OF INTERSECTION
LP	LOW POINT	VPT	VERTICAL POINT OF TANGENCY
LT	LEFT	WATR	WATER LINE
ME	MATCH EXISTING		
MH	MANHOLE		
PC	POINT OF CURVATURE		

TOWN OF PARKER STORM DRAINAGE INFRASTRUCTURE NOTES

- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- THE CONTRACTOR SHALL COMPLY WITH THE 'COLORADO WATER QUALITY CONTROL ACT' (TITLE 25, ARTICLE 8 CRS), THE 'PROTECTION OF FISHING STREAMS' TITLE 33, ARTICLE 5, CRS), THE 'CLEAN WATER ACT' (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72 (6 CSR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUTED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES.
- CONSULT WITH STORMWATER ENGINEERING DIVISION
STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (SPOT CLASS 2) SHALL CONFORM TO DOT STANDARD SPECIFICATION 203.07.
- ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:
 - 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC)
 - 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
- EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS AND STRUCTURES. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
- PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

TOWN OF PARKER DEPARTMENT OF ENGINEERING/ PUBLIC WORKS STANDARD CONSTRUCTION NOTES

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- THE TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE TOWN INSPECTOR'S ABSENCE. HOWEVER, THE TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER DIRECTOR OF ENGINEERING/PUBLIC WORKS, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY USE PERMIT APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) 'SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION'.
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR. SUCH RESTORATION MAY BE REQUIRED TO BE COMPLETED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR 'STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY'. FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WOOD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE FOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND REORDER SERVICE.
- ALL CONCRETE PAVING AND SIDEWALK JOINTS SHALL BE SAW CUT TO A MINIMUM DEPTH OF 1/3 (ONE-THIRD) THE THICKNESS OF CONCRETE.

GENERAL NOTES:

- THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS AT AND ADJACENT TO THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THE WORK SHOWN ON THE PLANS OR DESCRIBED IN THE SPECIFICATIONS IN A SATISFACTORY MANNER. THE CONTRACTOR SHALL FURNISH ALL MATERIALS, EQUIPMENT, TOOLS, LABOR, AND INCIDENTALS TO COMPLETE THE WORK.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AT THEIR EXPENSE TO COMPLETE ALL GRADING, UTILITY WORK AND INSTALLATION OF EROSION CONTROL MEASURES.
- THE CONTRACTOR SHALL COORDINATE WORK WITH ALL ADJACENT PROJECTS.
- DURING CONSTRUCTION IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING UTILITY LINES SHOWN ON THESE PLANS AND THOSE UTILITY LINES WHICH MAY NOT BE SHOWN ON THESE PLANS.
- EXISTING SITE INFORMATION SHOWN ON THESE DRAWINGS IS BASED UPON AVAILABLE SURVEY BY AZTEC CONSULTANTS, INC. DATED 08/11/2022. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF THE COMPLETENESS OR ACCURACY OF THE EXISTING CONDITIONS PRESENTED. THE CONTRACTOR SHALL INSPECT THE SITE TO ASSESS EXISTING CONDITIONS, AND SATISFY THEMSELVES THAT THE DOCUMENTS ACCURATELY REPRESENT THE SITE CONDITIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES FOUND DURING INITIAL INSPECTIONS, OR SUBSEQUENTLY, SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY FOR RESOLUTION RELATIVE TO THE INTENDED CONSTRUCTION.
- THE EXISTING UTILITIES SHOWN ON THESE PLANS HAVE BEEN LOCATED USING FIELD SURVEY DATA LOCATING MANHOLES, WATER VALVES, ELECTRICAL EQUIPMENT, AND OTHER FEATURES FOUND AT THE SURFACE. NO SUBSURFACE EXPLORATION HAS BEEN DONE TO VERIFY EXACT LOCATION OF EXISTING UTILITIES. THE CONTRACTOR SHALL POthOLE EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICT PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT AND DELAY RELATED TO UNVERIFIED EXISTING CONDITIONS, WHERE THE CONTRACTOR FINDS CONFLICTS OR DISCREPANCIES, THEY SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY APPROPRIATE ENTITIES FOR UTILITY LOCATES AND START OF CONSTRUCTION PRIOR TO ANY WORK BEING STARTED.
- THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH ALL AFFECTED PARTIES PRIOR TO ANY OUTAGES OR SHUTDOWN OF SERVICE.
- ALL QUANTITIES SHOWN ON THESE PLANS ARE SHOWN FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL QUANTITIES PRIOR TO CONSTRUCTION AND ORDERING OF ANY MATERIALS.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL FOLLOW/VERIFY THE INTENT OF THE DESIGN AT ALL LOCATIONS OF TIE-IN TO EXISTING FEATURES.

DEMOLITION NOTES:

- UNLESS OTHERWISE NOTED, EXISTING UTILITIES AND STRUCTURES SHALL REMAIN AND BE PROTECTED IN PLACE.
- THE CONTRACTOR SHALL PROTECT ALL ADJACENT IMPROVEMENTS (BUILDINGS, ROADWAYS, FENCES, PARKING LOTS, UTILITIES, ETC.) FROM DAMAGE AND EROSION. ALL DISTURBED AREAS OUTSIDE OF THE LIMITS OF WORK SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.

HORIZONTAL CONTROL NOTES:

- BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.
- THE CONTRACTOR SHALL VERIFY THAT BUILDING GRID LOCATIONS ARE CONSISTENT WITH LATEST ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.

PAVING NOTES:

- THE CONTRACTOR SHALL ADJUST EXISTING AND PROPOSED MANHOLES, WATER VALVES AND OTHER SURFACE APPURTENANCES WITHIN THE LIMITS OF WORK TO FINISHED GRADE.
- THE CONTRACTOR SHALL REFER TO ARCHITECTURAL/LANDSCAPE PLANS FOR CONCRETE SCORING PATTERNS, COLOR AND FINISHING.
- PROPOSED PAVEMENT DESIGN IS BASED UPON THE GEOTECHNICAL REPORT BY KUMAR & ASSOCIATES DATED 01/17/2023. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF PAVEMENT PERFORMANCE.

UTILITY NOTES:

- LOCATION OF ALL EXISTING UTILITIES (PRIVATE AND PUBLIC) SHALL BE IDENTIFIED AND VERIFIED BY THE CONTRACTOR PRIOR TO MOBILIZATION, CONSTRUCTION AND ORDERING OF MATERIALS. IF ANY CONFLICTS OR DISCREPANCIES ARE FOUND THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY. THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT, REPAIR AND DELAYS RELATED TO ANY AND ALL UNVERIFIED EXISTING CONDITIONS.
- ALL UTILITY LENGTHS AND QUANTITIES SHOWN ON THESE PLANS ARE FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND QUANTITIES PRIOR TO ORDERING OF ANY MATERIALS.
- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
- WHERE FLARED END SECTIONS ARE REQUIRED, THE LENGTH OF PIPE INCLUDES THE FLARED END SECTION.
- ALL UTILITY WORK SHALL BE COMPLETED PRIOR TO ANY PAVING.
- CONTRACTOR SHALL SURVEY THE LOCATION AND ELEVATION OF ALL INSTALLED UTILITIES PRIOR TO TRENCH BACKFILL, AND PROVIDE SURVEY INFORMATION TO THE ENGINEER UPON COMPLETION OF THE PROJECT FOR AS-BUILT DOCUMENTATION.

GRADING NOTES:

- SITE GRADING (EXCAVATION, EMBANKMENTS, AND COMPACTION) SHALL CONFORM TO THE RECOMMENDATIONS OF THE LATEST GEOTECHNICAL REPORT FOR THIS PROPERTY.
- SPOT ELEVATIONS MAY BE ABBREVIATED FOR CLARITY. REFER TO THE ADJACENT CONTOUR LABELS FOR FULL ELEVATIONS.
- MAXIMUM PROPOSED SLOPES SHALL BE 4:1 UNLESS NOTED OTHERWISE.
- SPOT ELEVATIONS ARE TO FLOWLINE UNLESS NOTED OTHERWISE.
- PROPOSED GRADING SHALL MEET AND MATCH EXISTING GRADE AT LIMITS OF DISTURBANCE. PROPOSED GRADING AS SHOWN REFLECTS PLANNED FINISHED GRADE. THE CONTRACTOR SHALL ACCOUNT FOR PAVEMENT DEPTH, SUBGRADE REQUIREMENTS, OVEREXCAVATION, TOPSOIL, AND ANY PLANNED SURFACING CONSTRUCTION.
- MAXIMUM TEMPORARY SLOPES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEERS RECOMMENDATIONS AND IN COMPLIANCE WITH OSHA REQUIREMENTS.
- THE CONTRACTOR SHALL CONFORM TO OVEREXCAVATION REQUIREMENTS DETAILED WITHIN THE PROJECT SPECIFICATIONS AND GEOTECHNICAL REPORT. OVEREXCAVATION REQUIREMENTS ARE NOT DEPICTED ON THE CIVIL PLANS.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works Date

Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

COB-001 19 JULY 2024

Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:

CIVIL NOTES AND LEGEND

Parker Town Hall Expansion
Parker, CO
22-093

Town of Parker
20120 Main Street
Parker, CO 80138

Architect
AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80211
Telephone: 303-294-9448

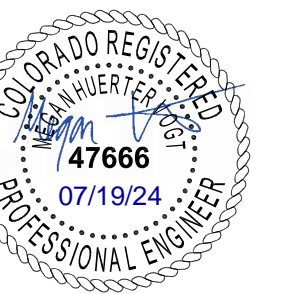
Civil Engineer
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4582 S. Ute Street #750
Denver, CO 80227
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Landscape Architect
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1130 31st Street #101
Denver, CO 80202
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Structural Engineer
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Golden, CO 80401
Telephone: 303-384-9910

Mechanical + Plumbing Engineer
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Lawrence, CO 80427
Telephone: 303-988-4514

Electrical Engineer + Technology
AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034

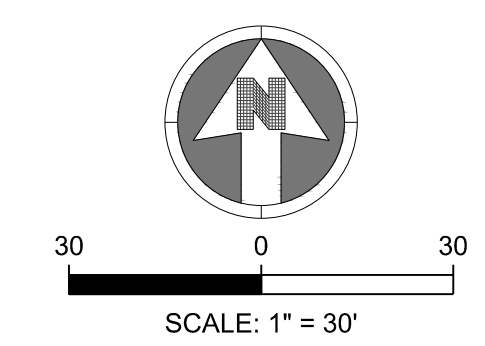
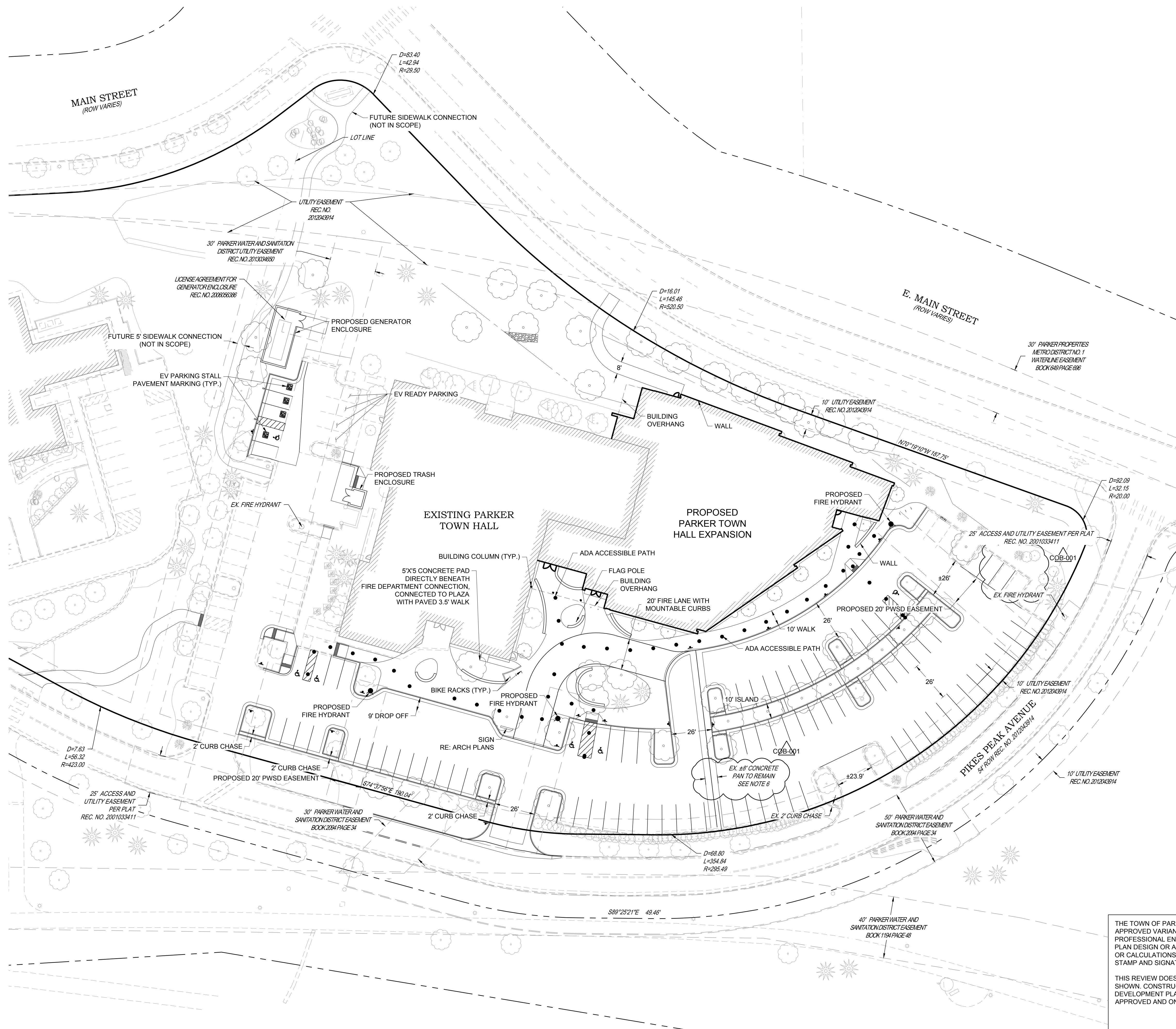


Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

C08-001 19 JULY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

SITE PLAN



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
 - ADD ALTERNATE 1 - SEAL COAT EXISTING ASPHALT AND RESTRIPE PARKING LOT.
 - ADD ALTERNATE 2 - ADD ADDITIONAL PEDESTRIAN CROSSING INCLUDING PUSH BUTTON SIGNAL AND RAISED CROSSWALK ON PIKES PEAK AVENUE.
 - A SITE INSPECTION IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION.
 - NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THE CURB GUTTER AND FIRST LIFT OF ASPHALT PINE (UNPAVED).
 - THE EXISTING CONCRETE PAVEMENT MAY NEED TO BE REPLACED, IF NEEDED, MATCH EXISTING 8' WIDTH, GRADES, AND CONCRETE THICKNESS.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works _____ Date _____

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

20120 Main Street
Parker, CO 80138

Architect

AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80202
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Landscape Architect

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Telephone: 303-628-0000

Structural Engineer

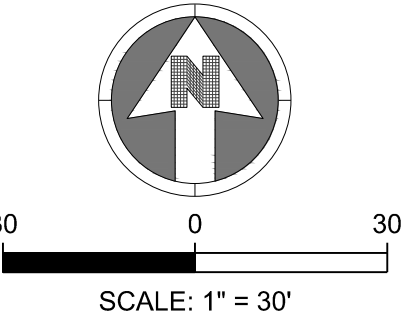
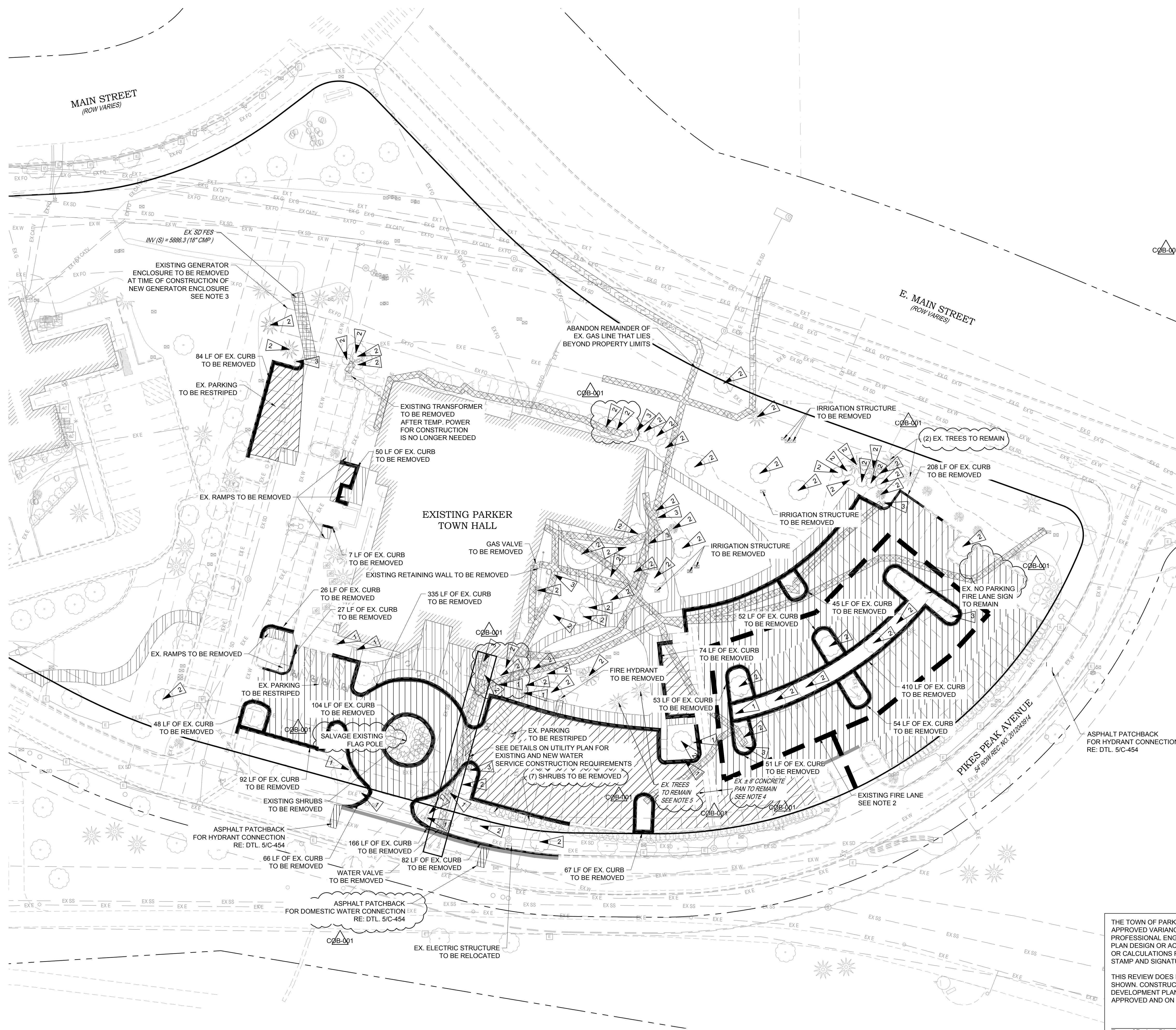
KL&A, Inc.
1717 Washington Ave.
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Mechanical + Plumbing Engineer

The Ballard Group, Inc.
2525 S. Wadsworth Blvd. #200
Lawrence, CO 80407
Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
 - A SITE INSPECTION BY A REPRESENTATIVE OF THE SOUTH METRO FIRE DEPARTMENT IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION. NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THAT CURB, GUTTER, AND FIRST LIFT OF ASPHALT ARE INSTALLED FOR THE FULL LENGTH OF THE FIRE LANE.
 - THE EXISTING GENERATOR SHALL REMAIN IN PLACE AS LONG AS POSSIBLE UNTIL REMOVAL IS NECESSARY TO CONSTRUCT THE NEW GENERATOR ENCLOSURE. AT THIS TIME, THE CONTRACTOR SHALL LOAD, TRANSPORT, AND OFFLOAD THE EXISTING GENERATOR TO TOWN OF PARKER PUBLIC WORKS OPERATIONS CENTER (PWOC) LOCATED AT 9045 TAMMY LANE, PARKER, CO 80134. COORDINATE DELIVERY WITH ADRAIN ORZESCU 303-949-8279.
 - THE EXISTING CONCRETE PAN MAY NEED TO BE REPLACED, IF NEEDED, MATCH EXISTING 8' WIDTH, GRADES, AND CONCRETE THICKNESS.
 - EXISTING TREES TO REMAIN WILL BE EVALUATED THROUGHOUT CONSTRUCTION AND MAY NEED TO BE REPLACED.
 - 48 TREES TO BE REMOVED. EXISTING TREES ARE SHOWN AS 2X LARGER THAN SHRUBS TYPICALLY.

LEGEND:

	UTILITY TO BE REMOVED
	CURB AND GUTTER REMOVAL
	CONCRETE SIDEWALK REMOVAL
	ASPHALT PAVEMENT REMOVAL SEE NOTE 2
	UTILITY TO BE ABANDONED
	SAWCUT LINE
	REMOVE/RELOCATE SIGN
	REMOVE/RELOCATE TREE OR SHRUB SEE NOTE 6
	REMOVE/RELOCATE LIGHT POLE

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Town of Parker, Director of Engineering/Public Works _____ Date _____



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DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

C0B-001 19 JULY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

DEMOLITION PLAN

Parker Town Hall Expansion
Parker, CO
22-093

Town of Parker
20120 Mainstreet
Parker, CO 80138

Architect
AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80221
Telephone: 303-294-9448

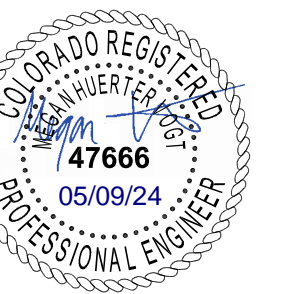
Civil Engineer
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Denver, CO 80227
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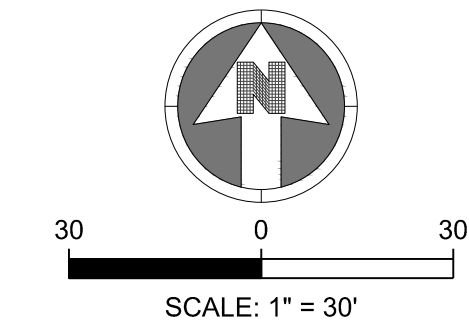
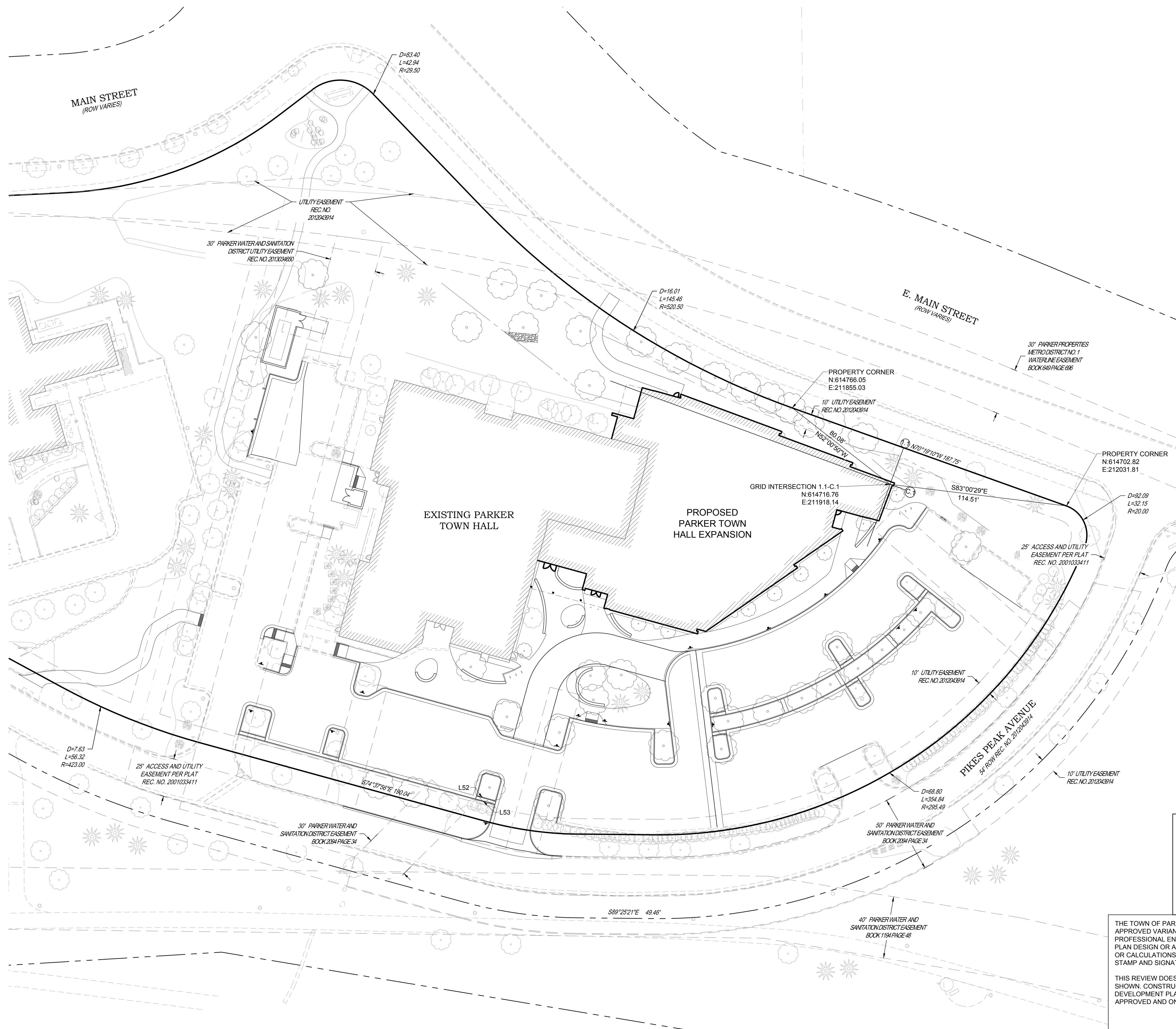
Mechanical + Plumbing Engineer
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1900 Wazee Street #205
Lawrence, CO 80427
Telephone: 303-988-4514

Electrical Engineer + Technology
AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:



NOTES:
1. SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.

BASIS OF BEARING:
BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1995", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.

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Town of Parker, Director of Engineering/Public Works _____ Date _____

**Parker Town Hall
Expansion**

**Parker, CO
22-093**

Town of Parker
20120 Mainstreet
Parker, CO 80138

Architect

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3198 Speer Boulevard
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Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



CURVE TABLE						
SEGMENT	LENGTH	RADIUS	CHORD DIRECTION	CHORD LENGTH	START N.E.	END N.E.
C1	7.82'	5.00'	S60°12'50"W	7.05'	614623.91, 211520.91	614620.41, 211514.79
C2	1.56'	1.00'	S60°12'50"W	1.41'	614618.76, 211515.36	614618.06, 211514.14
C3	1.60'	1.00'	N61°19'20"E	1.44'	614595.30, 211523.22	614595.99, 211524.48
C4	8.00'	5.00'	N61°19'20"E	7.18'	614594.59, 211525.54	614598.03, 211531.83
C5	8.11'	5.00'	S31°22'20"E	7.25'	614586.87, 211572.28	614580.68, 211576.05
C6	3.54'	250.00'	S78°13'50"E	3.54'	614580.68, 211576.05	614579.96, 211579.51
C7	3.58'	5.00'	N80°50'10"E	3.51'	614579.96, 211579.51	614580.52, 211582.98
C8	3.93'	5.00'	S82°50'20"W	3.83'	614584.97, 211593.75	614584.49, 211589.95
C9	3.92'	5.00'	N52°09'40"W	3.82'	614564.79, 211661.66	614567.14, 211658.64
C10	3.92'	5.00'	S52°09'40"E	3.82'	614566.75, 211666.24	614564.41, 211669.26
C11	4.75'	3.00'	N60°01'00"E	4.27'	614545.13, 211703.02	614547.26, 211706.71
C12	55.00'	227.36'	S82°16'00"E	54.87'	614566.23, 211711.68	614558.84, 211766.05
C13	4.75'	3.00'	S44°32'40"E	4.27'	614539.23, 211765.77	614536.18, 211768.77
C14	7.54'	250.00'	N89°14'40"E	7.54'	614536.18, 211768.77	614536.28, 211776.30
C15	4.99'	3.00'	N40°45'40"E	4.43'	614536.28, 211776.30	614539.64, 211779.20
C16	31.37'	20.00'	S38°04'40"W	28.25'	614609.51, 211790.89	614587.27, 211773.47
C17	157.40'	178.00'	N59°32'30"E	152.32'	614608.90, 211785.14	614686.11, 211916.44
C18	13.70'	9.00'	S77°49'50"W	12.42'	614688.72, 211928.58	614686.11, 211916.44
C19	4.52'	3.00'	N78°18'30"E	4.10'	614688.52, 211928.91	614689.35, 211932.92
C20	4.67'	3.00'	S83°55'40"W	4.21'	614657.30, 211931.19	614656.85, 211927.00
C21	4.75'	3.00'	N6°08'20"W	4.27'	614628.14, 211962.95	614632.39, 211962.49
C22	4.75'	3.00'	N85°26'50"E	4.27'	614624.81, 211956.21	614625.15, 211960.47
C23	64.00'	232.00'	N46°42'00"E	63.80'	614592.49, 211896.94	614634.60, 211944.87
C24	9.00'	232.00'	N57°42'50"E	9.00'	614587.69, 211889.33	614592.49, 211896.94
C25	83.00'	232.00'	N69°04'20"E	82.56'	614558.20, 211812.22	614587.69, 211889.33
C26	9.01'	232.00'	N80°26'10"E	9.01'	614558.20, 211803.33	614558.20, 211812.22
C27	10.36'	222.00'	N80°08'30"E	10.36'	614566.63, 211802.14	614568.41, 211812.34
C28	80.53'	222.00'	N68°24'40"E	80.09'	614568.41, 211812.34	614597.88, 211886.81
C29	9.79'	222.00'	N56°45'20"E	9.79'	614597.88, 211886.81	614603.24, 211895.00
C30	56.59'	222.00'	N48°11'20"E	56.44'	614603.24, 211895.00	614640.87, 211937.07
C31	4.67'	3.00'	S4°31'40"E	4.21'	614654.90, 211925.38	614650.70, 211925.71
C32	4.67'	3.00'	N79°05'30"W	4.21'	614615.62, 211886.49	614616.42, 211882.36
C33	4.67'	3.00'	S12°36'20"W	4.21'	614614.73, 211879.78	614610.62, 211878.86
C34	9.71'	220.00'	N56°45'20"E	9.71'	614599.57, 211885.75	614604.89, 211893.87
C35	4.67'	3.00'	N55°46'50"W	4.21'	614583.14, 211809.42	614585.51, 211805.94
C36	4.61'	3.00'	S37°07'20"W	4.17'	614584.97, 211802.85	614581.65, 211800.33
C37	10.21'	220.00'	N80°08'00"E	10.21'	614568.62, 211801.90	614570.37, 211811.95
C38	4.88'	3.00'	S53°25'50"E	4.36'	614541.90, 211805.11	614539.30, 211808.61
C39	4.75'	3.00'	N34°40'10"E	4.27'	614539.97, 211812.57	614543.48, 211815.00
C40	9.15'	234.00'	N80°26'30"E	9.15'	614554.72, 211803.57	614556.23, 211812.59
C41	4.75'	3.00'	S76°31'30"E	4.27'	614574.87, 211897.09	614573.87, 211901.24
C42	4.75'	3.00'	N11°57'00"E	4.27'	614575.81, 211904.31	614579.99, 211905.19
C43	9.08'	234.00'	N57°42'50"E	9.08'	614585.98, 211890.37	614590.83, 211898.04
C44	4.70'	3.00'	N31°34'20"W	4.23'	614514.24, 211706.84	614517.85, 211704.63
C45	8.76'	276.00'	S75°33'10"E	8.76'	614520.04, 211696.14	614517.85, 211704.63
C46	4.72'	3.00'	S60°20'00"W	4.24'	614520.04, 211696.14	614517.94, 211692.45
C47	26.79'	15.00'	S35°51'00"E	23.37'	614496.08, 211686.47	614477.14, 211700.16
C48	12.97'	98.00'	S78°34'10"E	12.96'	614482.98, 211669.48	614480.41, 211682.18
C50	4.71'	3.00'	N29°38'00"W	4.24'	614524.83, 211667.38	614528.52, 211665.29
C51	4.71'	3.00'	S60°22'00"W	4.24'	614530.39, 211658.48	614528.29, 211654.79
C52	72.25'	46.00'	S60°18'10"W	65.05'	614619.29, 211740.92	614587.07, 211684.42
C53	9.27'	26.00'	S84°54'50"E	9.22'	614609.71, 211775.96	614608.90, 211785.14
C54	40.84'	26.00'	S60°18'10"W	36.77'	614600.00, 211735.65	614581.79, 211703.71
C55	2.72'	46.00'	S76°24'00"E	2.72'	614590.42, 211770.68	614589.78, 211773.33

LINE TABLE				
SEGMENT	LENGTH	DIRECTION	STARTING N.E.	ENDING N.E.
L1	13.62'	S14°00'39"W	614796.36, 211539.22	614783.15, 211535.92
L2	18.21'	N75°59'21"W	614783.15, 211535.92	614787.55, 211518.26
L3	11.07'	S74°42'16"E	614731.06, 211564.58	614728.14, 211575.27
L4	13.72'	N15°37'16"E	614714.92, 211571.57	614728.14, 211575.27
L5	6.93'	S74°42'16"E	614715.40, 211571.71	614713.58, 211578.38
L6	6.83'	N74°49'40"W	614688.37, 211559.51	614690.16, 211552.92
L7	8.00'	N74°58'26"W	614621.84, 211528.63	614623.91, 211520.91
L8	1.45'	S15°24'02"W	614620.41, 211514.79	614619.01, 211514.41
L9	2.15'	S15°24'02"W	614619.01, 211514.41	614616.94, 211513.84
L10	12.02'	N74°58'26"W	614615.64, 211526.97	614618.76, 211515.36
L11	11.93'	S15°28'28"W	614607.48, 211527.67	614595.99, 211524.48
L12	4.24'	N72°49'40"W	614594.59, 211525.54	614595.84, 211521.48
L13	8.00'	S15°28'28"W	614605.74, 211533.97	614598.03, 211531.83
L14	8.00'	N74°46'21"W	614612.38, 211569.11	614614.48, 211561.39
L15	17.72'	N74°55'09"W	614601.68, 211576.27	614608.29, 211559.16
L16	15.34'	S15°04'51"W	614601.68, 211576.27	614586.87, 211572.28
L17	8.03'	N60°18'26"E	614580.52, 211582.98	614584.49, 211589.95
L18	67.29'	S74°37'56"E	614584.97, 211593.75	614567.14, 211658.64
L19	9.25'	S29°41'34"E	614564.79, 211661.66	614556.75, 211666.24
L20	35.01'	N74°37'56"W	614545.13, 211703.02	614554.41, 211669.26
L21	19.60'	S14°39'52"W	614566.23, 211711.68	614547.26, 211706.71
L22	19.62'	N0°48'13"E	614539.23, 211765.77	614558.84, 211766.05
L23	47.97'	N6°51'27"W	614539.64, 211779.20	614587.27, 211773.47
L24	15.44'	N35°09'56"E	614689.35, 211932.92	614701.97, 211941.81
L25	2.54'	N39°42'02"E	614654.90, 211925.38	614656.85, 211927.00
L26	40.00'	N51°29'12"W	614632.39, 211962.49	614657.30, 211931.19
L27	3.89'	N39°39'17"E	614625.15, 211960.47	614628.14, 211962.95
L28	14.98'	S49°12'15"E	614634.60, 211944.87	614624.81, 211956.21
L29	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L30	15.02'	S49°06'45"E	614650.70, 211925.71	614640.87, 211937.07
L31	15.02'	N34°30'28"W	614603.24, 211895.00	614615.62, 211896.49
L32	3.09'	N56°45'22"E	614614.73, 211879.78	614616.42, 211882.36
L33	15.02'	S31°58'48"E	614610.62, 211878.86	614597.88, 211886.81
L34	15.02'	N11°11'46"W	614568.41, 211812.34	614583.14, 211809.42
L35	3.14'	N80°04'12"E	614584.97, 211802.85	614585.51, 211805.94
L36	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L37	40.03'	S6°51'27"E	614581.65, 211800.33	614541.90, 211805.11
L38	4.01'	N80°29'03"E	614539.30, 211808.61	614539.97, 211812.57
L39	14.98'	N10°40'38"W	614543.48, 211815.00	614558.20, 211812.22
L40	14.98'	S31°10'33"E	614587.69, 211889.33	614574.87, 211897.09
L41	3.63'	N57°42'47"E	614573.87, 211901.24	614575.81, 211904.31
L42	14.98'	N33°23'54"W	614579.99, 211905.19	614592.49, 211896.94
L43	16.32'	S13°19'10"W	614514.24, 211706.84	614498.36, 211703.08
L44	22.66'	N15°18'26"E	614496.08, 211686.47	614517.94, 211692.45
L45	16.25'	S79°16'45"E	614480.41, 211682.18	614477.38, 211698.15
L46	13.07'	N74°46'42"W	614482.98, 211669.48	614486.41, 211656.87
L48	79.01'	S74°46'42"E	614509.16, 211565.62	614488.42, 211641.86
L50	7.06'	S74°37'56"E	614530.39, 211658.48	614528.52, 211665.29
L51	14.98'	N15°22'04"E	614513.84, 211650.82	614528.29, 211654.79
L52	13.06'	S74°37'56"E	614515.77, 211651.35	614512.31, 211663.94
L53	13.06'	S74°37'56"E	614513.84, 211650.82	614510.38, 211663.41
L54	92.00'	N74°37'56"W	614513.84, 211650.82	614538.23, 211562.11
L55	10.15'	N55°50'28"W	614538.23, 211562.11	614543.92, 211553.71
L56	19.51'	N15°22'04"E	614538.23, 211562.11	614557.03, 211567.28
L57	9.47'	N55°50'28"W	614540.26, 211562.67	614545.58, 211554.84
L58	35.51'	S15°18'26"W	614587.07, 211684.42	614552.82, 211675.04
L59	36.32'	N74°42'16"W	614609.71, 211775.96	614619.29, 211740.92
L60	36.32'	S74°42'16"E	614600.00, 211735.65	614590.42, 211770.68
L61	35.53'	S15°18'26"W	614581.79, 211703.71	614547.52, 211694.33

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Town of Parker, Director of Engineering/Public Works _____ Date _____

Issue _____ Date _____

SCHEM

Parker Town Hall Expansion

Parker, CO
22-093

Town of Parker
20120 Main Street
Parker, CO 80138

Architect
AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80202
Telephone: 303-294-9448

Civil Engineer
S.A. Miro, Inc.
4525 S. Ulster Street #750
Denver, CO 80227
Telephone: 303-741-3737

Landscape Architect
Wenk Associates
1130 1st Street #101
Denver, CO 80202
Telephone: 303-628-0000

Structural Engineer
K&A, Inc.
1717 Washington Ave.
Golden, CO 80202
Telephone: 303-384-9910

Mechanical + Plumbing Engineer
The Ballard Group, Inc.
2525 S. Wadsworth Blvd. #200
Lawrence, CO 80227
Telephone: 303-968-4514

Electrical Engineer + Technology
AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-956-3034

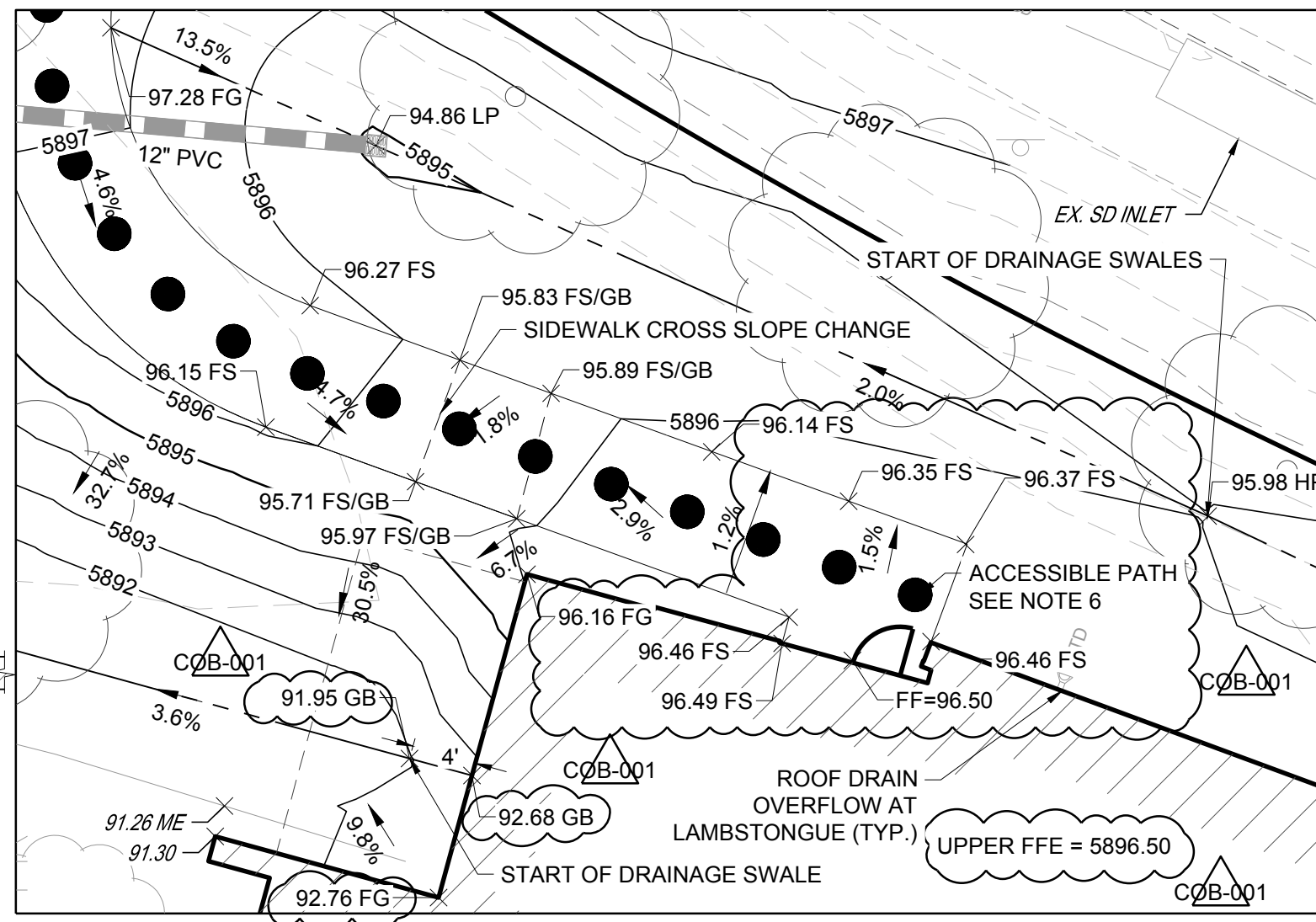
"TTW" DENOTES TRUE TOP OF WALL ELEVATIONS (WALL HEIGHT 1" MIN., 1.5" MAX.) FLAT TOP (CONSISTENT ELEVATION) FOR ALL WALLS*
*SEE NOTE 5

"FS" DENOTES FINISHED SURFACE AT HARD SCAPE SIDE OF WALL

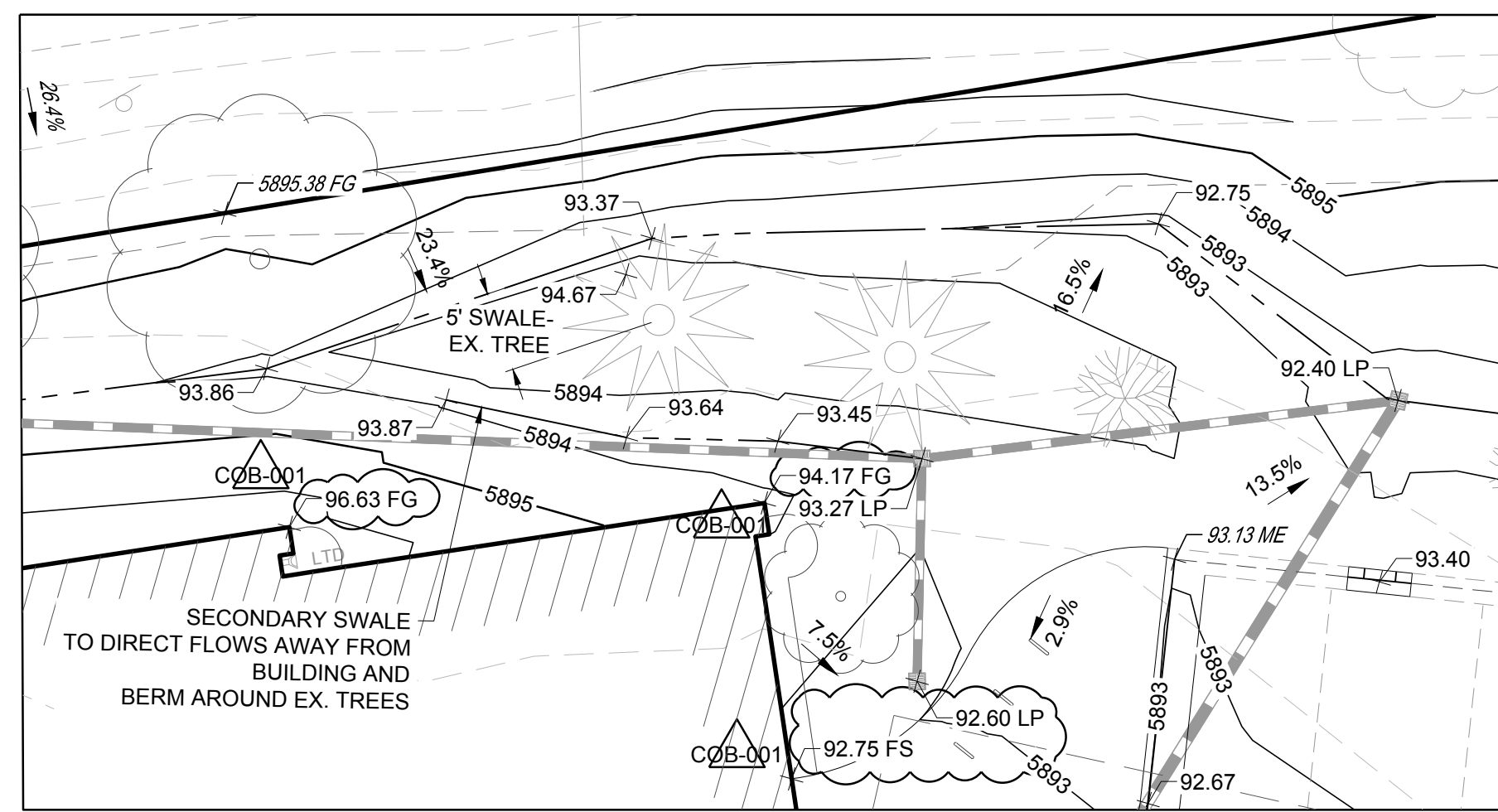
"FG" DENOTES FINISHED GRADE AT SOFT SCAPE SIDE OF WALL

WALL

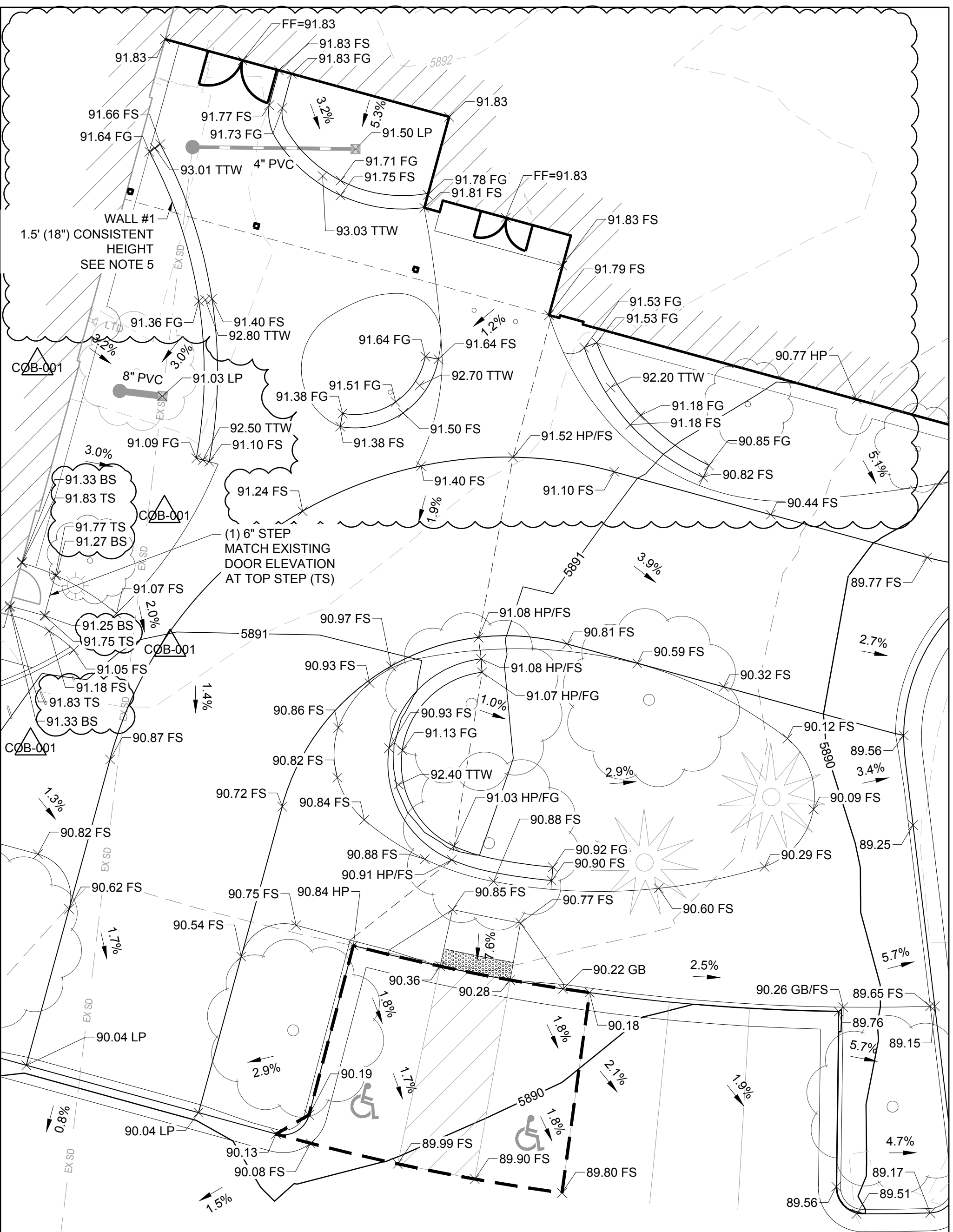
SEAT WALL GRADING LABEL DETAIL
NOT TO SCALE



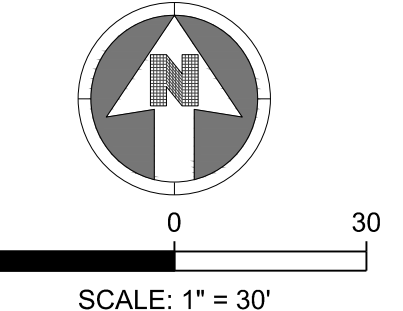
GRADING ENLARGEMENT 3
SCALE 1" = 10'



GRADING ENLARGEMENT 2
SCALE 1" = 10'



GRADING ENLARGEMENT 1
SCALE 1" = 10'



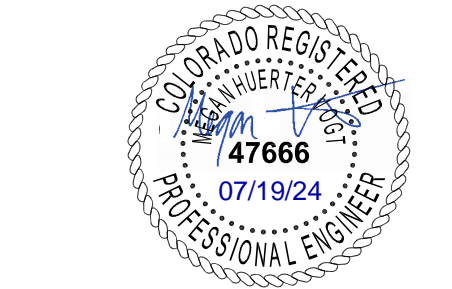
NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- MAXIMUM GRADE SHALL BE 2% IN ALL DIRECTIONS AT EVERY ACCESSIBLE PARKING STALL AND ACCESSIBLE.
- SPOT ELEVATIONS ARE TO THE FLOWLINE UNLESS OTHERWISE NOTED.
- SPOT ELEVATIONS HAVE BEEN ABBREVIATED FOR CLARITY. REFER TO THE ADJACENT CONTOUR LABELS FOR FULL ELEVATIONS.
- TRUE TOP OF WALL (TTW) ELEVATIONS SHALL REMAIN CONSISTENT AND WALL HEIGHT WILL VARY RELATIVE TO SURROUNDING FINISHED GRADES AND FINISHED SURFACES EXCEPTS WALL #1. WALL #1 HEIGHT WILL REMAIN CONSISTENT AND TRUE TOP OF WALL ELEVATION WILL VARY TO MATCH THE ADJACENT GRADES.
- ACCESSIBLE PATHS SHALL HAVE A 5% MAXIMUM TRAVERSE SLOPE AND A 2% MAXIMUM CROSS SLOPE.

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Town of Parker, Director of Engineering/Public Works _____ Date _____



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024
C0B-001	19 JULY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

GRADING PLAN

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80211
Telephone: 303-294-9448

Civil Engineer

S.A. Miro, Inc.
4582 S. Ulster Street #750
Denver, CO 80227
Telephone: 303-741-3737

Landscape Architect

Wenk Associates
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Denver, CO 80202
Telephone: 303-628-0000

Structural Engineer

KL&A, Inc.
1717 Washington Ave.
Golden, CO 80401
Telephone: 303-384-9910

Mechanical + Plumbing Engineer

The Ballard Group, Inc.
2525 S. Wadsworth Blvd. #200
Lakewood, CO 80227
Telephone: 303-988-4514

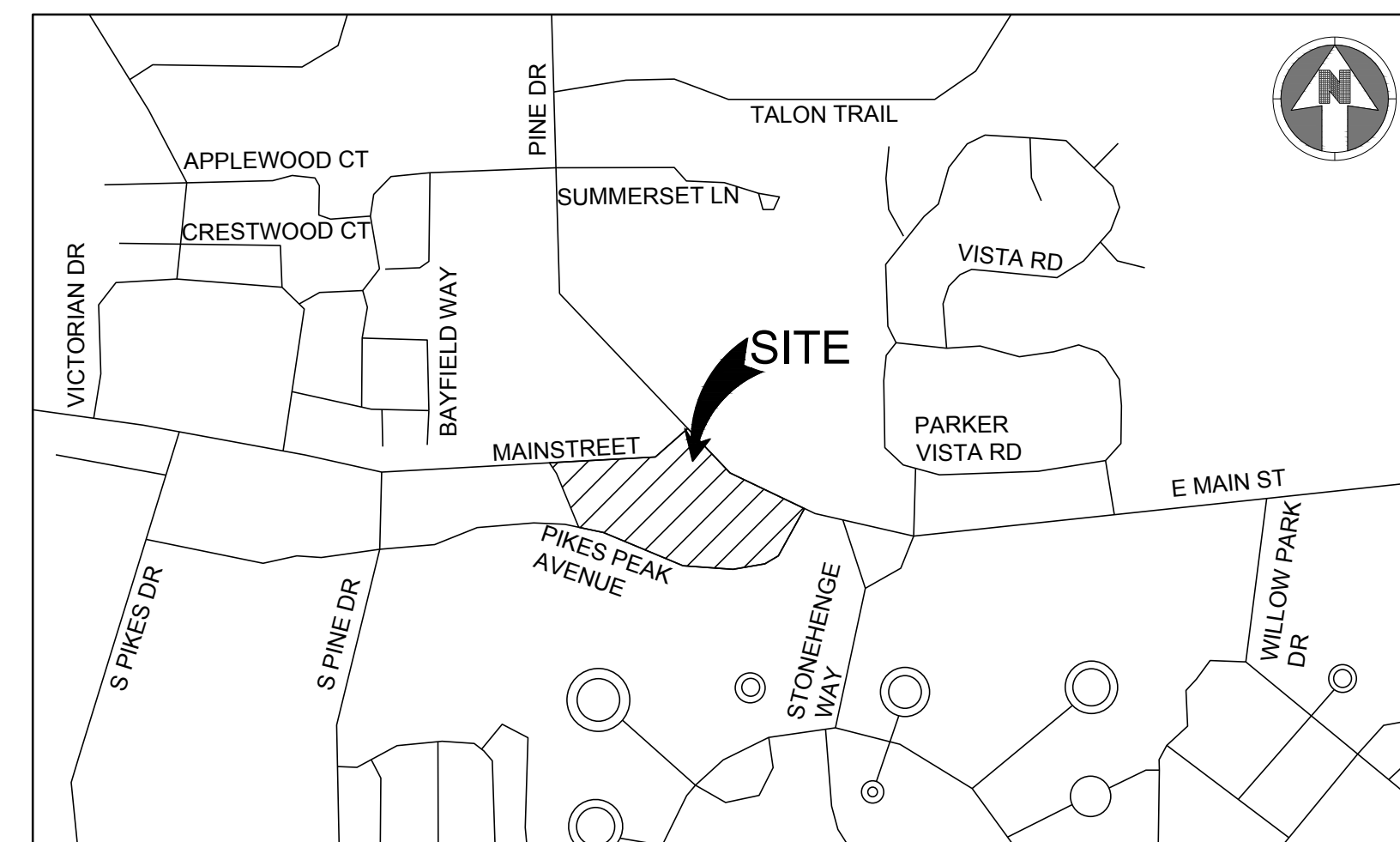
Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034

EROSION CONTROL CONSTRUCTION PLANS
FOR

PARKER TOWN HALL

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



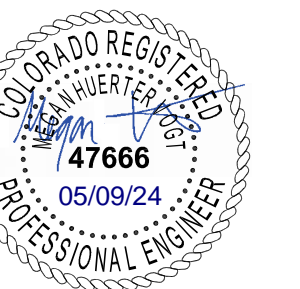
VICINITY MAP
NTS

Sheet List Table	
Sheet Number	Sheet Title
C-240	EROSION CONTROL COVER SHEET
C-241	INITIAL EROSION CONTROL PLAN
C-242	INTERIM EROSION CONTROL PLAN
C-243	FINAL EROSION CONTROL PLAN
C-244	OFF-SITE EROSION CONTROL PLAN
C-251	EROSION CONTROL DETAILS
C-252	EROSION CONTROL DETAILS
C-253	EROSION CONTROL DETAILS
C-254	EROSION CONTROL DETAILS
C-255	EROSION CONTROL DETAILS
C-256	EROSION CONTROL DETAILS
C-257	EROSION CONTROL DETAILS
C-258	EROSION CONTROL DETAILS
C-259	EROSION CONTROL DETAILS

ENGINEER'S STATEMENT

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.

MEGAN HUERTER VOGT
REGISTERED PROFESSIONAL ENGINEER
COLORADO NO. 47666



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

CONTACTS:

OWNER'S REPRESENTATIVE TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546	CIVIL ENGINEER S.A. MIRO INC. MEGAN H. VOGT 4582 SOUTH ULSTER STREET SUITE 750 DENVER, CO 80237 303-741-3737	SURVEYOR AZTEC CONSULTANTS, INC. DANIEL E. DAVIS 300 EAST MINERAL AVE, SUITE 1 LITTLETON, CO 80122 P: (303)-713-1888
SOUTH METRO FIRE DISTRICT 9195 E. MINERAL AVENUE CENTENNIAL, CO 80112 P: (720) 989-2244	PARKER WATER & SANITATION DISTRICT 18100 WOODMAN ROAD PARKER, CO 80134 P: (303) 841-4627	TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546

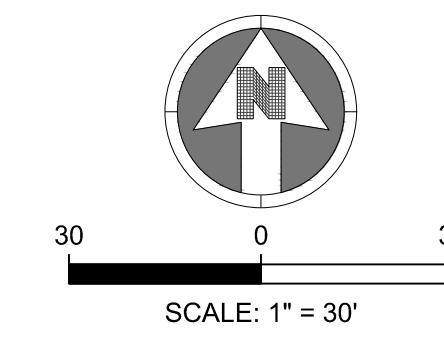
BASIS OF BEARING:
BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°29'07" EAST, A DISTANCE OF 2,648.44 FEET.

BENCHMARK:
1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTELTON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL
ELEVATION=5399.27 (NAVD 88).

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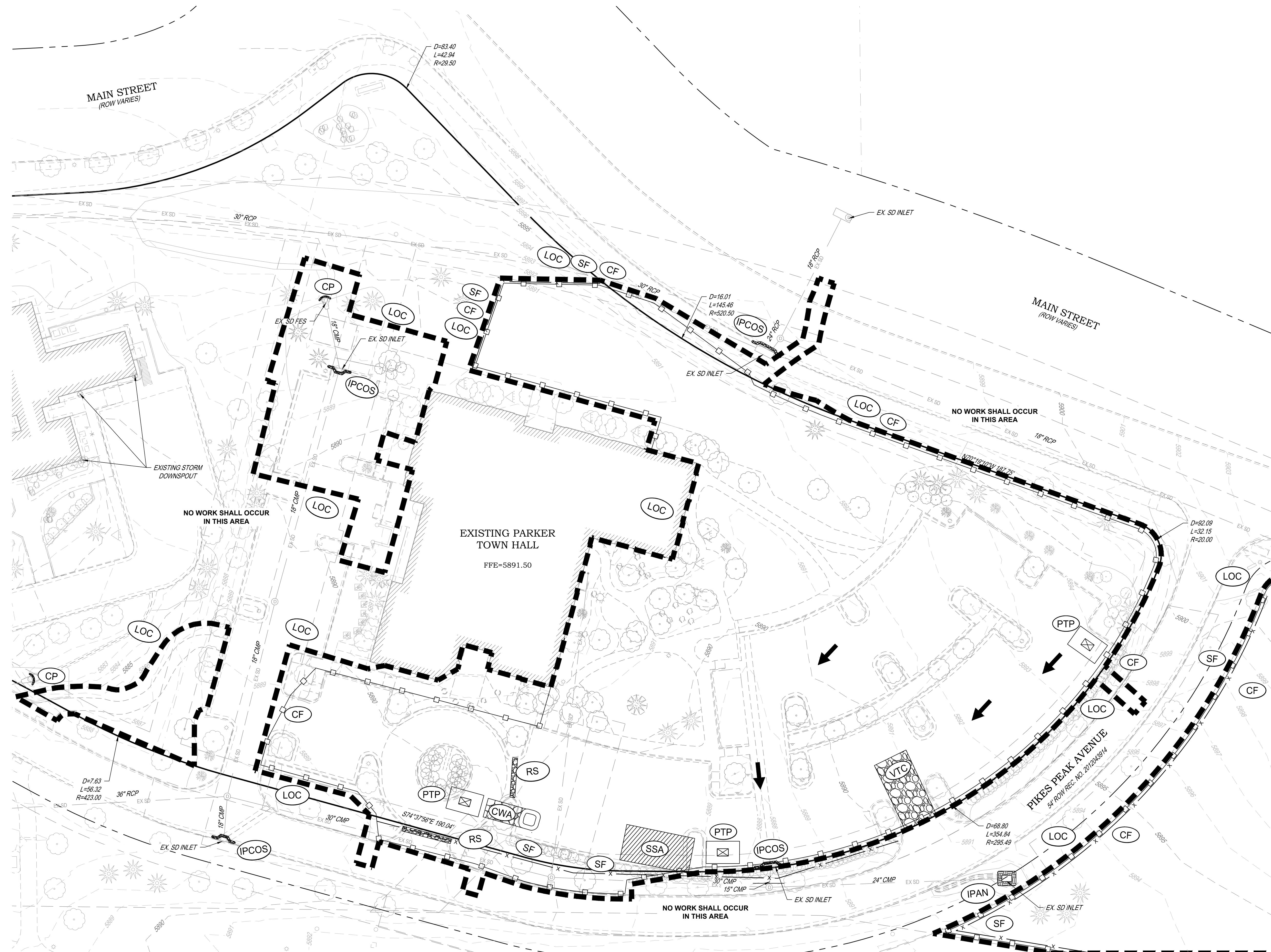
Town of Parker, Director of Engineering/Public Works _____ Date _____



NOTES:
1. SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.

LEGEND:

	(CD)	CHECK DAM
	(CF)	CONSTRUCTION FENCE
	(CP)	CULVERT PROTECTION
	(CWA)	CONCRETE WASHOUT AREA
	(D)	DEWATERING
	(DD)	DIVERSION DITCH
	(DP)	DETENTION POND PROTECTION
	(DTC)	DEBRIS TRASH CONTROL
	(ECB)	EROSION CONTROL BLANKET
	(IPAN)	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	(IPAP)	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	(IPCOG)	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	(IPCOS)	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	(LP)	LOT PROTECTION
	(MWP)	MASONRY WORK PROTECTION
	(PTP)	PORTABLE TOILET PROTECTION
	(RCSC)	ROUGH CUT STREET CONTROL
	(RS)	ROCK SOCK
	(RSS)	ROCK SOCK IN SWALE
	(SB)	STRAW BALE
	(SCL)	SEDIMENT CONTROL LOGS
	(SF)	SILT FENCE
	(SMC)	SEEDING, MULCHING AND CRIMPING
	(SR)	SURFACE ROUGHING
	(SSA)	STABILIZED STAGING AREA
	(STP)	SIDEWALK TRANSITION PROTECTION
	(TI)	TEMPORARY IRRIGATION
	(TSB)	TEMPORARY SEDIMENT BASIN
	(VTC)	VEHICLE TRACKING CONTROL
	(LOC)	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW



- TOWN OF PARKER NOTES:**
- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADE AFTER TOPSOIL, SOD, PAVING ETC., EXCEPT IN THE AREA OF SOIL STOCK PILE.
 - SEE TOWN OF PARKER CONSTRUCTION BEST MANAGEMENT PRACTICES.
 - THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ECS.
 - THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
 - THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
 - ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
 - LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
 - PROVIDE SEDIMENT CONTROL LOGS (SCL) ALONG BACK OF ALL CURBS ADJACENT TO LANDSCAPE/PERVIOUS AREAS INCLUDING LANDSCAPE ISLANDS AND ALL ATTACHED PEDESTRIAN WALKS.
 - ANY ONSITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION. CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.
 - MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
 - THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND THE ECS.

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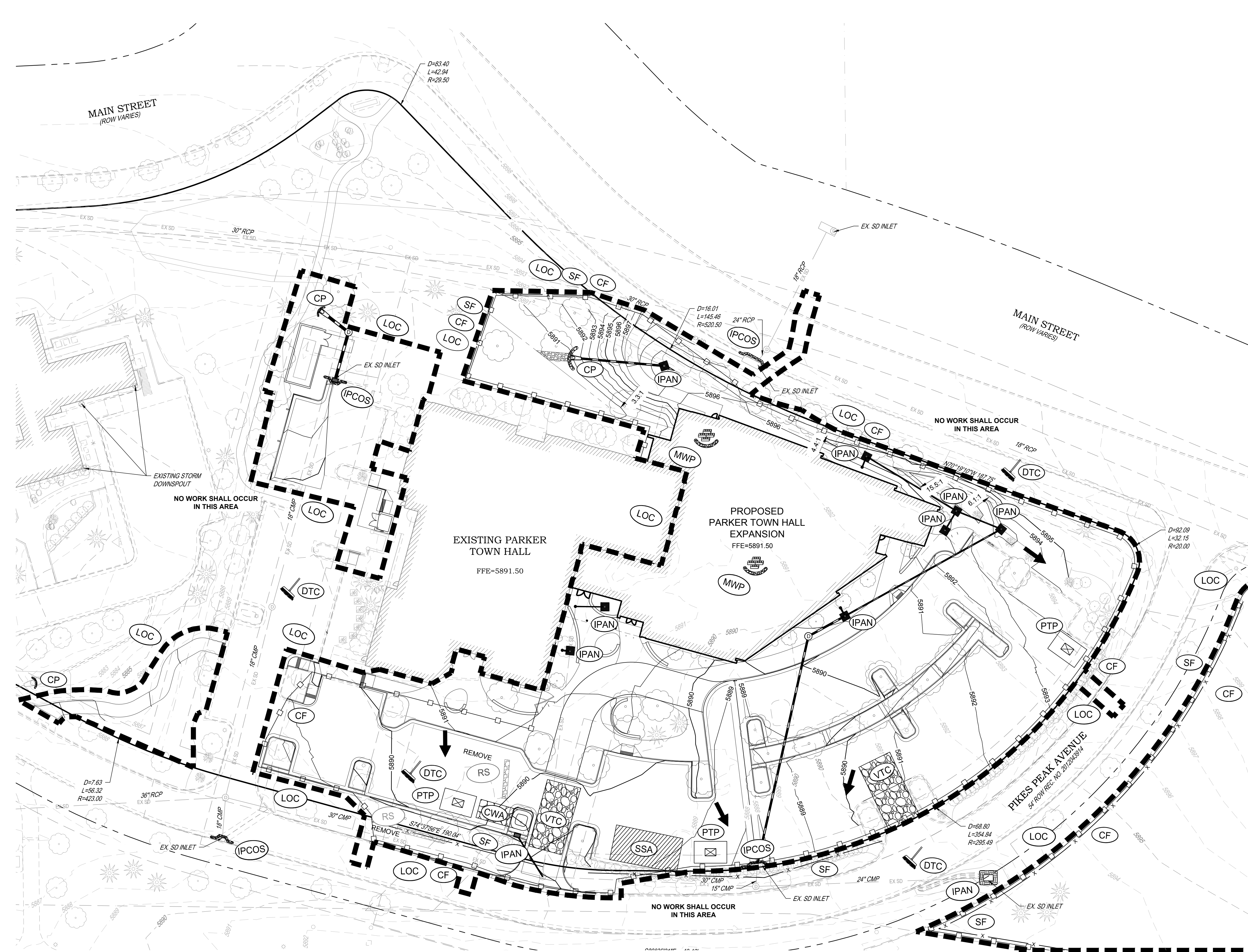
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Town of Parker, Director of Engineering/Public Works _____ Date _____



Issue	Date
Schematic Design	01 MAY 2023
Design Development	20 OCT 2023
Permit / GMP SET	17 MAY 2024

Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:



NOTES:
 1. SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.
 2. ADDITIONAL PTP'S MAY BE NEEDED IN THE INTERIM AND FINAL PHASE.
 3. EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL SLOPES STEEPER THAN 4:1.

LEGEND:

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
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	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW

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 11. THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND THE ECS.

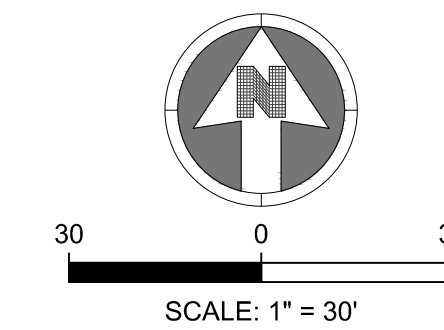
THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.
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Town of Parker, Director of Engineering/Public Works _____ Date _____



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

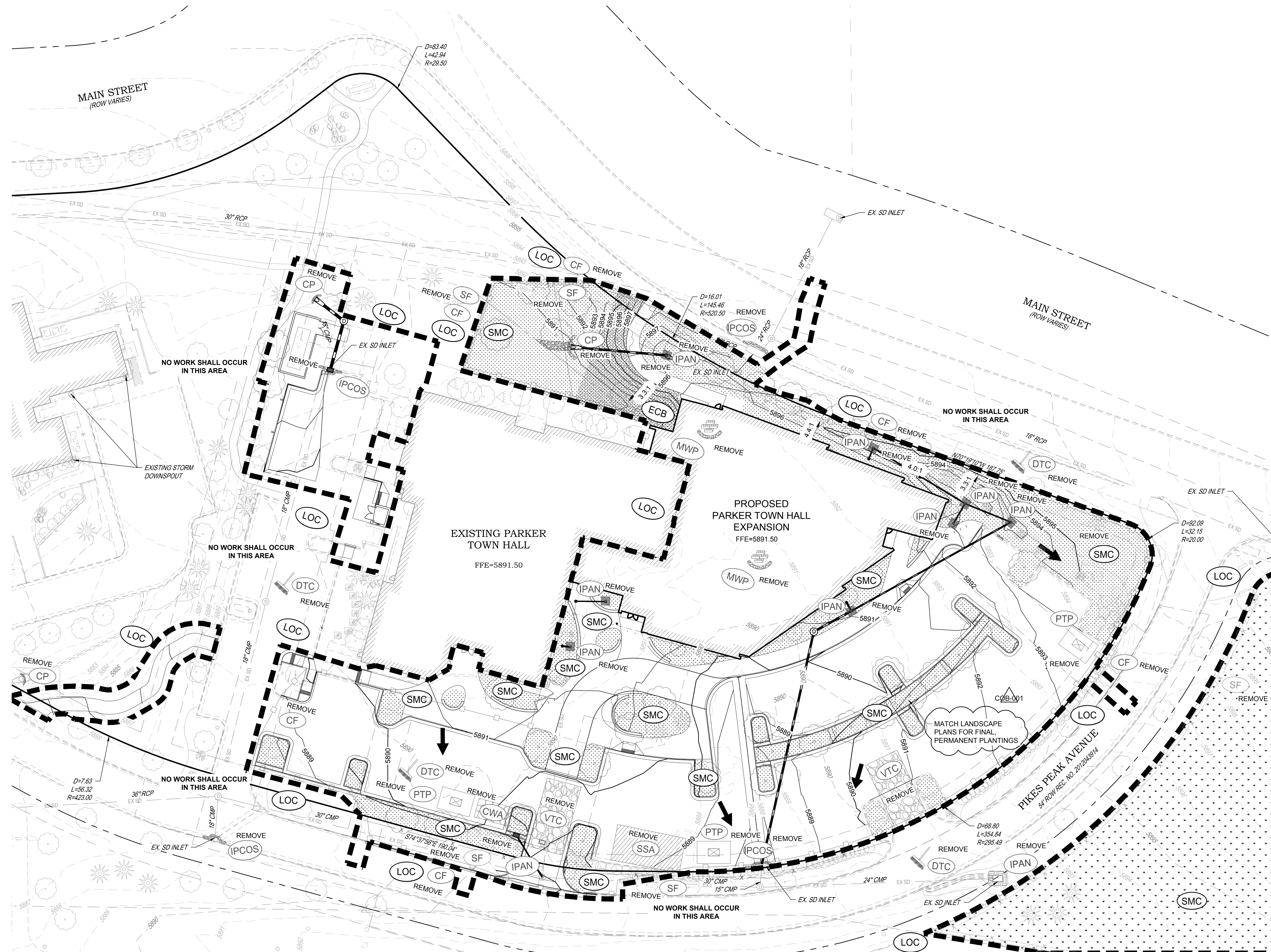
Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:



- NOTES:**
- SEE SHEETS C-251-C-259 FOR EROSION CONTROL NOTES, LEGEND AND DETAILS.
 - ADDITIONAL PTP'S MAY BE NEEDED IN THE INTERIM AND FINAL PHASE.
 - EROSION CONTROL BLANKETS ARE REQUIRED FOR ALL SLOPES GREATER THAN 4%.
 - SEE LANDSCAPE PLANS FOR FINAL PLANTINGS.

LEGEND:

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
	CWA	CONCRETE WASHOUT AREA
	D	DEWATERING
	DD	DIVERSION DITCH
	DP	DETENTION POND PROTECTION
	DTC	DEBRIS TRASH CONTROL
	ECB	EROSION CONTROL BLANKET
	IPAN	INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
	IPAP	INLET PROTECTION FOR AREA INLETS IN PAVEMENT
	PCOC	INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
	PCOS	INLET PROTECTION, CURB ON SUMP, TYPE R INLET
	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	NATIVE SEEDING, MULCHING AND CRIMPING SEE NOTE 4
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION
	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL
	LOC	LIMITS OF CONSTRUCTION
		FLOW DIRECTION ARROW



- TOWN OF PARKER NOTES:**
- ALL GRADES SHOWN ARE FINAL FINISHED SURFACE GRADE AFTER TOPSOIL, SOD, PAVING ETC. EXCEPT IN THE AREA OF SOIL STOCK PILE.
 - SEE TOWN OF PARKER CONSTRUCTION BEST MANAGEMENT PRACTICES.
 - THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL, AND THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND ECS.
 - THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED.
 - THE TOWN MAY ALLOW THE USE OF PROPRIETARY INLET PROTECTION FOR THE PROPOSED IPAPS FOR THE SAKE OF MAINTENANCE. HOWEVER, THE CONTRACTOR MUST SEEK APPROVAL PRIOR TO INSTALLATION.
 - ALL DIRTY STREET, CURBS, GUTTERS, SIDEWALKS, DRIVE AISLES, ETC., MUST BE THOROUGHLY CLEANED THROUGHOUT THE DAY AND IMMEDIATELY BEFORE THE END OF THE WORK DAY.
 - LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
 - PROVIDE SEDIMENT CONTROL LOGS (SCL) ALONG BACK OF ALL CURBS ADJACENT TO LANDSCAPE/PERVIOUS AREAS INCLUDING LANDSCAPE ISLANDS AND ALL ATTACHED PEDESTRIAN WALKS.
 - ANY ONSITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN PRIOR TO INSTALLATION. CONTACT RANDY CAPRA AT RCAPRA@PARKERONLINE.ORG OR 303-805-3163.
 - MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
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Town of Parker, Director of Engineering/Public Works _____ Date _____



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

C08-001 19 JULY 2024

Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:

Architect

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Landscape Architect

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Denver, CO 80202
Telephone: 303-628-0000

Structural Engineer

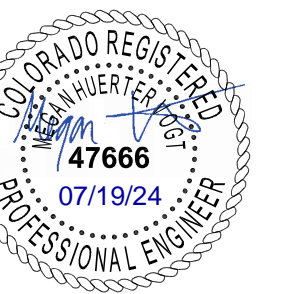
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Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



Issue Date

Schematic Design 01 MAY 2023
Design Development 20 OCT 2023
Permit / GMP SET 17 MAY 2024

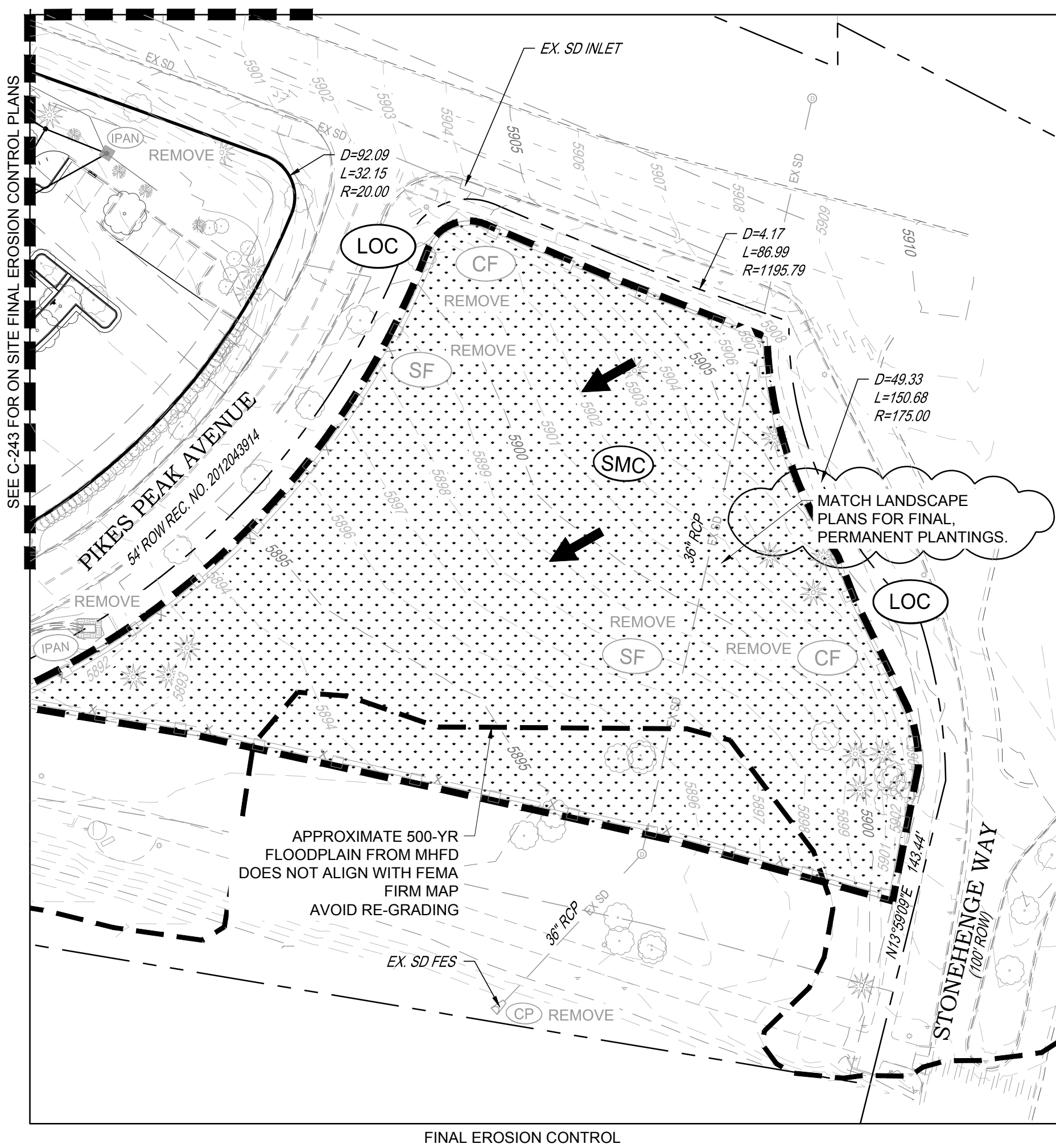
COB-001 19 JULY 2024

Project Number: 22-139

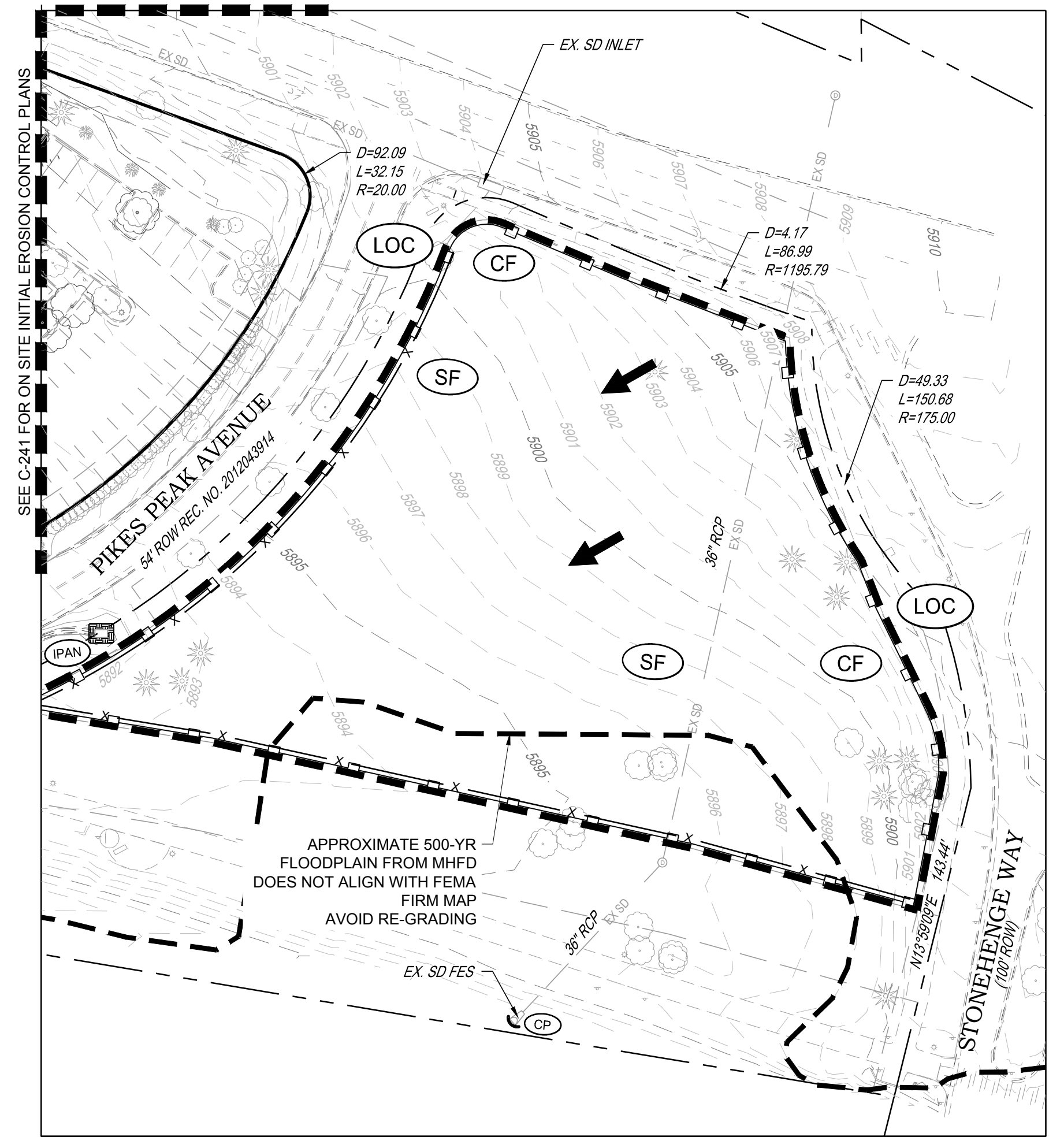
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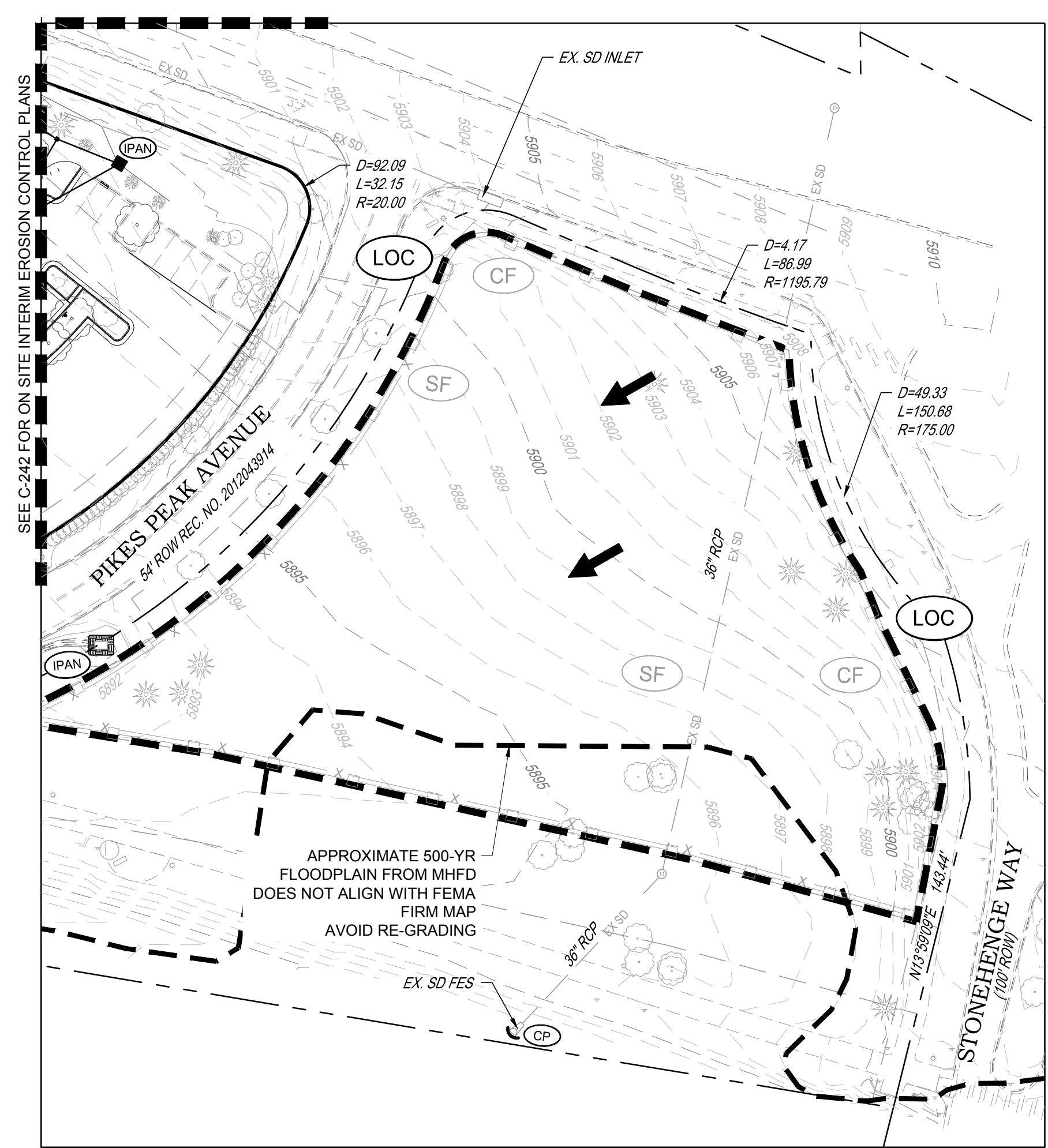
Approved By:



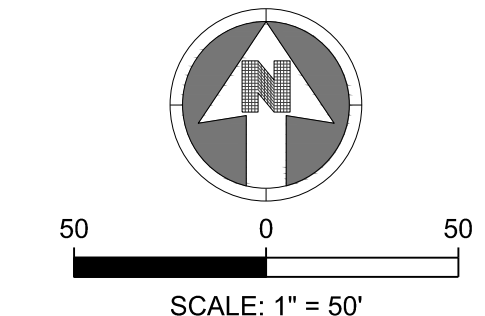
FINAL EROSION CONTROL



INITIAL EROSION CONTROL



INTERIM EROSION CONTROL



NOTES:

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LEGEND:

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
	CP	CULVERT PROTECTION
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Parker Town Hall
Expansion

Parker, CO
22-093

Town of Parker

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THE DRAWING AND ITS USE ARE TO BE MADE TO THE DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

	TI	TEMPORARY IRRIGATION
	TSB	TEMPORARY SEDIMENT BASIN
	VTC	VEHICLE TRACKING CONTROL

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
LEGEND
3 OF 3
04-2012

THE DRAWING AND ITS USE ARE TO BE MADE TO THE DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

	CD	CHECK DAM
	CF	CONSTRUCTION FENCE
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CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
LEGEND
1 OF 3
04-2012

THE DRAWING AND ITS USE ARE TO BE MADE TO THE DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

25. ALL CHEMICAL OR HAZARDOUS MATERIAL SPILLS, INCLUDING CONCRETE WASHOUT WATER, WHICH MAY ENTER WATERS OF THE STATE OF COLORADO, WHICH INCLUDES BUT ARE NOT LIMITED TO, SURFACE WATER, GROUND WATER, DRY GULCHES OR STORM SEWERS LEADING TO SURFACE WATER, SHALL BE IMMEDIATELY REPORTED TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) PER 25-69-601, AND THE TOWN OF PARKER. RELEASES OF PETROLEUM PRODUCTS AND CERTAIN HAZARDOUS SUBSTANCES LISTED UNDER THE FEDERAL CLEAN WATER ACT (40 CFR PART 116) MUST BE REPORTED TO THE NATIONAL RESPONSE CENTER AND THE CDPHE. SPILLS THAT POSE AN IMMEDIATE SAFETY HAZARD SHALL BE REPORTED TO 911.

26. THE CLEANING OF CONCRETE TRUCKS AND EQUIPMENT IS RESTRICTED TO THE APPROVED CONCRETE WASHOUT LOCATION ON THE JOB SITE. CONCRETE WASH WATER SHALL NOT BE DISCHARGED TO STATE WATERS OR STORM SEWER SYSTEMS.

27. VEHICLE AND EQUIPMENT DEGREASING IS PROHIBITED ON THE JOB SITE.

28. ALL DEWATERING ON SITE SHALL BE COORDINATED WITH THE TOWN'S INSPECTOR. A STATE PERMIT MAY BE REQUIRED FOR DEWATERING. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR OBTAINING AND ADHERING TO ALL APPLICABLE PERMITS.

29. HYDRAULIC SEEDING AND/OR HYDRAULIC MULCHING ARE ONLY ALLOWED IN AREAS UNDER TEMPORARY OR PERMANENT IRRIGATION OR FOR THE PURPOSE OF TEMPORARY SOIL STABILIZATION.

30. APPLICABLE CONSTRUCTION BMPs SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL ALL LANDSCAPING HAS BEEN INSTALLED AND THE DESIRABLE VEGETATION HAS REACHED A POINT IN WHICH EROSION AND SEDIMENTATION IS NO LONGER A CONCERN AS DETERMINED BY THE TOWN'S INSPECTOR.

31. GRADING SECURITY RELEASE REQUIREMENTS:

- 1) DEVELOPABLE PROPERTY IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H OR ITEM I (BELOW).
 - A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
 - B. UNIFORM PERENNIAL VEGETATION COVER HAS BEEN ESTABLISHED WITH AN INDIVIDUAL PLANT DENSITY OF AT LEAST SEVENTY PERCENT (70%) OF PRE-DISTURBANCE LEVEL.
 - C. ALL BMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
 - D. IF ANY EROSION IS PRESENT, IT IS INEFFECTIVE AND IS NOT LEAVING THE SITE AND/OR LEADING INTO ANY ON-SITE DRAINAGE INFRASTRUCTURE THAT MAY CONVEY SURFACE WATER OFF SITE.
 - E. WEEDS REPRESENT NO MORE THAN FIFTY PERCENT (50%) OF THE TOTAL VEGETATION ON THE SITE.
 - F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
 - G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
 - H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.
 - I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR ASSIGNMENT AS PROVIDED BY SECTION 11.0.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION AT THE TIME OF CLOSING TO ENSURE THAT THE NEW SITE OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
GEN NOTES
3 OF 4
04-2012

THE DRAWING AND ITS USE ARE TO BE MADE TO THE DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

1. GRADING, DRILLING, CLEARING, EXCAVATING, BACK-FILLING, SOIL STRIPPING, SOIL IMPORTING, EXPORTING OR ANY OTHER FORM OF SOIL DISTURBANCE SHALL NOT COMMENCE UNTIL A TOWN OF PARKER GRADING/EXCAVATION PERMIT HAS BEEN ISSUED.
2. THE TOWN OF PARKER GRADING/EXCAVATION PERMIT IS VALID FOR A PERIOD OF 2 YEARS FROM THE DATE OF ISSUANCE. ANY LAPSE IN PERMIT COVERAGE MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER AND/OR FINES.
3. ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE CBMP PLAN. INSTALLATION NOTES AND DETAILS AND/OR PERMITTED VARIANCES. ALL MATERIALS, WORKMANSHIP AND INSTALLATION METHODS ARE SUBJECT TO INSPECTION BY THE TOWN'S INSPECTOR. THE TOWN OF PARKER RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS, WORKMANSHIP AND/OR INSTALLATION METHODS THAT DO NOT CONFORM TO THE CBMP PLAN AND/OR PERMITTED VARIANCES.
4. THE EROSION CONTROL SUPERVISOR SHALL REVISE OR MODIFY THE EROSION AND SEDIMENT CONTROL MEASURES IF IT BECOMES APPARENT THAT THE ORIGINAL PLAN IS INADEQUATE, OR AS A RESULT OF DEFICIENCIES IDENTIFIED DURING INSPECTIONS PERFORMED BY THE TOWN'S INSPECTOR.
5. THE INSTALLATION OF ADDITIONAL EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs) MAY BE REQUIRED OF THE EROSION CONTROL SUPERVISOR, PROPERTY OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS AT ANY TIME THROUGHOUT THE DURATION OF THE PROJECT CONSTRUCTION AND/OR SUBSEQUENT REVEGETATION PERIOD.
6. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE SITE REMAINS IN COMPLIANCE WITH THE NOTICE OF NUISANCE REPORTS (NARR), CONSTRUCTION SITE RUNOFF CONTROL INSPECTION REPORTS, APPROVED CBMP PLANS AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
7. THE EROSION CONTROL SUPERVISOR SHALL BE READILY AVAILABLE TO DISCUSS AND CORRECT ANY PROBLEMS THAT MAY ARISE RELATING TO GRADING, EROSION AND SEDIMENT CONTROL.
8. IF IT IS ANTICIPATED THAT ALL OR A PORTION OF THE PROJECT WILL OCCUR DURING SEASONS SUSCEPTIBLE TO SNOWFALL, THE USE OF CERTAIN EROSION AND SEDIMENT CONTROL BMPs ADJACENT TO PUBLIC ROADWAYS MAY NEED TO BE RECONSIDERED DUE TO THE NATURE OF SNOW REMOVAL OPERATIONS. AN APPROVED ALTERNATIVE MAY BE NECESSARY TO MINIMIZE DAMAGE FROM THESE OPERATIONS. THE TOWN OF PARKER ASSUMES NO RESPONSIBILITY FOR DAMAGE TO ANY BMPs AS A RESULT OF SNOW FLOWING AND SNOW REMOVAL.
9. AREAS OF LAND DISTURBANCE EQUAL TO 40 ACRES OR GREATER SHALL NOT BE EXPOSED FOR MORE THAN 30 CONSECUTIVE DAYS WITHOUT TEMPORARY OR PERMANENT STABILIZATION.
10. AUTHORIZED EXEMPTIONS MAY BE ALLOWED TO THE 40-ACRE LIMIT FOR REMOVAL AND STORAGE OF CUT MATERIAL. WHERE (A) GEOTECHNICAL LIMITATIONS RESTRICT THE USE OF TEMPORARY OR PERMANENT STABILIZATION OF THE STORED MATERIAL (E.G., SWELLING SOILS, ROCK) AND (B) WHEN THE OWNER CAN DEMONSTRATE THAT THE 40-ACRE LIMIT IS PHYSICALLY AND/OR FINANCIALLY IMPRACTICABLE FOR SITES GRANTED THIS EXEMPTION, A PHASING AND EARTHWORK QUANTITIES PLAN SHALL BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT AND APPROVED PRIOR TO THE COMMENCEMENT OF LAND DISTURBANCE ACTIVITIES. SUBMITTAL REQUIREMENTS INCLUDE:
 - (i) PHASING PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
 - (ii) EARTHWORK QUANTITY PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
 - (iii) BMP PLAN SHOWING SPECIFIC EROSION AND SEDIMENT CONTROLS FOR EACH PHASE.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
GEN NOTES
1 OF 4
04-2012

THE DRAWING AND ITS USE ARE TO BE MADE TO THE DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

	LP	LOT PROTECTION
	MWP	MASONRY WORK PROTECTION
	PTP	PORTABLE TOILET PROTECTION
	RCSC	ROUGH CUT STREET CONTROL
	RS	ROCK SOCK
	RSS	ROCK SOCK IN SWALE
	SB	STRAW BALE
	SCL	SEDIMENT CONTROL LOGS
	SF	SILT FENCE
	SMC	SEEDING, MULCHING AND CRIMPING
	SR	SURFACE ROUGHING
	SSA	STABILIZED STAGING AREA
	STP	SIDEWALK TRANSITION PROTECTION

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
LEGEND
2 OF 3
04-2012

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3) NONDEVELOPABLE PROPERTY IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H AND J, OR ITEMS I AND J (BELOW).

- A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
- B. ALL BMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
- C. EROSION IS PRESENT.
- D. THE VEGETATION REPRESENTS A PERENNIAL STAND OF A DENSE, UNIFORM SURFACE OF GRASSES WITH NO AREA GREATER THAN ONE (1) SQUARE FOOT THAT IS BARE OR OF DESIRABLE VEGETATION. INFREQUENT, WIDELY SCATTERED AREAS WHERE NATIVE VEGETATION HAS NOT YET TAKEN HOLD MAY QUALIFY FOR ACCEPTANCE AT THE DISCRETION OF THE TOWN.
- E. WEEDS REPRESENT NO MORE THAN TEN PERCENT (10%) OF THE TOTAL VEGETATION ON SITE.
- F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
- G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
- H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.
- I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED FOR THE APPLICABLE SITE OR ASSIGNMENT AS PROVIDED BY SECTION 11.0.150 OF THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION AT THE TIME OF CLOSING ON THE SALE OF A SITE THAT IS SUBJECT TO A GRADING PERMIT, TO ENSURE THAT THE NEW PROPERTY OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.
- J. ALL KNOWN DRAINAGE ISSUES ASSOCIATED WITH THE PROJECT HAVE BEEN MITIGATED AND A SUFFICIENT AMOUNT OF TIME HAS PASSED TO ENSURE THAT SUCH ISSUES HAVE BEEN CORRECTED. THIS REQUIREMENT DOES NOT INCLUDE THOSE DRAINAGE ISSUES ORIGINATING IN RESIDENTIAL LOTS.

(D) NOXIOUS WEEDS MUST BE CONTROLLED AS PROVIDED UNDER STATE LAW AND SECTION 6.01.260 OF THE TOWN OF PARKER MUNICIPAL CODE FAILURE TO CONTROL NOXIOUS WEEDS ON THE SITE MAY CONSTITUTE A NUISANCE, SUBJECT TO THE PENALTIES CONTAINED IN THE CODE.

DEFINITIONS:
DEVELOPABLE PROPERTY MEANS ANY LAND THAT HAS BEEN GRADED AND IS PART OF A PLATTED LOT OR PLATTED TRACT OF RECORD, THAT WAS PLATTED FOR FUTURE DEVELOPMENT, INCLUDING RESIDENTIAL HOME CONSTRUCTION OR PUBLIC IMPROVEMENTS.
NONDEVELOPABLE PROPERTY MEANS LAND THAT HAS BEEN GRADED AND WILL NOT BE FURTHER DISTURBED AS PART OF ANY FUTURE DEVELOPMENT. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO: PARKS, OPEN SPACE, HOMEOWNER ASSOCIATION OR BUSINESS ASSOCIATION PLATTED TRACTS, DETENTION PONDS AND DRAINAGEWAYS.

32. FAILURE TO COMPLY WITH ANY OF THE REQUIREMENTS DESCRIBED IN THIS SECTION MAY RESULT IN THE ISSUANCE OF A NOTICE OF INTENT TO ISSUE A STOP WORK ORDER, A STOP WORK ORDER AND/OR THE REMEDIES/PENALTIES DESCRIBED IN CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE.

33. ANY PERSON CONVICTED OF VIOLATING ANY PROVISION OF THE TOWN OF PARKER, GRADING & EARTH MOVEMENT SECTION OF THE MUNICIPAL CODE SHALL BE GUILTY OF A MISDEMEANOR AND, UPON CONVICTION, BE PUNISHED BY A FINE OF NOT MORE THAN FOUR HUNDRED NINETY NINE DOLLARS (\$499.00) FOR EACH SEPARATE OFFENSE. EACH DAY A VIOLATION CONTINUES SHALL CONSTITUTE A SEPARATE OFFENSE. THE TOWN ALSO MAY SEEK IN MUNICIPAL COURT AN INJUNCTION, ABATEMENT, RESTITUTION OR ANY OTHER REMEDY TO PREVENT, ENJOIN, ABATE OR REMOVE THE VIOLATION. A PERSON CONVICTED OF A VIOLATION UNDER CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE SHALL BE LIABLE FOR THE ACTUAL COST OF REHABILITATING THE PROPERTY. THE COSTS MAY BE RECOVERED AS RESTITUTION IN MUNICIPAL COURT PROCEEDINGS OR IN A SEPARATE CIVIL ACTION.

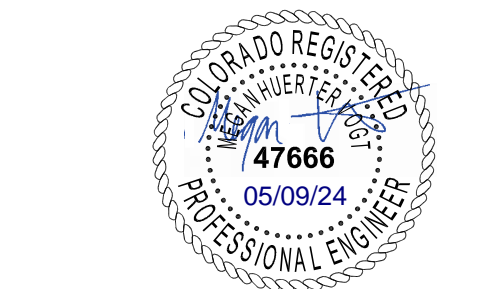
34. THE TOWN OF PARKER RESERVES THE RIGHT TO ALLOW MODIFICATIONS AND SUBSTITUTIONS TO THE BMP NOTES AND DETAILS WHEN SUCH MODIFICATIONS AND SUBSTITUTIONS OFFER THE SAME LEVEL OF PROTECTION AS THE STANDARD REQUIREMENTS BASED UPON THE SPECIFIC SITUATION, AS DETERMINED BY TOWN STAFF. DUE TO THE INSURANCE AND LIABILITY OF SUCH MODIFICATIONS AND SUBSTITUTIONS, THE APPROVAL OF SUCH VARIATIONS MAY NOT BE DOCUMENTED BY TOWN STAFF.

