

O'Reilly AUTO PARTS

O'REILLY AUTO PARTS PARKER, CO (PK2)

LOT 1 OF PARKER POINTE SUBDIVISION FILING NO 1

LOCATED IN THE NORTHEAST 1/4 SECTION OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST, OF THE 6TH P.M., TOWN OF PAKER, COUNTY OF DOUGLAS, STATE OF COLORADO

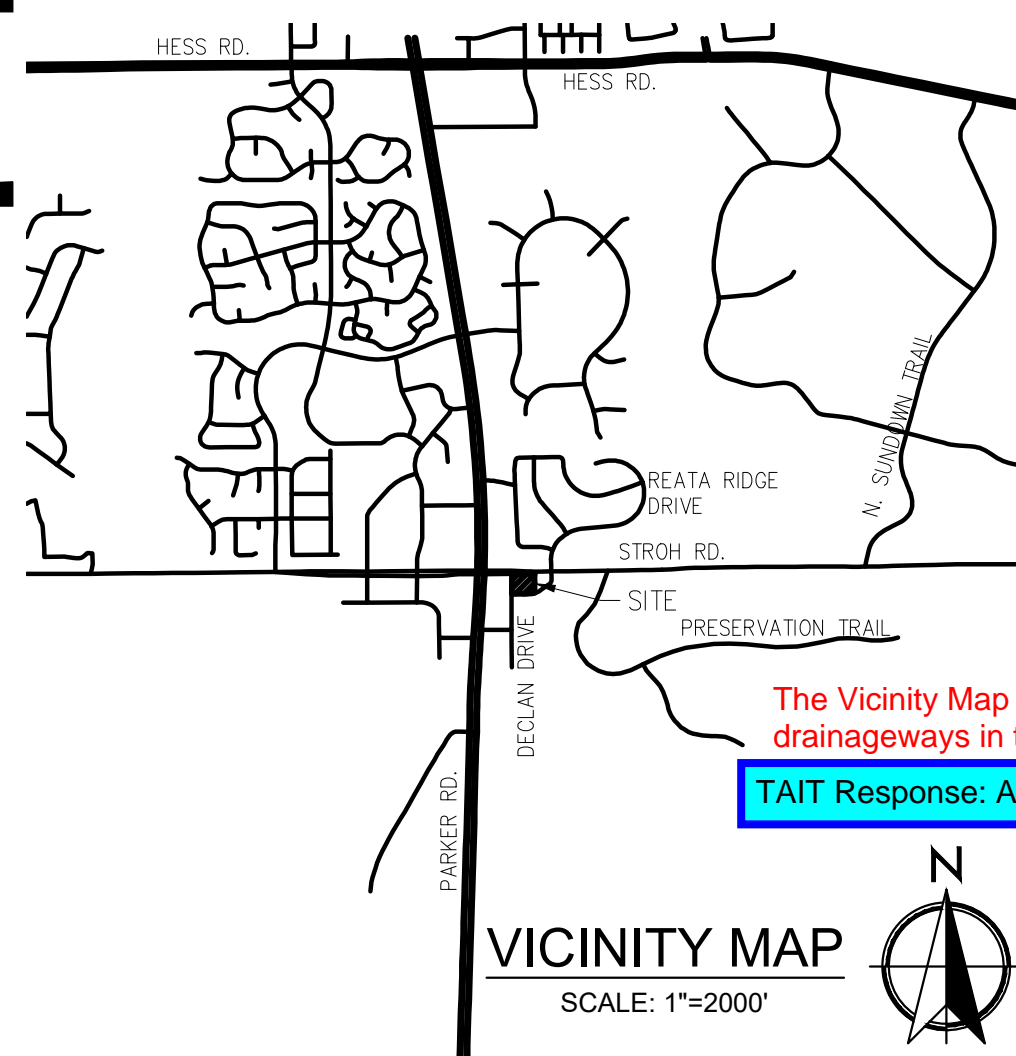
TOWN OF PARKER CONTACTS

BUILDING
JON NELSON
JNELSON@PARKERCO.GOV
(303) 805-3194

FIRE LIFE SAFETY
RANDY CAPRA
RCAPRA@PARKERCO.GOV
(303) 805-3163

ENGINEERING
ALEX MESTDAGH
AMESTDAGH@PARKERCO.GOV
(303) 805-3204

PLANNING
AMBER WOOD HICKEN
AHICKEN@PARKERCO.GOV
(303) 805-3338



The Vicinity Map should include any major drainageways in the area. (Kinney Creek)
TAIT Response: Added major drainageways