CBMP
CONSTRUCTION BEST MANAGEMENT PRACTICES
GEN NOTES
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04-2012

THE DRAWING AND ITS USE ARE TO BE MADE TO THE DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

11. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
12. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.
13. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TROTTLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.
14. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SUCH AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.
15. ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.
16. ALL TRASH RECEPTACLES ON SITE SHALL BE EMPTIED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONTAINED TO THE RECEPTACLE.
17. ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.
18. ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REBAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS, CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.
19. THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT. FAILURE TO ADEE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.
20. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PARKED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.
21. NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.
22. ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL (BLANKETS), TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS.
23. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.
24. A WATER SOURCE SHALL BE AVAILABLE ONSITE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FUGITIVE DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.

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CONSTRUCTION BEST MANAGEMENT PRACTICES
GEN NOTES
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04-2012



Issue	Date
Schematic Design	01 MAY 2023
Design Development	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

EROSION CONTROL DETAILS

Parker Town Hall Expansion

Parker, CO 22-093
Town of Parker
 20120 Mainstreet
 Parker, CO 80138

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 AndersonMasonDale Architects, P.C.
 3198 Speer Boulevard
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 Telephone: 303-294-9448

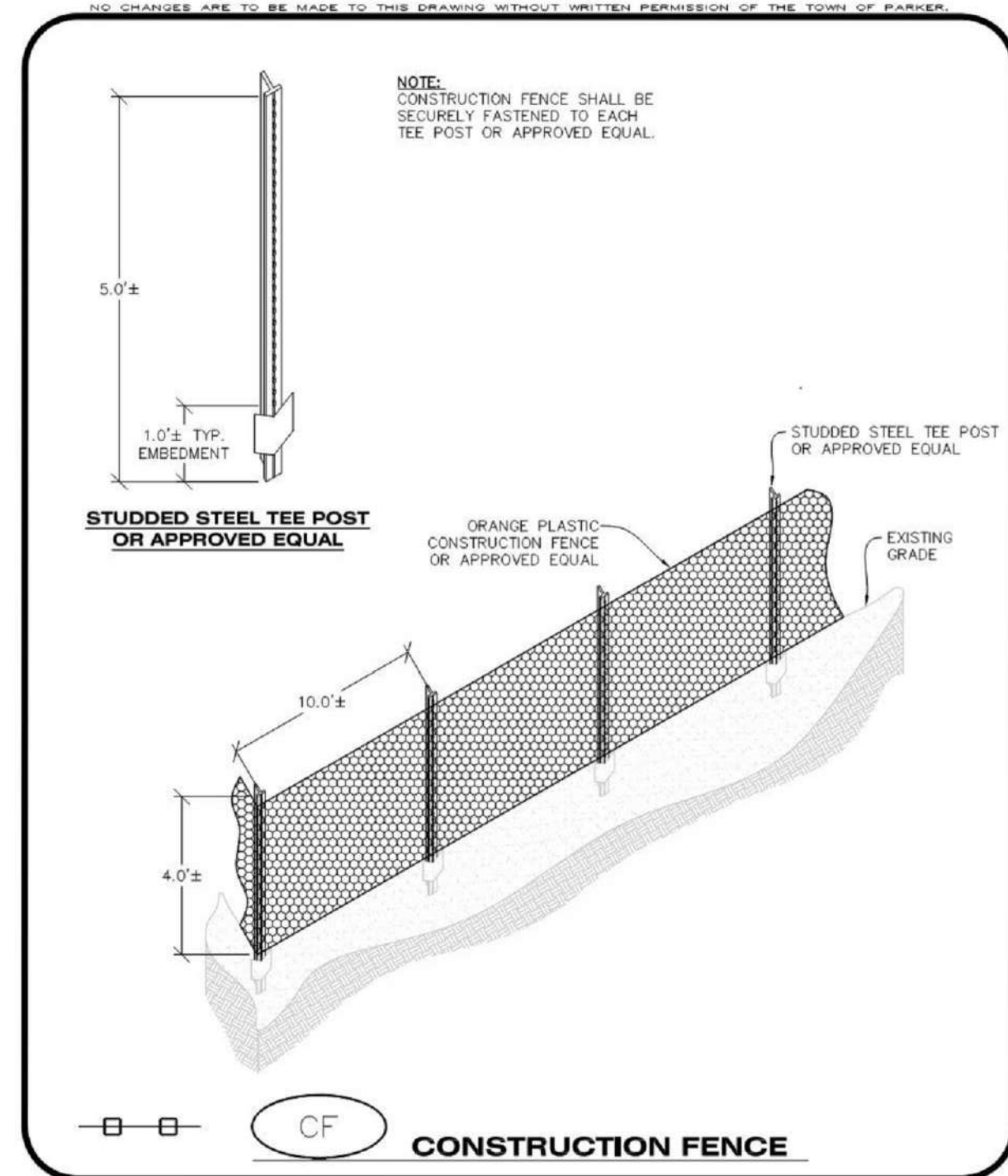
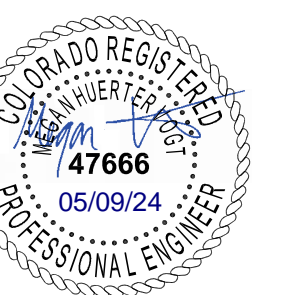
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 Telephone: 303-741-3737

Landscape Architect
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 1130 31st Street #101
 Denver, CO 80205
 Telephone: 303-628-0000

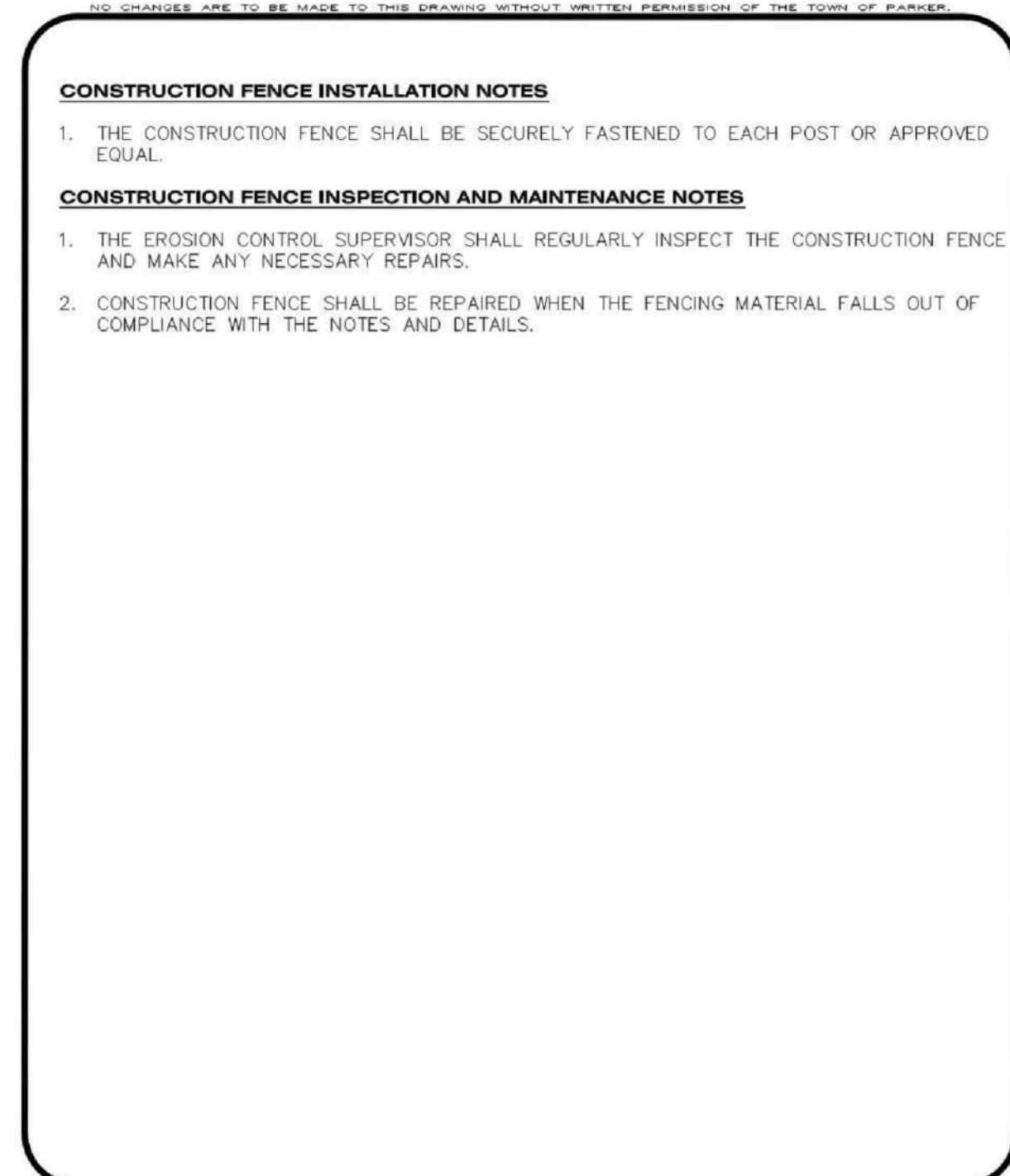
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 1717 Washington Ave.
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 Telephone: 303-384-9910

Mechanical + Plumbing Engineer
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 2525 S. Wadsworth Blvd. #200
 Lakewood, CO 80227
 Telephone: 303-988-4514

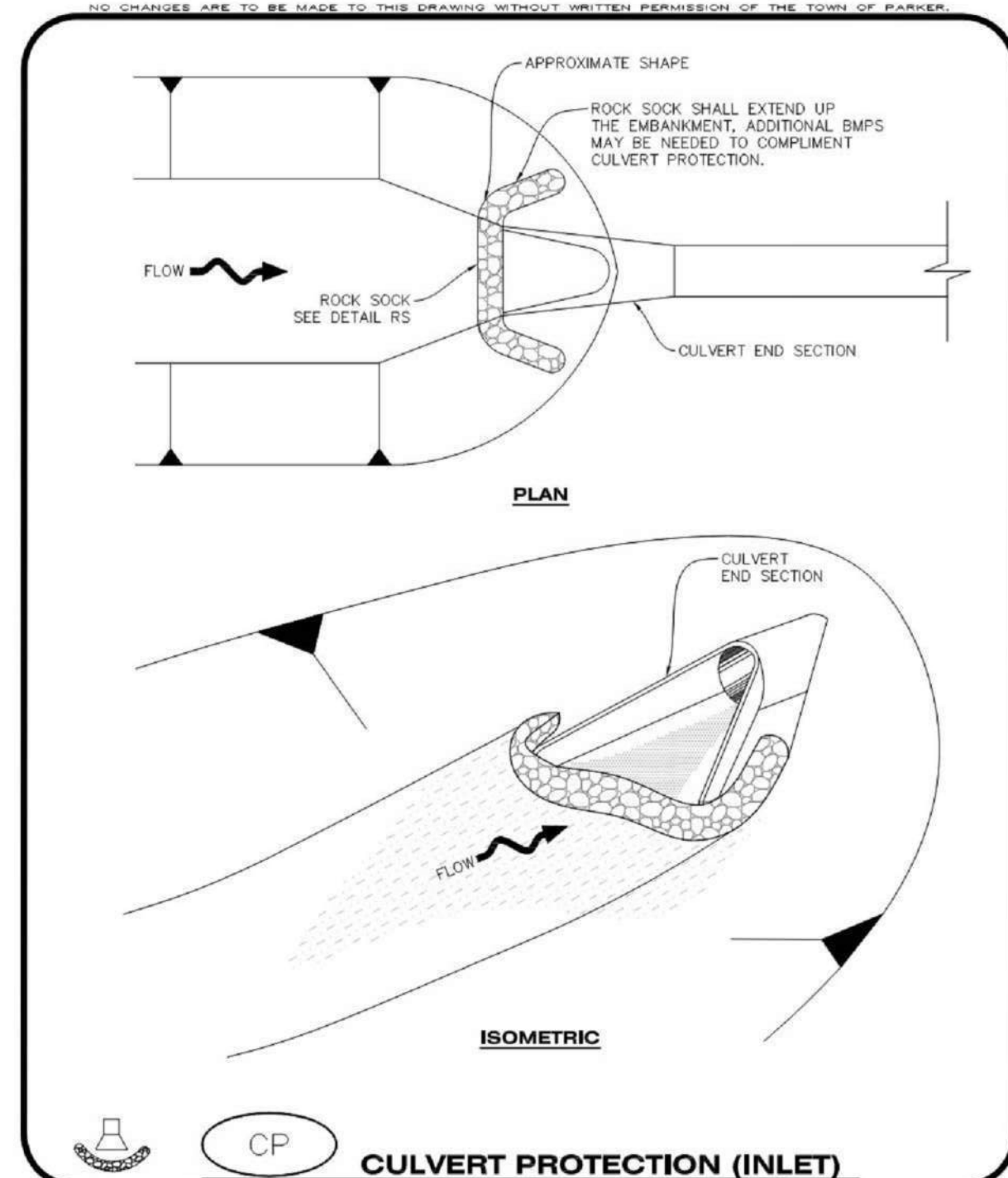
Electrical Engineer + Technology
 AE Design
 1900 Wazee Street #205
 Denver, CO 80202
 Telephone: 303-296-3034



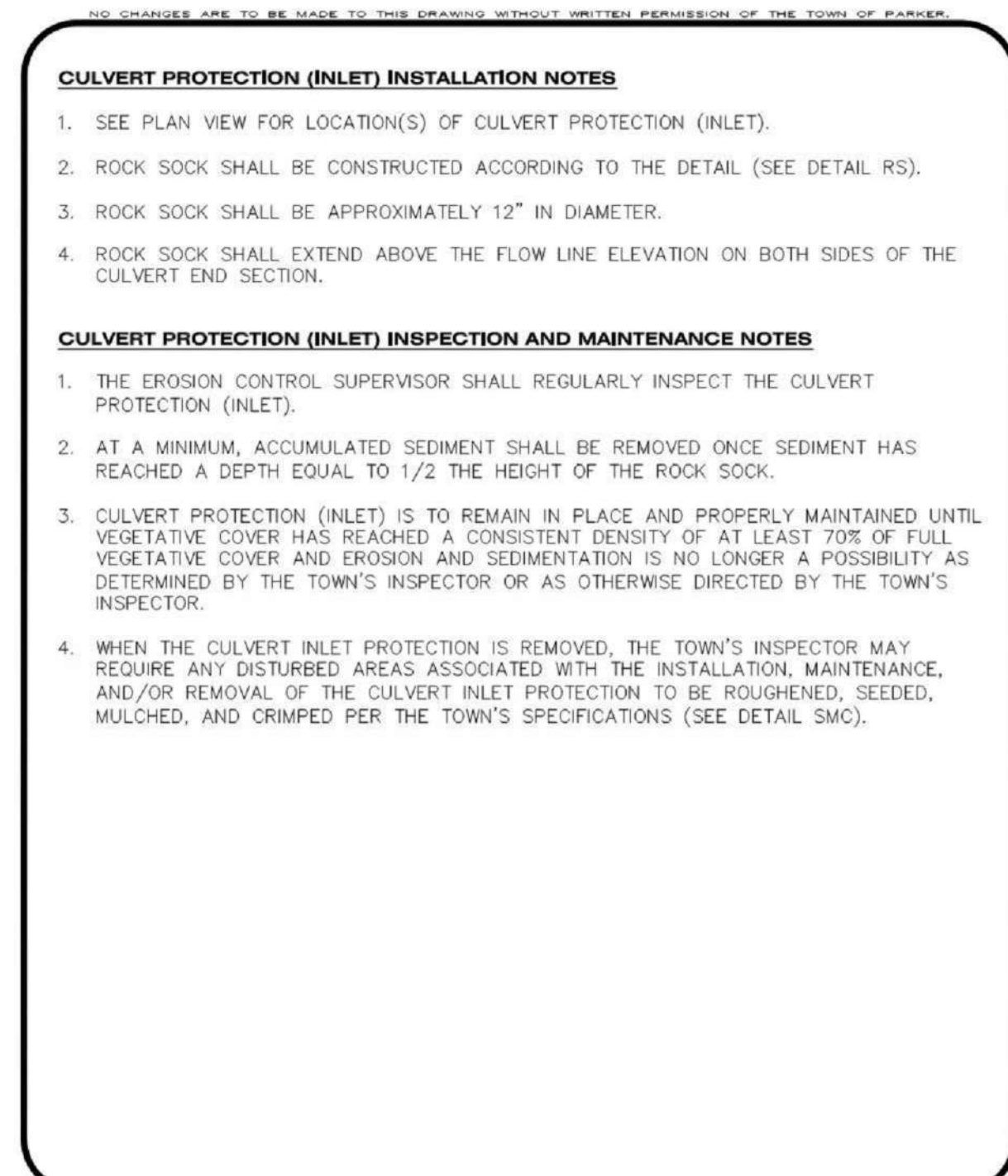
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 Parker COLORADO CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CF 1 OF 2 Oct. 2013



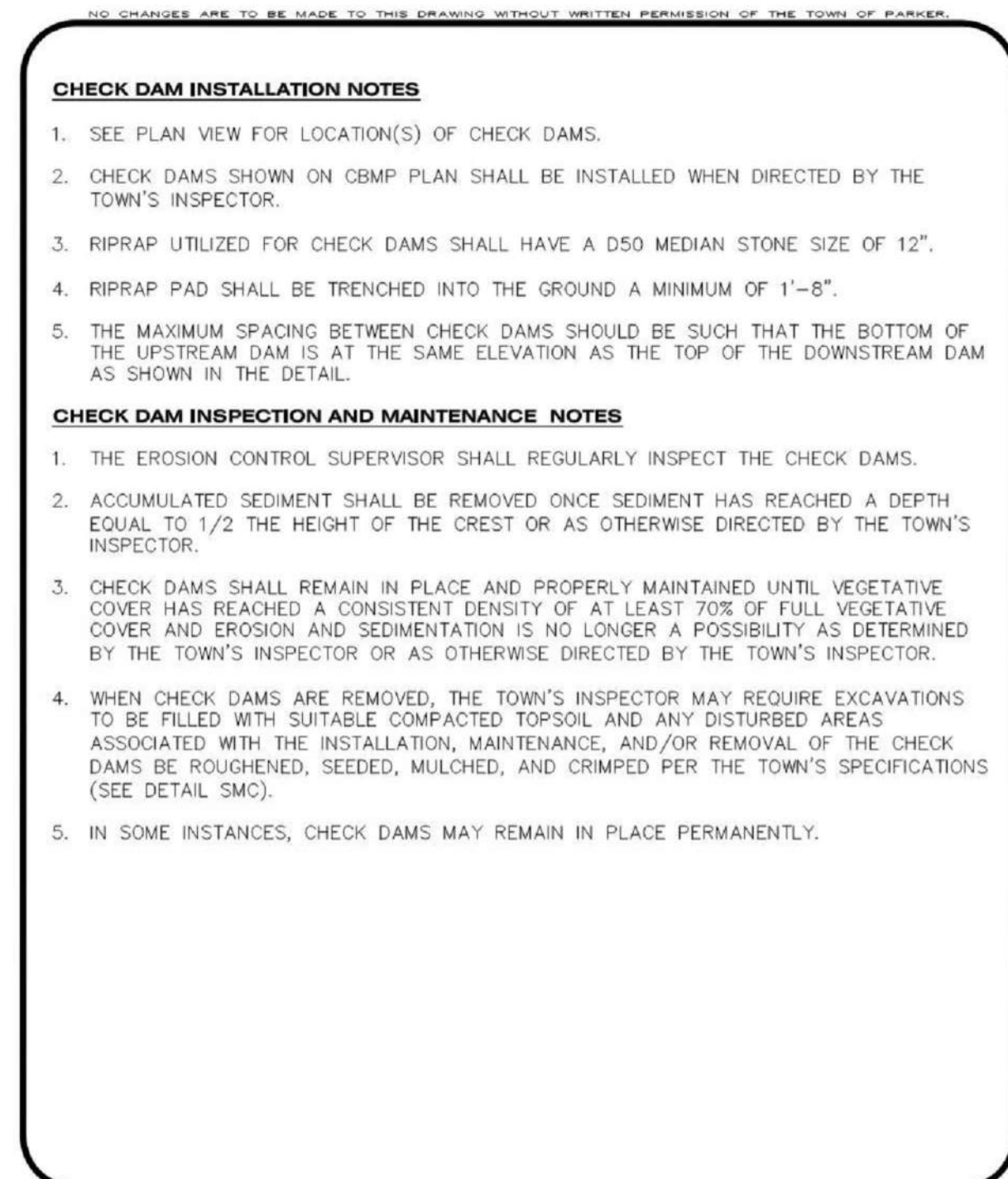
CF CONSTRUCTION FENCE
 Parker COLORADO CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CF 2 OF 2 Oct. 2013



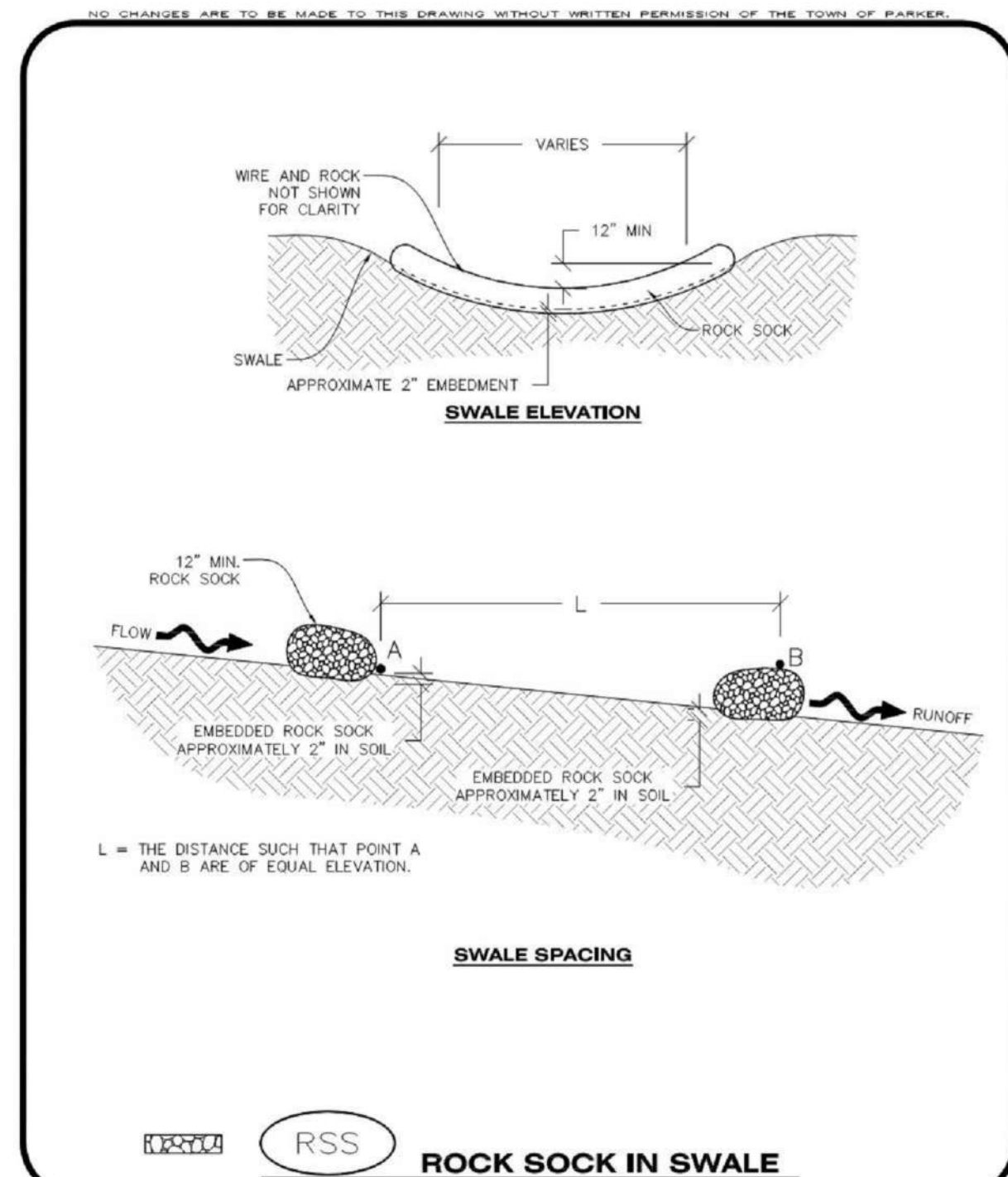
CP CULVERT PROTECTION (INLET)
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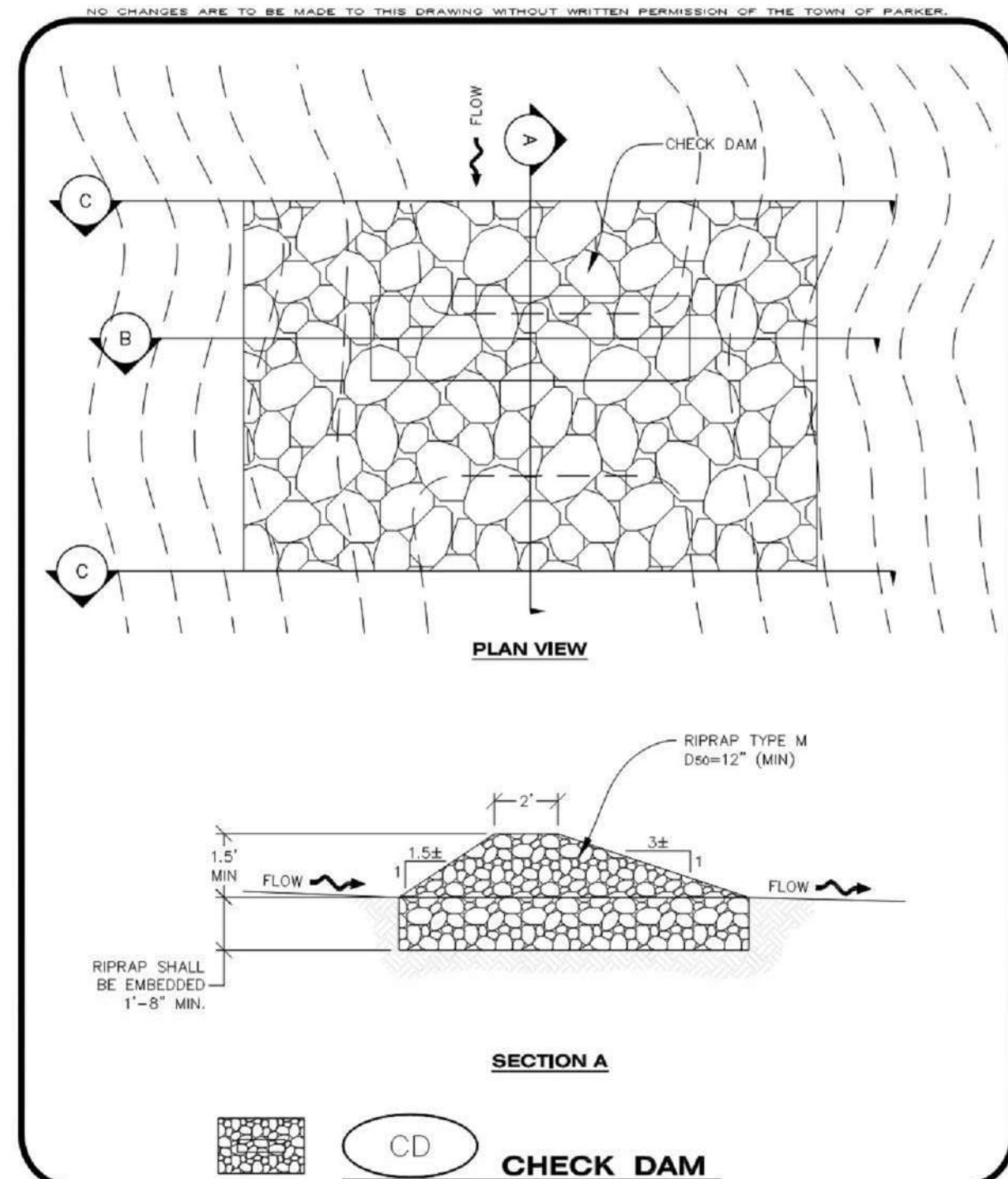
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 Parker COLORADO CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CP 2 OF 2 Oct. 2013



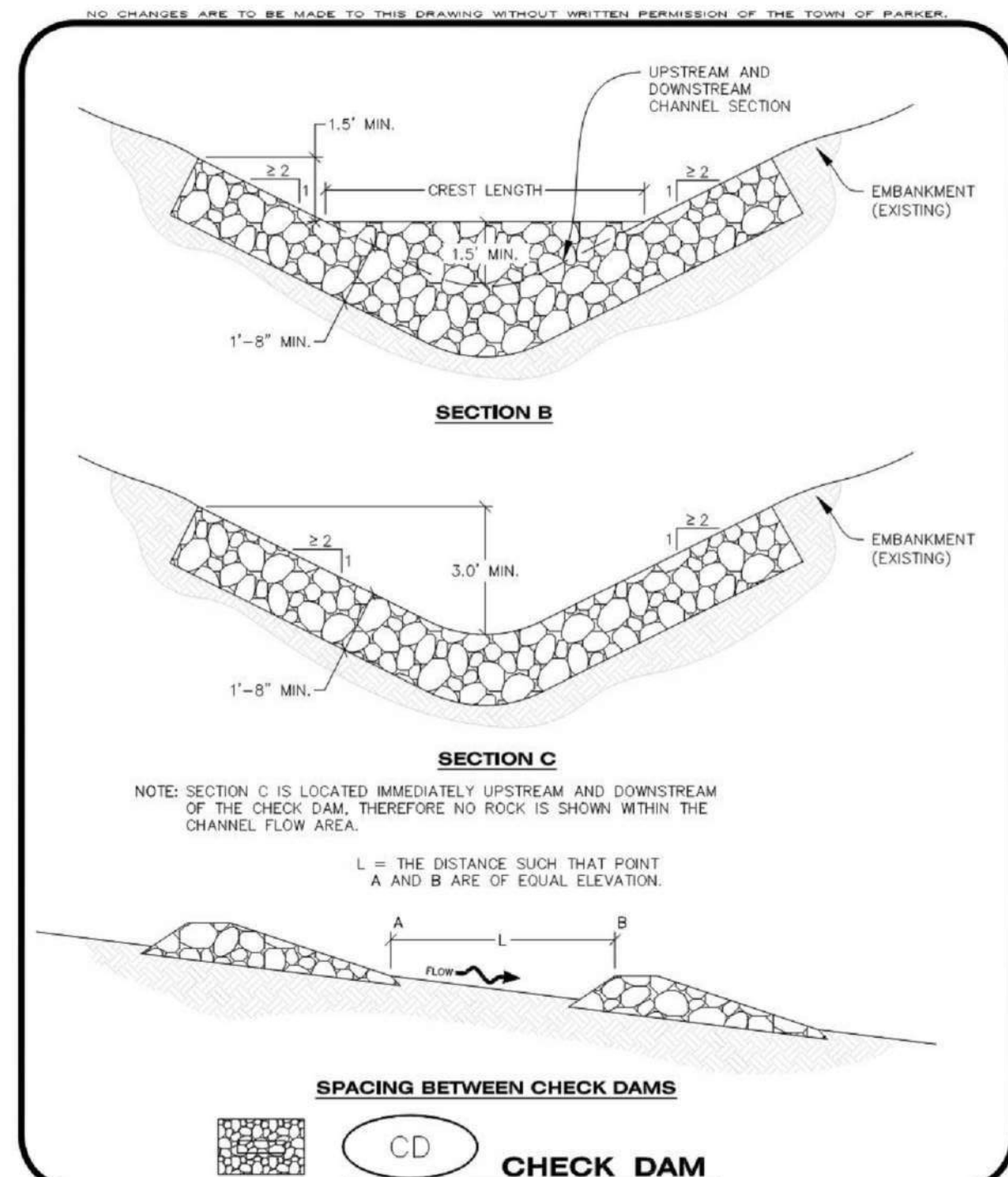
CD CHECK DAM
 Parker COLORADO CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CD 3 OF 3 Oct. 2013



RSS ROCK SOCK IN SWALE
 Parker COLORADO CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES RSS 1 OF 1 Oct. 2013



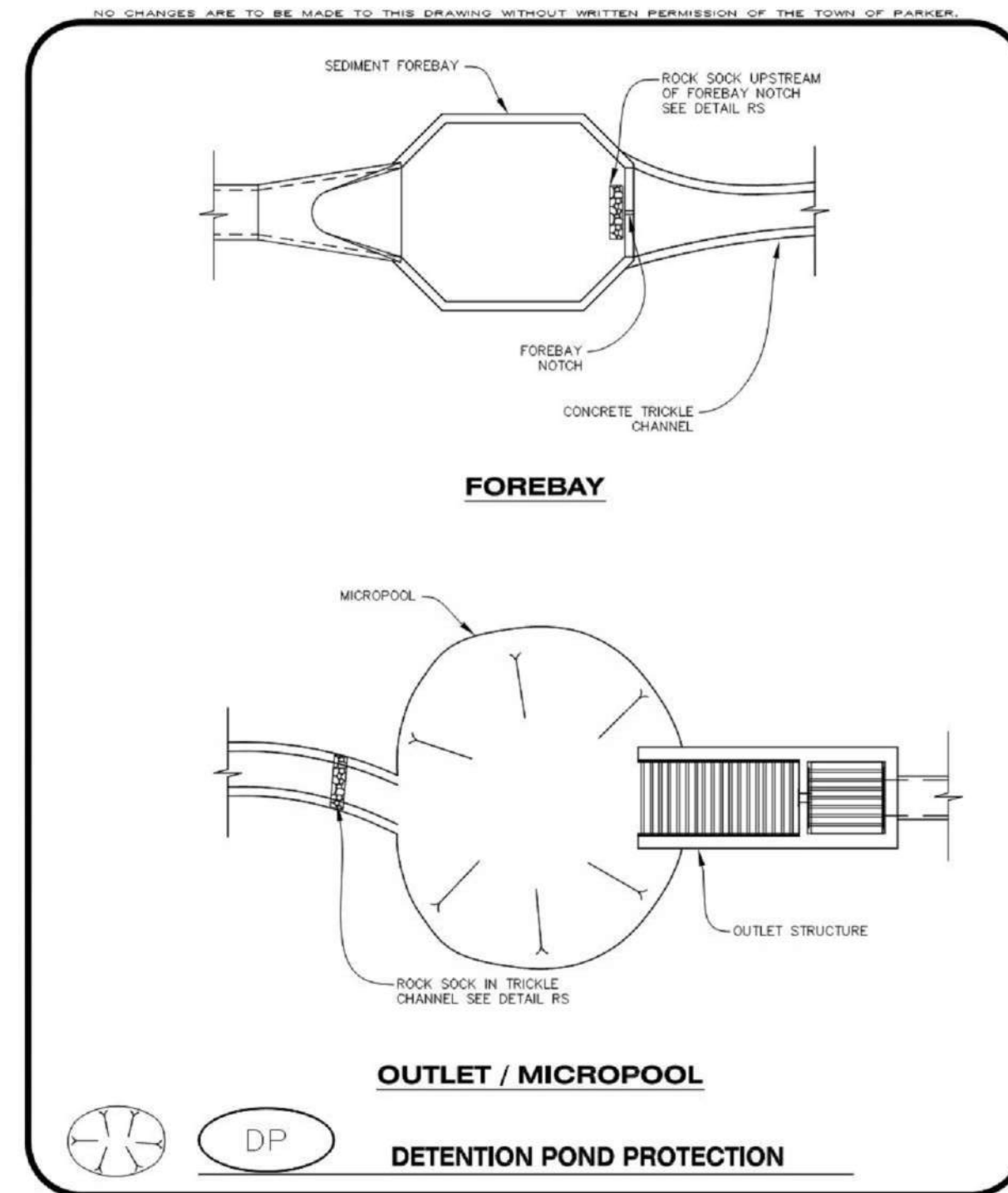
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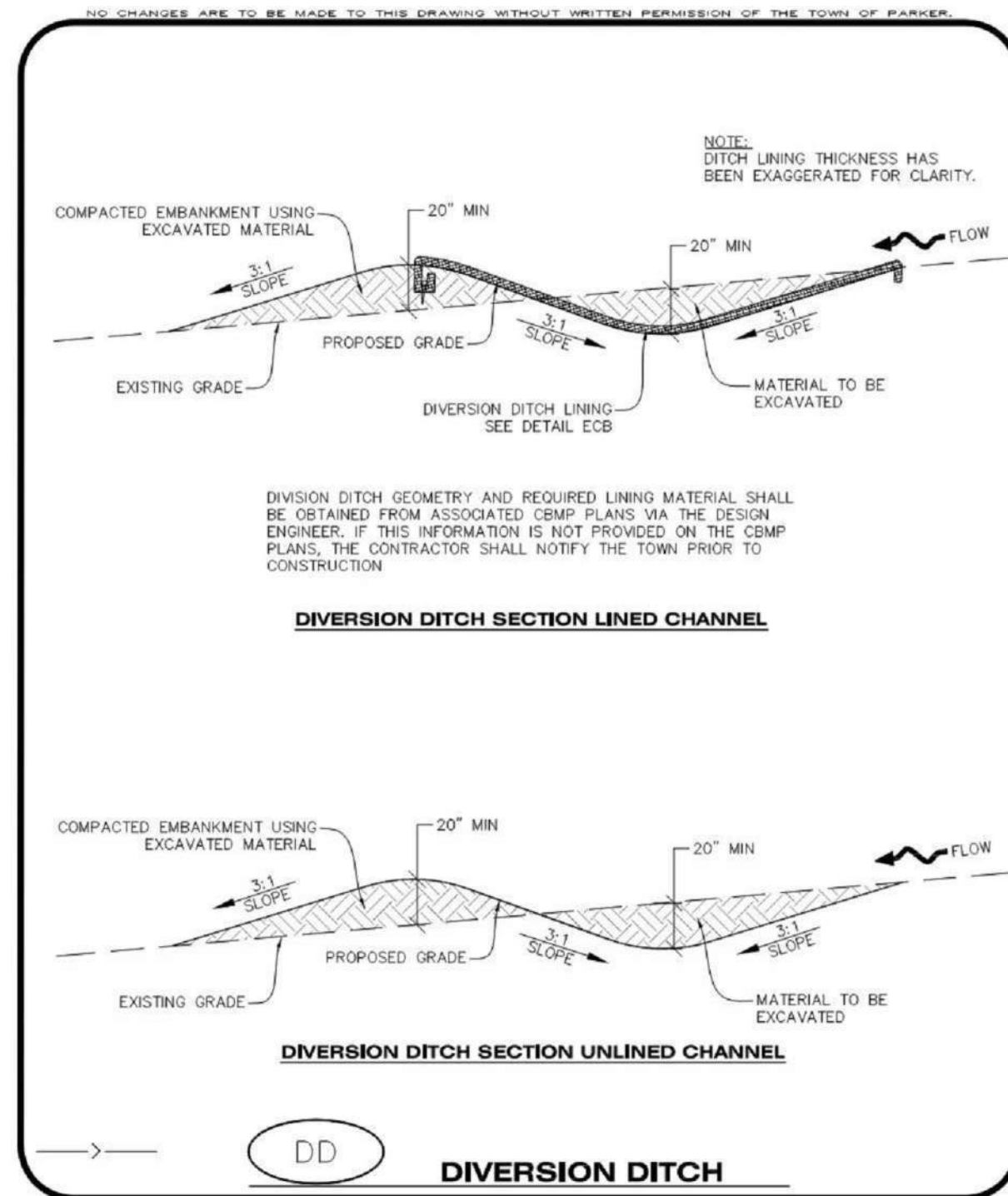
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 Parker COLORADO CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CD 2 OF 3 Oct. 2013

Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

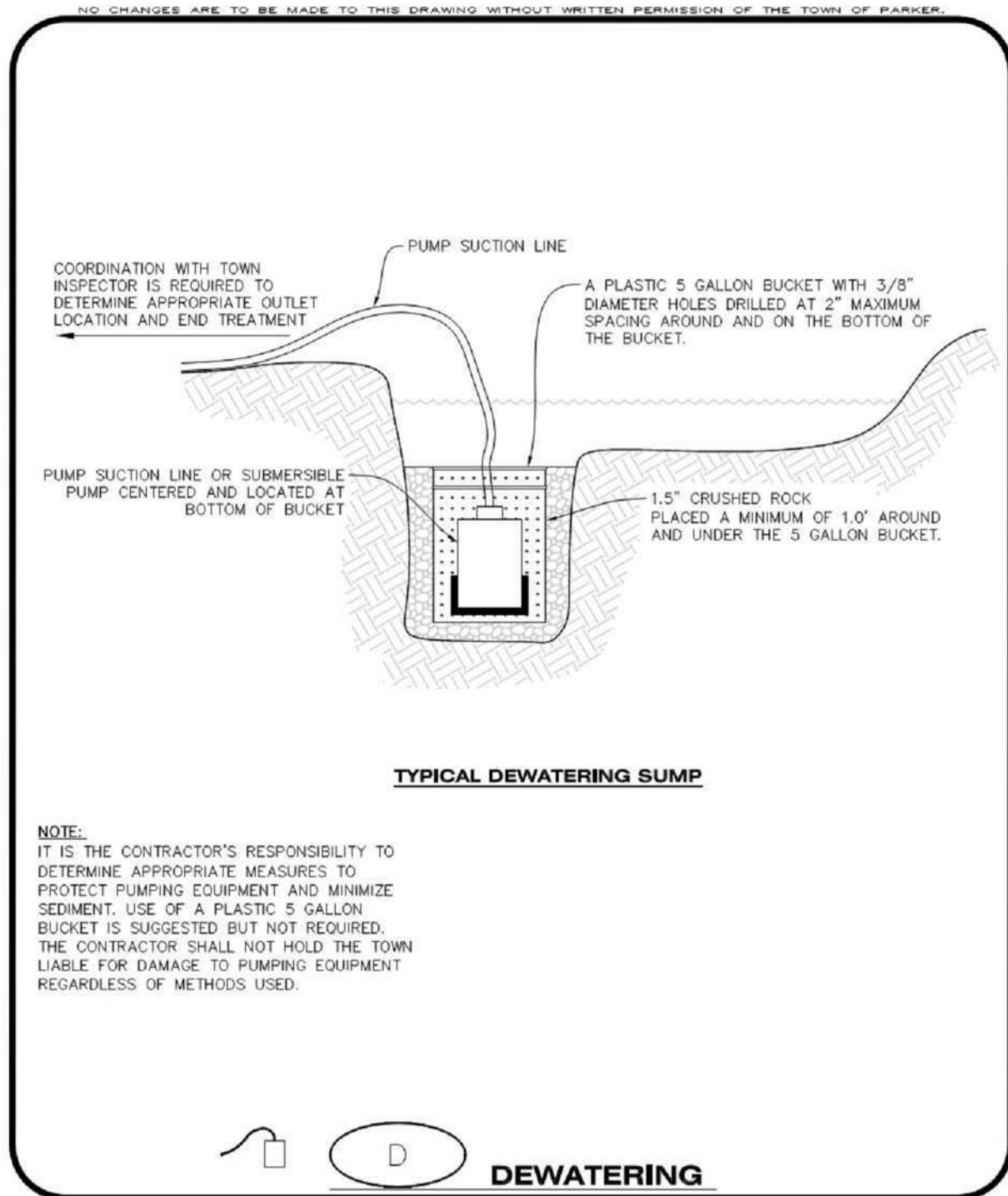
Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:



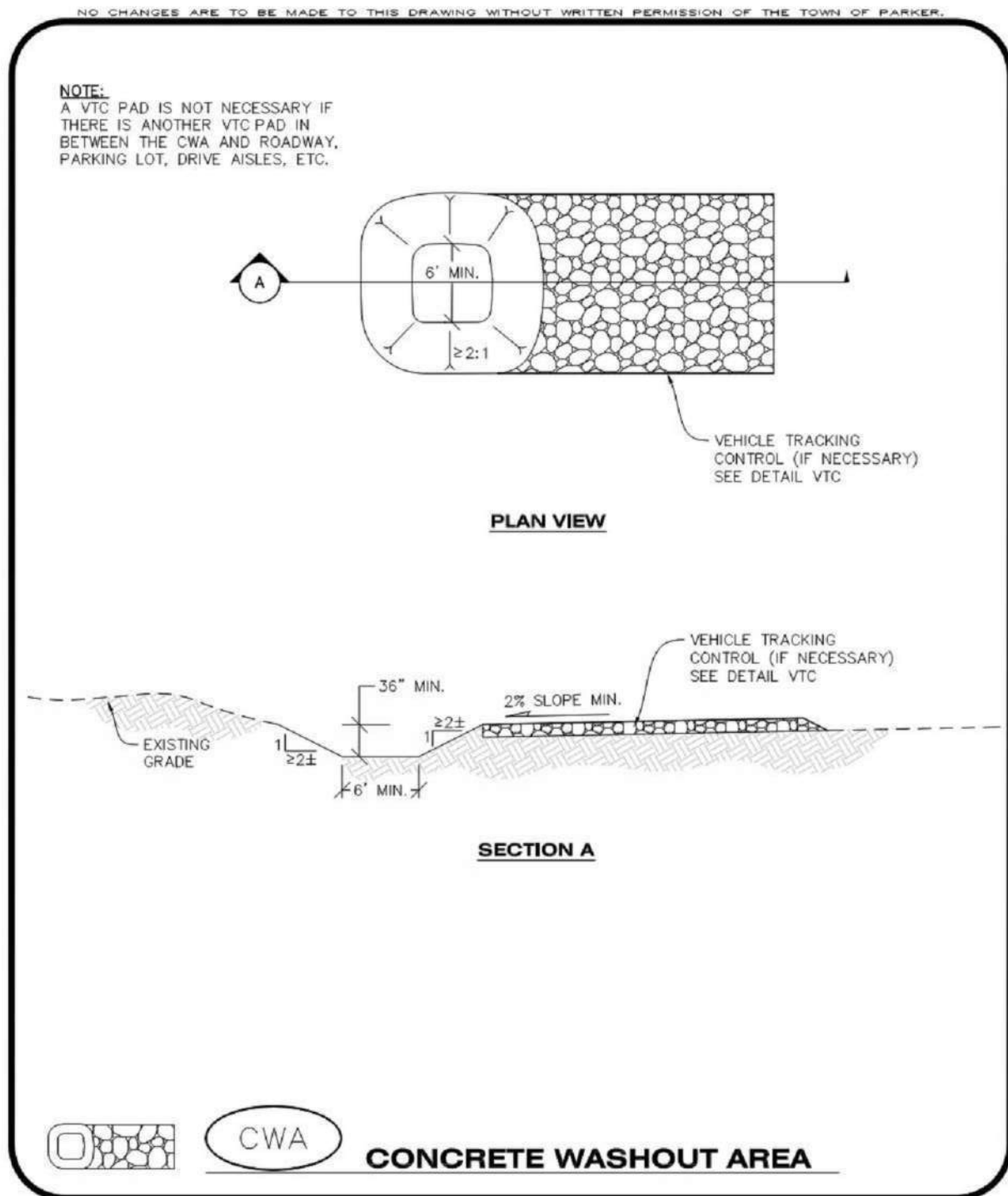
FOREBAY
OUTLET / MICROPOOL
DETECTION POND PROTECTION
CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES DP 1 OF 2 Oct. 2012



DIVERSION DITCH SECTION LINED CHANNEL
DIVERSION DITCH SECTION UNLINED CHANNEL
DIVERSION DITCH
CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES DD 1 OF 2 Oct. 2012



TYPICAL DEWATERING SUMP
DEWATERING
CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES D 1 OF 2 Oct. 2012



CONCRETE WASHOUT AREA
CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CWA 1 OF 2 Oct. 2012

DETECTION POND PROTECTION INSTALLATION NOTES

- DETECTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).

DETECTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
- OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES DP 2 OF 2 Oct. 2012

DIVERSION DITCH INSTALLATION NOTES

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
- A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
- ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
- THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
- THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
- DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
- DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES DD 2 OF 2 Oct. 2012

DEWATERING INSTALLATION NOTES

- IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

DEWATERING MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES D 2 OF 2 Oct. 2012

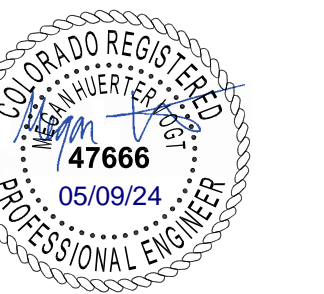
CONCRETE WASHOUT AREA INSTALLATION NOTES

- CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
- A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
- IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.

CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES

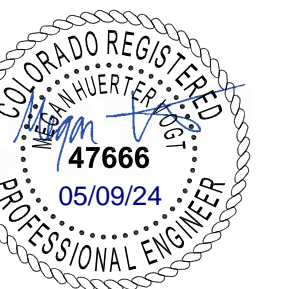
- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
- CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
- CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
- CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
- CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
- WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES CWA 2 OF 2 Oct. 2012



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:



THIS DRAWING IS TO BE MADE TO THE DRAWING WITHOUT WRITING PERMISSION OF THE TOWN OF PARKER.

AREA INLET PROTECTION INSTALLATION NOTES

- SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
- THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
- AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 75% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
IPAP 1 OF 1 04/ 2013

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EROSION CONTROL BLANKET INSTALLATION NOTES

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
- EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THESE CALCULATIONS, TURF REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.
- IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.
- EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.
- ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
- ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELISOR.

MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN	SL50
APPROVED EQUAL	APPROVED EQUAL

- IN MOST CASES, EROSION CONTROL BLANKETS FOR SLOPE PROTECTION ARE TO REMAIN IN PLACE PERMANENTLY.

EROSION CONTROL BLANKET MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

ECB 3 OF 3 04/ 2013

THIS DRAWING IS TO BE MADE TO THE DRAWING WITHOUT WRITING PERMISSION OF THE TOWN OF PARKER.

ECB EROSION CONTROL BLANKET (SLOPE)

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DTC DEBRIS AND TRASH CONTROL

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
IPAP 1 OF 1 04/ 2013

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
ECB 3 OF 3 04/ 2013

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
ECB 3 OF 3 04/ 2013

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
DTC 1 OF 2 04/ 2013

THIS DRAWING IS TO BE MADE TO THE DRAWING WITHOUT WRITING PERMISSION OF THE TOWN OF PARKER.

IPAP INLET PROTECTION FOR AREA INLETS IN PAVEMENT

THIS DRAWING IS TO BE MADE TO THE DRAWING WITHOUT WRITING PERMISSION OF THE TOWN OF PARKER.

IPAN INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT

THIS DRAWING IS TO BE MADE TO THE DRAWING WITHOUT WRITING PERMISSION OF THE TOWN OF PARKER.

ECB EROSION CONTROL BLANKET (CHANNEL)

THIS DRAWING IS TO BE MADE TO THE DRAWING WITHOUT WRITING PERMISSION OF THE TOWN OF PARKER.

DEBRIS CONTROL NOTES:

- A COMBINATION OF SURFACE SCRAPING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THESE AREAS.
- ALL CHEMICAL SPILLS AND/OR STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EXTENT PRACTICABLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VAC-TRUCK.
- ON-SITE PERSONNEL, DELIVERY DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUAL DEBRIS AND TRASH CONTROL.

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CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
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CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
IPAN 1 OF 1 04/ 2013

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
ECB 2 OF 3 04/ 2013

CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES
DTC 2 OF 2 04/ 2013

Parker Town Hall Expansion

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22-093
Town of Parker
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Parker, CO 80138

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3198 Speer Boulevard
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Telephone: 303-294-9448

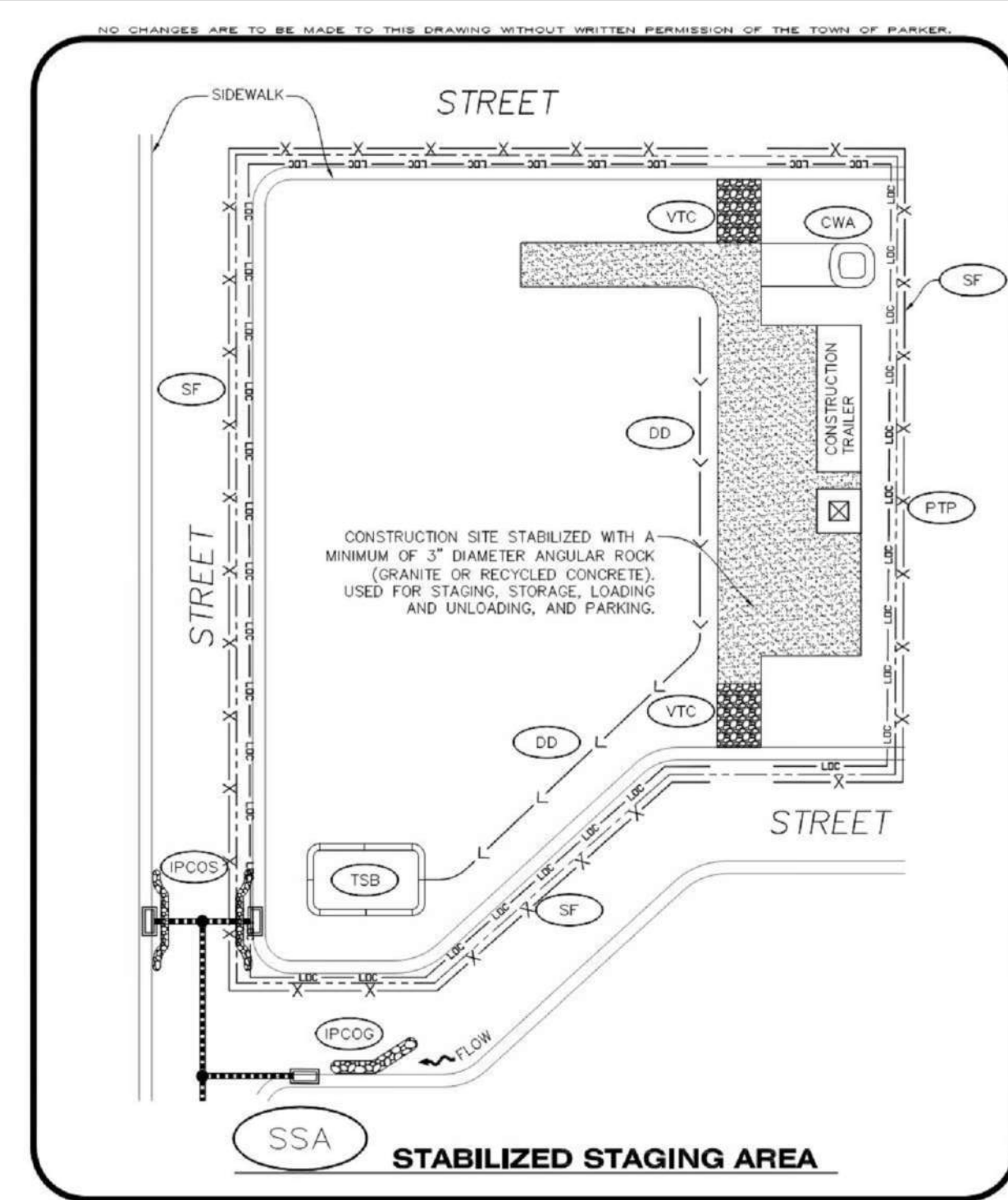
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Landscape Architect
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2012

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

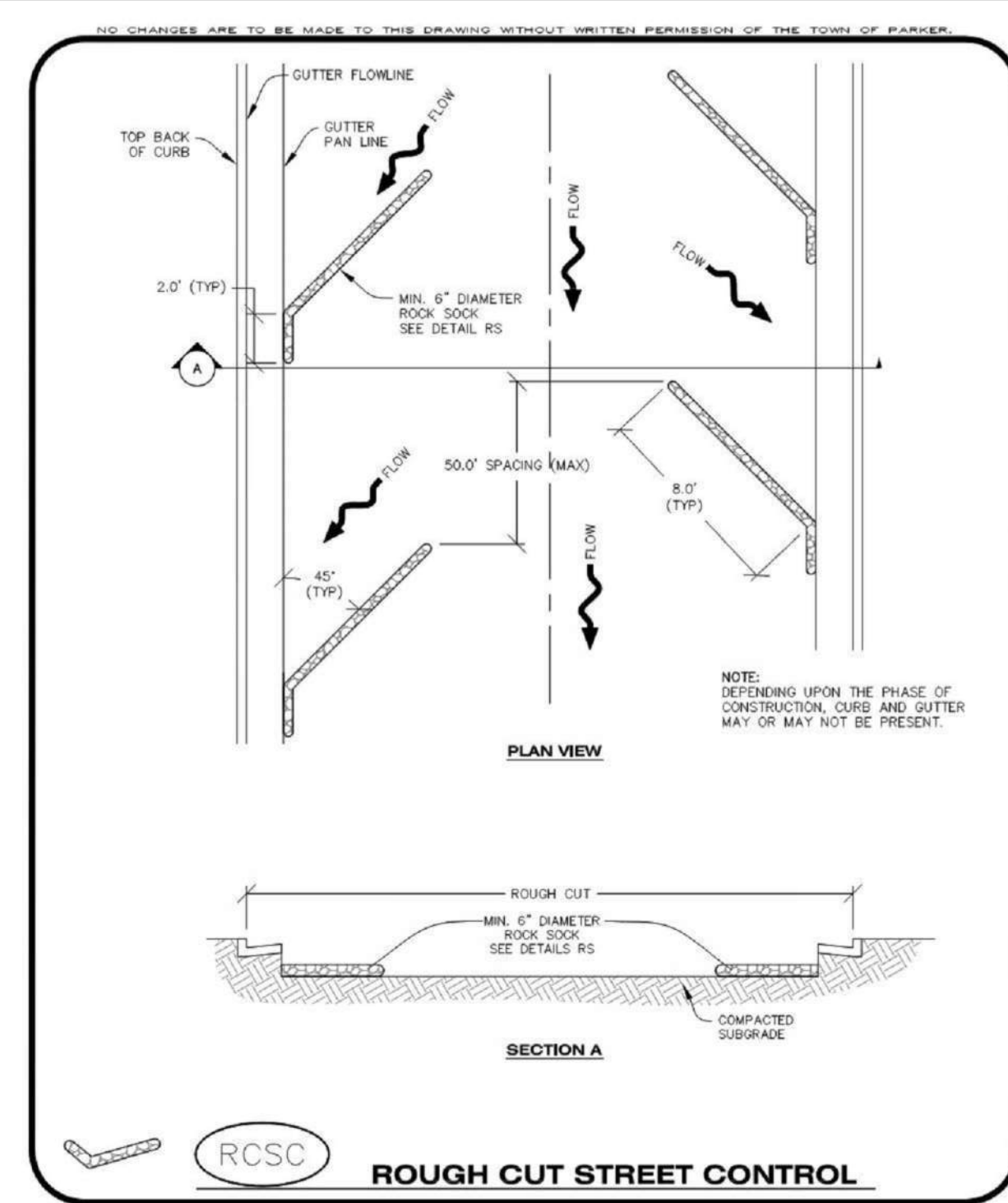
STABILIZED STAGING AREA INSTALLATION NOTES

- SEE CBMP PLAN FOR LOCATION OF STAGING AREA. CONTRACTOR MAY MODIFY LOCATION AND SIZE OF STABILIZED STAGING AREA WITH TOWN APPROVAL.
- STABILIZED STAGING AREA SHALL BE LARGE ENOUGH TO FULLY CONTAIN PARKING, STORAGE, AND LOADING OPERATIONS.
- THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM OF 3" DIAMETER OF ANGULAR ROCK (GRANITE OR RECYCLED CONCRETE).
- SSA FOR SMALLER SITES MAY NOT BE PRACTICAL. IN THESE AND SIMILAR SITUATIONS, VARIANCES MAY BE PERMITTED BY THE TOWN.

STABILIZED STAGING AREA INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STAGING AREA.
- STABILIZED STAGING AREA SHALL BE ENLARGED AS NECESSARY TO CONTAIN PARKING, STORAGE, LOADING, AND UNLOADING.

CBMP | **SSA**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2012



CBMP | **RCSC**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2012

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

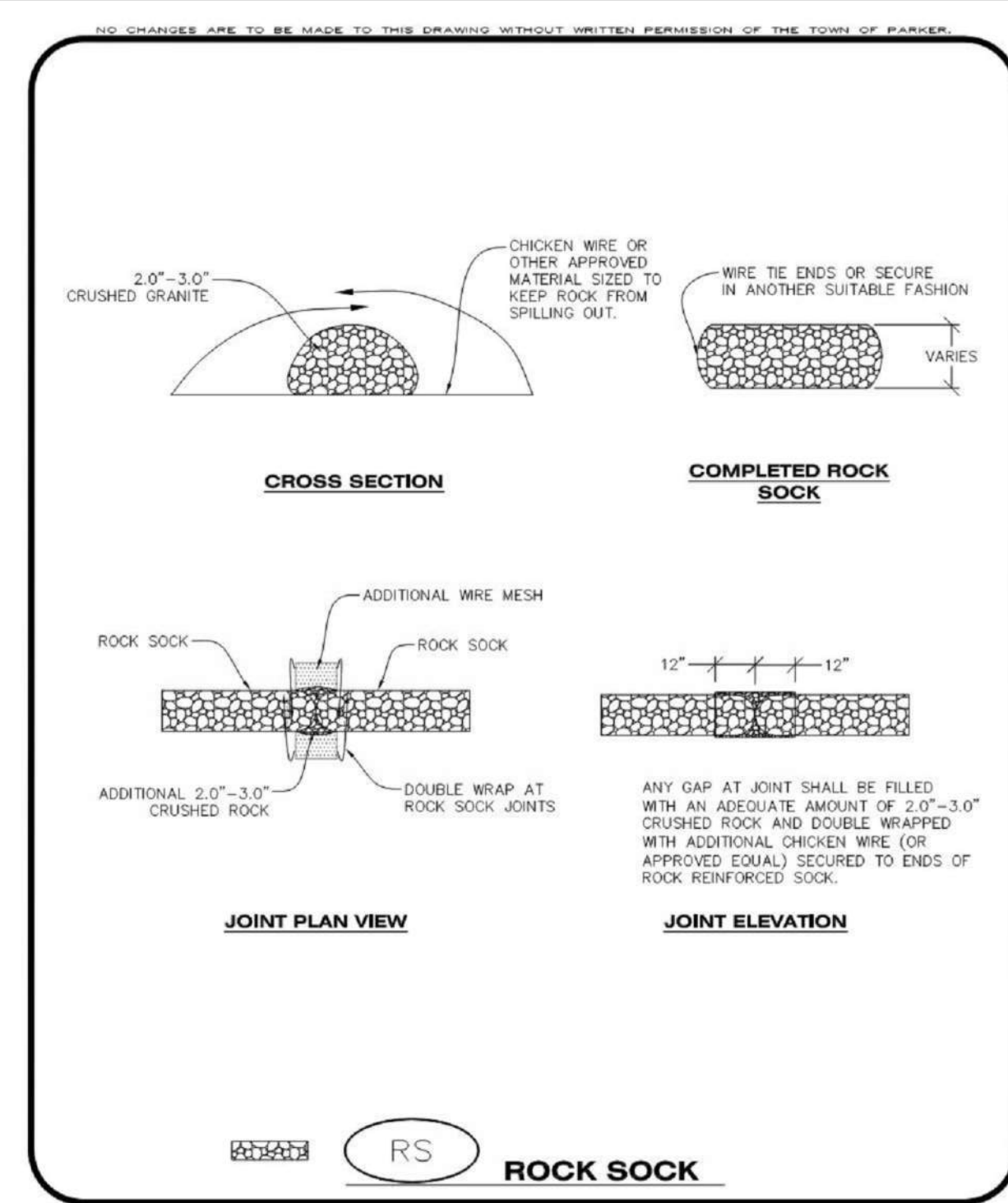
ROUGH CUT STREET CONTROL INSTALLATION NOTES

- SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
- THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.

ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
- ROUGH CUT STREET CONTROL SHALL BE REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
- ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUB-GRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE RCSC SHOULD BE REMOVED IN INCREMENTS BASED ON SUBGRADE PREPARATION.

CBMP | **RCSC**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2012



CBMP | **RS**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2012

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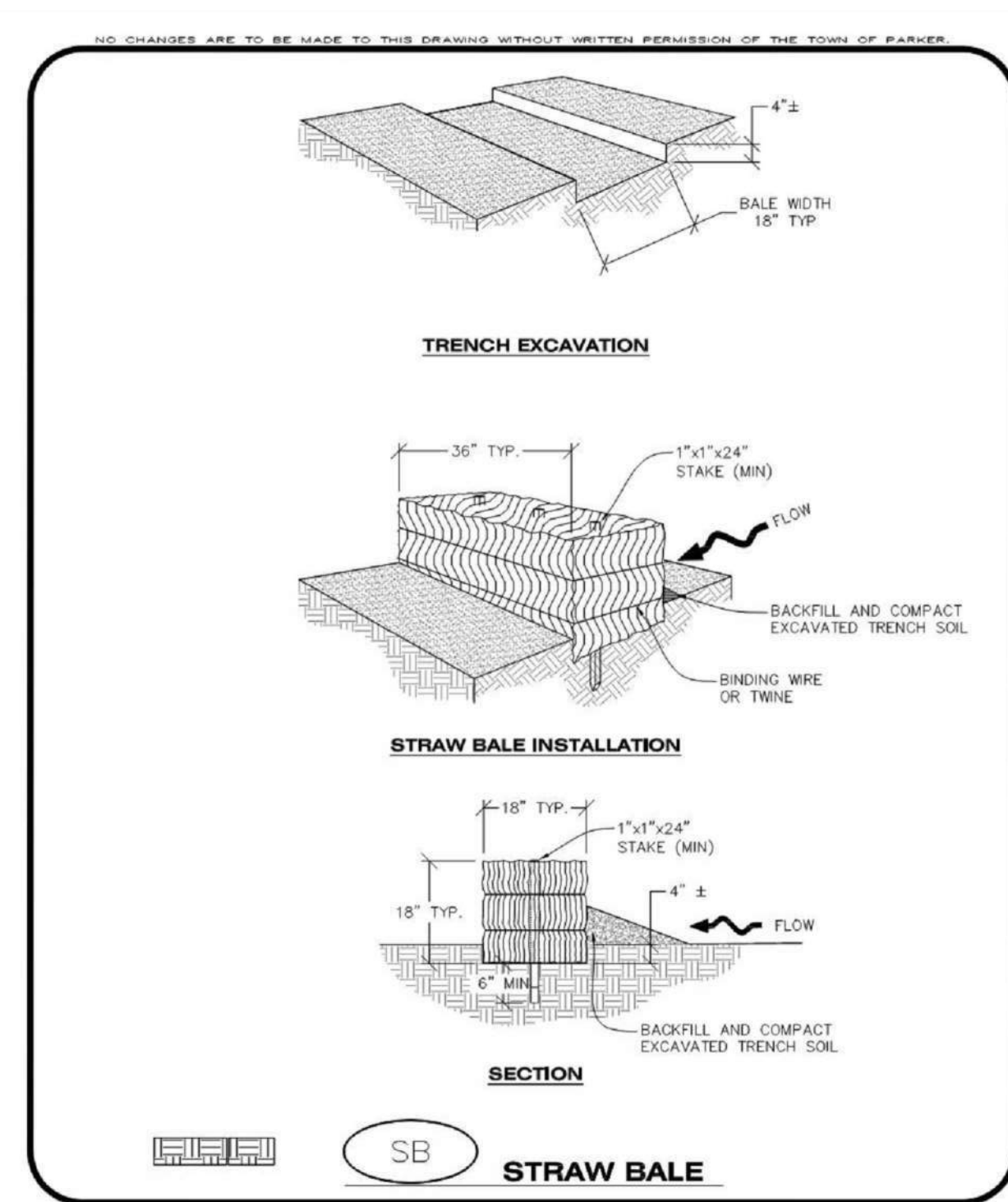
ROCK SOCK INSTALLATION NOTES

- SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
- CRUSHED ROCK SHALL BE APPROXIMATELY 2.0'-3.0" GRANITE IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
- TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCATION SUSCEPTIBLE TO VEHICLE OR PEDESTRIAN TRAFFIC. TUBULAR MARKERS SHALL CONFORM TO THE TUBULAR MARKER DETAIL.

ROCK SOCK INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

CBMP | **RS**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2012



CBMP | **SB**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2012

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STRAW BALE INSTALLATION NOTES

- SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
- TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36"x18"x18".
- TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1"x1"x24".
- WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
- STRAW BALES SHALL BE SPACED AND POSITIONED ACCORDING TO DETAILS.

STRAW BALE INSPECTION AND MAINTENANCE NOTES

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
- STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
- STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMO).

CBMP | **SB**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2012



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

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Landscape Architect

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Structural Engineer

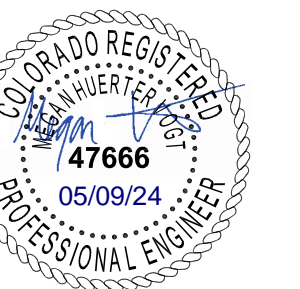
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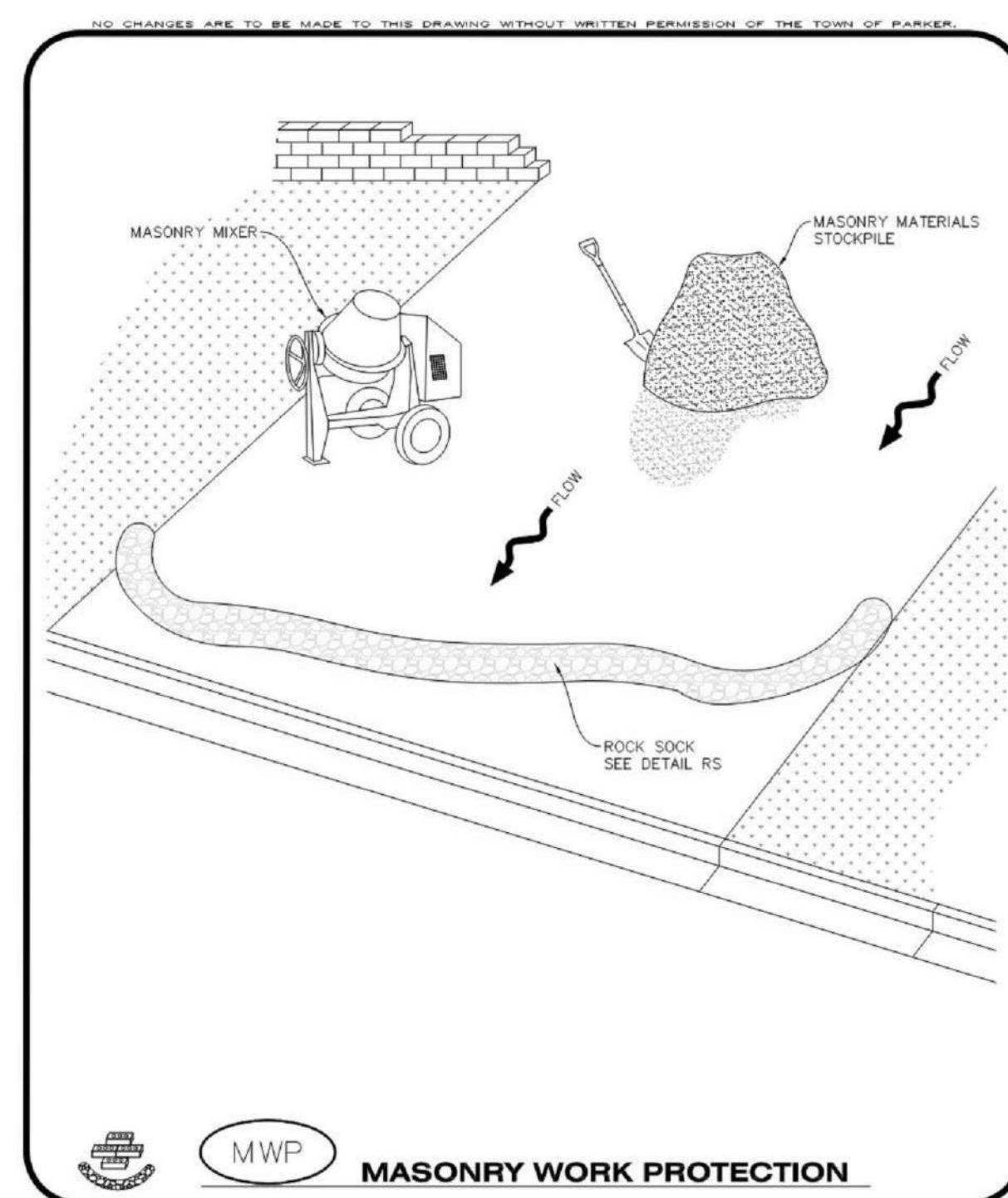
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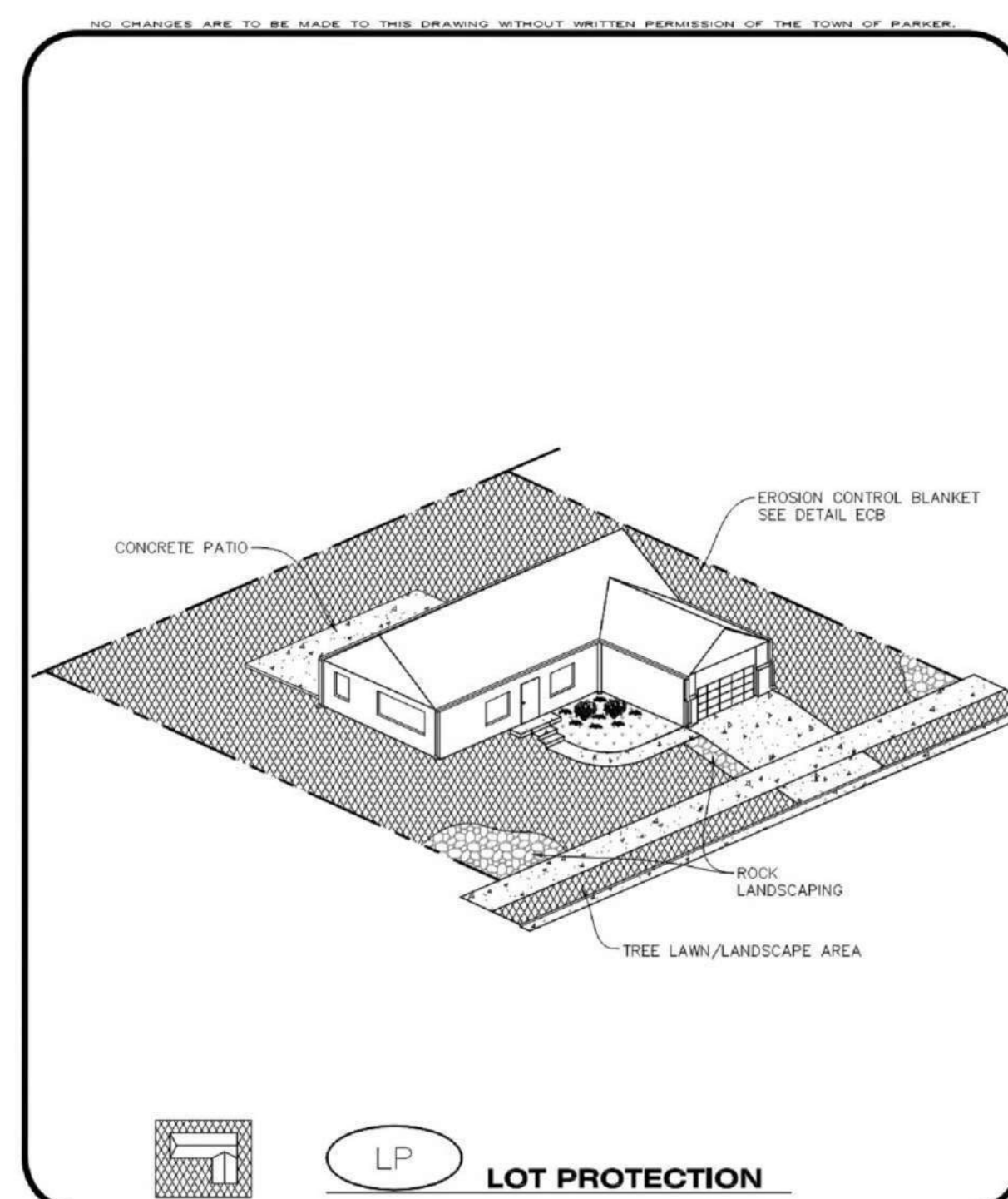


Issue: _____ Date: _____
SCHEMATIC DESIGN 01 MAY 2023
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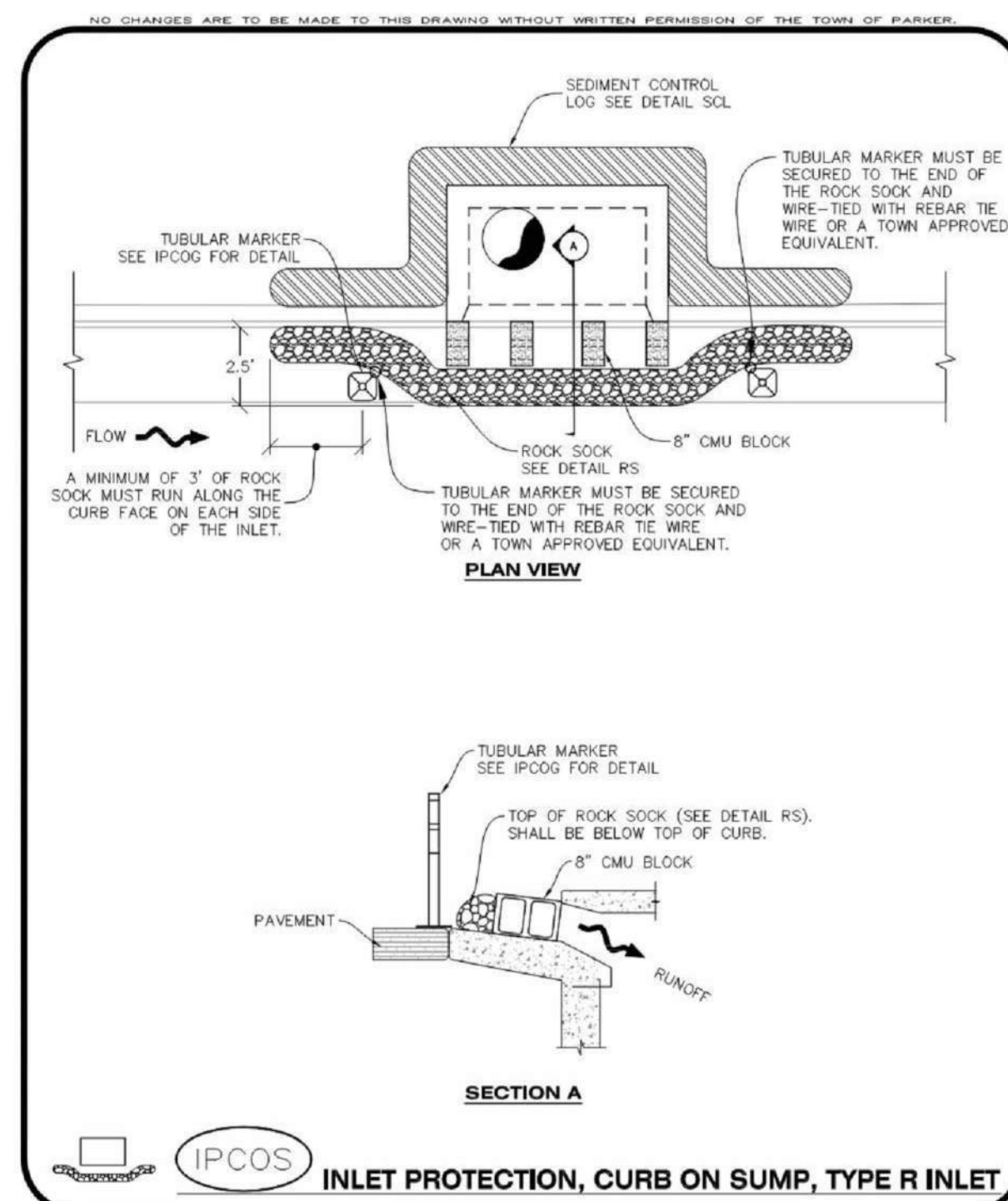
Project Number: 22-139
Drawn By: _____
Reviewed By: _____
Approved By: _____



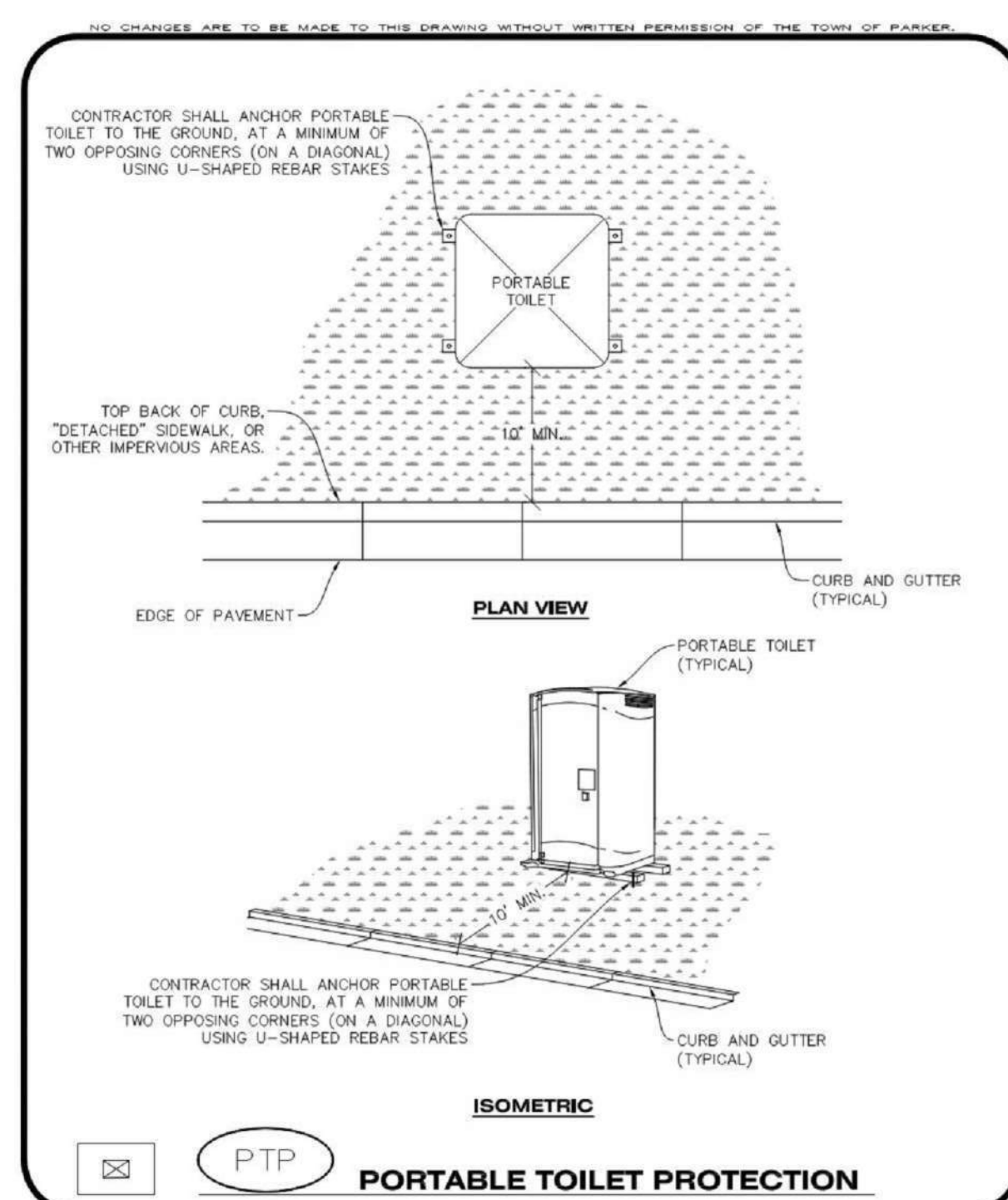
Parker COLORADO | **CBMP** | **MWP**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2017



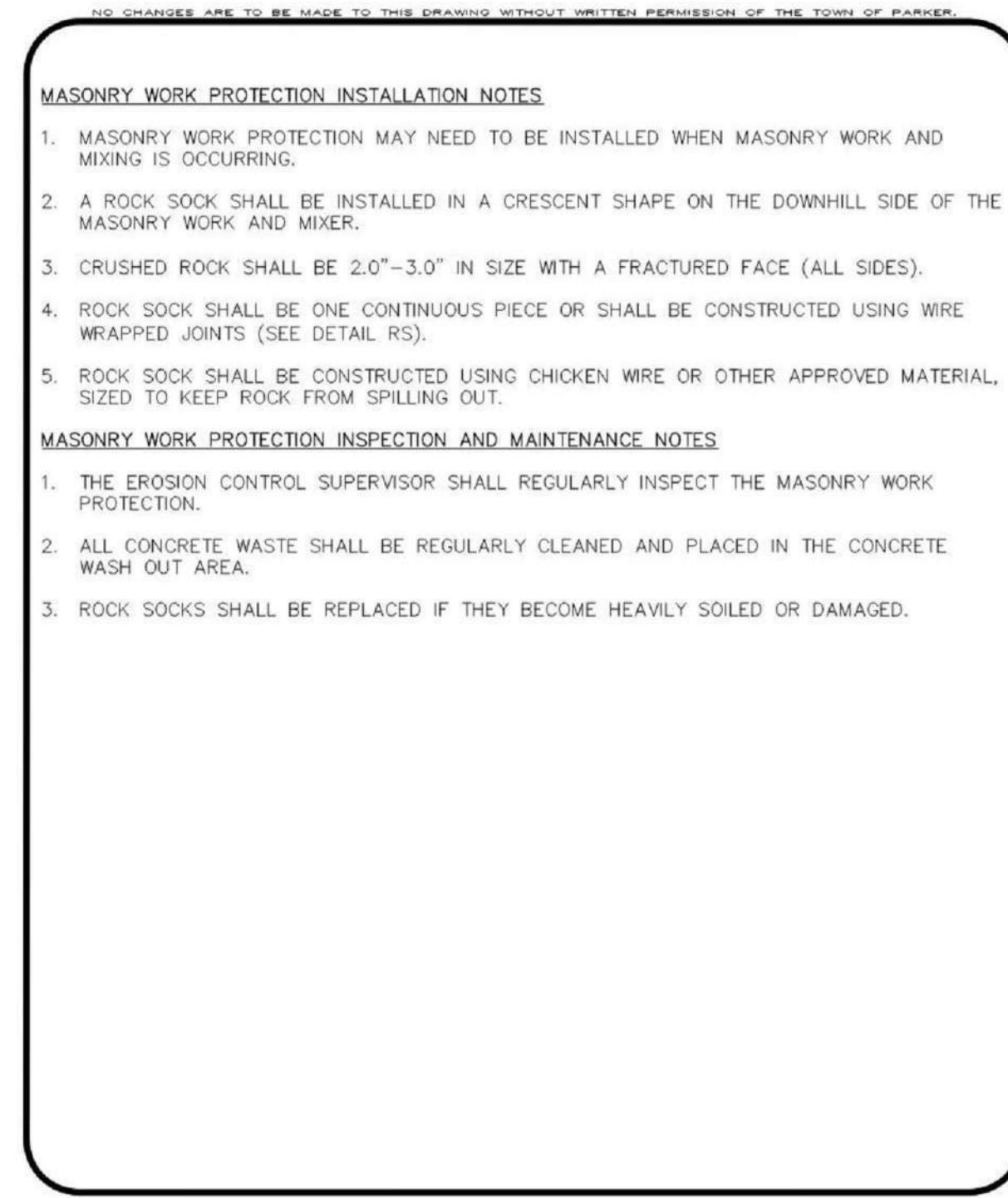
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2017



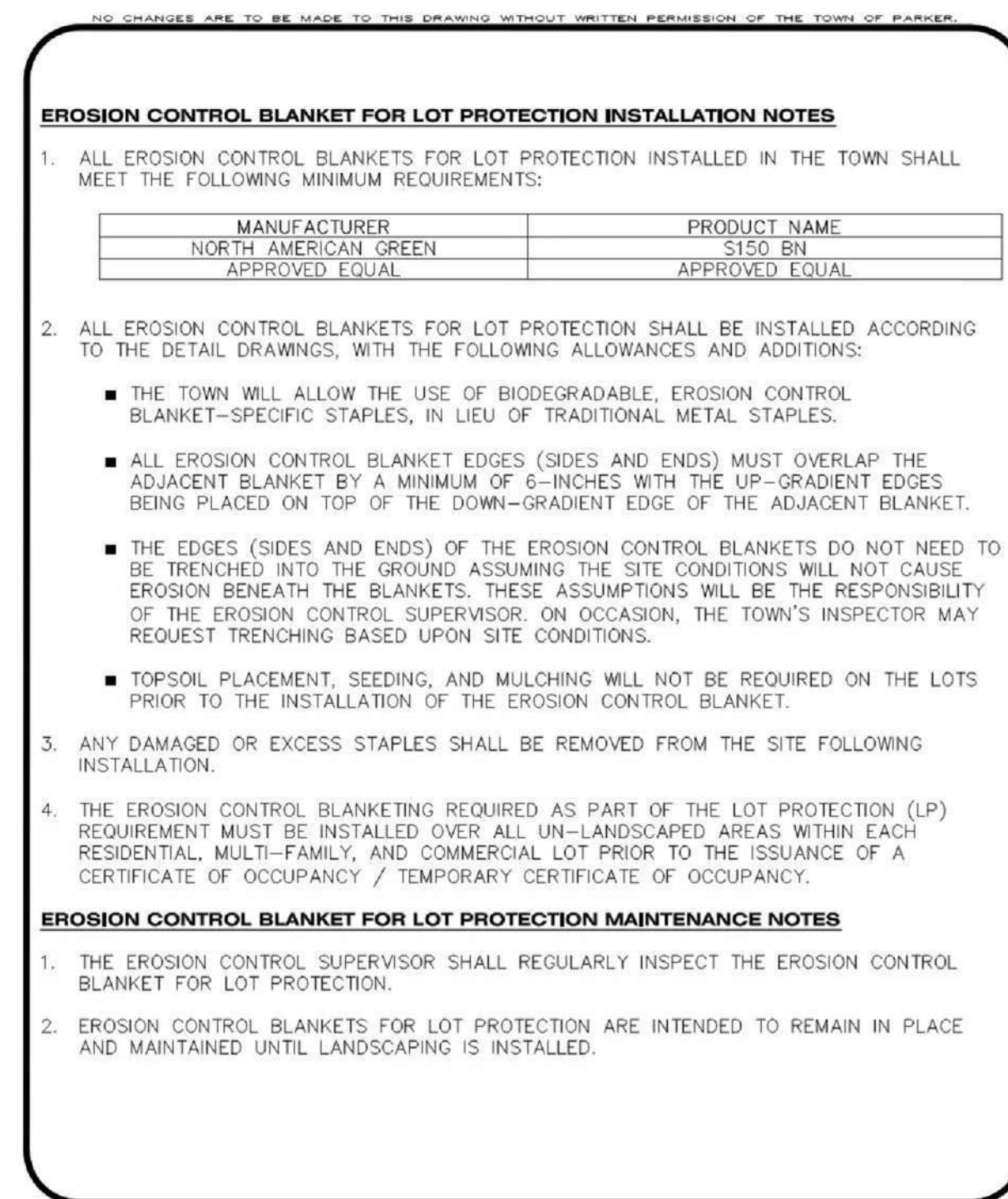
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2017



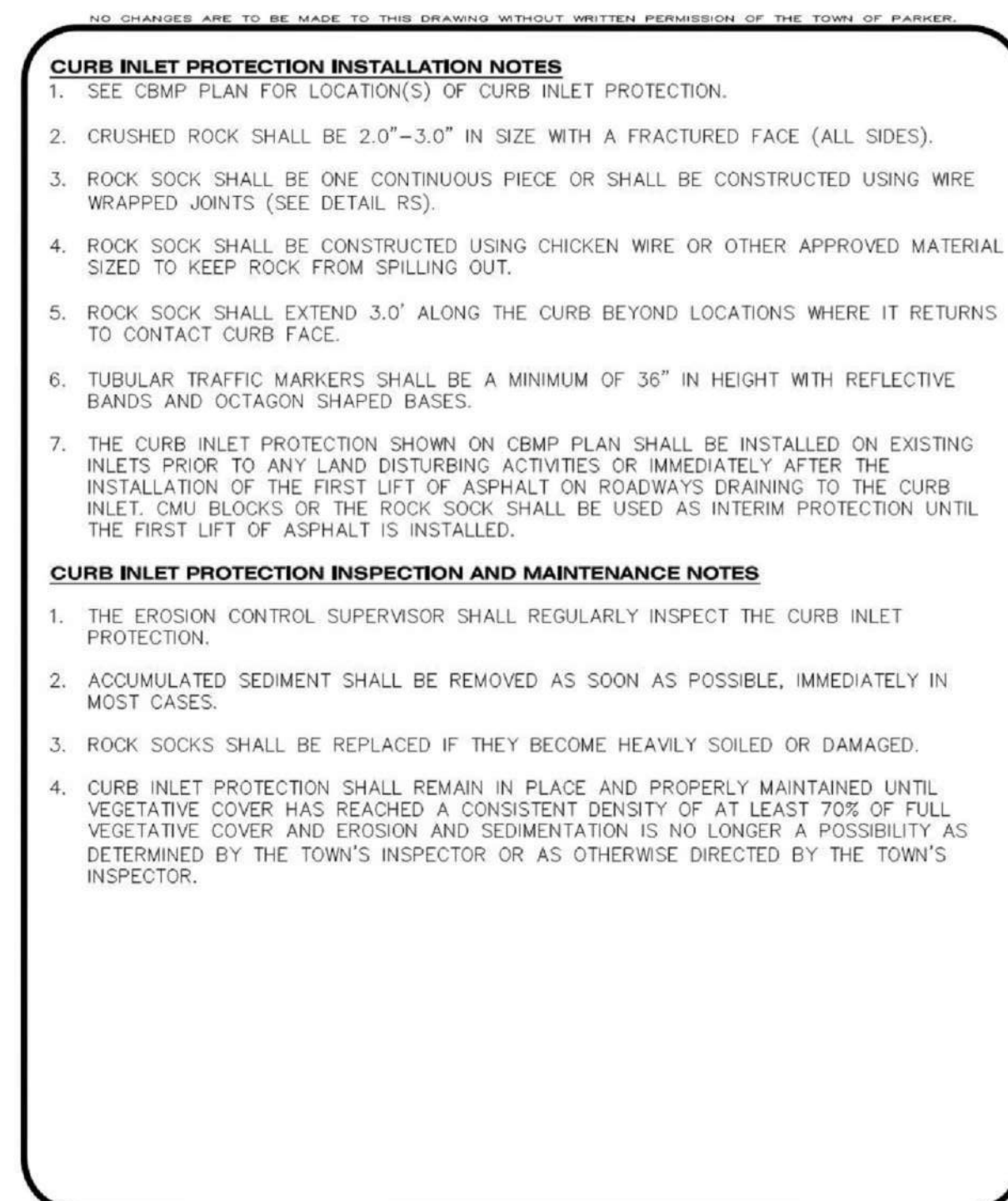
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
04/1/2017



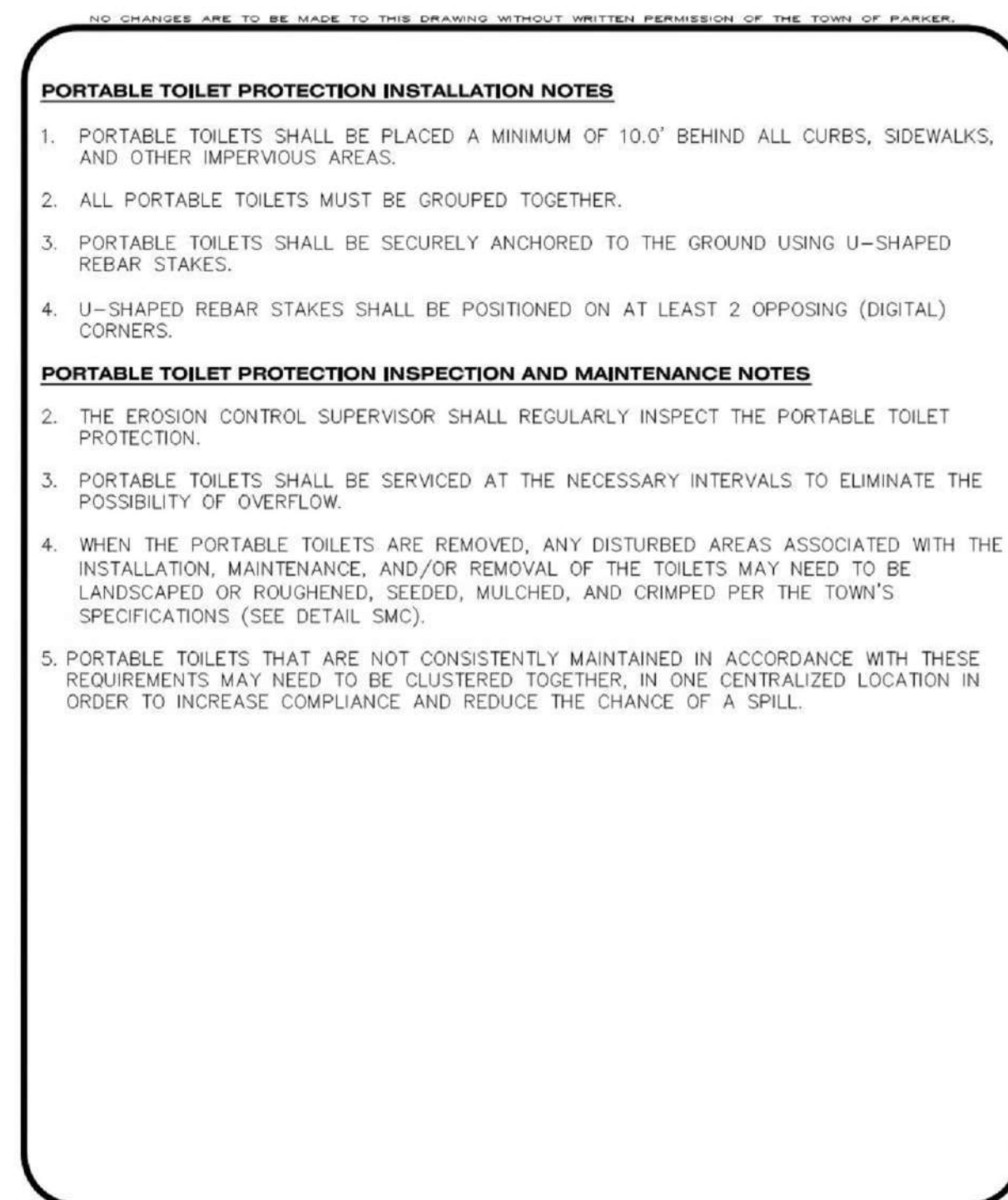
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2017



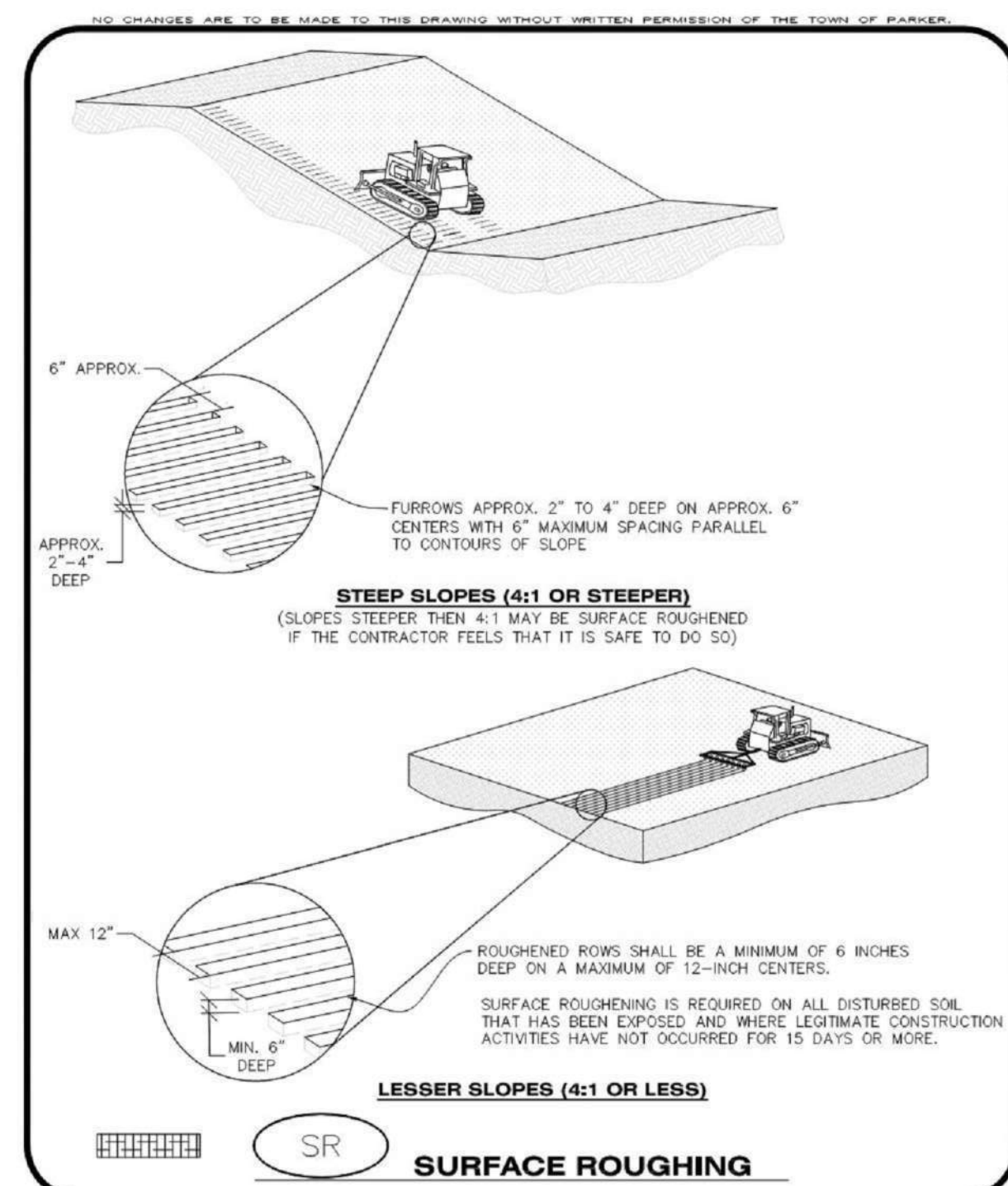
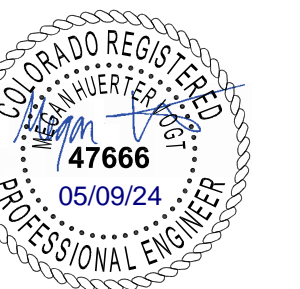
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2017



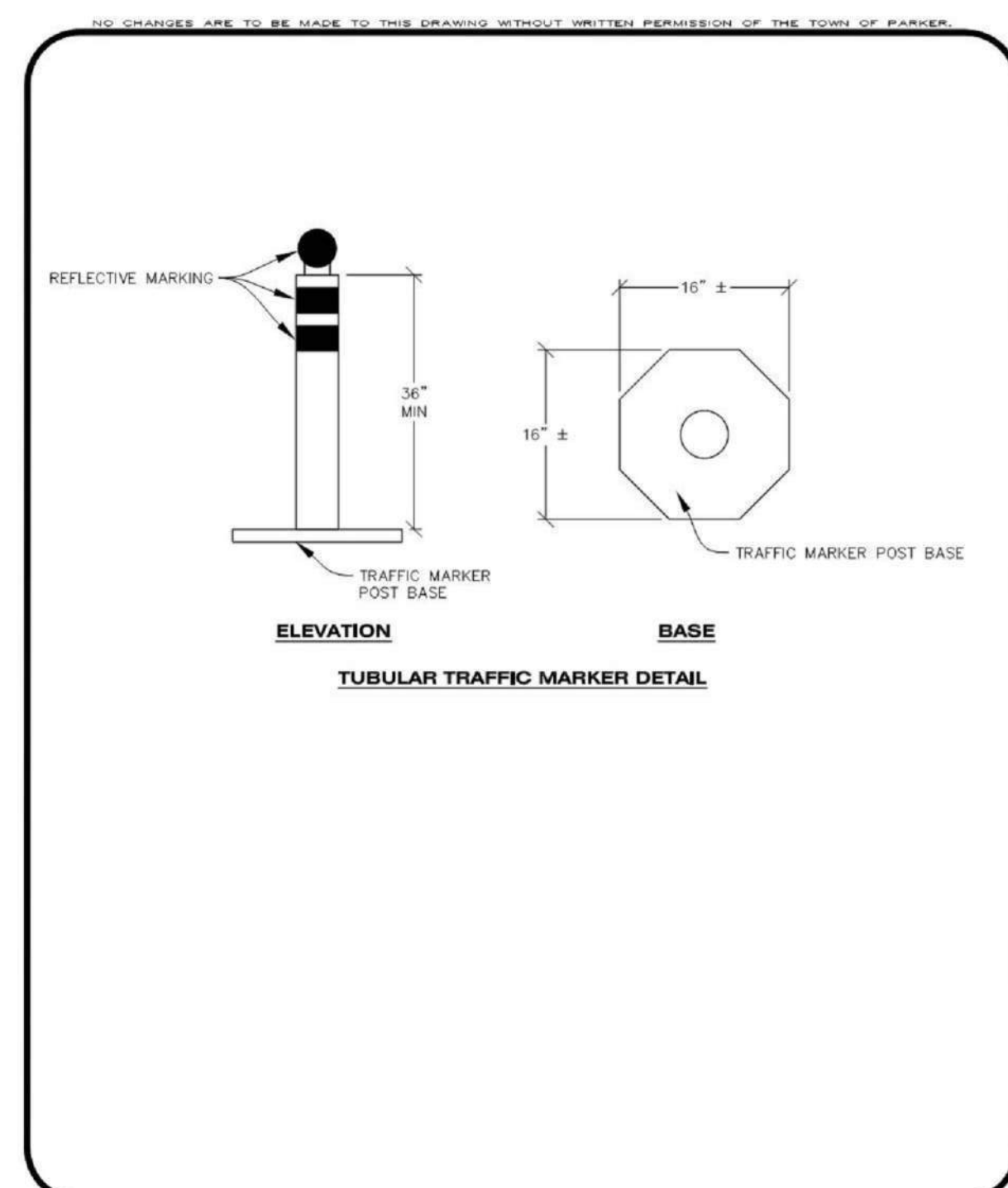
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2017



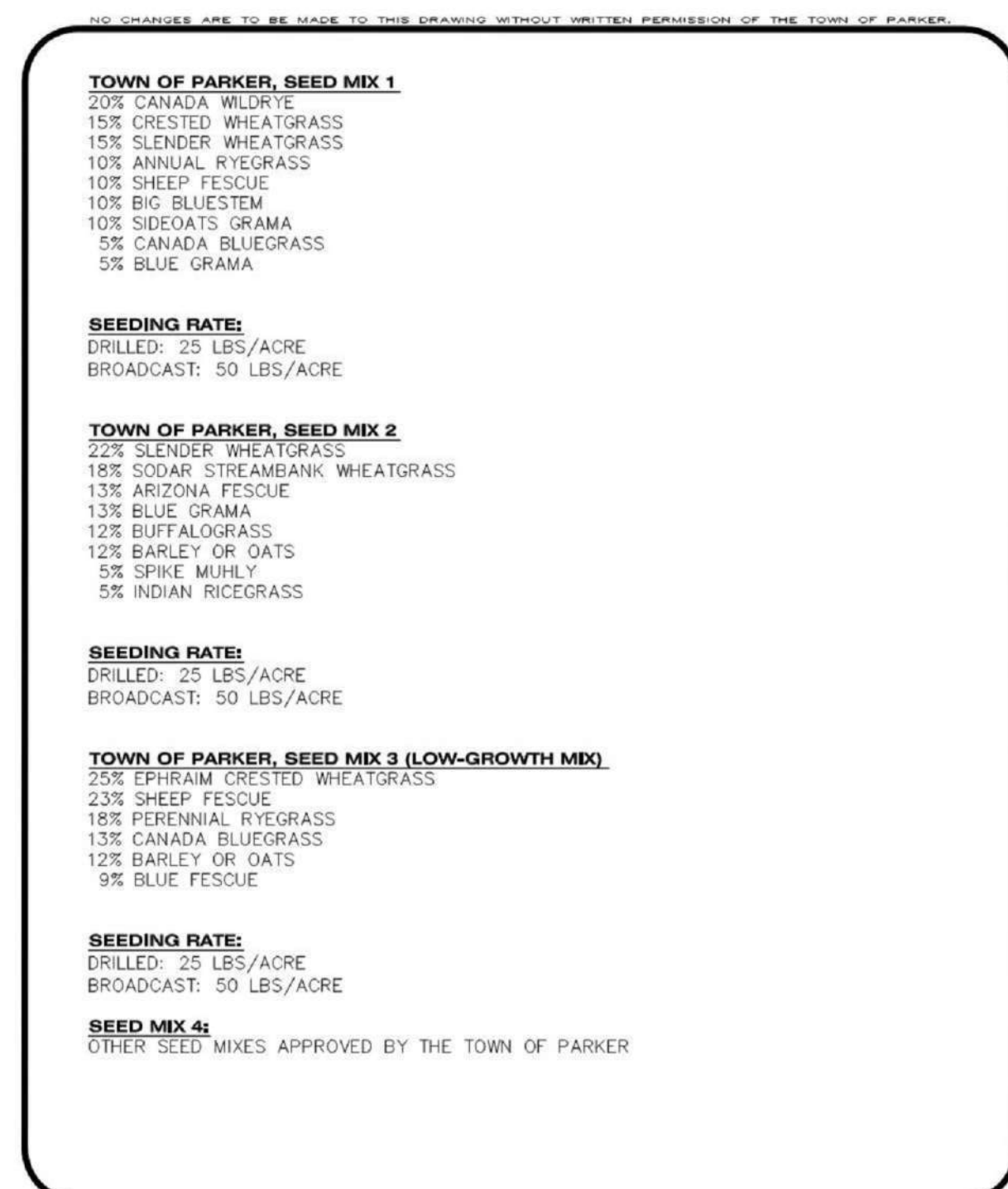
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2
04/1/2017



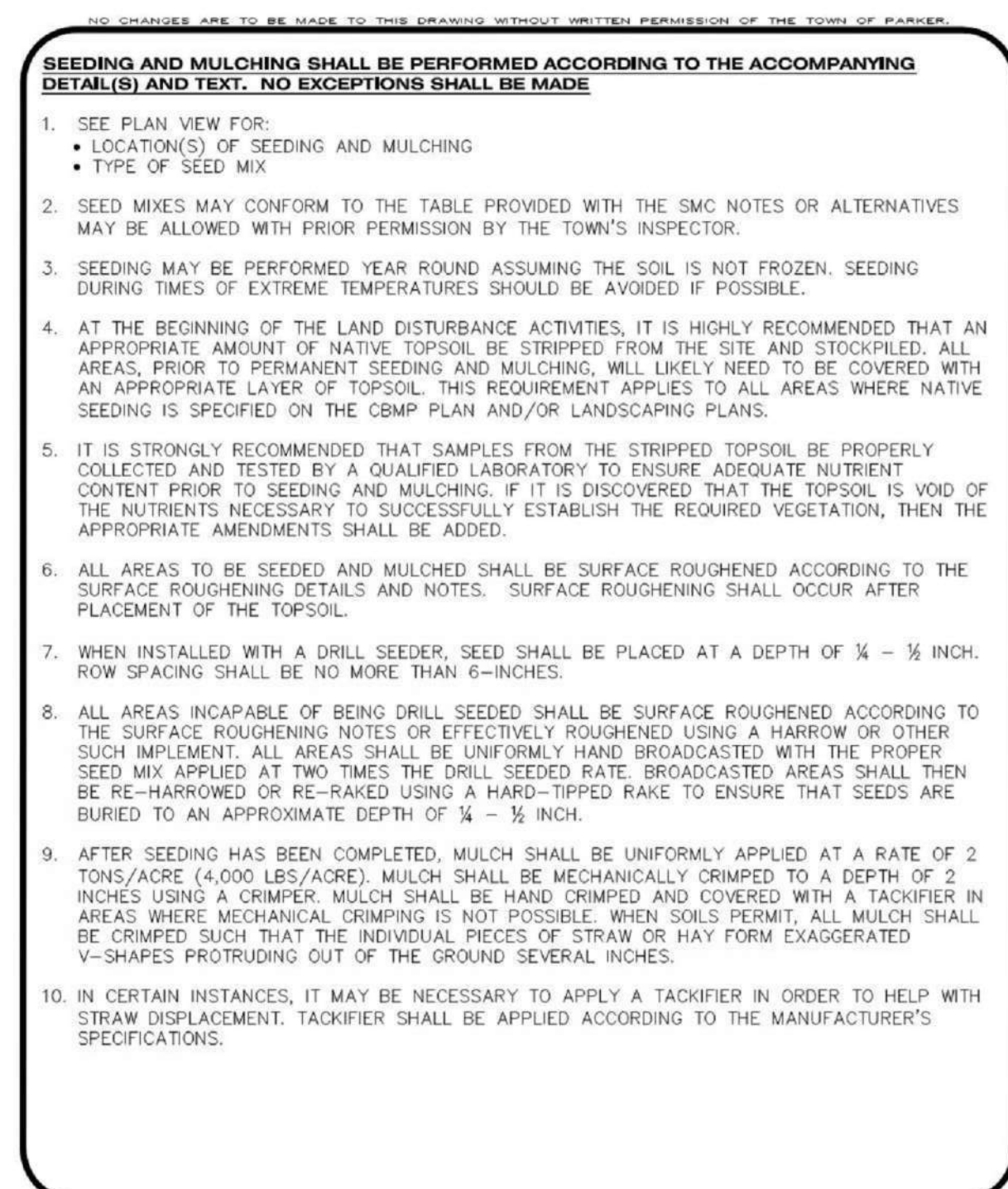
CBMP | **SR**
CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2
OCT. 2013



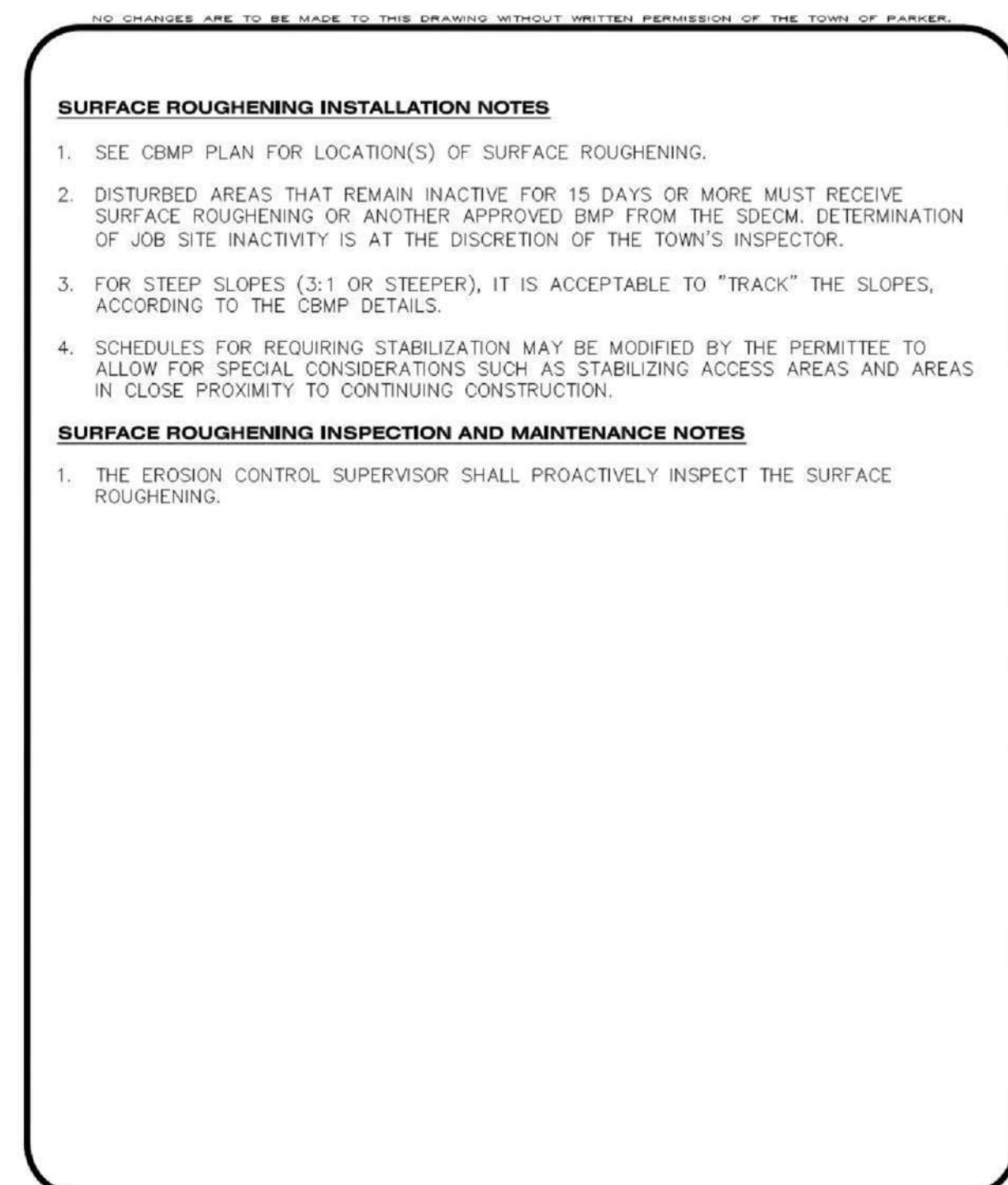
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3
OCT. 2013



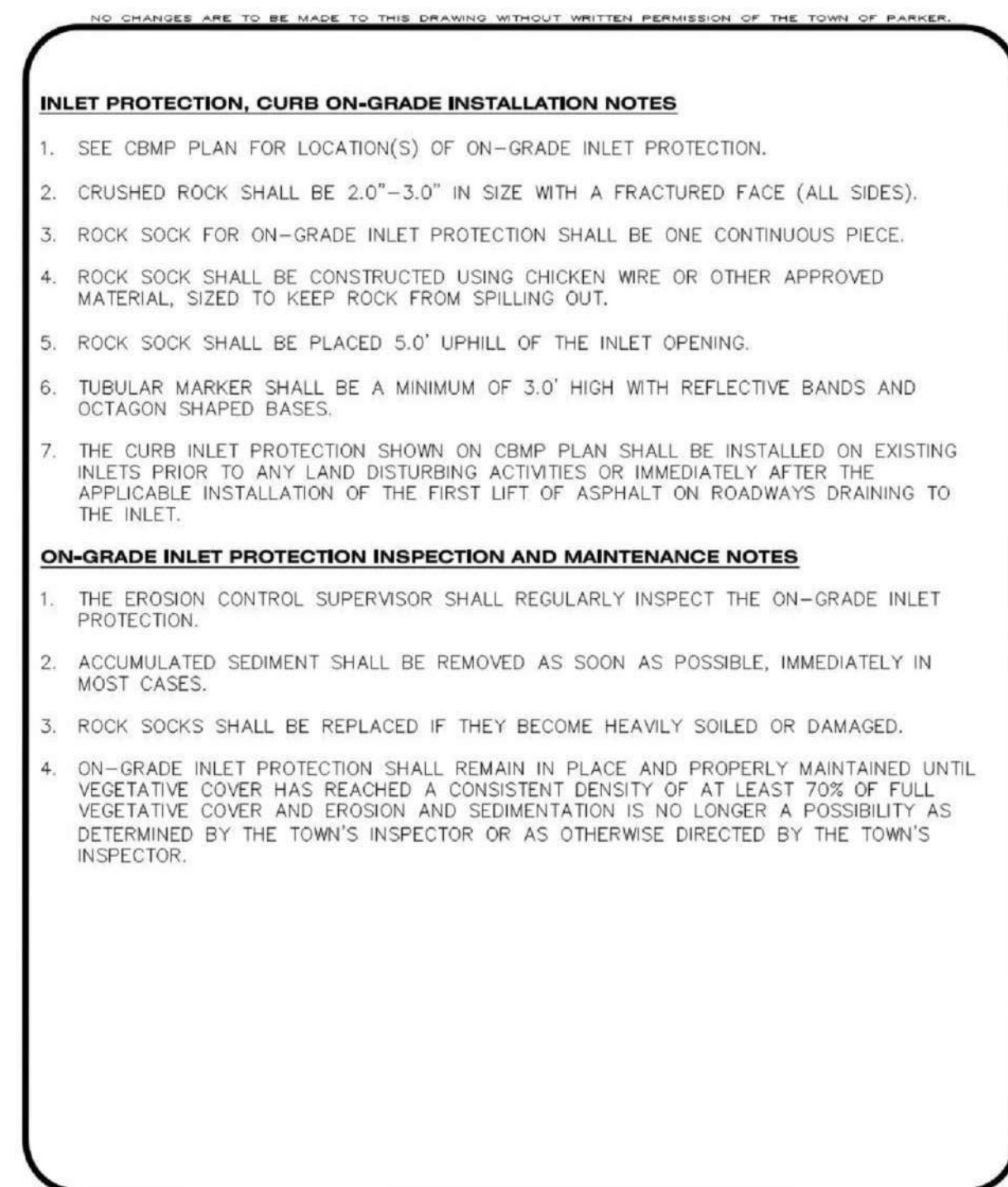
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3
OCT. 2013



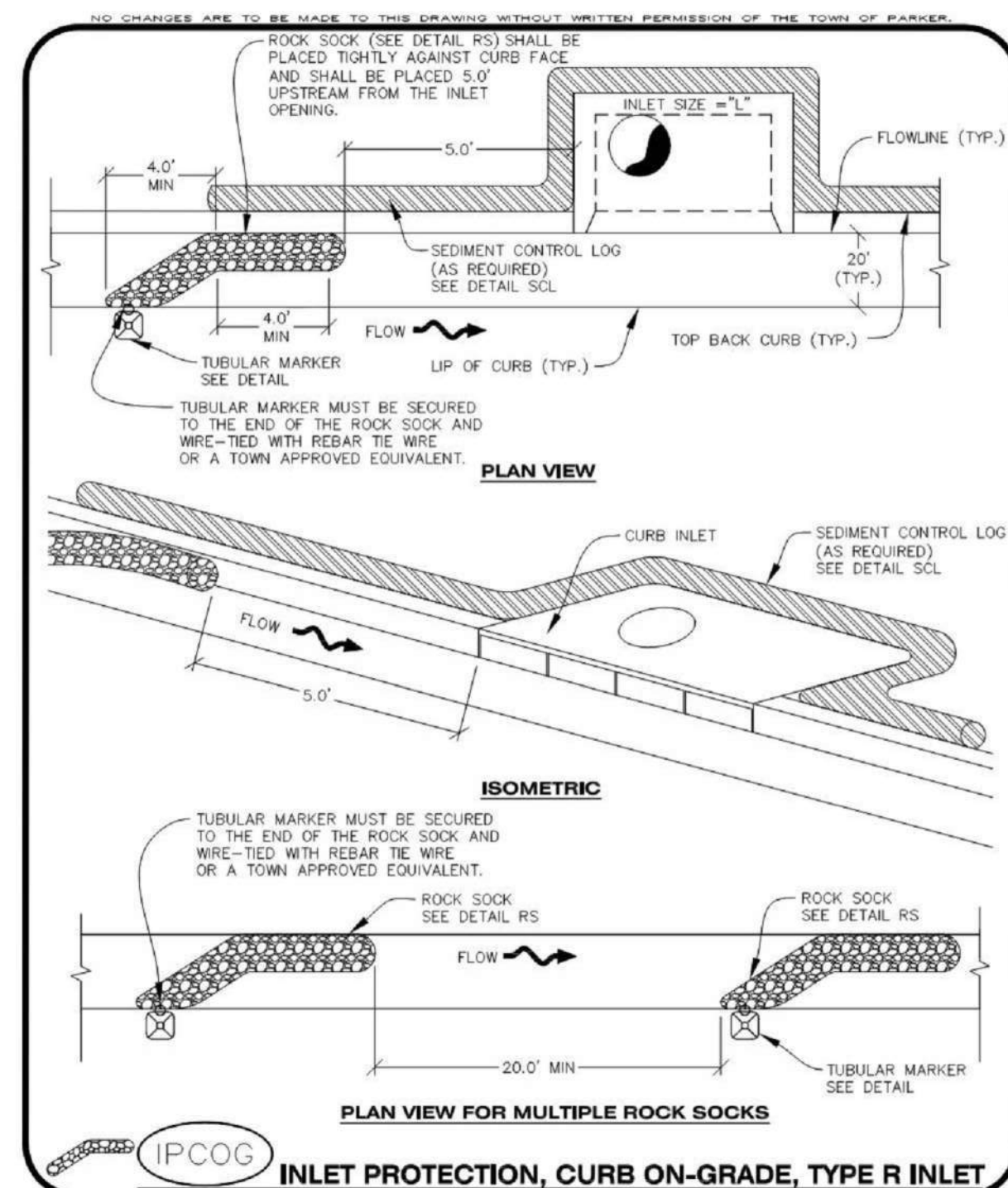
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3
OCT. 2013



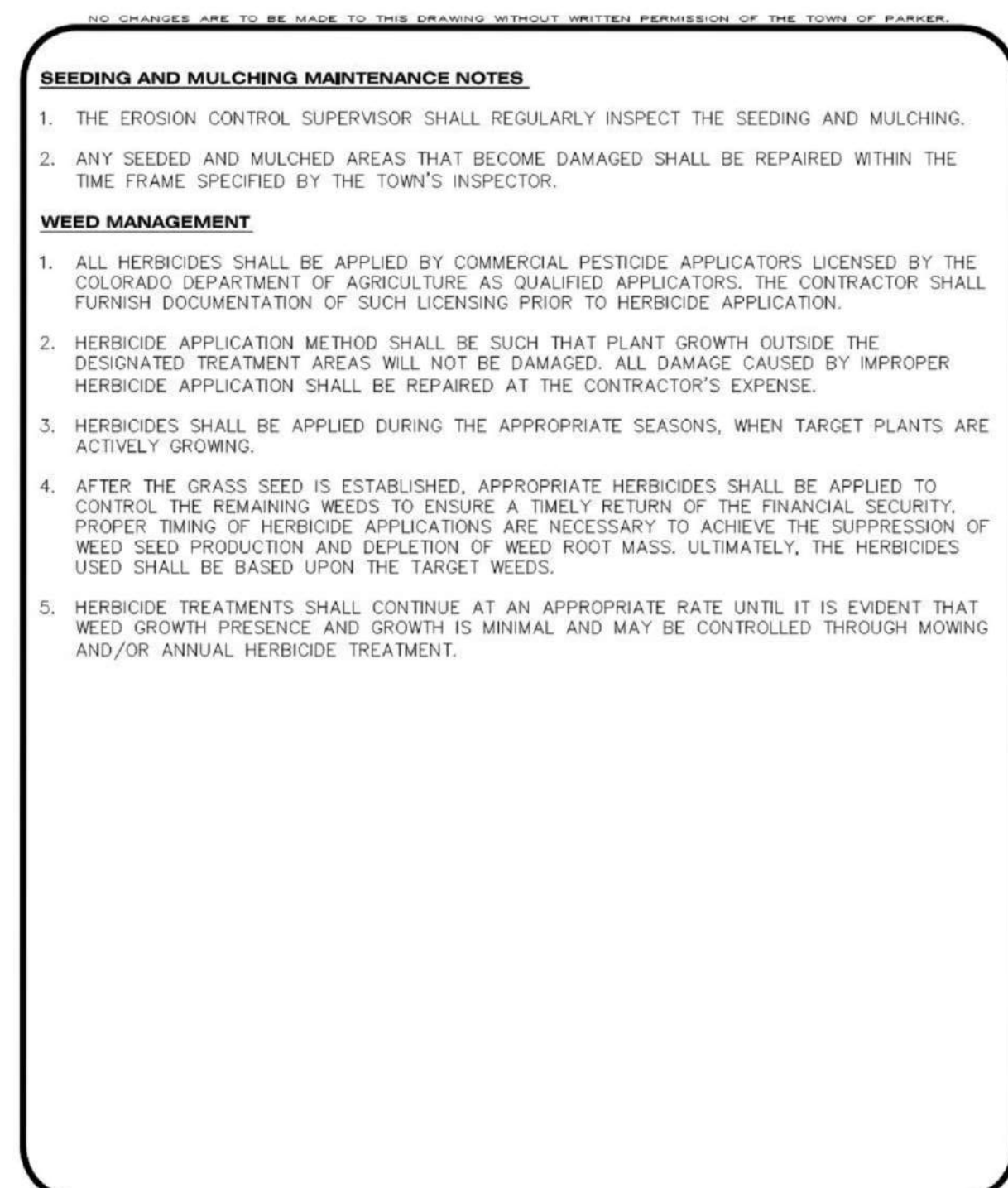
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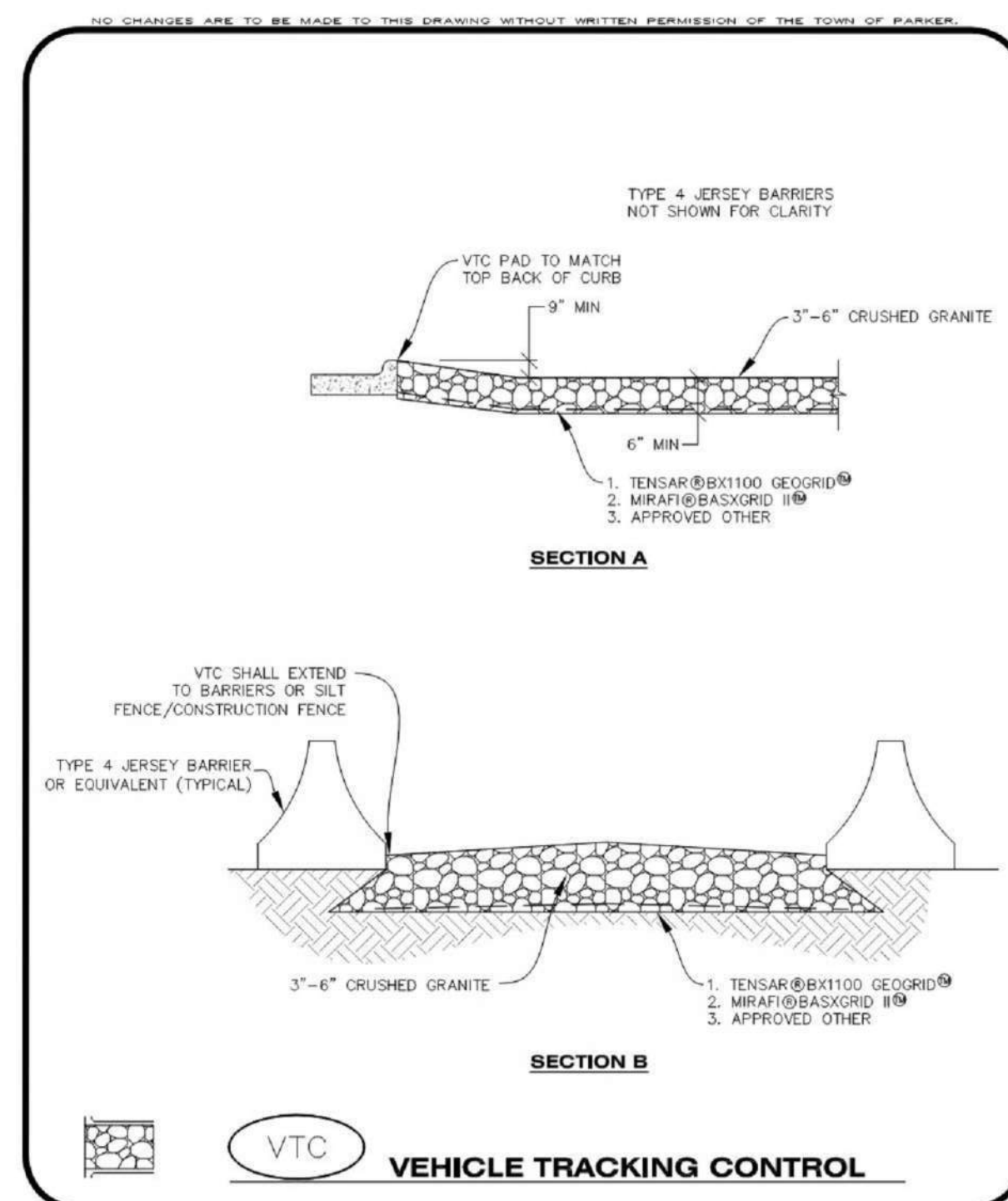
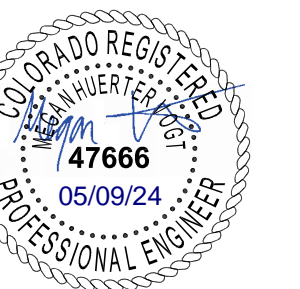
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3
OCT. 2013



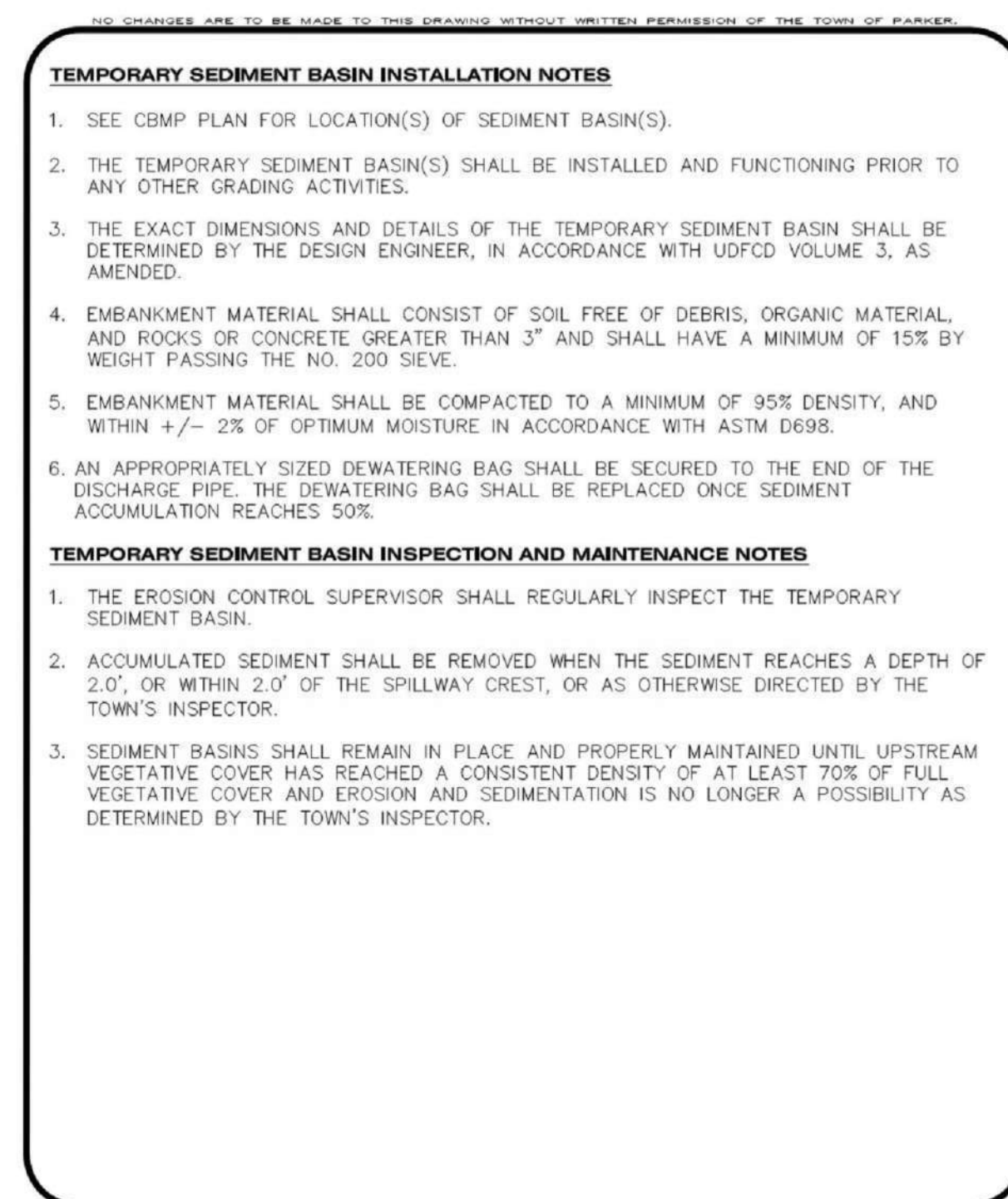
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3
OCT. 2013



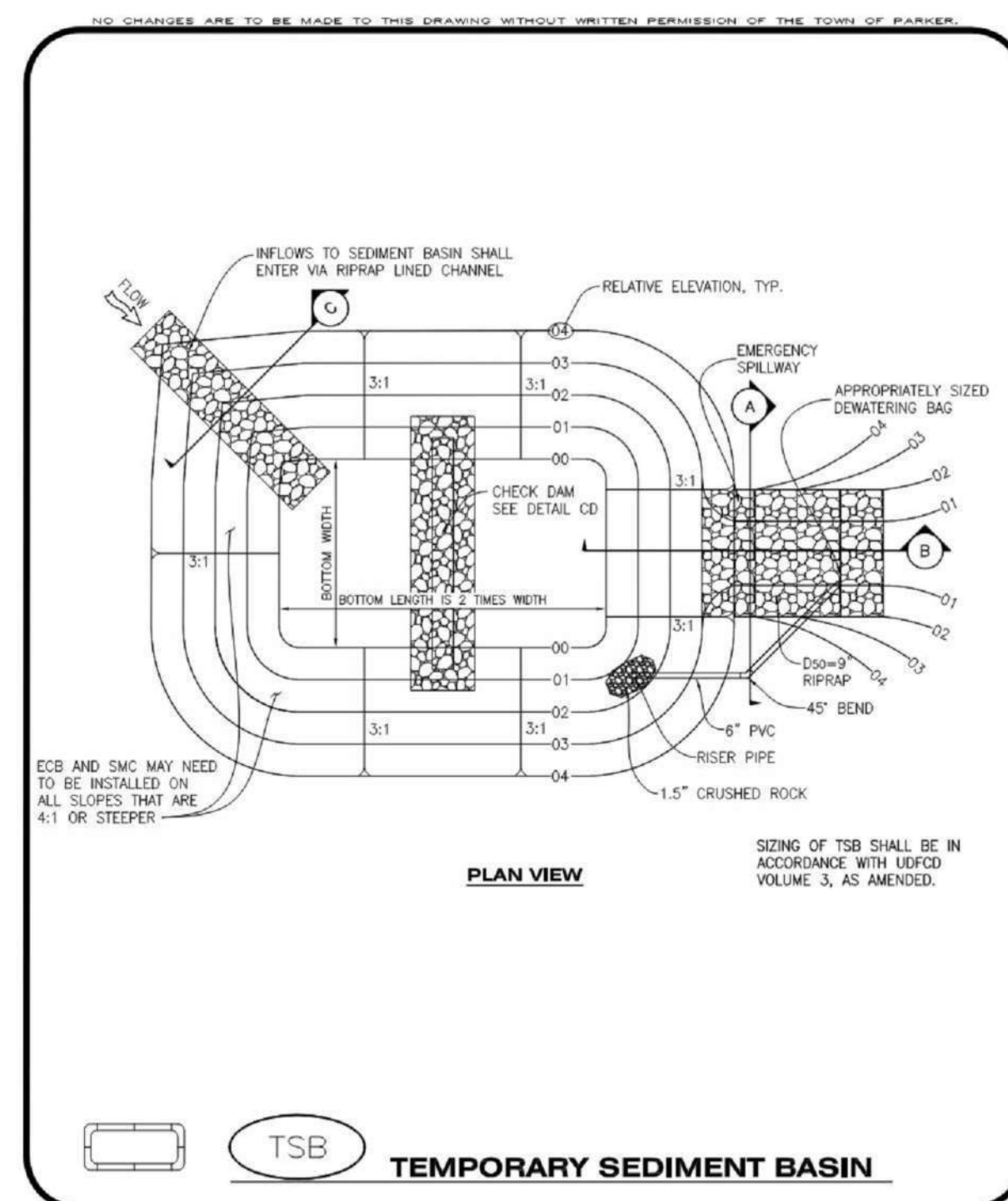
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CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3
OCT. 2013



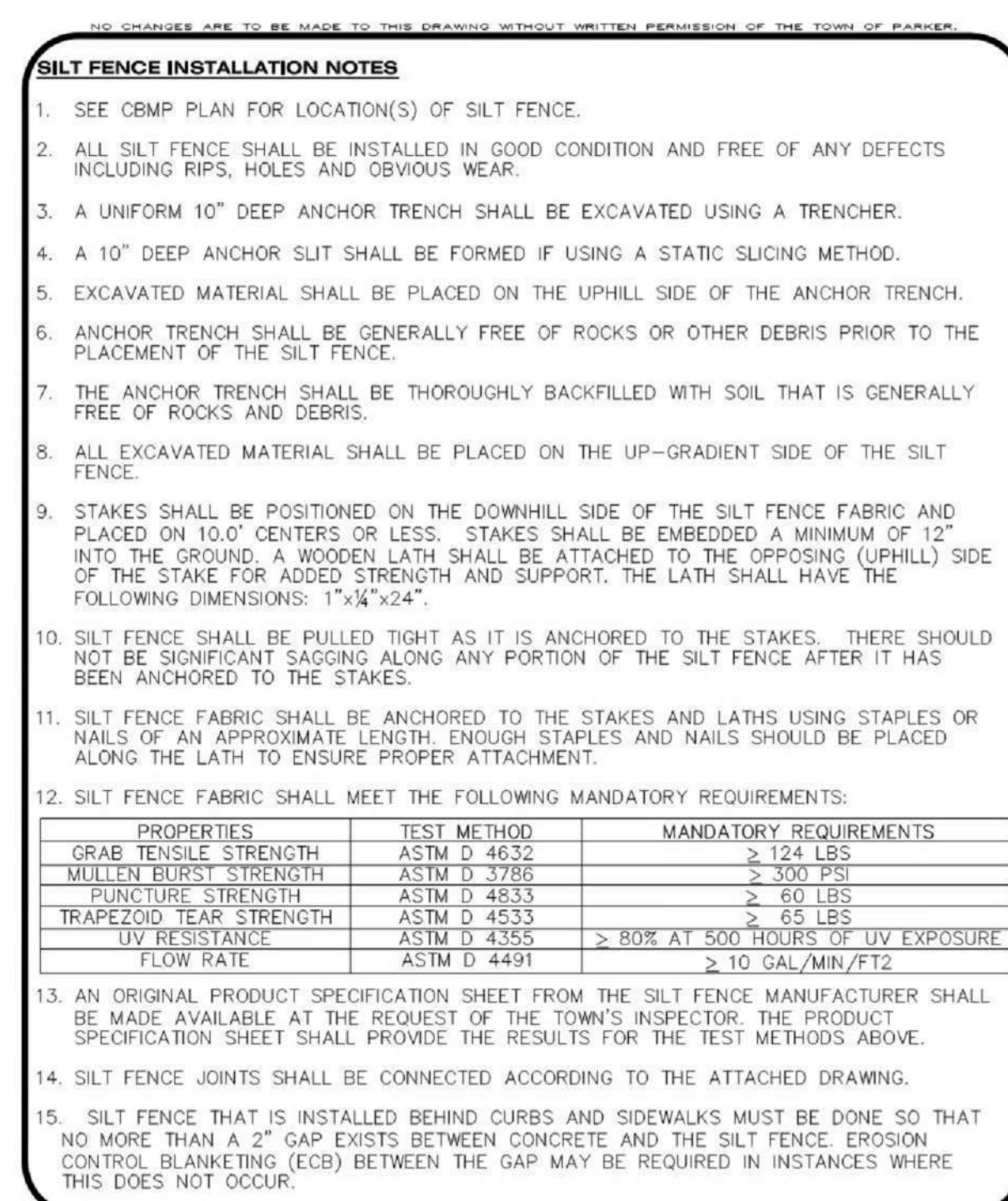
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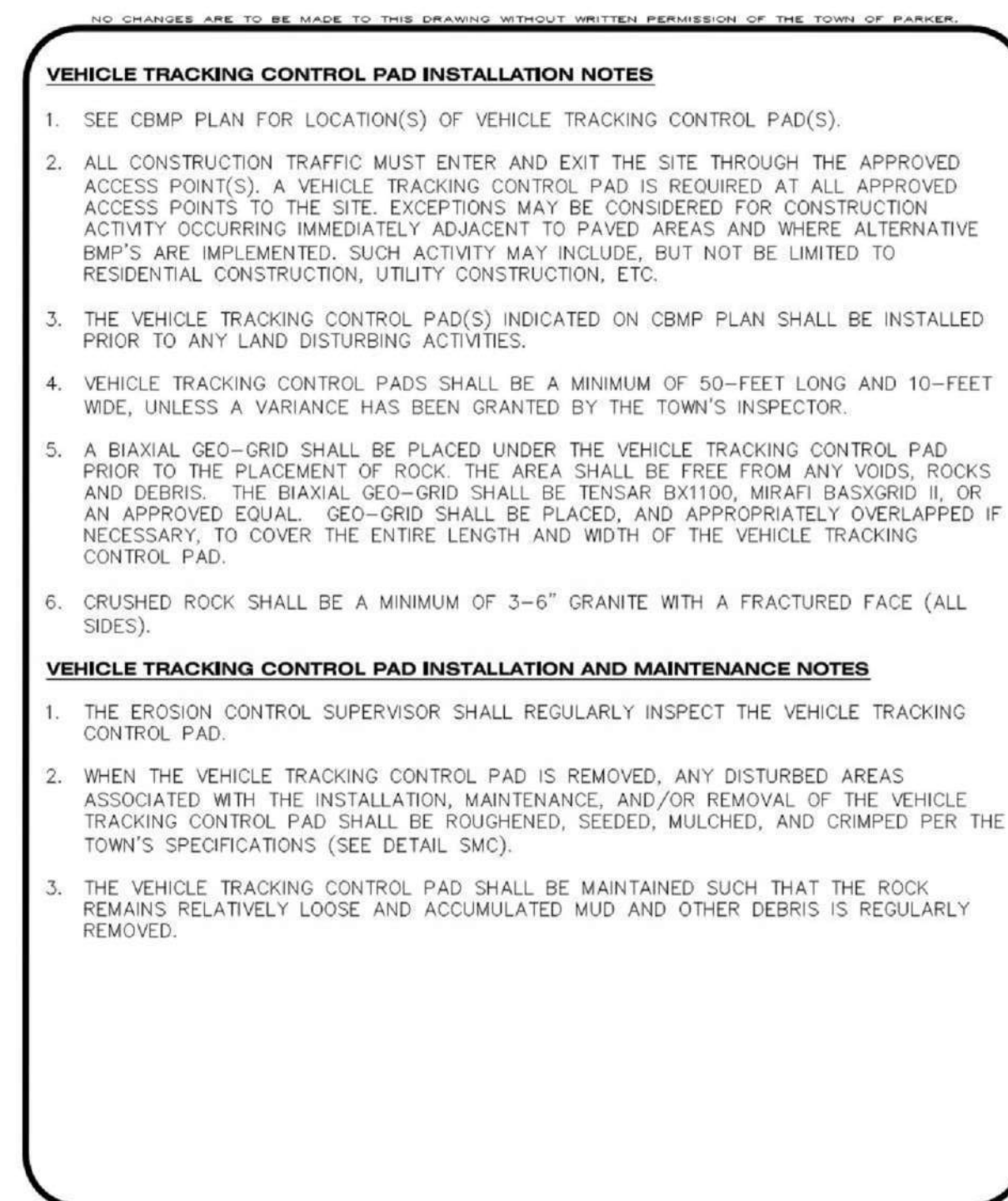
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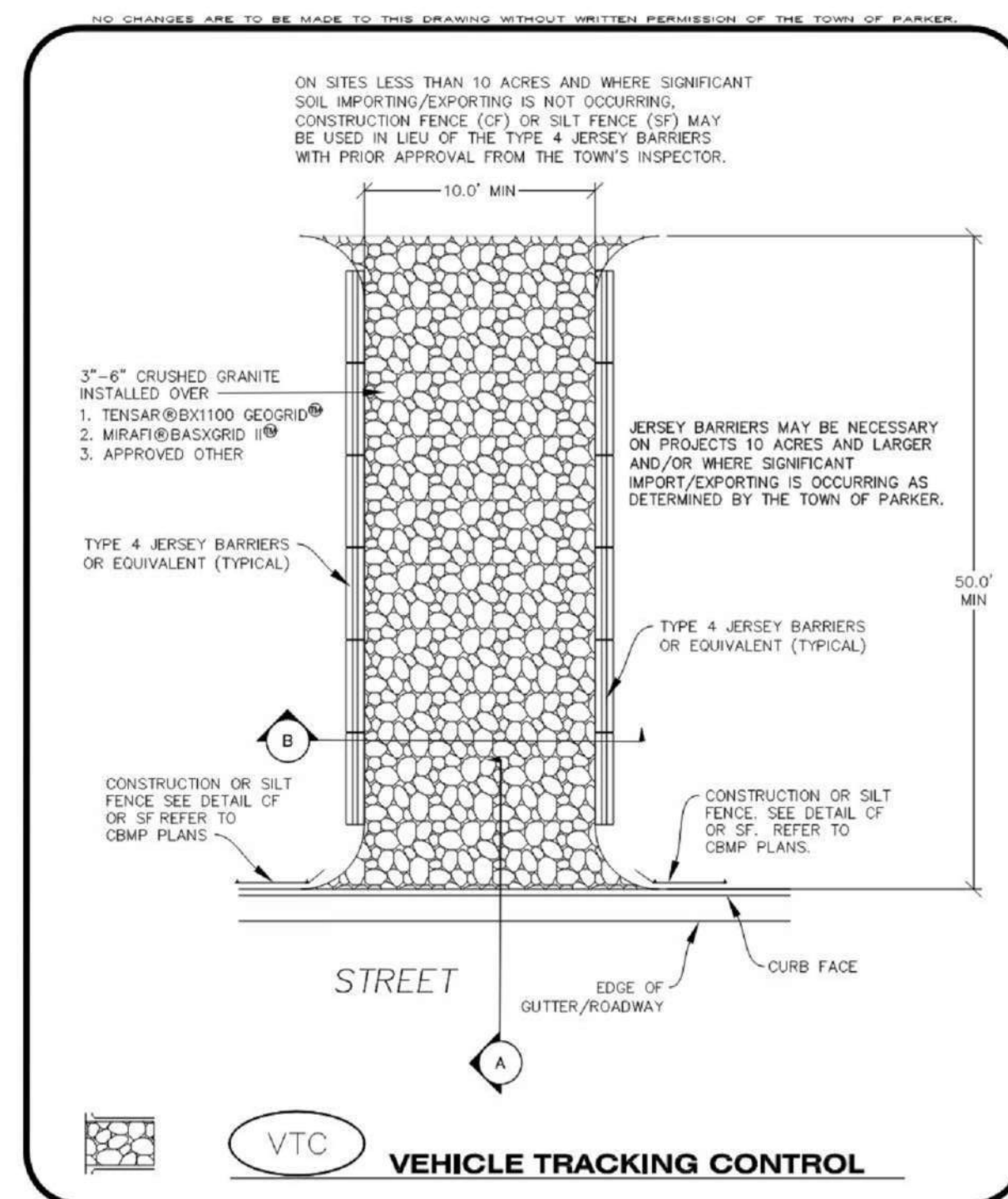
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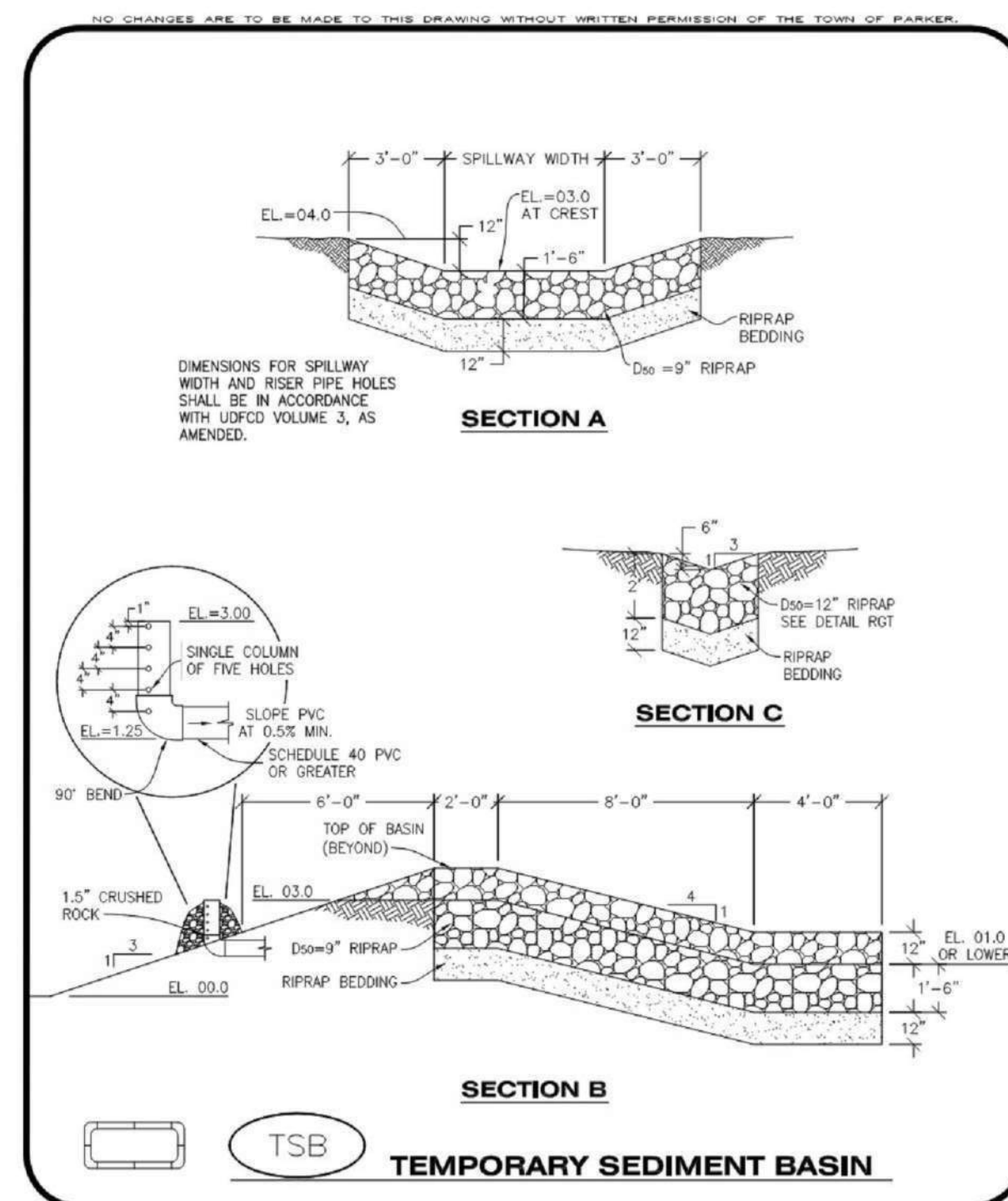
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CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES VTC 1 OF 3 04. 2013



CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES TSB 2 OF 3 04. 2013



CBMP CONSTRUCTION BEST MANAGEMENT PRACTICES SF 4 OF 4 04. 2013

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

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Parker, CO 80138

Architect

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Landscape Architect

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Telephone: 303-628-0003

Structural Engineer

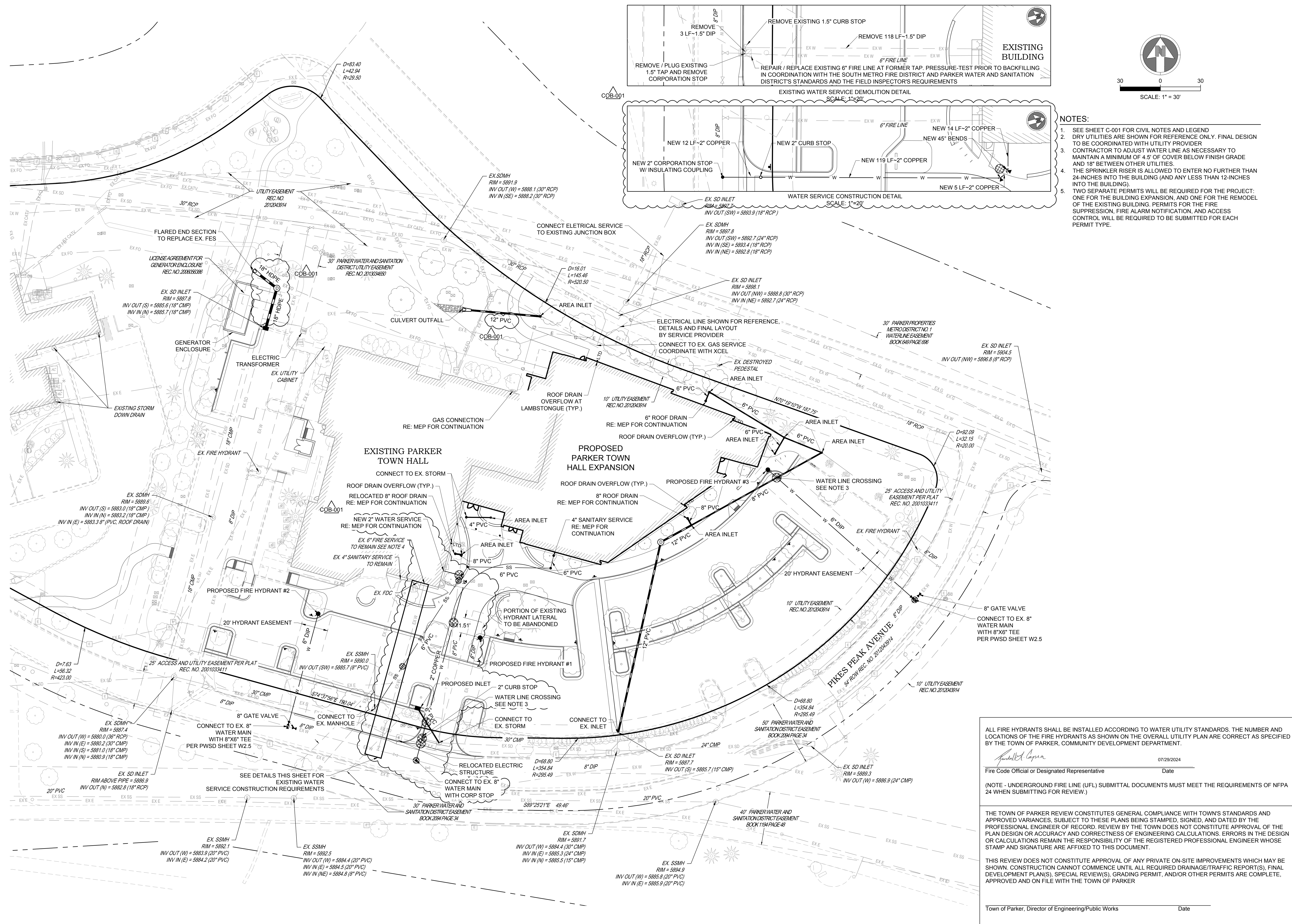
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Electrical Engineer + Technology

AE Design
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Denver, CO 80202
Telephone: 303-295-3034



ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO WATER UTILITY STANDARDS. THE NUMBER AND LOCATIONS OF THE FIRE HYDRANTS AS SHOWN ON THE OVERALL UTILITY PLAN ARE CORRECT AS SPECIFIED BY THE TOWN OF PARKER, COMMUNITY DEVELOPMENT DEPARTMENT.

Signature _____ Date 07/29/2024

Fire Code Official or Designated Representative _____ Date _____

(NOTE - UNDERGROUND FIRE LINE (UFL) SUBMITTAL DOCUMENTS MUST MEET THE REQUIREMENTS OF NFPA 24 WHEN SUBMITTING FOR REVIEW.)

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works _____ Date _____



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

COB-001 19 JULY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

UTILITY PLAN

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

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Parker, CO 80138

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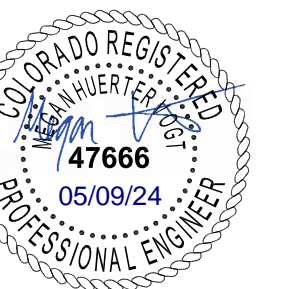
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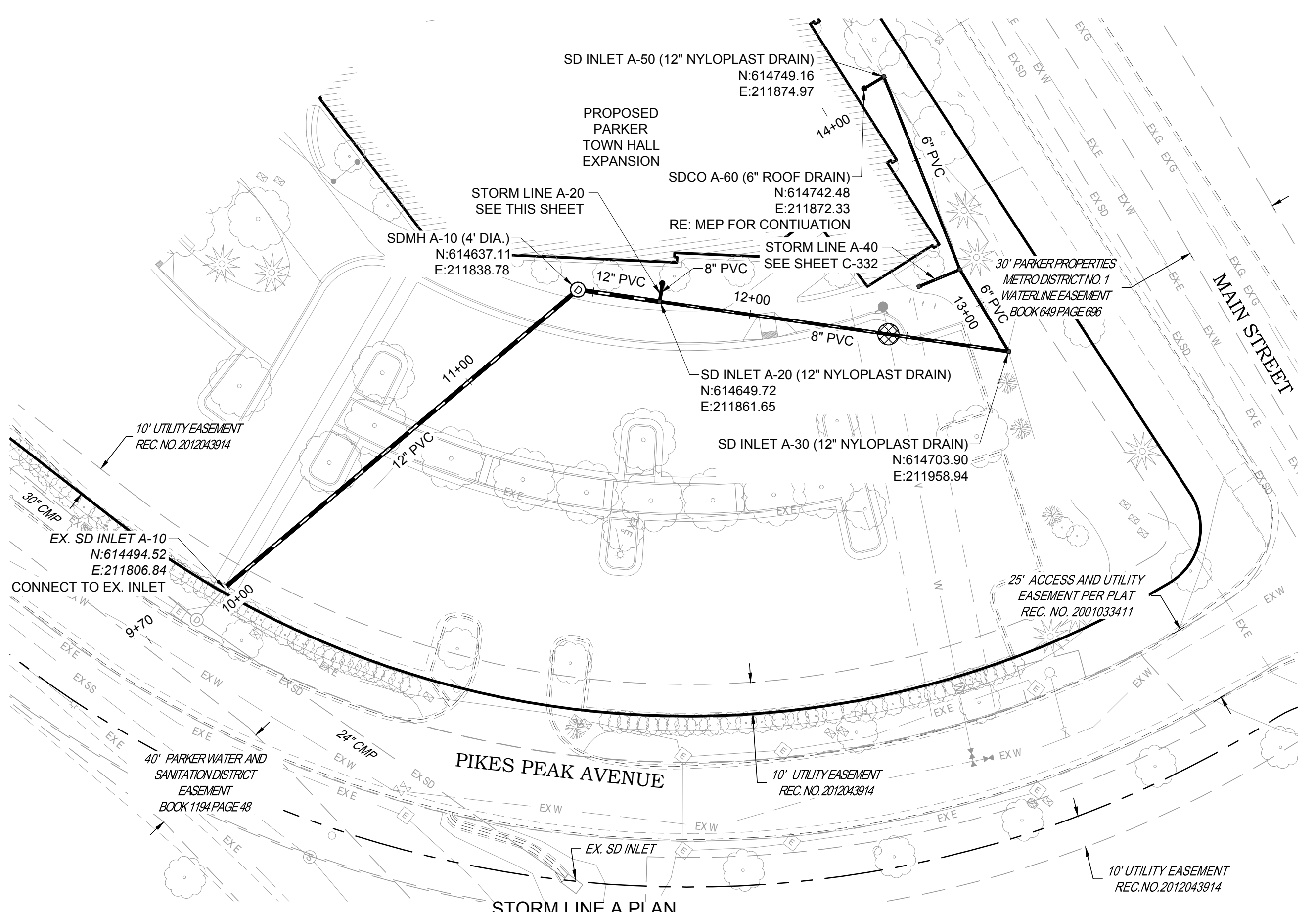
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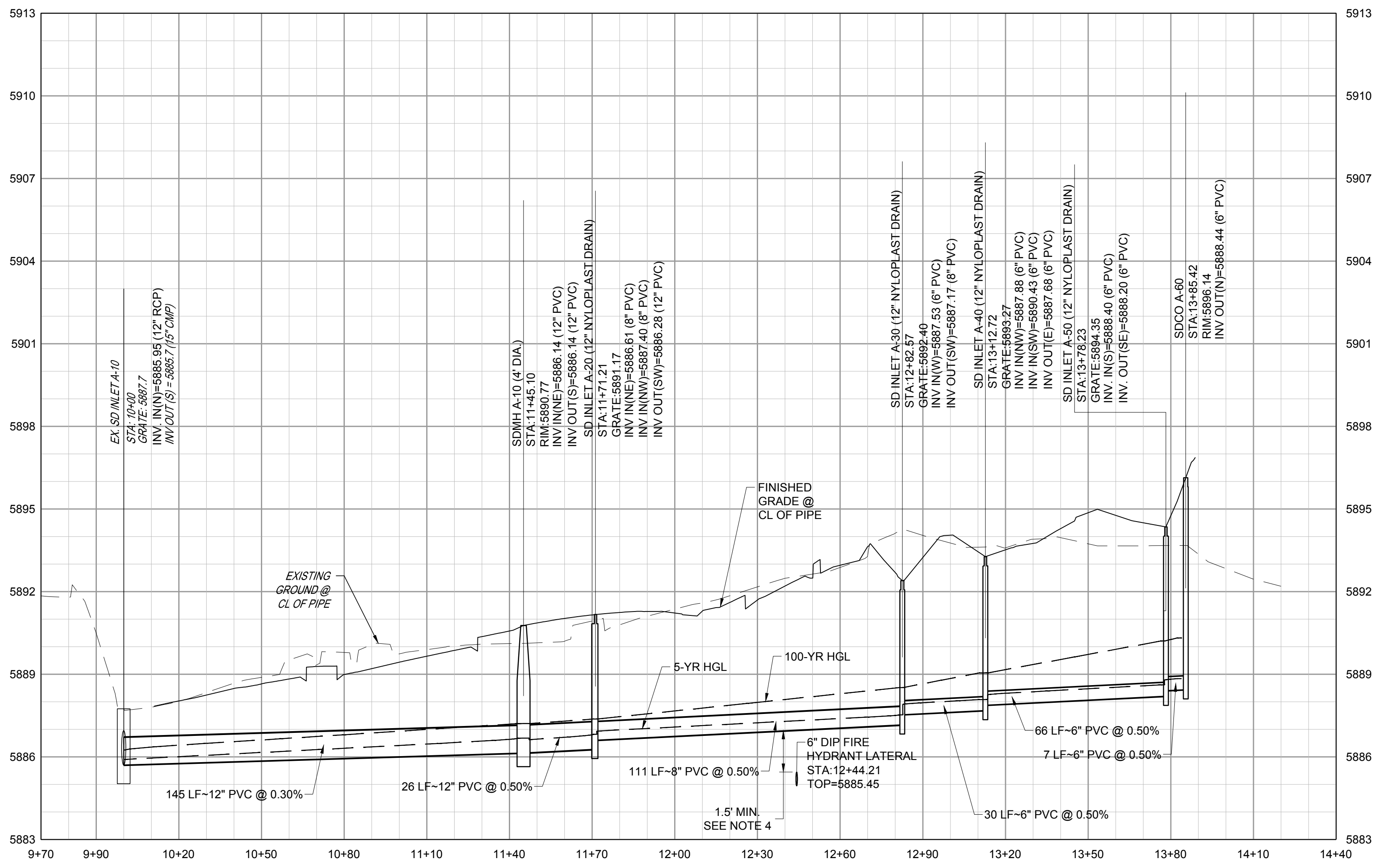
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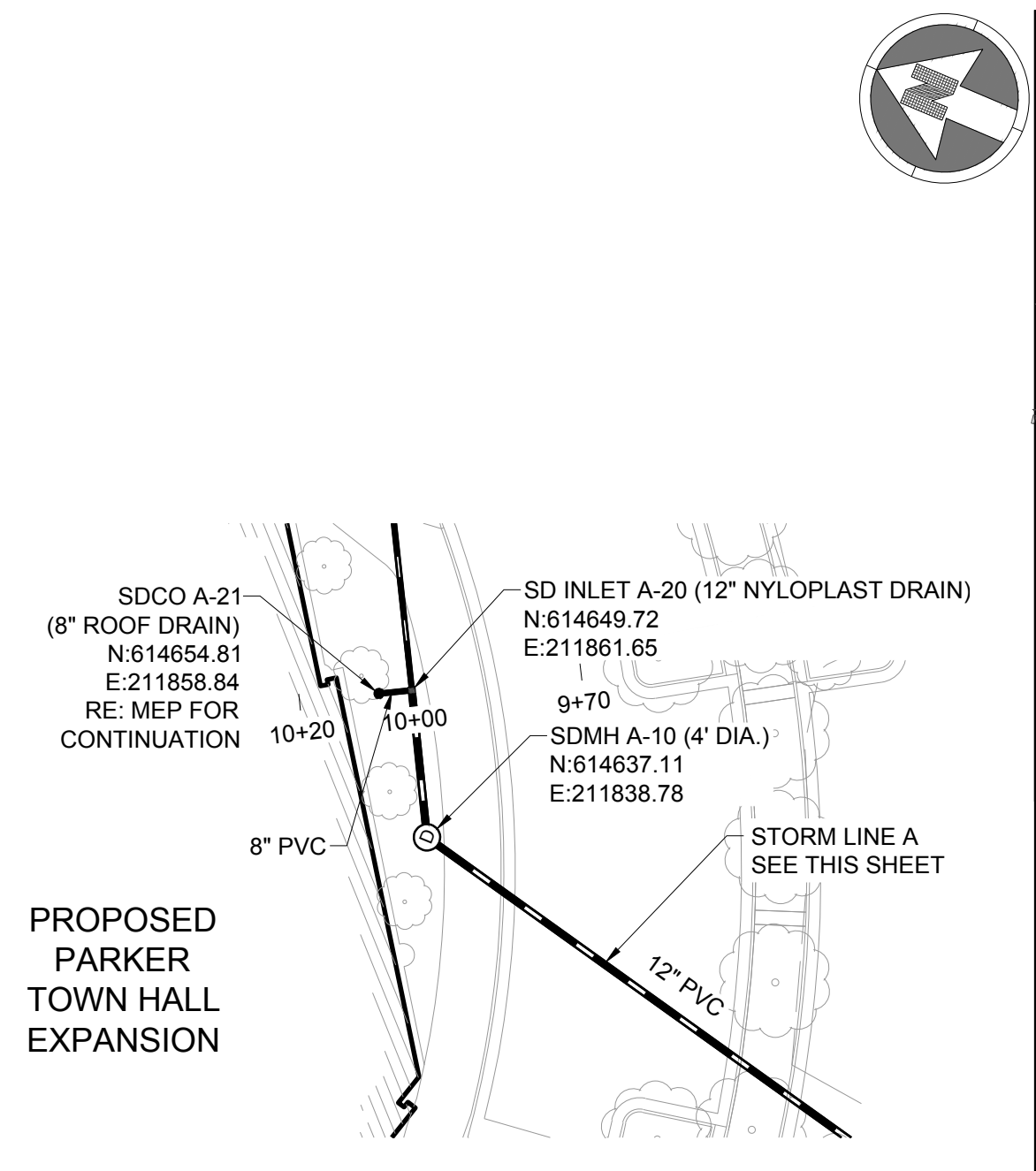


STORM LINE A PLAN

VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'

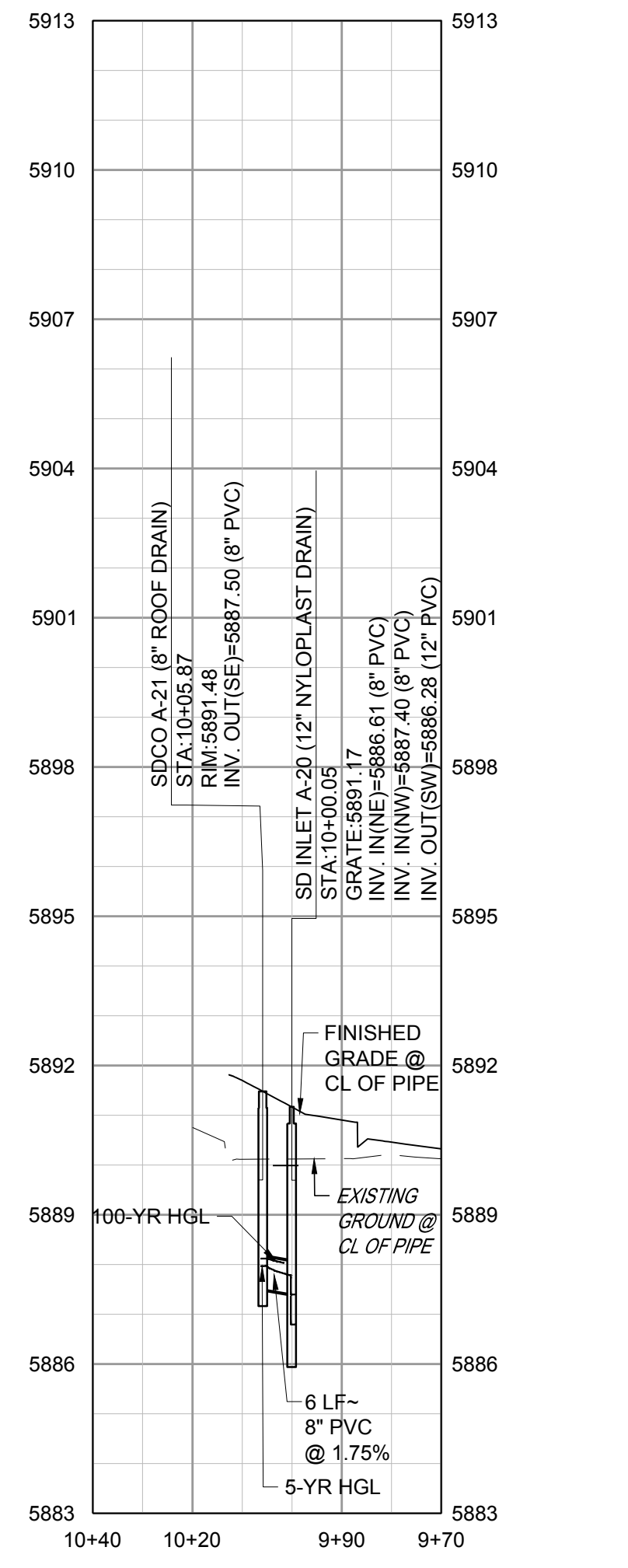


STORM LINE A PROFILE

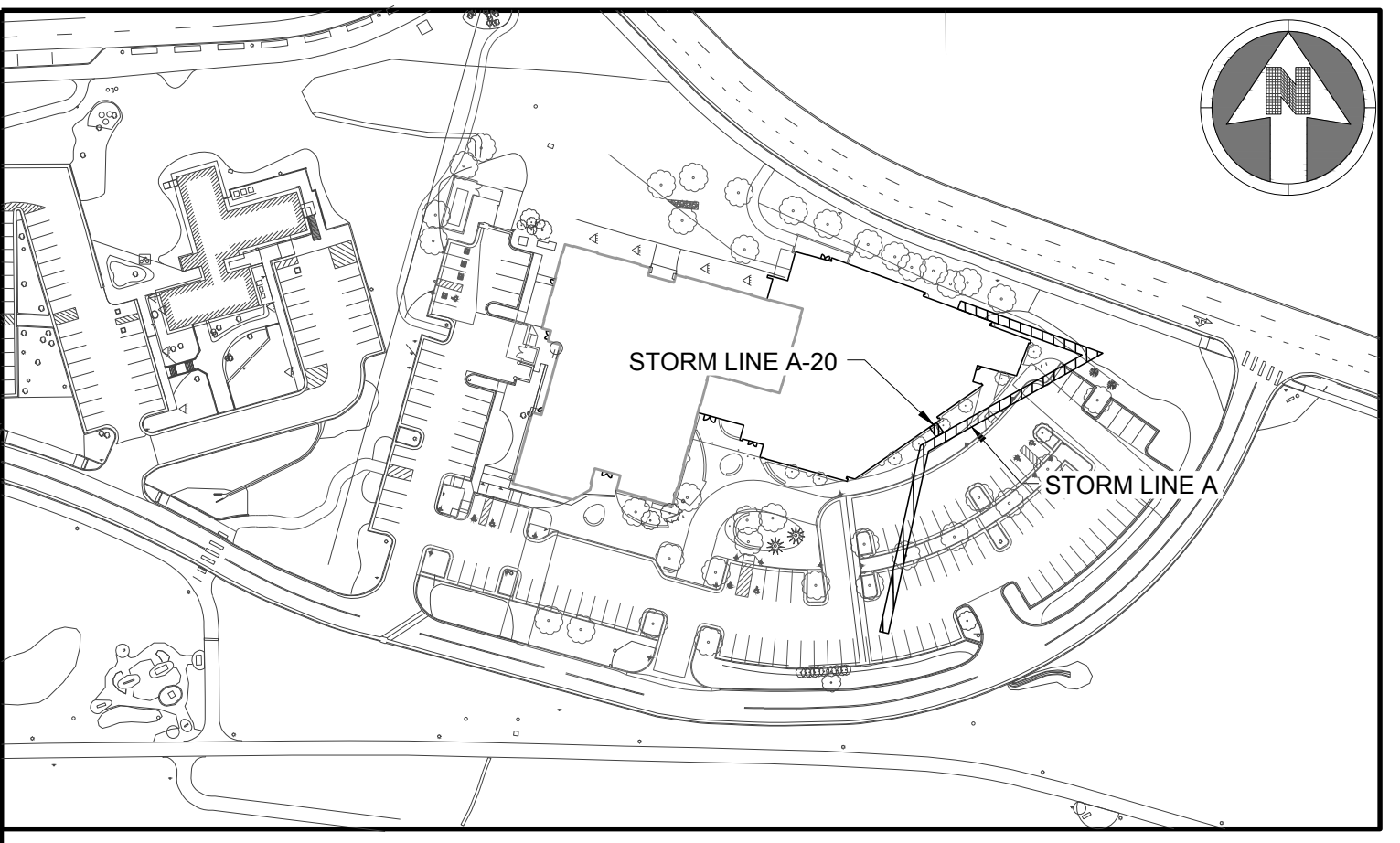


STORM LINE A-20 PLAN

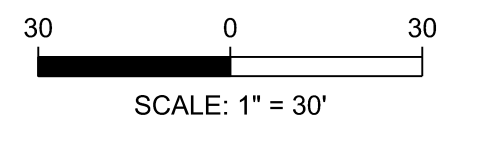
VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'



STORM LINE A-20 PROFILE



KEYMAP



NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- ALL EXISTING DRY UTILITIES ARE ASSUMED TO BE 3' DEEP TO TOP OF PIPE.
- ALL EXISTING WATER LINES ARE ASSUMED TO BE 4.5' DEEP TO TOP OF PIPE, UNLESS OTHERWISE NOTED.
- CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
- THE CONTRACTOR SHALL POTHOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY.
- MATCH EXISTING FES (FLARED END SECTION) INVERT, EXISTING GROUND AND FES PIPE INVERT. FIELD VERIFY INVERT AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY. ENSURE FES REMAINS AS LOW POINT FOR EXISTING DRAINAGE SWALES.

LEGEND:

UTILITY CROSSING

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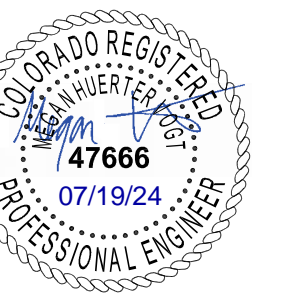
KL&A, Inc.
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Telephone: 303-384-9910

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2525 S. Wadsworth Blvd. #200
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Telephone: 303-988-4514

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Denver, CO 80202
Telephone: 303-296-3034



Issue Date

Schematic Design 01 MAY 2023
Design Development 20 OCT 2023
Permit / GMP SET 17 MAY 2024

COB-001 19 JULY 2024

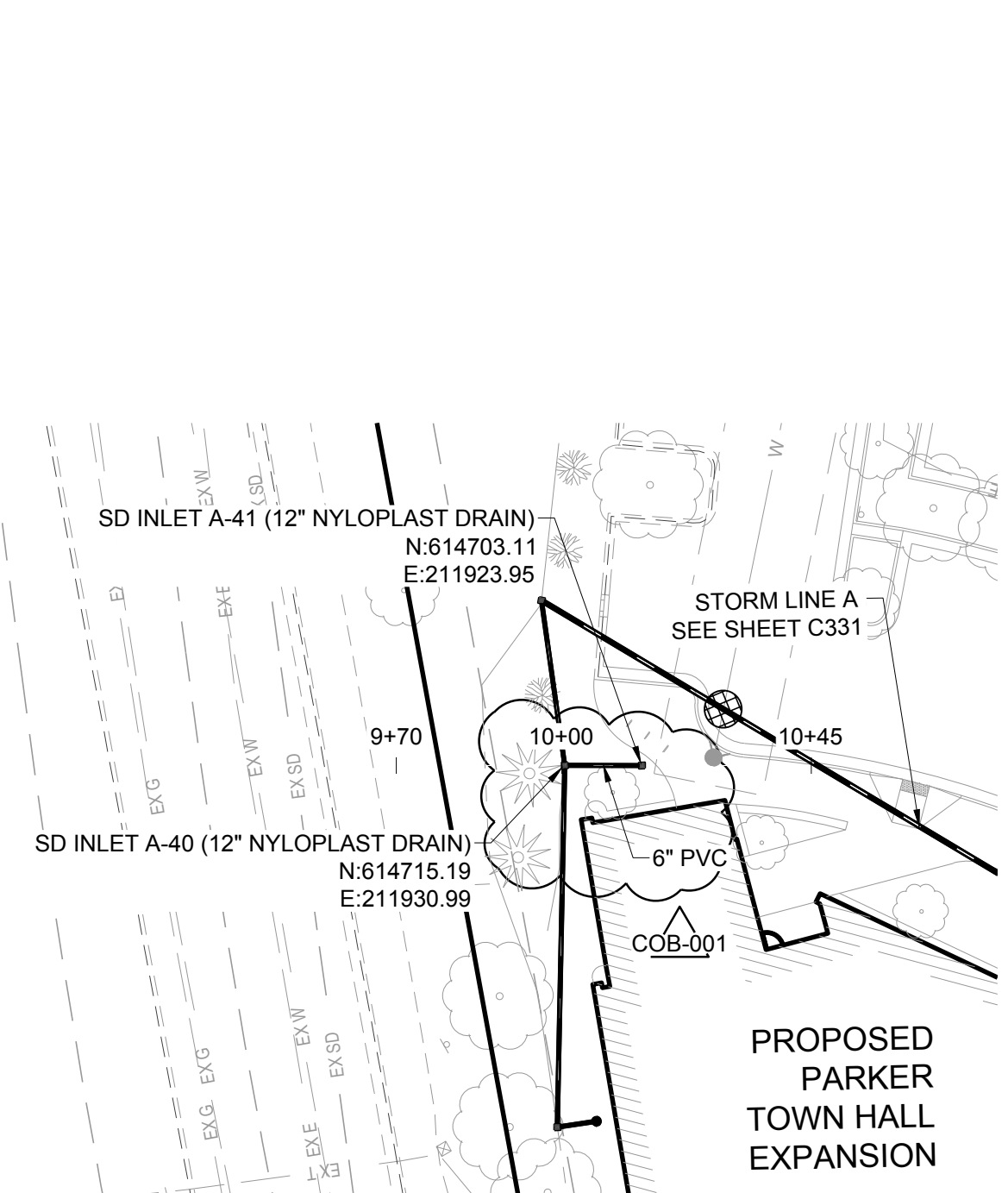
Project Number: 22-139

Drawn By:

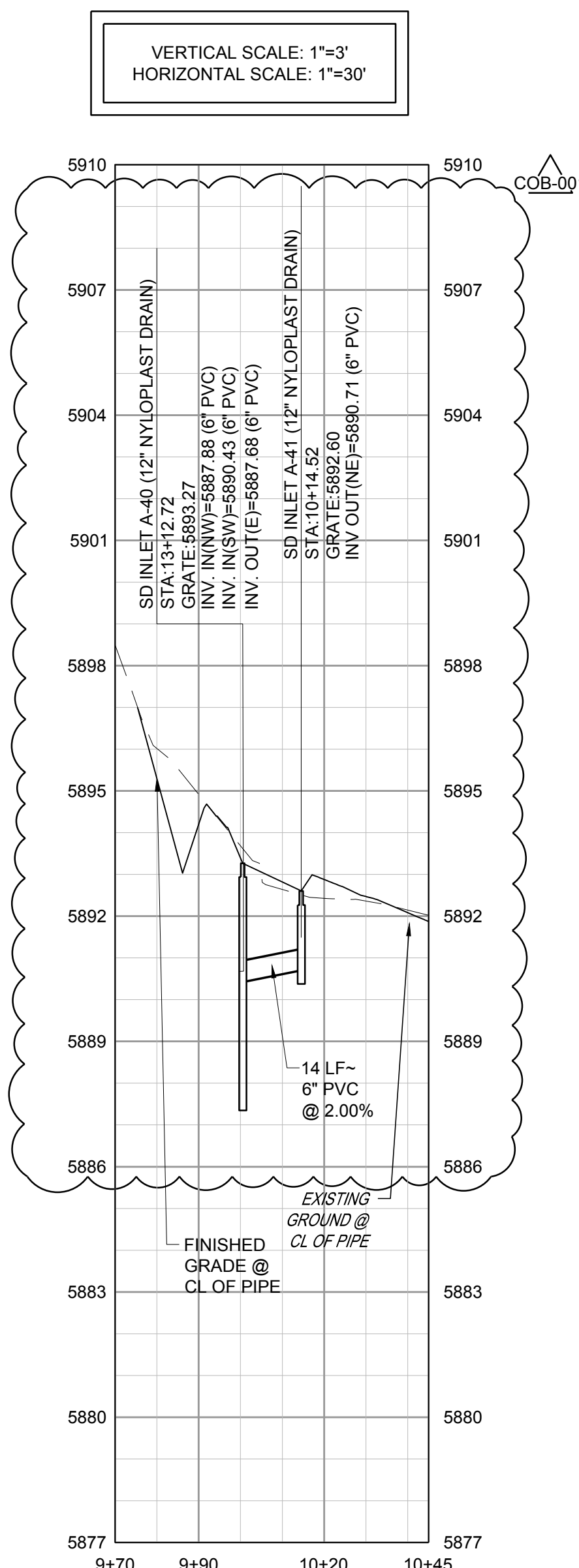
Reviewed By:

Approved By:

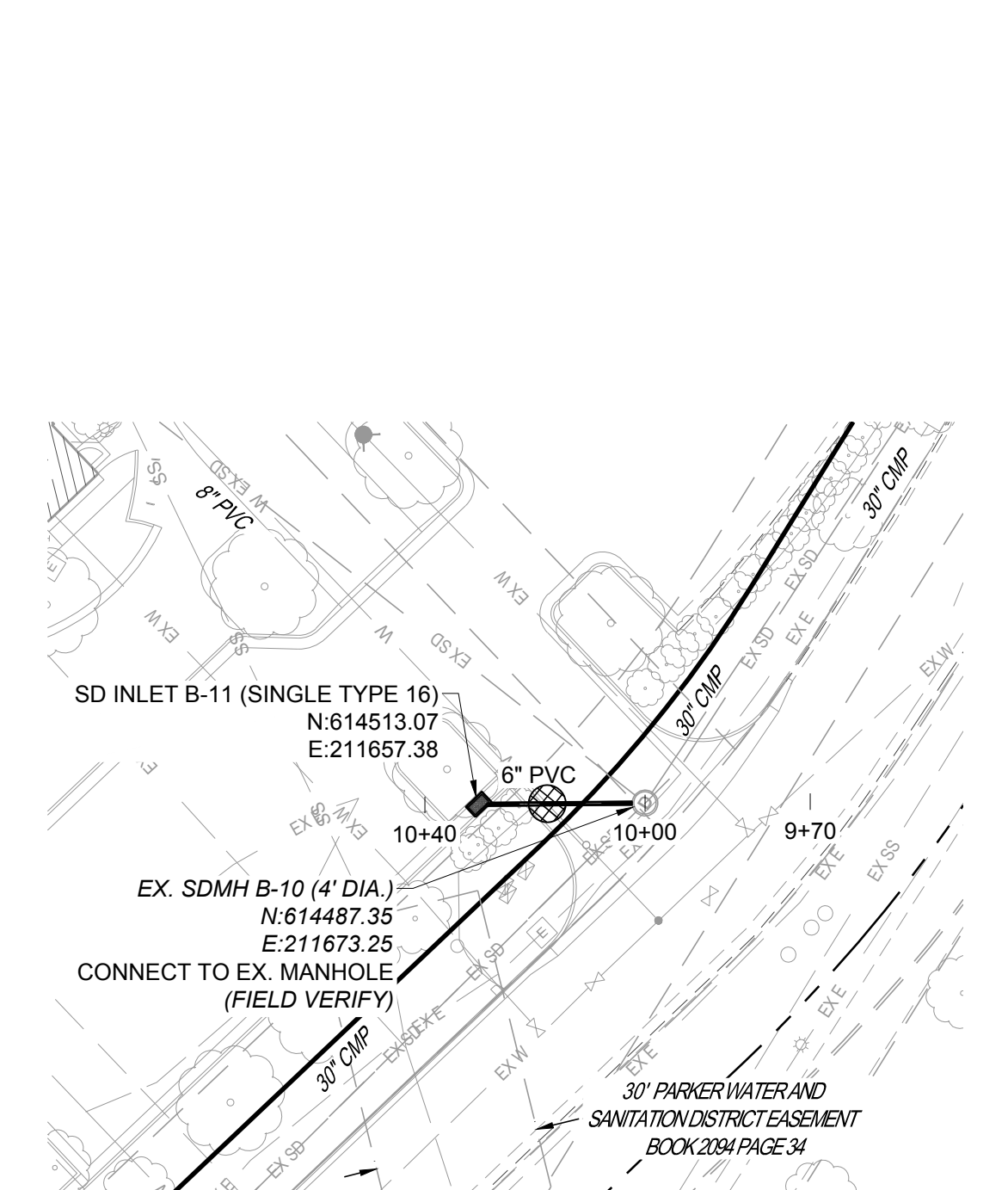
STORM PLAN AND PROFILES



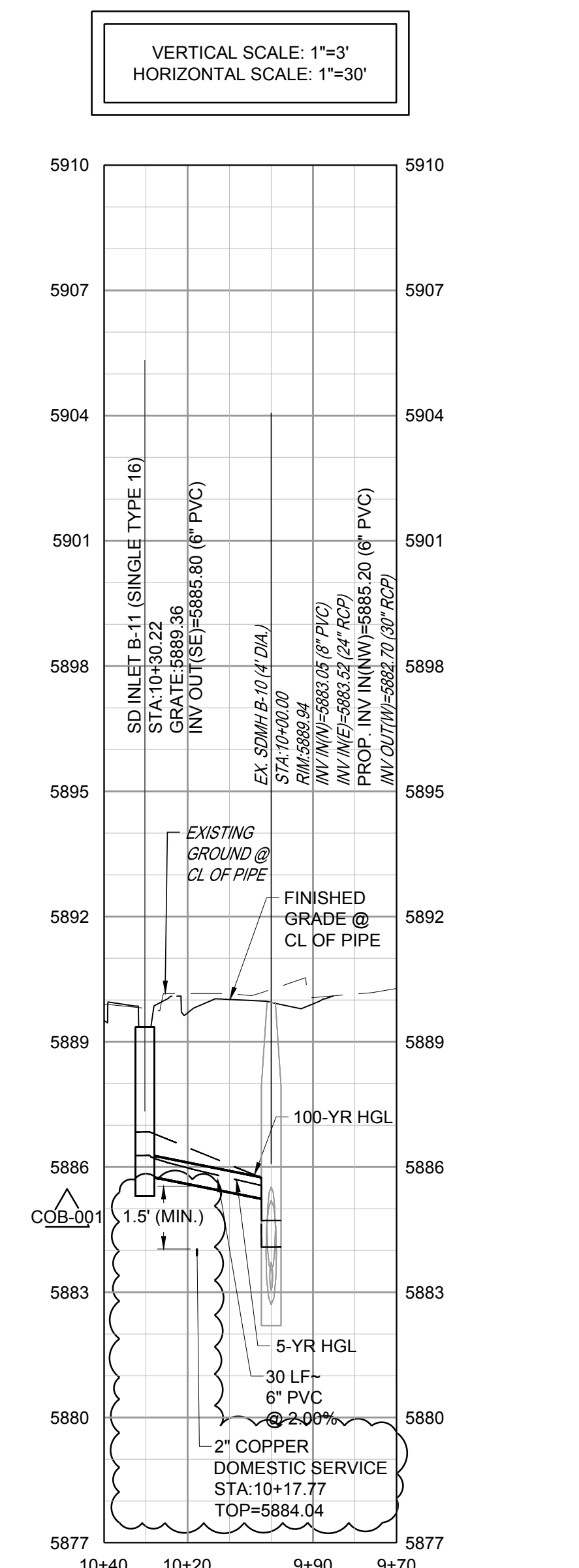
STORM LINE A-40 PLAN



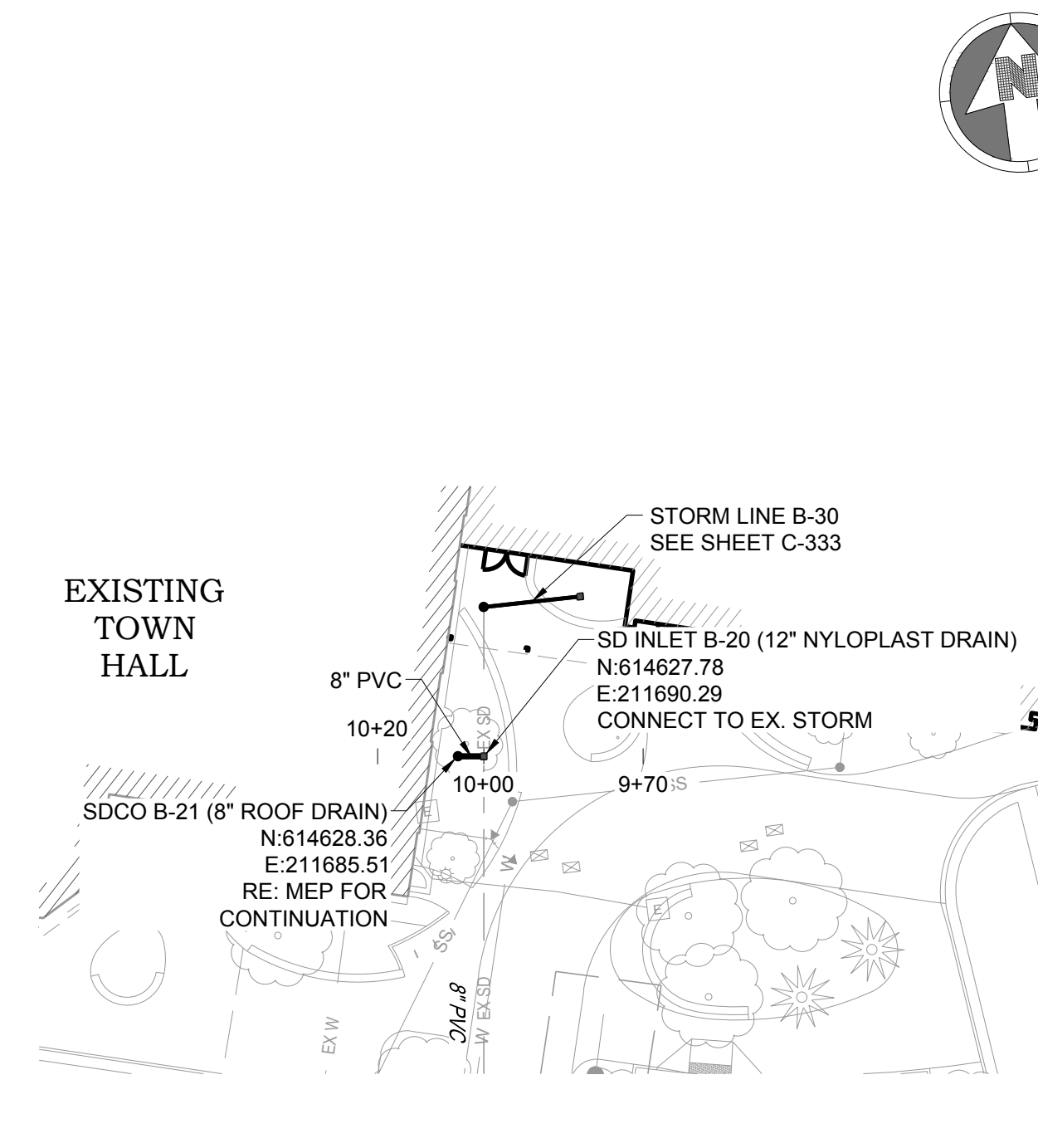
STORM LINE A-40 PROFILE



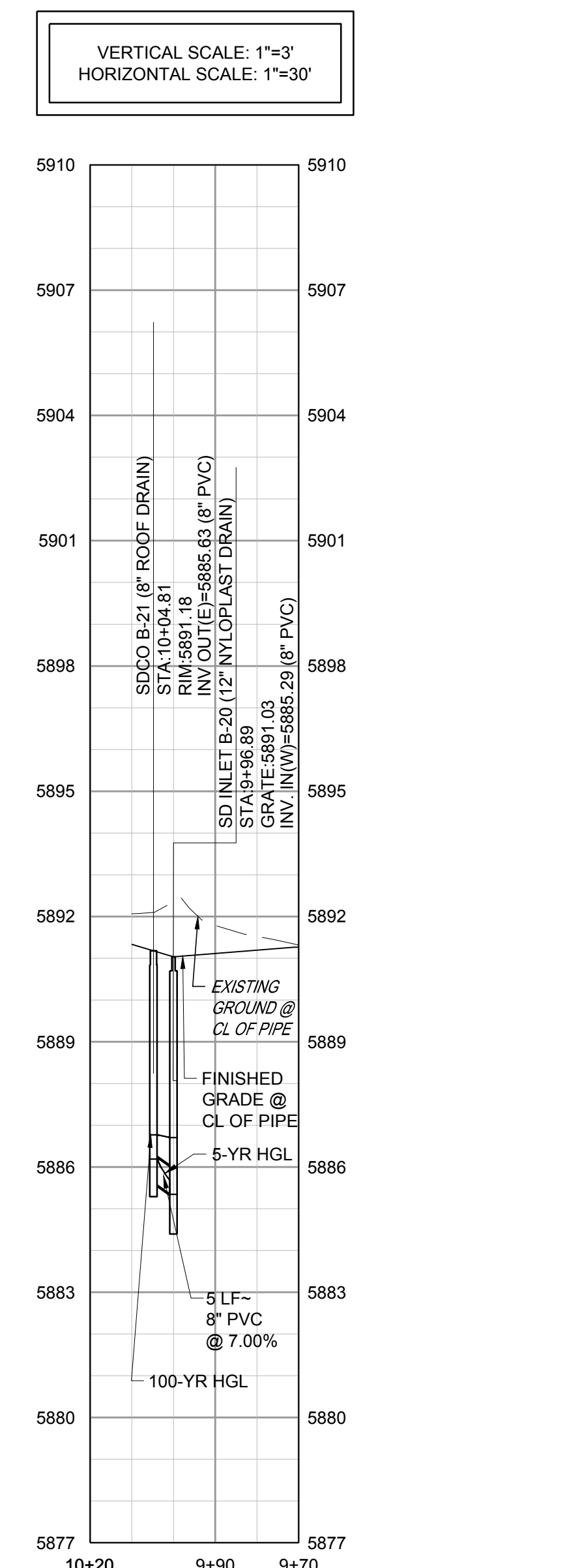
STORM LINE B-10 PLAN



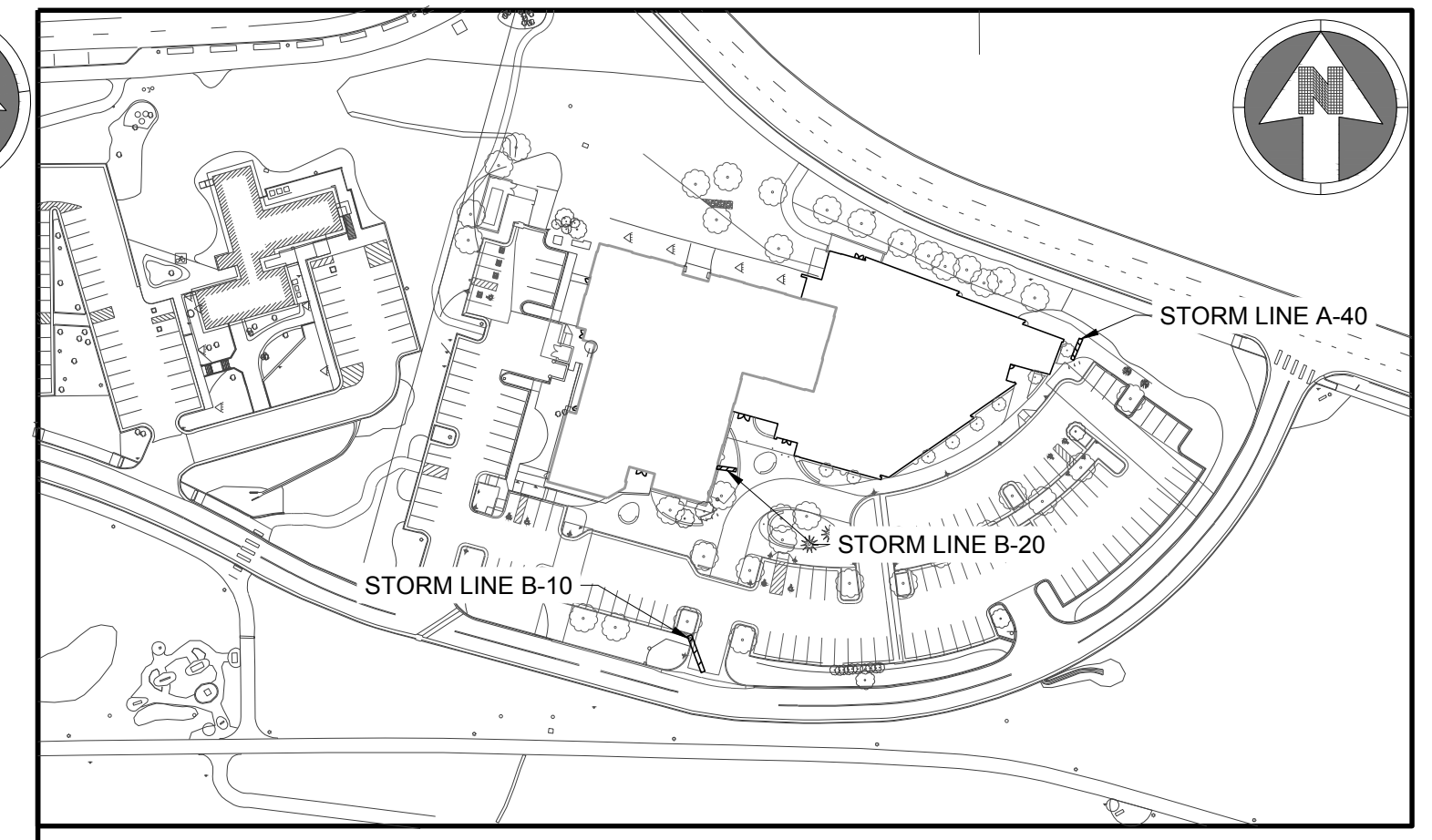
STORM LINE B-10 PROFILE



STORM LINE B-20 PLAN



STORM LINE B-20 PROFILE



NOTES:

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LEGEND:

UTILITY CROSSING

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Town of Parker, Director of Engineering/Public Works Date

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80211
Telephone: 303-294-9448

Civil Engineer

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Issue	Date
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DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

COB-001 19 JULY 2024

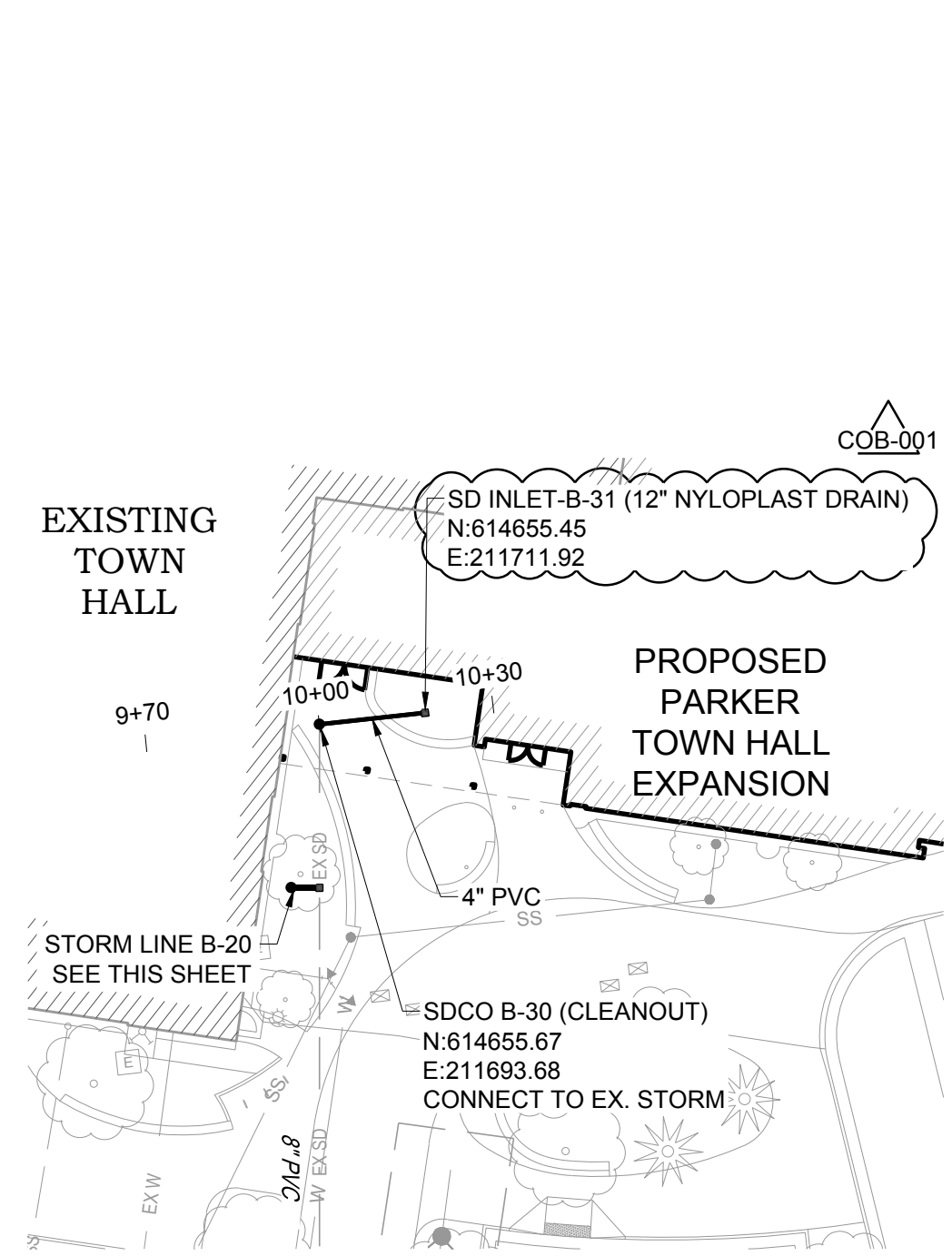
Project Number: 22-139

Drawn By:

Reviewed By:

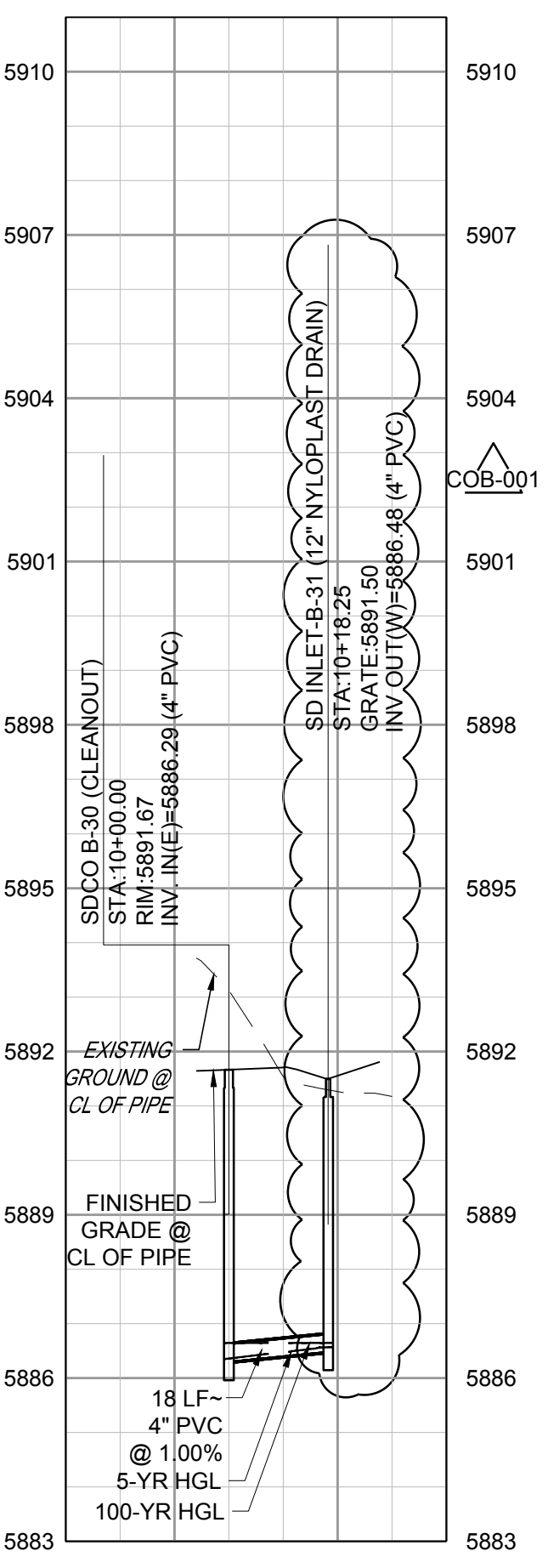
Approved By:

STORM PLAN AND PROFILES

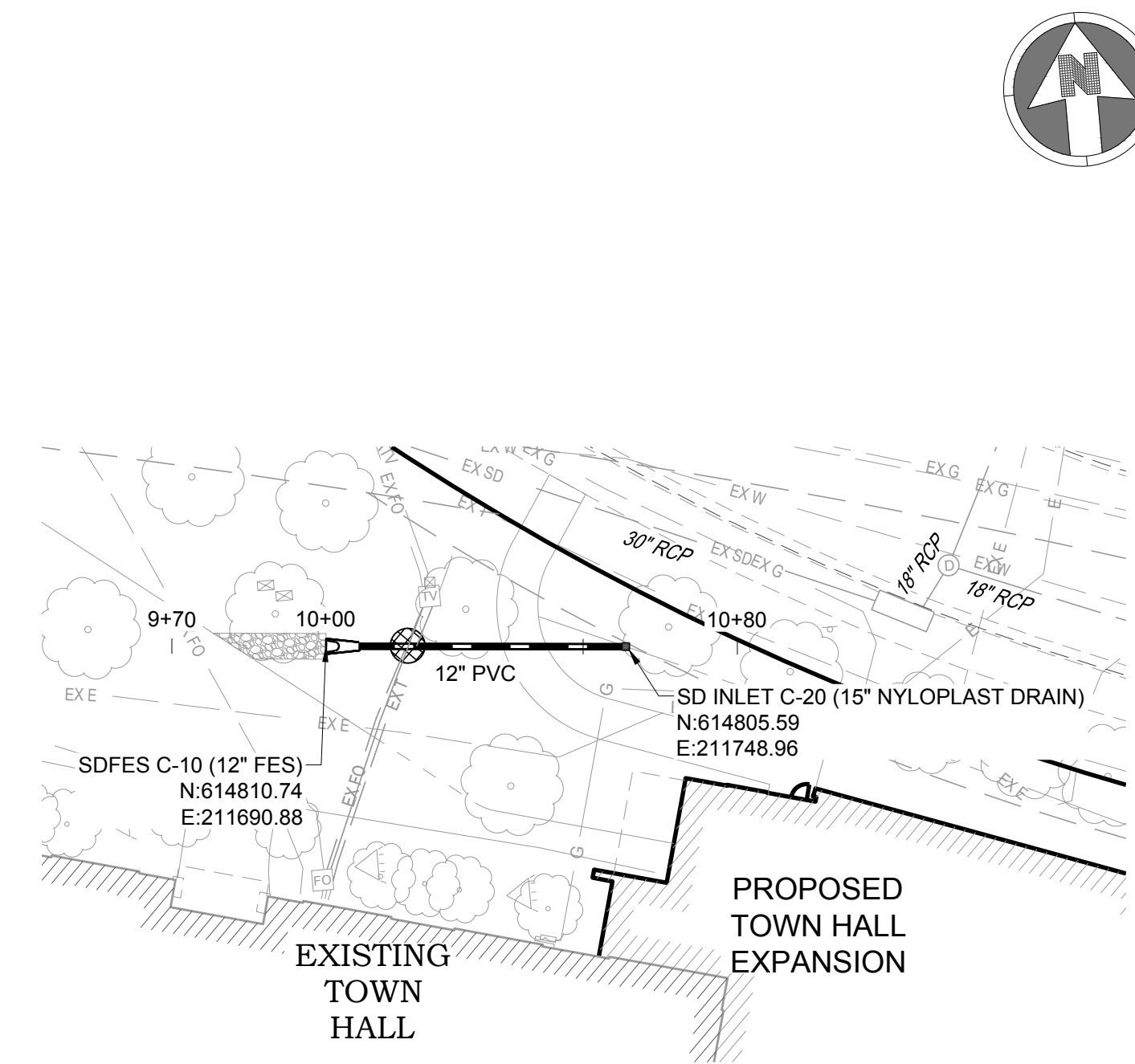


STORM LINE B-30 PLAN

VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'

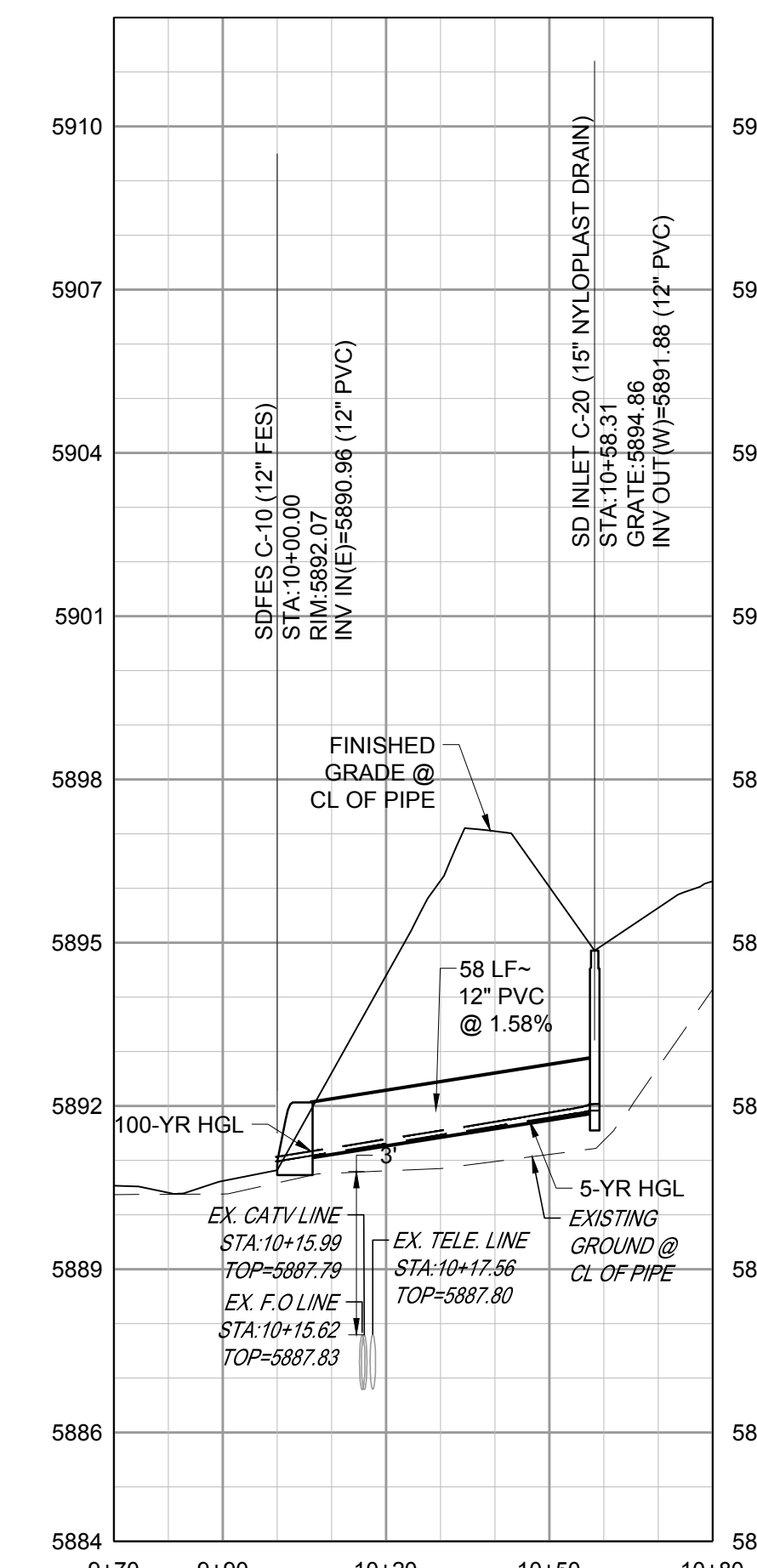


STORM LINE B-30 PROFILE

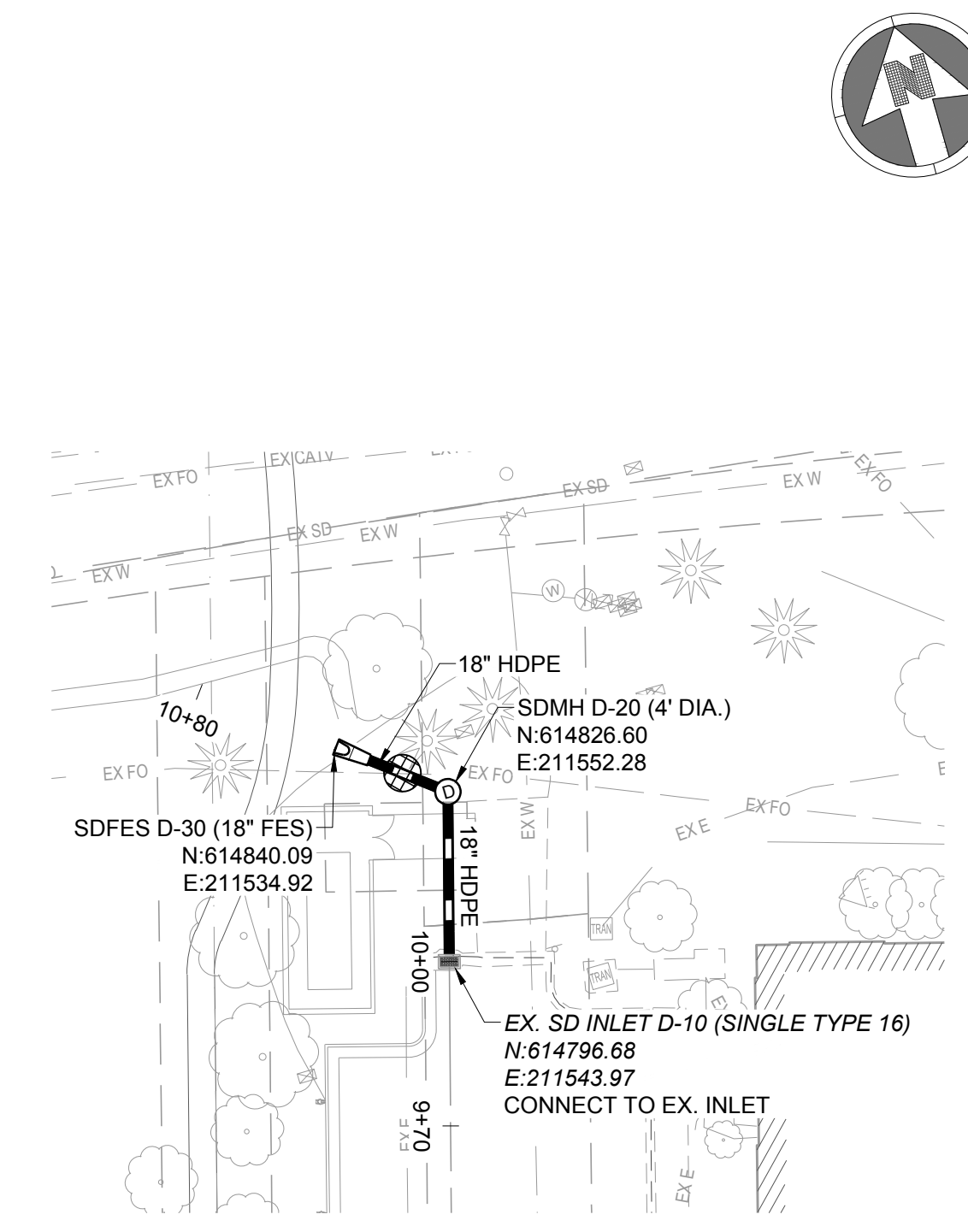


STORM LINE C PLAN

VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'

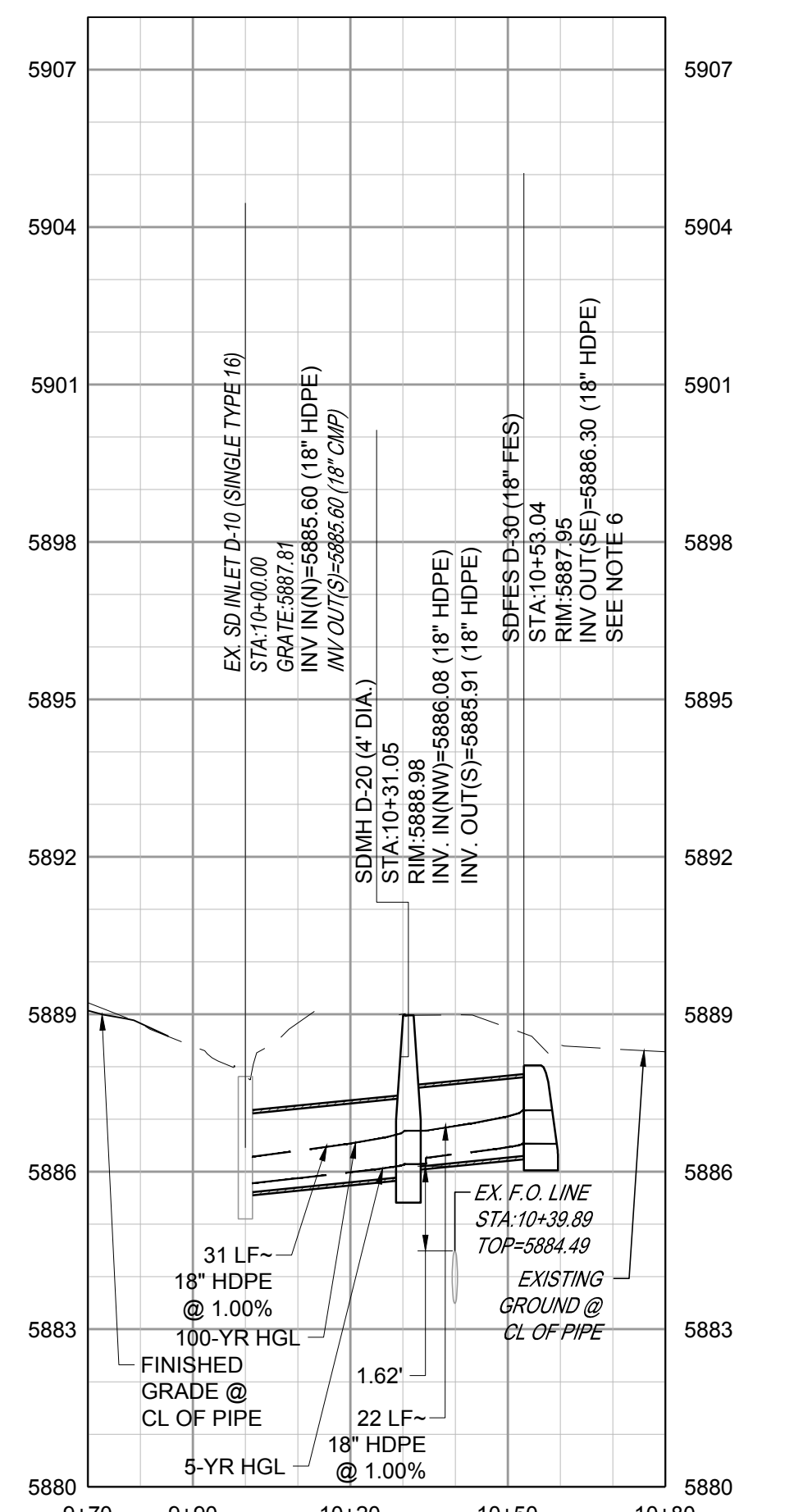


STORM LINE C PROFILE

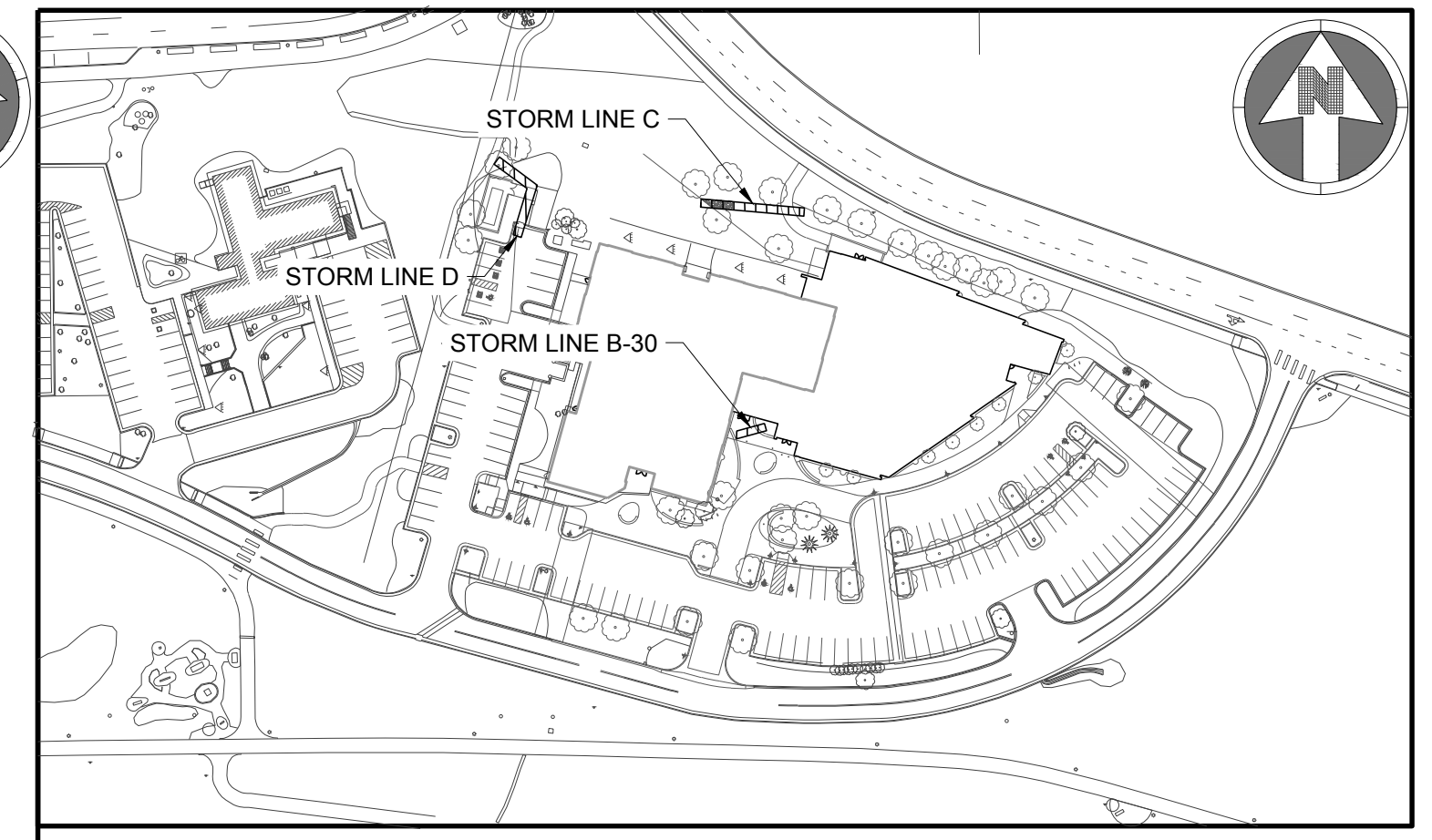


STORM LINE D PLAN

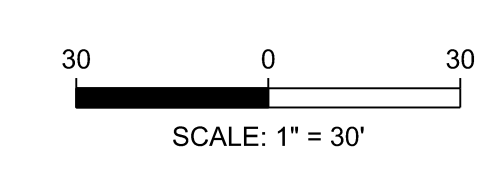
VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'



STORM LINE D PROFILE



KEYMAP



NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
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LEGEND:

UTILITY CROSSING

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Parker, CO
22-093

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Parker, CO 80138

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Denver, CO 80211
Telephone: 303-294-9448

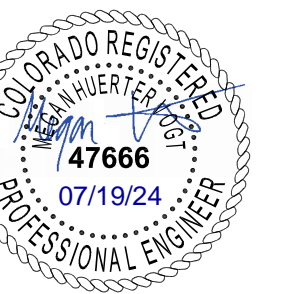
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Telephone: 303-741-3737

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Telephone: 303-296-3034

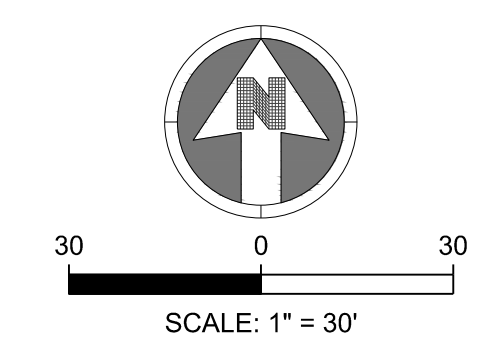
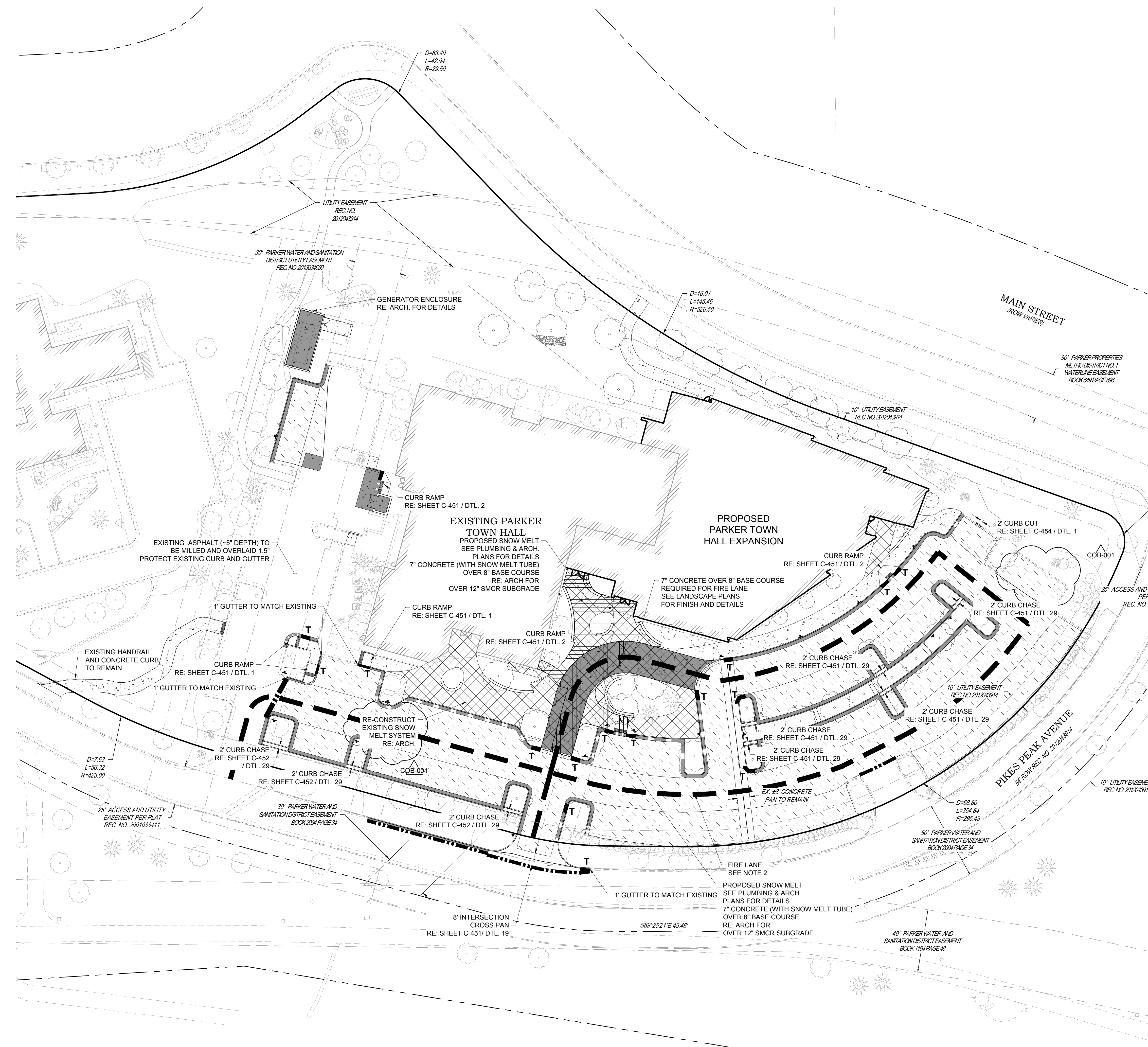


Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024
C08-001	19 JULY 2024

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

PAVING PLAN

C-411



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
 - A SITE INSPECTION BY A REPRESENTATIVE OF THE SOUTH METRO FIRE DEPARTMENT IS REQUIRED PRIOR TO DEMOLITION AND CONSTRUCTION. NO VERTICAL CONSTRUCTION ON THIS SITE WILL BE ALLOWED UNTIL SUCH TIME THAT CURB, GUTTER, AND FIRST LIFT OF ASPHALT ARE INSTALLED FOR THE FULL LENGTH OF THE FIRE LANE.
 - SMCR = SCARIFY, MOISTURE CONTENT, AND RECOMPACT PER SPECIFICATIONS. SEE SECTION 31 20 00 FOR DETAILS.

- LEGEND:**
- PROPOSED 6" CATCH CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-451 / DTL. 3
 - PROPOSED 6" SPILL CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-451 / DTL. 3
 - PROPOSED FLUSH CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 2
 - PROPOSED MOUNTABLE CURB AND GUTTER OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-451 / DTL. 3
 - PROPOSED PATCHBACK RE: SHEET C-453 / DTL. 3
 - PROPOSED CURB TRANSITION OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-454 / DTL. 3 & 4
 - PROPOSED 6" COMPOSITE ASPHALT OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 1-A
 - PROPOSED 6" CONCRETE PAVEMENT OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 1-B
 - PROPOSED 7" CONCRETE PAVEMENT OVER 8" BASE COURSE, OVER 12" SMCR SUBGRADE RE: SHEET C-453 / DTL. 1-B
 - REFER TO LANDSCAPE PLANS
 - SNOW MELT SYSTEM RE: ARCH & PLUMBING
 - ADD ALTERNATE SNOW MELT SYSTEM RE: ARCH & PLUMBING

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Town of Parker, Director of Engineering/Public Works _____ Date _____

**Parker Town Hall
Expansion**

Parker, CO
22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80211
Telephone: 303-294-9448

Civil Engineer

S.A. Miro, Inc.
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Landscape Architect

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Telephone: 303-628-0000

Structural Engineer

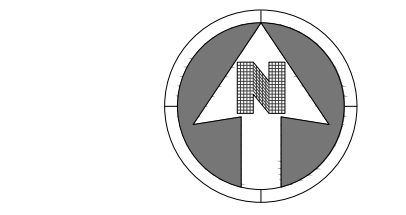
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Lakewood, CO 80227
Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
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Denver, CO 80202
Telephone: 303-296-3034



SCALE: 1" = 30'

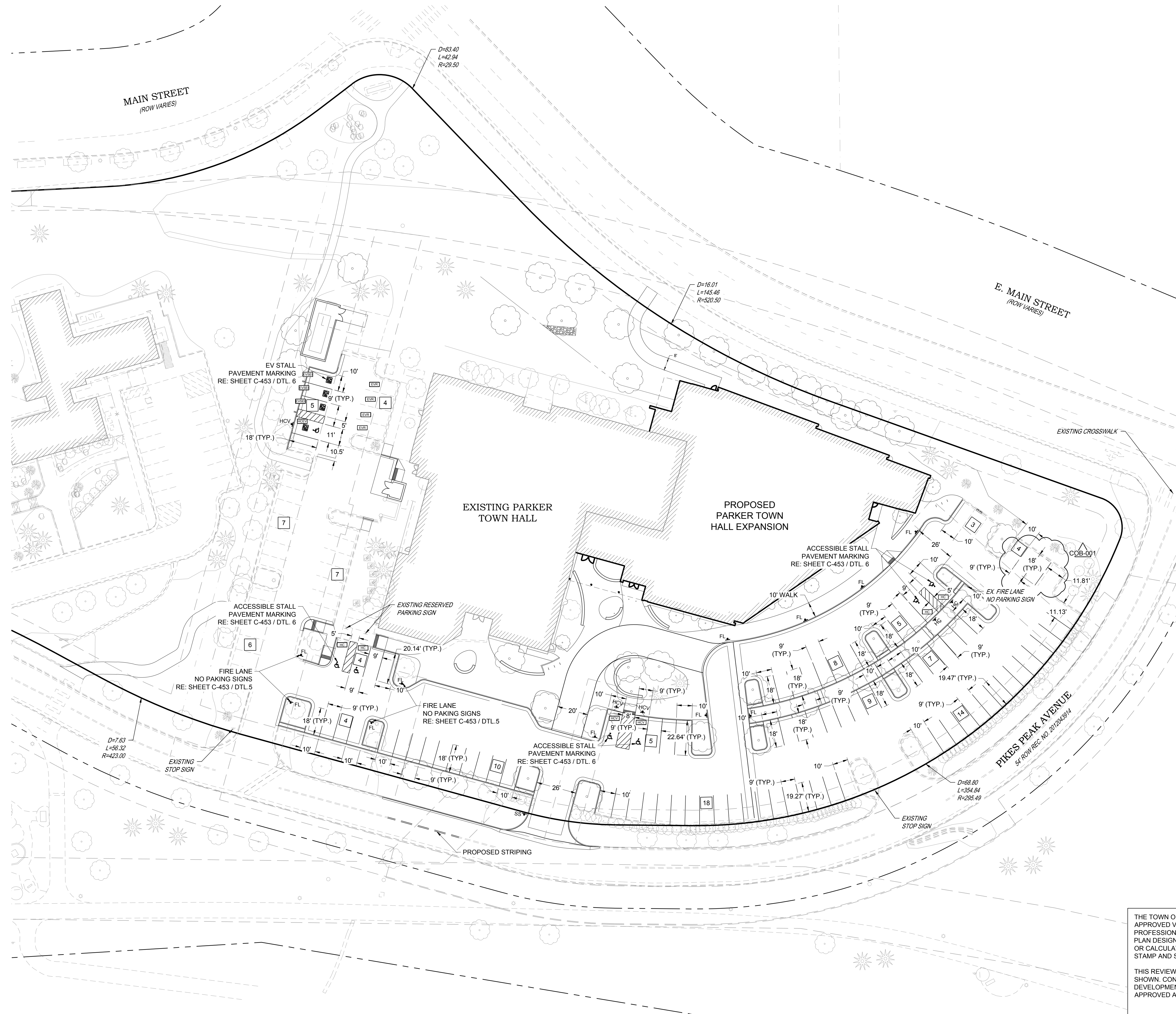
NOTES:

- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND.
- PARKING STALLS ADJACENT TO LANDSCAPE ISLANDS SHALL BE A MINIMUM OF 10' IN WIDTH.
- PARKING STALLS WITHOUT AN ASSOCIATED PARKING CLASSIFICATION ARE STANDARD PARKING STALLS.

LEGEND:

- ▲ HC PROPOSED "HANDICAP PARKING" SIGN
RE: SHEET C-453 / DTL. 5
- ▲ HCV PROPOSED "VAN ACCESSIBLE
HANDICAP PARKING" SIGN
RE: SHEET C-453 / DTL. 5
- ▲ SS PROPOSED "STOP" SIGN
RE: SHEET C-453 / DTL. 5
- ▲ FL PROPOSED "FIRE LANE -
NO PARKING" SIGN
RE: SHEET C-453 / DTL. 5
- 9 PARKING STALL COUNT
9'X18' MINIMUM DIMENSIONS
UNLESS OTHERWISE NOTED
- C COMPACT
PARKING CLASSIFICATION
- HC STANDARD ACCESSIBLE
PARKING CLASSIFICATION
- HCV ACCESSIBLE VAN
PARKING CLASSIFICATION
- HCEV ACCESSIBLE ELECTRIC VEHICLE
PARKING CLASSIFICATION
- UNIV UNIVERSAL VEHICLE
PARKING CLASSIFICATION
- EVSEI ELECTRIC VEHICLE SUPPLY EQUIPMENT INSTALLED
PARKING CLASSIFICATION
- EVR ELECTRIC VEHICLE READY
PARKING CLASSIFICATION
- EVC ELECTRIC VEHICLE CAPABLE
PARKING CLASSIFICATION

PARKING COUNT TABLE			
PARKING TYPE	# OF PROPOSED SPACES	# OF EXISTING SPACES TO REMAIN	TOTAL
STANDARD	75	31	106
COMPACT	0	0	0
STANDARD ACCESSIBLE	4	0	4
ACCESSIBLE VAN	2	0	2
ACCESSIBLE EV	1	0	1
UNIVERSAL	0	0	0
EVSE INSTALLED	3	0	3
EV READY	4	0	4
EV CAPABLE	0	0	0
TOTAL	89	31	120



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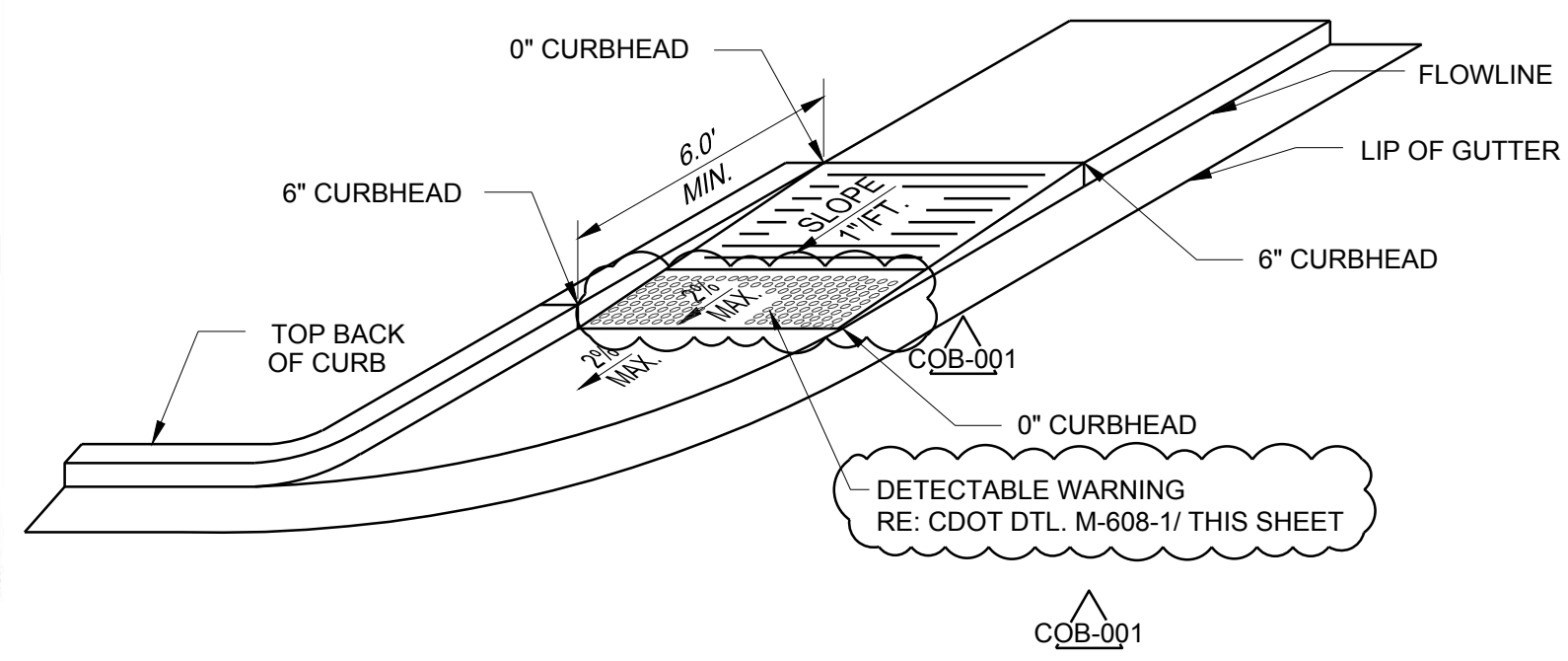
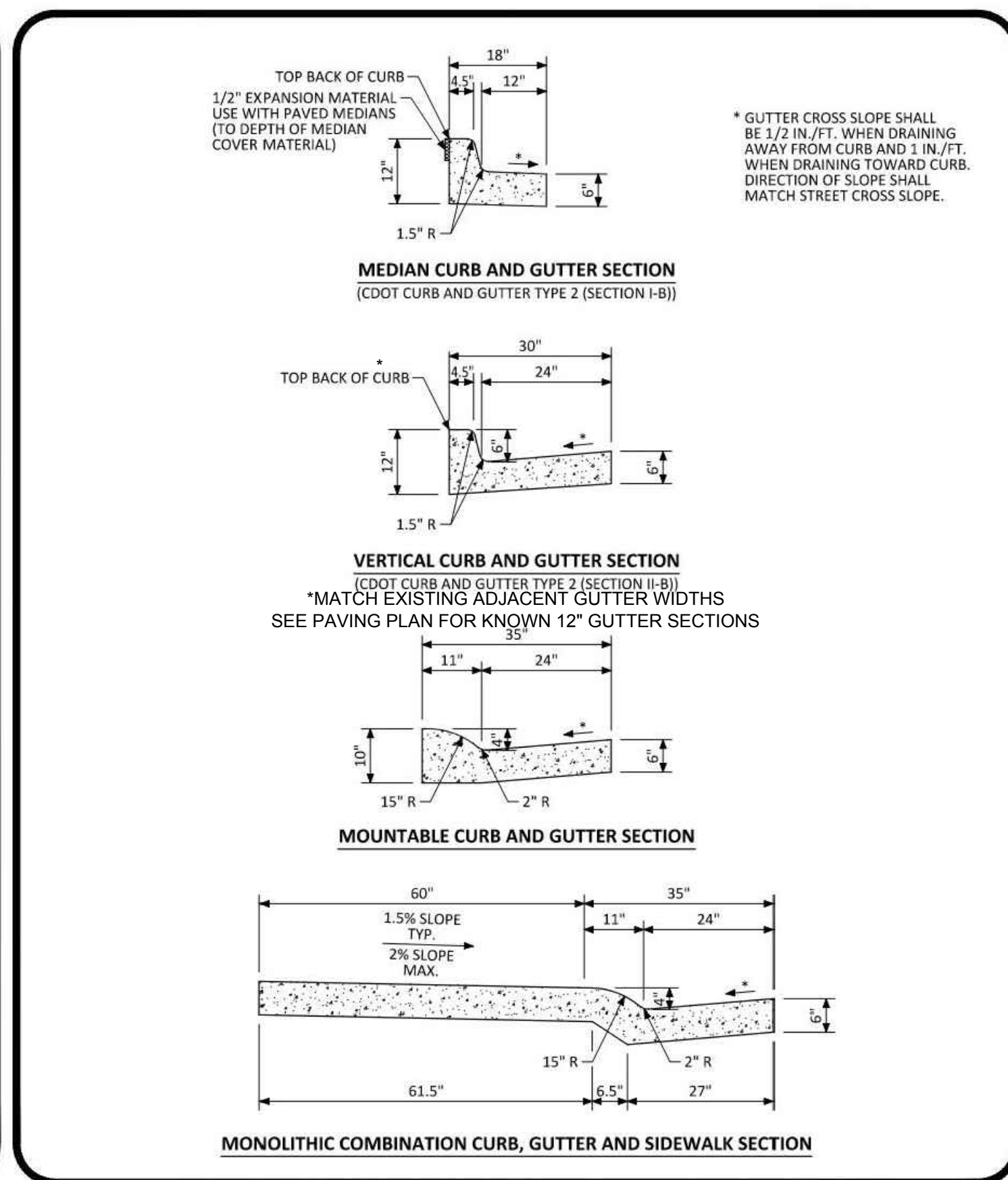
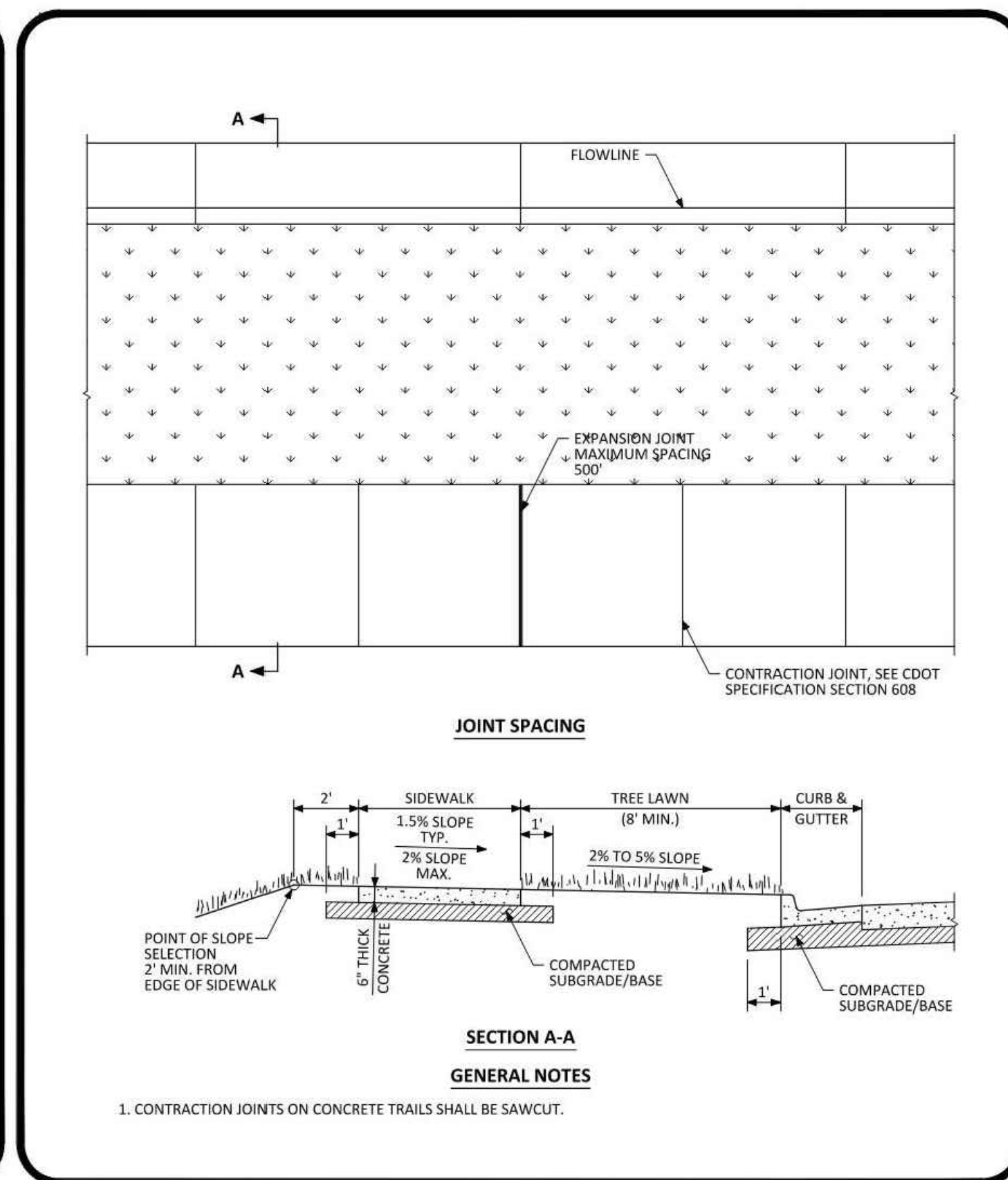
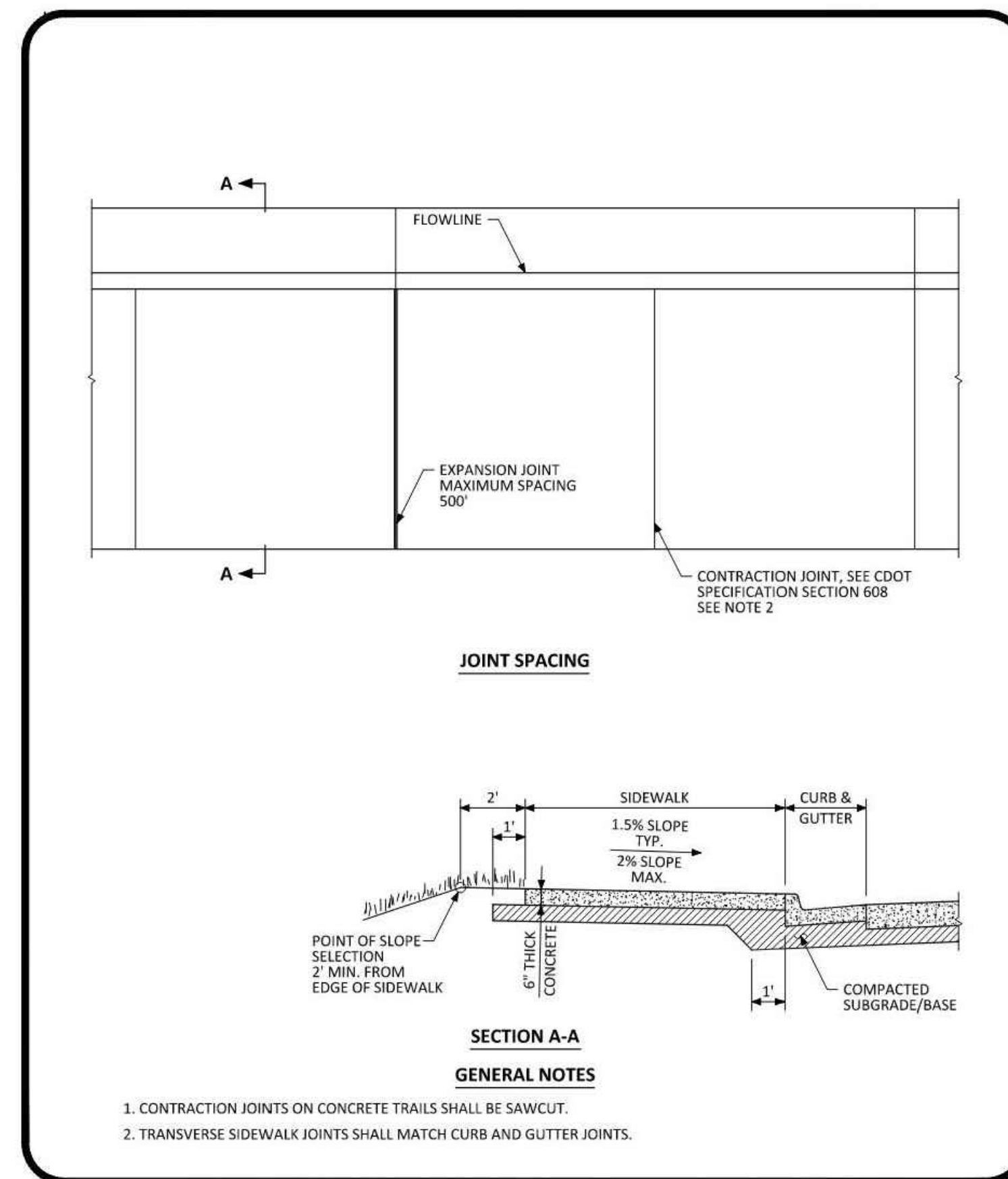


Issue _____ Date _____

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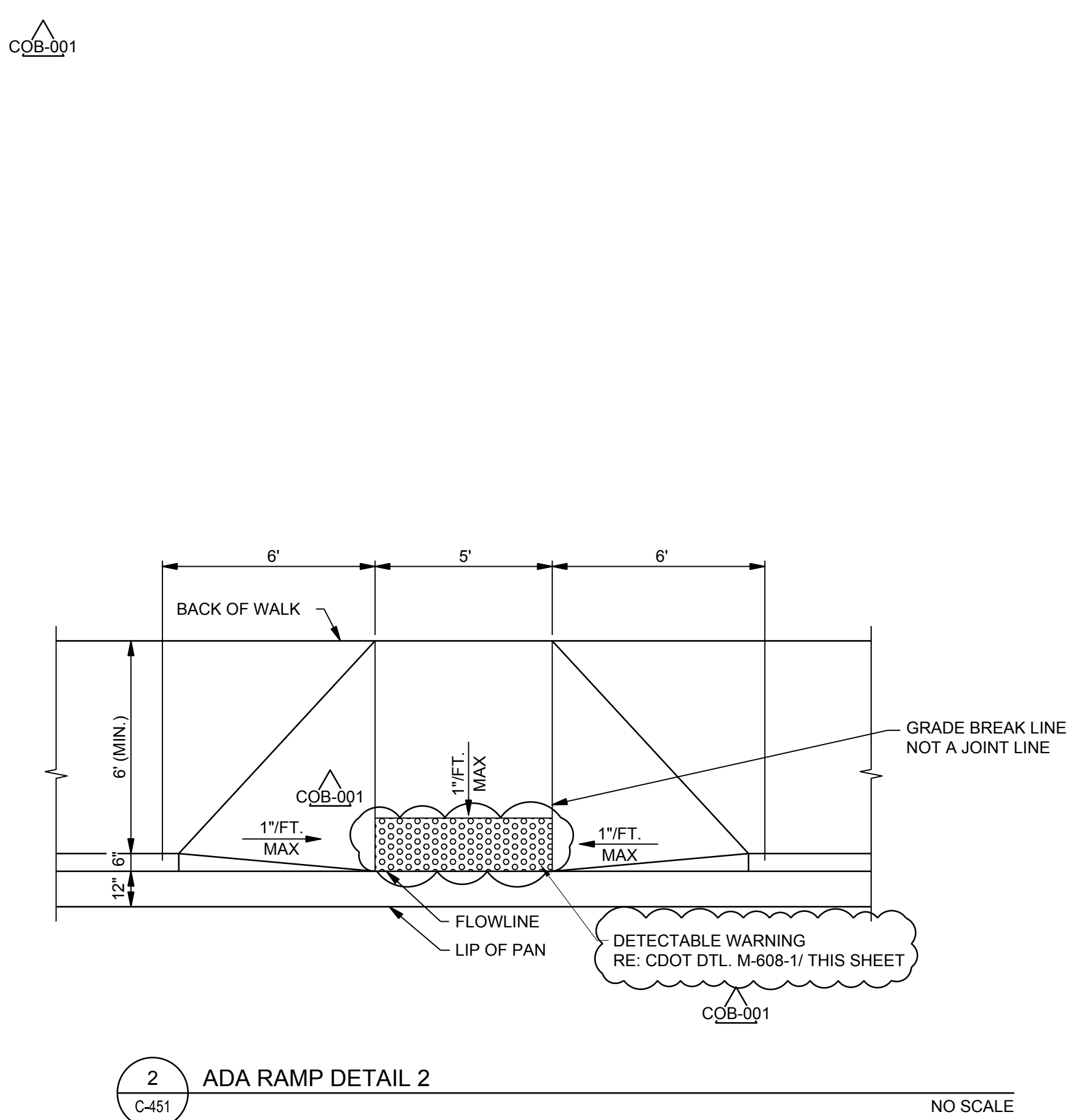
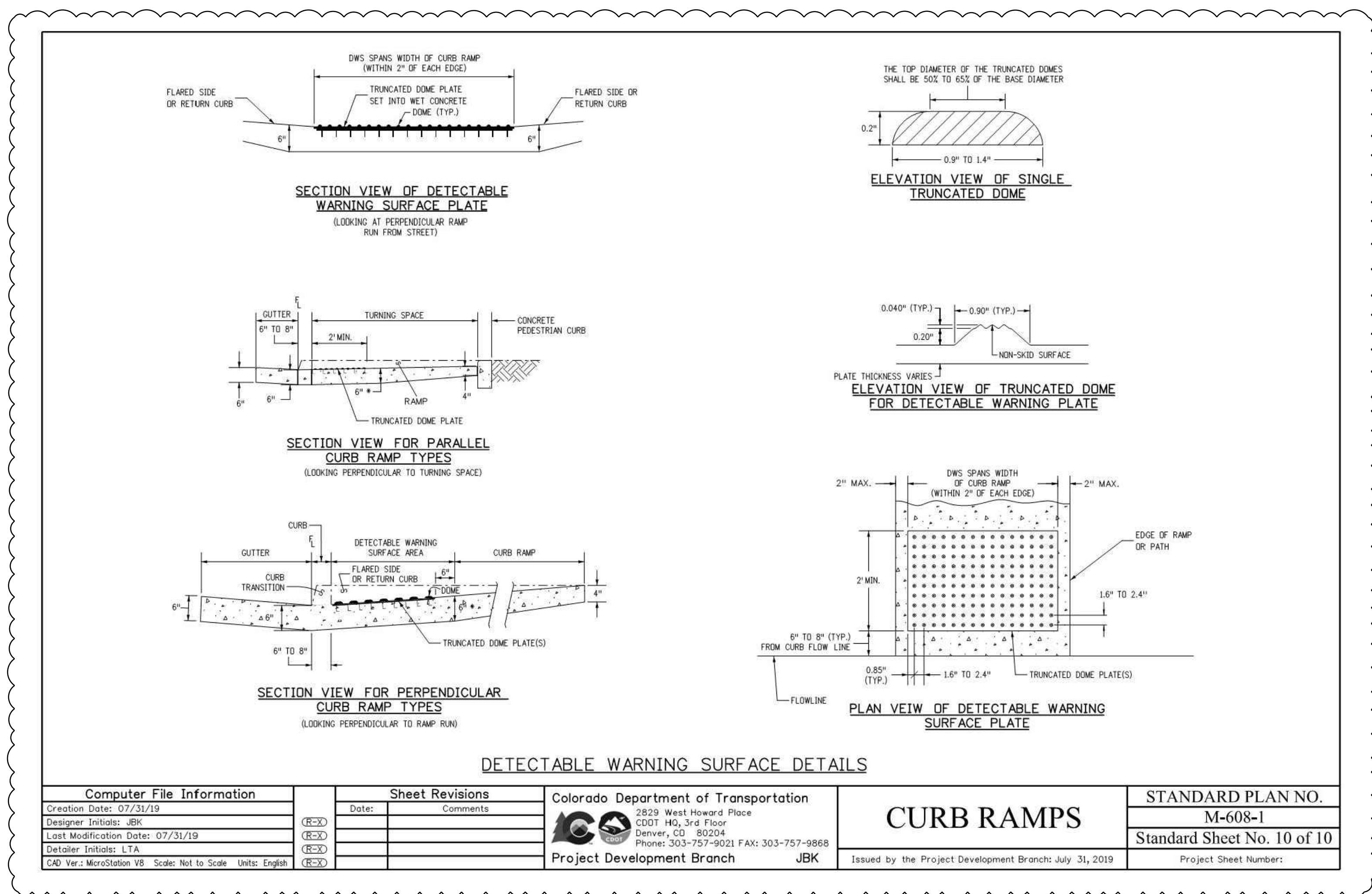
Project Number: 22-139
Drawn By: _____
Reviewed By: _____
Approved By: _____



PARKER COLORADO ATTACHED SIDEWALK STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 4 OF 2

PARKER COLORADO DETACHED SIDEWALK STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 4 OF 2

PARKER COLORADO CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 3 OF 1



Computer File Information	Sheet Revisions	Colorado Department of Transportation	STANDARD PLAN NO.
Creation Date: 07/31/19	Date: _____	2829 West Howard Place	M-608-1
Designer Initials: JBR	Comments: _____	CDOT, 3rd Floor	Standard Sheet No. 10 of 10
Last Modification Date: 07/31/19	_____	Denver, CO 80204	
Detailer Initials: LTA	_____	Phone: 303-757-9921 FAX: 303-757-9868	
CDOT Ver. Modification V8 Scale: Not to Scale Units: English	_____	Project Development Branch JBK	Issued by the Project Development Branches July 31, 2019



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Parker Town Hall Expansion

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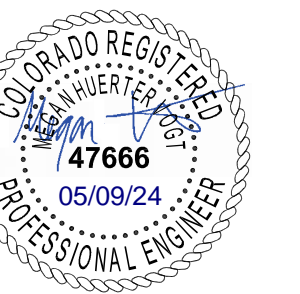
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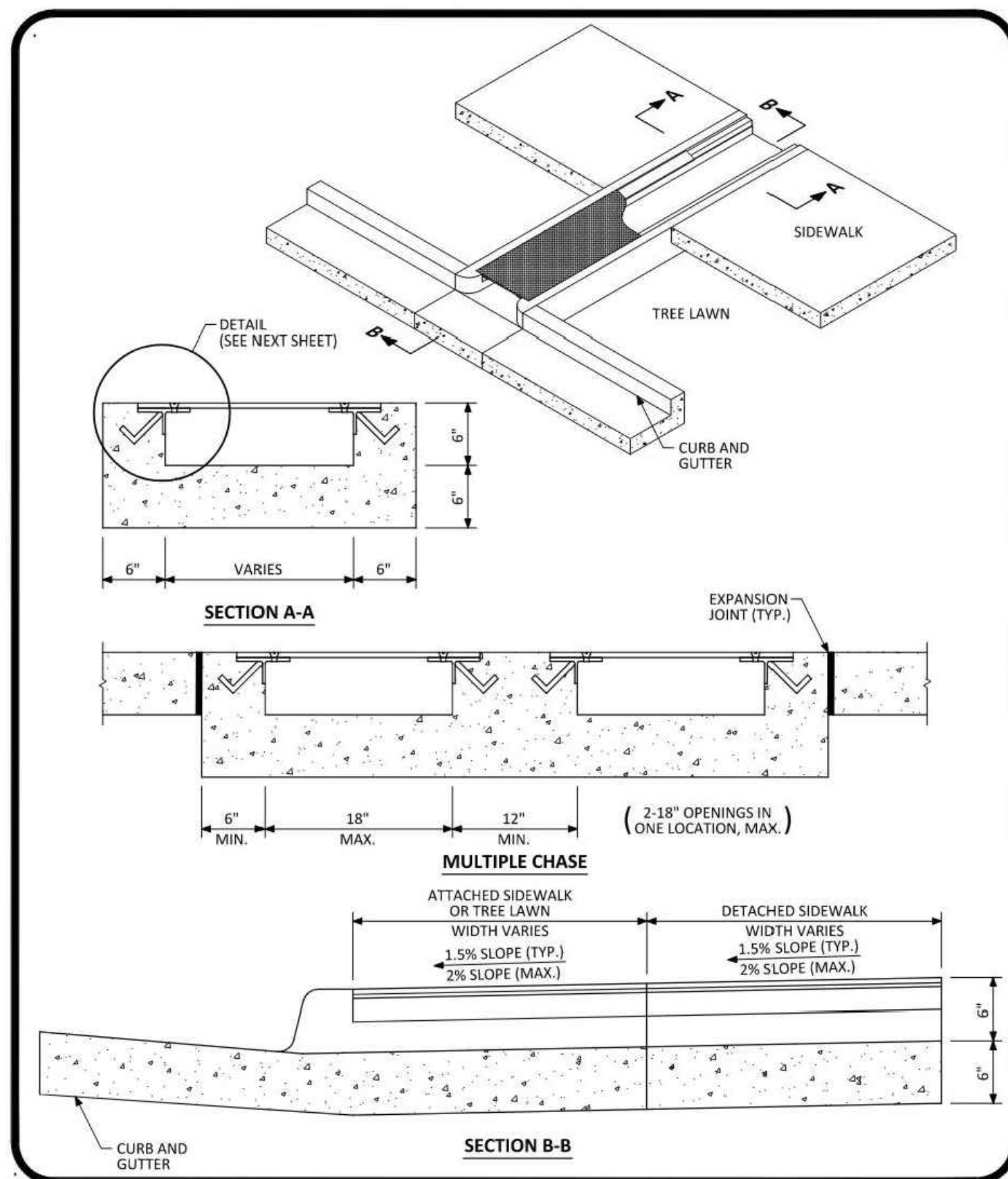
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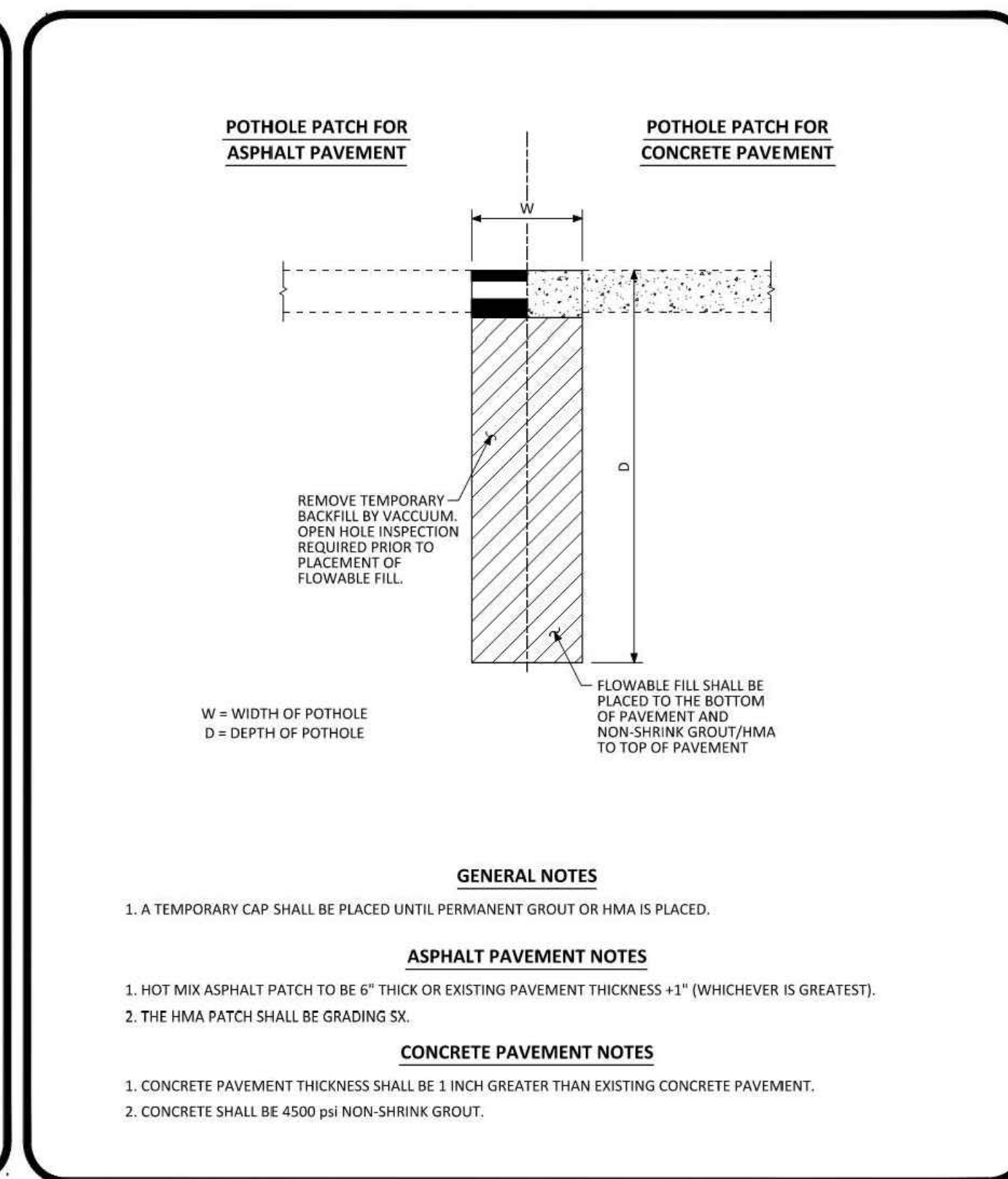
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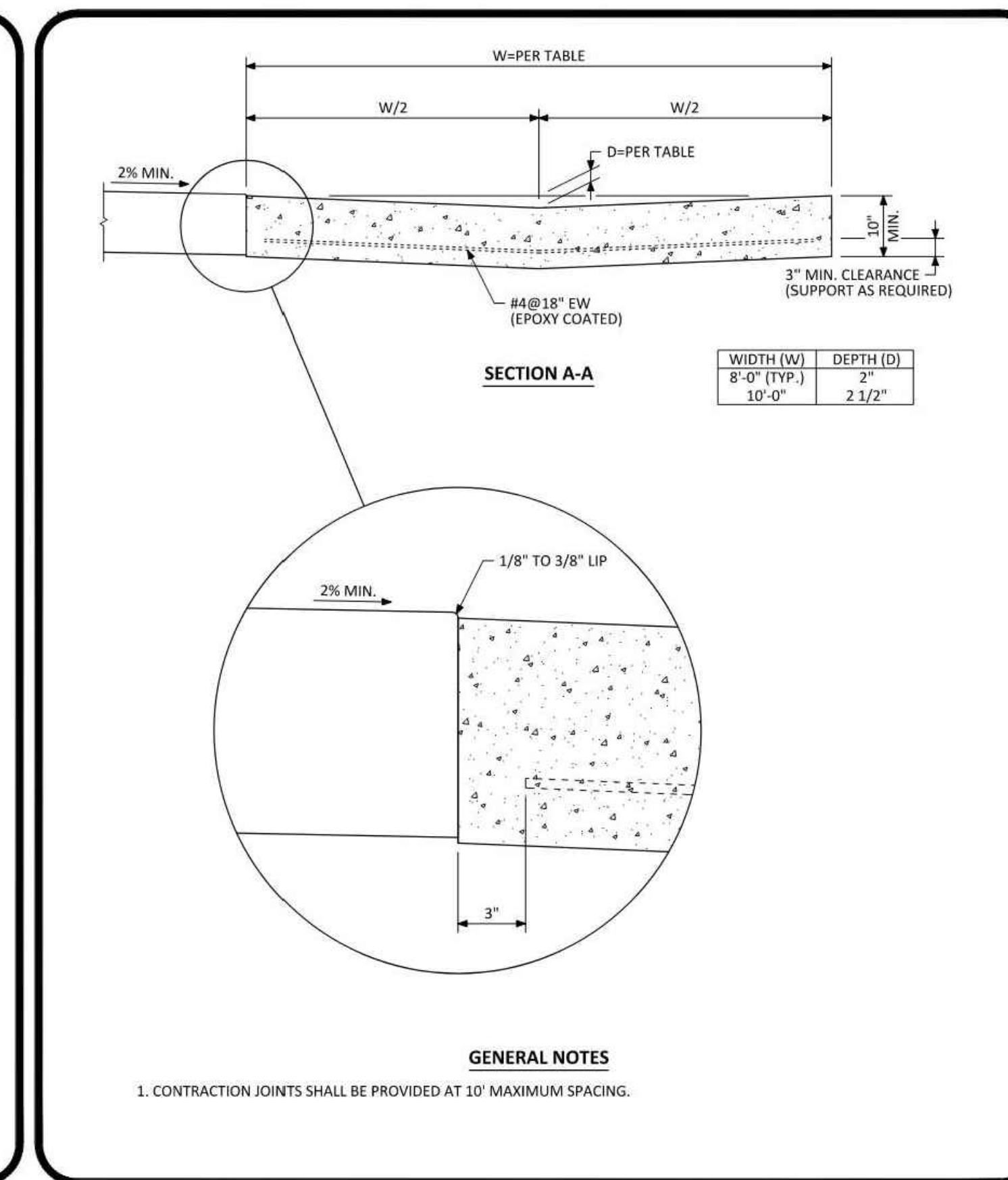
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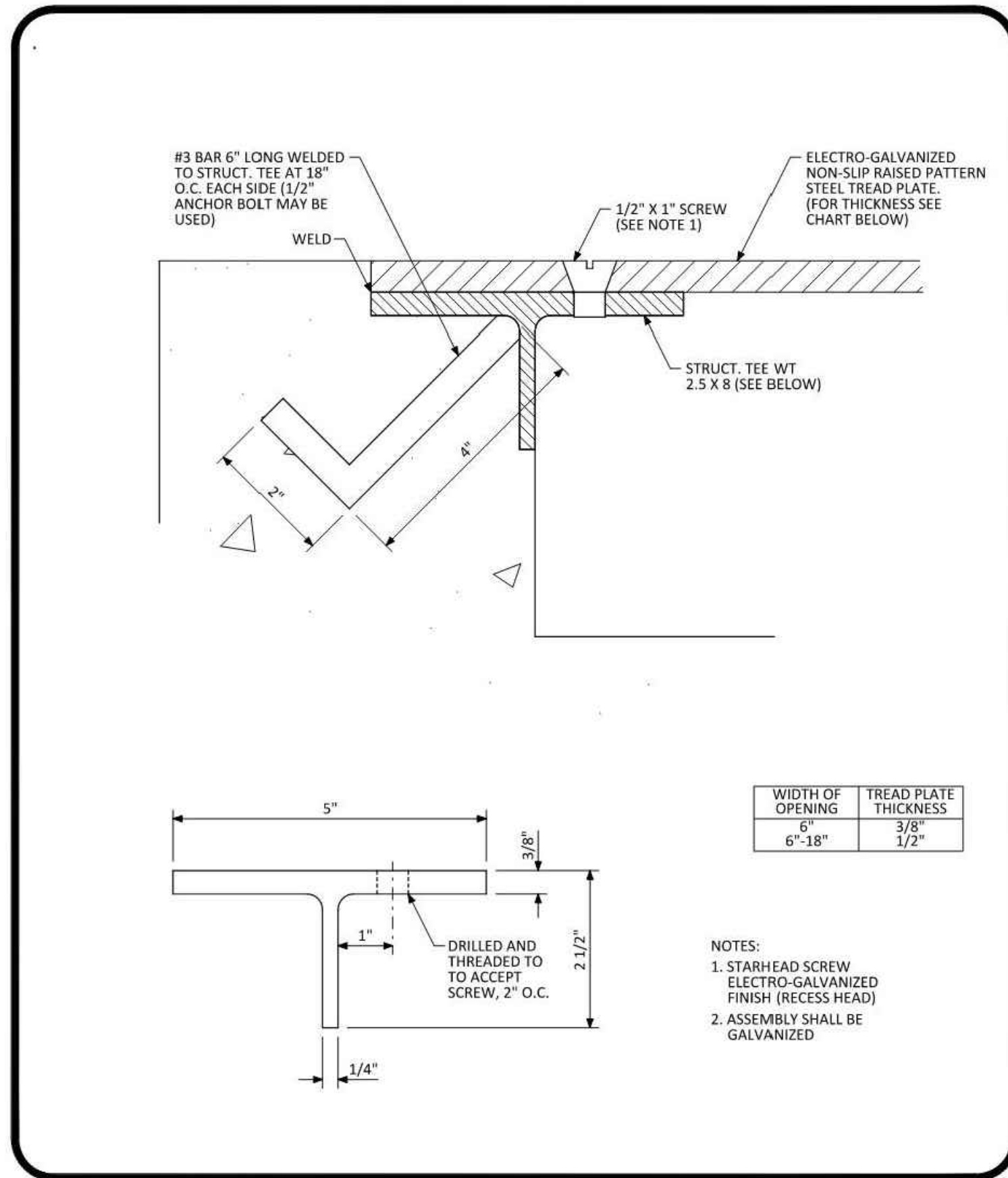
PARKER COLORADO CURB CHOSE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 29 1 OF 2



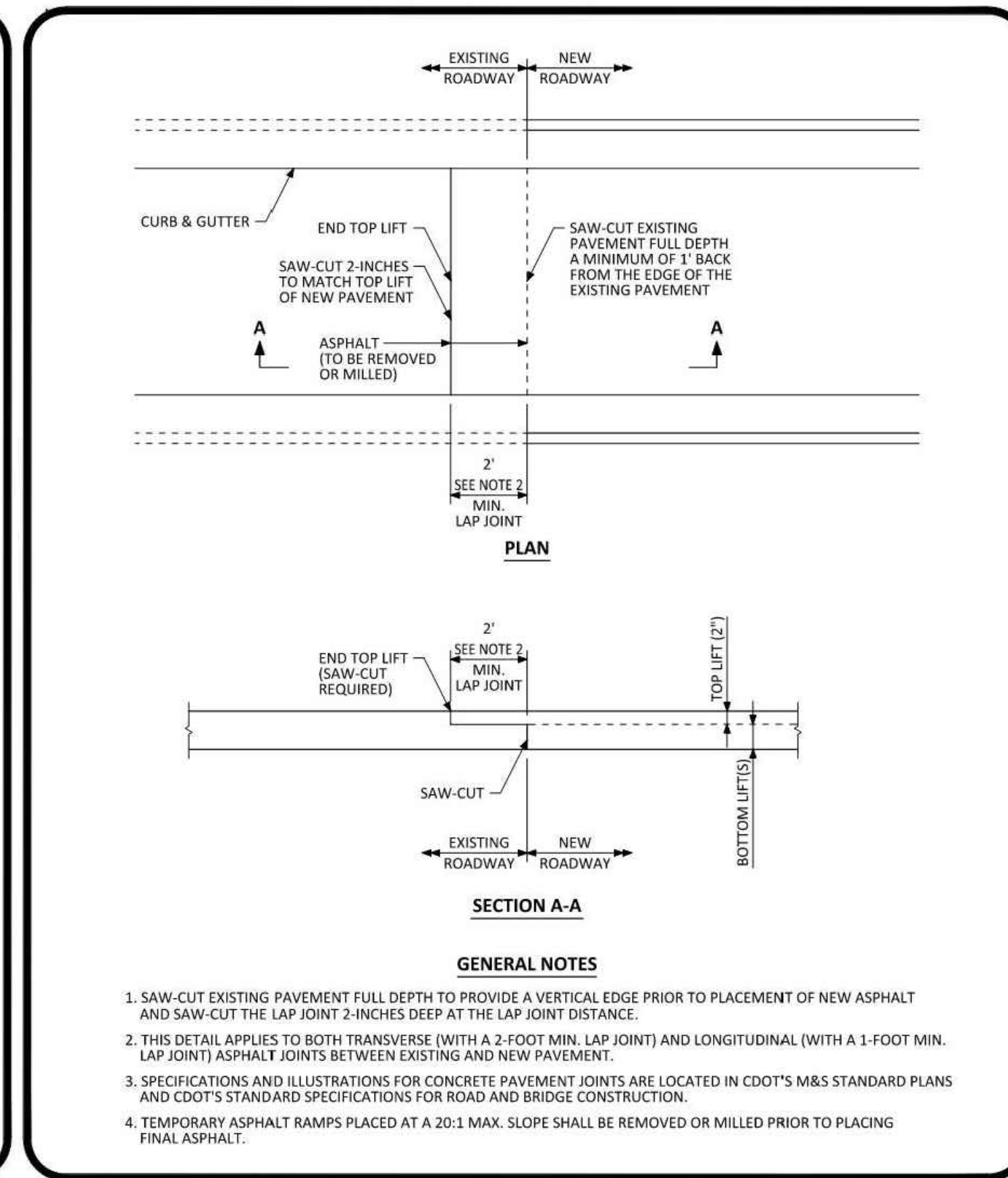
PARKER COLORADO POT HOLE RESTORATION STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 25 2 OF 2



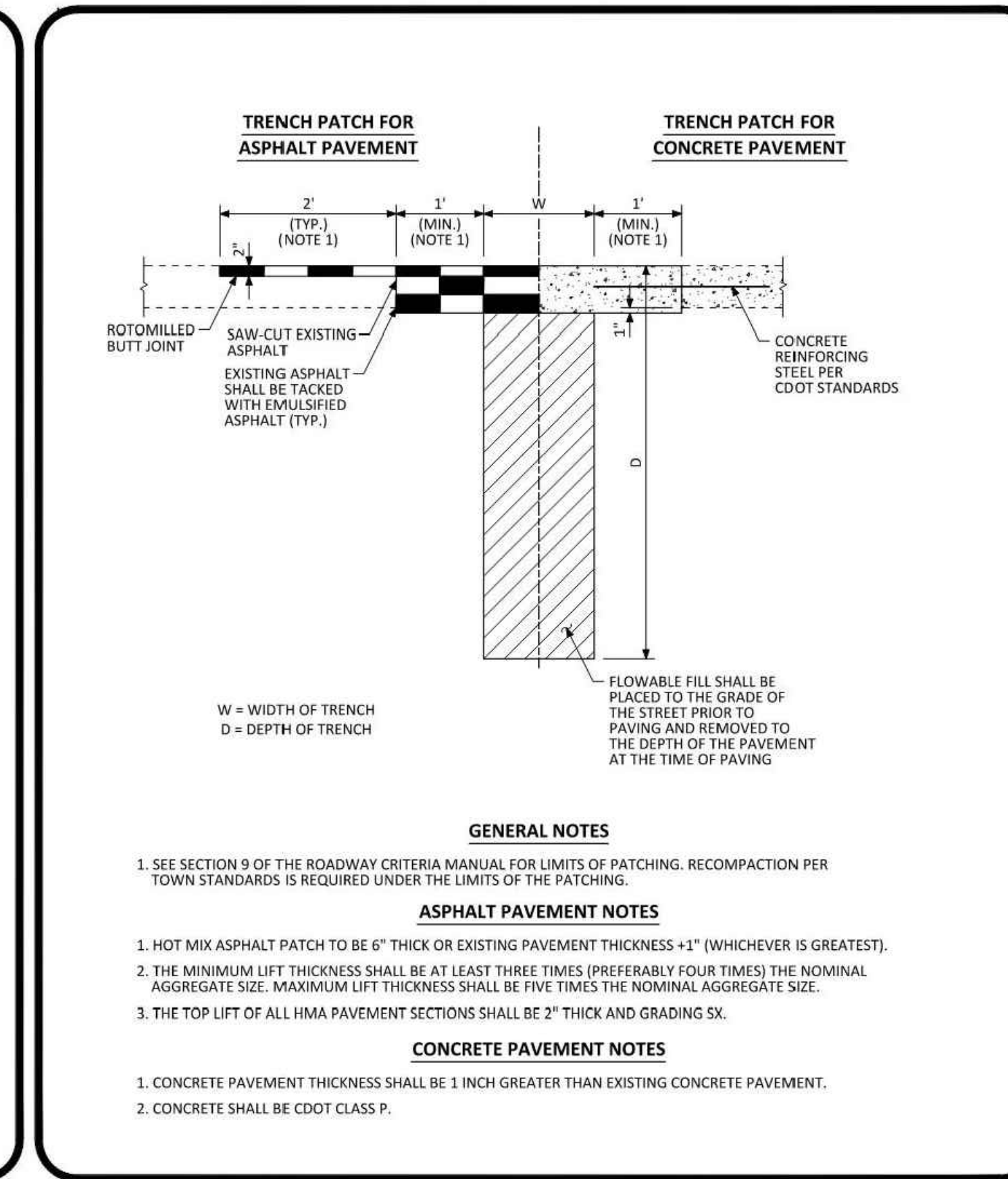
PARKER COLORADO INTERSECTION CROSS PAN LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 19 1 OF 2



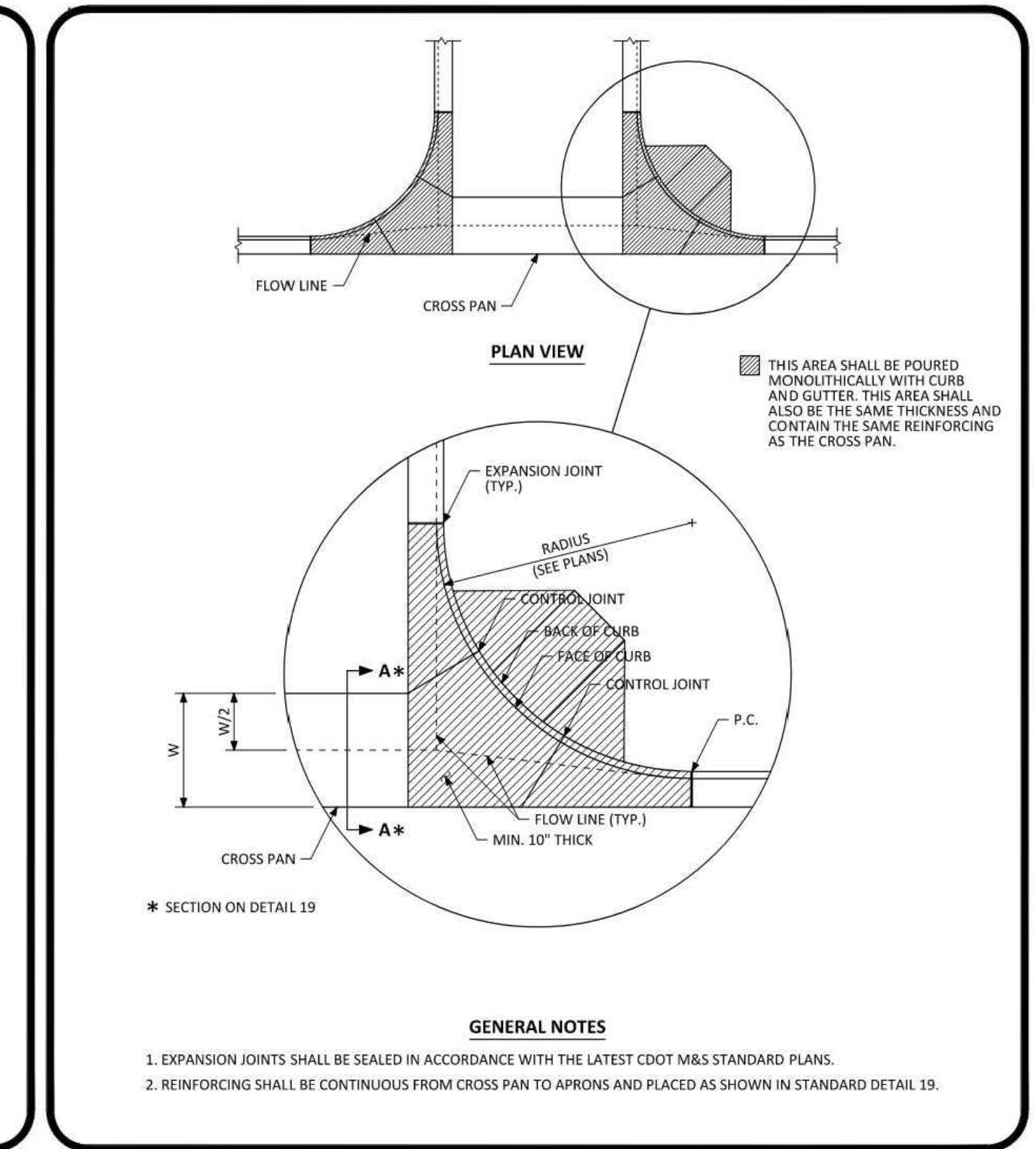
PARKER COLORADO CURB CHOSE DRAIN (NON-RESIDENTIAL) LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 29 2 OF 2



PARKER COLORADO ASPHALT CONNECTION BETWEEN EXISTING AND NEW PAVEMENT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 26 1 OF 1



PARKER COLORADO MINIMUM STREET PATCH DETERMINATION STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 25 1 OF 2



PARKER COLORADO INTERSECTION CROSS PAN LAYOUT STANDARD DETAIL DATE NOVEMBER 2020 DETAIL 19 2 OF 2

Project Number: 22-139
Drawn By:
Reviewed By:
Approved By:

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

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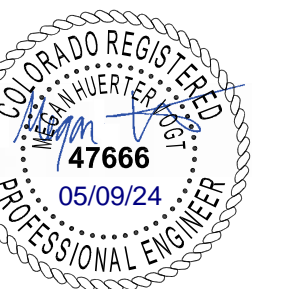
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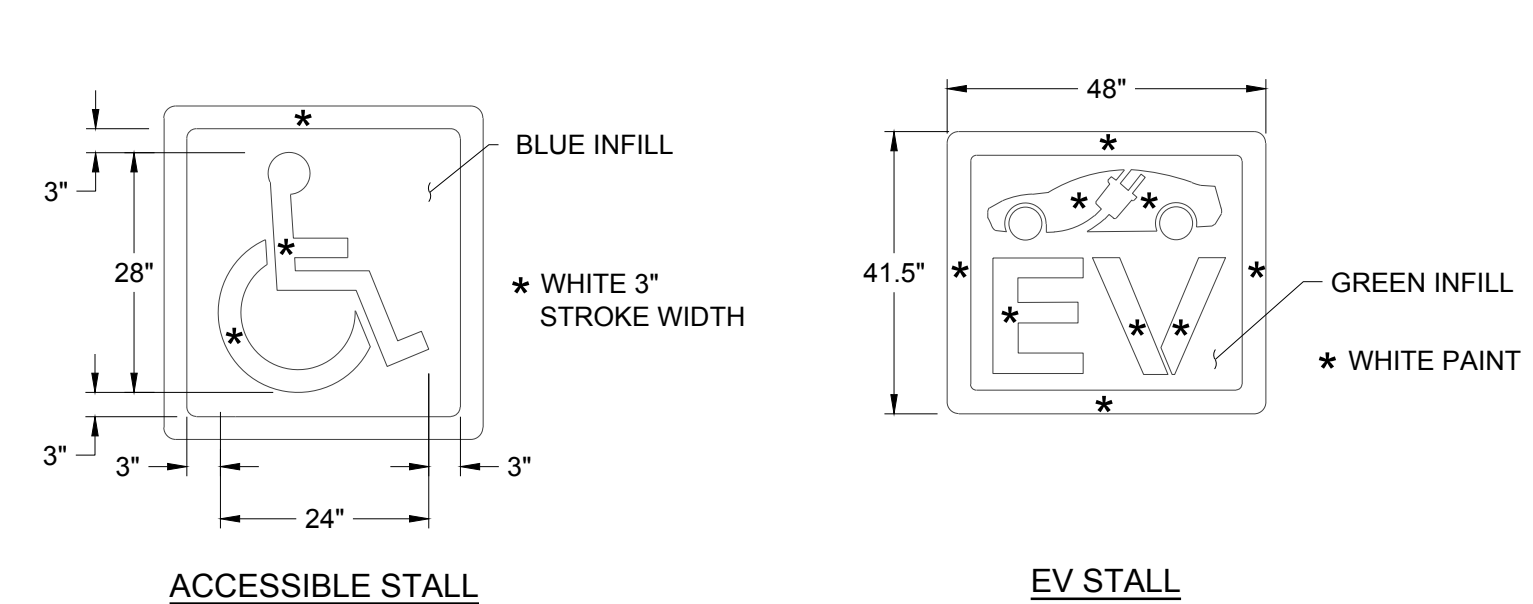
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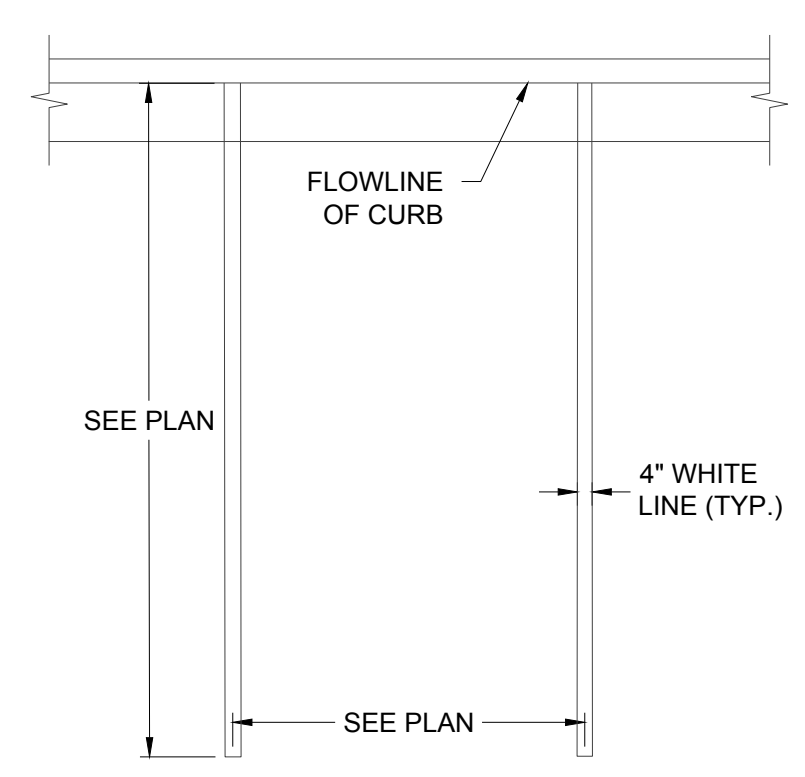


Issue	Date
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DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024



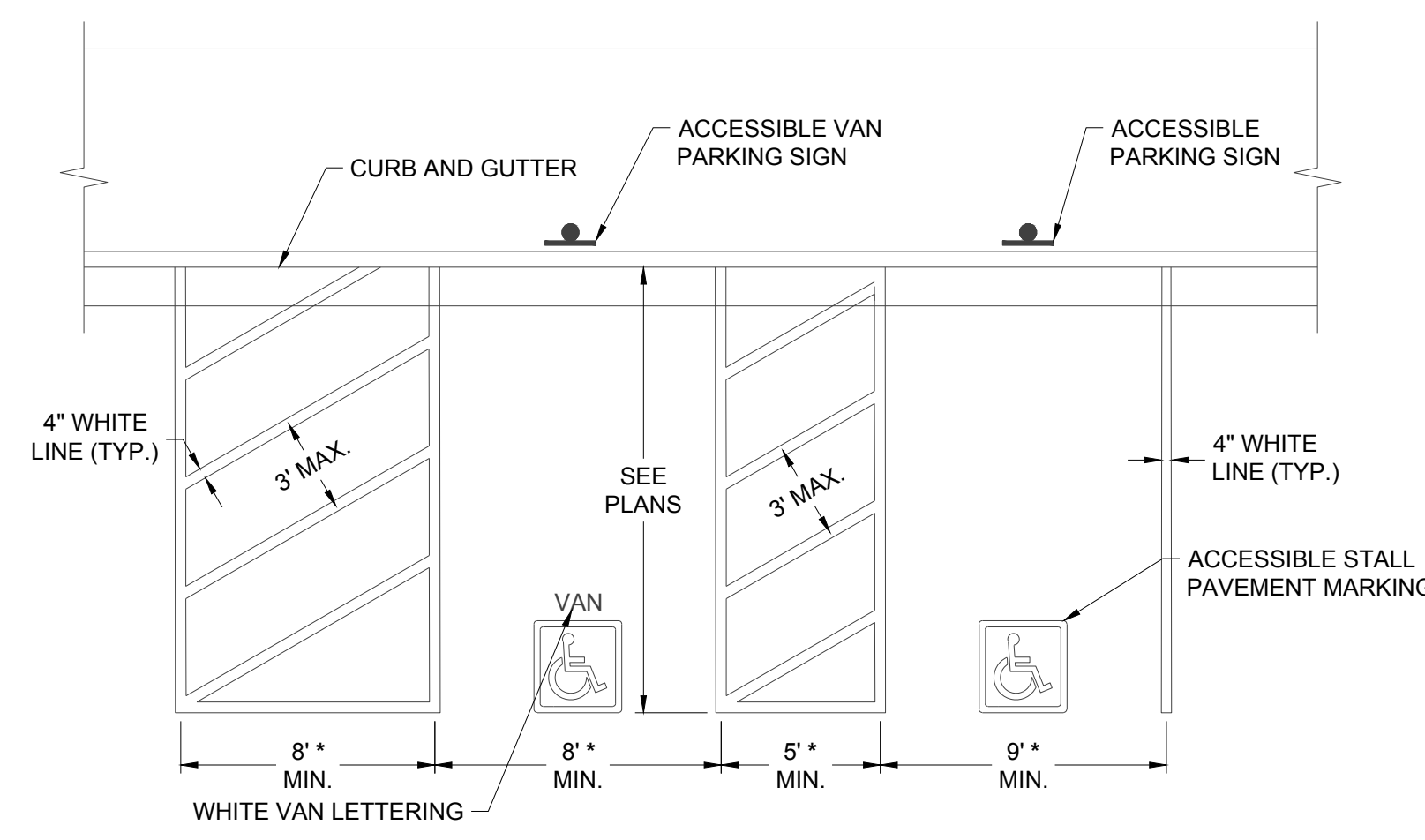
NOTES:
1. WHITE PAINT OR THERMOPLASTIC MARKINGS SHALL BE USED UNLESS OTHERWISE SPECIFIED BY AUTHORITY HAVING JURISDICTION. ALSO COMPARE AGAINST PROJECT SPECIFIC CIVIL SPECS TO ENSURE THEY MATCH.

6 PAVEMENT MARKINGS DETAIL NO SCALE



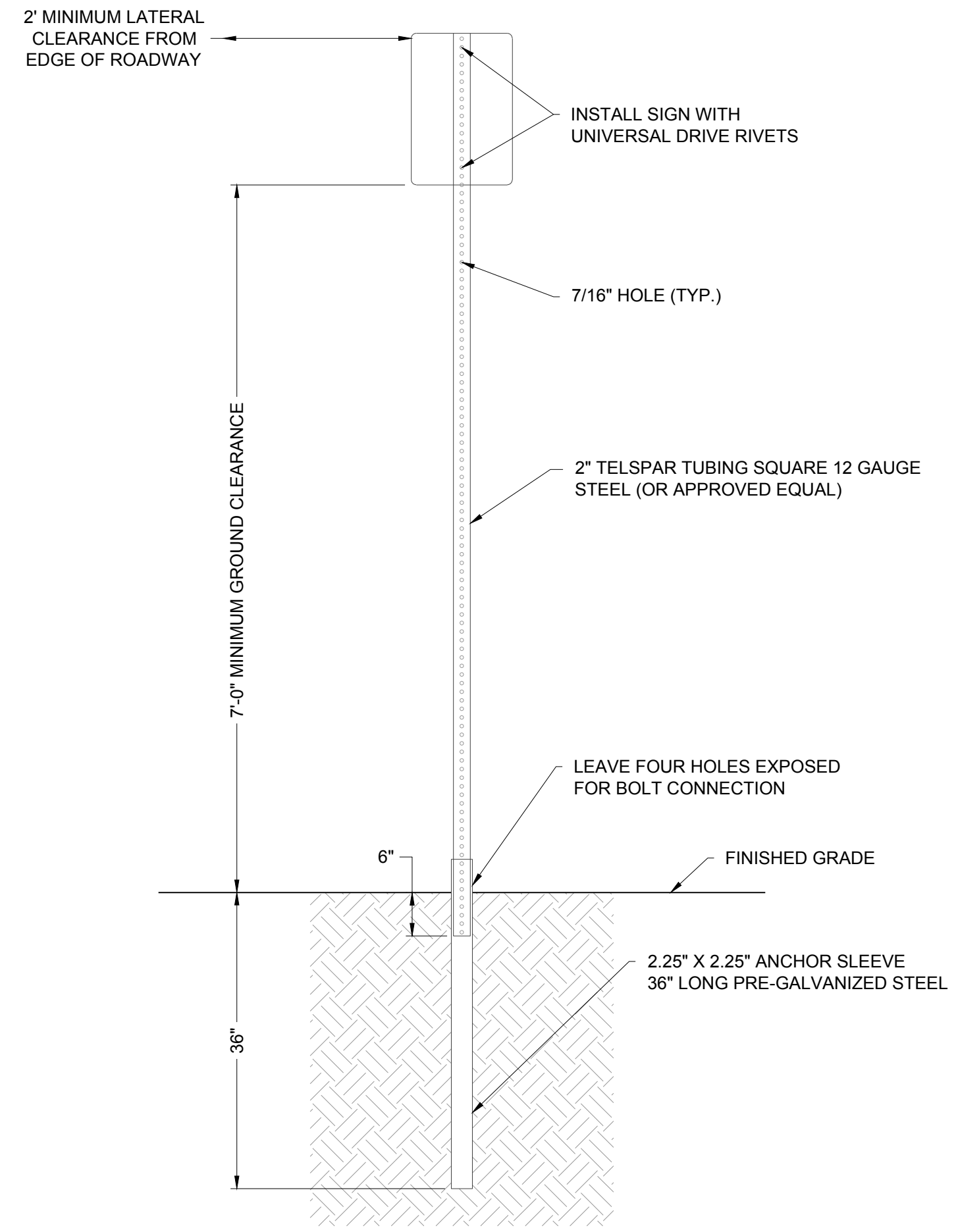
NOTES:
1. PARKING STALLS ADJACENT TO LANDSCAPE ISLANDS SHALL BE MINIMUM OF 10' IN WIDTH.

7 TYPICAL PARKING STALL DETAIL NO SCALE



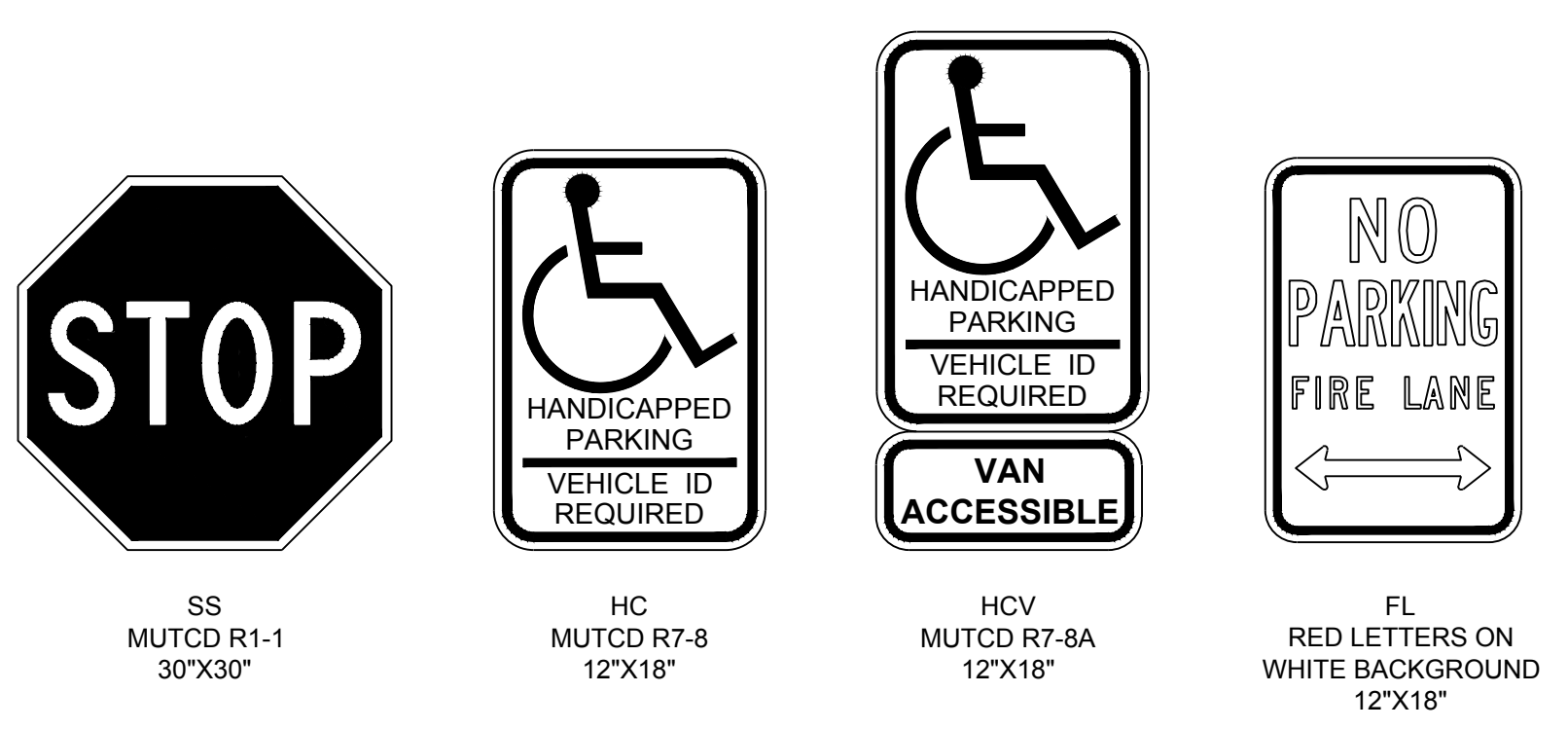
NOTE:
THE ACCESSIBLE STALLS AND ASSOCIATED AISLES SHALL BE SLOPED AT A MAXIMUM OF 2% IN ANY DIRECTION. REFER TO GRADING PLANS FOR SPECIFIC ELEVATIONS AND CONTACT THE ENGINEER IF THERE ARE ANY CONCERNS. *SEE PLAN FOR ACTUAL DIMENSIONS

8 TYPICAL ACCESSIBLE STALLS DETAIL NO SCALE

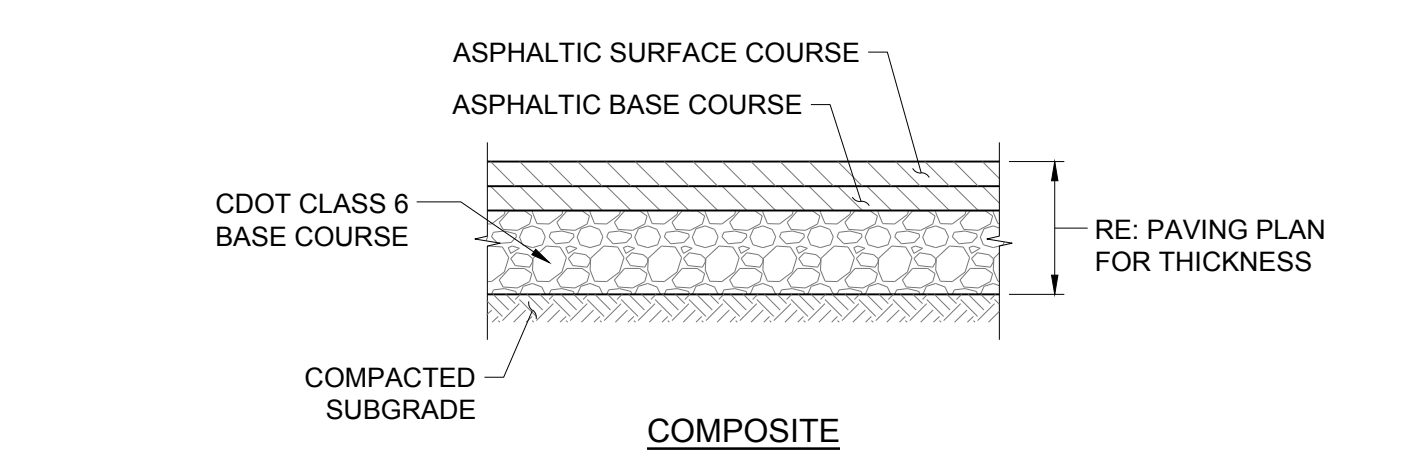


NOTES:
1. SIGN POST INSTALLATION SHALL FOLLOW THE MANUFACTURE'S SPECIFICATIONS.
2. ALL TUBING SHALL BE #12 U.S. STANDARD GAUGE WALL THICKNESS.
3. SIGN AND POST SHALL BE PLUMB AND FREE OF VERTICAL AND LATERAL MOVEMENT.
4. SIGN SHALL BE PARALLEL TO FLOWLINE FOR PARKING STALL INSTALLATION AND 90° FOR ROADWAY INSTALLATION.
5. DRIVE ANCHOR POST INTO THE GROUND TOGETHER, LEAVING FOUR HOLES EXPOSED FOR BOLT CONNECTION. FOR HAND INSTALLATION, DRIVE CAP OF PROPER SIZE SHALL BE USED TO PREVENT DAMAGE TO TUBING. SIDEWALK SHALL BE POURED AROUND A PVC SLEEVE. EXISTING SIDEWALK SHALL BE CORE DRILLED FOR A MINIMUM OF A 4" PVC SLEEVE.
6. INSERT SIGN POST INTO ANCHOR SLEEVE APPROXIMATELY 6" AND BOLT IN PLACE.
7. A 10" POST WILL WORK FOR SIGNS OF 30" OR LESS. ANY SIGNS LARGER WILL REQUIRE A 11" OR 12" POST DEPENDING ON SIGN SIZE TO MAINTAIN 7' OF GROUND CLEARANCE.

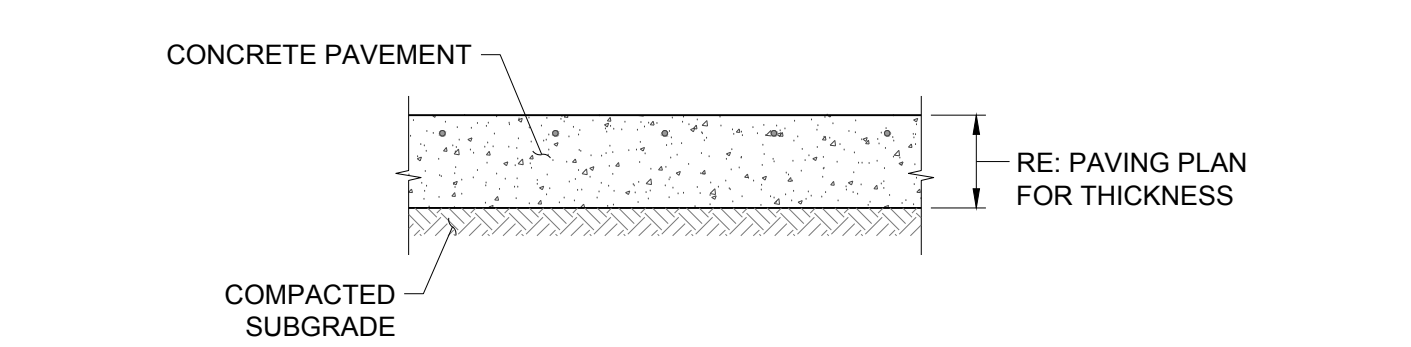
4 SIGN SUPPORT DETAIL NO SCALE



5 SIGN DETAILS NO SCALE

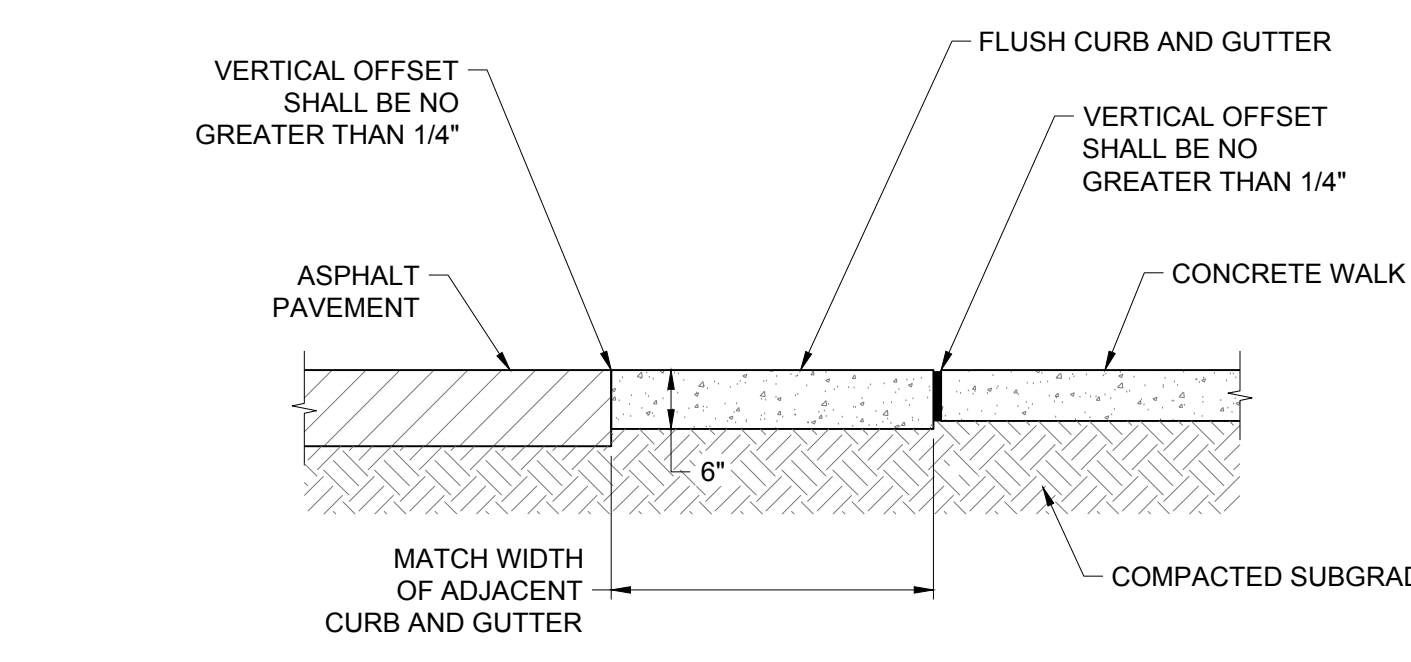


A ASPHALT PAVEMENT SECTIONS NO SCALE

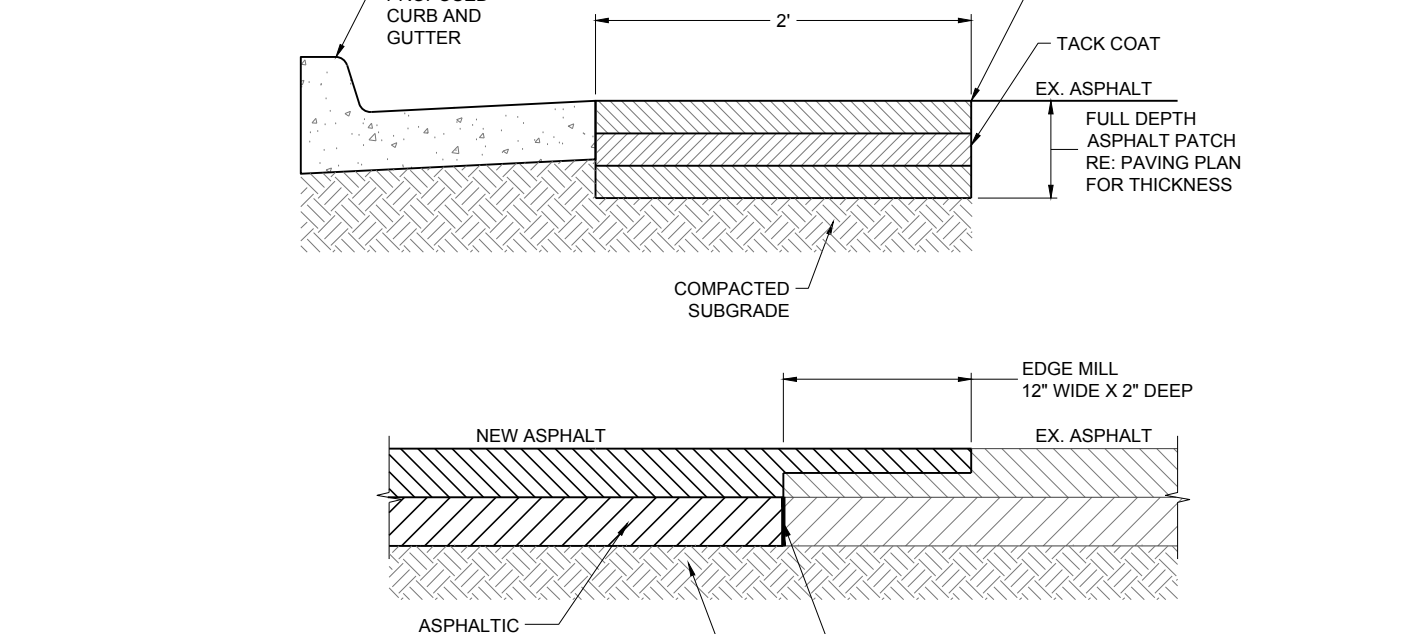


B CONCRETE PAVEMENT SECTION NO SCALE

1 PAVEMENT SECTIONS NO SCALE



2 FLUSH CURB AND GUTTER DETAIL NO SCALE



3 ASPHALT PATCHBACK DETAIL NO SCALE

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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Town of Parker, Director of Engineering/Public Works _____ Date _____

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SITE DETAILS

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22-093

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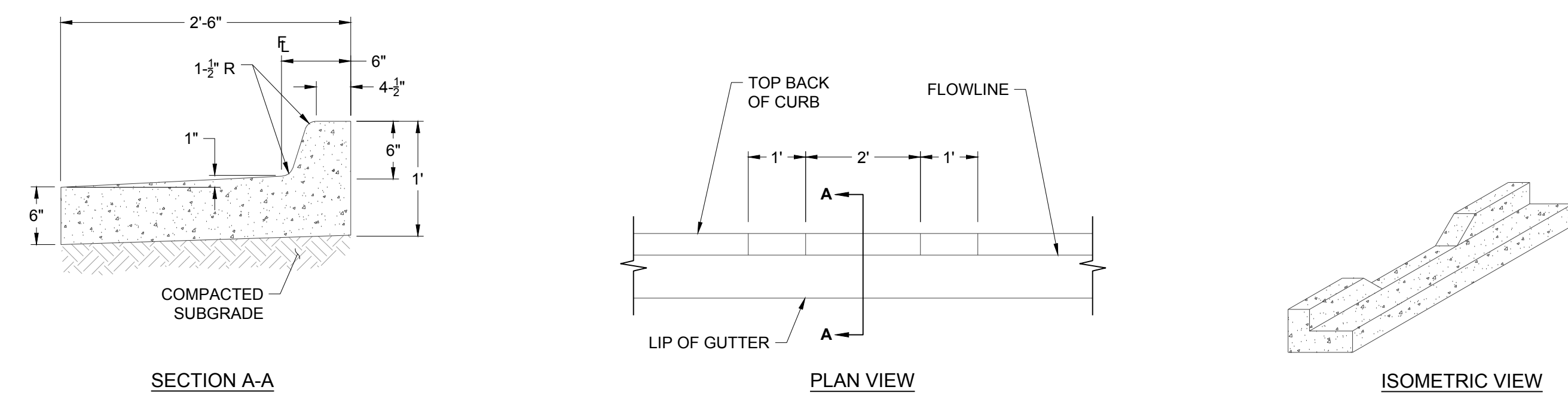
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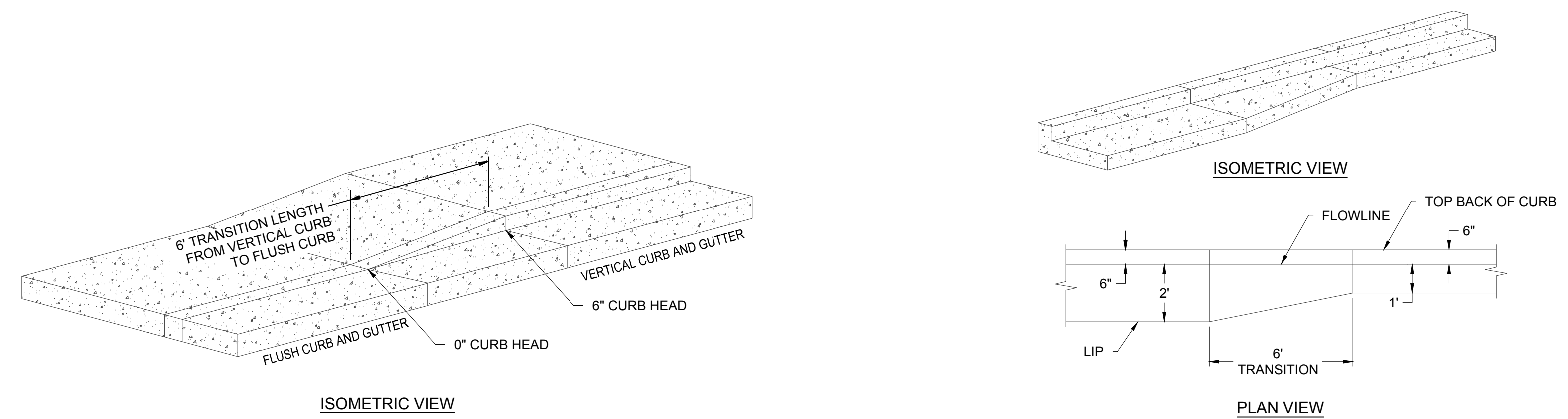
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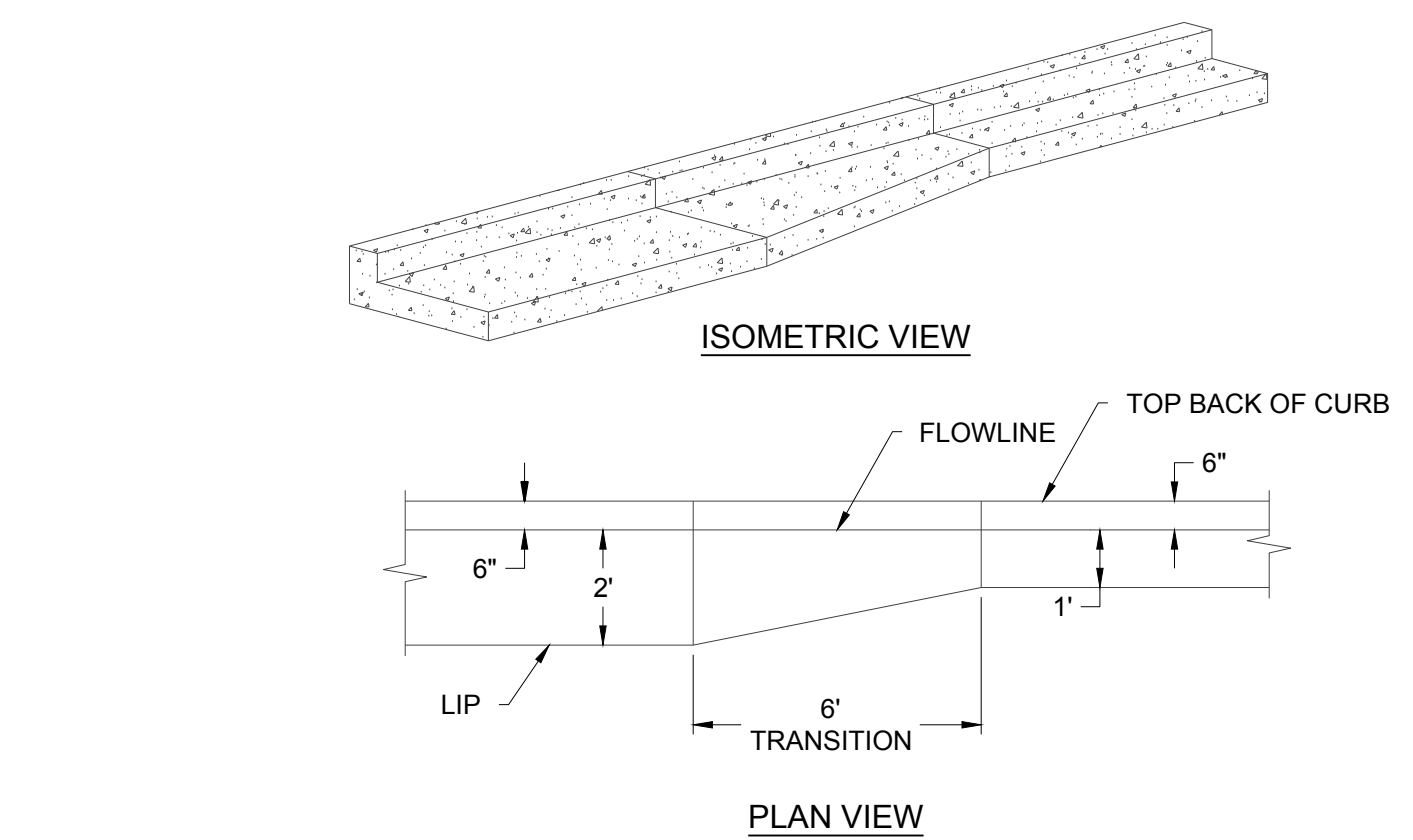
1
C-454 2' CURB CUT DETAIL

NO SCALE



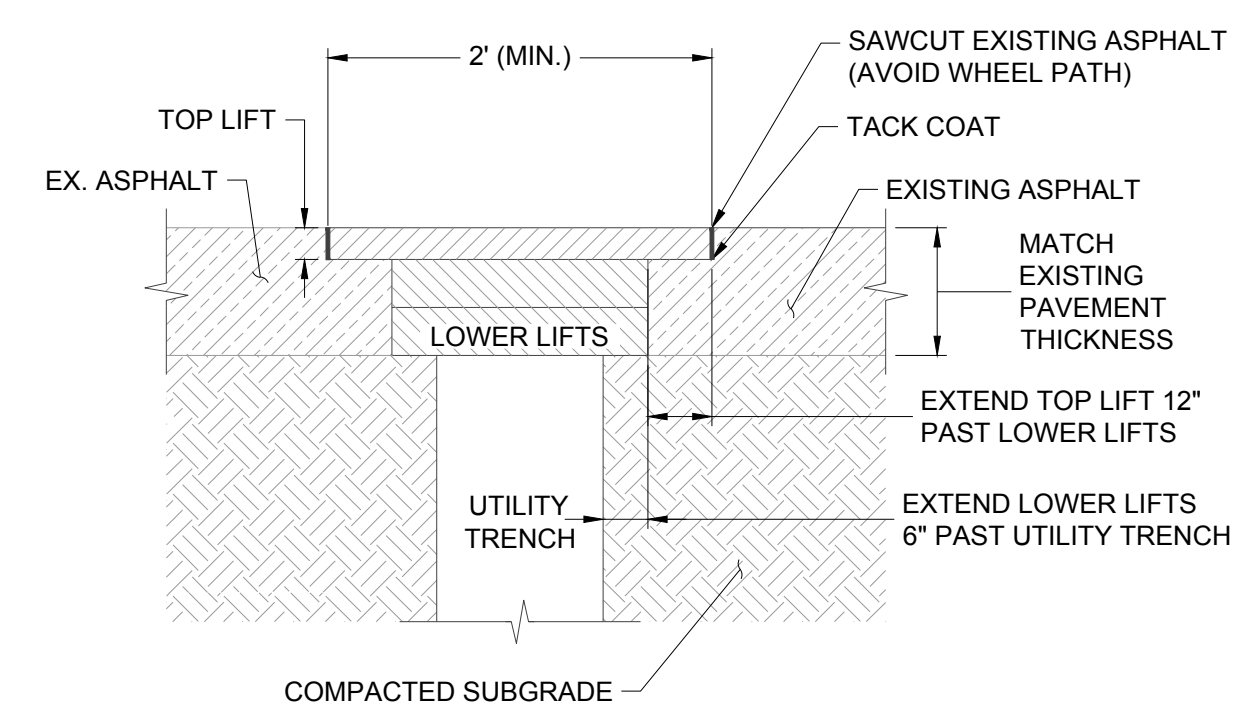
3
C-454 TRANSITION FROM 6" VERTICAL CURB TO FLUSH CURB DETAIL

NO SCALE



2
C-454 TRANSITION FROM 2' GUTTER TO 1' GUTTER DETAIL

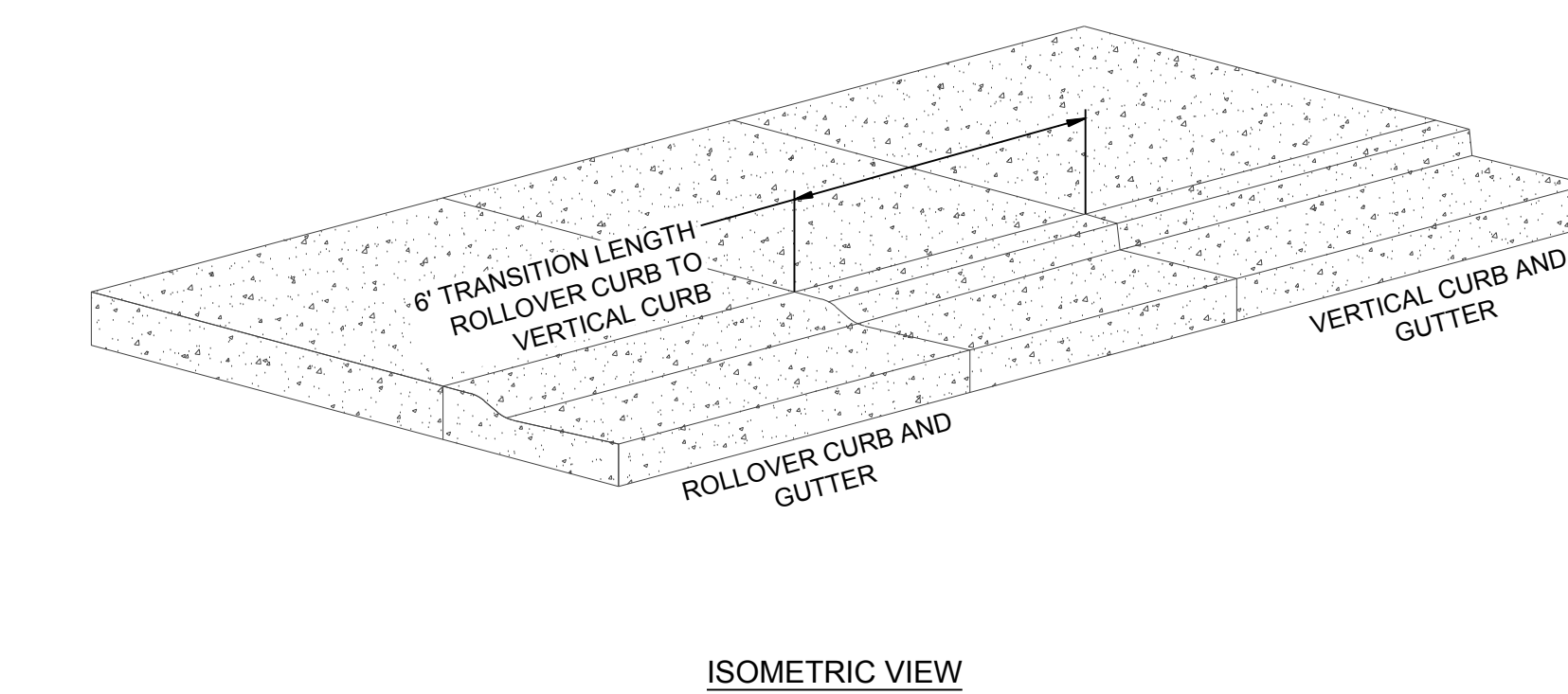
NO SCALE



5
C-454 UTILITY CUT ASPHALT PATCHBACK DETAIL

3" MAXIMUM COMPACTED LIFTS

NO SCALE



4
C-454 TRANSITION FROM ROLLOVER CURB TO 6" VERTICAL CURB DETAIL

NO SCALE

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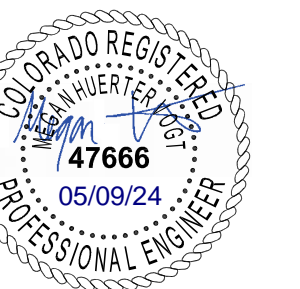
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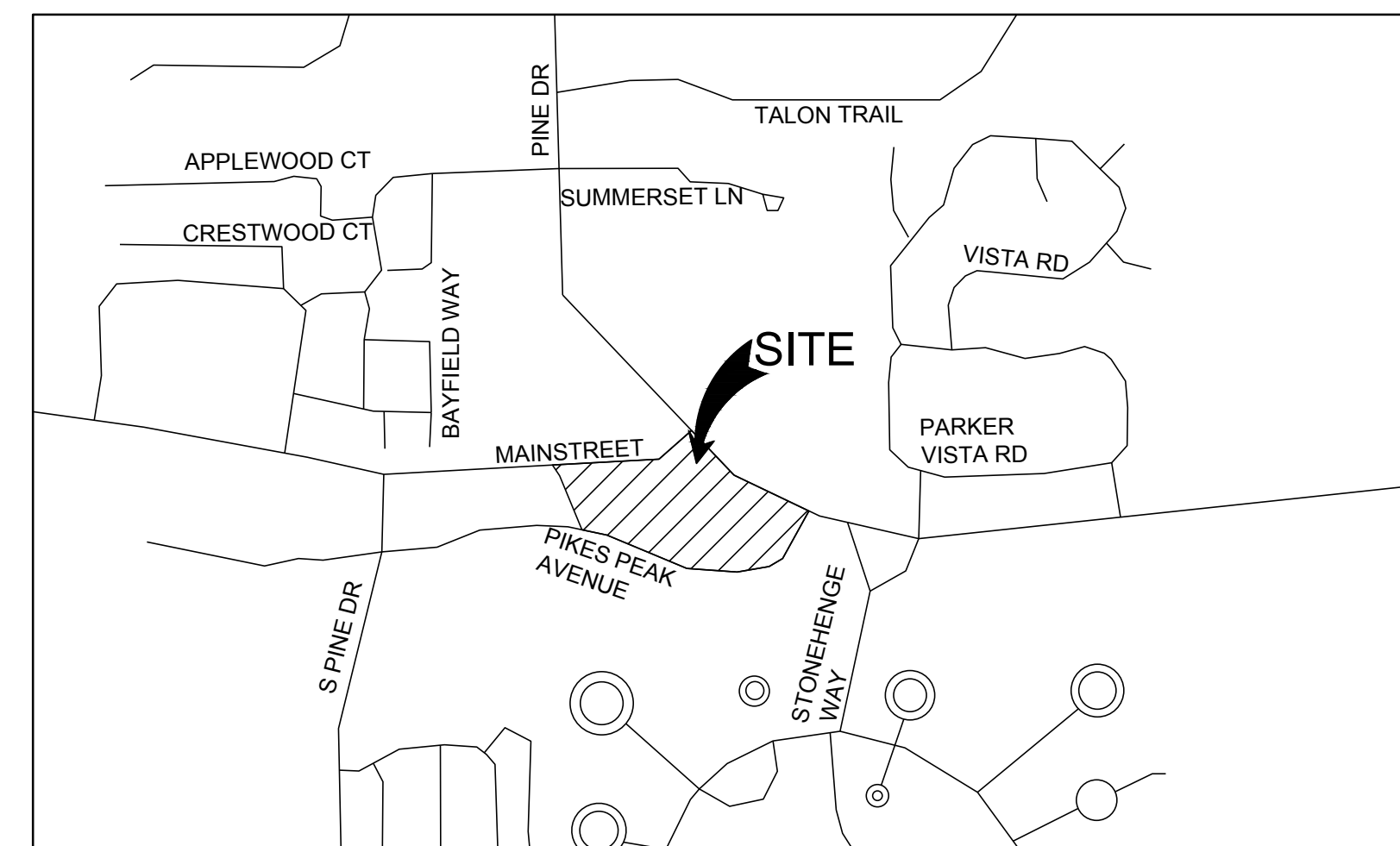


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CONSTRUCTION PLANS
FOR
PARKER TOWN HALL

LOT 1, PARKER TOWN HALL, 3RD AMENDMENT, SUBDIVISION EXEMPTION PLAT, ACCORDING TO THE PLAT THEREOF RECORDED
FEBRUARY 5, 2015 AT RECEPTION NO. 2015007140, COUNTY OF DOUGLAS, STATE OF COLORADO



VICINITY MAP
NTS

Sheet List Table	
Sheet Number	Sheet Title
C-500	WATER AND SANITARY COVER SHEET
C-501	WATER AND SANITARY NOTES AND LEGEND
C-511	UTILITY PLAN
C-531	SANITARY PLAN AND PROFILE
C-551	WATER AND SANITARY DETAILS
C-552	WATER AND SANITARY DETAILS

ENGINEER'S STATEMENT

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF COLORADO.

MEGAN HUERTER VOGT
REGISTERED PROFESSIONAL ENGINEER
COLORADO NO. 47666

MATERIALS LIST	
ITEM	QUANTITY
6" PVC	203 LF
2" TYPE K COPPER	121 LF
6" DIP	233 LF
2" DIP	129 LF
FIRE HYDRANT	3
6" CLEANOUT	3
8"X6" TEE	2
8" GATE VALVE	2
2" CURB STOP	1

FIRE FLOW BLOCK

TOTAL FIRE FLOW REQUIRED FOR THIS SITE IS 2,625 GPM MINIMUM @ 20 PSI RESIDUAL PRESSURE
THIS FLOW MUST BE PROVIDED FROM A MINIMUM OF 3 FIRE HYDRANTS INDIVIDUALLY. EACH FIRE HYDRANT MUST SUPPLY 1500 GPM MINIMUM @ 20 PSI RESIDUAL PRESSURE
CODE USED FOR ANALYSIS: 2018 IFC WITH APPENDICES B, C AND D
OCCUPANCY GROUPS: A3, B
CONSTRUCTION TYPE: IIb
FIRE FLOW CALCULATION AREA: 64,656 SF
TOTAL BUILDING AREA: 64,656 SF
BUILDING IS ANTICIPATED TO BE CONSTRUCTED WITH AN AUTOMATED FIRE SPRINKLER SYSTEM (NFPA 13) SO FIRE FLOW REQUIREMENT HAS BEEN REDUCED BY 50%.

THE DISTRICT INSPECTOR MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO START OF CONSTRUCTION. CALL PARKER WATER AND SANITATION DISTRICT AT 303-841-4627. THE DISTRICT WILL PROVIDE PERIODIC INSPECTIONS OF THE 24 HOUR NOTICE TO THE INSPECTOR IS REQUIRED FOR SCHEDULING INSPECTIONS. ANY WORK ACCOMPLISHED WITHOUT THE APPROVAL OF THE INSPECTOR WILL BE SUBJECT TO REJECTION.
REVIEWED FOR CONFORMANCE TO PARKER WATER AND SANITATION DISTRICT STANDARDS.
BY: _____
(DISTRICT REPRESENTATIVE)
DATE: _____
APPROVED FOR CONSTRUCTION:
PARKER WATER AND SANITATION DISTRICT
BY: _____
(DISTRICT ENGINEER)
DATE: _____

CONTACTS:

OWNER'S REPRESENTATIVE TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546	CIVIL ENGINEER S.A. MIRO INC. MEGAN H. VOGT 4582 SOUTH ULSTER STREET SUITE 750 DENVER, CO 80237 303-741-3737	SURVEYOR AZTEC CONSULTANTS, INC. DANIEL E. DAVIS 300 EAST MINERAL AVE, SUITE 1 LITTLETON, CO 80122 P: (303)-713-1888
SOUTH METRO FIRE DISTRICT 9195 E. MINERAL AVENUE CENTENNIAL, CO 80112 P: (720) 989-2244	PARKER WATER & SANITATION DISTRICT 18100 WOODMAN ROAD PARKER, CO 80134 P: (303) 841-4627	TOWN OF PARKER 20120 E. MAIN STREET PARKER, CO 80138 P: (303) 840-9546

BASIS OF BEARING:

BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°29'07" EAST, A DISTANCE OF 2,648.44 FEET.

BENCHMARK:

1-1/4" ALUMINUM ALLOY CAP SET ON NUMBER 5 REBAR STAMPED "AZTEC CP 51" LOCATED IN THE NORTH PARKING LOT OF CORNERSTONE PARK IN LITTELTON, IN THE GRASSY ISLAND 80.3 FEET EAST OF THE EAST EDGE THE GRAVEL TRAFFIC CIRCLE, AND 8 FEET NORTH OF THE EDGE OF GRAVEL
ELEVATION=5399.27 (NAVD 88).

ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO WATER UTILITY STANDARDS. THE NUMBER AND LOCATIONS OF THE FIRE HYDRANTS AS SHOWN ON THE OVERALL UTILITY PLAN ARE CORRECT AS SPECIFIED BY THE TOWN OF PARKER, COMMUNITY DEVELOPMENT DEPARTMENT.

Fire Code Official or Designated Representative _____ Date _____
(NOTE - UNDERGROUND FIRE LINE (UFL) SUBMITTAL DOCUMENTS MUST MEET THE REQUIREMENTS OF NFPA 24 WHEN SUBMITTING FOR REVIEW.)

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Expansion

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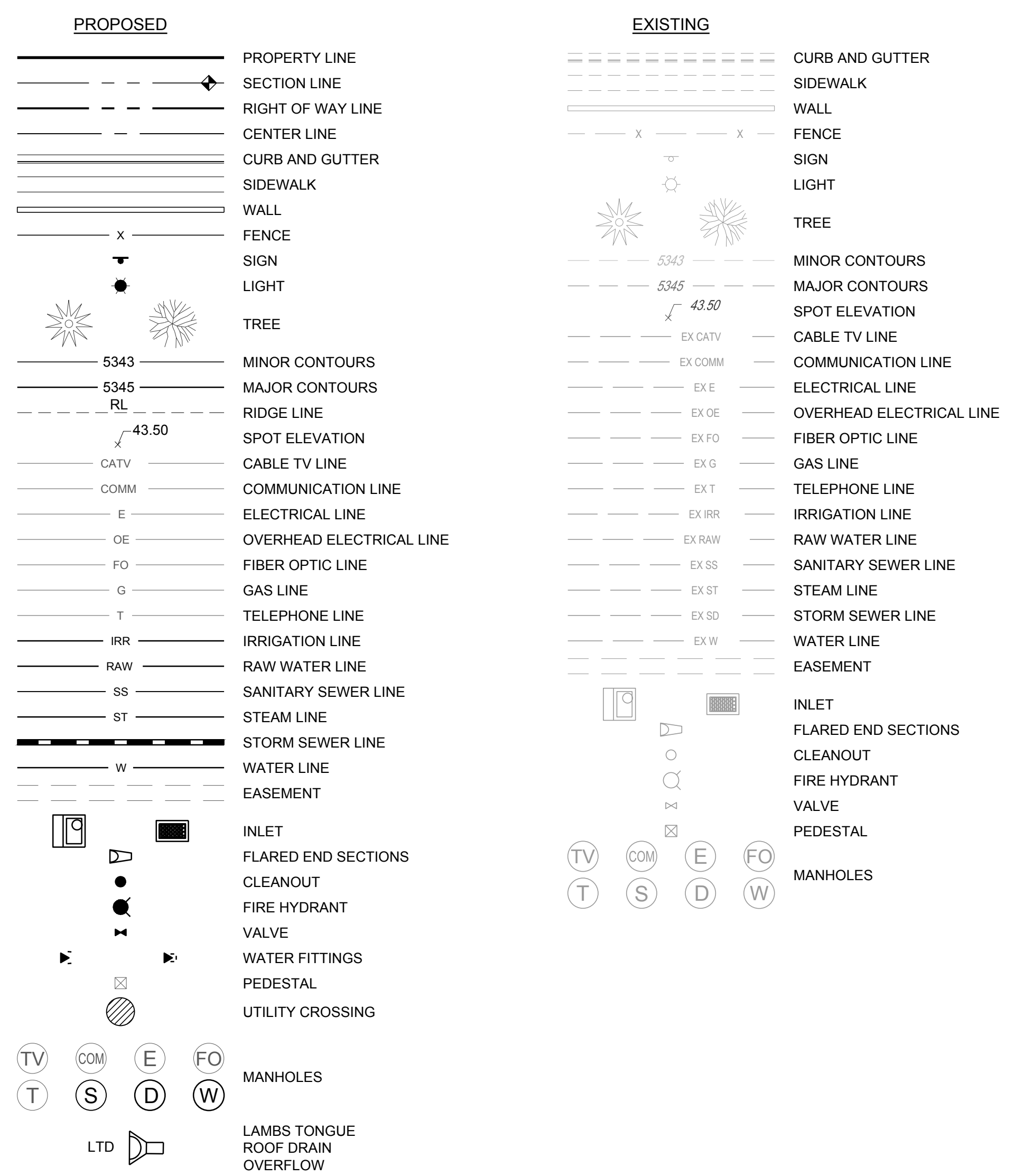
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Denver, CO 80202
Telephone: 303-296-3034



MASTER LEGEND:



PWSD GENERAL NOTES:

- ALL MATERIALS AND WORKMANSHIP SHALL BE IN CONFORMANCE WITH THE PWSD ENGINEERING STANDARDS AND SPECIFICATIONS, LATEST VERSION.
- THE CONTRACTOR SHALL CONTACT ALL APPROPRIATE UTILITY COMPANIES AND THE TOWN OF PARKER, DOUGLAS COUNTY AND OTHER NEIGHBORING CITIES AND TOWNS PRIOR TO THE BEGINNING OF ANY CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ANY EXISTING UTILITY (INCLUDING DEPTHS) WHICH MAY CONFLICT WITH THE PROPOSED CONSTRUCTION. ALL EXISTING UTILITIES SHALL BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. DAMAGED UTILITIES SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE.
- ALL ITEMS SHOWN ON THE PLANS AS EXISTING ARE SHOWN IN APPROXIMATE LOCATIONS ONLY. THE ACTUAL LOCATIONS MAY VARY FROM THE PLANS, ESPECIALLY IN THE CASE OF UNDERGROUND UTILITIES. WHENEVER THE CONTRACTOR DISCOVERS A DISCREPANCY IN LOCATIONS CONTACT THE PWSD IMMEDIATELY.
- PWSD ENGINEER AND OTHER APPROVING AGENCIES ARE TO BE NOTIFIED 48 HOURS PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL PERMITS NECESSARY TO PERFORM THE PROPOSED WORK.
- ALL CONCRETE SHALL BE A MINIMUM OF CLASS A, 6 SACK, TYPE II, 3000-POUND COMPRESSION STRENGTH.
- THE DESIGN ENGINEER SHALL SUBMIT ONE (1) SET OF "AS-BUILT" DRAWINGS AND ELECTRONIC FILES TO THE DISTRICT ENGINEER FOR APPROVAL BY DISTRICT. AFTER APPROVAL HAS BEEN GRANTED BY THE DISTRICT ENGINEER, FULL SIZE PRINTS SHALL BE TRANSMITTED TO THE PWSD OFFICE.
- NO WORK SHALL BE BACKFILLED UNTIL THE CONSTRUCTION HAS BEEN INSPECTED AND APPROVED FOR BACKFILLING BY THE DISTRICT ENGINEER OR REPRESENTATIVE OF THE DISTRICT ENGINEER. ALL BACKFILL MATERIAL SHALL BE COMPACTED TO 95% STANDARD PROCTOR DENSITY. COMPACTION TESTS MUST BE SUBMITTED TO DISTRICT ENGINEER PRIOR TO PROBATIONARY ACCEPTANCE.
- TRENCHES SHALL BE EXCAVATED AND THE PIPE EXPOSED FOR THE INSPECTION AT ANY LOCATION ON THE PROJECT IF SO ORDERED BY THE INSPECTOR.
- THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR THE PROPER FUNCTIONING OF LINES (WATER AND DEPTHS) FOR UP TO TWO (2) YEARS FROM THE DATE OF PROBATIONARY ACCEPTANCE OF THE LINES BY THE DISTRICT. ANY MALFUNCTION DURING THIS PERIOD OF GUARANTEE SHALL BE REMEDIED BY THE CONTRACTOR TO THE SATISFACTION OF THE DISTRICT ENGINEER AT NO EXPENSE TO THE DISTRICT.
- SURFACE GRADES ARE TO BE WITHIN PLUS OR MINUS ONE FOOT OF FINISHED GRADE AND VERIFICATION OF COMPACTION RESULTS OBTAINED PRIOR TO THE INSTALLATION OF WATER AND SEWER LINES. RESULTS MUST BE SUBMITTED TO THE DISTRICT ENGINEER.
- ALL SERVICES WILL BE PERMANENTLY MARKED ON CURB FACE AS FOLLOWS:
"X" FOR SANITARY SERVICE SEWERS
"Y" FOR WATER SERVICES
- NO TREES ALLOWED IN EASEMENTS OR WITHIN 8' OF WATER OR SANITARY SEWER MAINS IN RIGHT OF WAY.
- THE CONTRACTOR SHALL HAVE IN HIS POSSESSION AT ALL TIMES ONE (1) SIGNED COPY OF THE PLANS WHICH HAVE BEEN APPROVED BY THE PWSD AND THE DISTRICT ENGINEER AND ONE (1) COPY OF THE LATEST PWSD SPECIFICATION MANUAL.

PWSD WATER SYSTEM NOTES:

- ALL MATERIALS AND WORKMANSHIP SHALL BE IN CONFORMANCE WITH THE PWSD ENGINEERING STANDARDS AND SPECIFICATIONS, LATEST REVISION.
- ALL WATER MAINS SHALL BE PER PWSD MATERIALS SPECIFICATIONS. ALL MATERIALS IN CONTACT WITH POTABLE WATER SHALL BE SUITABLE FOR CHLORINATED AND CHLORAMINATED WATER.
- ALL BENDS, PLUGS, REDUCERS, AND FIRE HYDRANTS TO BE RODDED OR MEGA-LUGGED. ALL FITTINGS SHALL BE WRAPPED PER PWSD MATERIALS SPECIFICATIONS.
- THERE SHALL BE A MINIMUM COVER OF 4.5 FEET OVER ALL WATER MAINS.
- FIRE HYDRANTS SHALL CONFORM TO AWWA C-502 "DRY BARREL FIRE HYDRANTS" PIPE HYDRANT ASSEMBLIES PER PWSD SPECIFICATIONS.
- ALL BENDS, TEES, FIRE HYDRANTS, BLOW-OFFS, AND PLUGS AT DEAD-END MAINS SHALL BE PROTECTED FROM THRUST WITH CONCRETE THRUST BLOCKS.
- PRECAUTIONS SHALL BE TAKEN TO PROTECT THE INTERIOR OF PIPES, FITTINGS, AND VALVES AGAINST CONTAMINATION. ALL OPENINGS IN THE PIPELINE SHALL BE CLOSED WITH WATERTIGHT PLUGS WHEN PIPE LAYING IS STOPPED AT THE CLOSE OF THE DAY'S WORK OR FOR OTHER REASONS, SUCH AS REST BREAKS OR MEAL PERIODS.
- VALVES IN STREETS ARE TO BE LOCATED AT PROPERTY LINE EXTENSIONS EXCEPT FOR TAPPING TEES; WHERE AN ADDITIONAL VALVE SHALL BE PLACED ON THE TAPPING TEE. OTHER LOCATIONS SHOWN ON THE PLANS.
- WHEN NECESSARY TO LOWER OR RAISE WATER LINES AT STORM DRAINS AND OTHER UTILITY CROSSING, A MINIMUM CLEARANCE OF 1.5 FEET SHALL BE MAINTAINED BETWEEN OUTSIDE OF PIPES.
- THE CONTRACTOR SHALL NOTIFY THE PWSD AT LEAST 48 HOURS PRIOR TO ANY CONSTRUCTION. IF WORK IS SUSPENDED FOR ANY PERIOD OF TIME AFTER INITIAL START-UP, THE CONTRACTOR MUST NOTIFY THE DISTRICT ENGINEER 48 HOURS PRIOR TO RE-START.
- PIPE BEDDING SHALL BE A CLEAN, WELL-GRADED SAND OR SQUEEGEE SAND IN ACCORDANCE WITH PWSD STANDARDS AND SPECIFICATIONS, LATEST REVISION.
- THE CONTRACTOR SHALL NOTIFY THE PUBLIC UTILITY COMPANIES AND DETERMINE THE LOCATION OF ALL EXISTING UNDERGROUND UTILITIES PRIOR TO PROCEEDING WITH THE EXCAVATION. ALL WORK PERFORMED IN THE AREA OF THE PUBLIC UTILITIES SHALL BE PERFORMED ACCORDING TO THE REQUIREMENTS OF THESE AGENCIES.
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED IN ACCORDANCE WITH THE SOILS REPORTS AND COMPACTION TEST RESULTS SUBMITTED TO THE DISTRICT ENGINEER PRIOR TO PROBATIONARY ACCEPTANCE.
- AT LEAST FIVE (5) DAYS PRIOR TO THE START OF CONSTRUCTION, A PRE-CONSTRUCTION MEETING WILL BE HELD AT THE OFFICE OF THE DISTRICT ENGINEER AND ATTENDED BY THE CONTRACTOR, DESIGN ENGINEER, AND REPRESENTATIVE OF OTHER APPROVING AGENCIES.
- CHLORINATION AND FLUSHING: ALL WATER MAINS SHALL BE INSTALLED AND CHLORINATED PER PWSD SPECIFICATIONS. CHLORINATION OF FINISHED PIPELINE COMPLETED BEFORE HYDROSTATIC TESTING.
- HYDROSTATIC TESTING: ALL WATER MAINS SHALL BE TESTED PER THE REQUIREMENTS OF THE PWSD SPECIFICATIONS UP TO MINIMUM OF 150 PSI IN THE PRESENCE OF A PWSD INSPECTOR/ENGINEER.

PWSD SANITARY SEWER SYSTEM NOTES:

- ALL MATERIALS AND WORKMANSHIP SHALL BE IN CONFORMANCE WITH THE PWSD ENGINEERING STANDARDS AND SPECIFICATIONS, LATEST VERSION.
- ALL SEWER MAINS SHALL BE PVC, ASTM D-3034, SDR 35 OR APPROVED EQUAL. ALL SEWER MAIN SIZE IS 8" IN DIAMETER UNLESS OTHERWISE NOTED.
- SEWER LINES SHALL BE INSTALLED 5.0 FEET SOUTH OR WEST OF STREET CENTERLINE. UNLESS OTHERWISE SHOWN ON PLANS, SEWER LINES SHALL BE 10.00 FEET FROM WATER LINES EXCEPT WHEN CROSSING EACH OTHER. SEWER LINES WHICH CROSS LESS THAN 1.50 FEET VERTICALLY FROM THE WATER MAIN SHALL BE ENCASED IN CONCRETE AS PER PWSD SPECIFICATIONS. USE RUBBER GASKETS FOR PVC ENCASMENT.
- SEWER RIM ELEVATIONS SHOWN ARE APPROPRIATE ONLY AND ARE NOT TO BE TAKEN AS FINAL ELEVATIONS. RING AND COVER SHALL BE SET IN CENTERED CONCRETE RINGS WITH RAM-NECK FOR ADJUSTMENT TO MATCH FINAL PAVEMENT ELEVATIONS.
- THE CONTRACTOR AND SURVEY CREW SHALL VERIFY ELEVATIONS OF EXISTING SEWER LINES AND MANHOLES TO BE TIED TO PRIOR TO CONSTRUCTION OR STAKING OF SANITARY SEWER.
- NO UNDERDRAIN SYSTEM WILL BE ALLOWED TO BE PLACED IN MAINLINE OR SEWER SERVICE TRENCHES.
- SEWER SERVICE TEES FOR EACH UNIT SHALL BE STAKED BY A SURVEY CREW AND FURNISHED AND INSTALLED BY THE CONTRACTOR. THE CONTRACTOR SHALL FURNISH TO THE ENGINEER "AS CONSTRUCTED" LOCATION OF TEES. SEWER WYES SHALL BE USED RATHER THAN TEES.
- ALL MANHOLES SHALL BE 48-INCHES IN DIAMETER WITH 24-INCH RING AND COVER. ECCENTRIC CONE UNLESS OTHERWISE SPECIFIED. ALL MANHOLES SHALL HAVE SHAPED INVERTS.
- PIPE BEDDING SHALL BE CLASS "B" AND SHALL CONFORM TO ASTM C-33 OR D-448 GRADATION NO. 6 OR NO. 67.
- AT LEAST FIVE (5) DAYS PRIOR TO THE START OF CONSTRUCTION, A PRE-CONSTRUCTION MEETING WILL BE HELD AT THE OFFICE OF THE DISTRICT ENGINEER AND ATTENDED BY THE CONTRACTOR AND REPRESENTATIVES OF THE OTHER APPROVING AGENCIES. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT THE DISTRICT ENGINEER TO SCHEDULE THIS MEETING.
- THE CONTRACTOR SHALL HAVE IN HIS POSSESSION AT ALL TIMES ONE (1) SIGNED COPY OF THE PLANS WHICH HAVE BEEN APPROVED BY THE PWSD AND THE DISTRICT ENGINEER AND ONE (1) COPY OF THE LATEST PWSD SPECIFICATION MANUAL.
- ALL SEWER LINES SHALL BE TESTED IN ACCORDANCE WITH THE PWSD STANDARDS AND SPECIFICATIONS PRIOR TO ACCEPTANCE OR ANY CONNECTION TO AN EXISTING SEWER LINE.
- PRIOR TO STARTING WORK WHERE SEWER MAIN IS TO BE CONNECTED TO EXISTING PWSD SEWER SYSTEMS, THE NEAREST MANHOLE TO THE POINT OF THE IN-SHALL BE PLUGGED WITH A PLUMBER'S PLUG ON THE INLET SIDE BY THE CONTRACTOR. THIS PLUG SHALL REMAIN IN PLACE UNTIL FINAL PROBATIONARY ACCEPTANCE BY PWSD. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PUMPING AND CLEANING THESE MANHOLES AND REMOVING THE PLUG WHEN INSTRUCTED BY THE DISTRICT.
- PRIOR TO PROBATIONARY ACCEPTANCE WALK-THROUGH, THE CONTRACTOR SHALL JET CLEAN THE ENTIRE SANITARY SEWER SYSTEM AND PUMP OUT AT THE PLUGGED MANHOLE.

GENERAL NOTES:

- THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS AT AND ADJACENT TO THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- THE CONTRACTOR SHALL PERFORM ALL WORK NECESSARY TO COMPLETE THE WORK SHOWN ON THE PLANS OR DESCRIBED IN THE SPECIFICATIONS IN A SATISFACTORY MANNER. THE CONTRACTOR SHALL FURNISH ALL MATERIALS, EQUIPMENT, TOOLS, LABOR, AND INCIDENTALS TO COMPLETE THE WORK.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AT THEIR EXPENSE TO COMPLETE ALL GRADING, UTILITY WORK AND INSTALLATION OF EROSION CONTROL MEASURES.
- THE CONTRACTOR SHALL COORDINATE WORK WITH ALL ADJACENT PROJECTS.
- DURING CONSTRUCTION IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PROTECT EXISTING UTILITY LINES SHOWN ON THESE PLANS AND THOSE UTILITY LINES WHICH MAY NOT BE SHOWN ON THESE PLANS.
- EXISTING SITE INFORMATION SHOWN ON THESE DRAWINGS IS BASED UPON AVAILABLE SURVEY BY AZTEC CONSULTANTS, INC. DATED 08/11/2022. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF THE COMPLETENESS OR ACCURACY OF THE EXISTING CONDITIONS PRESENTED. THE CONTRACTOR SHALL INSPECT THE SITE TO ASSESS EXISTING CONDITIONS, AND SATISFY THEMSELVES THAT THE DOCUMENTS ACCURATELY REPRESENT THE SITE CONDITIONS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES FOUND DURING INITIAL INSPECTIONS, OR SUBSEQUENTLY, SHALL BE REPORTED TO THE ENGINEER IMMEDIATELY FOR RESOLUTION RELATIVE TO THE INTENDED CONSTRUCTION.
- THE EXISTING UTILITIES SHOWN ON THESE PLANS HAVE BEEN LOCATED USING FIELD SURVEY DATA LOCATING MANHOLES, WATER VALVES, ELECTRICAL EQUIPMENT, AND OTHER FEATURES FOUND AT THE SURFACE. NO SUBSURFACE EXPLORATION HAS BEEN DONE TO VERIFY EXACT LOCATION OF EXISTING UTILITIES. THE CONTRACTOR SHALL POPTHOLE EXISTING UTILITIES PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICT PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT AND DELAY RELATED TO UNVERIFIED EXISTING CONDITIONS, WHERE THE CONTRACTOR FINDS CONFLICTS OR DISCREPANCIES. THEY SHALL BE REPORTED IMMEDIATELY TO THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY APPROPRIATE ENTITIES FOR UTILITY LOCATES AND START OF CONSTRUCTION PRIOR TO ANY WORK BEING STARTED.
- THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH ALL AFFECTED PARTIES PRIOR TO ANY OUTAGES OR SHUTDOWN OF SERVICE.
- ALL QUANTITIES SHOWN ON THESE PLANS ARE SHOWN FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL QUANTITIES PRIOR TO CONSTRUCTION AND ORDERING OF ANY MATERIALS.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL FOLLOW/VERIFY THE INTENT OF THE DESIGN AT ALL LOCATIONS OF TIE-IN TO EXISTING FEATURES.

DEMOLITION NOTES:

- UNLESS OTHERWISE NOTED, EXISTING UTILITIES AND STRUCTURES SHALL REMAIN AND BE PROTECTED IN PLACE.
- THE CONTRACTOR SHALL PROTECT ALL ADJACENT IMPROVEMENTS (BUILDINGS, ROADWAYS, FENCES, PARKING LOTS, UTILITIES, ETC.) FROM DAMAGE AND EROSION. ALL DISTURBED AREAS OUTSIDE OF THE LIMITS OF WORK SHALL BE RESTORED TO THEIR ORIGINAL CONDITION.

HORIZONTAL CONTROL NOTES:

- BEARINGS SHOWN HEREON ARE GRID BEARINGS DERIVED FROM GPS OBSERVATIONS BASED UPON THE COLORADO COORDINATE SYSTEM OF 1983 CENTRAL ZONE (NAD 83, 2011), REFERENCED TO THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 23, TOWNSHIP 6 SOUTH, RANGE 66 WEST, SIXTH PRINCIPAL MERIDIAN, BEING MONUMENTED AT THE NORTHWEST CORNER OF SAID SECTION 23 BY A 3-1/4" DIAMETER ALUMINUM CAP STAMPED IN PART "AZTEC CONSULTANTS, INC. LS 17666 2003" AND AT THE WEST QUARTER CORNER OF SAID SECTION 23 BY A 2-3/4" ALUMINUM CAP STAMPED IN PART "LS 13155 1993", TAKEN TO BEAR NORTH 00°25'07" EAST, A DISTANCE OF 2,648.44 FEET.
- THE CONTRACTOR SHALL VERIFY THAT BUILDING GRID LOCATIONS ARE CONSISTENT WITH LATEST ARCHITECTURAL PLANS PRIOR TO CONSTRUCTION.

PAVING NOTES:

- THE CONTRACTOR SHALL ADJUST EXISTING AND PROPOSED MANHOLES, WATER VALVES AND OTHER SURFACE APPURTENANCES WITHIN THE LIMITS OF WORK TO FINISHED GRADE.
- THE CONTRACTOR SHALL REFER TO ARCHITECTURAL/LANDSCAPE PLANS FOR CONCRETE SCORING PATTERNS, COLOR AND FINISHING.
- PROPOSED PAVEMENT DESIGN IS BASED UPON THE GEOTECHNICAL REPORT BY KUMAR & ASSOCIATES DATED 01/17/2023. S.A. MIRO, INC. MAKES NO WARRANTIES OR CERTIFICATION OF PAVEMENT PERFORMANCE.

UTILITY NOTES:

- LOCATION OF ALL EXISTING UTILITIES (PRIVATE AND PUBLIC) SHALL BE IDENTIFIED AND VERIFIED BY THE CONTRACTOR PRIOR TO MOBILIZATION, CONSTRUCTION AND ORDERING OF MATERIALS. IF ANY CONFLICTS OR DISCREPANCIES ARE FOUND THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY. THE CONTRACTOR SHALL BEAR THE FULL COST OF REMOVAL, REPLACEMENT, REPAIR AND DELAYS RELATED TO ANY AND ALL UNVERIFIED EXISTING CONDITIONS.
- ALL UTILITY LENGTHS AND QUANTITIES SHOWN ON THESE PLANS ARE FOR BIDDING PURPOSES ONLY. CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND QUANTITIES PRIOR TO ORDERING OF ANY MATERIALS.
- PIPE LENGTHS ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE.
- WHERE FLARED END SECTIONS ARE REQUIRED, THE LENGTH OF PIPE INCLUDES THE FLARED END SECTION.
- ALL UTILITY WORK SHALL BE COMPLETED PRIOR TO ANY PAVING.
- CONTRACTOR SHALL SURVEY THE LOCATION AND ELEVATION OF ALL INSTALLED UTILITIES PRIOR TO TRENCH BACKFILL, AND PROVIDE SURVEY INFORMATION TO THE ENGINEER UPON COMPLETION OF THE PROJECT FOR AS-BUILT DOCUMENTATION.

GRADING NOTES:

- SITE GRADING (EXCAVATION, EMBANKMENTS, AND COMPACTION) SHALL CONFORM TO THE RECOMMENDATIONS OF THE LATEST GEOTECHNICAL REPORT FOR THIS PROPERTY.
- SPOT ELEVATIONS MAY BE ABBREVIATED FOR CLARITY. REFER TO THE ADJACENT CONTOUR LABELS FOR FULL ELEVATIONS.
- MAXIMUM PROPOSED SLOPES SHALL BE 4:1 UNLESS NOTED OTHERWISE.
- SPOT ELEVATIONS ARE TO FLOWLINE UNLESS NOTED OTHERWISE.
- PROPOSED GRADING SHALL MEET AND MATCH EXISTING GRADE AT LIMITS OF DISTURBANCE.
- PROPOSED GRADING AS SHOWN REFLECTS PLANNED FINISHED GRADE. THE CONTRACTOR SHALL ACCOUNT FOR PAVEMENT DEPTH, SUBGRADE REQUIREMENTS, OVEREXCAVATION, TOPSOIL, AND ANY PLANNED SURFACING CONSTRUCTION.
- MAXIMUM TEMPORARY SLOPES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEERS RECOMMENDATIONS AND IN COMPLIANCE WITH OSHA REQUIREMENTS.
- THE CONTRACTOR SHALL CONFORM TO OVEREXCAVATION REQUIREMENTS DETAILED WITHIN THE PROJECT SPECIFICATIONS AND GEOTECHNICAL REPORT. OVEREXCAVATION REQUIREMENTS ARE NOT DEPICTED ON THE CIVIL PLANS.

Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:

WATER AND SANITARY NOTES AND LEGEND

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80202
Telephone: 303-294-9448

Civil Engineer

S.A. Mro, Inc.
4582 S. Usher Street #750
Denver, CO 80227
Telephone: 303-741-3737

Landscape Architect

Wenk Associates
1130 31st Street #101
Denver, CO 80202
Telephone: 303-628-0000

Structural Engineer

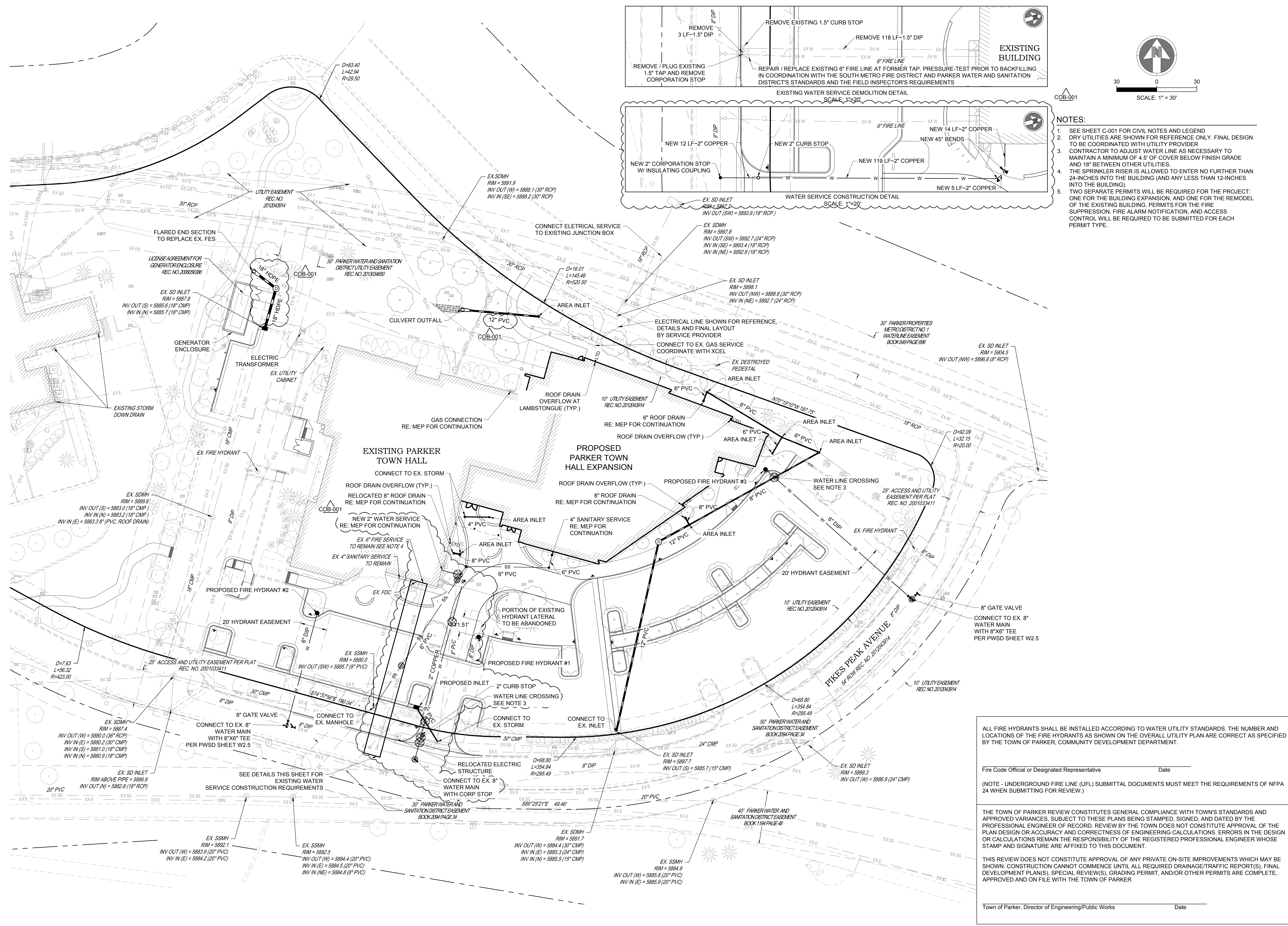
KL&A, Inc.
1717 Washington Ave.
Golden, CO 80401
Telephone: 303-384-9910

Mechanical + Plumbing Engineer

The Ballard Group, Inc.
2525 S. Wadsworth Blvd. #200
Lawrence, CO 80227
Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



- NOTES:**
- SEE SHEET C-001 FOR CIVIL NOTES AND LEGEND
 - DRY UTILITIES ARE SHOWN FOR REFERENCE ONLY. FINAL DESIGN TO BE COORDINATED WITH UTILITY PROVIDER
 - CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES
 - THE SPRINKLER RISER IS ALLOWED TO ENTER NO FURTHER THAN 24-INCHES INTO THE BUILDING (AND ANY LESS THAN 12-INCHES INTO THE BUILDING)
 - TWO SEPARATE PERMITS WILL BE REQUIRED FOR THE PROJECT: ONE FOR THE BUILDING EXPANSION, AND ONE FOR THE REMODEL OF THE EXISTING BUILDING. PERMITS FOR THE FIRE SUPPRESSION, FIRE ALARM NOTIFICATION, AND ACCESS CONTROL WILL BE REQUIRED TO BE SUBMITTED FOR EACH PERMIT TYPE.

ALL FIRE HYDRANTS SHALL BE INSTALLED ACCORDING TO WATER UTILITY STANDARDS. THE NUMBER AND LOCATIONS OF THE FIRE HYDRANTS AS SHOWN ON THE OVERALL UTILITY PLAN ARE CORRECT AS SPECIFIED BY THE TOWN OF PARKER, COMMUNITY DEVELOPMENT DEPARTMENT.

Fire Code Official or Designated Representative _____ Date _____

(NOTE - UNDERGROUND FIRE LINE (UFL) SUBMITTAL DOCUMENTS MUST MEET THE REQUIREMENTS OF NFPA 24 WHEN SUBMITTING FOR REVIEW.)

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER

Town of Parker, Director of Engineering/Public Works _____ Date _____



Issue	Date
SCHEMATIC DESIGN	01 MAY 2023
DESIGN DEVELOPMENT	20 OCT 2023
PERMIT / GMP SET	17 MAY 2024

COB-001 19 JULY 2024

Project Number: 22-139
 Drawn By:
 Reviewed By:
 Approved By:

UTILITY PLAN

Parker Town Hall Expansion

Parker, CO
22-093

Town of Parker

20120 Mainstreet
Parker, CO 80138

Architect

AndersonMasonDale Architects, P.C.
3198 Speer Boulevard
Denver, CO 80202
Telephone: 303-294-9448

Civil Engineer

S.A. Miro, Inc.
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Landscape Architect

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Structural Engineer

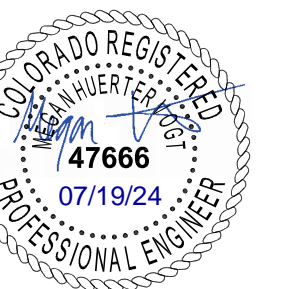
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Lakewood, CO 80227
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Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



Issue Date

Schematic Design 01 MAY 2023
Design Development 20 OCT 2023
Permit / GMP Set 17 MAY 2024

COB-001 19 JULY 2024

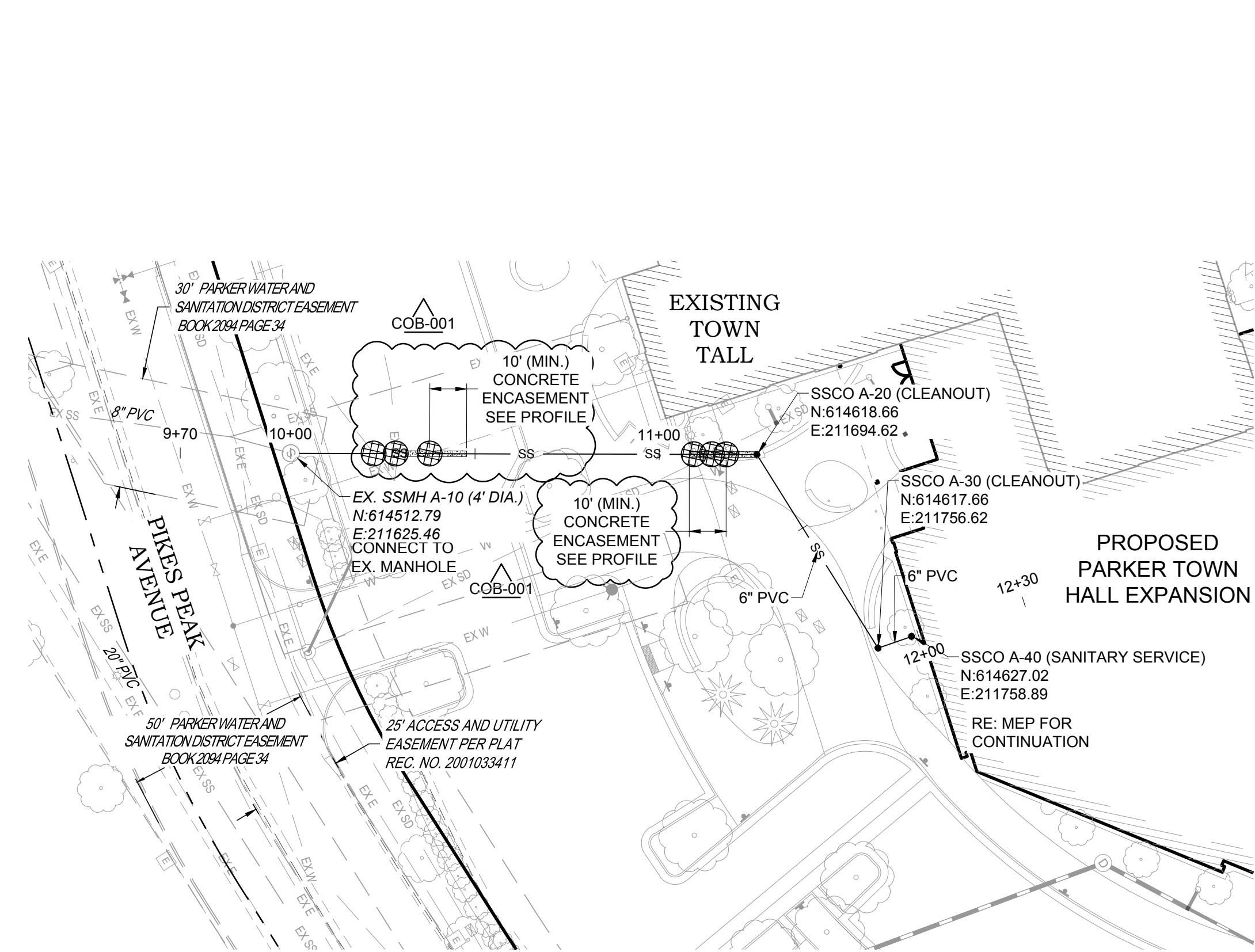
Project Number: 22-139

Drawn By:

Reviewed By:

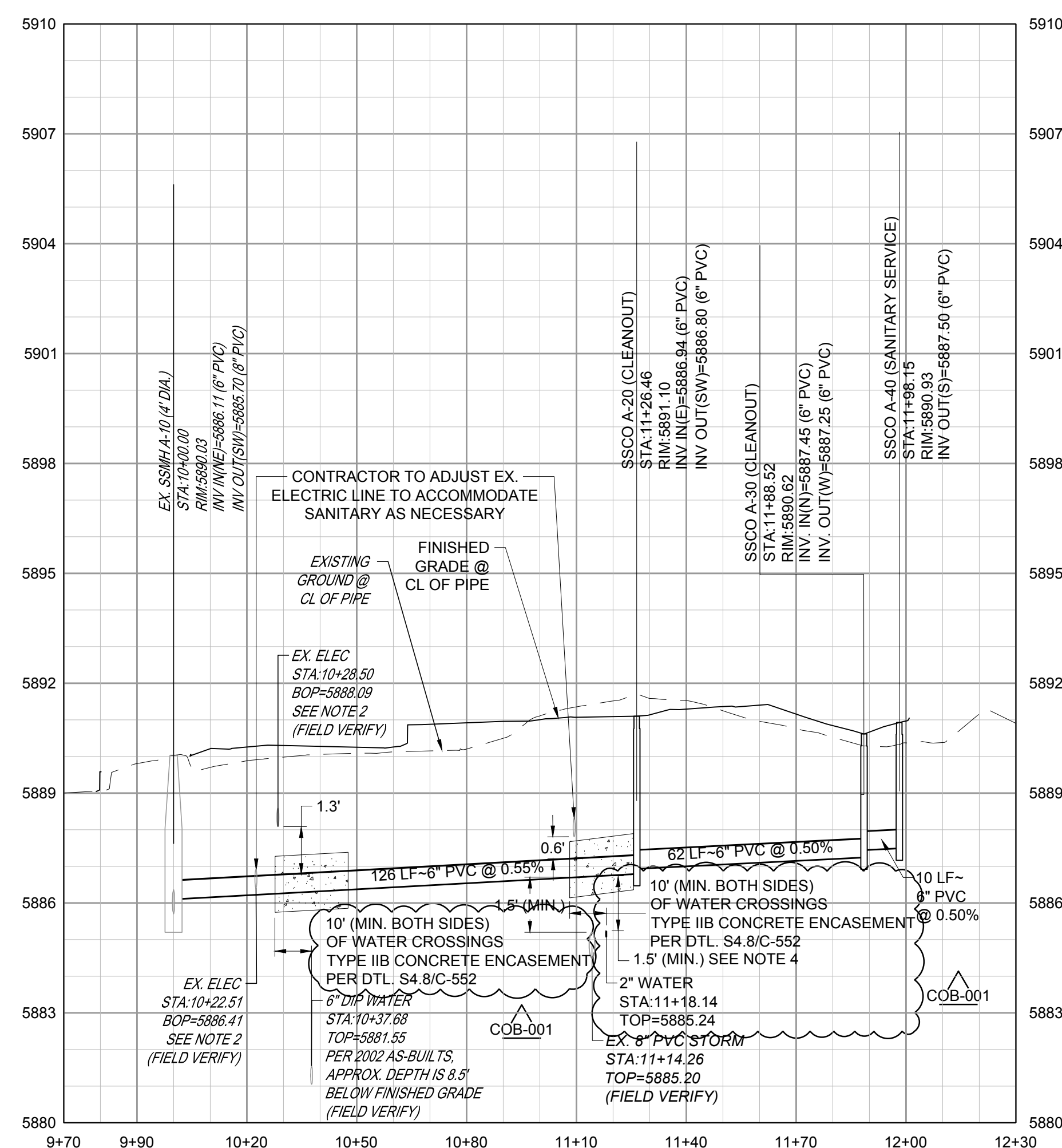
Approved By:

SANITARY PLAN AND PROFILE

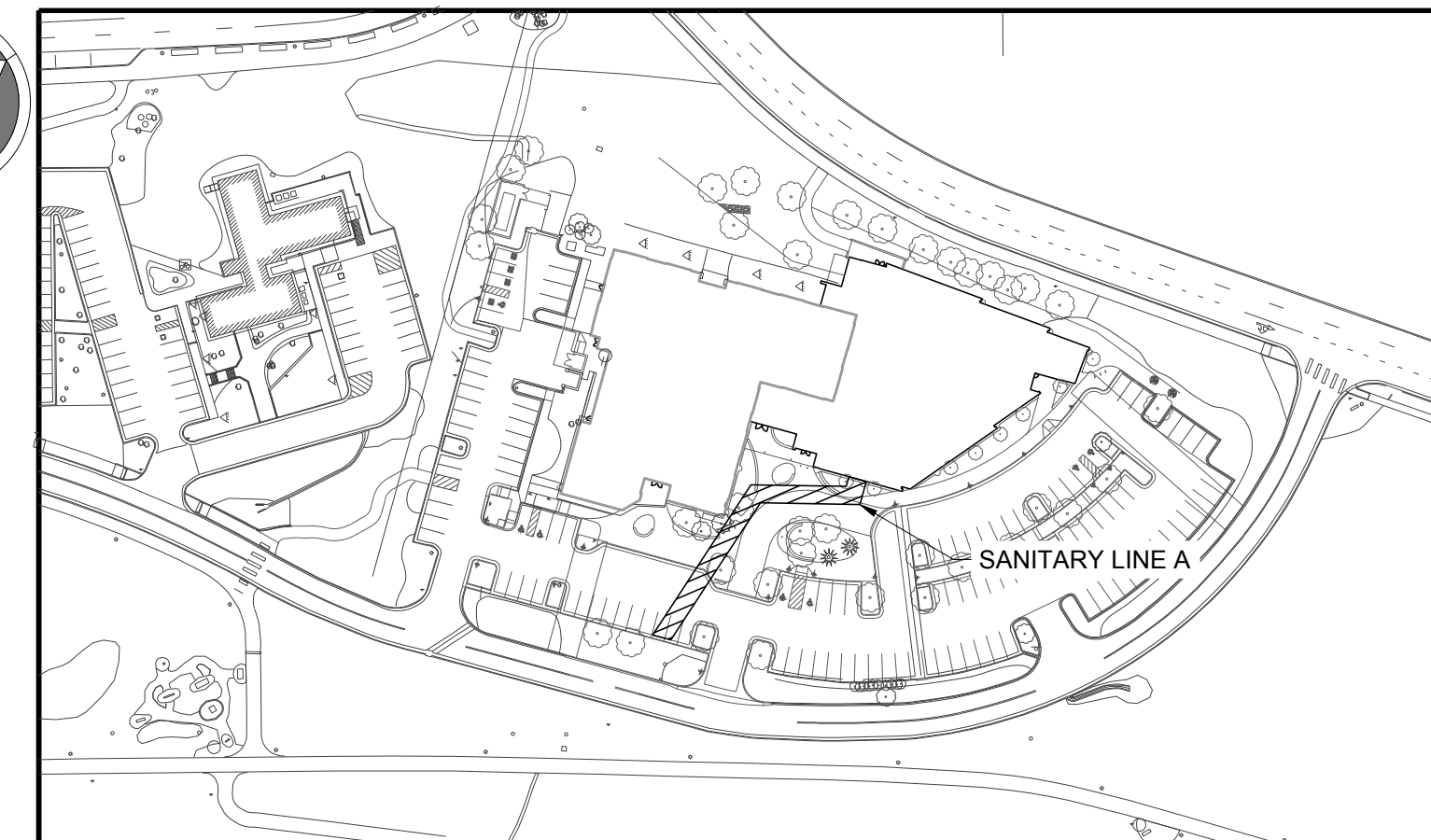
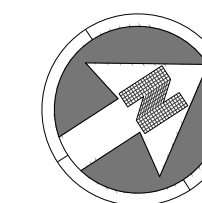


SANITARY LINE A PLAN

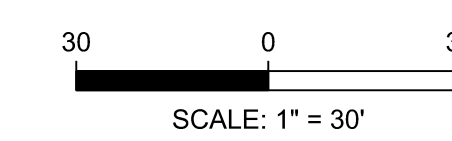
VERTICAL SCALE: 1"=3'
HORIZONTAL SCALE: 1"=30'



SANITARY LINE A PROFILE



KEYMAP
NTS



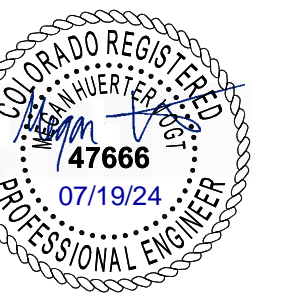
NOTES:

- SEE SHEET C-501 FOR CIVIL NOTES AND LEGEND.
- ALL EXISTING DRY UTILITIES ARE ASSUMED TO BE 3' DEEP TO TOP OF PIPE.
- ALL EXISTING WATER LINES ARE ASSUMED TO BE 4.5' DEEP TO TOP OF PIPE UNLESS OTHERWISE NOTED.
- CONTRACTOR TO ADJUST WATER LINE AS NECESSARY TO MAINTAIN A MINIMUM OF 4.5' OF COVER BELOW FINISH GRADE AND 18" BETWEEN OTHER UTILITIES.
- THE CONTRACTOR SHALL FOTHOLE ALL EXISTING UTILITY CROSSINGS PRIOR TO CONSTRUCTION AND NOTIFY THE ENGINEER OF ANY POTENTIAL CONFLICTS IMMEDIATELY.

LEGEND:



UTILITY CROSSING



STANDARD TRENCH SECTION

STREET CUT PATCH

PIPE DIAMETER	MINIMUM WIDTH	MAXIMUM WIDTH
4"	1'-0"	1'-6"
6"	1'-6"	2'-0"
8"	2'-0"	2'-6"
10"	2'-6"	3'-0"
12"	3'-0"	3'-6"
14"	3'-6"	4'-0"
16"	4'-0"	4'-6"
18"	4'-6"	5'-0"
20"	5'-0"	5'-6"
24"	6'-0"	6'-6"

**PARKER WATER & SANITATION DISTRICT
TYPICAL TRENCH SECTION
PIPE PROTECTION**

SCALE: NONE DATE: 2/98
APPROVED: PVR 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W4.1

11-1/4, 22-1/2, 45° & 90° BENDS

TEE

DEAD END

PIPE SIZE	BENDS	TEE OR DEAD END
4"	11-1/4, 22-1/2, 45, 90	1.50
6"	11-1/4, 22-1/2, 45, 90	1.50
8"	11-1/4, 22-1/2, 45, 90	1.50
10"	11-1/4, 22-1/2, 45, 90	1.50
12"	11-1/4, 22-1/2, 45, 90	1.50
14"	11-1/4, 22-1/2, 45, 90	1.50
16"	11-1/4, 22-1/2, 45, 90	1.50
18"	11-1/4, 22-1/2, 45, 90	1.50
20"	11-1/4, 22-1/2, 45, 90	1.50
24"	11-1/4, 22-1/2, 45, 90	1.50

**PARKER WATER & SANITATION DISTRICT
CONCRETE KICKBLOCKS
BEARING SURFACES AND INSTALLATION**

SCALE: NONE DATE: 2/98
APPROVED: PVR 4/01 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W3.18

PIPE SIZE	LENGTH OF TIED PIPE
4"	1'-0"
6"	1'-0"
8"	1'-0"
10"	1'-0"
12"	1'-0"
14"	1'-0"
16"	1'-0"
18"	1'-0"
20"	1'-0"
24"	1'-0"

**PARKER WATER & SANITATION DISTRICT
LENGTH OF TIED PIPE**

SCALE: NONE DATE: 2/98
APPROVED: PVR 4/01 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W3.11

PIPE LOCATION SYSTEM

PLAN

SECTION VIEW @ FIRE HYDRANT

**PARKER WATER & SANITATION DISTRICT
LOCATOR WIRE DETAILS**

SCALE: NONE DATE: 2/98
APPROVED: PVR 4/01 10/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W3.6

PIPE BEDDING:

(a) Installation of Bedding and Pipe: After completion of the trench excavation and proper preparation of the foundation, six inches (6") of bedding material shall be placed on the trench bottom for support under the pipe. Bed holes shall be dug deep enough to provide a minimum of two inches (2") of concrete between the bed and bedding material. All pipe shall be installed in such a manner as to insure full support of the pipe barrel over its entire length. After the pipe is adjusted for line and grade, and the joint is made, the bedding material shall be carefully poured and tamped under the haunches of the pipe and in the previously dug bed holes.

(b) Tamping in haunches defined as the act of placing approved bedding material under the haunches of the pipe, paying particular attention to voids, bed holes, and any holes. The purpose of tamping is to ensure uniform support for the pipe.

The limits of bedding shall be from six inches (6") below the bottom of the pipe to twelve inches (12") above the top of the pipe. Approved backfill may then be installed to the groundline. See Chapter 2 of these Standards for backfill and composition of backfill.

Composition of bedding is not required. The only requirement is sufficient tamping to achieve uniform support under the pipe. See Sheet W4.1 of the Standard Details for a typical trench cross section.

(c) Bedding Material: The bedding material shall be a clean well-graded sand or squeegee sand and shall conform to the following limits when tested by means of laboratory sieves:

Sieve Size	Total Percent Passing by Weight
3/8 inch	100
No. 4	70-100
No. 10	38-85
No. 20	20-60
No. 40	8-50
No. 60	2-30
No. 100	1-10
No. 200	0-3

Squeegee Sand
for use with 20-inch or smaller

Sieve Size	Total Percent Passing by Weight
3/8 inch	100
No. 200	0-3

If approved by the District, fines from the trench walls and spoils pile may be used to provide uniform support for the pipe. No rock or items larger than that allowed by the sieve analysis, or any other detrimental materials, shall be placed closer to the pipe than six inches (6"). Approved bedding materials shall be discarded on the job site to be used in the event natural materials become unsatisfactory. The District reserves the right to require the use of the specified bedding material at any time.

**PARKER WATER & SANITATION DISTRICT
PIPE BEDDING**

SCALE: NONE DATE: 2/98
APPROVED: PVR 2/00 2/00 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W4.2

PLAN OF VALVE

VALVE OPENING & CLOSING PROCEDURE

NOTE: ONLY RIGHT HAND OPENING VALVES ALLOWED.

**PARKER WATER & SANITATION DISTRICT
VALVE OPERATION**

SCALE: NONE DATE: 2/98
APPROVED: PVR 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W3.4

MECHANICAL JOINT RESTRAINT

WEDGE DETAIL

BOLT HOLE DETAIL

NOMINAL PIPE SIZE	NO. OF BOLTS	NO. OF WEDGES	W INCHES	J INCHES	F INCHES	M INCHES
4"	2	2	11.12	9.5	7.00	0.88
6"	3	3	13.37	11.75	9.15	1.00
8"	4	4	15.62	14.00	11.20	1.10
10"	6	6	17.88	16.25	13.30	1.25
12"	8	8	19.88	18.25	15.30	1.35
14"	10	10	21.88	20.25	17.30	1.45
16"	12	12	23.88	22.25	19.30	1.55
18"	14	14	25.88	24.25	21.30	1.65
20"	16	16	27.88	26.25	23.30	1.75
24"	20	20	31.88	30.25	27.30	1.95

**PARKER WATER & SANITATION DISTRICT
MECHANICAL JOINT RESTRAINT DETAILS**

SCALE: NONE DATE: 2/98
APPROVED: PVR 2/00 2/00 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W3.17

VALVE BOX DETAIL

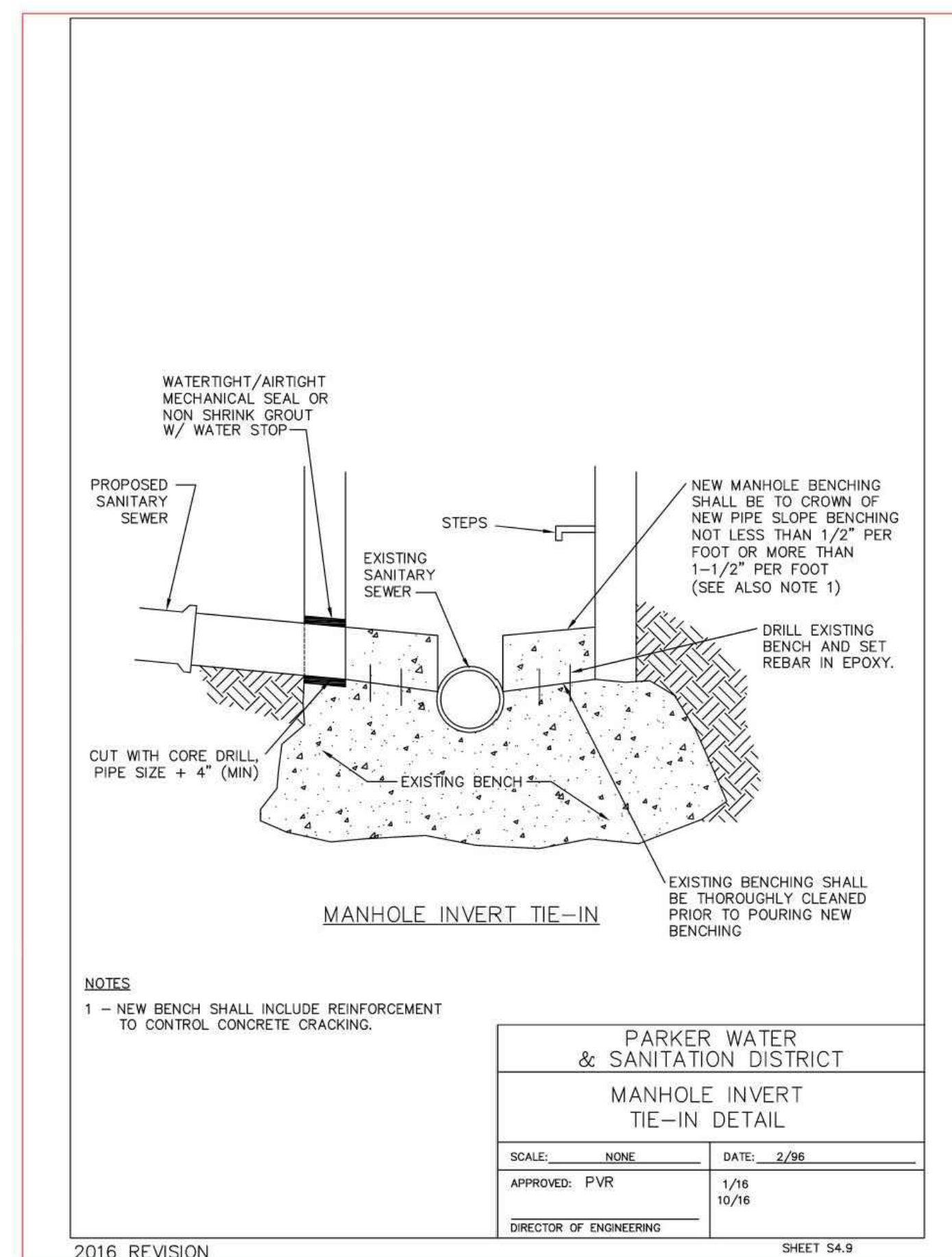
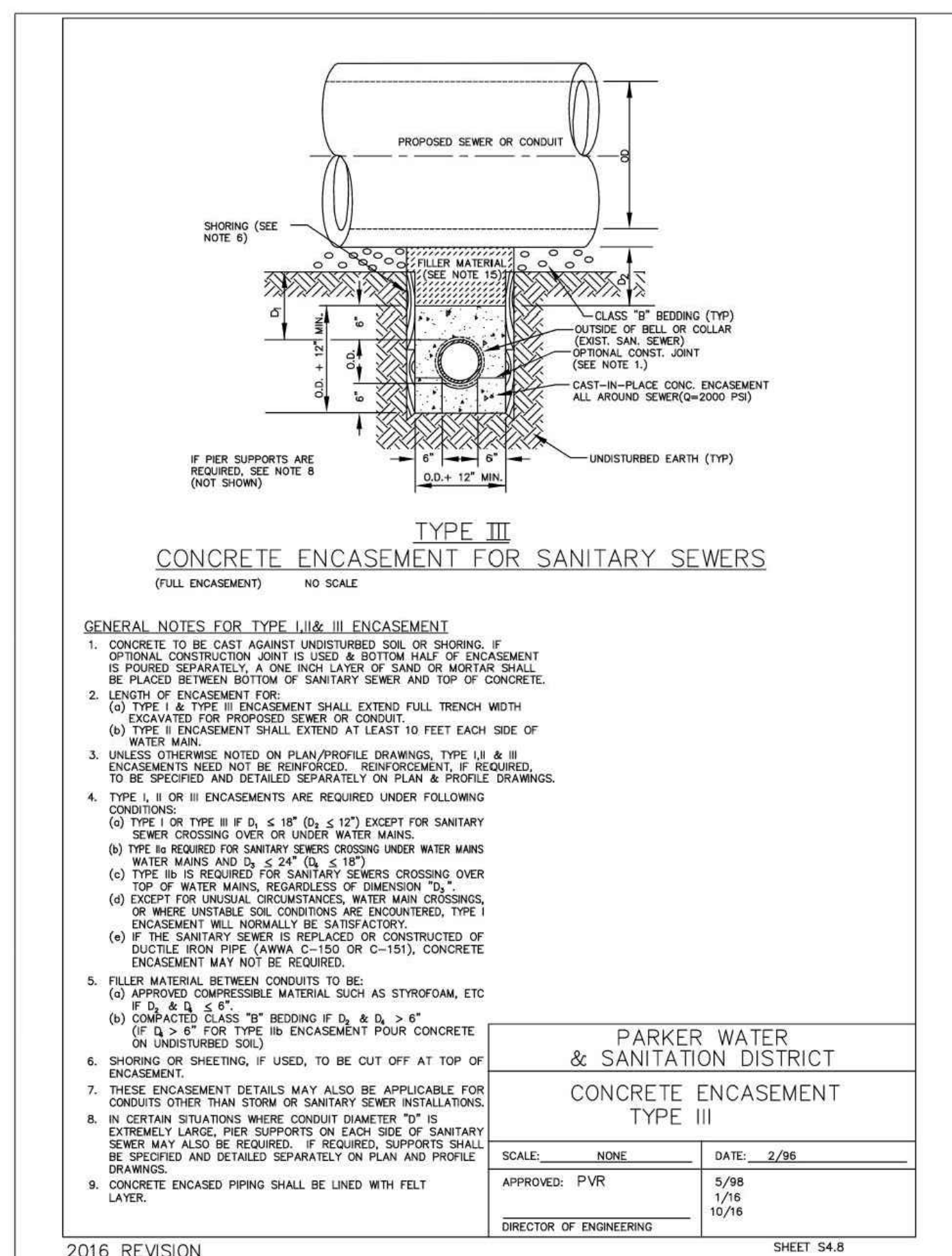
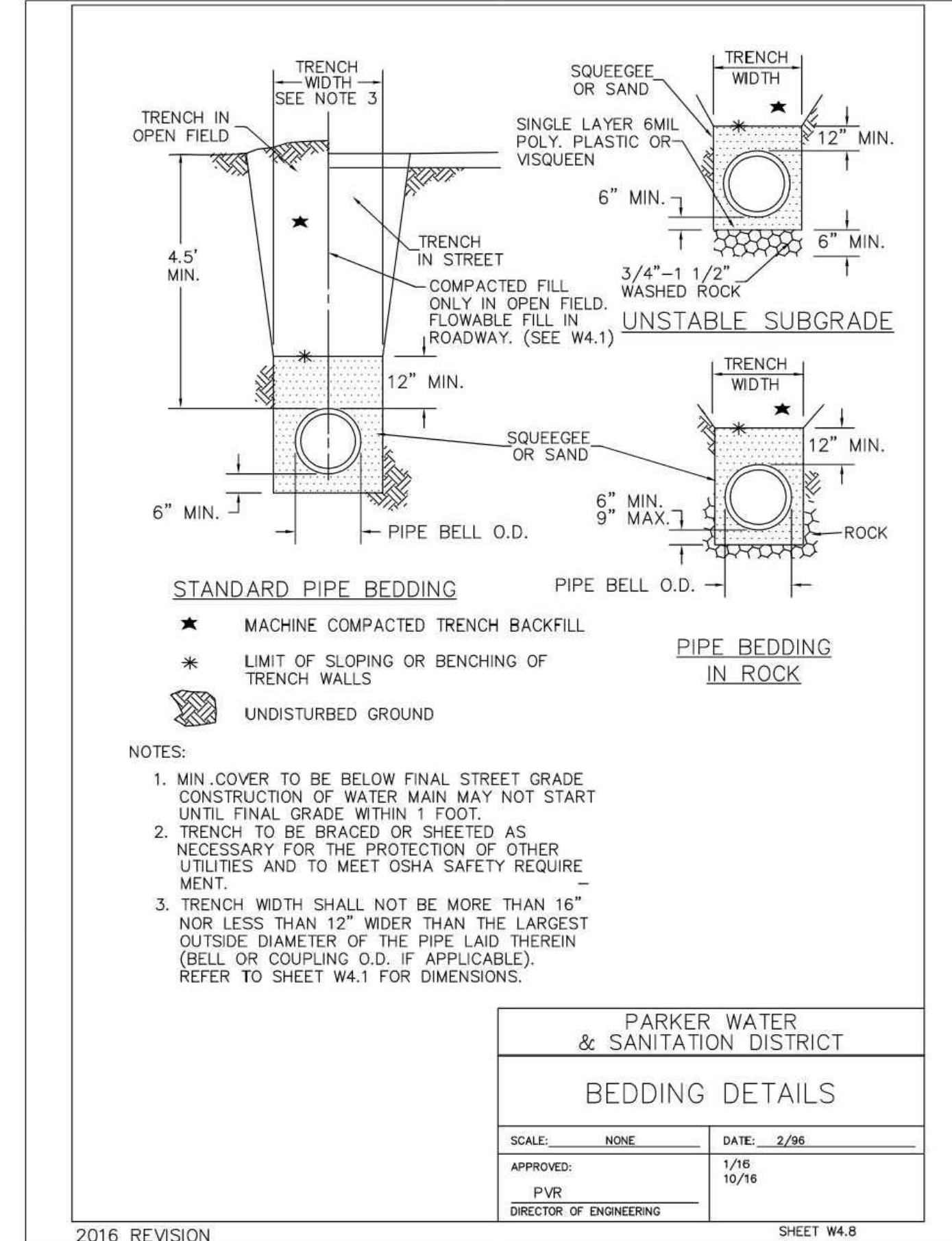
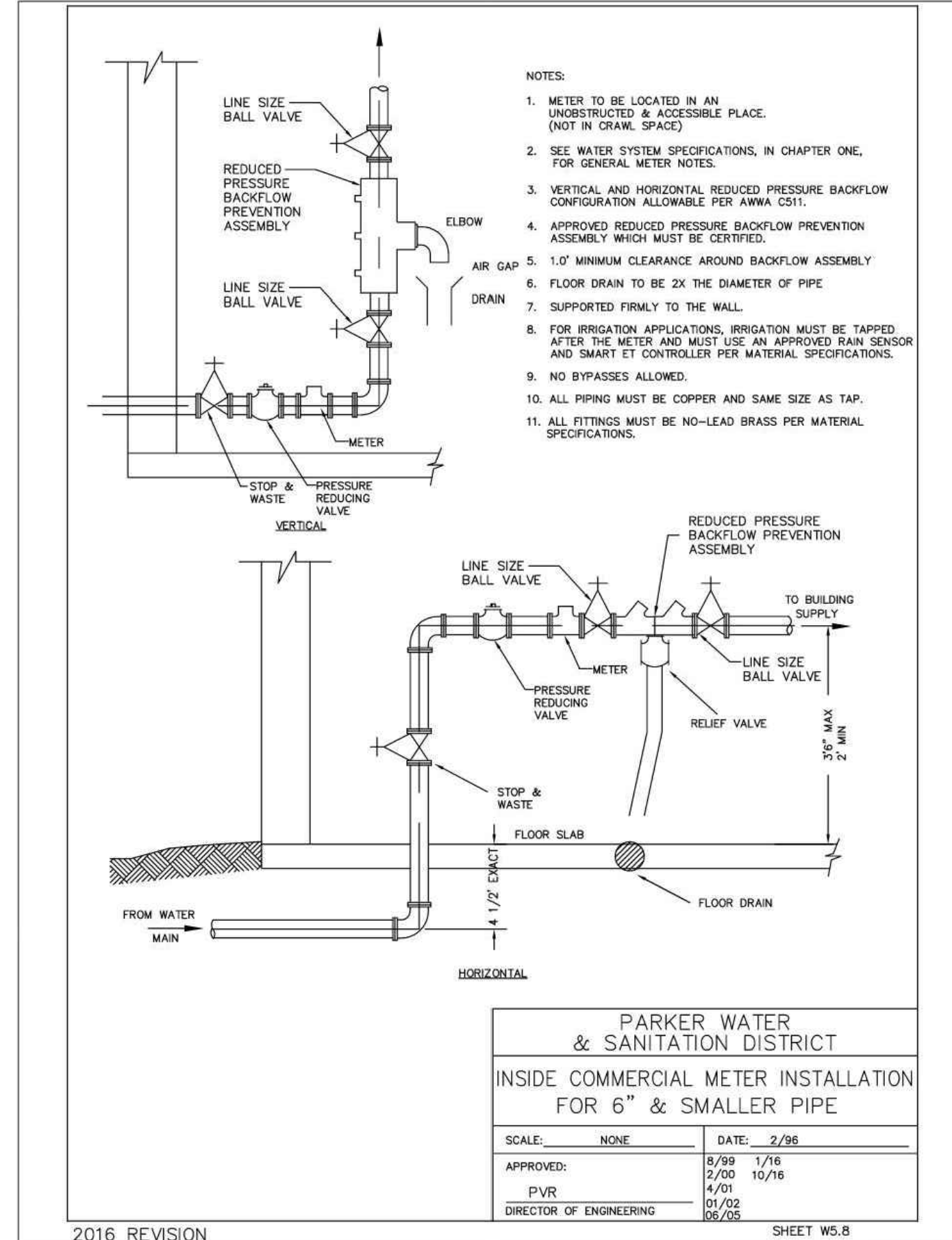
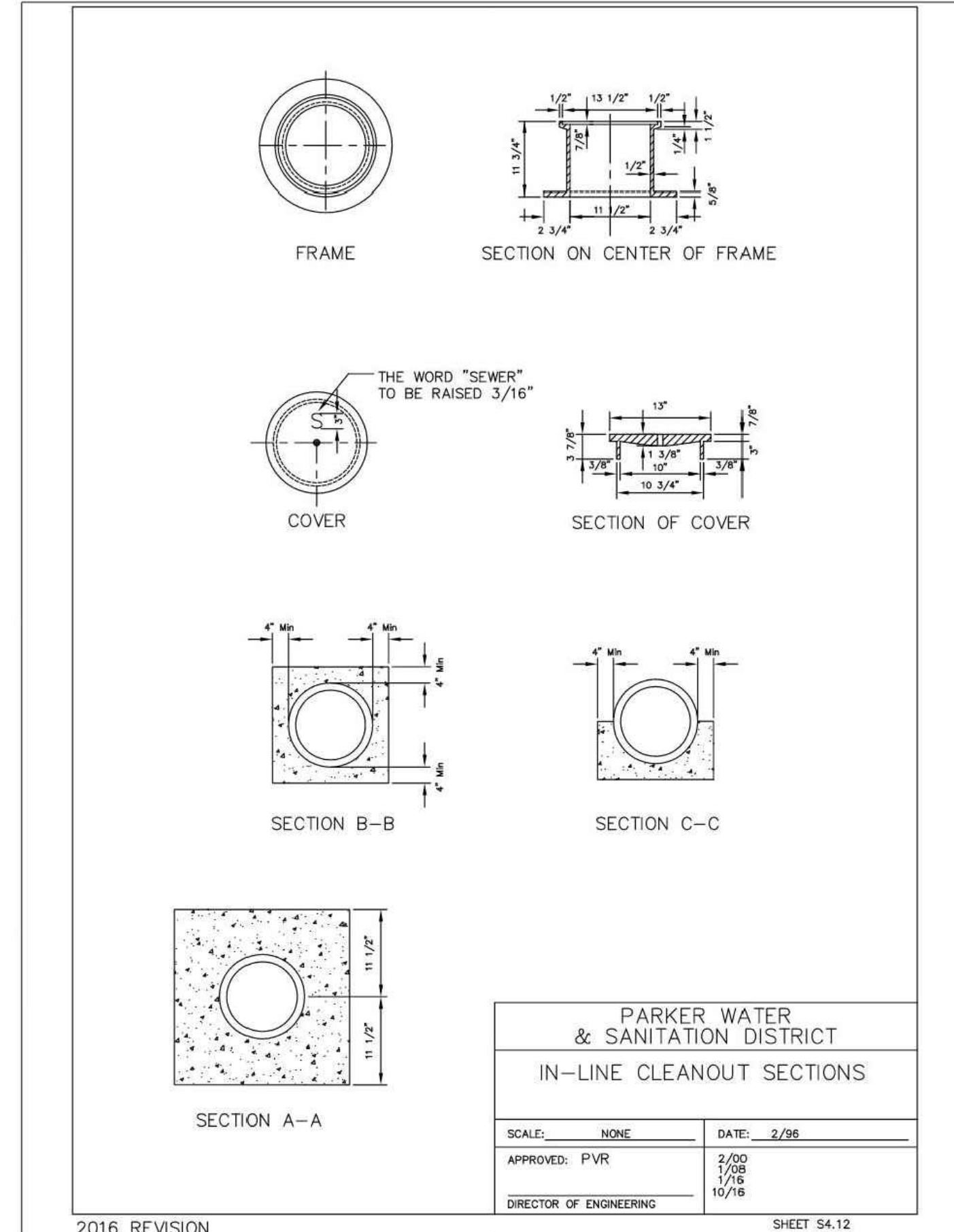
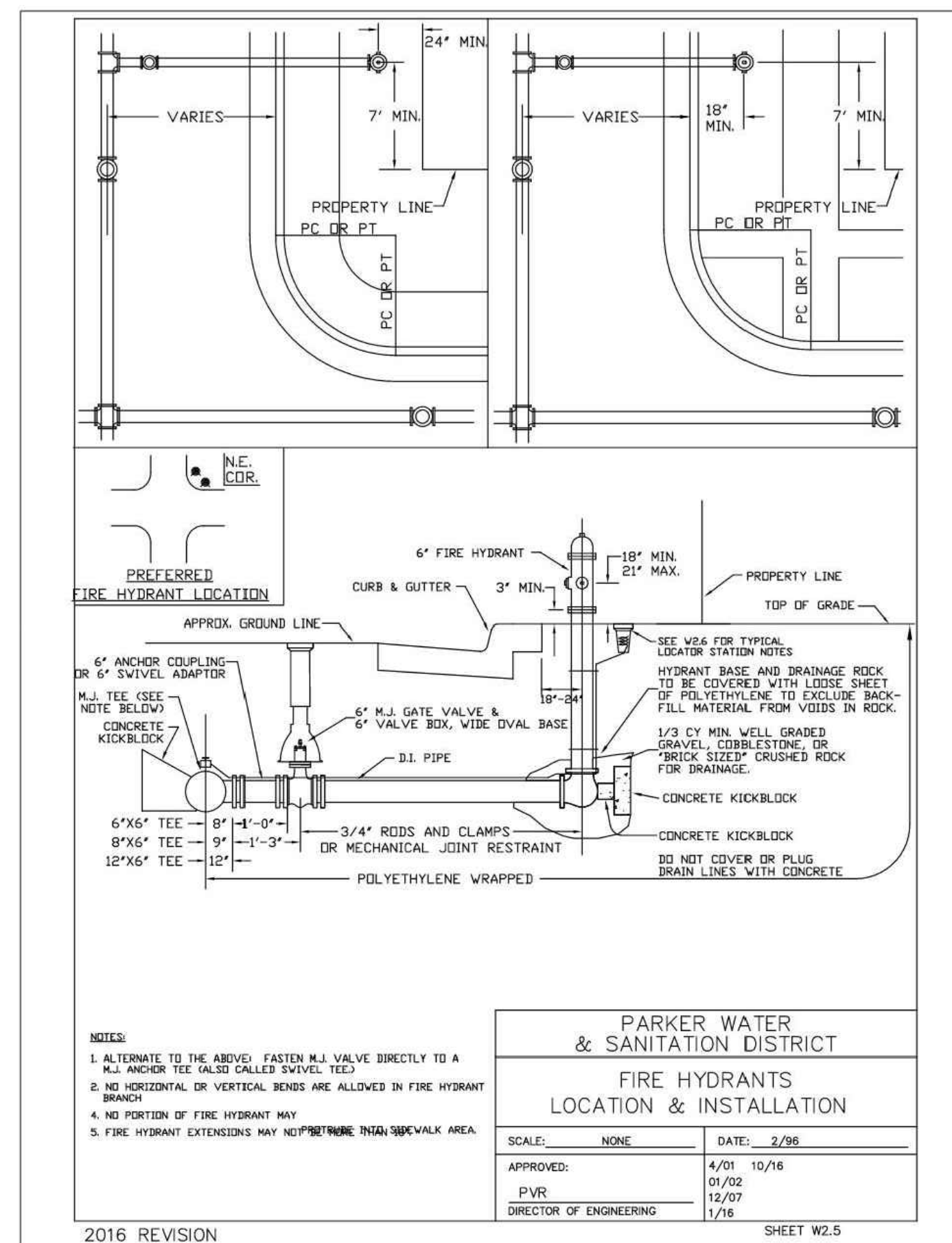
6-INCH VALVE BOXES

CHECK MATERIAL SPECIFICATIONS

**PARKER WATER & SANITATION DISTRICT
VALVE BOX DETAIL**

SCALE: NONE DATE: 2/98
APPROVED: PVR 2/00 2/00 1/16 10/16
DIRECTOR OF ENGINEERING

2016 REVISION SHEET W3.10



GENERAL NOTES

- 1. CONTRACTOR SHALL FIELD VERIFY ALL ITEMS REQUIRING REMOVAL AND PROTECTION WITH OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION.
- 2. CONTRACTOR SHALL VERIFY LOCATIONS OF EXISTING TREES AND SHRUBS TO BE REMOVED OR RELOCATED WITH OWNER'S REPRESENTATIVE PRIOR TO START OF CONSTRUCTION. CONTRACTOR SHALL MARK/FLAG INDIVIDUAL TREES TO BE REMOVED AND TO REMAIN FOR THE APPROVAL OF OWNER'S REPRESENTATIVE.
- 3. PRIOR TO COMMENCING DEMOLITION OPERATIONS, CONTRACTOR SHALL CONTACT ALL UTILITY LOCATOR SERVICES AND CONFIRM ALL SERVICE LINES AND UTILITY LOCATIONS WITHIN THE LIMITS OF WORK.
- 4. PRIOR TO DEMOLITION OPERATIONS, CONTRACTOR SHALL PLACE TREE PROTECTION FENCING, BARRICADES, SIGNAGE, AND EROSION CONTROL MEASURES AS DESCRIBED IN THE PLAN AND SPECIFICATIONS AND APPROVED CASMP.
- 5. CONTRACTOR SHALL THOROUGHLY REMOVE AND DISPOSE OF ALL PLANT MATERIALS, INCLUDING ROOTS, WITHIN AREAS SHOWN FOR DEMOLITION.
- 6. THE CONTRACTOR SHALL BE AWARE THAT UNDERGROUND IMPROVEMENTS MAY EXIST THAT ARE NOT SHOWN IN THE CONSTRUCTION PLANS INCLUDING, BUT NOT LIMITED TO, FOUNDATIONS, DEBRIS, ELECTRIC AND COMMUNICATION CONDUITS, PIPES, AND STORM DRAIN AND SEWER CONNECTIONS. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE IF THESE UNKNOWN IMPROVEMENTS ARE LOCATED, EXPOSED, OR DAMAGED.
- 7. CONTRACTOR SHALL NOT PROCEED WITH CONSTRUCTION WHEN IT IS APPARENT THAT PREVIOUSLY UNKNOWN OBSTRUCTIONS AND/OR GRADE DIFFERENCES EXIST THAT MAY NOT HAVE BEEN KNOWN DURING DESIGN. SUCH CONDITIONS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER'S REPRESENTATIVE FOR DIRECTION. THE CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ALL NECESSARY REVISIONS AND REPAIR COSTS THAT ARE THE RESULT OF FAILURE TO GIVE SUCH NOTIFICATION.
- 8. CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING THEMSELVES FAMILIAR WITH ALL UNDERGROUND UTILITIES, PIPES AND STRUCTURES. CONTRACTOR SHALL TAKE RESPONSIBILITY FOR ANY COST INCURRED DUE TO DAMAGE TO UTILITIES. CONTACT UNDERGROUND CONSULTING SOLUTIONS (303-904-7422) PRIOR TO ANY CONSTRUCTION ACTIVITY.
- 9. CONTRACTOR IS RESPONSIBLE FOR SECURING AND PROPERLY DISPLAYING ALL GOVERNING PERMITS, ALL REQUIRED TOWN OF PARKER / COUNTY PERMITS, AND STATE PERMITS.
- 10. CONTRACTOR TO INSTALL ALL HARDSCAPE IN ACCORDANCE WITH THE STATE OF COLORADO AND THE AMERICAN DISABILITY ACTS STANDARDS.
- 11. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION OF THE SUBCONTRACTORS ACCOMPLISHMENT OF SCOPE OF WORK. CONTRACTOR SHALL COORDINATE CONSTRUCTION WITH OTHER TRADES WORKING ON THE SITE SIMULTANEOUSLY.
- 12. REFER TO CIVIL DOCUMENTS FOR GRADING AND ADDITIONAL SITE INFORMATION.
- 13. CONTRACTOR SHALL PROVIDE MOCKUPS AND SHOP DRAWINGS FOR APPROVAL PER SPECIFICATIONS PRIOR TO CONSTRUCTION. ALL IMPROVEMENTS SHALL BE CONSTRUCTED TO MEET THE APPROVED MOCKUP OR SHOP DRAWING.
- 14. CONTRACTOR SHALL REPLACE OR REPAIR TO ORIGINAL CONDITION ALL UTILITIES, PAVING, CURBING, ETC., DAMAGED AS A RESULT OF HIS OPERATIONS AT NO ADDITIONAL COST TO OWNER.

LANDSCAPE NOTES

- 1. ALL SITE IMPROVEMENTS MUST BE FIELD SURVEYED AND STAKED BY CONTRACTOR. OWNER'S REPRESENTATIVE SHALL APPROVE STAKED LOCATION OF IMPROVEMENTS PRIOR TO INSTALLATION.
- 2. OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT SHALL APPROVE FINAL STAKING BY CONTRACTOR OF ALL CONCRETE FLATWORK IN PLAZA AREAS PRIOR TO CONSTRUCTION AND ALL FORM WORK PRIOR TO POURING.
- 3. ALL DIMENSIONS SHALL BE FIELD VERIFIED BY CONTRACTOR PRIOR TO CONSTRUCTION. ANY DEVIATION FROM THESE PLANS MUST BE APPROVED BY OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT PRIOR TO CONSTRUCTION.
- 4. PRIOR TO EXCAVATION, UNDERGROUND UTILITIES MUST BE FIELD VERIFIED (SEE GENERAL NOTES).
- 5. SAW CUT CONTROL JOINTS SHALL BE SPACED AS INDICATED ON THE PLANS AND DETAILS. EXPANSION JOINTS SHALL BE PLACED EVERY 100LF ALONG CONCRETE PATHS. CONTRACTOR SHALL LAYOUT ALL CONTROL JOINTS AND EXPANSION JOINTS IN THE FIELD WITH THE OWNER'S REPRESENTATIVE.
- 6. DO NOT SCALE THE PLANS. DIMENSIONS SUPERSEDE DRAWING SCALES. VERIFY LAYOUT PRIOR TO EXCAVATION.
- 7. DIMENSIONS ARE DRAWN TO FACE OF CURB, WALL, AND EDGE UNLESS OTHERWISE SPECIFIED.
- 8. IF THERE IS A DISCREPANCY BETWEEN THE PLANS AND SPECIFICATIONS, THE SPECIFICATIONS WILL SUPERSEDE PLANS.

TOWN OF PARKER- TREE PROTECTION DURING DEVELOPMENT-SITE

- 1. PRIOR TO THE START OF CONSTRUCTION, ALL TREES AND SHRUBS ON THE SITE SHALL BE CLEARLY IDENTIFIED IN THE FIELD AS EITHER "PRESERVE," "RELOCATE" OR "REMOVE" BY THE PROPERTY OWNER. TOWN STAFF SHALL VERIFY THE IDENTIFICATION OF TREES AND SHRUBS ACCORDING TO THE APPROVED TREE CONSERVATION PLAN. TREE OR SHRUB REMOVAL OR REPLACEMENT SHALL NOT COMMENCE UNTIL THE LANDOWNER HAS OBTAINED WRITTEN VERIFICATION OF THE FIELD IDENTIFICATIONS IN ACCORDANCE WITH THE APPROVED TREE CONSERVATION PLAN WITHIN THIRTY (30) DAYS OF WRITTEN NOTIFICATION OF FIELD STAKING. TREES AND SHRUBS TO BE SAVED MAY BE IDENTIFIED IN GROUPS.
- 2. DURING DEVELOPMENT OR RAZING ACTIVITY, THE LANDOWNER SHALL CAUSE TO BE INSTALLED EFFECTIVE PROTECTION AROUND ALL TREE PRESERVATION AREAS AND SHALL FURTHER INSTALL MECHANISMS SUCH AS TREE WELLS (DEPENDENT UPON SPECIES), RETAINING WALLS OR OTHER STRUCTURES NECESSARY TO PROTECT INDIVIDUAL TREES AND SHRUBS DESIGNATED FOR PRESERVATION. SUCH PROTECTIVE MEASURES SHALL BE SPECIFIED ON THE TREE CONSERVATION PLAN AND SHALL BE DESIGNED AND INSTALLED IN A MANNER CONSISTENT WITH GOOD HORTICULTURAL PRACTICES AND SUBJECT TO THE APPROVAL OF THE PLANNING DIRECTOR, WITH A RECOMMENDATION FROM THE PARKS AND RECREATION DEPARTMENT.
- 3. THE TOWN SHALL FIELD VERIFY ALL LOCATIONS, SIZES AND QUANTITIES OF REPLACEMENT OR RELOCATED TREES AND SHRUBS PRIOR TO THE WARRANTY PERIOD ACCEPTANCE OF PUBLIC IMPROVEMENTS ASSOCIATED WITH THE DEVELOPMENT. IF NO PUBLIC IMPROVEMENTS ARE INVOLVED, VERIFICATION SHALL OCCUR PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY. WARRANTY PERIOD ACCEPTANCE OR CERTIFICATES OF OCCUPANCY SHALL NOT BE AUTHORIZED UNTIL WRITTEN VERIFICATION OF COMPLIANCE WITH THE TREE CONSERVATION PLAN IS OBTAINED FROM THE TOWN.
- 4. ANY REPLACEMENT OR RELOCATED TREE OR SHRUB WHICH DIES WITHIN ONE (1) YEAR OF WRITTEN VERIFICATION OF COMPLIANCE SHALL BE REPLACED WITHIN ONE (1) YEAR OR THE NEXT GROWING SEASON WITH THE SAME APPROVED SIZE AND SPECIES OF THE TREE OR SHRUB WHICH DIED. (H)ACTS HARMFUL TO TREES AND SHRUBS.

TOWN OF PARKER- TREE PROTECTION DURING DEVELOPMENT-PUBLIC PROPERTY

- 1. NO PERSON SHALL ABUSE, MUTILATE OR OTHERWISE DAMAGE ANY TREE OR SHRUB LOCATED ON PUBLIC PROPERTY, INCLUDING THOSE LOCATED IN THE PUBLIC RIGHT-OF-WAY. HOWEVER, NOTHING IN THIS SECTION SHALL BE CONSTRUED TO PREVENT REASONABLE AND PROPER TRIMMING OF TREES AND SHRUBS LOCATED ON PUBLIC OR PRIVATE PROPERTY BY AUTHORIZED PERSONS IN ACCORDANCE WITH ACCEPTED ARBORICULTURAL AND HORTICULTURAL PRACTICES.
- 2. NO PERSON SHALL ATTACH ANY SIGN, NOTICE, PLACARD, ELECTRICAL WIRE OR OTHER INJURIOUS DEVICE TO ANY TREE OR SHRUB LOCATED ON PUBLIC PROPERTY OR WITHIN A PUBLIC RIGHT-OF-WAY, NOR SHALL ANY PERSON CAUSE ANY SUBSTANCE HARMFUL TO TREES OR SHRUBS TO COME IN CONTACT WITH THEM, OR PREVENT WATER AND OXYGEN FROM REACHING THEIR ROOTS.

TOWN OF PARKER- VIOLATIONS

- 1. ENFORCEMENT OF ANY PROVISION OF THIS SECTION SHALL BE IN ACCORDANCE WITH SECTION 13.03.040 OF THIS TITLE.
- 2. THE REMOVAL OR DESTRUCTION OF EACH TREE AND SHRUB IN VIOLATION OF THE PROVISIONS OF THIS SECTION FOR EACH DAY THE VIOLATION HAS OCCURRED IN VIOLATION OF AN APPROVED TREE CONSERVATION PLAN BY THE TOWN SHALL BE CONSIDERED A SEPARATE OFFENSE.
- 3. IN ADDITION TO THE REMEDIES DESCRIBED IN THIS SECTION, THE TOWN MAY REFUSE TO ISSUE ANY BUILDING PERMITS OR CERTIFICATES OF OCCUPANCY UNTIL THE VIOLATION IS CURED.

PLANTING NOTES

- 1. THE CONTRACTOR SHALL SUPPLY ALL PLANTS IN QUANTITIES SUFFICIENT TO COMPLETE THE WORK SHOWN ON THE PLAN. SHOULD ANY DISCREPANCIES EXIST BETWEEN QUANTITIES SHOWN IN THE PLANT LIST AND THOSE SHOWN IN THE DRAWINGS, THE DRAWINGS SHALL TAKE PRECEDENCE.
- 2. ALL PLANT MATERIAL SHALL BE REVIEWED BY THE OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION. CONTRACTOR SHALL INSURE PLANT MATERIAL MEET THE SPECIFICATIONS OF THE AMERICAN STANDARDS FOR NURSERY STOCK (LATEST EDITION) AS SET FORTH BY THE AMERICAN ASSOCIATION OF NURSERYMEN.
- 3. CONTRACTOR SHALL FURNISH PLANT MATERIALS FREE OF PESTS OR PLANT DISEASES. PRE-SELECTED OR "TAGGED" MATERIAL MUST BE INSPECTED BY THE CONTRACTOR AND CERTIFIED PEST AND DISEASE FREE. IT IS THE CONTRACTOR'S OBLIGATION TO WARRANT ALL PLANT MATERIALS PER THE SPECIFICATIONS.
- 4. CONTRACTOR SHALL REVIEW TREE LAYOUT WITH OWNER'S REPRESENTATIVE TO DETERMINE CONFLICTS OR INTERFERENCE PRIOR TO STAKING. CONTRACTOR SHALL STAKE PLANT LOCATIONS FOR THE REVIEW BY THE OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT. PRIOR TO DIGGING PLANTING PITS, THE CONTRACTOR SHALL RELOCATE ANY PLANT AS DIRECTED BY THE OWNER'S REPRESENTATIVE OR LANDSCAPE ARCHITECT PRIOR TO PLANTING AT NO ADDITIONAL COST TO THE OWNER.
- 5. ALL LANDSCAPE AREAS SHALL BE COVERED WITH A MINIMUM OF 4 INCH DEPTH MULCH, UNLESS EROSION CONTROL FABRIC IS INSTALLED. SUBMIT MULCH SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- 6. ANY LANDSCAPE AREAS BEYOND LIMITS OF WORK DISTURBED BY CONSTRUCTION OPERATIONS, NOT DIRECTED BY OWNER'S REPRESENTATIVE, SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
- 7. CONTAMINATION OF PLANTING MIX/TOPSOIL TO REMAIN DURING CONSTRUCTION WILL REQUIRE COMPLETE REPLACEMENT AT CONTRACTOR'S EXPENSE.

PLANT SCHEDULE

CODE	QTY	BOTANICAL / COMMON NAME	CONT
TREES - DECIDUOUS			
AG	4	ACER GINNALA / AMUR MAPLE	2" CAL
CS	3	CATALPA SPECIOSA / NORTHERN CATALPA	2" CAL
GS	8	GLEDITSIA TRIACANTHOS INERMIS 'SHADEMASTER' / SHADEMASTER HONEY LOCUST	2" CAL
GK	2	GYMNOCLADUS DIOICIA / KENTUCKY COFFEETREE	2" CAL
MR	3	MALLUS X 'RADIANT' / RADIANT CRABAPPLE	2" CAL
MS	1	MALLUS X 'SPRING SNOW' / SPRING SNOW CRABAPPLE	2" CAL
PA	6	PYRUS CALLERYANA 'AUTUMN BLAZE' / AUTUMN BLAZE CALLERY PEAR	2" CAL
QB	8	QUERCUS BICOLOR / SWAMP WHITE OAK	2" CAL
QF	8	QUERCUS ROBUR 'FASTIGIATA' / SKYROCKET® ENGLISH OAK	2" CAL
QR	4	QUERCUS RUBRA / RED OAK	2" CAL
TREES - EVERGREEN			
AC	1	ABIES CONCOLOR / WHITE FIR	8" HT
PN	2	PINUS NIGRA / AUSTRIAN PINE	8" HT
CODE	QTY	BOTANICAL / COMMON NAME	SIZE
SHRUBS - DECIDUOUS			
ASA	12	AMELANCHIER ALNIFOLIA / SASKATOON SERVICEBERRY	5 GAL
ALC	40	AMORPHA CANESCENS / LEADPLANT	5 GAL
AMS	32	ARONIA MELANOCARPA 'SMNAMPEN' / LOW SCAPE SNOWFIRE™ BLACK CHOKEBERRY	5 GAL
AMM	36	ARONIA MELANOCARPA 'UCONNAM165' / LOW SCAPE MOUND® BLACK CHOKEBERRY	5 GAL
CCB	29	CARYOPTERIS X CLANDONENSIS / BLUE MIST SPIREA	5 GAL
CBD	11	CORNUS SERICEA 'BAILEY' / BAILEY RED TWIG DOGWOOD	5 GAL
CSB	45	CYTISUS PURGANS 'SPANISH GOLD' / SPANISH GOLD BROOM	5 GAL
FNF	5	FORESTIERA NEOMEXICANA / NEW MEXICO PRIVET	5 GAL
HKA	99	HYPERICUM KALMIANUM 'AMES' / AMES ST. JOHNSWORT	5 GAL
PPC	81	PRUNUS BESSEYI 'P011S' / PAWNEE BUTTES® SAND CHERRY	5 GAL
RAC	41	RIBES ALPINUM / ALPINE CURRANT	5 GAL
SBF	100	SPIRAEA X BIMALDA 'FIRELIGHT' / FIRELIGHT SPIREA	5 GAL
VDA	7	VIBURNUM DENTATUM 'ARROWWOOD' / ARROWWOOD VIBURNUM	5 GAL
GRASSES			
BGG	54	BOUTELLOUA GRACILIS 'BLONDE AMBITION' / BLONDE AMBITION BLUE GRAMA	1 GAL
CAG	28	CALAMAGROSTIS X ACUTIFLORA 'AVALANCHE' / AVALANCHE FEATHER REED GRASS	1 GAL
ETG	81	ELYMUS TRACHYCAULUS SSP. TRACHYCAULUS / SLENDER WHEATGRASS	1 GAL
IRG	94	IMPERATA CYLINDRICA 'RED BARON' / JAPANESE BLOOD GRASS	1 GAL
MCG	93	MELICA CILIATA / SILKY-SPIKE MELIC GRASS	1 GAL
MPG	61	MISCANTHUS X 'PURPURASCENS' / PURPLE FLAME GRASS	1 GAL
MLG	33	MUHLENBERGIA CAPILLARIS 'LENCA' / REGAL MIST® PINK MUHLY GRASS	1 GAL
OHG	90	ORYZOPSIS HYMENOIDES / INDIAN RICEGRASS	1 GAL
PSG	20	PANICUM VIRGATUM 'SHENANDOAH' / SHENANDOAH SWITCH GRASS	1 GAL
PAG	124	PENNISETUM ALOPECUROIDES 'HAMELN' / HAMELN DWARF FOUNTAIN GRASS	1 GAL
SNG	50	SORGHASTRUM NUTANS / INDIAN GRASS	1 GAL
SHG	71	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSEED	1 GAL
GROUND COVER			
AUK	25	ARCTOSTAPHYLOS UVA-URSI / KINNIKINNICK	1 GAL
CPP	28	CERATOSTIGMA PLUMBAGINOIDES / DWARF CREEPING PLUMBAGO	1 GAL
CVG	87	CHRYSOGONUM VIRGINIANUM / GREEN-AND-GOLD	1 GAL
PSS	52	PHLOX STOLONIFERA 'SHERWOOD PURPLE' / CREEPING PHLOX	1 GAL
PSC	55	PHLOX SUBULATA / CREEPING PHLOX	1 GAL
TRC	80	THYMUS X 'RED CREEPING' / RED CREEPING THYME	1 GAL
VRC	125	VERONICA X 'REAVIS' / CRYSTAL RIVER CREEPING SPEEDWELL	1 GAL
PERENNIALS			
CRV	72	CENTRANTHUS RUBER / RED VALERIAN	1 GAL
ELP	61	ECHINACEA PURPUREA / PURPLE CONEFLOWER	1 GAL
GAB	71	GAILLARDIA ARISTATA / BLANKET FLOWER	1 GAL
PPP	83	PENSTEMON PALMERI / PALMER'S PENSTEMON	1 GAL
PDJ	42	PERSICARIA AFFINIS 'BORDER JEWEL' / HIMALAYAN BORDER JEWEL FLEECEFLOWER	1 GAL
RAB	42	RUDBECKIA AMPLEXICAULIS / BLACK-EYED SUSAN	1 GAL
SCB	24	SCABIOSA CAUCASICA 'BUTTERFLY BLUE' / BUTTERFLY BLUE PINCHUSHION FLOWER	1 GAL
VGS	71	VERONICA X 'GOODNESS GROWS' / GOODNESS GROWS SPEEDWELL	1 GAL
SHRUB EVERGREEN			
JHB	23	JUNIPERUS HORIZONTALIS 'BAR HARBOR' / BAR HARBOR CREEPING JUNIPER	5 GAL

TOWN OF PARKER NATIVE SEED MIX 1

(SEEDING RATES: DRILLED: 25 LBS/ACRE or BROADCAST: 50 LBS/ACRE)

- 20% SWITCHGRASS
- 15% EPHRAIM CRESTED WHEATGRASS
- 10% INDIAN RICEGRASS
- 15% GALLETTA GRASS
- 10% SIDEOTS GRAMA
- 15% BLUE GRAMA
- 10% BUFFALO GRASS
- 5% ANNUAL RYEGRASS

100% TOTAL

TOWN OF PARKER STANDARD LANDSCAPE NOTES

- 1. PLANT MATERIAL SPECIFICATIONS.
- 1.1. (1) PLANT MATERIALS SHALL BE SELECTED FROM THE TOWN'S RECOMMENDED PLANT SPECIES NOT INCLUDED IN THE RECOMMENDED PLANT LIST MAY BE SUBMITTED FOR CONSIDERATION, SO LONG AS THE SPECIES IS NOT ON THE EXCLUDED LIST. ALL PLANTS SHALL MEET OR EXCEED THE PLANT QUALITY AND SPECIES STANDARDS OF THE AMERICAN STANDARD FOR NURSERY STOCK ANSI Z60.1-2004, AS AMENDED. SELECTED PLANT MATERIALS SHALL BE HEALTHY AND SHALL BE NURSERY-GROWN AND ADAPTED TO THE LOCAL AREA. ARTIFICIAL PLANT MATERIALS ARE PROHIBITED WITHIN LANDSCAPED AREAS; THIS INCLUDES SYNTHETIC TURF.
- 1.2. (2) ALL PLANT MATERIALS MUST MEET THE FOLLOWING REQUIREMENTS:
 - 1.1.1. A. DECIDUOUS SHADE TREES: BETWEEN 2-INCH CALIPER AND 2½-INCH CALIPER (MEASURED SIX [6] INCHES ABOVE THE GROUND).
 - 1.1.2. B. ORNAMENTAL AND FLOWERING TREES: BETWEEN 1½-INCH CALIPER AND 2-INCH CALIPER (MEASURED SIX [6] INCHES ABOVE THE GROUND).
 - 1.1.3. C. EVERGREENS: BETWEEN SIX (6) FEET TALL AND EIGHT (8) FEET TALL (MEASURED TO MID-POINT OF MOST RECENT YEAR'S GROWTH). D. SHRUBS: FIVE-GAL OR CONTAINER (TWO [2] TO THREE [3] FEET IN HEIGHT FOR DECIDUOUS, EIGHTEEN- TO TWENTY-FOUR-INCH SPREAD FOR JUNIPERS, OR AS PROVIDED BY THE AMERICAN STANDARD FOR NURSERY STOCK ANSI Z60.1-2004, AS AMENDED).
- 2. PLANTING STANDARDS.
- 2.1. LANDSCAPING SHALL COMPLY WITH THE PLANTING STANDARDS AS PROVIDED IN THE TOWN'S PUBLIC WORKS TECHNICAL REFERENCE MANUAL, AS AMENDED.
- 2.2. PLANT MATERIALS SHALL BE SPACED APPROPRIATELY TO ALLOW ADEQUATE ROOM FOR THE PLANT AT MATURITY.
- 2.3. TREE PITS AND CUTOUTS MUST MEET THE CRITERIA SET FORTH IN THE TOWN'S PUBLIC WORKS TECHNICAL REFERENCE MANUAL, AS AMENDED.
- 2.4. LANDSCAPING SHALL BE PROTECTED FROM VEHICLES BY THE PLACEMENT OF WHEEL STOPS, CURBS OR OTHER ACCEPTABLE MEANS, APPROVED BY THE TOWN.
- 2.5. ALL TREES SHALL BE SPACED APPROPRIATELY FROM LIGHT POLES, SIGNS, STRUCTURES, ETC.
- 2.6. TREES SHALL NOT BE ELIMINATED THROUGH THE LOCATION OF LIGHT POLES IN PARKING LOT ISLANDS AND WITHIN THE PARKING LOT PERIMETER LANDSCAPING.
- 3. NONLIVING LANDSCAPE MATERIAL STANDARDS.
- 3.1. NONLIVING LANDSCAPE MATERIALS FOR GROUND COVER MAY CONSIST OF BARK, WOOD CHIPS, WOOD SHAVINGS, ROCKS, STONES OR OTHER MATERIALS APPROVED DURING THE SITE PLAN PROCESS.
- 3.2. WOOD MULCH SHALL CONSIST OF NATURAL CEDAR FIBER MULCH AND SHALL BE APPLIED A MINIMUM OF FOUR (4) INCHES IN DEPTH, INSTALLED OVER A BASE OF GEOTEXTILE FABRIC. A NO GEOTEXTILE FABRIC SHALL BE USED IN AREAS OF PERENNIAL FLOWER AND GROUND COVER PLANTINGS. B. NATURAL CEDAR FIBER MULCH SHALL BE INSTALLED OVER AREAS OF PERENNIAL FLOWER AND GROUND COVER PLANTINGS TO A DEPTH OF TWO (2) INCHES.
- 4. IRRIGATION REQUIREMENTS.
- 4.1. ALL LANDSCAPED AREAS SHALL BE SERVED BY A FUNCTIONING AUTOMATIC IRRIGATION SYSTEM.
- 4.2. LANDSCAPED AREAS IN EXCESS OF FIVE THOUSAND (5,000) SQUARE FEET SHALL HAVE A PROPERLY DESIGNED IRRIGATION SYSTEM PROVIDING FULL COVERAGE TO ALL TURF AND PLANT MATERIAL AREAS FROM ROTARY/GEAR-DRIVE SPRINKLERS, POP-UP AND SURFACE SPRAY SPRINKLERS, BUBBLERS, DRIP OR TRICKLE IRRIGATION, AS APPROPRIATE FOR THE AREA TO BE LANDSCAPED. RESIDENTIAL LANDSCAPED AREAS LESS THAN FIVE THOUSAND (5,000) SQUARE FEET MAY BE IRRIGATED BY HAND, SO LONG AS AN ADEQUATE WATER SOURCE, SUCH AS OUTDOOR HOSE BIBS, IS WITHIN FIFTY (50) FEET OF THE LANDSCAPED AREA.
- 4.2.1. A LANDSCAPE PLANTINGS ARE TO BE GROUPED ACCORDING TO WATER NEEDS.
- 4.2.2. TREES, SHRUBS AND PLANTINGS IN BED AREAS SHALL BE IRRIGATED BY DRIP, BUBBLER SYSTEMS, LOW VOLUME SPRAY HEADS OR SUBSURFACE IRRIGATION SYSTEMS, WHICH MAY INCLUDE THE PRODUCT NETAFIM OR APPROVED EQUIVALENT. SUBSURFACE IRRIGATION SYSTEMS ARE STRONGLY ENCOURAGED DUE TO THEIR EFFICIENCY AND MINIMIZED RUNOFF POTENTIAL.
- 4.2.3. HIGH-WATER-USE TURF AREAS, WHICH MAY INCLUDE BLUEGRASS, FESCUE OR OTHER HIGH-WATER-USE TURF, MAY BE IRRIGATED USING POP-UP OR ROTARY/GEAR-DRIVE SPRINKLERS. POP-UP HEIGHTS ON SPRINKLER HEADS SHOULD BE ADEQUATE TO CLEAR THE GRASS HEIGHT WHEN EXTENDED.
- 4.2.4. THE USE OF SUBSURFACE IRRIGATION FOR ALTERNATIVE LOW-WATER-USE TURF GRASS AREAS, WHICH MAY INCLUDE BUFFALO GRASS, BLUE GRAMA OR OTHER LOW-WATER-USE TURF, IS ENCOURAGED.
- 4.2.5. TEMPORARY OR PERMANENT IRRIGATION MUST BE PROVIDED TO NATIVE GRASS AREAS FOR ESTABLISHMENT IN ORDER TO ASSIST IN COMPLYING WITH THE RE-VEGETATION STANDARDS AS PROVIDED IN THE STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL, AS AMENDED. TEMPORARY IRRIGATION FOR ESTABLISHMENT OF NATIVE VEGETATION MAY BE INSTALLED ABOVE GROUND AND REMOVED IMMEDIATELY AFTER ESTABLISHMENT IS COMPLETE, OR IN ANY CASE, NO LONGER THAN ONE (1) YEAR.
- 4.2.6. UNDEVELOPED LOTS PLANNED FOR DEVELOPMENT, ROADWAYS AND/OR OTHER CAPITAL IMPROVEMENT PROJECTS MAY NOT REQUIRE TEMPORARY OR PERMANENT IRRIGATION AS DETERMINED BY THE DEPARTMENT OF ENGINEERING.
- 4.3. IRRIGATION SYSTEMS SHALL BE DESIGNED AND MAINTAINED TO MINIMIZE OVERSPRAY AND RUNOFF ONTO ADJACENT IMPERVIOUS SURFACES, SUCH AS ROADWAYS, SIDEWALKS AND PARKING LOTS.
- 4.4. RAINFALL OR SOIL MOISTURE SENSORS, CHECK VALVES, PRESSURE REDUCERS, WATER-EFFICIENT SPRINKLER HEADS, WHICH MAY INCLUDE HUNTER MP ROTATORS OR APPROVED EQUIVALENT, AND FLOW SENSOR VALVES ARE ALL DESIRABLE TO REDUCE UNNECESSARY WATER USAGE.

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Lafayette, CO 80227
Telephone: 303-988-4514

Electrical Engineer + Technology

AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034



ASSOCIATES

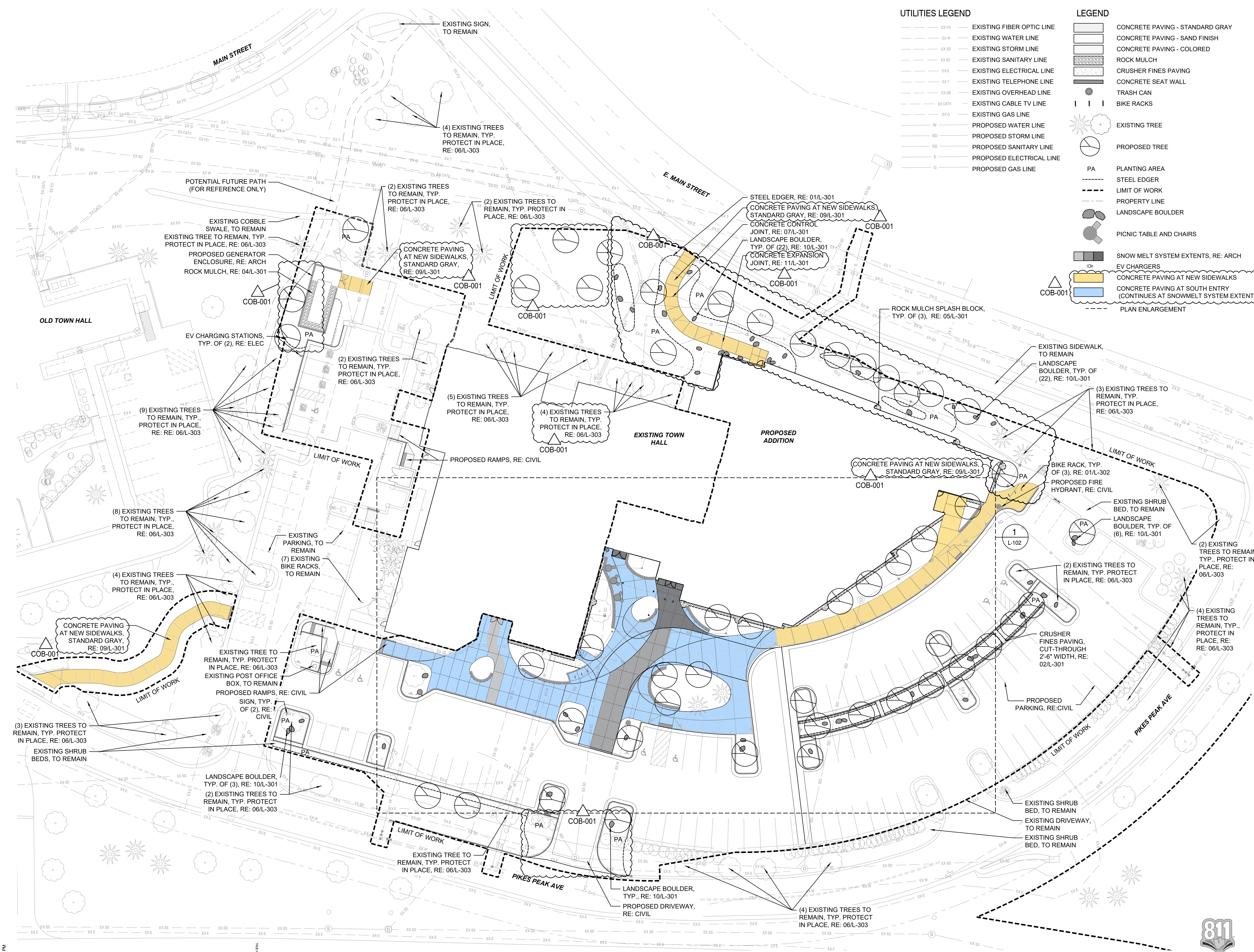
Issue	Date
PERMIT / GMP DRAWINGS	17 MAY 2024
COB-001	19 JULY 2024



Project Number:	22-093
Drawn By:	JM
Reviewed By:	TS
Approved By:	TS

LANDSCAPE COVER SHEET, NOTES, & SCHEDULES

L-000



UTILITIES LEGEND

EX FO	EXISTING FIBER OPTIC LINE
EX W	EXISTING WATER LINE
EX SD	EXISTING STORM LINE
EX SS	EXISTING SANITARY LINE
EX E	EXISTING ELECTRICAL LINE
EX T	EXISTING TELEPHONE LINE
EX OE	EXISTING OVERHEAD LINE
EX CATV	EXISTING CABLE TV LINE
EX G	EXISTING GAS LINE
W	PROPOSED WATER LINE
SD	PROPOSED STORM LINE
SS	PROPOSED SANITARY LINE
E	PROPOSED ELECTRICAL LINE
G	PROPOSED GAS LINE

LEGEND

[Pattern]	CONCRETE PAVING - STANDARD GRAY
[Pattern]	CONCRETE PAVING - SAND FINISH
[Pattern]	CONCRETE PAVING - COLORED
[Pattern]	ROCK MULCH
[Pattern]	CRUSHER FINES PAVING
[Pattern]	CONCRETE SEAT WALL
[Symbol]	TRASH CAN
[Symbol]	BIKE RACKS
[Symbol]	EXISTING TREE
[Symbol]	PROPOSED TREE
[Symbol]	PA
[Symbol]	STEEL EDGER
[Symbol]	LIMIT OF WORK
[Symbol]	PROPERTY LINE
[Symbol]	LANDSCAPE BOULDER
[Symbol]	PICNIC TABLE AND CHAIRS
[Symbol]	SNOW MELT SYSTEM EXTENTS, RE: ARCH
[Symbol]	EV CHARGERS
[Symbol]	CONCRETE PAVING AT NEW SIDEWALKS
[Symbol]	CONCRETE PAVING AT SOUTH ENTRY (CONTINUES AT SNOWMELT SYSTEM EXTENTS)
[Symbol]	PLAN ENLARGEMENT

AndersonMasonDale Architects

Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker
20120 Mainstreet
Parker, CO 80138

Architect

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Know what's below.
Call before you dig.

LANDSCAPE LAYOUT & MATERIALS PLAN

L-101

4/20/2023 5:24:20 PM

LAYOUT & MATERIALS PLAN

1
L-101

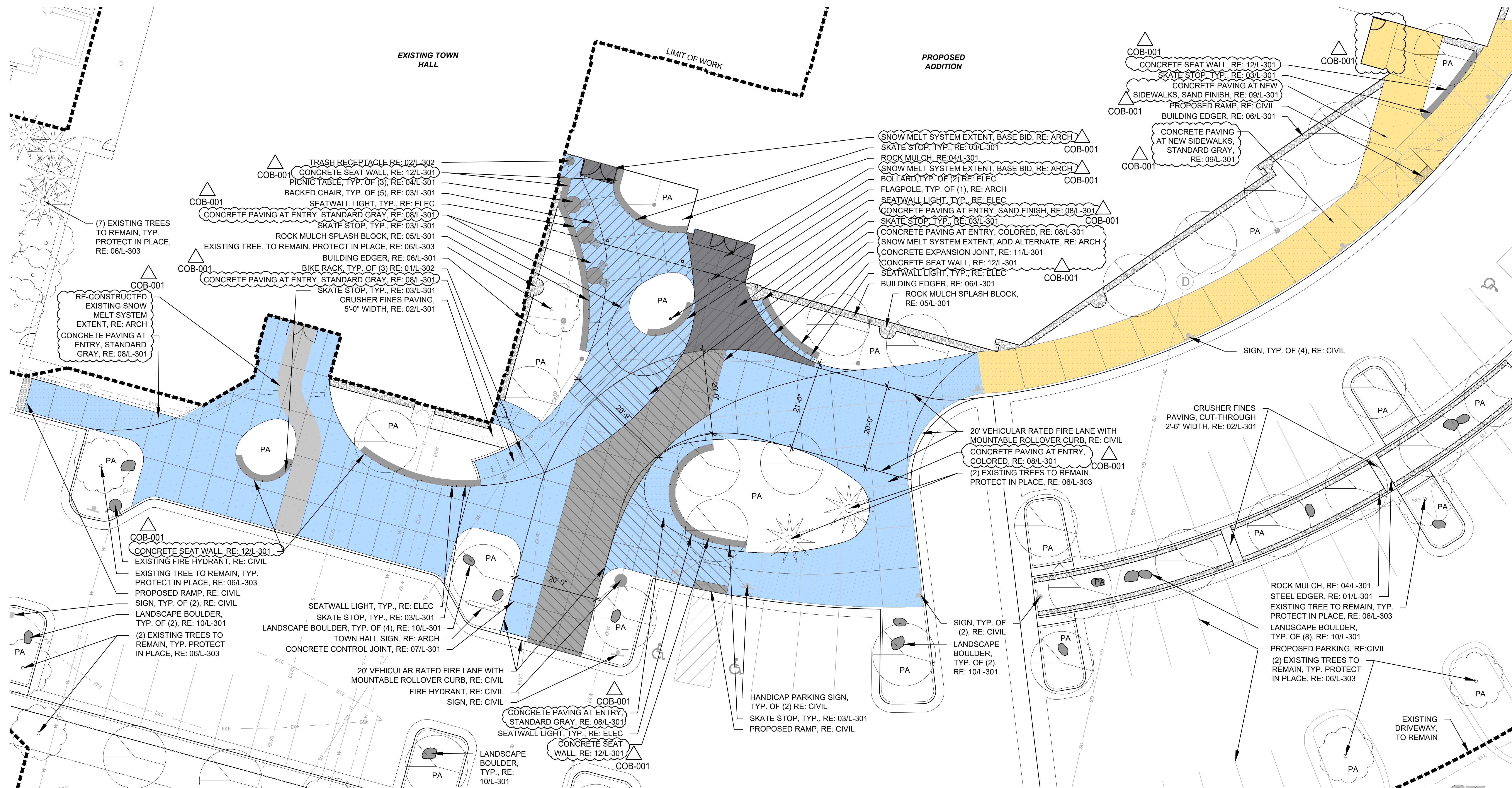
1" = 20'-0"

UTILITIES LEGEND

- EX FO --- EXISTING FIBER OPTIC LINE
- EX W --- EXISTING WATER LINE
- EX SD --- EXISTING STORM LINE
- EX SS --- EXISTING SANITARY LINE
- EX E --- EXISTING ELECTRICAL LINE
- EX T --- EXISTING TELEPHONE LINE
- EX OE --- EXISTING OVERHEAD LINE
- EX CATV --- EXISTING CABLE TV LINE
- EX G --- EXISTING GAS LINE
- W --- PROPOSED WATER LINE
- SD --- PROPOSED STORM LINE
- SS --- PROPOSED SANITARY LINE
- E --- PROPOSED ELECTRICAL LINE
- G --- PROPOSED GAS LINE

LEGEND

- [Pattern] CONCRETE PAVING, STANDARD GRAY
- [Pattern] CONCRETE PAVING, SAND FINISH
- [Pattern] CONCRETE PAVING, COLORED
- [Pattern] ROCK MULCH
- [Pattern] CRUSHER FINES PAVING
- [Symbol] CONCRETE SEAT WALL
- [Symbol] TRASH CAN
- [Symbol] BIKE RACKS
- [Symbol] EXISTING TREE
- [Symbol] PROPOSED TREE
- [Symbol] PA PLANTING AREA
- [Symbol] STEEL EDGER
- [Symbol] LIMIT OF WORK
- [Symbol] LANDSCAPE BOULDER
- [Symbol] PICNIC TABLE AND CHAIRS
- [Symbol] SNOW MELT SYSTEM EXTENT, RE: ARCH
- [Symbol] EV CHARGERS
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- [Symbol] CONCRETE PAVING AT SOUTH ENTRY (CONTINUES AT SNOWMELT SYSTEM EXTENTS)



Parker Town Hall Expansion

Parker, CO
22-093

Town of Parker
20120 Mainstreet
Parker, CO 80138

Architect

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Electrical Engineer + Technology

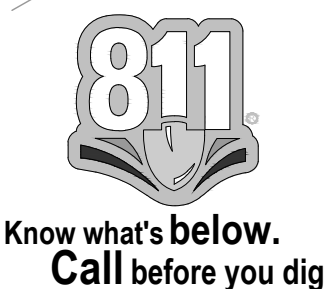
AE Design
1900 Wazee Street #205
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Telephone: 303-295-3034



Issue _____ Date _____
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COB-001 19 JULY 2024



Project Number: 22-093
Drawn By: JM
Reviewed By: TS
Approved By: TS



UTILITIES LEGEND

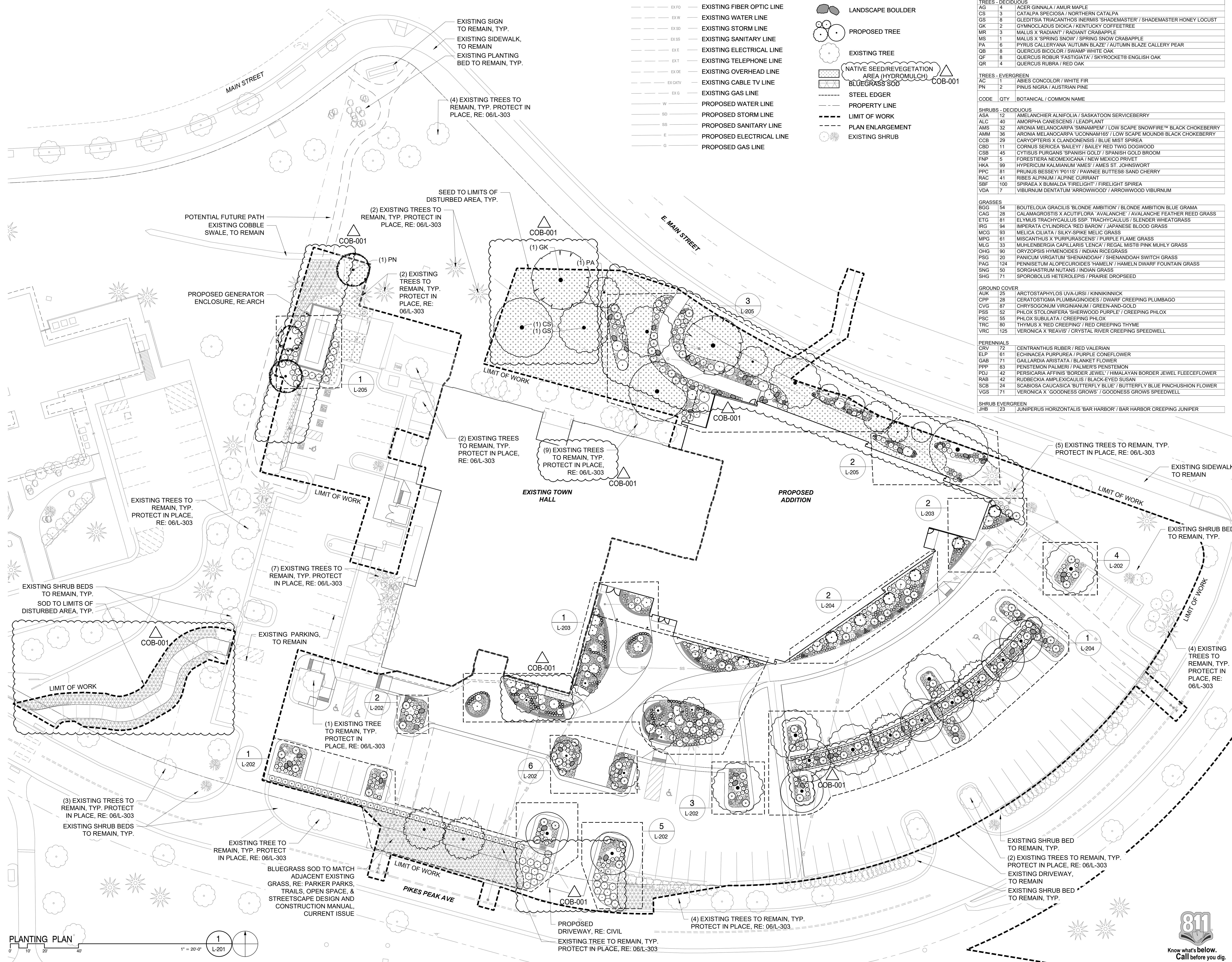
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---	G	PROPOSED GAS LINE

LEGEND

	LANDSCAPE BOULDER
	PROPOSED TREE
	EXISTING TREE
	NATIVE SEED/REVEGETATION AREA (HYDROMULCH)
	BLUEGRASS SOD
	STEEL EDGER
	PROPERTY LINE
	LIMIT OF WORK
	PLAN ENLARGEMENT
	EXISTING SHRUB

PLANT ABBREVIATION KEY

TREES - DECIDUOUS	
AG	4 ACER GINNALA / AMUR MAPLE
CS	3 CATALPA SPECIOSA / NORTHERN CATALPA
GS	8 GLEDITSIA TRIACANTHOS INERMIS / SHADEMASTER HONEY LOCUST
GK	2 GYMNOCLADUS DIOICHA / KENTUCKY COFFEETREE
MR	3 MALUS X 'RADIANT' / RADIANT GRABAPPLE
MS	1 MALUS X 'SPRING SNOW' / SPRING SNOW GRABAPPLE
PA	6 PYRUS CALLERYANA 'AUTUMN BLAZE' / AUTUMN BLAZE CALLERY PEAR
QB	8 QUERCUS BICOLOR / SWAMP WHITE OAK
QF	8 QUERCUS ROBUR 'FASTIGIATA' / SKYROCKET® ENGLISH OAK
QR	4 QUERCUS RUBRA / RED OAK
TREES - EVERGREEN	
AC	1 ABIES CONCOLOR / WHITE FIR
PN	2 PINUS NIGRA / AUSTRIAN PINE
SHRUBS - DECIDUOUS	
ASA	12 AMELANCHIER ALNIFOLIA / SASKATOON SERVICEBERRY
ALC	40 AMORPHA CANESCENS / LEADPLANT
AMS	32 ARONIA MELANOCARPA 'SMNAMPEN' / LOW SCAPE SNOWFIRE™ BLACK CHOKEBERRY
AMM	36 ARONIA MELANOCARPA 'UCONNAMI165' / LOW SCAPE MOUND® BLACK CHOKEBERRY
CCB	28 CARYOPHTERIS X GLANDROENSIS / BLUE MIST SPIREA
CBD	11 CORNUS SERICEA 'BAILEY' / BAILEY RED TWIG DOGWOOD
CSB	45 CYTISUS PURGANS 'SPANISH GOLD' / SPANISH GOLD BROOM
FNP	5 FORESTIERA NEOMEXICANA / NEW MEXICO PRIVET
HKA	99 HYPERICUM KALMIANUM 'AMES' / AMES ST. JOHNSWORT
PRC	81 PRUNUS BESSEYI 'P0115' / PAWNEE BUTTES® SAND CHERRY
RAC	41 RIBES ALPIMUM / ALPINE CURRANT
SBF	100 SPIRAEA X BUNALDA 'FIRELIGHT' / FIRELIGHT SPIREA
VDA	7 VIBURNUM DENTATUM 'ARROWWOOD' / ARROWWOOD VIBURNUM
GRASSES	
BGG	54 BOUTELOUA GRACILIS 'BLONDE AMBITION' / BLONDE AMBITION BLUE GRAMA
CAG	28 CALAMAGROSTIS X ACUTIFLORA 'AVALANCHE' / AVALANCHE FEATHER REED GRASS
ETG	81 ELYMUS TRACHYCAULUS SSP. TRACHYCAULUS / SLENDER WHEATGRASS
IRG	94 IMPERATA CYLINDRICA 'RED BARON' / JAPANESE BLOOD GRASS
MCG	93 MELICA CILIATA / SILKY SPIKE HELIC GRASS
MPG	61 MISCANTHUS X 'PURPURASCENS' / PURPLE FLAME GRASS
MLG	33 MUHLERBERGIA CAPILLARIS 'LENCA' / REGAL MIST® PINK MUHLY GRASS
OHG	90 ORYZOPSIS HYMENOIDES / INDIAN RICEGRASS
PSG	20 PANICUM VIRGATUM 'SHENANDOAH' / SHENANDOAH SWITCH GRASS
PAG	124 PANICUM VIRGATUM 'HAMELN' / HAMELN DWARF FOUNTAIN GRASS
SNG	50 SORGHASTRUM NUTANS / INDIAN GRASS
SHG	71 SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSIDE
GROUND COVER	
AUK	25 ARCTOSTAPHYLOS UVA-URSI / KINNIKINNICK
CPP	28 CERATOSTIGMA PLUMBAGINOIDES / DWARF CREEPING PLUMBAGO
CVG	87 CHRYSOGONUM VIRGINIANUM / GREEN-AND-GOLD
PSS	52 PHLOX STOLONIFERA 'SHERWOOD PURPLE' / CREEPING PHLOX
PSC	56 PHLOX SUBULATA / CREEPING PHLOX
TRC	80 THYMUS X 'RED CREEPING' / RED CREEPING THYME
VRC	125 VERONICA X 'REAVIS' / CRYSTAL RIVER CREEPING SPEEDWELL
PERENNIALS	
CRV	72 CENTRANTHUS RUBER / RED VALERIAN
ELP	61 ECHINACEA PURPUREA / PURPLE CONEFLOWER
GAB	71 GALLIARDIA ARISTATA / BLANKET FLOWER
PPP	83 PENSTEMON PALMERI / PALMER'S PENSTEMON
PBJ	42 PERSICARIA AFINIS 'BORDER JEWEL' / HIMALAYAN BORDER JEWEL FLEECEFLOWER
RAB	42 RUDBECKIA AMPLEXICAULIS / BLACK-EYED SUSAN
SCB	24 SCABIOSA CAUCASICA 'BUTTERFLY BLUE' / BUTTERFLY BLUE PINCHUSHION FLOWER
VGS	71 VERONICA X 'GOODNESS GROWS' / GOODNESS GROWS SPEEDWELL
SHRUB EVERGREEN	
JHB	23 JUNIPERUS HORIZONTALIS 'BAR HARBOR' / BAR HARBOR CREEPING JUNIPER



AndersonMasonDale Architects

Parker Town Hall Expansion
Parker, CO 22-093

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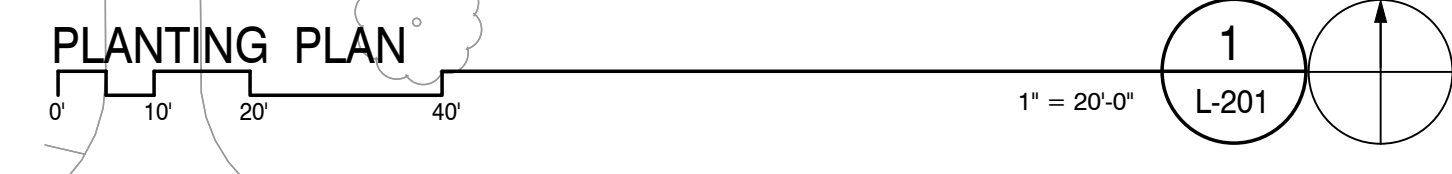


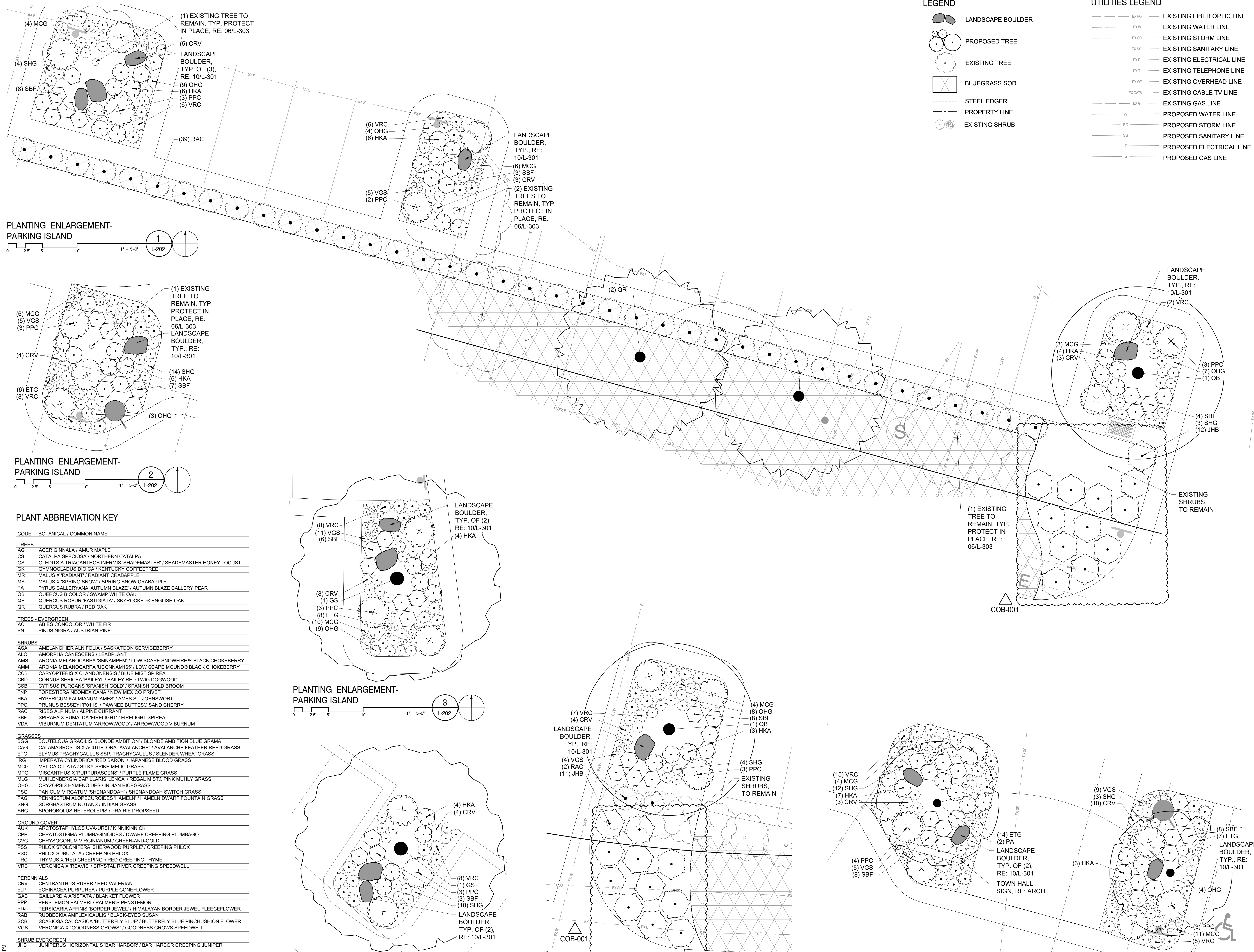
Project Number: 22-093
Drawn By: JM
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LANDSCAPE PLANTING PLAN

L-201





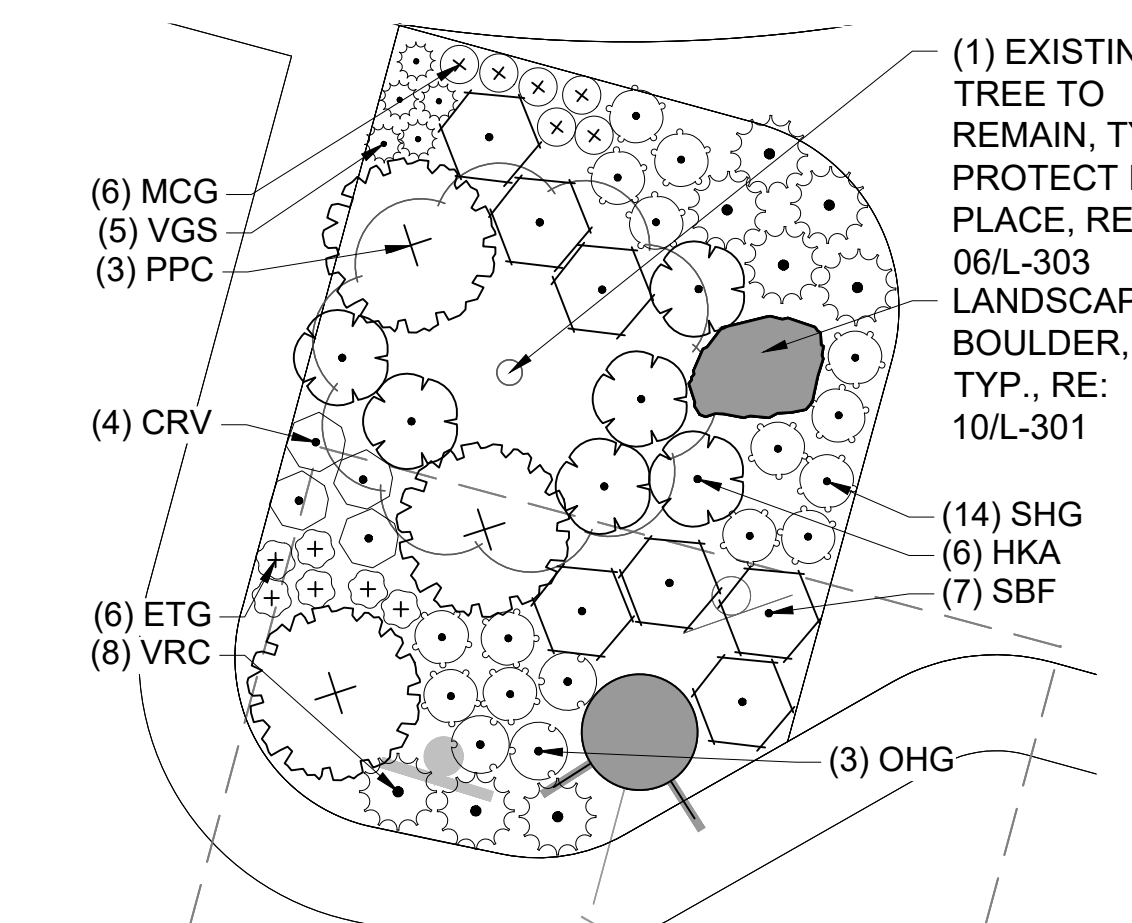
LEGEND

- LANDSCAPE BOULDER
- PROPOSED TREE
- EXISTING TREE
- BLUEGRASS SOD
- STEEL EDGER
- PROPERTY LINE
- EXISTING SHRUB

UTILITIES LEGEND

- EX FO — EXISTING FIBER OPTIC LINE
- EX W — EXISTING WATER LINE
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- W — PROPOSED WATER LINE
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PLANTING ENLARGEMENT- PARKING ISLAND



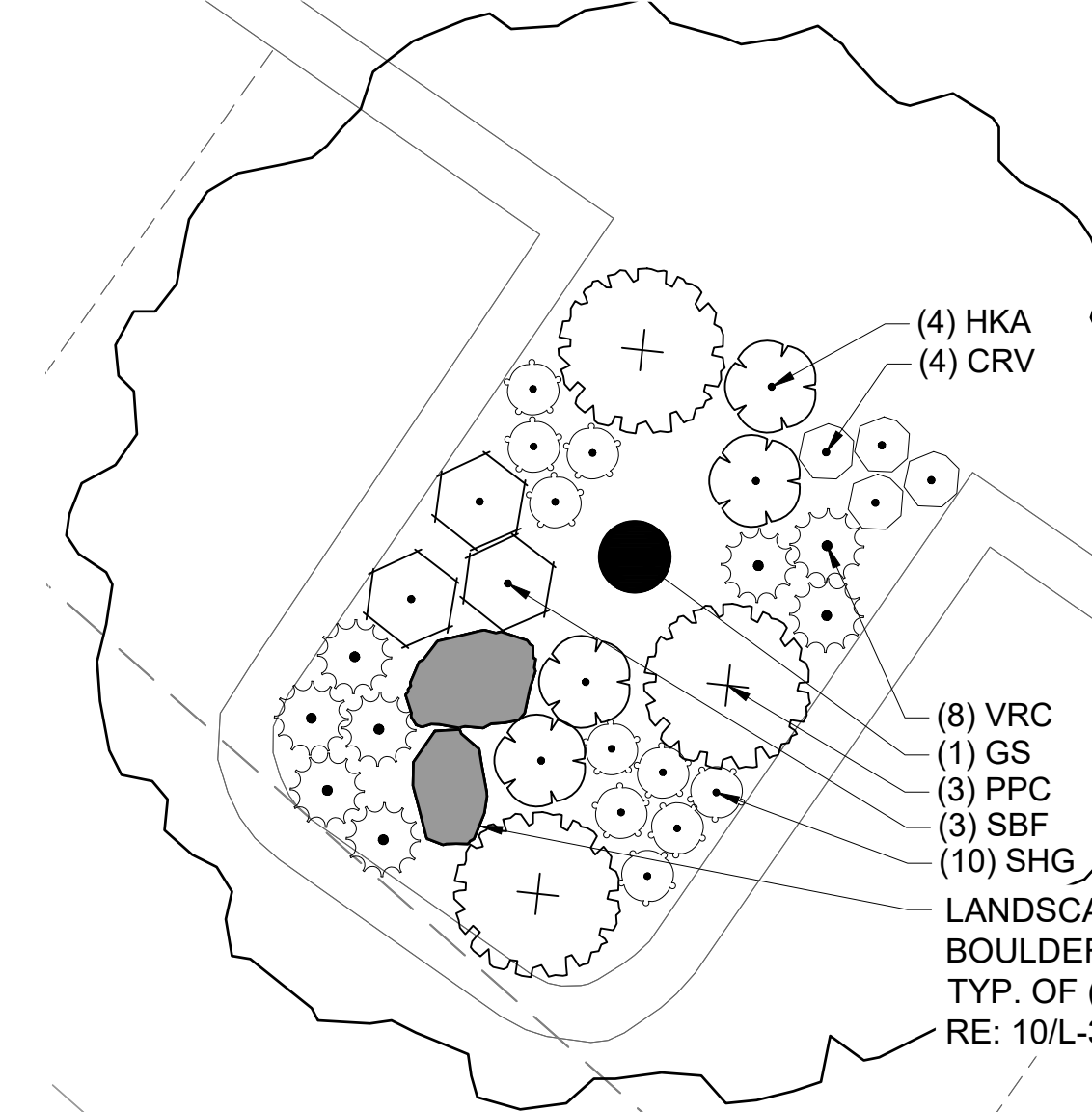
PLANTING ENLARGEMENT- PARKING ISLAND



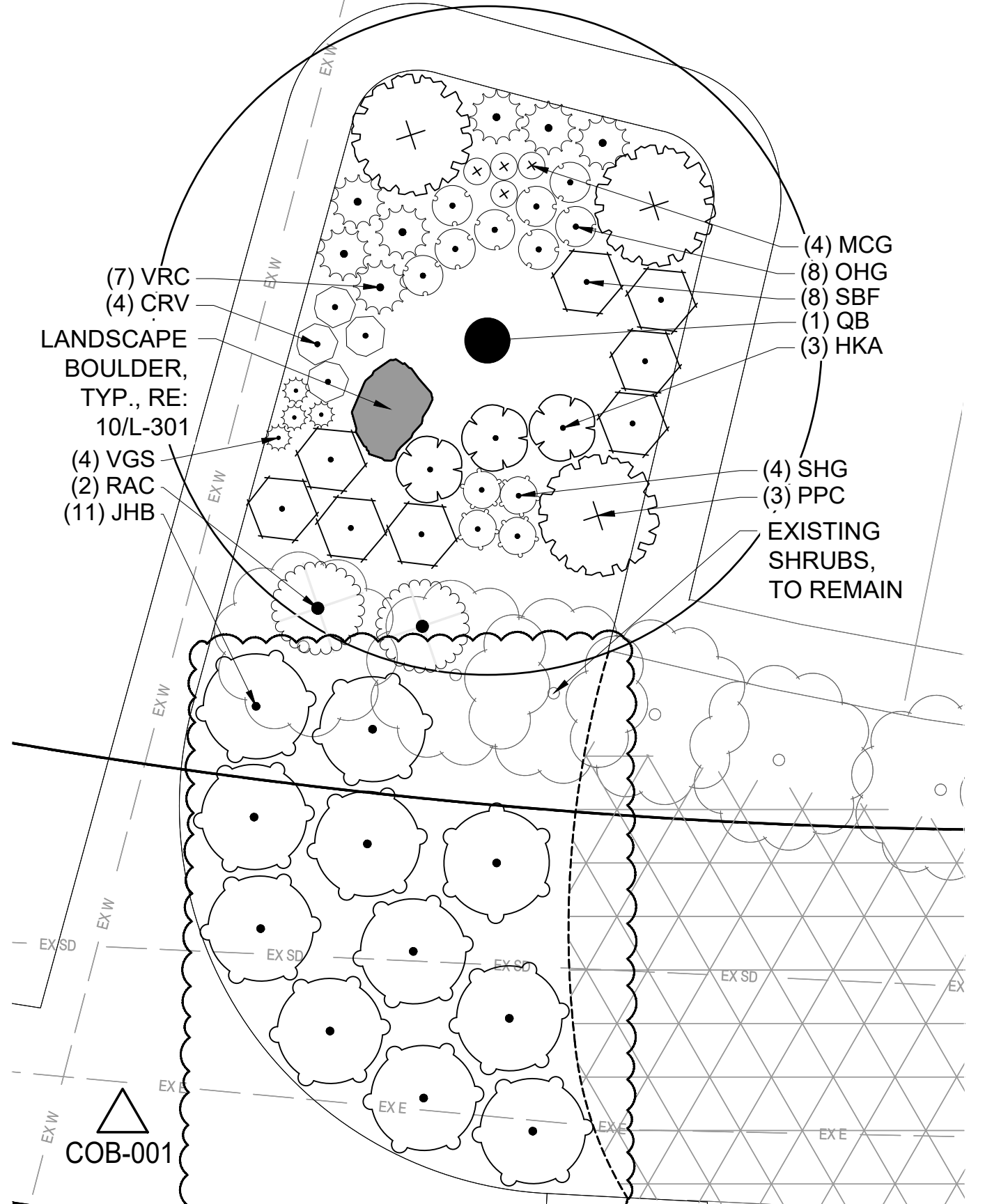
PLANT ABBREVIATION KEY

CODE	BOTANICAL / COMMON NAME
TREES	
AG	ACER GINNALA / AMUR MAPLE
CS	CATALPA SPECIOSA / NORTHERN CATALPA
GS	GLEDTISIA TRICANTIOS / NERIS / SHADEMASTER / HONEY LOCUST
GK	GYMNOCLADUS DIOICIA / KENTUCKY COFFEETREE
MR	MALUS X 'RADIANT' / RADIANT CRABAPPLE
MS	MALUS X 'SPRING SNOW' / SPRING SNOW CRABAPPLE
PA	PYRUS CALLERYANA 'AUTUMN BLAZE' / AUTUMN BLAZE CALLERY PEAR
QB	QUERCUS BICOLOR / SWAMP WHITE OAK
QF	QUERCUS ROBUR 'FASTIGIATA' / SKYROCKE® ENGLISH OAK
QR	QUERCUS RUBRA / RED OAK
TREES - EVERGREEN	
AC	ABIES CONCOLOR / WHITE FIR
PN	PINUS NIGRA / AUSTRIAN PINE
SHRUBS	
ASA	AMELANCHIER ALNIFOLIA / SASKATOON SERVICEBERRY
ALC	AMORPHA CANESCENS / LEADPLANT
AMS	ARONIA MELANOCARPA 'SMONAMPEN' / LOW SCAPE SNOWFIRE™ BLACK CHOKEBERRY
AMM	ARONIA MELANOCARPA 'UCONNAMI65' / LOW SCAPE MOUND® BLACK CHOKEBERRY
CBB	CARYOPTERIS X CLANDONENSIS / BLUE MIST SPIREA
CBQ	CORNUS SERICEA 'BAILEY' / BAILEY RED TWIG DOGWOOD
CSB	CYTISUS PURGANS 'SPANISH GOLD' / SPANISH GOLD BROOM
FNP	FORESTIERA NEOMEXICANA / NEW MEXICO PRIVET
HKA	HYPERICUM KALMIANUM 'AMES' / AMES ST. JOHNSWORT
PPC	PRUNUS BESSEYI 'POT15' / PAWNEE BUTTES® SAND CHERRY
RAC	RIBES ALPIMUM / ALPINE CURRANT
SBF	SPIRAEA X BIMALDA 'FIRELIGHT' / FIRELIGHT SPIREA
VDA	VIBURNUM DENTATUM 'ARROWWOOD' / ARROWWOOD VIBURNUM
GRASSES	
BGG	BOUTELOUA GRACILIS 'BLONDE AMBITION' / BLONDE AMBITION BLUE GRAMA
CAG	CALAMAGROSTIS X ACUTIFLORA 'AVALANCHE' / AVALANCHE FEATHER REED GRASS
ETG	ELYMUS TRACHYCAULUS SSP. TRACHYCAULUS / SLENDER WHEATGRASS
IRG	IMPERATA CYLINDRICA 'RED BARON' / JAPANESE BLOOD GRASS
MCG	MELICA DILATA / SILKY SPIKE MELIC GRASS
MPG	MISCANTHUS X 'PURPURASCENS' / PURPLE FLAME GRASS
MLG	MUHLENBERGIA CAPILLARIS 'LENCA' / REGAL MIST® PINK MUHLY GRASS
OHG	ORYZOPSIS HYMENOIDES / INDIAN RICEGRASS
PSG	PANICUM VIRGATUM 'SHENANDOAH' / SHENANDOAH SWITCH GRASS
PRG	PENNISETUM X 'CREPENSIS' 'HAMELN' / HAMELN DWARF FOUNTAIN GRASS
SNG	SORGHASTRUM NITANS / INDIAN GRASS
SHG	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPS EED
GROUND COVER	
AJK	ARCTOSTAPHYLOS UVA-URSI / KINNIKINNICK
CPP	CERATOSTIGMA PLUMBAGINOIDES / DWARF CREEPING PLUMBAGO
CVG	CHRYSOGONUM VIRGINIANUM / GREEN-AND-GOLD
PSS	PHLOX STOLONIFERA 'SHERWOOD PURPLE' / CREEPING PHLOX
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SHRUB EVERGREEN	
JHB	JUNIPERUS HORIZONTALIS 'BAR HARBOR' / BAR HARBOR CREEPING JUNIPER

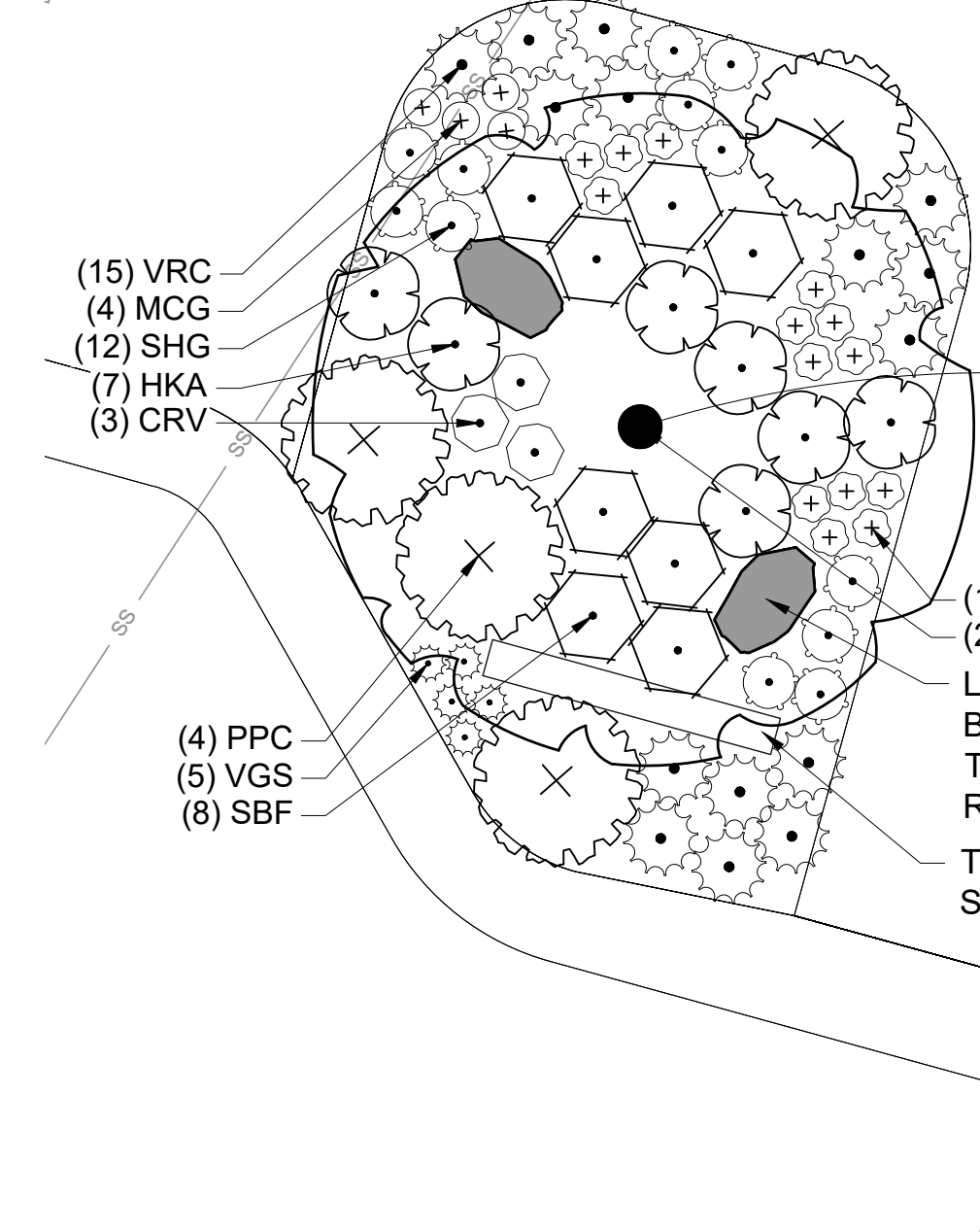
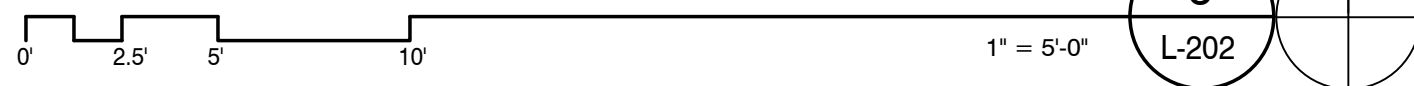
PLANTING ENLARGEMENT- PARKING ISLAND



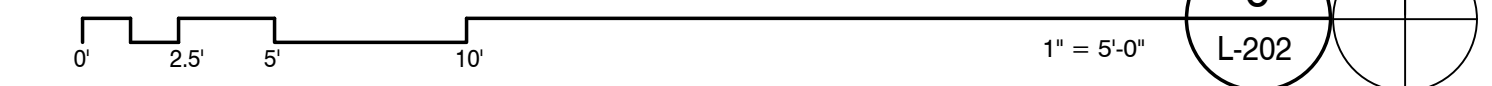
PLANTING ENLARGEMENT- PARKING ISLAND



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PLANTING ENLARGEMENTS

PLANT ABBREVIATION KEY

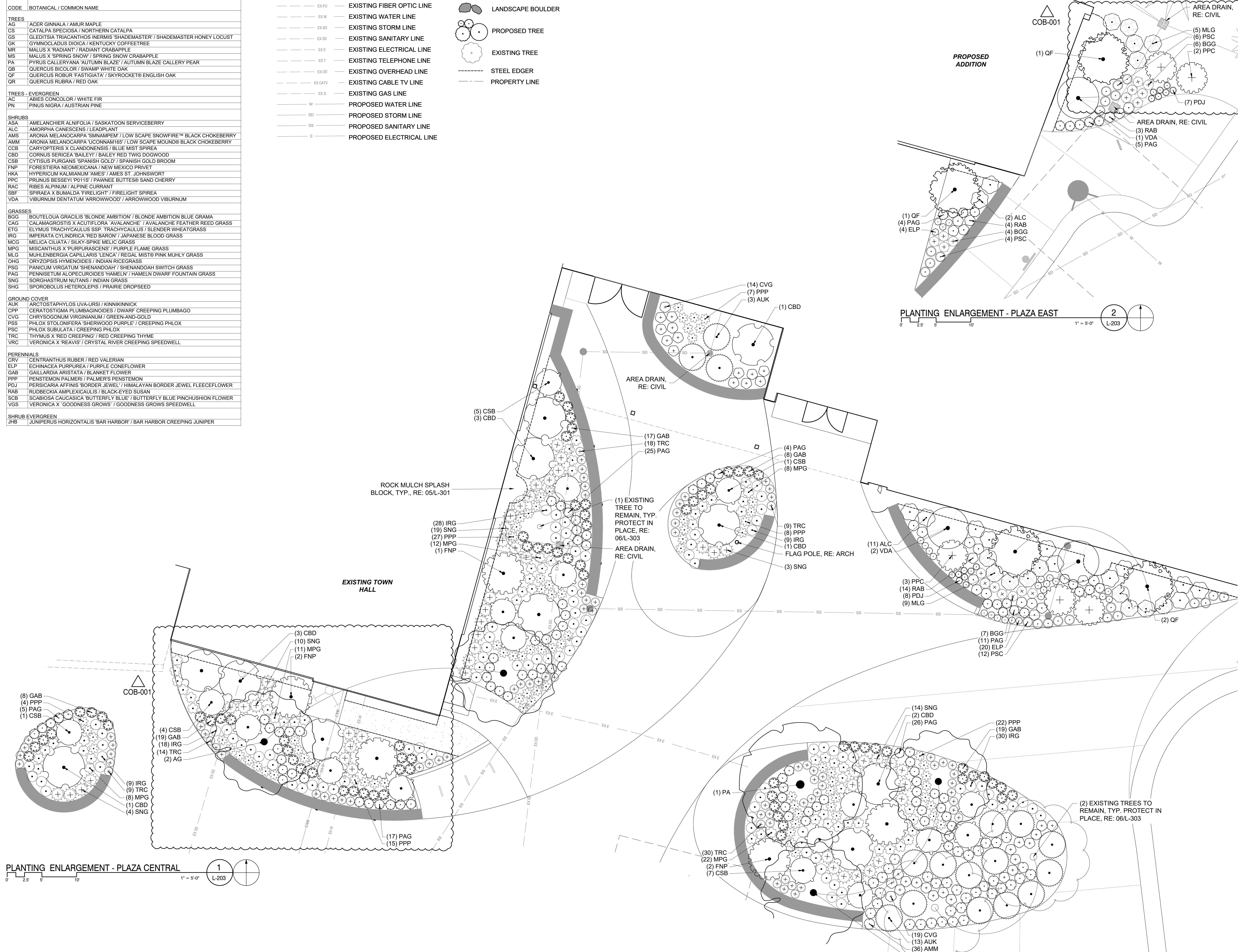
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TREES - EVERGREEN	
AC	ABIES CONCOLOR / WHITE FIR
PN	PINUS NIGRA / AUSTRIAN PINE
SHRUBS	
ASA	AMELANCHIER ALNIFOLIA / SASKATOON SERVICEBERRY
ALC	AMORPHA CANESCENS / LEADPLANT
AMS	ARONIA MELANOCARPA 'SMNAMPEN' / LOW SCAPE SNOWFIRE™ BLACK CHOKEBERRY
AMM	ARONIA MELANOCARPA 'UCONNAMI65' / LOW SCAPE MOUND® BLACK CHOKEBERRY
CCB	CARYOPTERIS X CLANDONENSIS / BLUE MIST SPIREA
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FNP	FORESTIERA NEOMEXICANA / NEW MEXICO PRIVET
HKA	HYPERICUM KALMIANUM 'AMES' / AMES ST. JOHNSWORT
PPC	PRUNUS BESSEYI 'POT1S' / PAWNEE BUTTES® SAND CHERRY
RAC	RIBES ALPINUM / ALPINE CURRANT
SBF	SPIRAEA X BUMALDA 'FIRELIGHT' / FIRELIGHT SPIREA
VDA	VIBURNUM DENTATUM 'ARROWWOOD' / ARROWWOOD VIBURNUM
GRASSES	
BGG	BOUTELOUA GRACILIS 'BLONDE AMBITION' / BLONDE AMBITION BLUE GRAMA
CAG	CALAMAGROSTIS X ACUTIFLORA 'AVALANCHE' / AVALANCHE FEATHER REED GRASS
ETG	ELYMUS TRACHYCAULUS SSP 'TRACHYCAULUS' / SLENDER WHEATGRASS
IRG	IMPERATA CYLINDRICA 'RED BARON' / JAPANESE BLOOD GRASS
MCG	MELICA CILIATA / SILKY-SPIKE MELIC GRASS
MPG	MISCANTHUS X 'PURPURASCENS' / PURPLE FLAME GRASS
MLG	MUHLENBERGIA CAPILLARIS 'LENCA' / REGAL MIST® PINK MUHLY GRASS
OHG	ORYZOPSIS HYMENOIDES / INDIAN RICEGRASS
PSC	PANICUM VIRGATUM 'SHEPHERD' / SHEPHERD SWITCH GRASS
PAG	PENNISETUM ALOPECUROIDES 'HADELN' / HADELN DWARF FOUNTAIN GRASS
SNG	SORGHASTRUM NUTANS / INDIAN GRASS
SHG	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSSEED
GROUND COVER	
AUK	ARCTOSTAPHYLOS UVA-URSI / KINNICKINICK
CPP	CERATOSTIGMA PLUMBAGINOIDES / DWARF CREEPING PLUMBAGO
CVG	CHRYSOGONUM VIRGINIANUM / GREEN-AND-GOLD
PSS	PHLOX STOLONIFERA 'SHERWOOD PURPLE' / CREEPING PHLOX
PSC	PHLOX SUBULATA / CREEPING PHLOX
TRC	THYMUS X 'RED CREEPING' / RED CREEPING THYME
VRC	VERONICA X 'REAVIS' / CRYSTAL RIVER CREEPING SPEEDWELL
PERENNIALS	
CRV	CENTRANTHUS RUBER / RED VALERIAN
ELP	ECHINACEA PURPUREA / PURPLE CONEFLOWER
GAB	GALLIARDA ARISTATA / BLANKET FLOWER
PPP	PENSTEMON PALMERI / PALMER'S PENSTEMON
PDJ	PERSICARIA AFFINIS 'BORDER JEWEL' / HIMALAYAN BORDER JEWEL FLEECEFLOWER
RAB	RUDBECKIA AMPLEXICAULIS / BLACK-EYED SUSAN
SCB	SCABIOSA CAUCASICA 'BUTTERFLY BLUE' / BUTTERFLY BLUE PINCHUSHION FLOWER
VGS	VERONICA X 'GOODNESS GROWS' / GOODNESS GROWS SPEEDWELL
SHRUB EVERGREEN	
JHB	JUNIPERUS HORIZONTALIS 'BAR HARBOR' / BAR HARBOR CREEPING JUNIPER

UTILITIES LEGEND

---	EX FO	EXISTING FIBER OPTIC LINE
---	EX W	EXISTING WATER LINE
---	EX SD	EXISTING STORM LINE
---	EX SS	EXISTING SANITARY LINE
---	EX E	EXISTING ELECTRICAL LINE
---	EX T	EXISTING TELEPHONE LINE
---	EX OE	EXISTING OVERHEAD LINE
---	EX CATV	EXISTING CABLE TV LINE
---	EX G	EXISTING GAS LINE
---	W	PROPOSED WATER LINE
---	SD	PROPOSED STORM LINE
---	SS	PROPOSED SANITARY LINE
---	E	PROPOSED ELECTRICAL LINE

LEGEND

	LANDSCAPE BOULDER
	PROPOSED TREE
	EXISTING TREE
	STEEL EDGER
	PROPERTY LINE



AndersonMasonDale Architects

Parker Town Hall Expansion

Parker, CO 22-093

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Parker, CO 80138

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Issue Date
PERMIT / GMP DRAWINGS 17 MAY 2024
COB-001 19 JULY 2024



Project Number: 22-093
Drawn By: JM
Reviewed By: TS
Approved By:

PLANTING ENLARGEMENTS

L-203

PLANT ABBREVIATION KEY

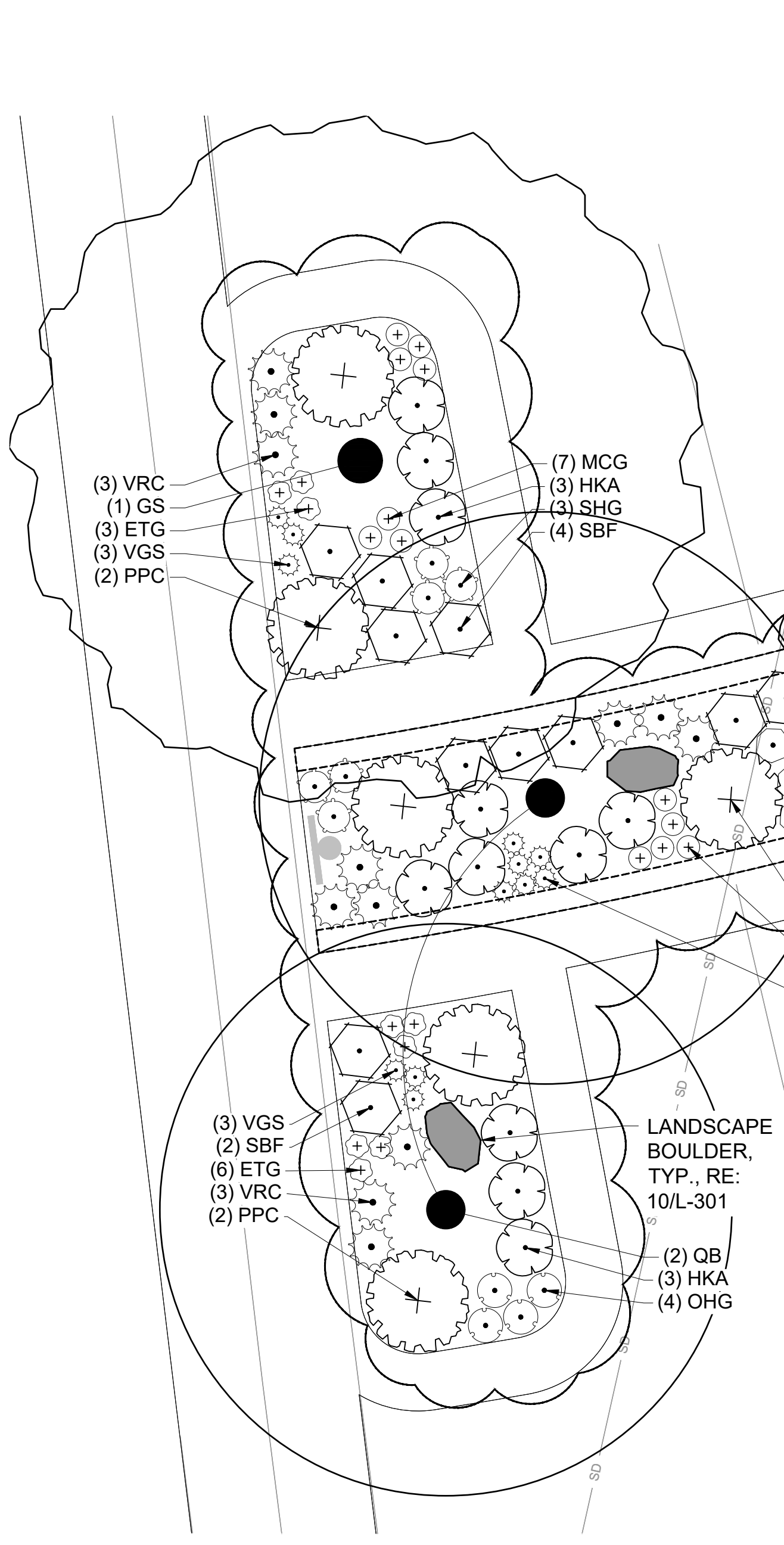
CODE	BOTANICAL / COMMON NAME
TREES	
AG	ACER GINNALA / AMUR MAPLE
CS	CATALPA SPECIOSA / NORTHERN CATALPA
GS	GLEDITSIA TRIACANTHOS INERMIS / SHADEMASTER / SHADEMASTER HONEY LOCUST
GK	GYMNOCLADUS DIOICA / KENTUCKY COFFEETREE
MR	MALUS X 'RADIANT' / RADIANT CRABAPPLE
MS	MALUS X 'SPRING SNOW' / SPRING SNOW CRABAPPLE
PA	PIRUS CALLERYANA 'AUTUMN BLAZE' / AUTUMN BLAZE CALLERY PEAR
QB	QUERCUS BICOLOR / SWAMP WHITE OAK
QF	QUERCUS ROBUR 'FASTIGIATA' / SKYROCKET® ENGLISH OAK
QR	QUERCUS RUBRA / RED OAK
TREES - EVERGREEN	
AC	ABIES CONCOLOR / WHITE FIR
PN	PINUS NIGRA / AUSTRIAN PINE
SHRUBS	
ASA	AMELANCHIER ALNIFOLIA / SASKATOON SERVICEBERRY
ALC	AMORPHIA CANESCENS / LEADPLANT
AMS	ARONIA MELANOCARPA 'SMNMPM' / LOW SCAPE SNOWFIRE™ BLACK CHOKEBERRY
AMM	ARONIA MELANOCARPA 'UCNANM165' / LOW SCAPE MOUND® BLACK CHOKEBERRY
CCB	CARYOPTERIS X CLANDONENSIS / BLUE MIST SPIREA
CBD	CORNUS SERICEA 'BAILEY' / BAILEY RED TWIG DOGWOOD
CSB	CYTISUS PURGANS 'SPANISH GOLD' / SPANISH GOLD BROOM
FNP	FORESTIERA NEOMEXICANA / NEW MEXICO PRIVET
HKA	HYPERICUM KALMIANUM 'AMES' / AMES ST. JOHNSWORT
PPC	PRUNUS BESSEYI 'P0115' / PAWNEE BUTTES® SAND CHERRY
RAC	RIBES ALPINUM / ALPINE CURRANT
SBF	SPIRAEA X BUMALDA 'FIRELIGHT' / FIRELIGHT SPIREA
VDA	VIBURNUM DENTATUM 'ARROWWOOD' / ARROWWOOD VIBURNUM
GRASSES	
BGG	BOUTELOUA GRACILIS 'BLONDE AMBITION' / BLONDE AMBITION BLUE GRAMA
CAG	CALAMAGROSTIS X ACUTIFLORA 'AVALANCHE' / AVALANCHE FEATHER REED GRASS
ETG	ELYMUS TRACHYCAULUS SSP. TRACHYCAULUS / SLENDER WHEATGRASS
IRG	IMPERATA CYLINDRICA 'RED BARON' / JAPANESE BLOOD GRASS
MCG	MELICA CILIATA / SILKY-SPIKE MELIC GRASS
MPG	MISCANTHUS X 'PURPURASCENS' / PURPLE FLAME GRASS
MLG	MUHLENBERGIA CAPILLARIS 'LENCA' / REGAL MIST® PINK MUHLY GRASS
OHG	ORYZOPSIS HYMENOIDES / INDIAN RICEGRASS
PSG	PANICUM VIRGATUM 'SHEWANDA®' / SHEWANDA® SWITCH GRASS
PAG	PENNISETUM ALOPECUROIDES 'HAMELN' / HAMELN DWARF FOUNTAIN GRASS
SNG	SORGHASTRUM NUTANS / INDIAN GRASS
SHG	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSSEED
GROUND COVER	
AUK	ARCTOSTAPHYLOS UVA-URSI / KINNICKINICK
CPP	CERATOSTIGMA PLUMBAGINOIDES / DWARF CREEPING PLUMBAGO
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UTILITIES LEGEND

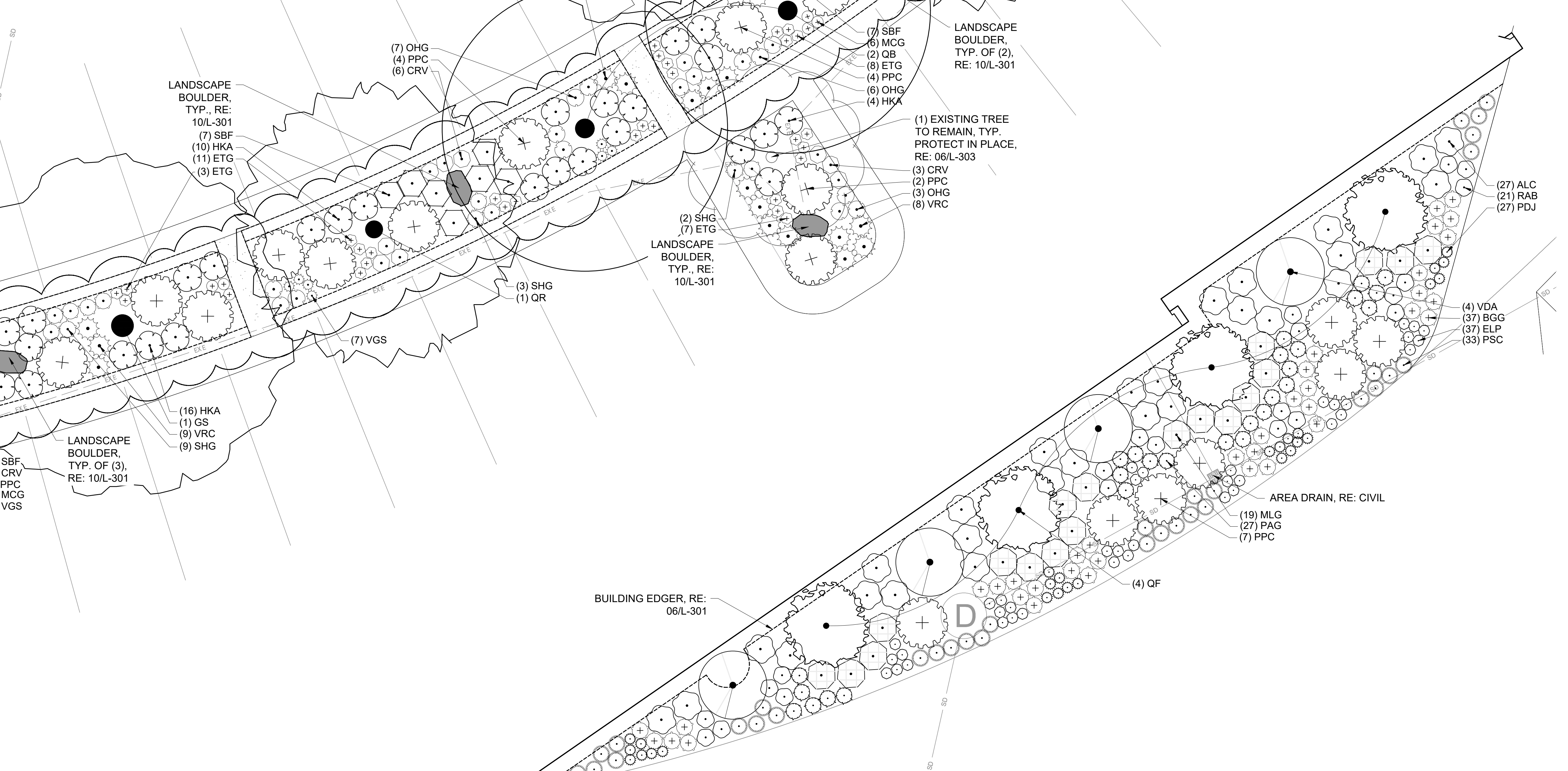
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---	EX SS	EXISTING SANITARY LINE
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LEGEND

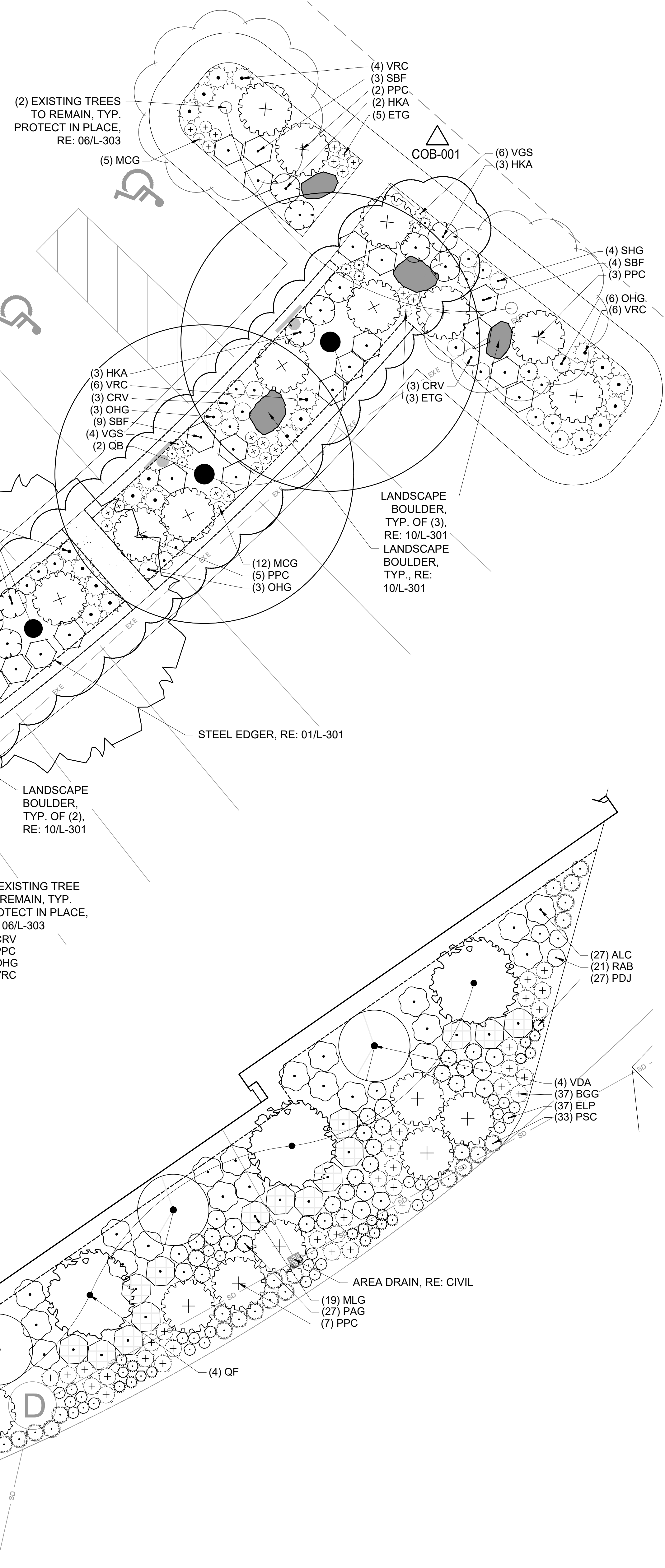
	LANDSCAPE BOULDER
	PROPOSED TREE
	EXISTING TREE
	STEEL EDGER
	PROPERTY LINE



PLANTING ENLARGEMENT - CENTRAL PARKING ISLAND
 Scale: 1" = 5'-0"
 Drawing ID: L-204



PLANTING ENLARGEMENT - BUILDING EDGE
 Scale: 1" = 5'-0"
 Drawing ID: L-204



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 Parker, CO 22-093
 Town of Parker
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 Parker, CO 80138

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PLANTING ENLARGEMENTS

L-204

LEGEND

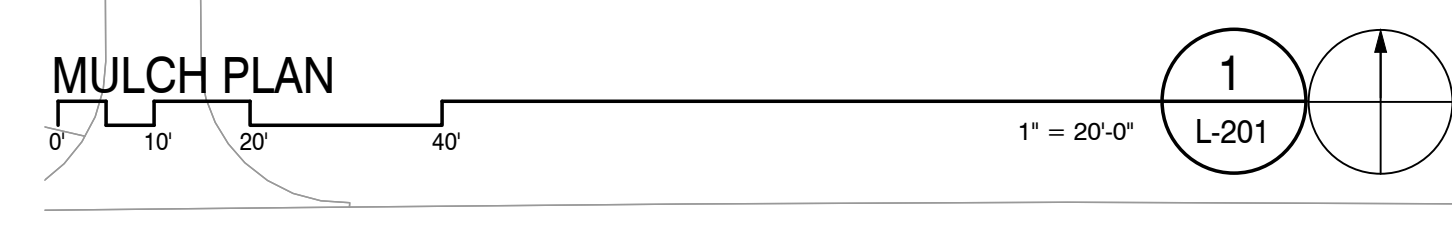
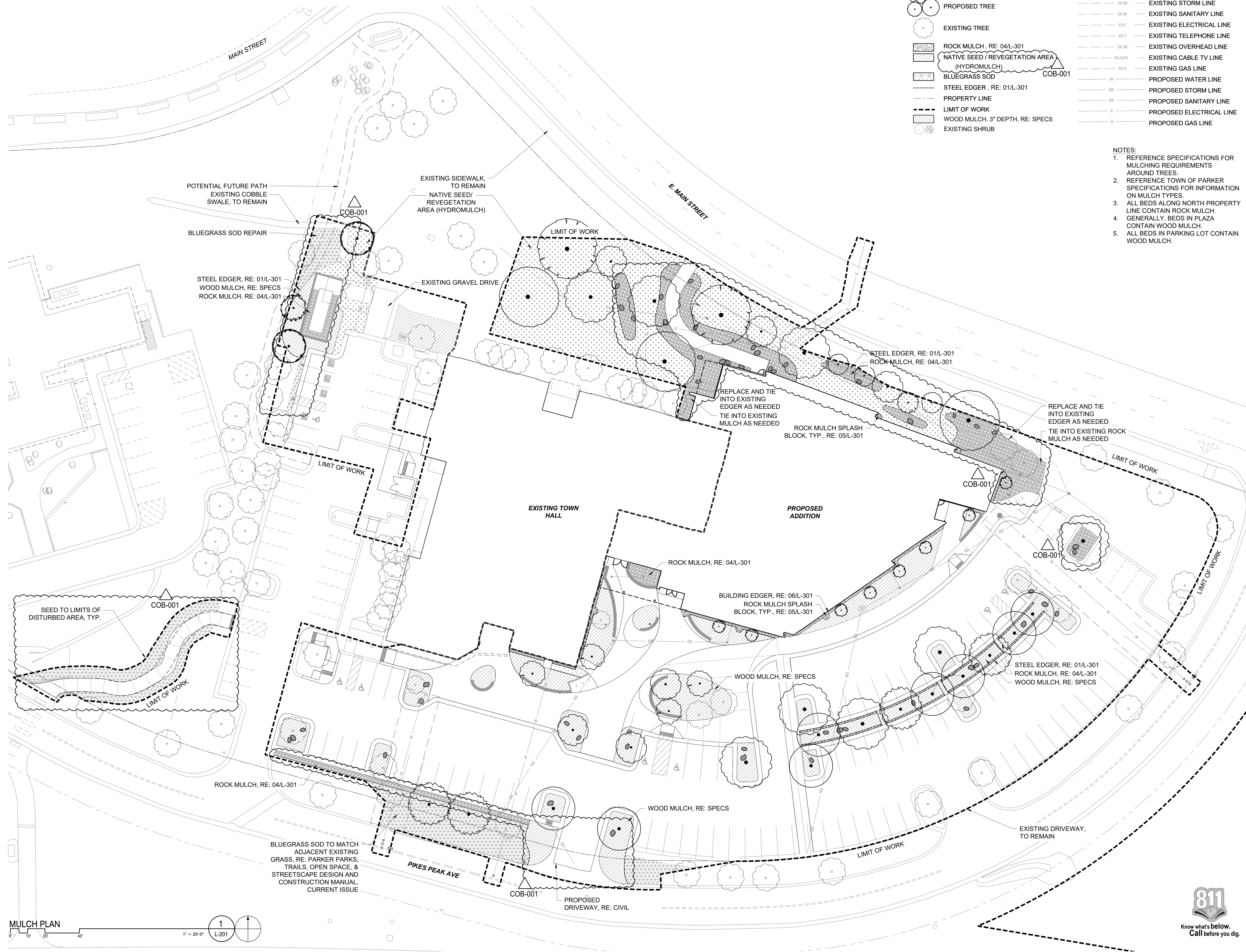
- LANDSCAPE BOULDER
- PROPOSED TREE
- EXISTING TREE
- ROCK MULCH, RE: 04/L-301
- NATIVE SEED / REVEGETATION AREA (HYDROMULCH)
- BLUEGRASS SOD
- STEEL EDGER, RE: 01/L-301
- PROPERTY LINE
- LIMIT OF WORK
- WOOD MULCH, 3" DEPTH, RE: SPECS
- EXISTING SHRUB

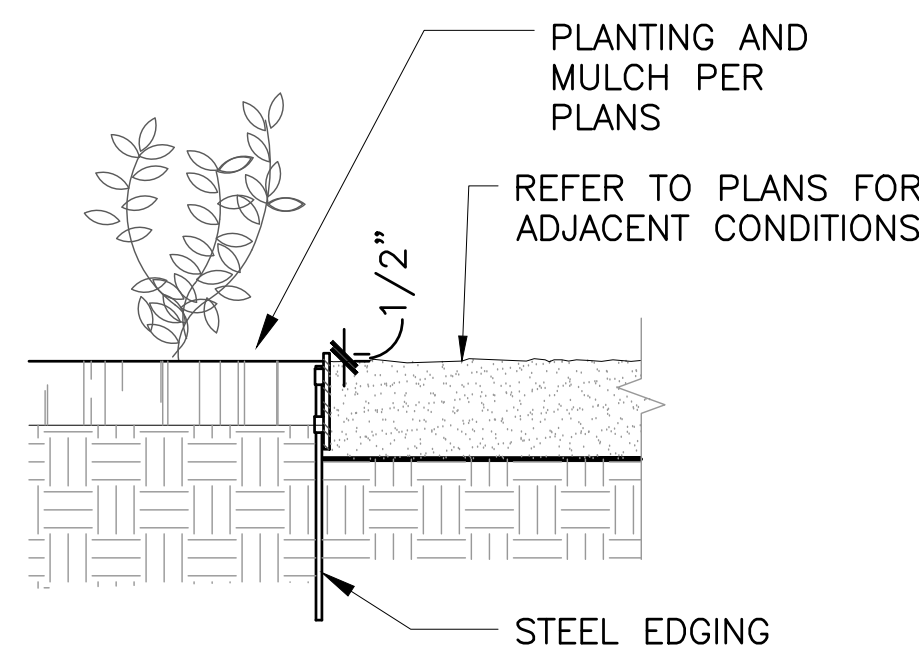
UTILITIES LEGEND

- EXFO — EXISTING FIBER OPTIC LINE
- EXW — EXISTING WATER LINE
- EXSD — EXISTING STORM LINE
- EXSS — EXISTING SANITARY LINE
- EXE — EXISTING ELECTRICAL LINE
- EXT — EXISTING TELEPHONE LINE
- EXOE — EXISTING OVERHEAD LINE
- EXCATV — EXISTING CABLE TV LINE
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- W — PROPOSED WATER LINE
- SD — PROPOSED STORM LINE
- SS — PROPOSED SANITARY LINE
- E — PROPOSED ELECTRICAL LINE
- G — PROPOSED GAS LINE

NOTES:

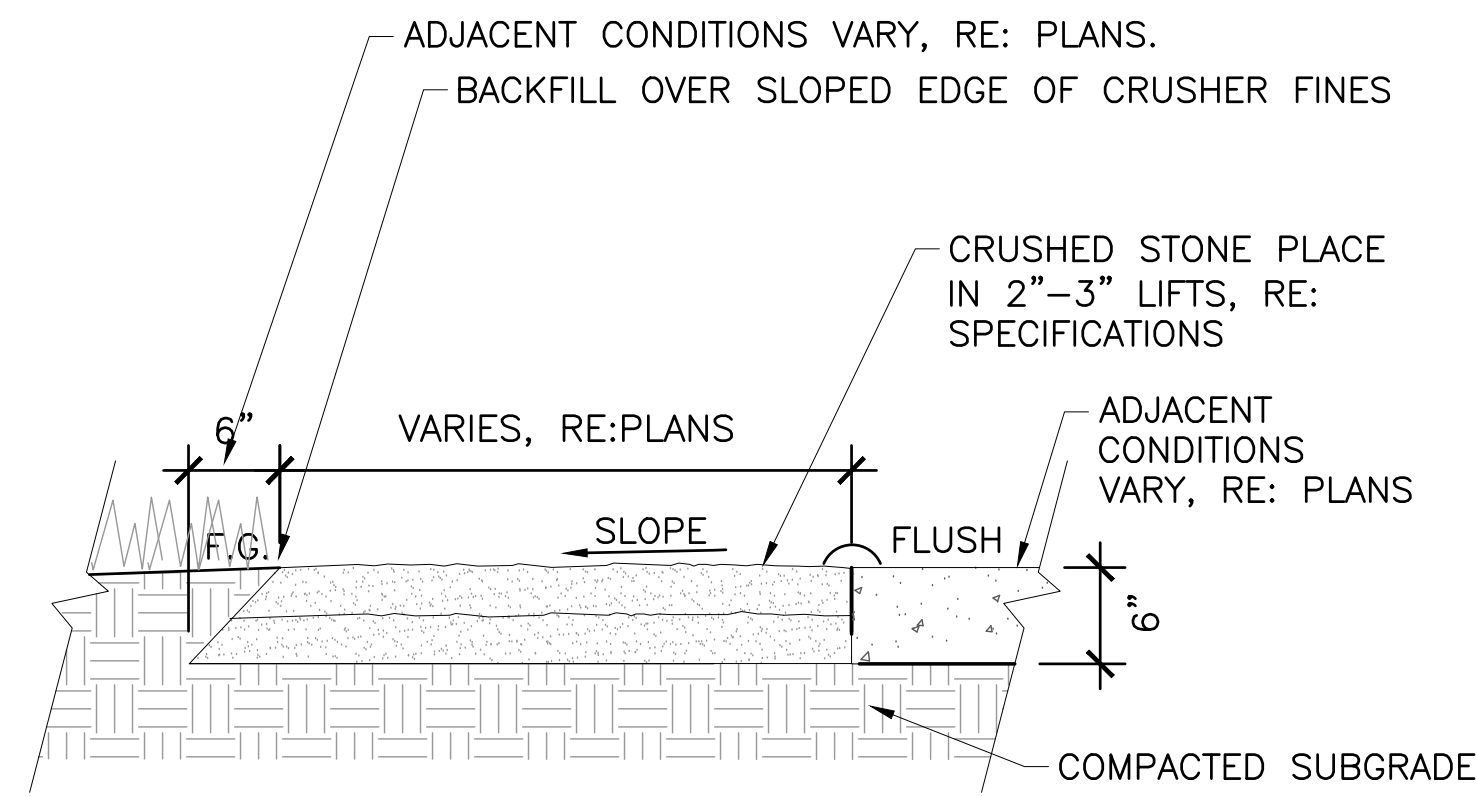
1. REFERENCE SPECIFICATIONS FOR MULCHING REQUIREMENTS AROUND TREES.
2. REFERENCE TOWN OF PARKER SPECIFICATIONS FOR INFORMATION ON MULCH TYPES.
3. ALL BEDS ALONG NORTH PROPERTY LINE CONTAIN ROCK MULCH. GENERALLY, BEDS IN PLAZA CONTAIN WOOD MULCH.
4. ALL BEDS IN PARKING LOT CONTAIN WOOD MULCH.





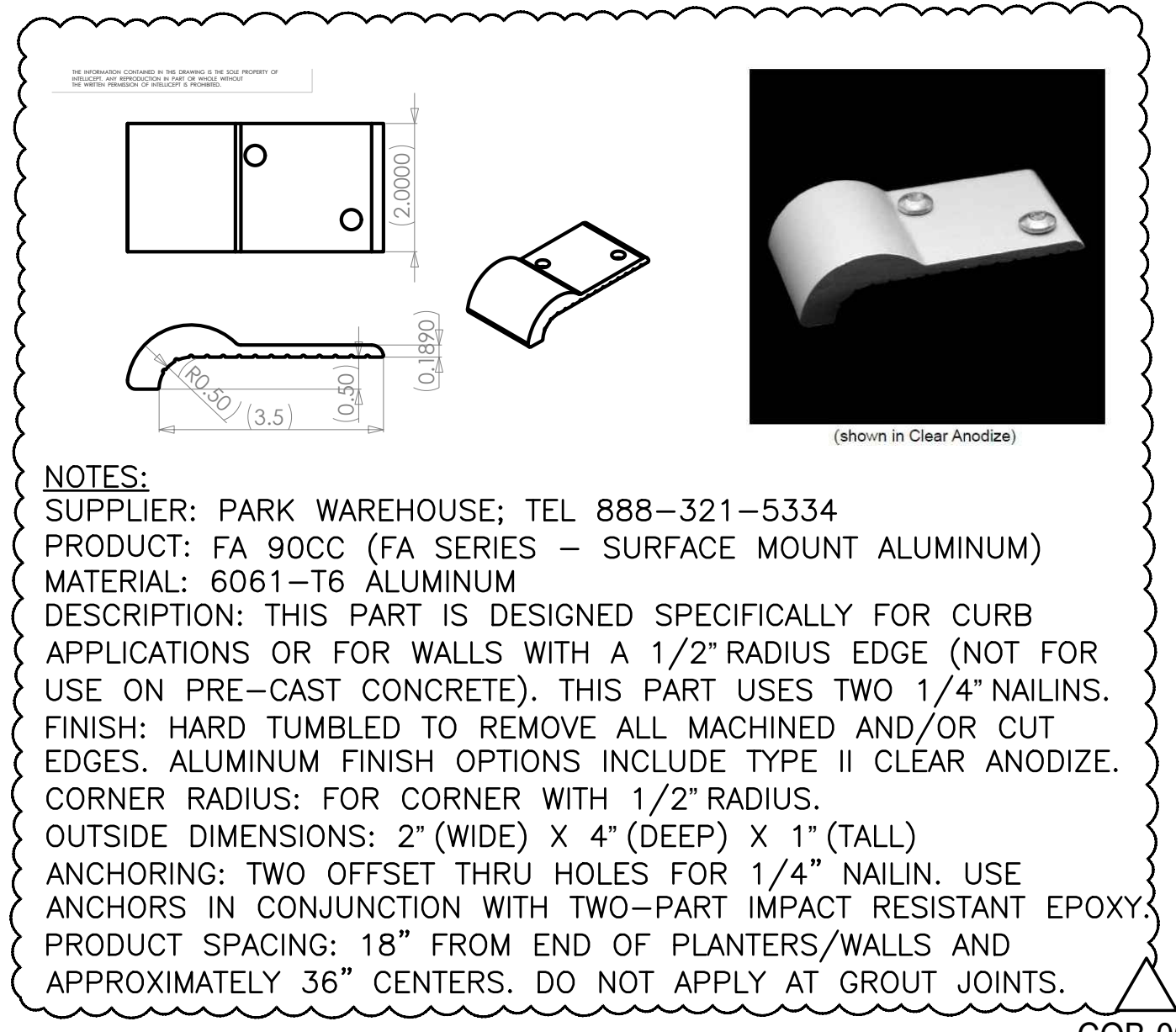
NOTES:
 1. STEEL EDGER SHALL BE COMMERCIAL TYPE ROLLED TOP STEEL EDGING, 1/2" X 6" HEIGHT X 16" LENGTH WITH TAPERED STEEL STAKES. INSTALL TOP OF EDGER FLUSH W/GROUND SURFACE.

01 STEEL EDGER
NTS

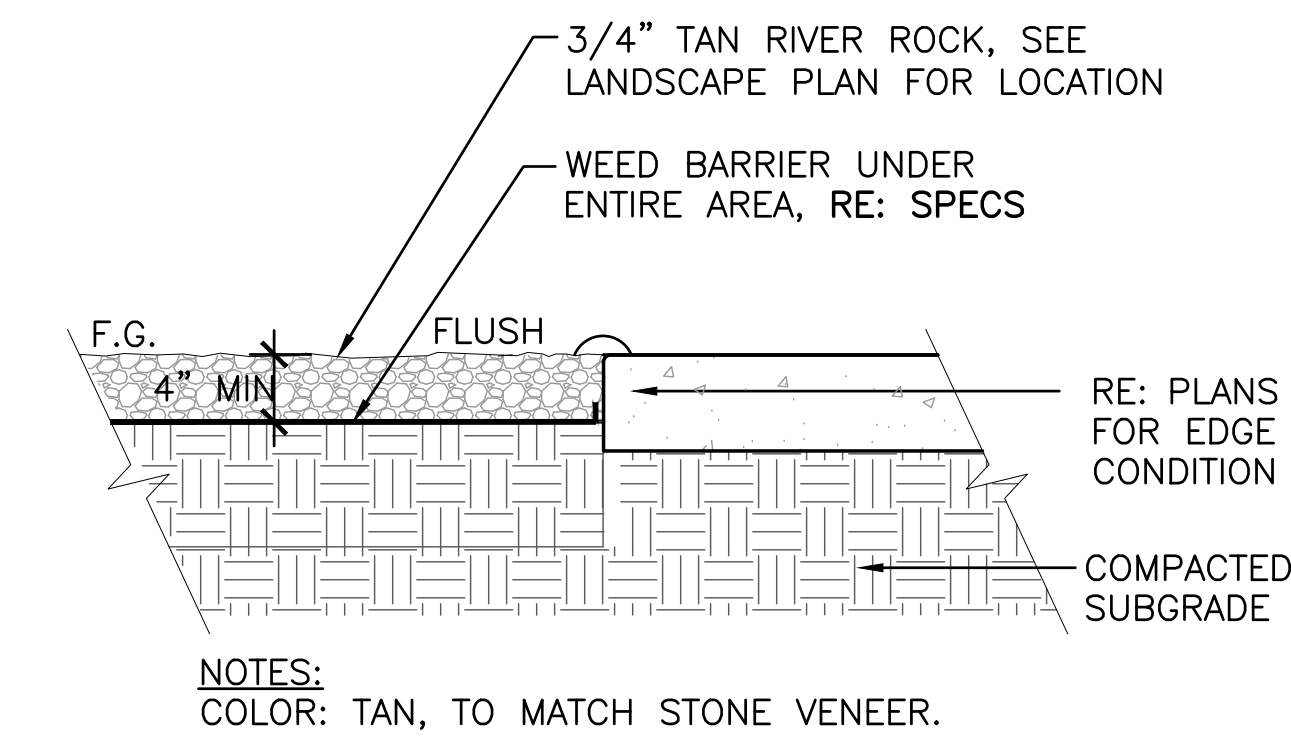


NOTES:
 1. CRUSHER FINES PAVING SHALL BE PLACED IN (2) 3" LIFTS. MIN.
 2. ALL CRUSHER FINES SHALL BE STABILIZED. REFERENCE SPECIFICATIONS FOR MORE INFORMATION
 3. REFER TO SPEC PRODUCT/INSTALLATION INFO.
 4. COLOR: TAN

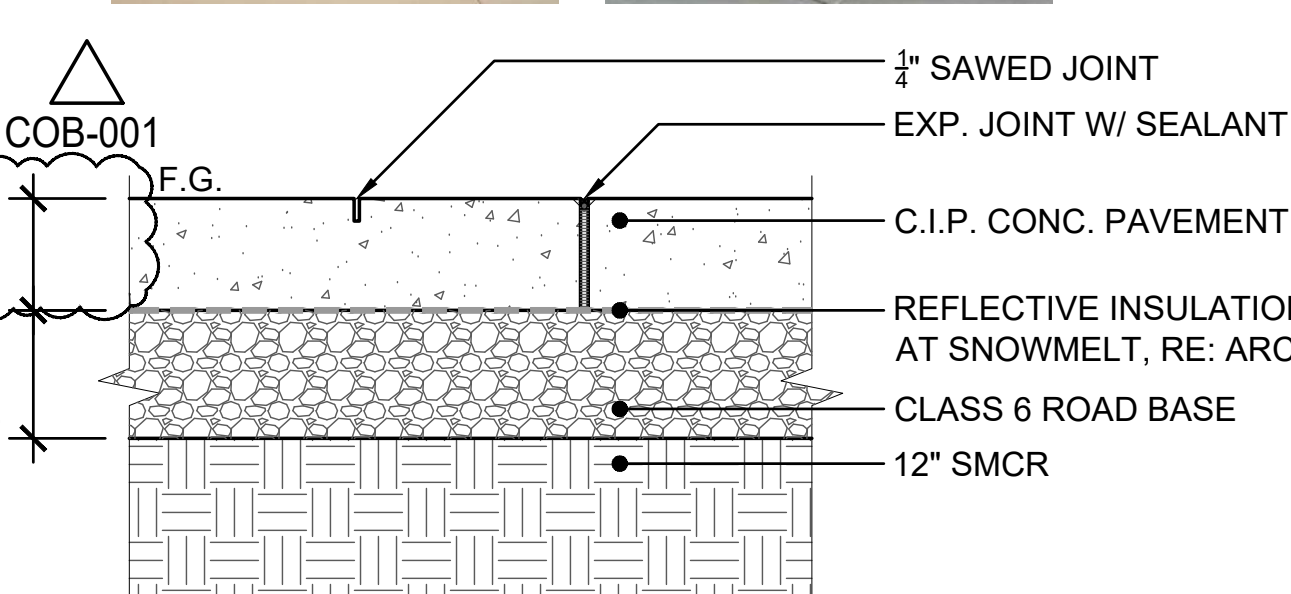
02 CRUSHER FINES PAVING
1"=1'-0"



03 SKATE STOP
NTS

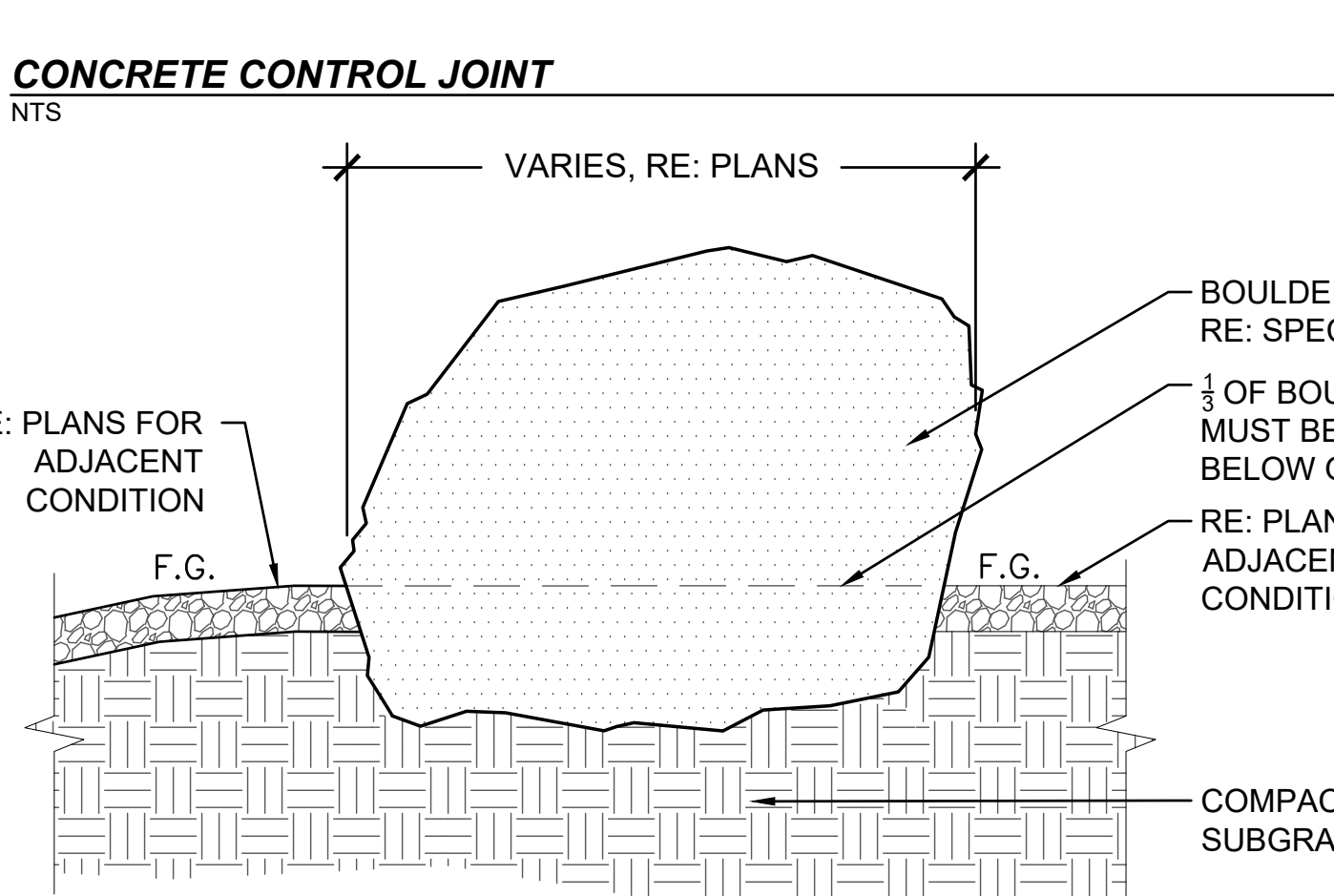


04 ROCK MULCH
NTS



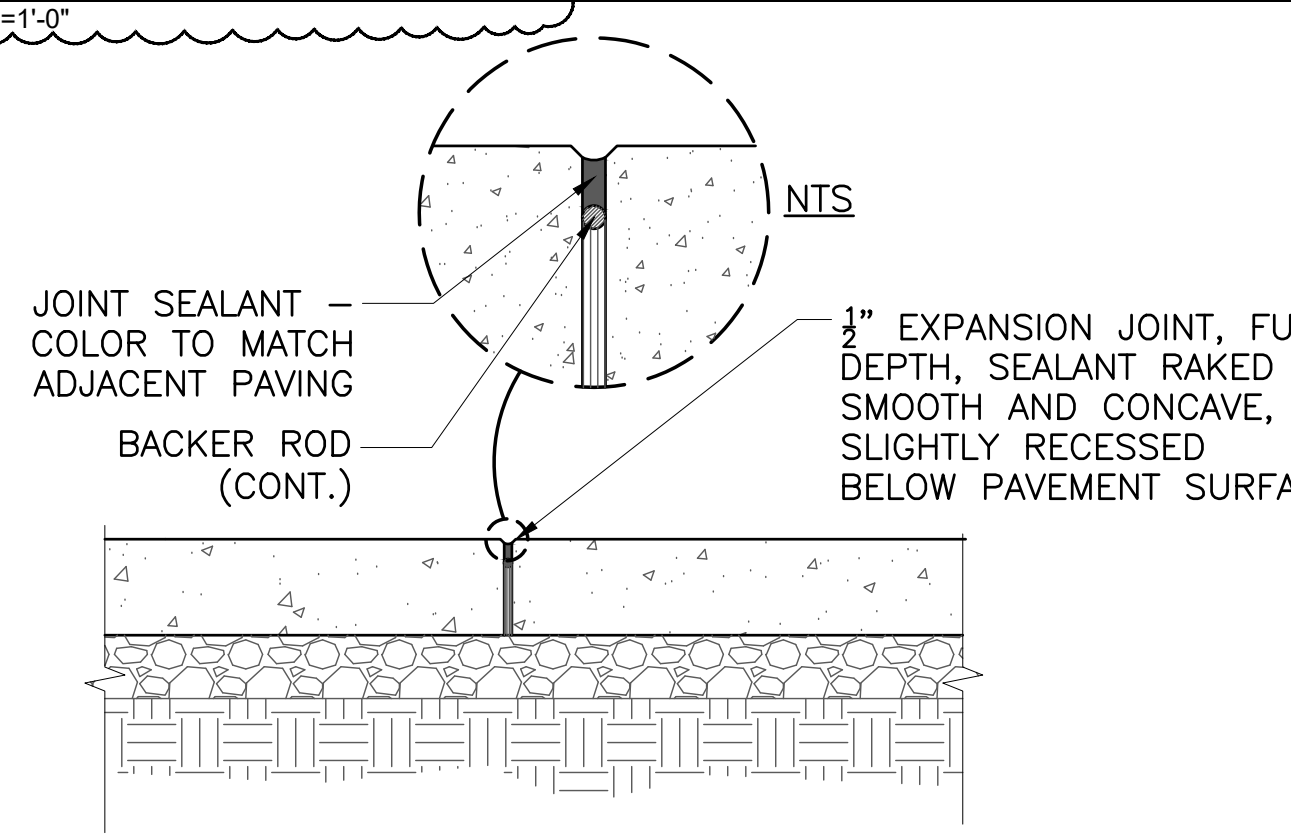
NOTES:
 1. REFER TO PLANS & SPECIFICATIONS FOR FINISHES.
 2. PROVIDE CONTROL & EXPANSION JOINTS WHERE INDICATED ON PLANS.
 3. PROVIDE SAWCUT JOINTS NOT MORE THAN 10'-0" ON CENTER, OR AS INDICATED ON THE PLANS.
 4. ALL CONCRETE PAVING SHALL BE 6" DEPTH.
 5. ALL WALKS TO HAVE 1.8% MAX CROSS SLOPE.
 6. REFERENCE SPECIFICATIONS FOR CONCRETE STRENGTH AND REINFORCEMENT REQUIREMENTS.
 7. SEE CIVIL FOR VEHICULAR CONCRETE - PEDESTRIAN.

08 CONCRETE PAVING AT ENTRY
1"=1'-0"



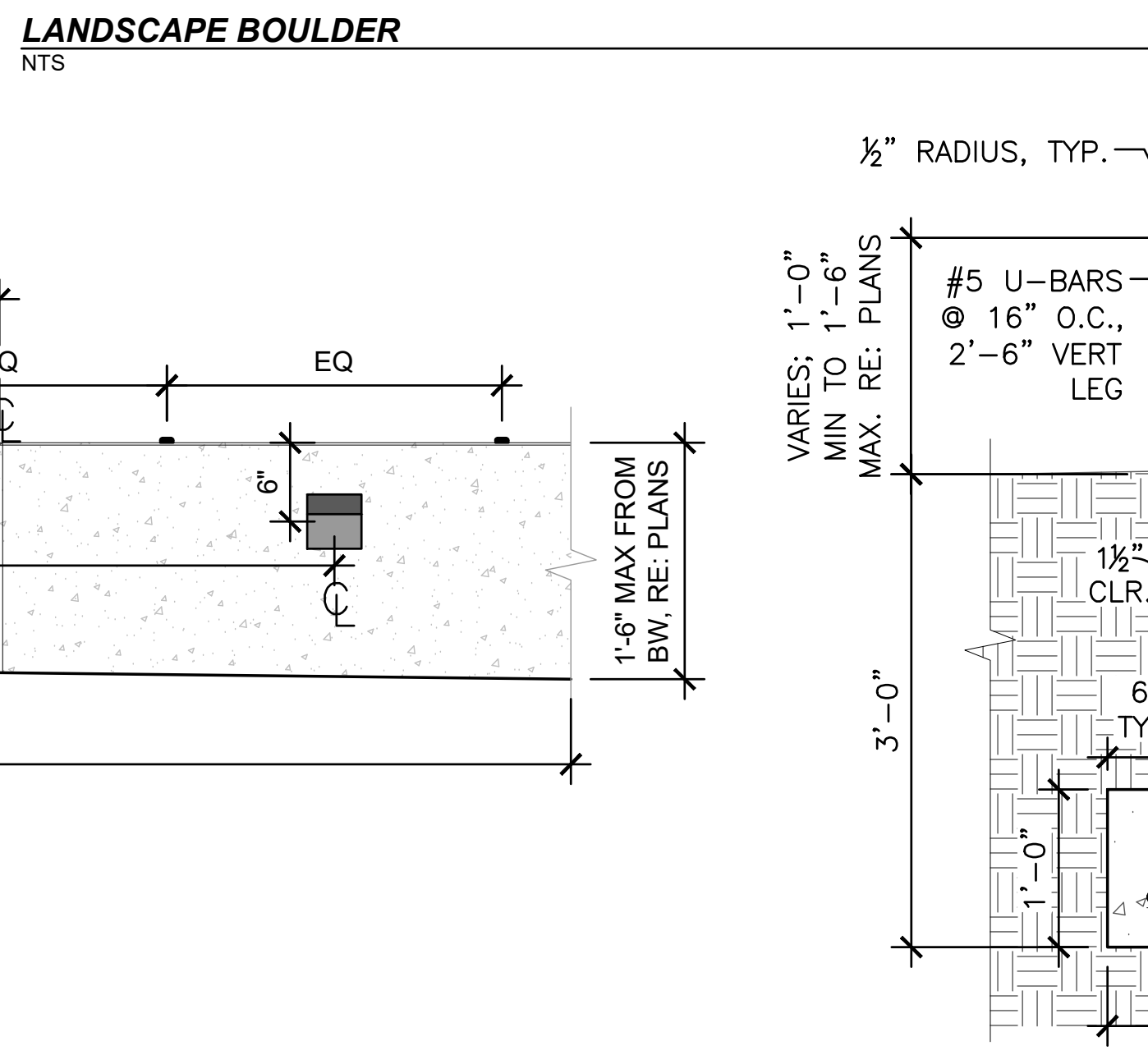
NOTES:
 1. COORDINATE BOULDER LOCATIONS IN THE FIELD WITH LANDSCAPE ARCHITECT OR OWNER'S REPRESENTATIVE.
 2. TOP OF BOULDER ELEVATION SHOULD NOT EXCEED 30" IN HEIGHT FROM ADJACENT FINAL GRADE.
 3. CONTRACTOR SHALL ANTICIPATE REHANDLING OF STONE BOULDERS TO ACHIEVE DESIRED DESIGN INTENT.

07 CONCRETE CONTROL JOINT
NTS

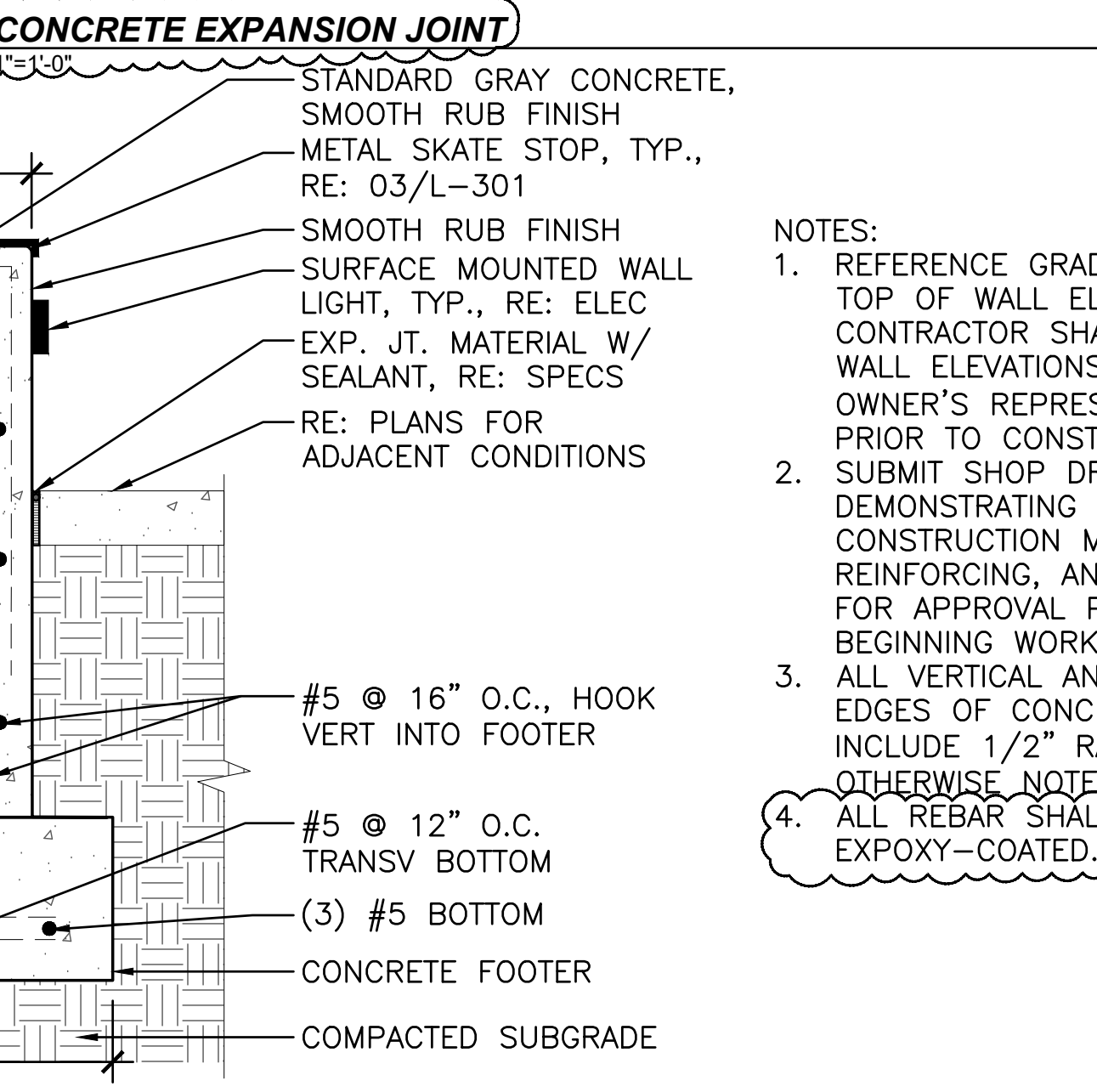


NOTE:
 1. REFERENCE SPECIFICATIONS FOR SEALANT COLOR (TO MATCH ADJACENT COLOR).
 2. REFERENCE PLAN FOR CONTROL JOINT LOCATIONS.
 3. CONTRACTOR TO PROVIDE MOCK-UP FOR REVIEW AND APPROVAL.

09 CONCRETE PAVING AT NEW SIDEWALKS
1"=1'-0"

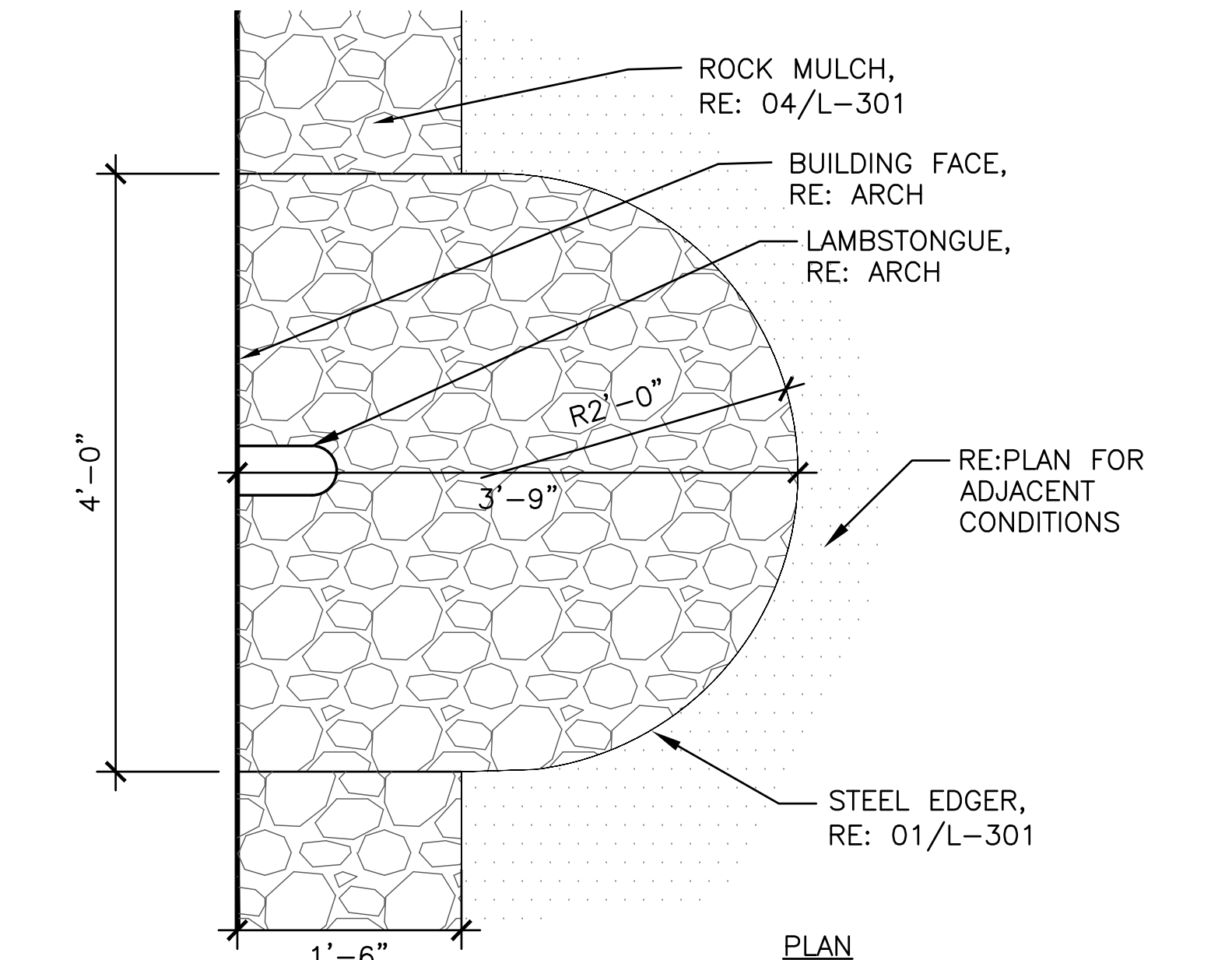


10 LANDSCAPE BOULDER
NTS

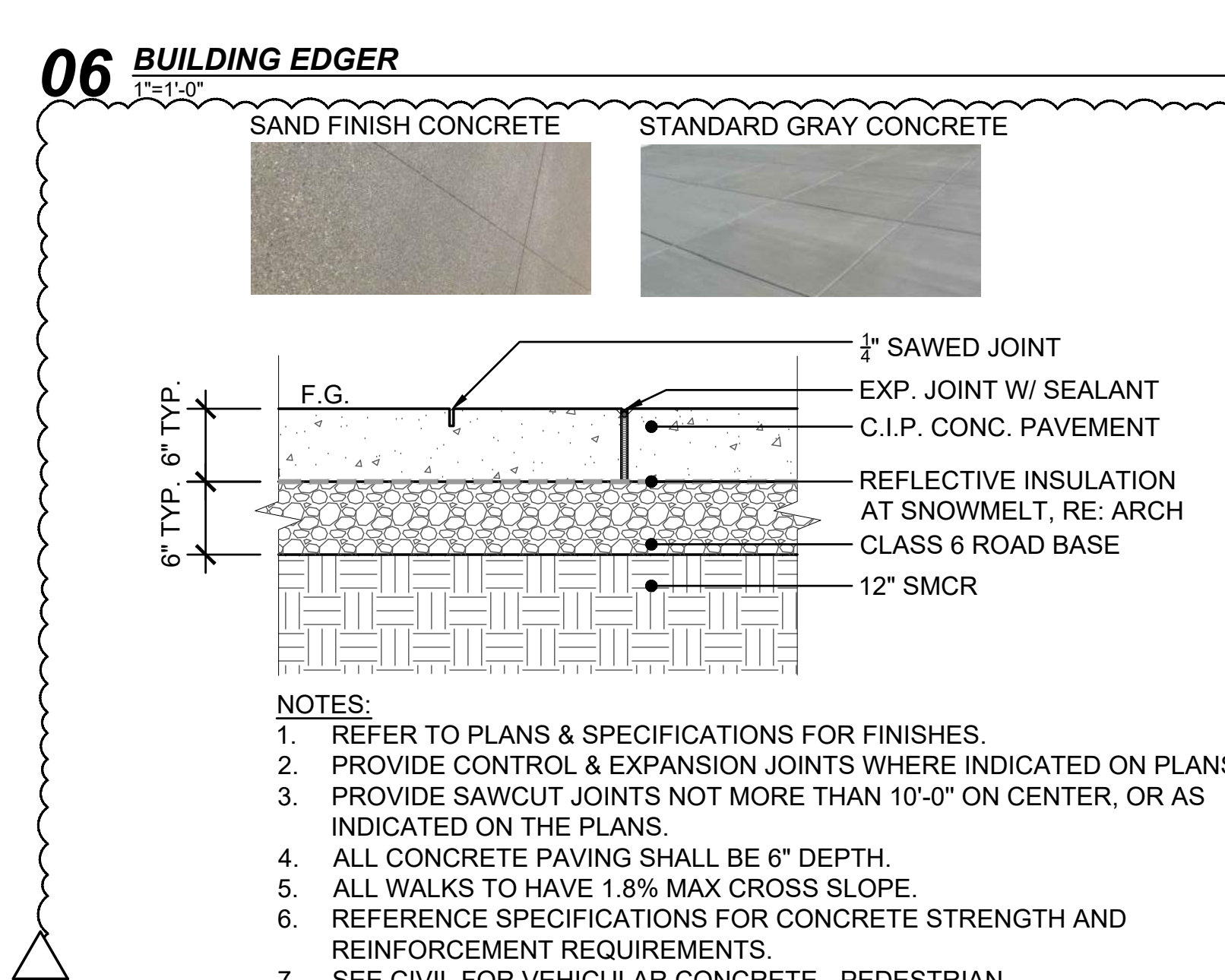


NOTES:
 1. REFERENCE GRADING PLAN FOR TOP OF WALL ELEVATIONS. CONTRACTOR SHALL VERIFY WALL ELEVATIONS WITH OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION.
 2. SUBMIT SHOP DRAWINGS DEMONSTRATING PREFERRED CONSTRUCTION METHOD, REINFORCING, AND JOINTING FOR APPROVAL PRIOR TO BEGINNING WORK.
 3. ALL VERTICAL AND HORIZONTAL EDGES OF CONCRETE TO INCLUDE 1/2" RADIUS IF NOT OTHERWISE NOTED.
 4. ALL REBAR SHALL BE EXPOXY-COATED.

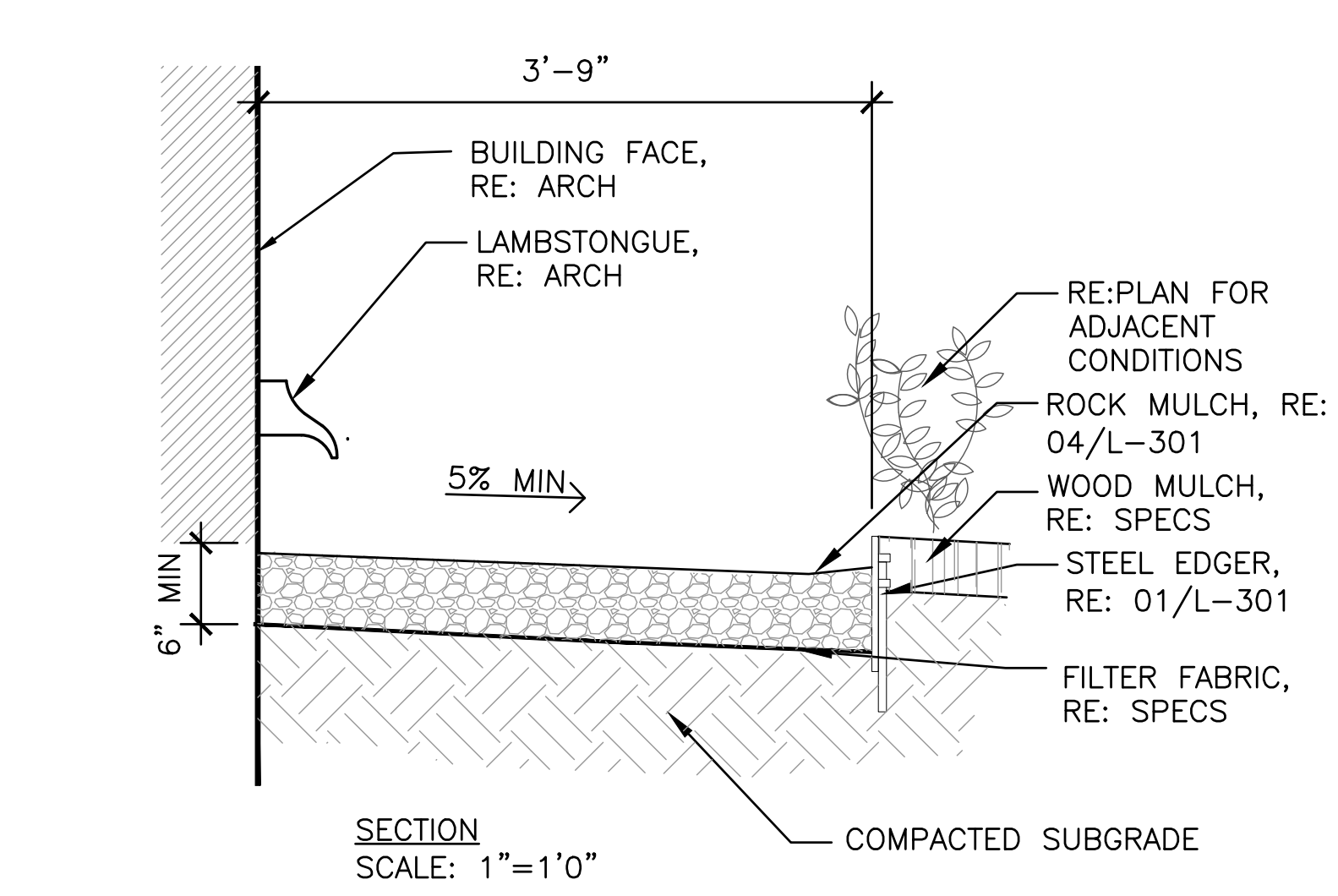
11 CONCRETE EXPANSION JOINT
1"=1'-0"



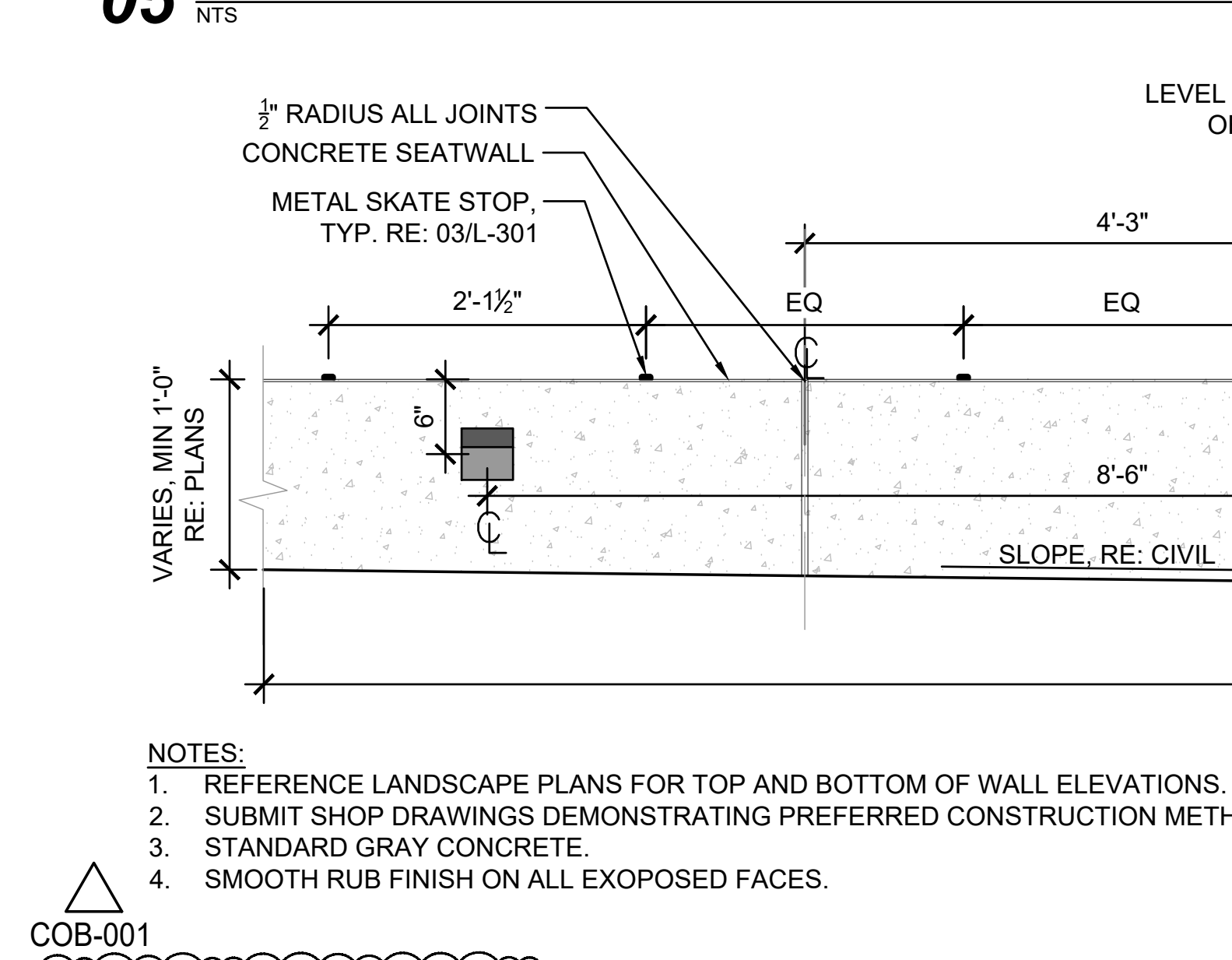
05 ROCK MULCH SPLASH BLOCK
NTS



06 BUILDING EDGER
1"=1'-0"

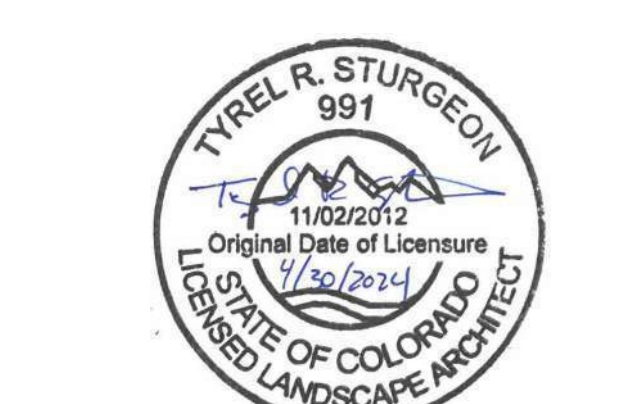


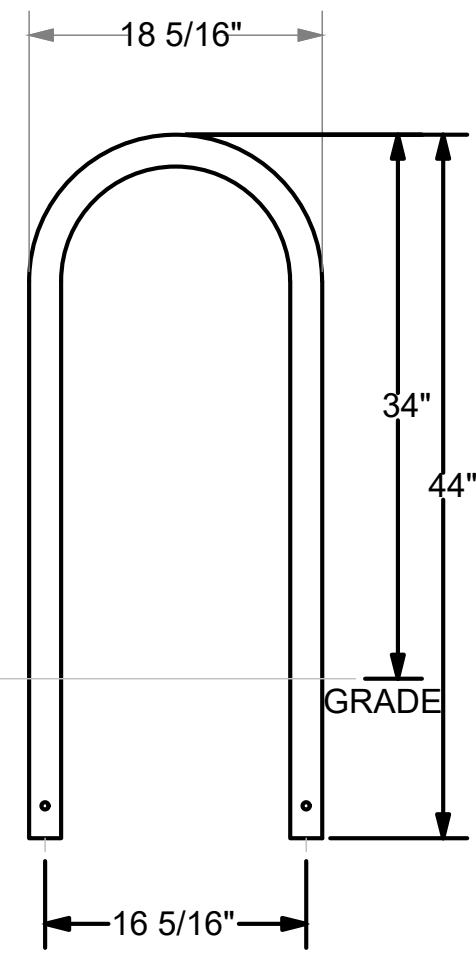
05 ROCK MULCH SPLASH BLOCK
NTS



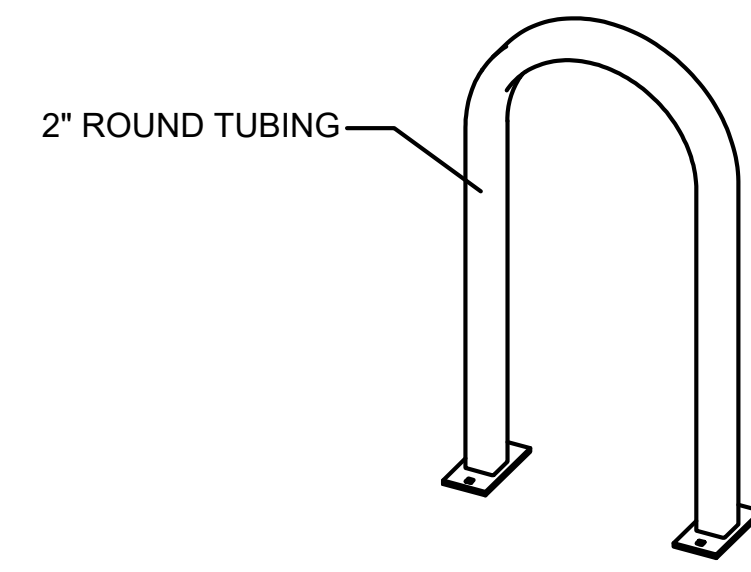
12 CONCRETE SEATWALL
1"=1'-0"

NOTES:
 1. REFERENCE LANDSCAPE PLANS FOR TOP AND BOTTOM OF WALL ELEVATIONS. CONTRACTOR SHALL VERIFY WALL ELEVATIONS WITH OWNER'S REPRESENTATIVE PRIOR TO CONSTRUCTION.
 2. SUBMIT SHOP DRAWINGS DEMONSTRATING PREFERRED CONSTRUCTION METHOD, REINFORCING, AND JOINTING FOR APPROVAL PRIOR TO BEGINNING WORK.
 3. STANDARD GRAY CONCRETE.
 4. SMOOTH RUB FINISH ON ALL EXPOSED FACES.

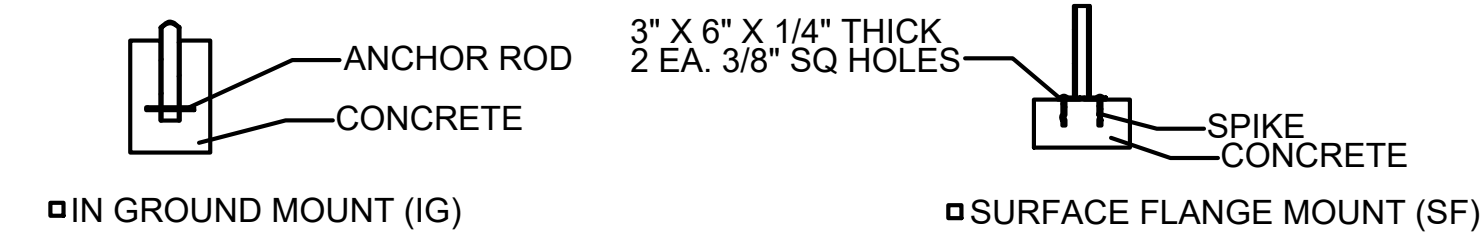




MADRAX DIVISION
GRABER MANUFACTURING, INC.
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P(800) 448-7931, P(608) 849-1080, F(608) 849-1081
WWW.MADRAX.COM, E-MAIL: SALES@MADRAX.COM



CHECK DESIRED MOUNT

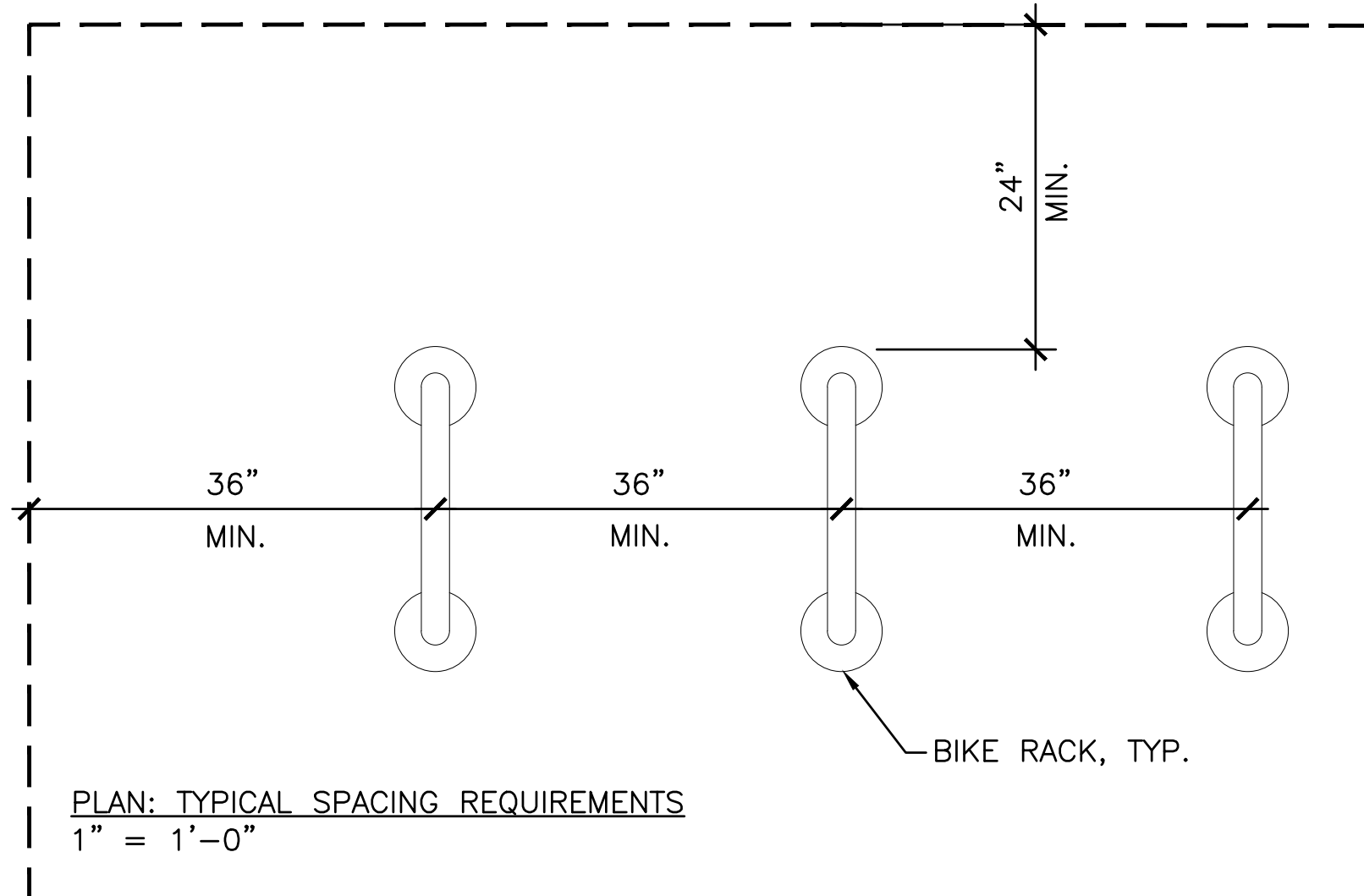


PRODUCT: U200-IG(SF)
DESCRIPTION: 'U' BIKE RACK
2 BIKE, SURFACE OR IN GROUND MOUNT
COLOR: BLACK

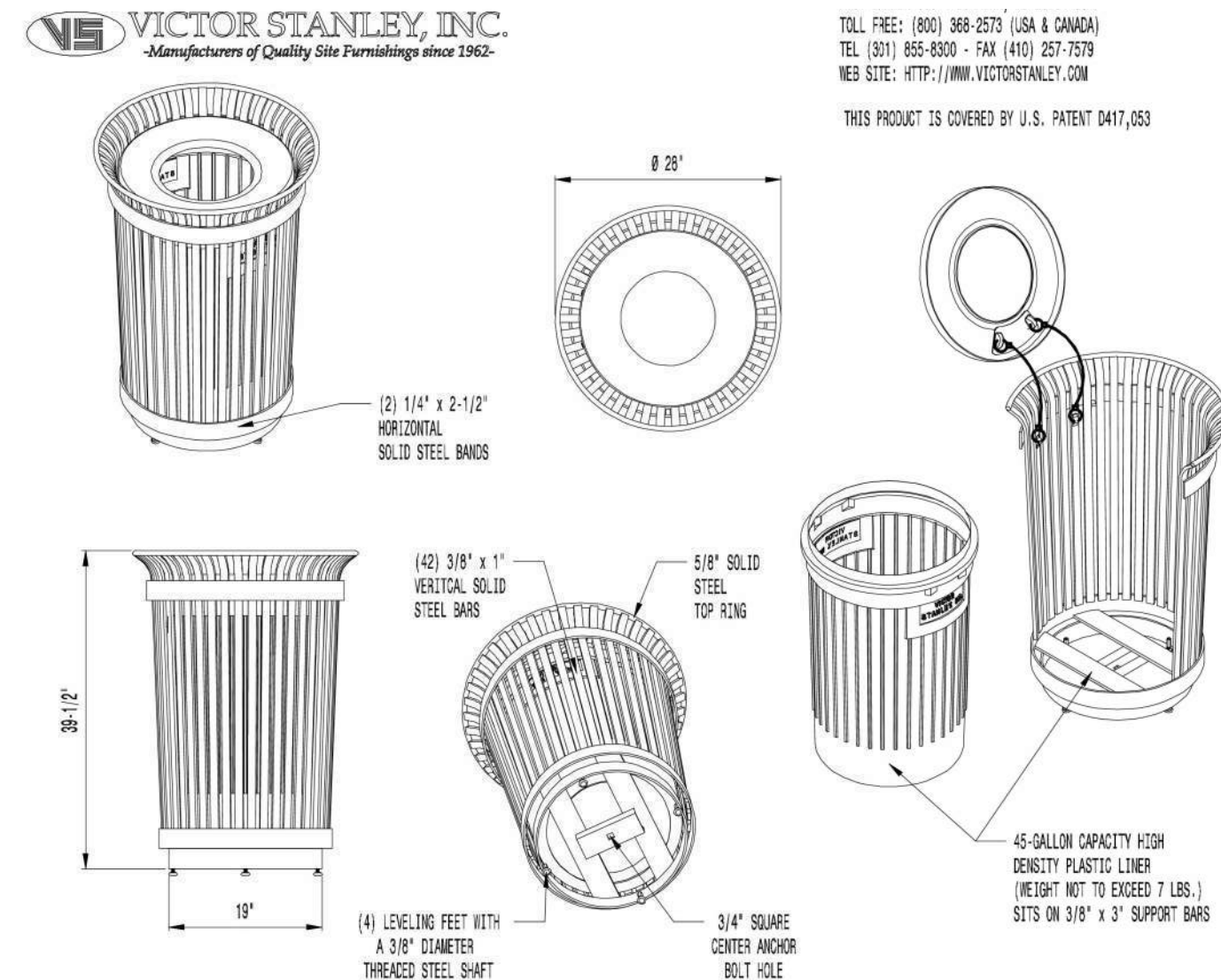
DATE: 10-22-18
ENG: SMC

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- NOTES:
1. INSTALL BIKE RACKS ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
 2. CONSULTANT TO SELECT COLOR (FINISH). SEE MANUFACTURER'S SPECIFICATIONS.
 3. SEE SITE PLAN FOR LOCATION OR CONSULT OWNER.



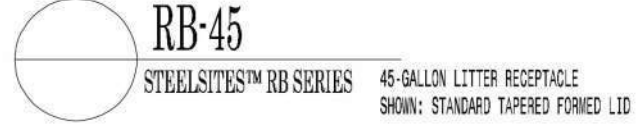
01 BIKE RACK
NTS



AVAILABLE OPTIONS:
COLOR: BLACK
AVAILABLE WITH STEEL PLAKERS IN VARIOUS SIZES AND PRESSURE SENSITIVE VINYL OUTDOOR DECALS

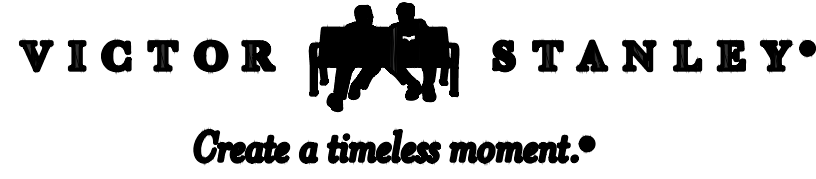
NOTES:

1. DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
2. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD COATING THICKNESS. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
3. IT IS NOT RECOMMENDED TO LOCATE ANCHOR BOLTS UNTIL TABLE IS IN PLACE. THIS VICTOR STANLEY, LLC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
4. ANCHOR BOLTS NOT PROVIDED BY VICTOR STANLEY, LLC.
5. FOR HIGH SALT ABRASIVE CLIMATES, HOT-DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. HOT-DIP GALVANIZING IS PERFORMED FOR VICTOR STANLEY, LLC. BY AN EXPERIENCED QUALIFIED FIRM TO WHICH PRODUCTS ARE SHIPPED FOR GALVANIZING. HOT-DIP GALVANIZING INCLUDES AN AGGRESSIVE PRE-TREATMENT AND IMMERSION IN A TANK OF CHARGED LIQUID ZINC AT OR AROUND 860°F (460°C). THE RESULTING SURFACE IS RESISTANT TO RUST BUT HAS SOME UNEVENNESS RESULTING FROM THE BONDING OF THE ZINC TO THE STEEL SURFACE. AS A RESULT, THE POWDER-COATING SURFACE FINISH OVER THAT GALVANIZED SURFACE MAY EXHIBIT BUMPS, UNEVENNESS, AND MAY NOT BE AS SMOOTH AS THE STANDARD FINISH. THIS UNEVEN AND INCONSISTENT FINISH IS NORMAL FOR GALVANIZING. CONTACT MANUFACTURER FOR DETAILS.
6. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
7. THIS PRODUCT IS SHIPPED PARTIALLY UNASSEMBLED.



CONTRACTOR TO PROVIDE ANCHOR BOLTS PER MANUFACTURER'S INSTRUCTION. REFERENCE SPECIFICATIONS. INSTALL ACCORDING TO MANUFACTURER'S INSTRUCTION. LIDS TO BE ATTACHED TO RECEPTACLES BY CHAIN. RECEPTACLES TO HAVE 42 GALLON STANDARD TRASH CANS WITHOUT LIDS AS LINERS. COLOR SHALL BE BLACK

02 TRASH RECEPTACLE
NTS



* ALL DIMENSIONS ARE IN INCHES *

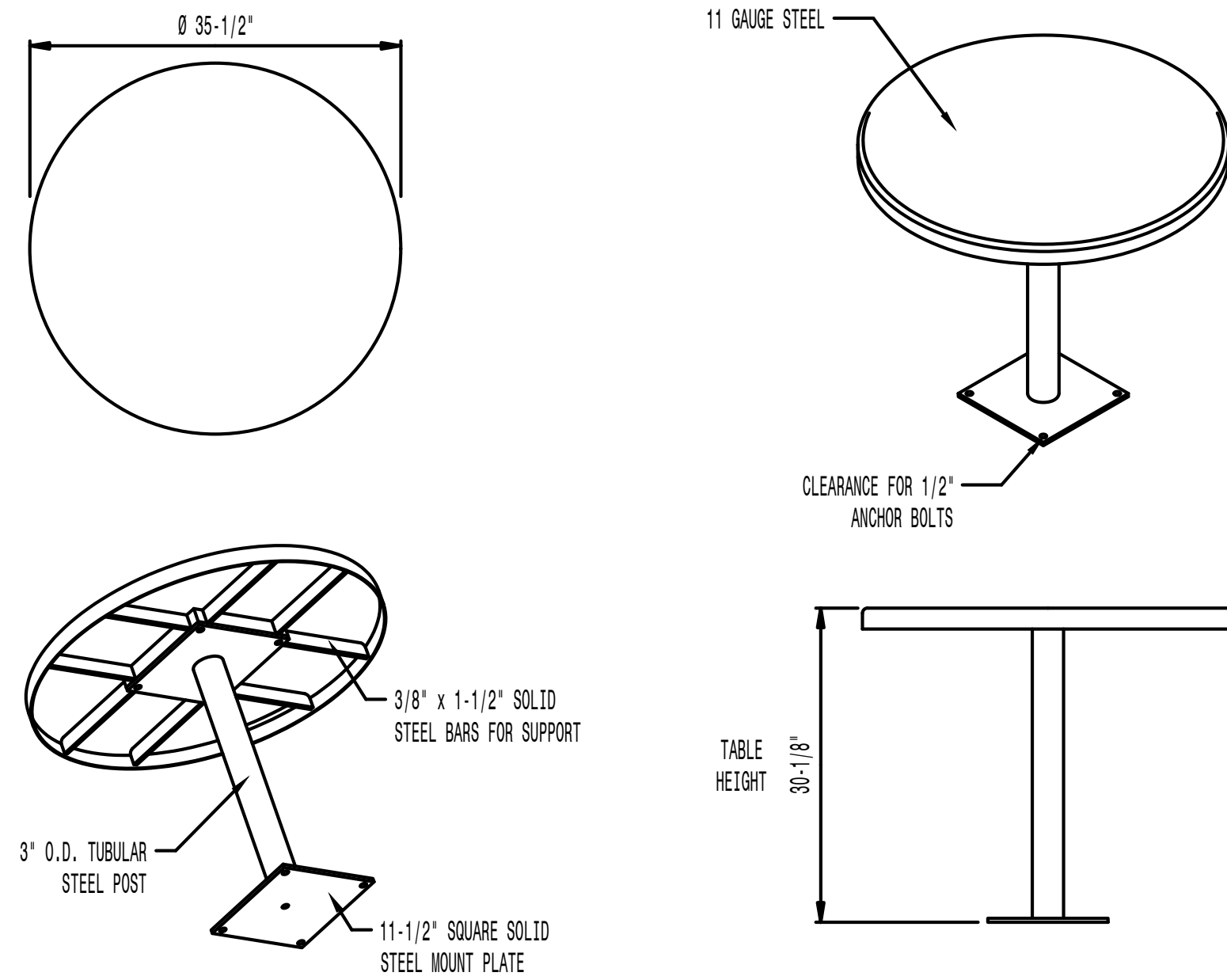


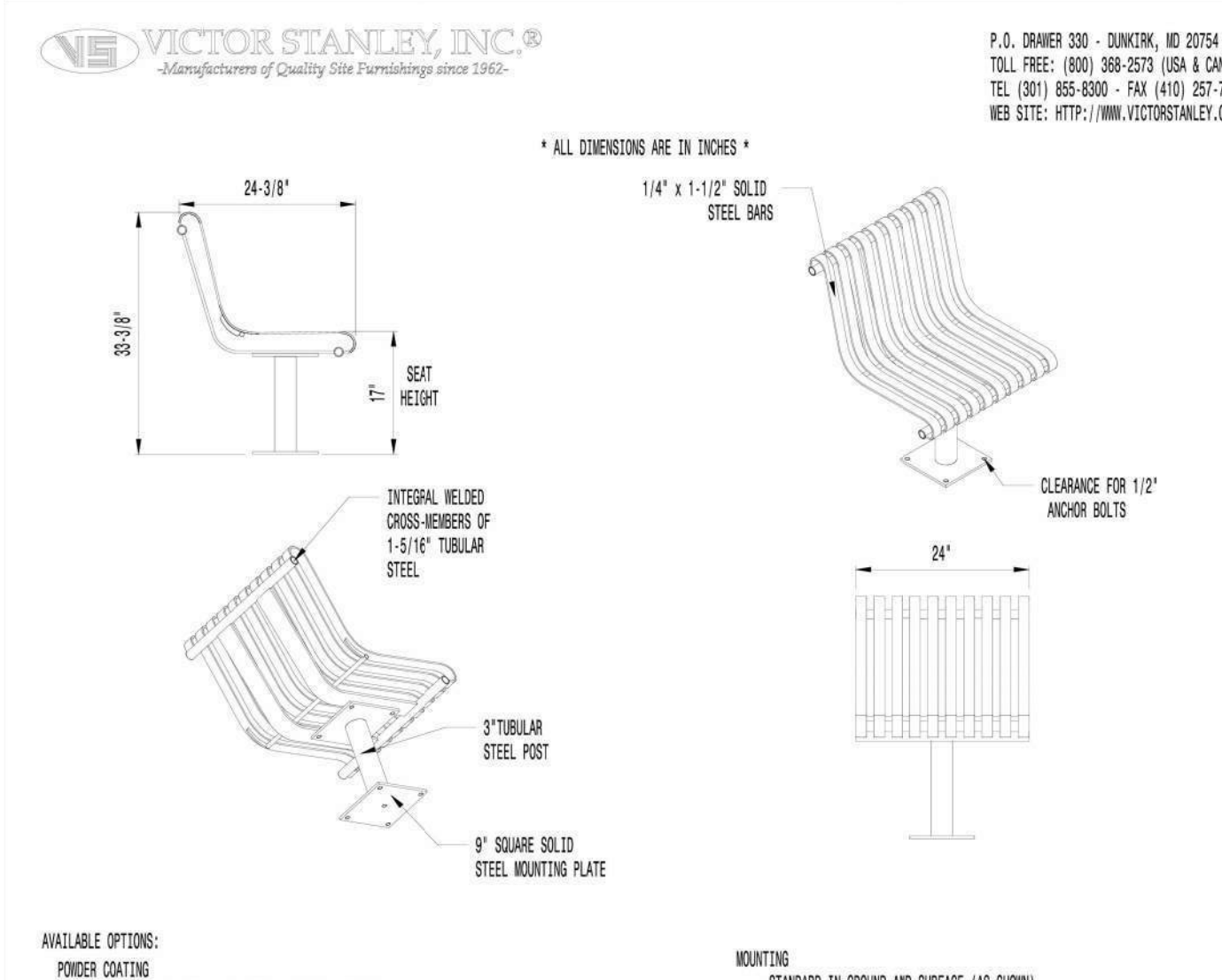
TABLE SIZE
STANDARD 36\"/>

- NOTES:
1. DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
 2. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD COATING THICKNESS. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
 3. IT IS NOT RECOMMENDED TO LOCATE ANCHOR BOLTS UNTIL TABLE IS IN PLACE. THIS VICTOR STANLEY, LLC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
 4. ANCHOR BOLTS NOT PROVIDED BY VICTOR STANLEY, LLC.
 5. FOR HIGH SALT ABRASIVE CLIMATES, HOT-DIP GALVANIZING BEFORE POWDER COATING IS AVAILABLE. HOT-DIP GALVANIZING IS PERFORMED FOR VICTOR STANLEY, LLC. BY AN EXPERIENCED QUALIFIED FIRM TO WHICH PRODUCTS ARE SHIPPED FOR GALVANIZING. HOT-DIP GALVANIZING INCLUDES AN AGGRESSIVE PRE-TREATMENT AND IMMERSION IN A TANK OF CHARGED LIQUID ZINC AT OR AROUND 860°F (460°C). THE RESULTING SURFACE IS RESISTANT TO RUST BUT HAS SOME UNEVENNESS RESULTING FROM THE BONDING OF THE ZINC TO THE STEEL SURFACE. AS A RESULT, THE POWDER-COATING SURFACE FINISH OVER THAT GALVANIZED SURFACE MAY EXHIBIT BUMPS, UNEVENNESS, AND MAY NOT BE AS SMOOTH AS THE STANDARD FINISH. THIS UNEVEN AND INCONSISTENT FINISH IS NORMAL FOR GALVANIZING. CONTACT MANUFACTURER FOR DETAILS.
 6. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
 7. THIS PRODUCT IS SHIPPED PARTIALLY UNASSEMBLED.



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REV. 4/11/24 DRAW V.F.T. 2024-200

04 PICNIC TABLE
NTS



AVAILABLE OPTIONS:
COLOR: BLACK
ANCHORS
AVAILABLE WITH OPTIONAL ANCHORS

- NOTES:
1. DRAWINGS NOT TO SCALE. DO NOT SCALE DRAWINGS.
 2. ALL FABRICATED METAL COMPONENTS ARE STEEL SHOTBLASTED, ETCHED, PHOSPHATIZED, PREHEATED, AND ELECTROSTATICALLY POWDER-COATED WITH T.G.I.C. POLYESTER POWDER COATINGS. PRODUCTS ARE FULLY CLEANED AND PRETREATED, PREHEATED AND COATED WHILE HOT TO FILL CREVICES AND BUILD COATING THICKNESS. COATED PARTS ARE THEN FULLY CURED TO COATING MANUFACTURER'S SPECIFICATIONS. THE THICKNESS OF THE RESULTING FINISH AVERAGES 8-10 MILS (200-250 MICRONS).
 3. IT IS NOT RECOMMENDED TO LOCATE ANCHOR BOLTS UNTIL SEAT IS IN PLACE. THIS VICTOR STANLEY, LLC. PRODUCT MUST BE PERMANENTLY AFFIXED TO THE GROUND. CONSULT YOUR LOCAL CODES FOR REGULATIONS.
 4. ANCHOR BOLTS NOT PROVIDED BY VICTOR STANLEY, LLC.
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 6. ALL SPECIFICATIONS ARE SUBJECT TO CHANGE. CONTACT MANUFACTURER FOR DETAILS.
 7. THIS PRODUCT IS SHIPPED PARTIALLY UNASSEMBLED.



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03 BACKED CHAIR
NTS

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Architects

Parker Town Hall
Expansion
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22-093

Town of Parker
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Telephone: 303-741-3737

Landscape Architect
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Denver, CO 80205
Telephone: 303-628-0003

Structural Engineer
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Golden, CO 80401
Telephone: 303-384-9910

Mechanical + Plumbing Engineer
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Lafayette, CO 80227
Telephone: 303-968-4514

Electrical Engineer + Technology
AE Design
1900 Wazee Street #205
Denver, CO 80202
Telephone: 303-296-3034

wenk
ASSOCIATES

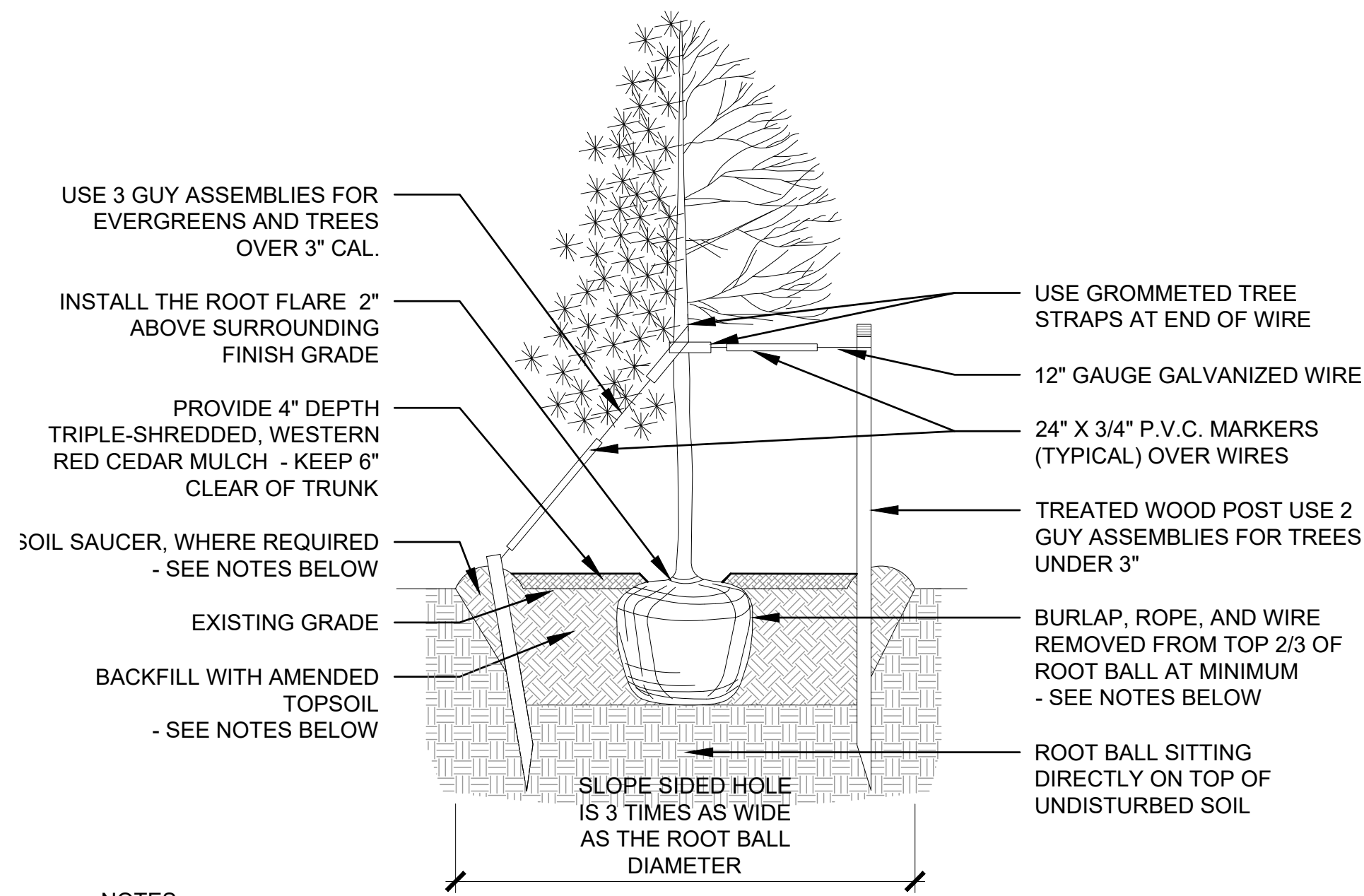
Issue Date
PERMIT / GMP DRAWINGS 17 MAY 2024
COB-001 19 JULY 2024



Project Number: 22-093
Drawn By: JM
Reviewed By: TS
Approved By: TS

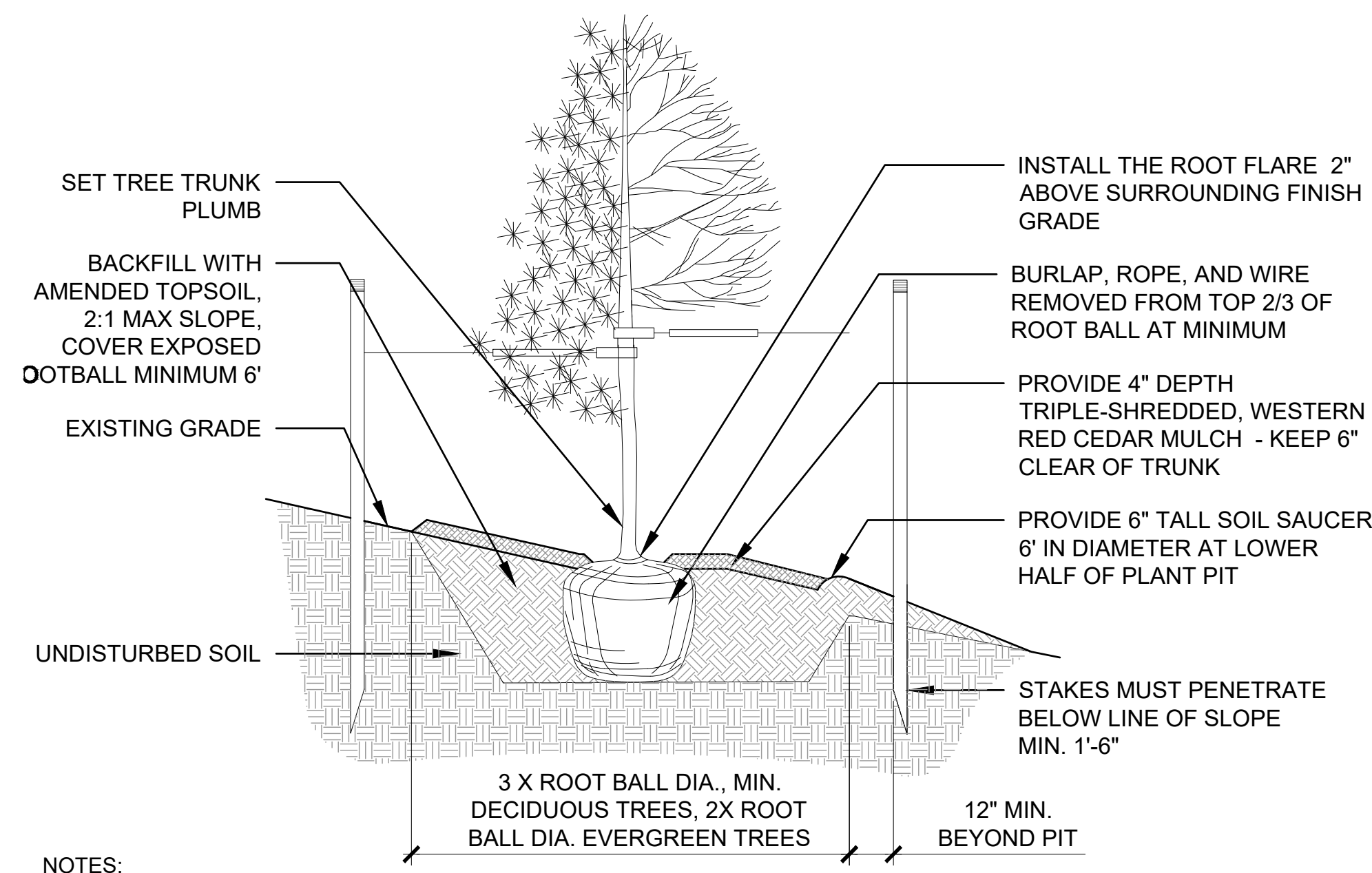
LANDSCAPE DETAILS - FURNISHINGS

L-302



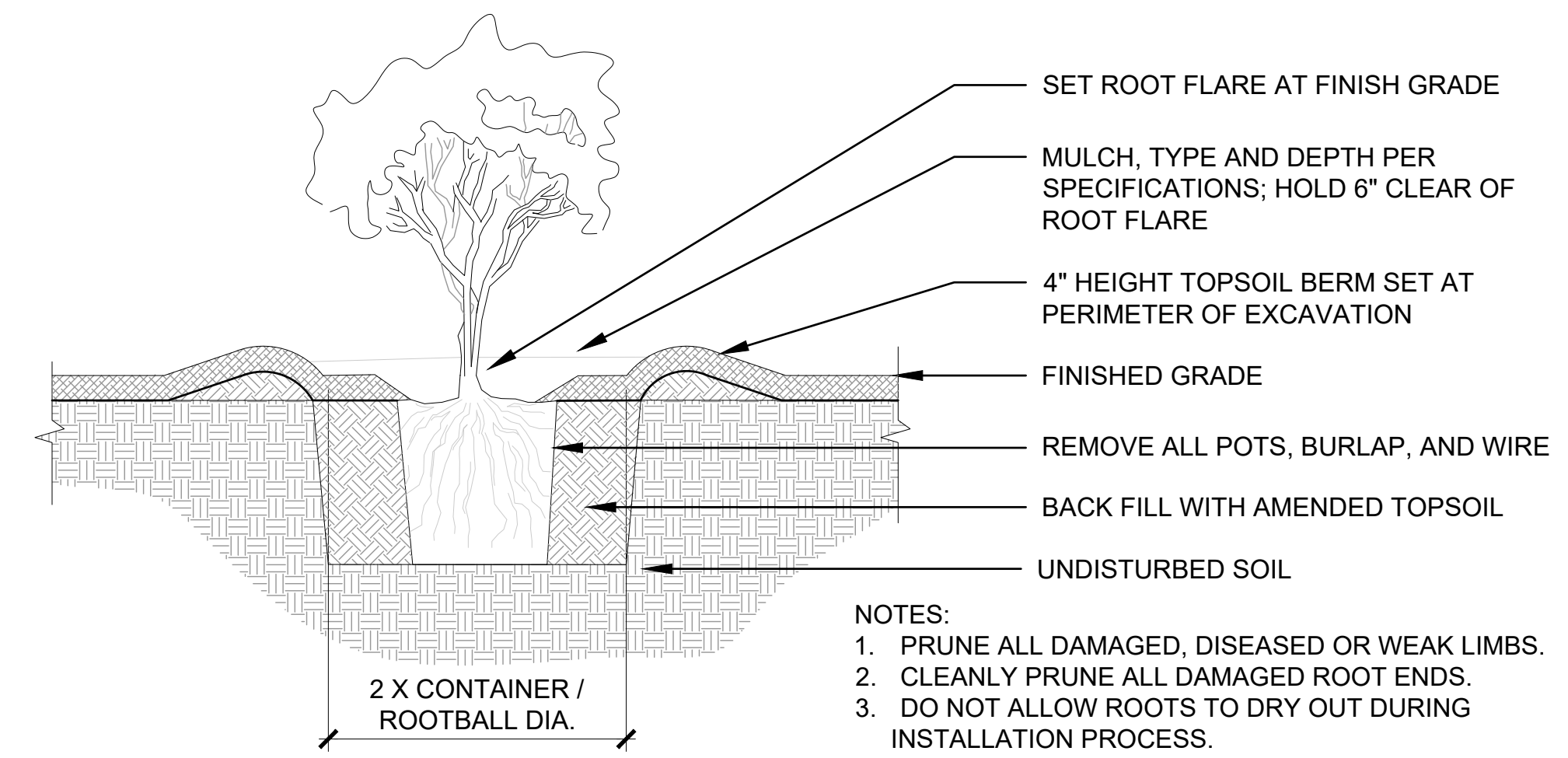
- NOTES:**
- EXCAVATE PLANTING HOLES WITH SLOPING SIDES, MAKE EXCAVATIONS AT LEAST THREE TIMES AS WIDE AS THE ROOT BALL DIAMETER AND NO DEEPER THAN THE DISTANCE FROM THE TOP OF THE ROOT BALL TO ALLOW FOR SETTLING. DO NOT DISTURB SOIL AT BOTTOM OF PLANTING HOLES, BUT DO SCORE THE SIDES OF THE PLANTING HOLE. THE PLANTING AREA SHALL BE LOOSENEED AND AERATED AT LEAST THREE TIMES THE DIAMETER OF THE ROOT BALL.
 - BACKFILL SHALL CONSIST OF APPROVED ON-SITE TOPSOIL BLENDED W/ COMPOST AMENDMENT. IF ON-SITE TOPSOILS ARE NOT APPROVED FOR USE, BACKFILL W/ IMPORTED TOPSOIL BLENDED WITH COMPOST AMENDMENT. REFER TO SPECIFICATION SECTION 329113 FOR ADDITIONAL INFORMATION.
 - TREES SHALL BE PLANTED WITH THE ROOT COLLAR/FLARE VISIBLE ABOVE GRADE AND TWO OR MORE STRUCTURAL ROOTS LOCATED WITHIN THE TOP 1\"/>

01 TOWN OF PARKER TREE PLANTING
NTS

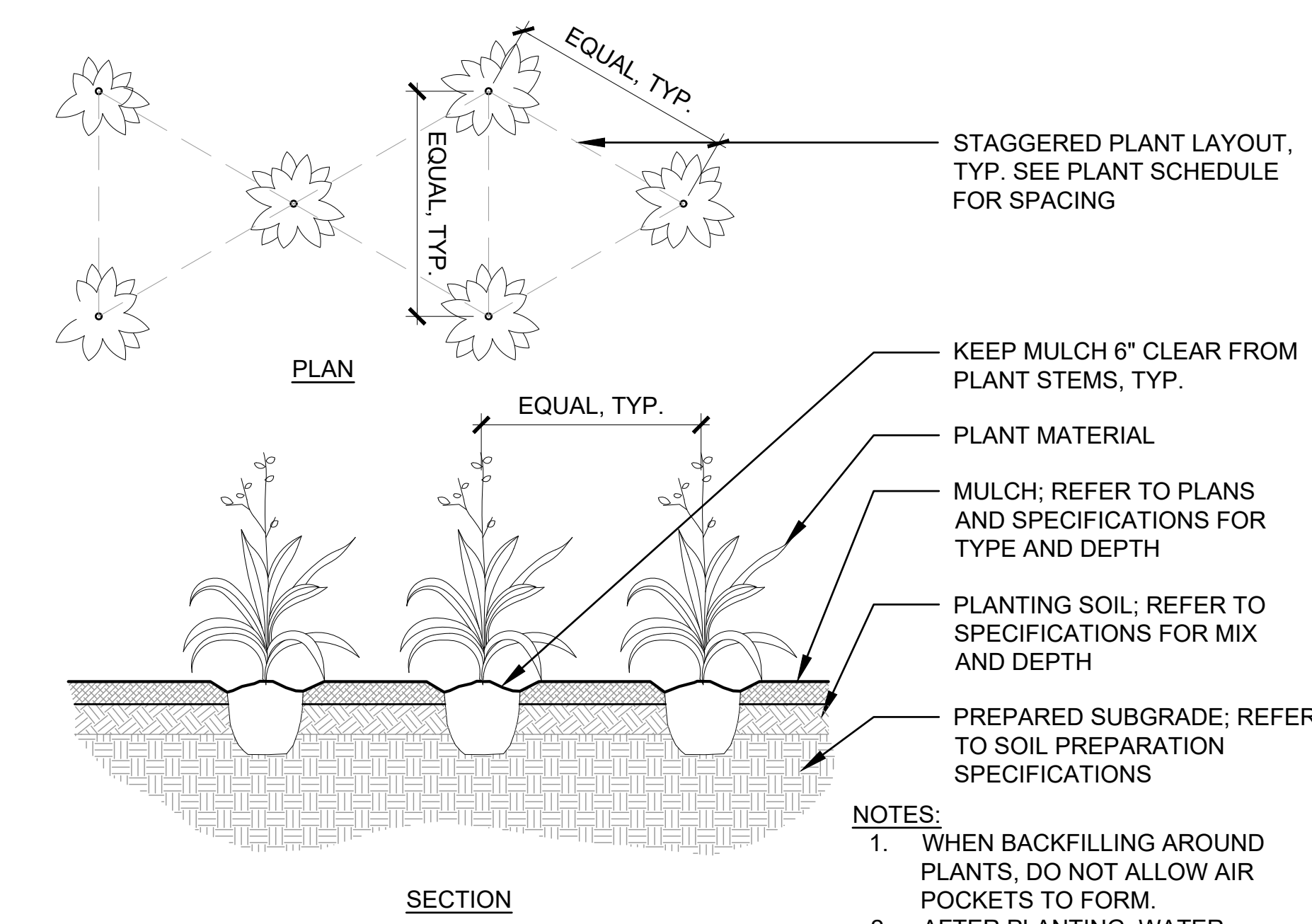


- NOTES:**
- THIS INSTALLATION SHALL APPLY TO ALL TREE TYPES AND SIZES PLANTED ON SLOPES 4:1 OF GREATER.
 - EXCAVATE PLANTING HOLES WITH SLOPING SIDES, MAKE EXCAVATIONS AT LEAST THREE TIMES AS WIDE AS THE ROOT BALL DIAMETER AND NO DEEPER THAN THE DISTANCE FROM THE TOP OF THE ROOT BALL TO ALLOW FOR SETTLING. DO NOT DISTURB SOIL AT BOTTOM OF PLANTING HOLES, BUT DO SCORE THE SIDES OF THE PLANTING HOLE. THE PLANTING AREA SHALL BE LOOSENEED AND AERATED AT LEAST THREE TIMES THE DIAMETER OF THE ROOT BALL.
 - BACKFILL SHALL CONSIST OF APPROVED ON-SITE TOPSOIL BLENDED W/ COMPOST AMENDMENT. OF ON-SITE TOPSOILS ARE NOT APPROVED FOR USE, BACKFILL W/ IMPORTED TOPSOIL BLENDED WITH COMPOST AMENDMENT. REFER TO SPECIFICATION SECTION 329113 FOR ADDITIONAL INFORMATION.
 - TREES SHALL BE PLANTED WITH THE ROOT COLLAR/FLARE VISIBLE ABOVE GRADE AND TWO OR MORE STRUCTURAL ROOTS LOCATED WITHIN THE TOP 1\"/>

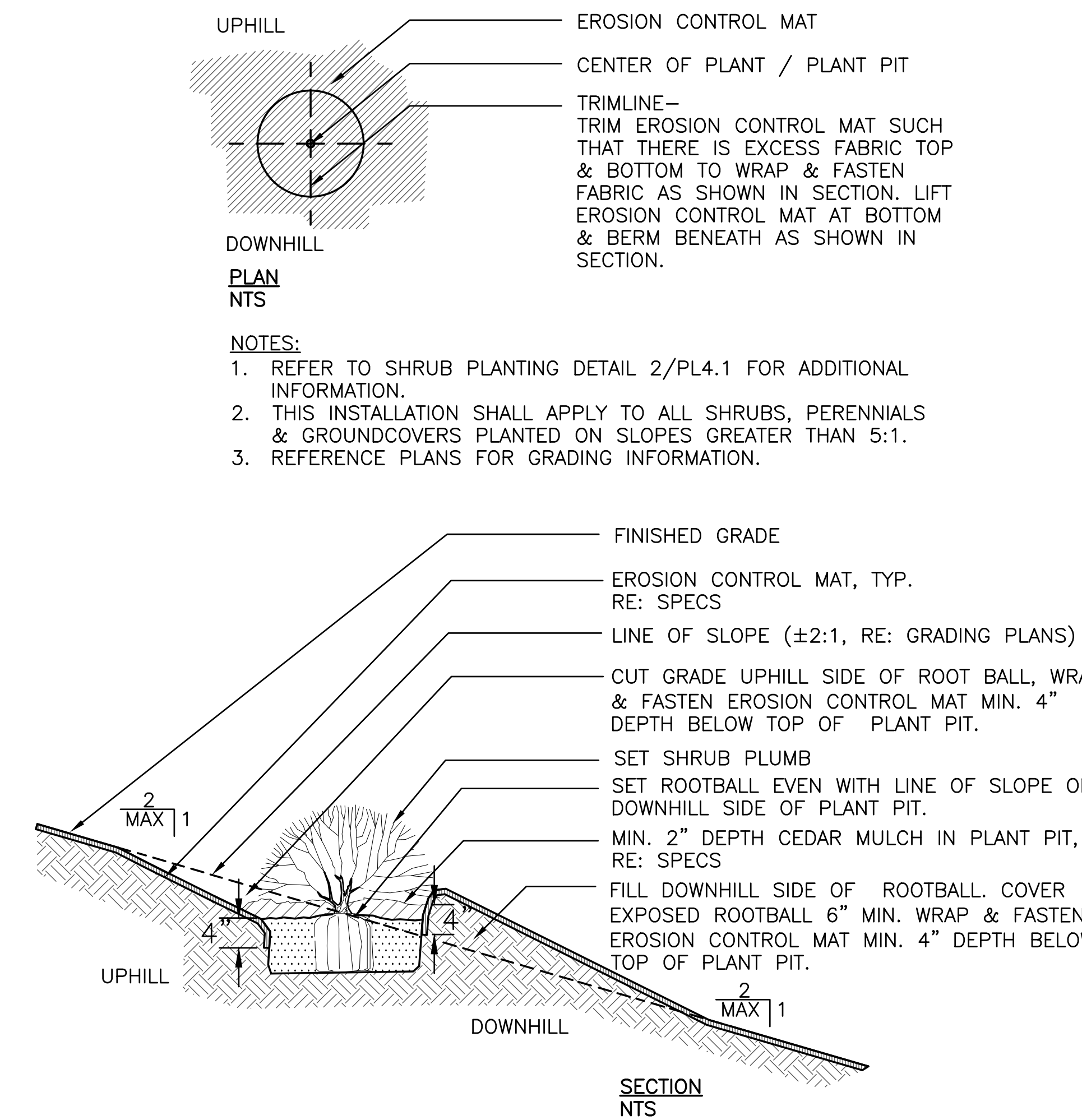
02 TOWN OF PARKER TREE PLANTING ON SLOPE
NTS



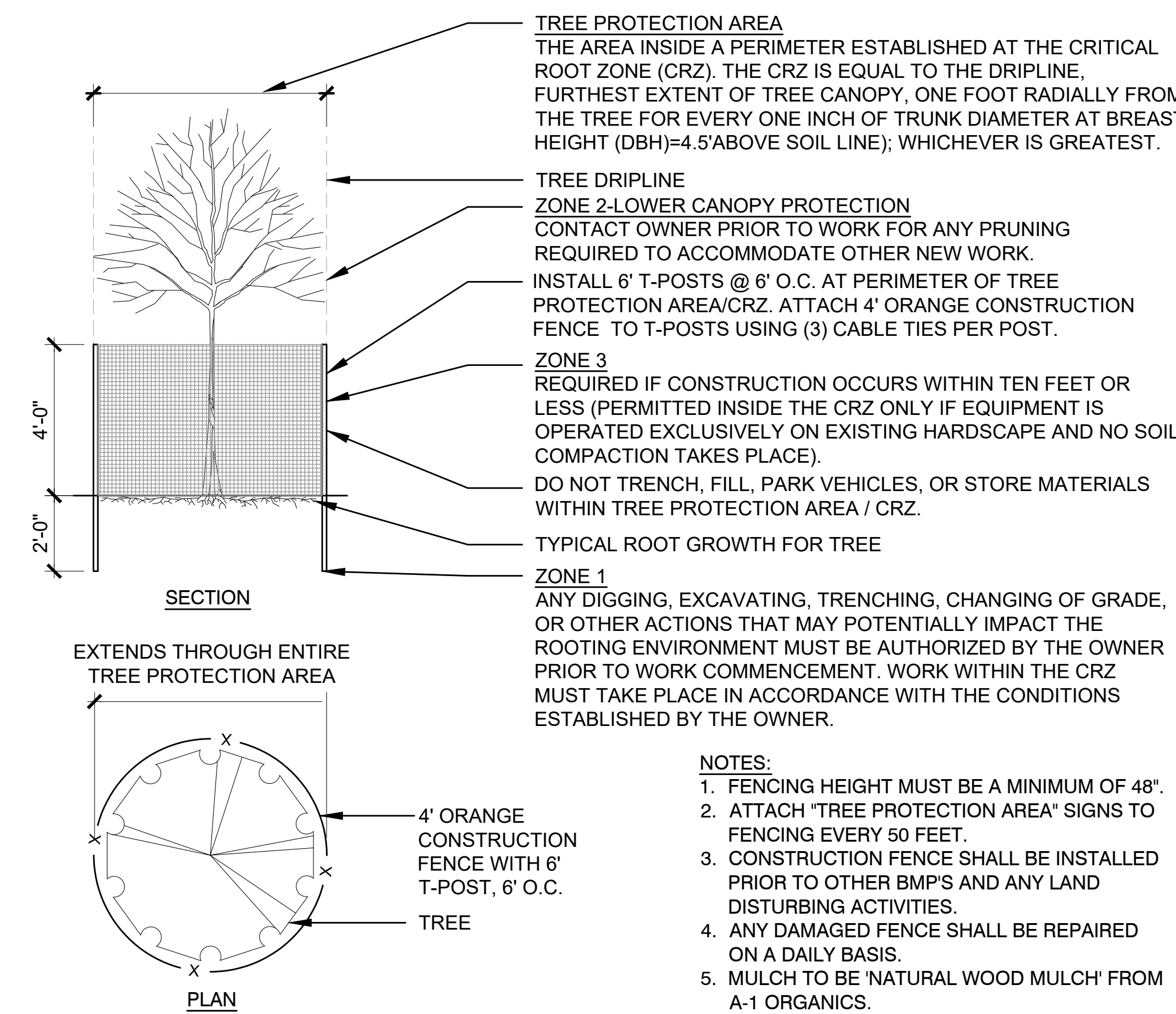
03 TOWN OF PARKER SHRUB PLANTING
NTS



04 TOWN OF PARKER PERENNIAL PLANTING
NTS



05 SHRUB PLANTING ON SLOPE
NTS



06 TOWN OF PARKER TREE PROTECTION
NTS





IRRIGATION CONSTRUCTION NOTES

- DRAWINGS AND BASE INFORMATION - ALL BASE AND PLANTING INFORMATION HAVE BEEN PROVIDED BY WENK. THE CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY PRESSURE OF ANY DISCREPANCIES BETWEEN THE UTILITY OR PLANTING PLANS AND THE IRRIGATION PLAN. IF CONTRACTOR FAILS TO NOTIFY HYDROSYSTEMS*KDI AND MAKES CHANGES TO THE IRRIGATION SYSTEM DESIGN, THEY ASSUME ALL COSTS AND LIABILITIES ASSOCIATED WITH THOSE FIELD CHANGES. REFER TO SPECIFICATIONS FOR ADDITIONAL PROJECT REQUIREMENTS. CONTACT IRRIGATION CONSULTANT FOR CURRENT SPECIFICATIONS IF NOT PROVIDED.
- SYSTEM PRESSURE - HYDROSYSTEMS*KDI HAS TESTED THE STATIC WATER PRESSURE AND IS FOUND TO BE 90 PSI. THE CONTRACTOR IS RESPONSIBLE TO FIELD VERIFY PRESSURE PRIOR TO COMMENCING ANY CONSTRUCTION AND NOTIFY HYDROSYSTEMS*KDI OF ANY VARIANCE FROM THE STATED PRESSURE IMMEDIATELY. WRITTEN DOCUMENTATION OF PRESSURE TEST AND RESULTS SHALL BE PROVIDED TO HYDROSYSTEMS*KDI AT CONSTRUCTION ONSET. IF CONTRACTOR FAILS TO FIELD VERIFY PRESSURE AND/OR NOTIFY HYDROSYSTEMS*KDI OR ANY VARIATIONS FROM THIS PRESSURE, THEN THEY ASSUME ALL CONSTRUCTION AND ENGINEERING COSTS ASSOCIATED WITH SYSTEM MODIFICATIONS REQUIRED TO ACCOMMODATE ACTUAL SITE PRESSURE. THIS SYSTEM HAS BEEN DESIGNED FOR A REQUIRED STATIC PRESSURE OF 90 PSI MINIMUM.
- IRRIGATION SYSTEM OPERATION INTENT - THIS IRRIGATION SYSTEM HAS BEEN DESIGNED TO IRRIGATE THE ESTABLISHED LANDSCAPE WITHIN A SIX NIGHT PER WEEK, SIX HOUR PER NIGHT WATERING WINDOW. ESTABLISHMENT WATERING WILL REQUIRE UP TO TWICE AS MUCH IRRIGATION FOR A FOUR TO SIX WEEK PERIOD. THE DESIGN IS BASED ON THE FOLLOWING PROJECTED WEEKLY APPLICATION RATES AFTER ESTABLISHMENT. THESE FIGURES ARE BASED ON A 30-YEAR AVERAGE WEATHER DATA AND WILL NEED TO BE ADJUSTED DUE TO SEASONAL CHANGES AND WEATHER CONDITIONS ABOVE AND BELOW THE AVERAGE VALUES UTILIZED.

BLUEGRASS TURF	2.48" PER WEEK PEAK SEASON
ORNAMENTAL PLANTINGS	1.24" PER WEEK PEAK SEASON
NATIVE SEED MIXES	1.15" PER WEEK PEAK SEASON (TWO SEASONS)

 NOTE: IT IS THE INTENT OF THIS DESIGN THAT NATIVE AREAS WOULD ONLY BE IRRIGATED FOR ESTABLISHMENT. SYSTEM WILL REMAIN FOR USE DURING YEARS WITH LESS THAN NORMAL RAINFALL.
- EQUIPMENT INSTALLATION - IT IS THE INTENT OF THIS DESIGN THAT ALL IRRIGATION EQUIPMENT BE INSTALLED WITHIN PROPERTY LIMITS AND WITHIN LANDSCAPED AREAS. INSTALLATION SHALL BE COORDINATED WITH OTHER UTILITY WORK. ALL OTHER UTILITIES SHALL TAKE PRECEDENCE OVER IRRIGATION LOCATION. ANY EQUIPMENT OTHER THAN VALVE BOXES OR SLEEVING THAT CONTAINS PIPE OR WIRES SHOWN OUTSIDE OF THESE LIMITS IS SHOWN IN THAT LOCATION FOR GRAPHICAL CLARITY ONLY. ALL VALVE BOXES SHALL BE INSTALLED A MINIMUM OF 3'-0" FROM EDGE OF ANY PAVED SURFACES UNLESS SPECIFICALLY INDICATED ON PLANS. BOXES INSTALLED IN OPEN TURF AREAS SHALL BE KEPT TO EDGES AND STAKED FOR REVIEW IF ALONG HIGH TRAFFIC AREAS. ALL VALVE BOXES SHALL BE PLACED A MINIMUM OF 3'-0" FROM THE CENTERLINE OF ANY DRAINAGE SWALE. ALL VALVE BOXES WITHIN PAVEMENT SHALL BE TIER 15 RATED BOXES FOR HEAVY DUTY NON-DELIBERATE TRAFFIC. BOX LID COLOR SHALL MATCH ADJACENT MATERIALS. I.E. GREEN IN TURF, TAN IN WOOD MULCH, GRAY IN STONE MULCH, PURPLE FOR RECLAIMED WATER SYSTEMS (IF REQUIRED). REFER TO LANDSCAPE PLANS FOR MATERIAL COLORS AND TYPES. ALL BOXES SHALL BE INSTALLED TO BE FLUSH WITH GRADE AND IN AN ORDERLY MANNER. WHERE MORTAR PAVING LIDS ARE INSTALLED ABOVE BOXES, IRRIGATION BOX WITH LID SHALL BE LOWERED TO ACCOMMODATE PAVING LID. REFER TO LANDSCAPE FOR ADDITIONAL INFORMATION. REFER TO MANUFACTURE RECOMMENDATIONS.
- PIPING INSTALLATION - IRRIGATION PIPING SHALL MAINTAIN A MINIMUM DISTANCE FROM BUILDING FOUNDATIONS OF 5 FEET OR AS DESCRIBED IN SOILS REPORT, WHICHEVER IS GREATER. NO SPRAY IRRIGATION SHALL OCCUR WITHIN 10 FEET OF THE FOUNDATION. NO DRIP IRRIGATION SHALL OCCUR WITHIN 5 FEET OF THE FOUNDATION UNLESS SOIL MOISTURE SENSORS ARE INSTALLED ON VALVES SERVICING THESE AREAS. ALL IRRIGATION PIPING AND EMISSION DEVICES LOCATED ON TOP OF OR WITHIN BUILDING STRUCTURE SHALL CONFORM TO WATERPROOFING CONSULTANT REQUIREMENTS. PIPE ROUTING MAY BE SHOWN WITHIN THESE DISTANCES FOR GRAPHICAL CLARITY ONLY.
- MANUAL DRAIN VALVES - CONTRACTOR TO INSTALL ONE MANUAL DRAIN VALVE ON PRESSURE SUPPLY LINE DIRECTLY DOWNSTREAM OF BACKFLOW PREVENTER AND AT ALL LOW POINTS AND DEAD ENDS OF PRESSURE SUPPLY PIPING TO ENSURE COMPLETE DRAINAGE OF SYSTEM. CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THESE LOCATIONS IN-FIELD AND INSTALLATION LOCATIONS SHALL BE NOTED ON AS-BUILTS.
- POP-UP SPRAY NOZZLES - CONTRACTOR TO INSTALL PLASTIC NOZZLES ON ALL POP-UP SPRAY HEADS. INSTALL 15 SERIES NOZZLES ON ALL HEADS SPACED AT 12 TO 14". INSTALL 12 SERIES NOZZLES ON ALL HEADS SPACED 10' TO 11". INSTALL 10 SERIES NOZZLES ON ALL HEADS SPACED AT 9' TO 9". INSTALL 8 SERIES NOZZLES ON ALL HEADS SPACED AT 6' TO 7". INSTALL SIDE STRIP NOZZLES ON ALL HEADS WITH AN 'S' DESIGNATION AND RIGHT AND LEFT CORNER STRIP NOZZLES ON ALL HEADS WITH AN 'L' OR 'R' DESIGNATION. INSTALL LOW FLOW '50' SERIES NOZZLES WITH SQUARE SHORT RADIUS PATTERN WHERE INDICATED AT SPACING SHOWN. VARIABLE ARC NOZZLES SHOULD BE UTILIZED ADJACENT TO CURVILINEAR SHRUB BEDS OR FOR ANY ANGLES THAT ARE NOT A STANDARD NOZZLE ANGLE.
- UNLABELED PIPING - ALL UNLABELED LATERAL PIPING SHALL BE 1" MINIMUM UNLESS OTHERWISE NOTED.
- SLEEVING - ALL SLEEVING UNDER PAVED SURFACES SHOWN ON PLANS IS BY CONTRACTOR UNLESS OTHERWISE NOTED. SLEEVING SHALL BE INSTALLED IN THE SIZES AND QUANTITIES SHOWN ON PLANS OR BASED ON THE SCHEDULE BELOW. WHERE SLEEVES ARE SHOWN BUT NOT LABELED, FOLLOW THE SCHEDULE BELOW. ALL MAINLINE, CONTROL WIRES AND DRIP LINES UNDER PAVED SURFACES ARE TO BE INSTALLED IN SLEEVING. ALL MAINLINE SLEEVE LOCATIONS TO INCLUDE A SEPARATE WIRE SLEEVE.

SLEEVED PIPE SIZE/WIRE QUANTITY	REQUIRED SLEEVE SIZE & QUANTITY
3/4" - 1 1/2" PIPING	2" PVC (1)
1 1/2" - 2" PIPING	4" PVC (1)
1-25 CONTROL WIRES	2" PVC (1)
26-50 CONTROL WIRES	3" PVC (1)
COMMUNICATION CABLE	2" PVC (1)

IRRIGATION SCHEDULE				
SYMBOL	MANUFACTURER	MODEL NO.	DESCRIPTION	DETAIL NO.
	RAIN BIRD	RD-06-S-P30 WITH MATCHED PRECIPITATION RATE SERIES NOZZLE	HI-POP SPRAY HEAD	7
	RAIN BIRD	RD-06-S-P30 WITH SST, CORNER SERIES NOZZLE	HI-POP SPRAY HEAD	7
	HUNTER	I-20-06-22 WITH NOZZLE (BLUE)	HI-POP GEAR DRIVEN ROTOR	8
	HUNTER	I-20-06-SS WITH NOZZLE (BLUE)	HI-POP GEAR DRIVEN ROTOR	8
	RAIN BIRD	EFB-CP WITH NIBCO BALL VALVE	ELECTRIC CONTROL VALVE	6
	WEATHERTRAK	OPTIFLOW XR	ELECTRIC CONTROLLER	5
	HUNTER	RAIN KLIK - WIRELESS	WEATHER SENSOR DEVICE	21
SLIDE GATE / GEAR BOX REFER TO REPRESENTATIONS AND DETAILS				
	AY MACDONALD	76001 - 1/4 TURN - 1"	MANUAL DRAIN VALVE	1
		LINE SIZE - 2 1/2" AND SMALLER	GATE VALVE	3
	RAIN BIRD	EFB-CP - 1.5"	MASTER CONTROL VALVE	20
	WEATHERTRAK	FLOW HD- WTLFLOWHD-P-150	FLOW SENSOR	4
		CLASS 200 BE - 2 1/2" & SMALLER	PVC MAINLINE	18
		CLASS 160	PVC SLEEVING	14
		CLASS 200 BE	PVC LATERAL	18
		CLASS 200 BE	SUBSURFACE HEADER PIPE - 1" PVC	16 & 18
	RAIN BIRD	EFB-CP-PRS-D - 1"	SUBSURFACE VALVE ASSEMBLY	10
	SPEARS	2121-005	DRIP LINE BLOW-OUT FLUSH VALVE	17
	NETAFIM	TLGV6-12 RINGS SPACED PER DETAIL	SUBSURFACE DRILINE RING - TREE	16
	NETAFIM	TLGV6-18 ROWS SPACED 18"	SUBSURFACE DRILINE RING	11, 12, 13, 14, 15
			14 ANS DIRECT BURY CONTROL WIRES	
			WIRE SPLICE BOX	9
			EXISTING BACKFLOW DEVICE	
			EXISTING CONTROLLER	
			EXISTING CONTROL VALVE	
			EXISTING MAINLINE	
			CONTROLLER & STATION NO. CONTROL VALVE SIZE	
			NUMBER OF SPARE WIRES - 2 CONTROL AND 1 SPARE WIRES TO INDICATED CONTROLLER - SEE CONSTRUCTION NOTES	

COB-001

Parker Water LANDSCAPE/IRRIGATION WORKSHEET
Tap sizing for dedicated irrigation taps

Yellow fields require user input

Irrigation Water Requirement = (ET_o x PF x LA)/IE

Define Landscape & Irrigation System:		Site Summary:	
High Water Use Plant Material *Cool season turfgrass (Kentucky bluegrass)		Total Irrigated Area (sq ft)	80390
Plant Factor (PF, %)	High 0.9	Avg Plant Factor (%)	0.69
Landscape Area (LA, sq ft)	47320	Avg Irrigation Efficiency (%)	0.71
Irrigation Efficiency (IE, %)	Overhead 0.65	Peak monthly ET, Parker, CO:	
		Peak monthly ET (July)	7.56 in
High-Moderate Water Use Plant Material *Hybrid grass types (Texas hybrids, fescue, etc.)		Irrigation Water Requirement (IWR):	
Plant Factor (PF, %)	High-Med 0.8	Peak monthly IWR:	381356 gal
Landscape Area (LA, sq ft)	0	Maximum flow requirement	39.9 GPM
Irrigation Efficiency (IE, %)	Overhead 0.65	*Max. flow requirements are based on peak monthly irrigation demands and follow PWSO's watering guidelines	
Moderate Water Use Plant Material *Traditional trees/shrubs/perennials, alternative grass types (buffalograss, blue grama, etc.)		Hydraulic Requirements:	
Plant Factor (PF, %)	Med 0.5	Tap Size (in)	Max GPM
Landscape Area (LA, sq ft)	18263	3/4"	10
Irrigation Efficiency (IE, %)	Drip 0.9	1"	18
		1 1/2"	40
		2"	71
		3"	189
		4"	380
Low Water Use Plant Material *Xeric trees/shrubs/perennials, native grass mixes		* 2" and smaller assumes Seamless K Copper Tap 3" and larger assumes Ductile Iron Pipe Class 350	
Plant Factor (PF, %)	Low 0.25	Tap Size Requirement	1 1/2"
Landscape Area (LA, sq ft)	14807		
Irrigation Efficiency (IE, %)	Overhead 0.65		

System Design Pressure: 90 PSI

Design pressure has been field tested & verified

This form has been completed in compliance with PWSO's Engineering Standards & Specifications Manual regarding irrigation design specifications

PWSO holds final discretion for determining appropriate plant factor designations based on landscape plan submission

[Visit Appendix B of the SMWSA Model Ordinance for Plant Factor Guidelines](#)

EXISTING 1.5" TAP- IRRIGATION

COB-001



Know what's below.
Call before you dig.
CALL A BUSINESS BEFORE YOU EXCAVATE FOR THE MARKING OF UTILITIES.



Irrigation Consulting & Water Management
13949 W. Colfax Ave., Suite 260 Lakewood, Colorado 80401
© 2023 360.5327 www.hydroystemskdi.com

A CONTROLLER LOCATION "A"

PEDESTAL MOUNT ONE CONTROLLER (REFER TO SCHEDULE FOR MODEL & STATION COUNT), REMOTE READY AT INDICATED LOCATION. 120 VOLT POWER IS AVAILABLE WITHIN 10 LF OF CONTROLLER LOCATION FROM BUILDING. BY OTHERS REMEP. FEES AND PERMITS ASSOCIATED WITH WORK ARE TO BE OBTAINED AND PAID BY CONTRACTOR. CONFIRM EXISTING CONTROL ZONE QUANTITY TO REMAIN OPERATIONAL WITH TOWN PERSONNEL. DUE TO THE CHANGE OF THE CONTROLLER LOCATION, EXISTING CONTROL WIRES MAY REQUIRE EXTENSION AND RECONNECTION TO THE NEW CONTROLLER LOCATION. EACH EXISTING CONTROL WIRE SHALL BE TONED AND CONFIRMED FOR USE BY EXISTING ZONES. INSTALL NEW CONTROL WIRES AND SPARES AS NECESSARY TO NEW CONTROL VALVES. COORDINATE TOTAL CONTROLLER CAPACITY WITH TOWN OF PARKER. PERSONNEL TO LOCATE SHIELDED CABLE FROM NEW FLOW SENSOR AND CONNECT TO DATA RETRIEVAL UNIT LOCATED IN ASSOCIATED CONTROLLER. LOCATE MASTER VALVE WIRES WITH SPARES AND CONNECT 2 OF THE WIRES FROM THE MASTER VALVE TO THE DATA RETRIEVAL BOARD IN ASSOCIATED CONTROLLER. FINAL CONTROLLER LOCATION SHALL BE APPROVED BY OWNER OR OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION. MOUNT ONE WEATHER SENSOR ON BUILDING WHERE THERE IS 20 FT. OF CLEARANCE FROM ANY OVERHANG OR OBSTRUCTING FEATURE. INSTALL WEATHER SENSOR RECEIVER IN CONTROLLER ENCLOSURE. FINAL WEATHER SENSOR LOCATION SHALL BE APPROVED BY OWNER OR OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION.

EXISTING IRRIGATION DAMAGE - CONTRACTOR SHALL REPAIR OR REPLACE ANY EXISTING IRRIGATION SYSTEMS DAMAGED DURING NEW INSTALLATION. REPAIR OR REPLACEMENT SHALL BE DETERMINED BY OWNER OR OWNER'S REPRESENTATIVE AND PAID FOR BY THE LANDSCAPE CONTRACTOR.

1 EXISTING POINT OF CONNECTION #1 - 1.5"

PEAK FLOW REQUIREMENT: 38 GPM, REQUIRED STATIC PRESSURE: 90 PSI
CONTRACTOR SHALL FIELD LOCATE EXISTING 1.5" BACKFLOW PREVENTER AT THIS APPROXIMATE LOCATION. CUT EXISTING MAINLINE DOWNSTREAM OF BACKFLOW AND ISOLATION GATE VALVE AND REMOVE EXISTING MASTER VALVE, FLOW SENSOR AND OTHER EXISTING EQUIPMENT AND TURN THEM OVER TO OWNER. MAKE NEW 2" CLASS 200 PVC MAINLINE CONNECTION TO EXISTING 1.5" BACKFLOW PREVENTER WITH THREADED SCHEDULE 40 PVC ADAPTER AND ROUTE NEW 2" MAINLINE AND MAKE CONNECTION TO EXISTING MAINLINE AS SHOWN. INSTALL ONE NEW 1.5" FLOW SENSOR, GATE VALVE, QUICK COUPLER VALVE, AND 1.5" MASTER VALVE AS SHOWN. EXTEND ONE FEET SHIELDED CABLE FROM FLOW SENSOR TO DATA RETRIEVAL UNIT LOCATED IN ASSOCIATED CONTROLLER. EXTEND 4 FT. 1/4" WIRE (TWO ORANGE AND TWO BLUE) AND CONNECT 2 OF THE WIRES FROM THE MASTER VALVE TO THE DATA RETRIEVAL BOARD IN ASSOCIATED CONTROLLER. SEE DETAIL SHEET FOR REQUIRED PIPE LENGTHS UPSTREAM AND DOWNSTREAM OF FLOW SENSING UNIT. WORK SHALL CONFORM TO LOCAL CODE, FEES, PERMITS AND INSPECTIONS ASSOCIATED WITH WORK ARE TO BE OBTAINED AND PAID FOR BY CONTRACTOR.

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EXISTING IRRIGATION DAMAGE - CONTRACTOR SHALL REPAIR OR REPLACE ANY EXISTING IRRIGATION SYSTEMS DAMAGED DURING NEW INSTALLATION. REPAIR OR REPLACEMENT SHALL BE DETERMINED BY OWNER OR OWNER'S REPRESENTATIVE AND PAID FOR BY THE LANDSCAPE CONTRACTOR.

IRRIGATION EQUIPMENT IS SHOWN OUTSIDE LANDSCAPED AREA FOR GRAPHICAL CLARITY ONLY. INSTALL ALL IRRIGATION EQUIPMENT WITHIN LANDSCAPED AREAS. LANDSCAPE AND UTILITY WORK SHALL TAKE PRECEDENCE. COORDINATE WITH OTHER TRADES FOR LOCATION OF IRRIGATION TRENCHING AND COMPONENT INSTALLATION.

CONTRACTOR SHALL LOCATE AND PROTECT EXISTING CONTROL VALVES IN STAGING AREA. WHEN CONSTRUCTION IS COMPLETED AND STAGING AREA IS REMOVED, CONTRACTOR TO INSTALL NEW ABOVE GROUND IRRIGATION TO HELP RE-ESTABLISH THE NATIVE IN THIS AREA. USE THESE (3) CONTROL VALVES AND COORDINATE WITH STAFF TO PROGRAM TIME FOR THESE VALVES. DAMAGE DONE TO THESE AREAS AS PART OF NEW CONSTRUCTION SHALL BE REPAIR OR REPLACED AT NO ADDITIONAL COST TO OWNER.

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COB-001

- FIELD LOCATE EXISTING 2.5" MAINLINE AT THIS APPROXIMATE LOCATION, CUT AND CAP BOTH SIDES OF THE MAINLINE EXTENDING SOUTH TO THE EXISTING IRRIGATION TAP. ABANDON IN PLACE.
- CUT AND CAP EXISTING 2" IRRIGATION WATER SUPPLY, IN MECHANICAL ROOM, UPSTREAM OF EXISTING 2" IRRIGATION BACKFLOW PREVENTER. RETURN 2" BACK FLOW TO CITY STAFF.
- ABANDON EXISTING 2" OR 2.5" MAINLINE AND CONTROL WIRES IN THIS AREA, REMOVE EXISTING CONTROL VALVES, QUICK COUPLER VALVES, GATE VALVES (IF ANY) AND TURN THEM OVER TO TOWN OF PARKER. PIPE AND WIRES TO BE ABANDONED IN PLACE OR REMOVED IF EXPOSED.
- FIELD LOCATE EXISTING 2" MAINLINE AT THIS APPROXIMATE LOCATION, CUT AND CONNECT NEW 2" PVC MAINLINE, USING SCHEDULE 40 PVC FITTINGS AND EXTEND NEW AS SHOWN. EXTEND NEW CONTROL WIRES WITH NEW MAINLINE. ALL EXISTING CONTROL VALVES ON OLD MAINLINE TO BE REMOVED AND RETURNED TO CITY STAFF. INSTALL NEW CONTROL VALVES AS SHOWN, IN SEPARATE TRENCH. REFER TO NOTE NO. 8.
- FIELD LOCATE EXISTING MAINLINE AT THIS APPROXIMATE LOCATION, CUT MAINLINE AND MAKE NEW 2" CLASS 200 PVC MAINLINE CONNECTION TO EXISTING WITH A SCHEDULE 40 TEE FITTING. INSTALL ONE 2" GATE VALVE AND EXTEND AS SHOWN. ROUTE (6) NEW CONTROL WIRES, (2) NEW COMMON WIRES, AND (3) NEW SPARE WIRES AT END OF MAINLINE AND EXTEND TO ALL NEW VALVES. INSTALL ALL WIRE SPLICES IN 10" ROUND VALVE BOX.
- FIELD LOCATE EXISTING MAINLINE AT THIS APPROXIMATE LOCATION, CUT MAINLINE AND MAKE NEW 2" CLASS 200 PVC MAINLINE CONNECTION TO EXISTING WITH A SCHEDULE 40 TEE FITTING. INSTALL ONE 2" GATE VALVE (WHERE SHOWN ON PLANS) AND EXTEND AS SHOWN. NO WIRE WILL BE REQUIRED IN THIS SECTION.
- RECONNECT NEW CONTROL WIRES FROM NEW CONTROLLER TO (7) EXISTING CONTROL WIRES, (1) EXISTING COMMON AT THIS APPROXIMATE LOCATION. EXTEND AND COIL (1) NEW COMMON AND (3) NEW SPARE WIRES TO THIS VALVE LOCATION. INSTALL ALL WIRE SPLICES IN 10" ROUND VALVE BOX.
- RECONNECT NEW CONTROL WIRES FROM NEW CONTROLLER LOCATION TO (13) EXISTING CONTROL WIRES, (1) EXISTING COMMON AT THIS APPROXIMATE LOCATION. EXTEND AND COIL (1) NEW COMMON AND (3) NEW SPARE WIRES TO THIS LOCATION. INSTALL ALL WIRE SPLICES IN 10" ROUND VALVE BOX.

- INSTALL NEW CONTROL CONTROL VALVES, AS SHOWN, CONNECT NEW CONTROL WIRES THAT ARE EXTENDED FROM NEW CONTROLLER LOCATION. EXTEND NEW LATERAL LINES AND INSTALL NEW SPRINKLER HEADS, OR DRIP AS SHOWN.
- INSTALL NEW TRENCH WITH NEW CONTROL WIRES NEXT TO EXISTING MAINLINE ROUTING, OFFSET BY 2" (IF NO MAINLINE, TRENCH WIRE IN ITS OWN TRENCH). RECONNECT NEW WIRES TO ALL EXISTING AND NEW CONTROL VALVES, ALONG NEW WIRE PATH. OLD CONTROL WIRES WILL BE ABANDONED IN PLACE. INSTALL ALL WIRE SPLICES IN 10" ROUND VALVE BOX. REPLACE AND REPAIR EXISTING IRRIGATION EQUIPMENT DAMAGED BY NEW CONSTRUCTION AND WIRE TRENCHING.
- INSTALL NEW SPRINKLERS AND EXTEND NEW LATERAL LINES IN THIS AREA TO BLEND INTO EXISTING NATIVE SPRINKLER COVERAGE. ASSURE OPERATION OF EXISTING ZONE. REPAIR AND/OR REPLACE ANY DAMAGED IRRIGATION EQUIPMENT IF DAMAGED DURING NEW CONSTRUCTION.
- LOCATE AND PROTECT EXISTING CONTROLLER AT THIS APPROXIMATE LOCATION. COORDINATE WITH CITY STAFF TO REMOVE (QTN. 24) WIRES THAT OPERATE ZONES AROUND TOWN HALL. ALL REMAINING WIRES TO REMAIN ON THIS CONTROLLER. EXTEND NEW WIRES (SAME QTN. & COLORS) FROM THIS EXISTING CONTROLLER TO NEW CONTROLLER. REPLACE AND REPAIR EXISTING IRRIGATION EQUIPMENT DAMAGED BY NEW CONSTRUCTION AND WIRE TRENCHING.
- LOCATE EXISTING CONTROL VALVE AT THIS APPROXIMATE LOCATION, CONNECT NEW LATERAL LINES AND INSTALL NEW SPRINKLER HEADS, AS SHOWN.
- CONTRACTOR TO REPLACE AND REPAIR EXISTING IRRIGATION EQUIPMENT DAMAGED BY NEW CONSTRUCTION, BORING AND WIRE TRENCHING.
- FIELD LOCATE EXISTING 2" PVC MAINLINE AT THIS APPROXIMATE LOCATION, CUT MAINLINE AND MAKE NEW 2" CLASS 200 PVC MAINLINE CONNECTION USING SCHEDULE 40 FITTINGS AND EXTEND AS SHOWN. ROUTE (15) NEW CONTROL WIRES, (2) NEW COMMON WIRES, AND (3) NEW SPARE WIRES BACK TO NEW CONTROLLER LOCATION. INSTALL ALL WIRE SPLICES IN 10" ROUND VALVE BOX.
- INSTALL NEW SLEEVES, AS SHOWN, SIZE AND QTN. NOTED. SLEEVES TO BE BORED IF NEW UNDER EXISTING PAVEMENT.
- RECONNECT NEW CONTROL WIRES FROM NEW CONTROLLER TO (10) EXISTING CONTROL WIRES, (1) EXISTING COMMON AT THIS APPROXIMATE LOCATION. EXTEND AND COIL (1) NEW COMMON AND (3) NEW SPARE WIRES TO THIS VALVE LOCATION. INSTALL ALL WIRE SPLICES IN 10" ROUND VALVE BOX.
- LOCATE AND PROTECT EXISTING 2" PVC MAINLINE AND CONTROL WIRES THAT OPERATE IRRIGATION IN 2 MEDIANS (10 CONTROL VALVES TOTAL FOR BOTH)

AndersonMasonDale Architects

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Parker, CO 22-093
Town of Parker
20120 Main Street
Parker, CO 80138

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Denver, CO 80202
Telephone: 303-294-9448

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wenk ASSOCIATES

Issue Date
PERMIT / GMP DRAWINGS 15 MAY 2024
COB-001 19 JULY 2024



Project Number: 22-093
Drawn By: KJD
Reviewed By: JSB/KJD
Approved By: JSB

IRRIGATION PLAN

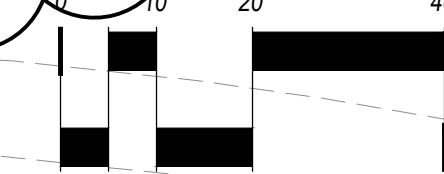
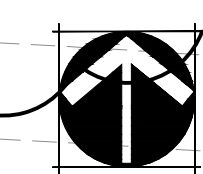
IR-101



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o. 303.980.5327 www.hydrostemskdi.com



Parker Town Hall Expansion

Parker, CO 22-093

Town of Parker

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Parker, CO 80138

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Telephone: 303-988-4514

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AE Design
1900 Wazee Street #205
Denver, CO 80222
Telephone: 303-296-3034



ASSOCIATES

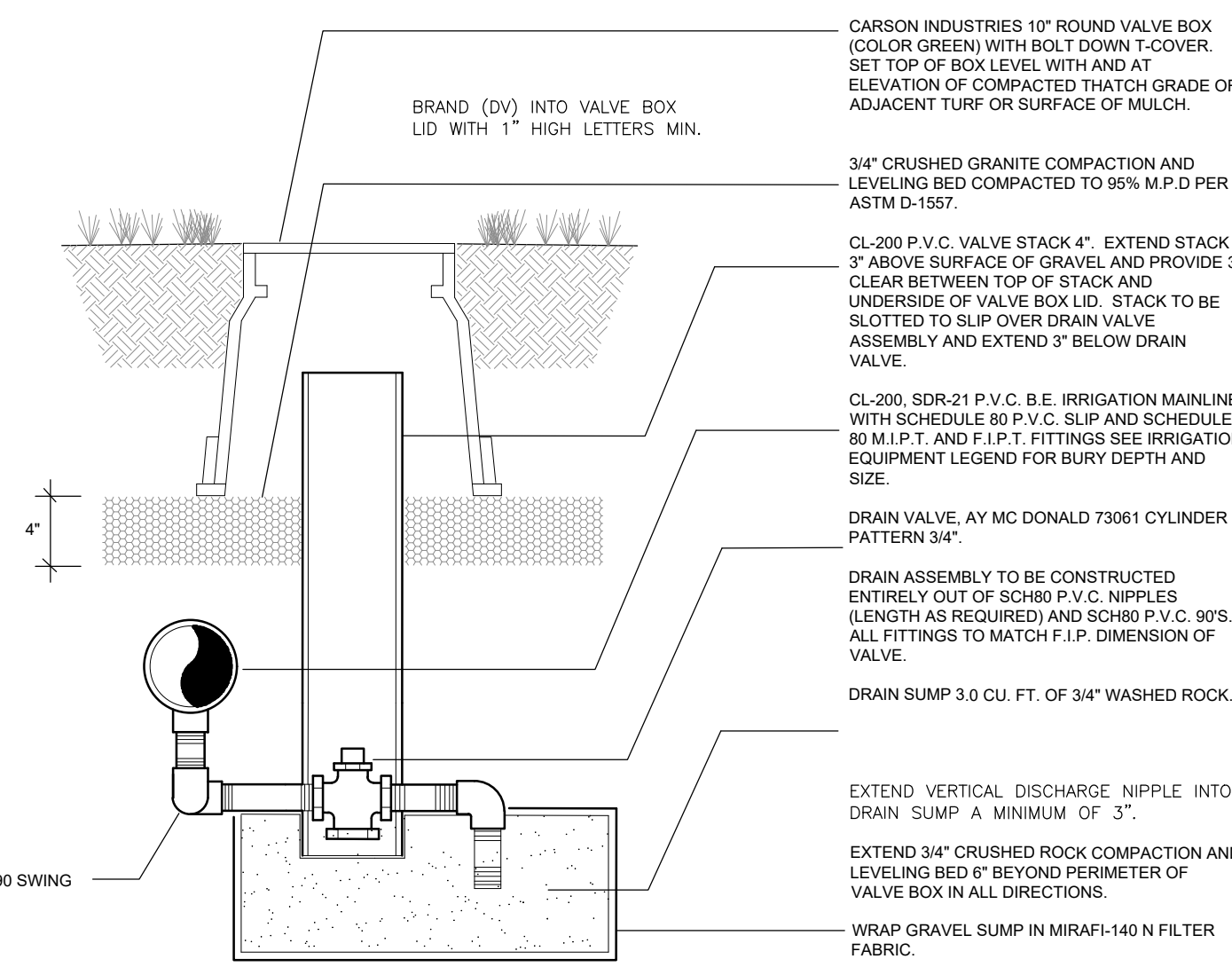
Issue	Date
PERMIT / GMP DRAWINGS	15 MAY 2024
COB-001	19 JULY 2024



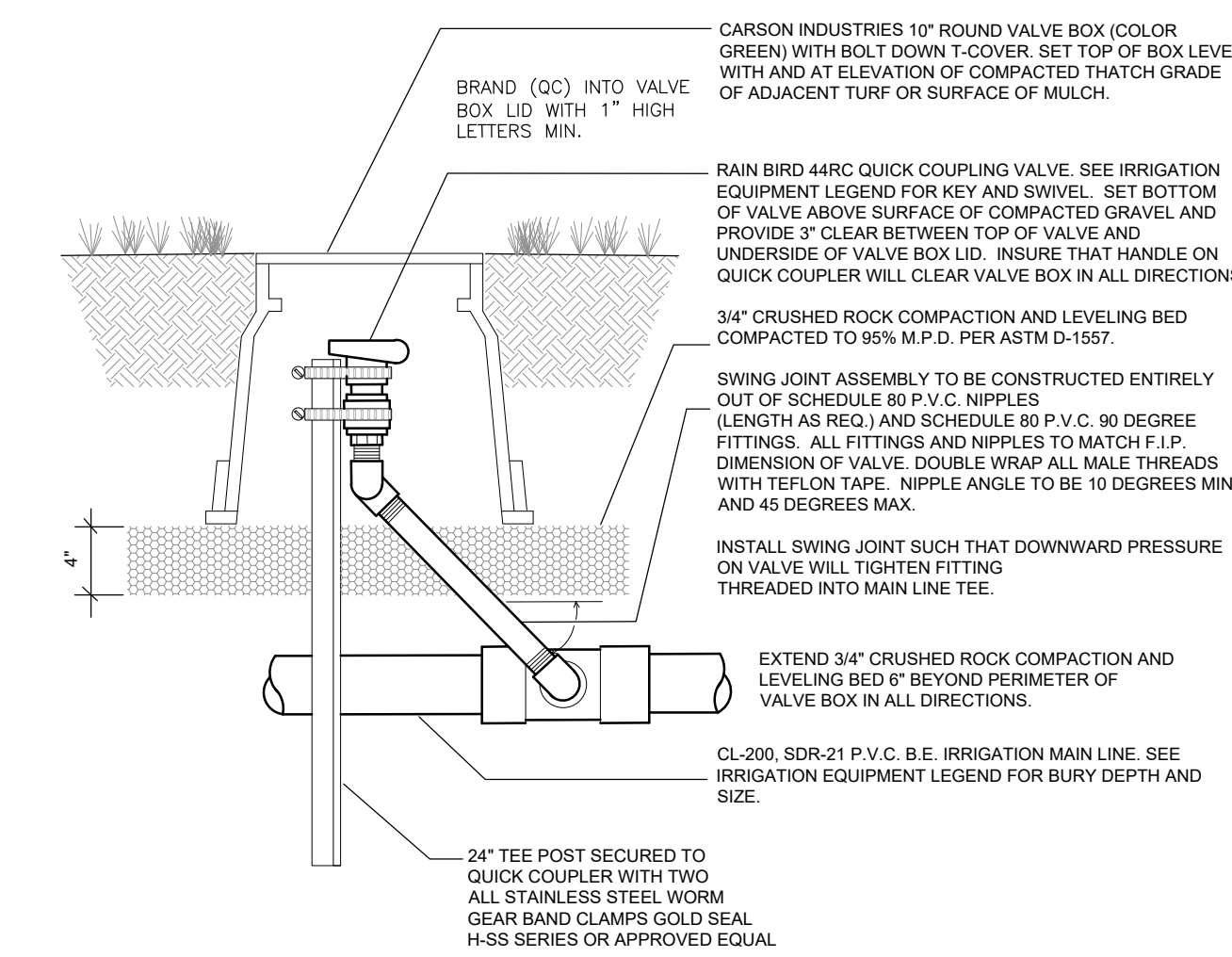
Project Number:	22-093
Drawn By:	KJD
Reviewed By:	JSB, KJD
Approved By:	JSB

IRRIGATION DETAILS

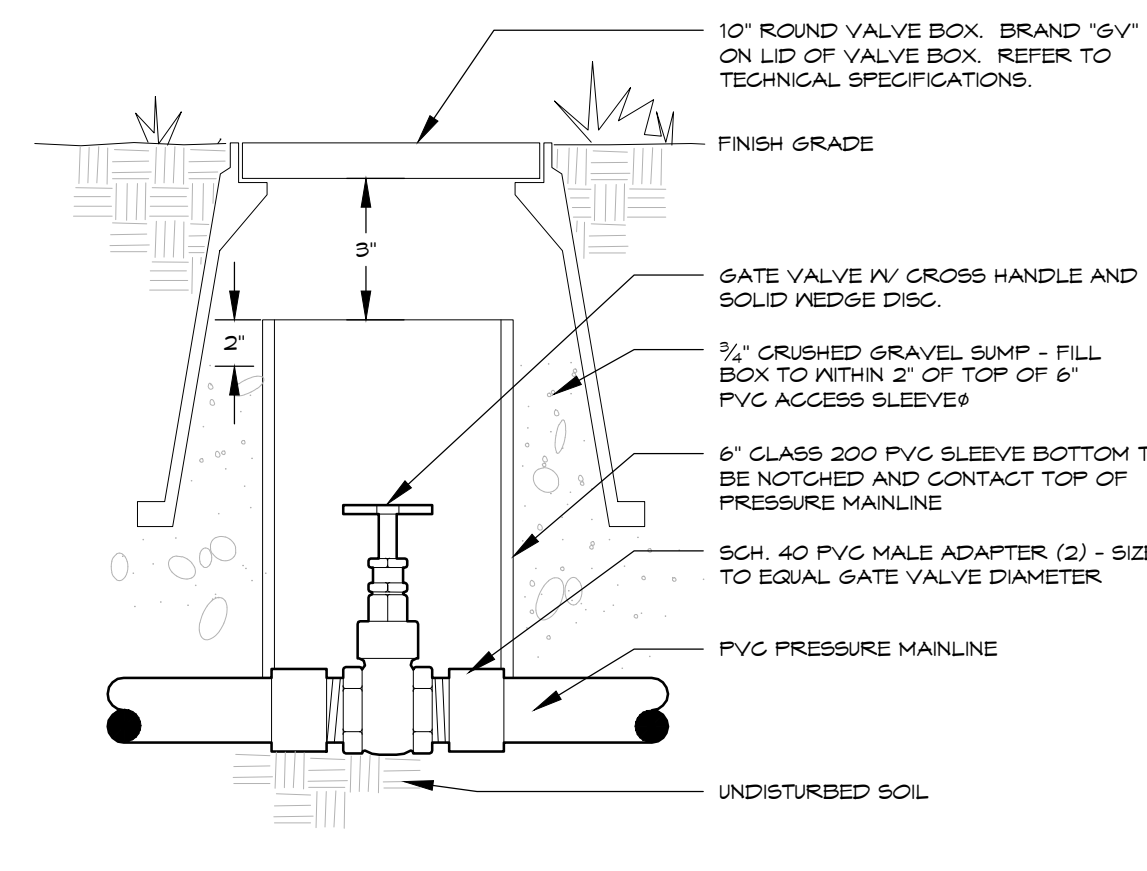
IR-201



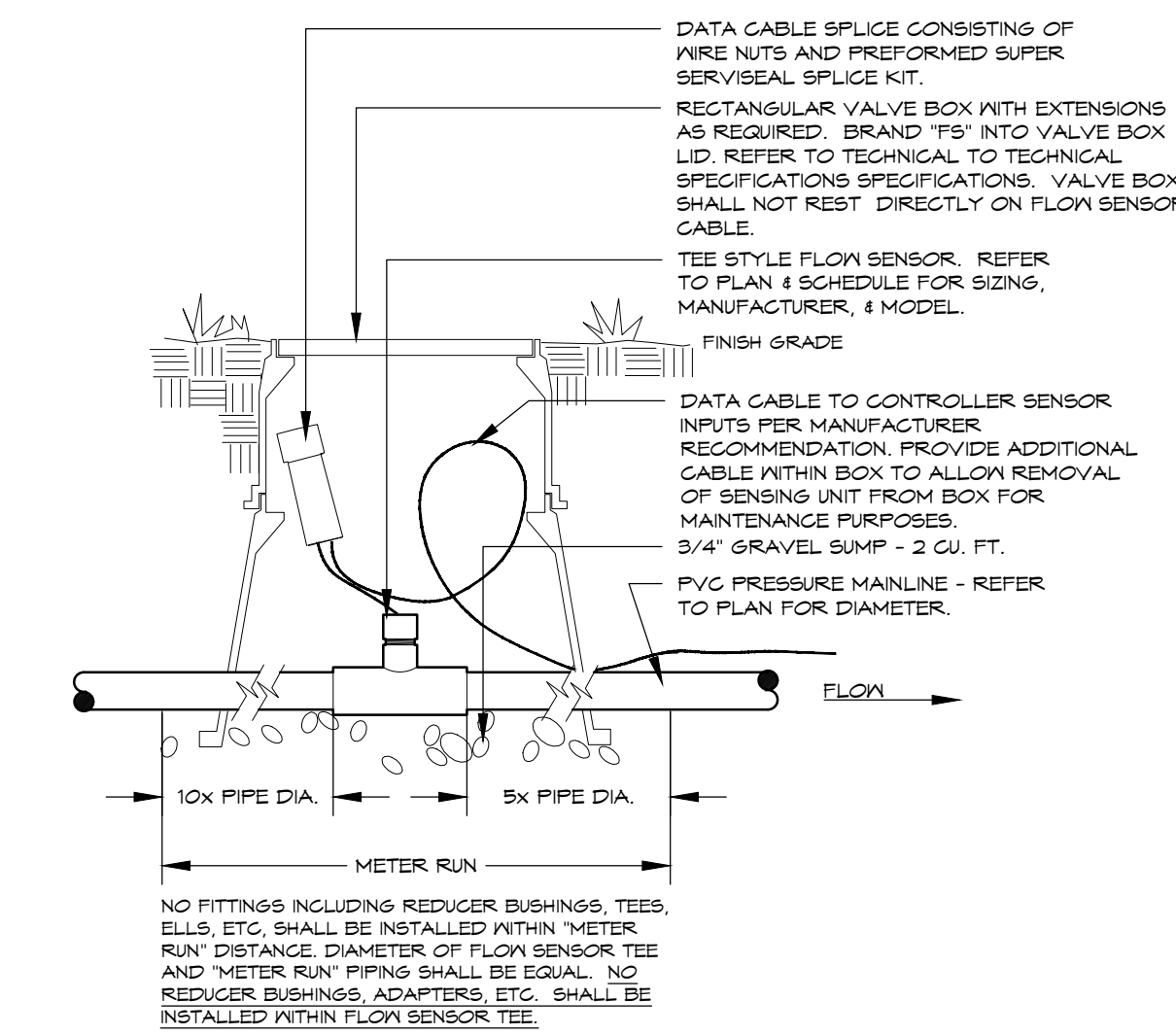
1 MANUAL DRAIN VALVE
NO SCALE



2 QUICK COUPLER VALVE
NO SCALE



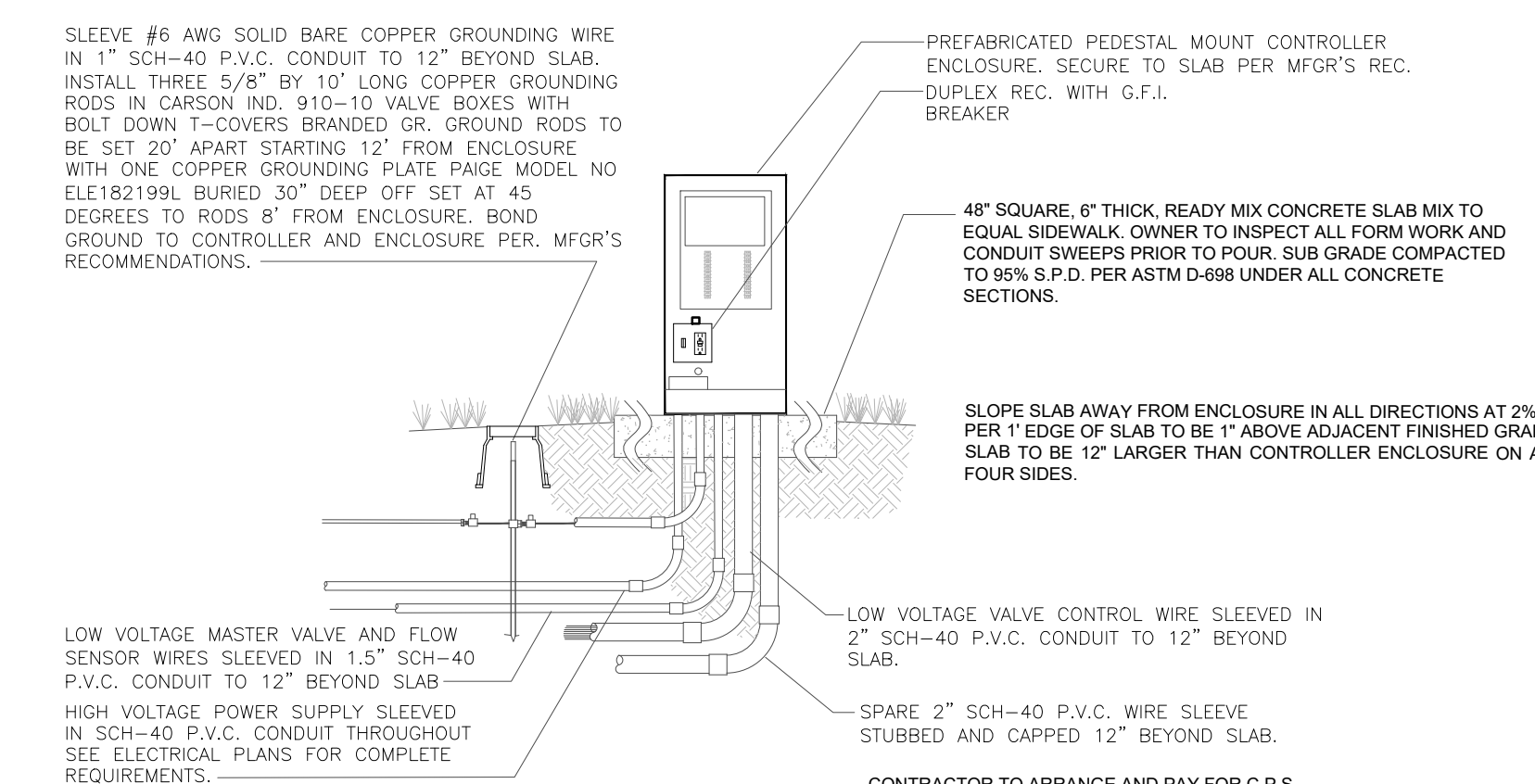
3 GATE VALVE
2.5\"/>



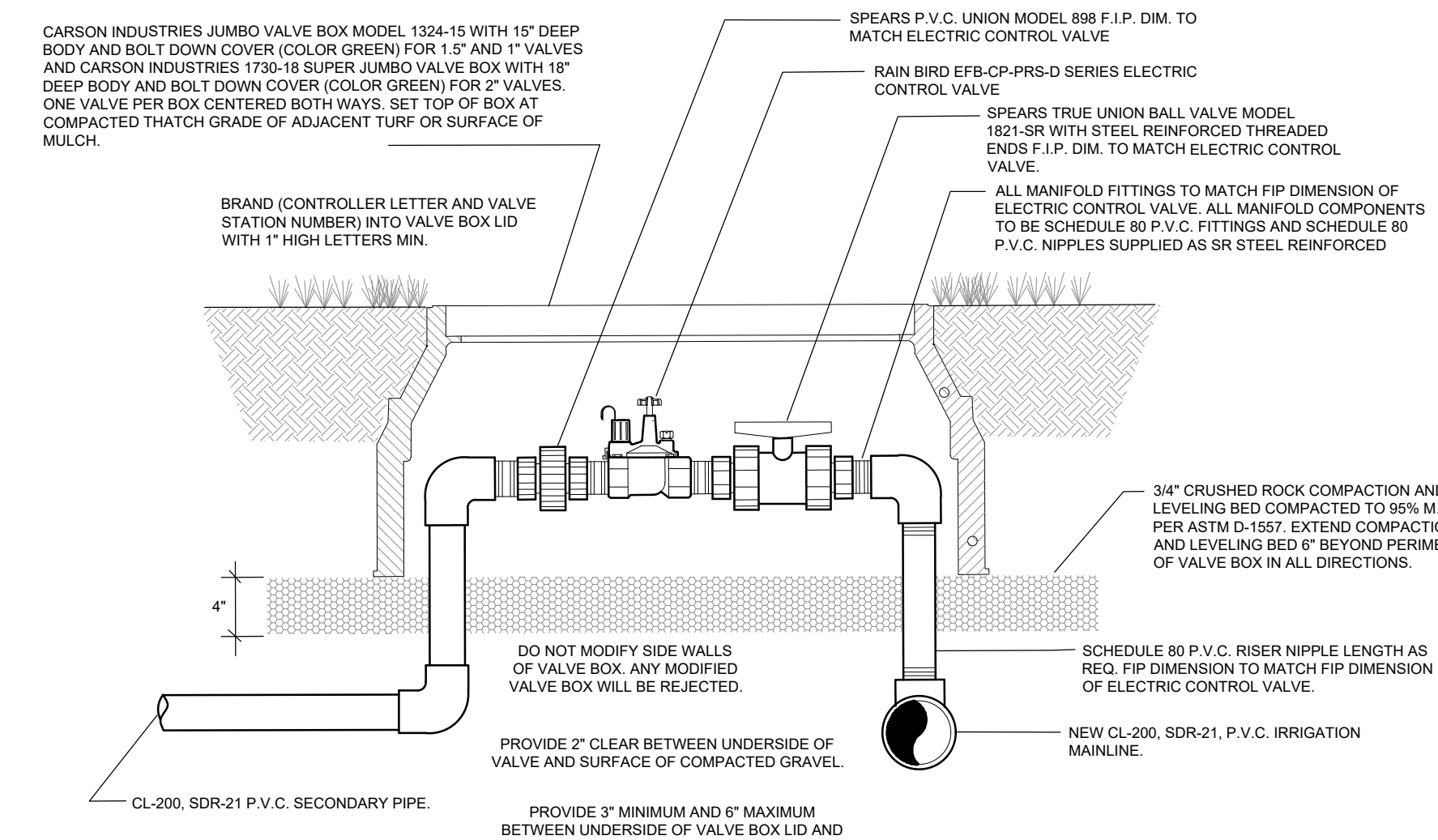
4 FLOW SENSOR
VIA DATA CABLE - TEE STYLE

Controller Specification Summary:

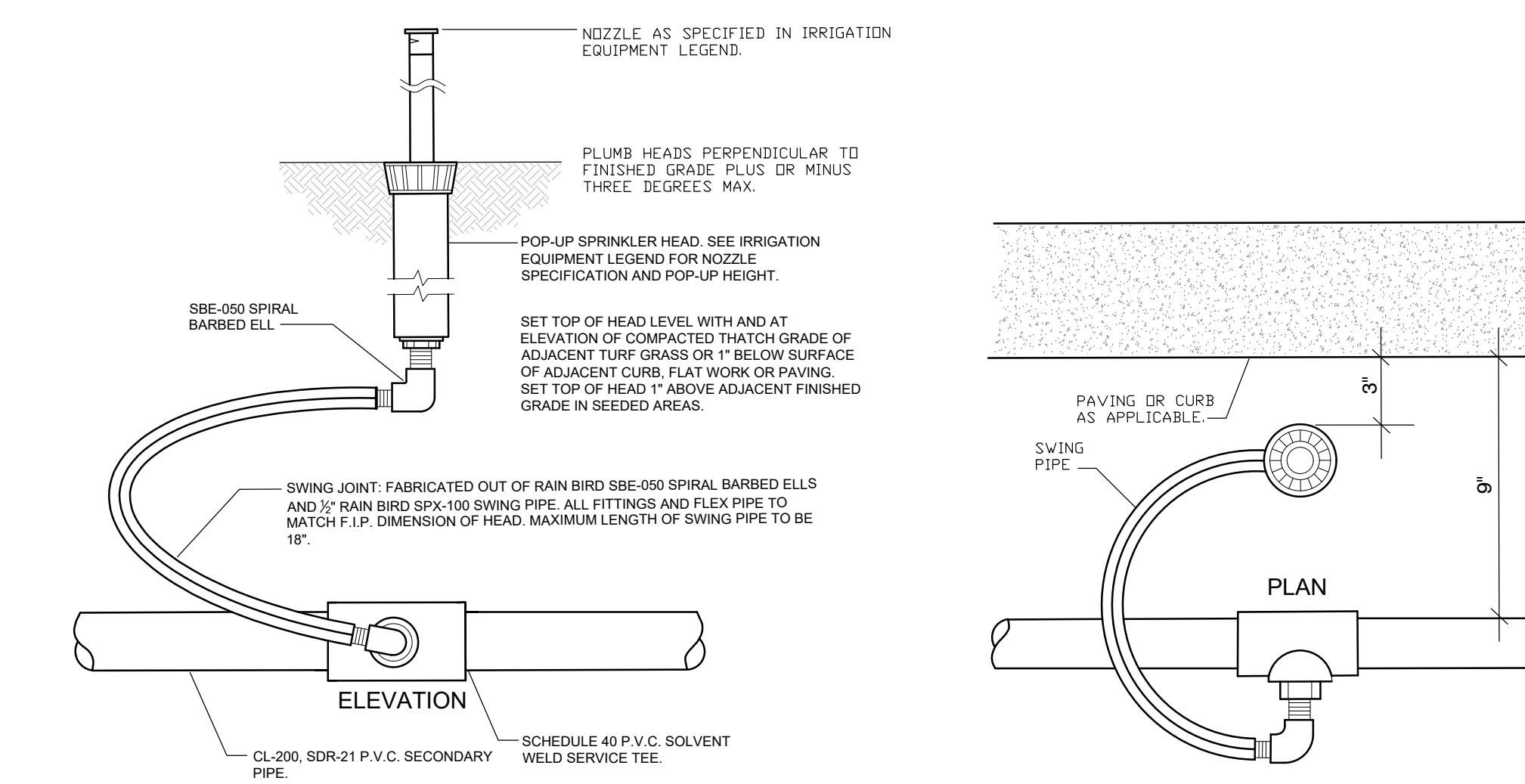
CONTROLLER (C1) - CONTRACTOR TO SUPPLY AND INSTALL ONE NEW TORO SENTINEL SERIES CONTROLLER MODEL NO. (SEE DRAWINGS)



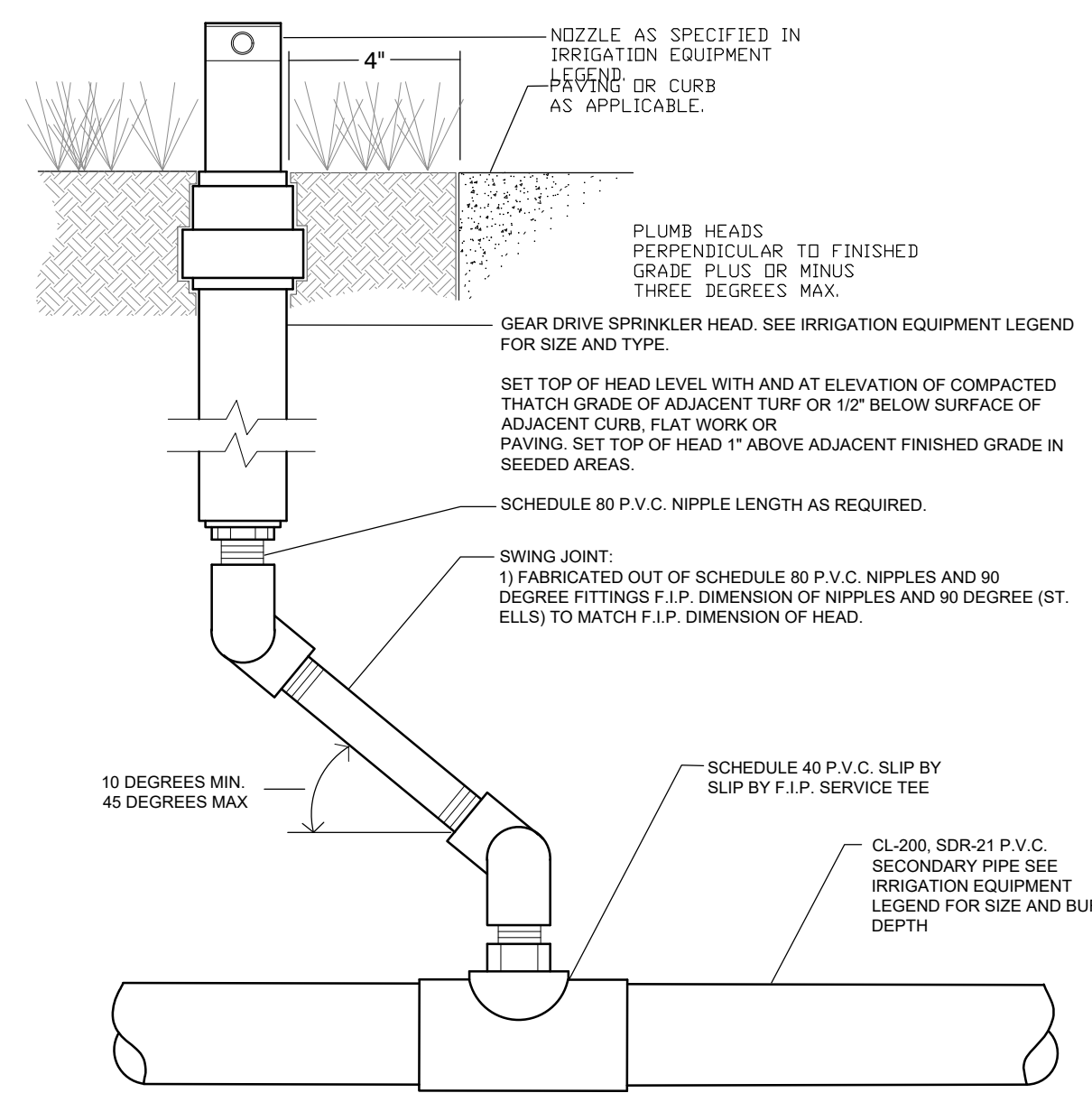
5 CONTROLLER AND ENCLOSURE ELEVATION
NO SCALE



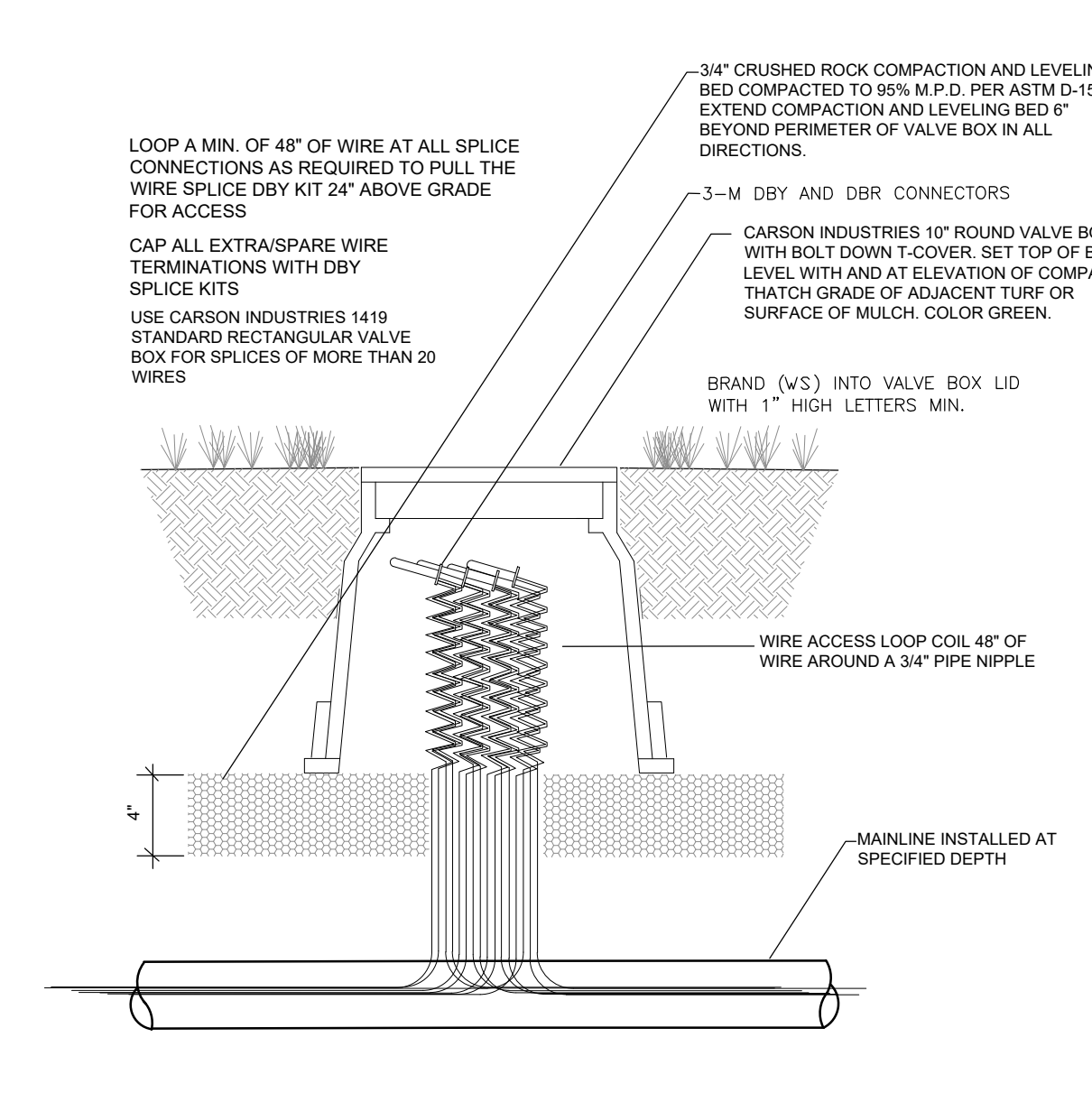
6 ELECTRIC CONTROL VALVE FOR SPRAY ZONES
NO SCALE



7 POP-UP SPRINKLER HEAD
NO SCALE



8 GEAR DRIVE SPRINKLER HEAD
NO SCALE



9 WIRE SPLICE ACCESS LOOP
NO SCALE

DIRECTORY

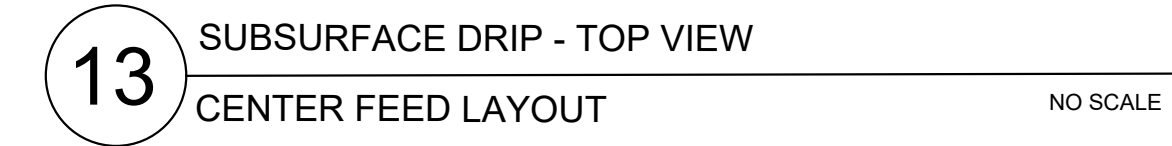
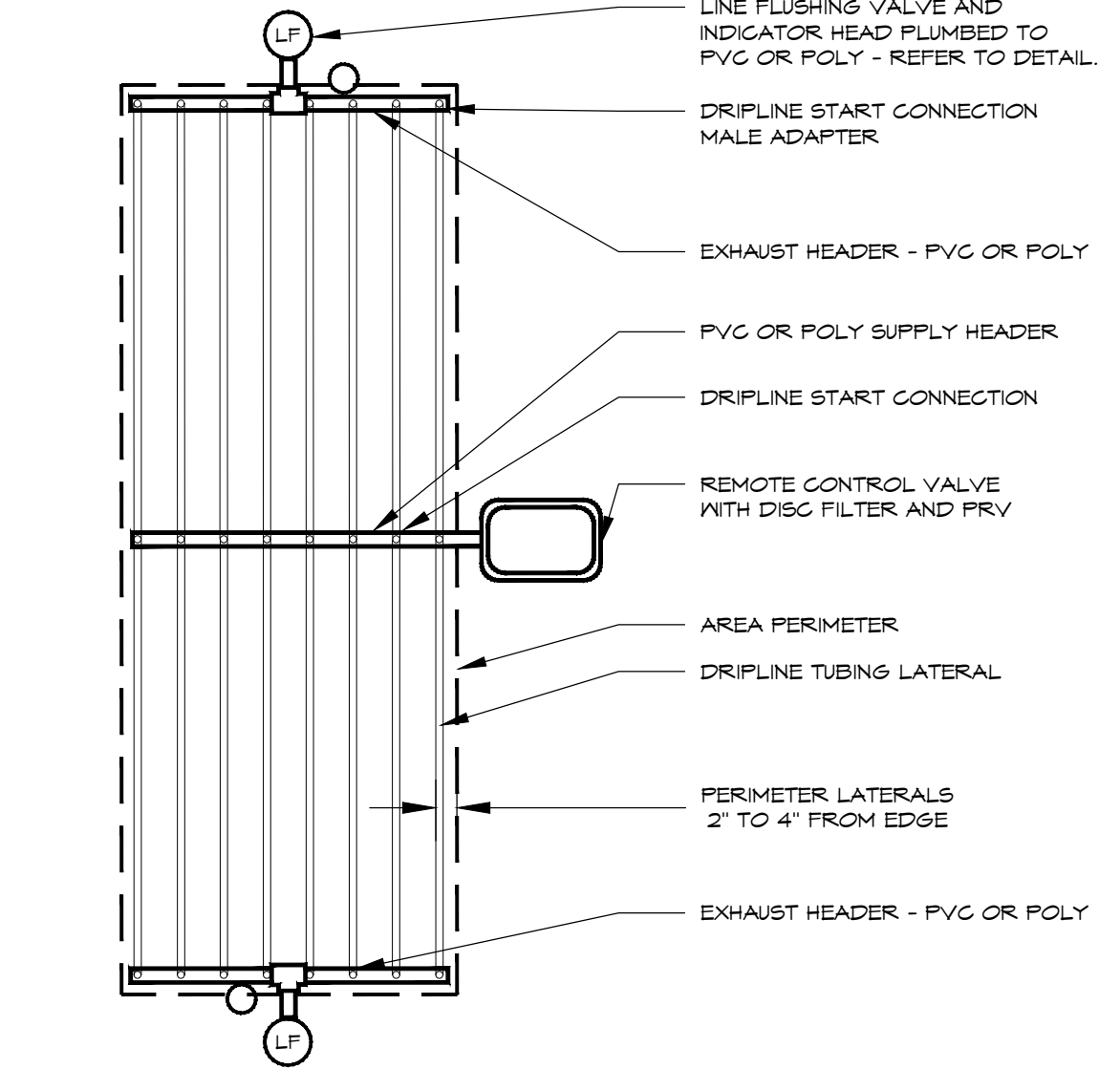
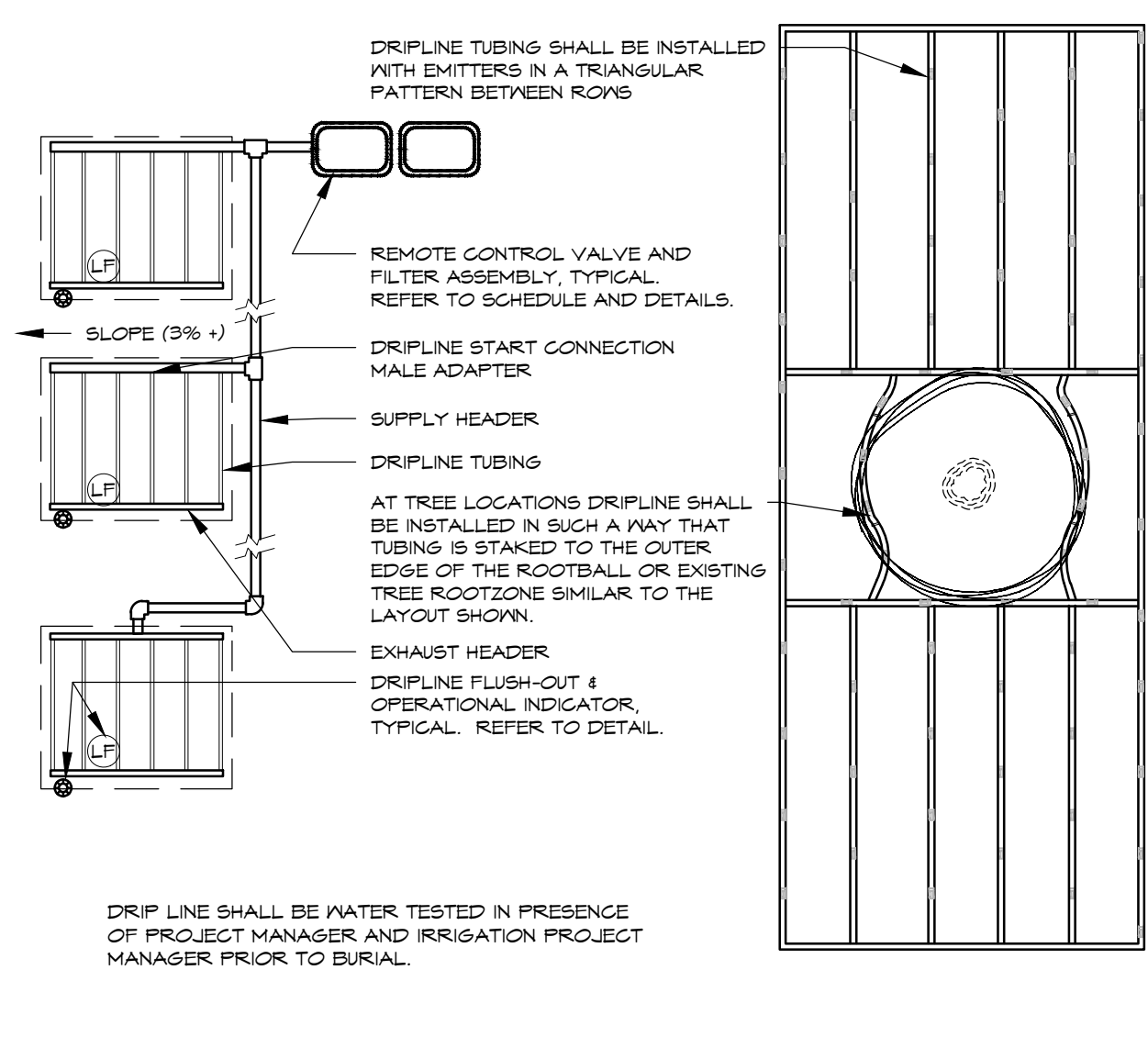
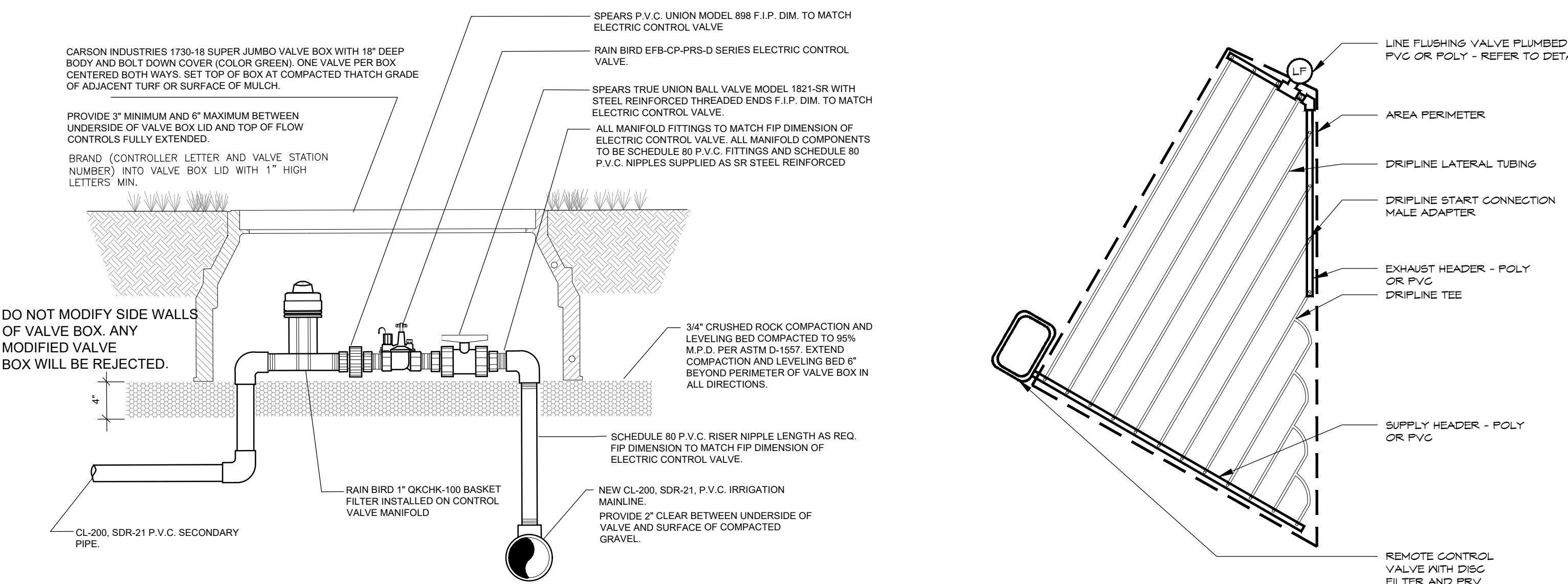
IRRIGATION SCHEDULE	IR-000
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o: 303.980.5327 www.hydrosystemsksdi.com

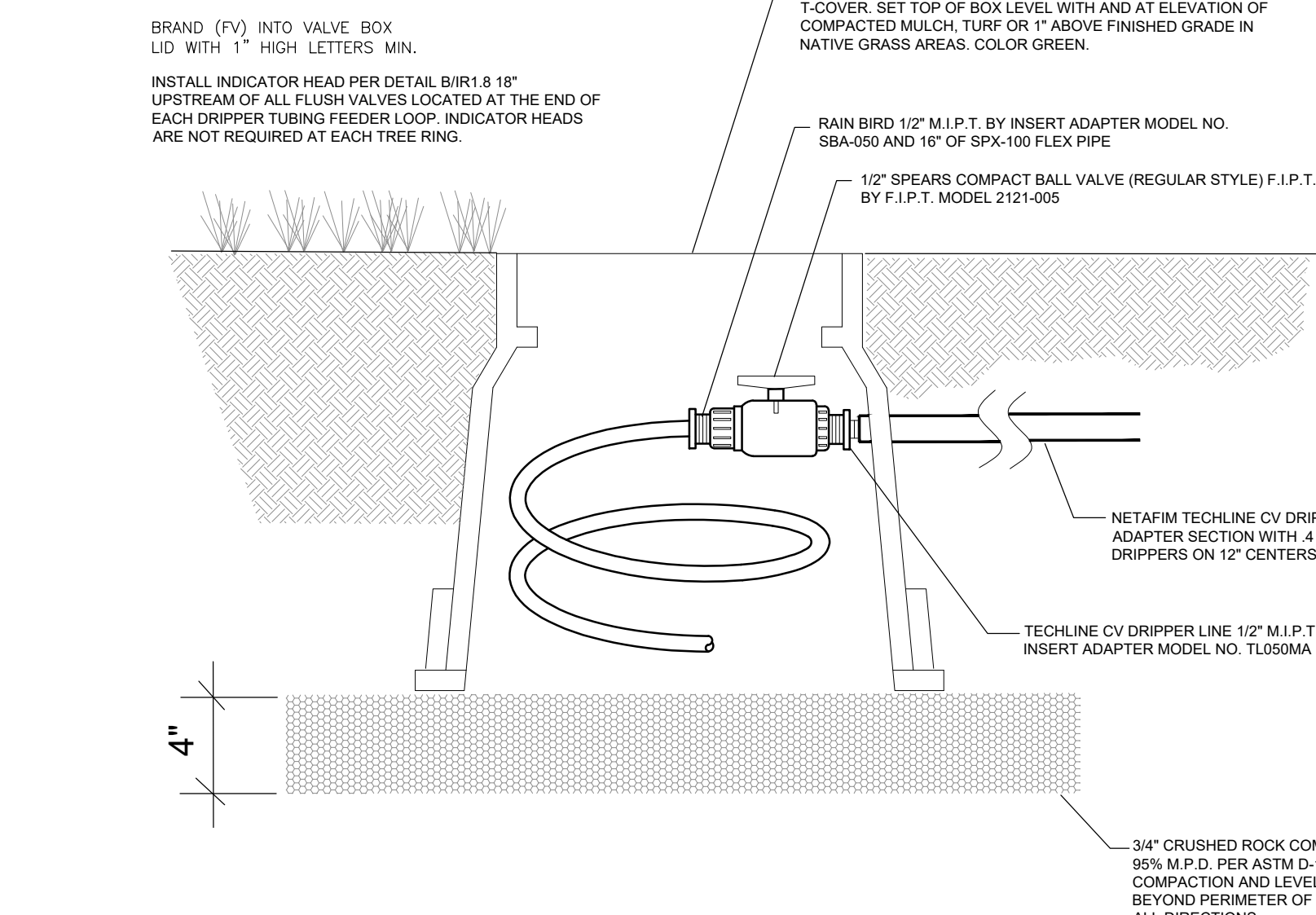
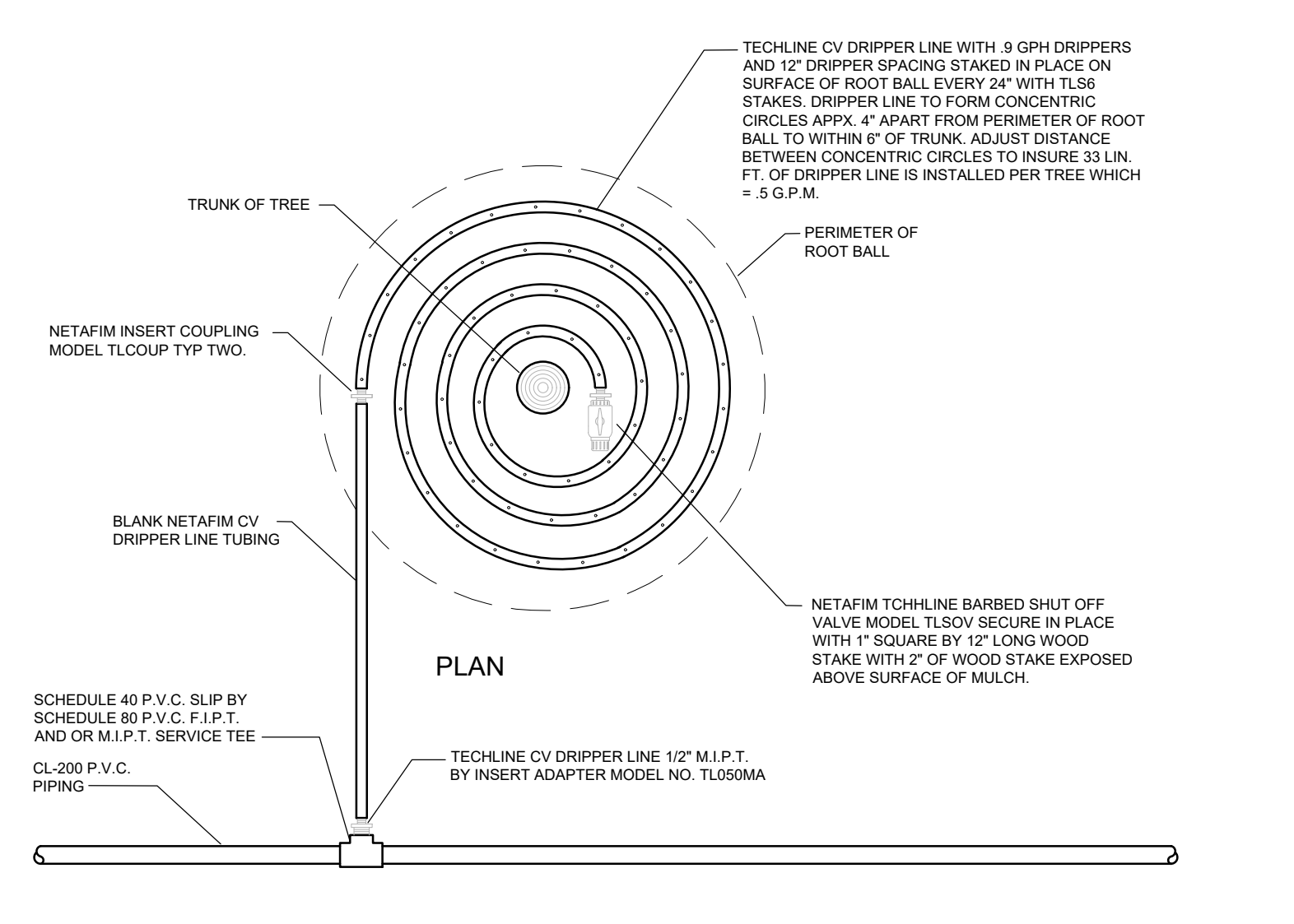
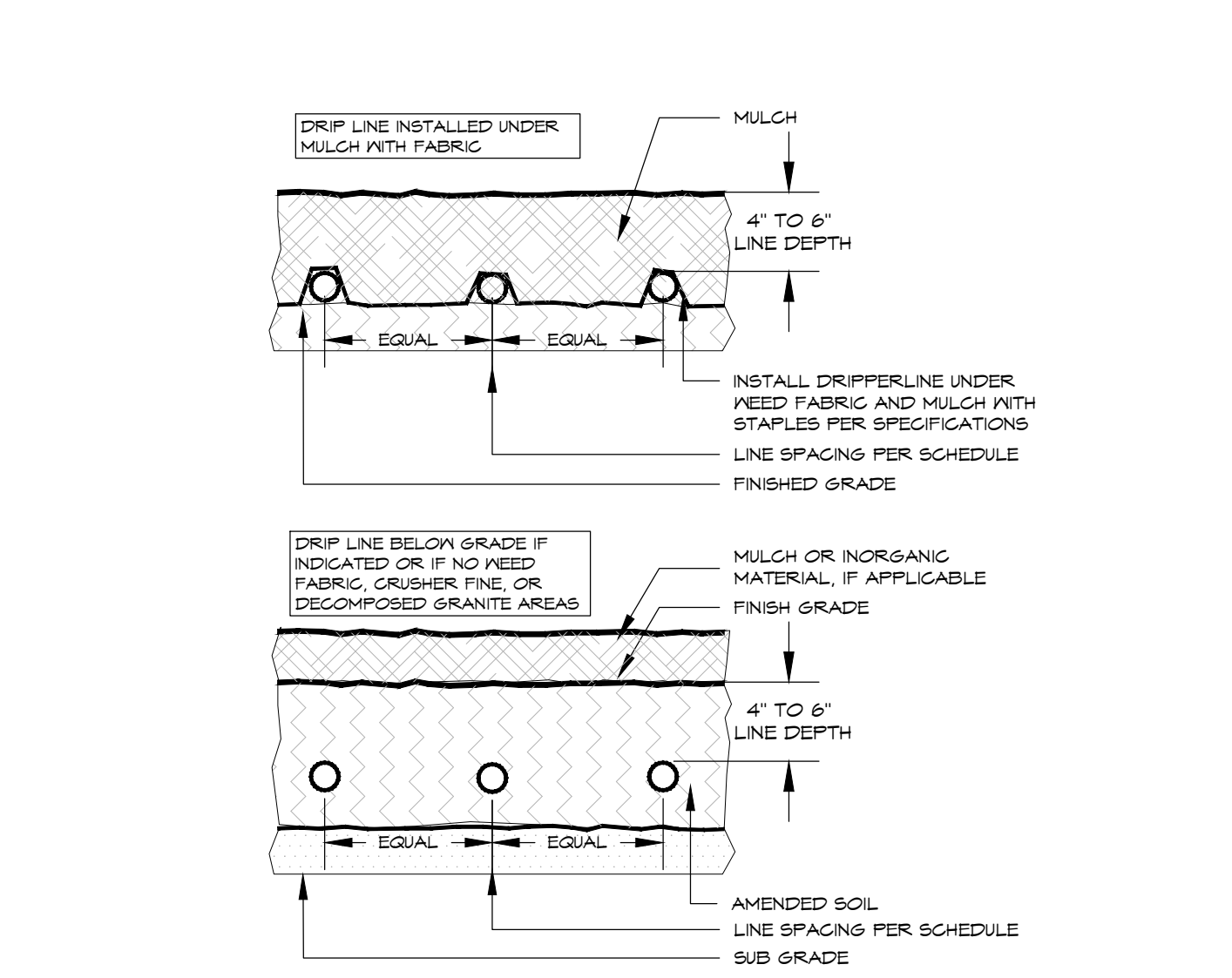
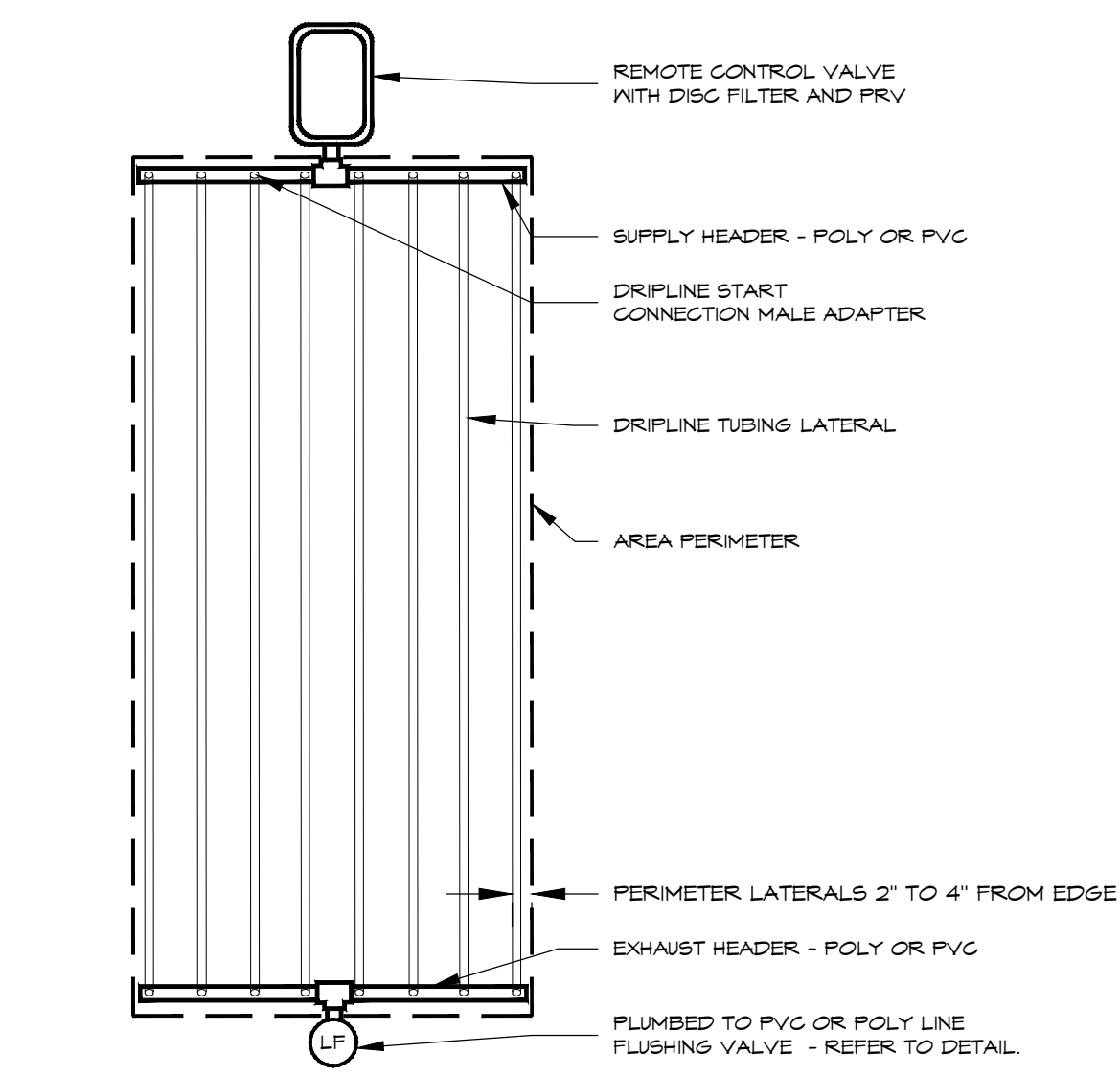


10 ELECTRIC CONTROL VALVE FOR DRIPPER LINE ZONES
 NO SCALE

11 SUBSURFACE DRIP - TOP VIEW IRREGULAR AREAS
 NO SCALE

12 EMITTER TUBING LAYOUT AREAS AROUND TREES
 NO SCALE

13 SUBSURFACE DRIP - TOP VIEW CENTER FEED LAYOUT
 NO SCALE

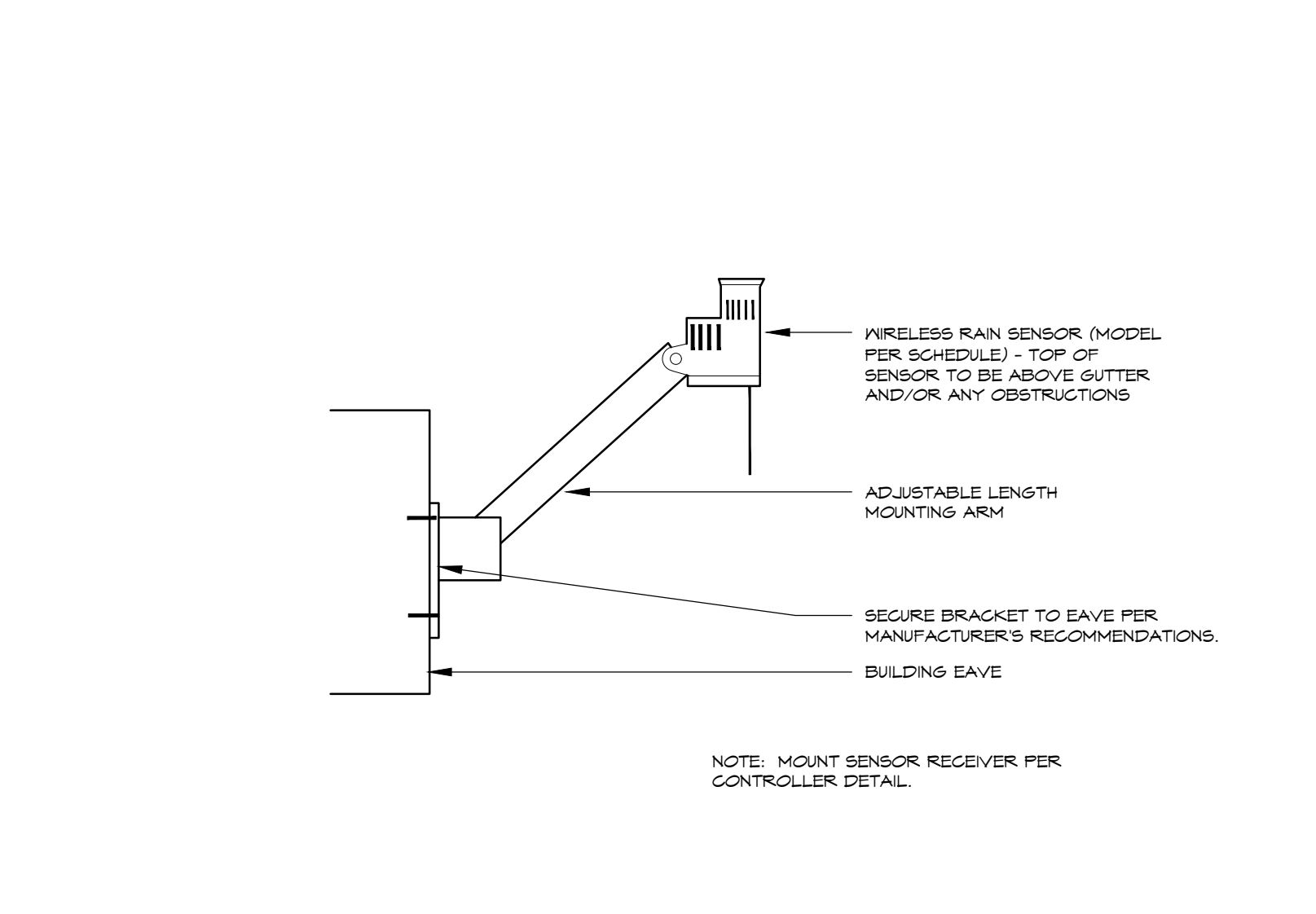
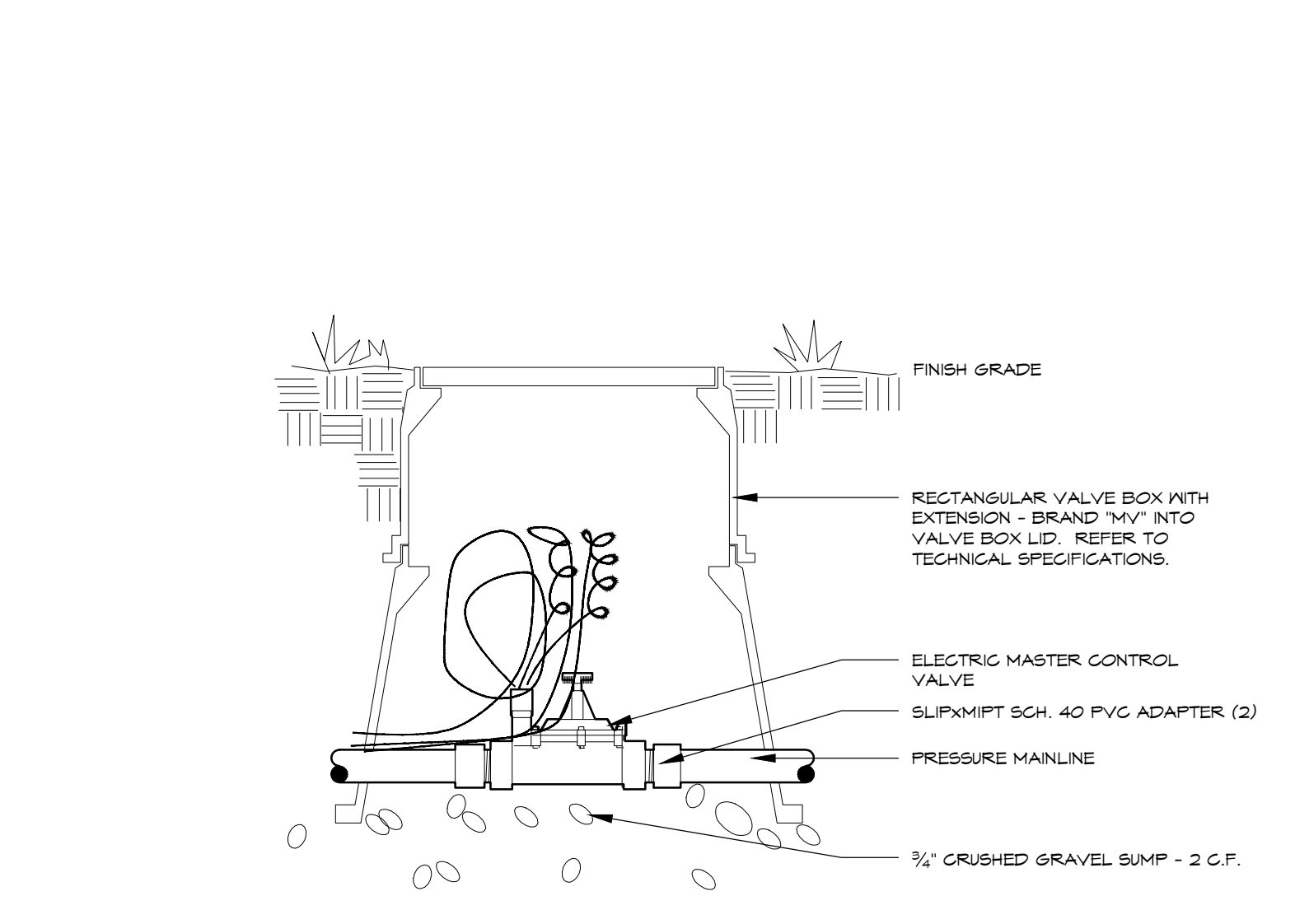
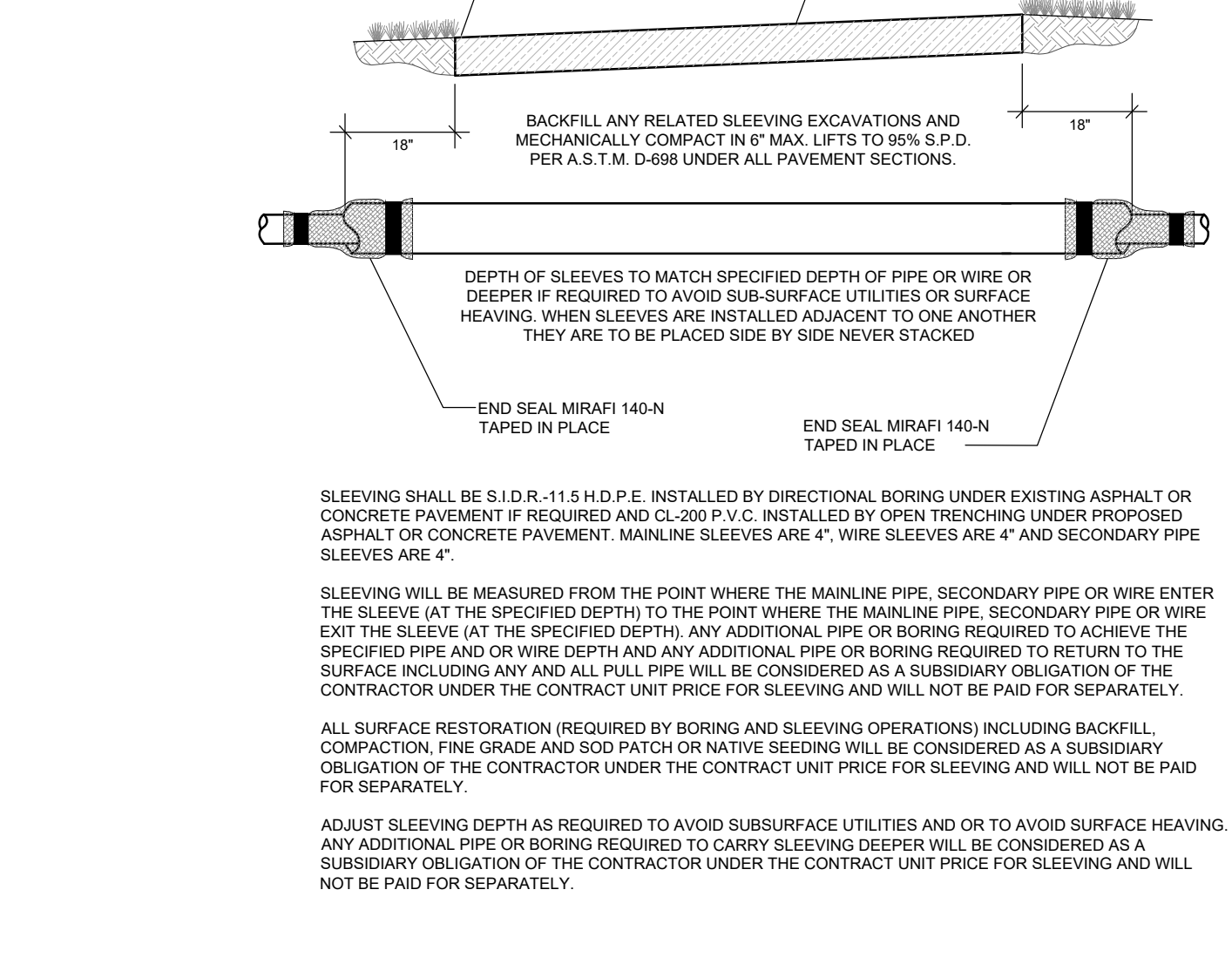
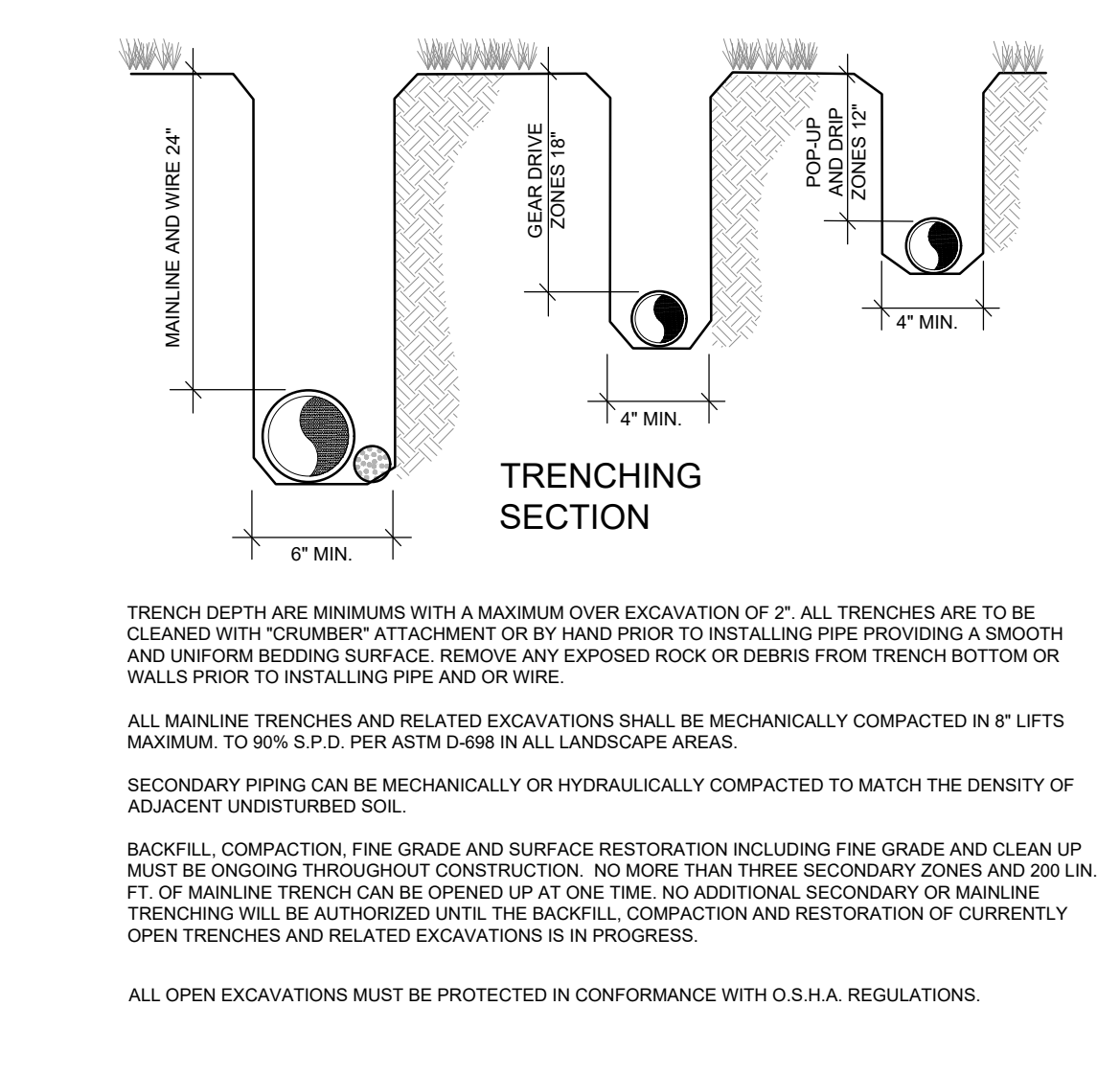


14 SUBSURFACE DRIP - TOP VIEW END FEED LAYOUT
 NO SCALE

15 SUBSURFACE DRIP HORIZONTAL VIEW
 NO SCALE

16 CV DRIPPER LINE WITH FLUSH VALVE INSTALLATION FOR TREE IRRIGATION IN NATIVE AND SHRUB AREAS
 NO SCALE

17 CV DRIPPER LINE FLUSH VALVE INSTALLATION AT END OF EACH DRIPPER PIPE LOOP
 NO SCALE



18 IRRIGATION TRENCHING
 NO SCALE

19 IRRIGATION SLEEVING
 NO SCALE

20 MASTER VALVE 24V - Small - Typical
 NO SCALE

21 WEATHER SENSOR EAVE MOUNTED - Wireless
 NO SCALE

DIRECTORY	
IRRIGATION SCHEDULE	IR-000
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