UTILITY PURVEYORS

WATER/SEWER
PARKER WATER AND SANITATION DISTRICT
LISA SINDALL
(303) 841-4627

NATURAL GAS
XCEL ENERGY
(800) 628-2121

ELECTRIC
CORE ELECTRIC COOPERATIVE
BROOKS KAUFMAN
(303) 688-3100

TELEPHONE
CENTURYLINK
(720) 578-5142

TAIT Response: Added requested information into an "Earthwork Summary" section of the coversheet

For Construction Plans, please provide on the cover sheet a table of the volume of cut, volume of fill, volume of material to be exported offsite, the steepest proposed slopes, the total area of land disturbance, the existing impervious area, the proposed impervious area (total impervious area for the site) and the area of land disturbance treated by a water quality control measure per the SDECM.

LOT AREA	0.999 AC / 43,310 SF
PROPOSED USE	COMMERCIAL RETAIL
DWELLING UNITS	NONE
TOTAL BUILDINGS	1
BUILDING GFA	8,000 SF
SETBACKS: REQUIRED	25' BUILDING SETBACK
PROVIDED	6'10" LANDSCAPE BUFFER 25' BUILDING SETBACK 6'10" LANDSCAPE BUFFER
PARKING:	
CALCULATION	1 SPACE PER 300 SF GFA
TOTAL REQUIRED	27
TOTAL PROVIDED	27
ACCESSIBLE REQUIRED	2
ACCESSIBLE PROVIDED	2
BICYCLE PARKING:	
CALCULATION	2 PER 10,000 SF GFA
REQUIRED	2
PROVIDED	2
PERCENTAGE LANDSCAPE	REFER TO LANDSCAPE PLANS

SHEET INDEX

- CT1.1 COVER SHEET
- CT1.2 GENERAL NOTES
- D1.1 DEMOLITION PLAN
- C1.1 SITE GRADING PLAN
- C1.2 SITE EROSION CONTROL PLAN
- C2.1 SITE DEVELOPMENT PLAN
- C2.2 SITE DETAILS
- C2.3 REFUSE ENCLOSURE
- C2.4 NYLOPLAST DETAILS
- C3.1 SITE UTILITY PLAN

ABBREVIATIONS

BACK OF CURB
BOTTOM OF PIPE
CURB FACE
EXPOSED FOUNDATION
EXISTING GROUND
EXISTING SURFACE
FINISHED FLOOR
FLOW LINE
FINISHED SURFACE
INVERT
SIDEWALK
TOP OF CURB
TOP OF GRATE
TOP OF PIPE

BC
BOP
CF
EF
EG
ES
FF
FL
FS
INV
SW
TC
TOG
TOP

CIVIL ENGINEER

320 North Lincoln Avenue
Loveland, CO 80537
p: 970.613.1447
www.tait.com
ENGINEERING ENVIRONMENTAL BUILDING LAND
Since 1964 Santa Ana Sacramento Denver Dallas
San Luis Obispo Riverside Boise

OWNER AND DEVELOPER

O'REILLY AUTO ENTERPRISES, LLC
CORPORATE OFFICES
233 SOUTH PATTERSON
SPRINGFIELD, MISSOURI 65802
417-862-2674 PHONE

BIDDING INFORMATION:
REFER TO OWNER'S WEB SITE:
HTTP://WWW.O'REILLYPLANROOM.COM

NOTE: REFER TO CURRENT PROJECTS LIST,
LOCATED AT BOTTOM OF SIGN IN PAGE, FOR INVITED
GENERAL CONTRACTORS.

ALL SUB-CONTRACTOR BIDS TO BE SUBMITTED TO
INVITED GENERAL CONTRACTORS ONLY.

CONTACT OWNER'S DESIGNATED REPRESENTATIVE
FOR ADDITIONAL PROJECT INFORMATION.

ARCHITECT

TIMOTHY M. GUILLOT
ARCHITECT
1736 East Sunshine, Suite 417
Springfield, Missouri 65804
417.862.0558
Fax: 417.862.3265
e-mail: architect@esterlyschneider.com

BASIS OF BEARING

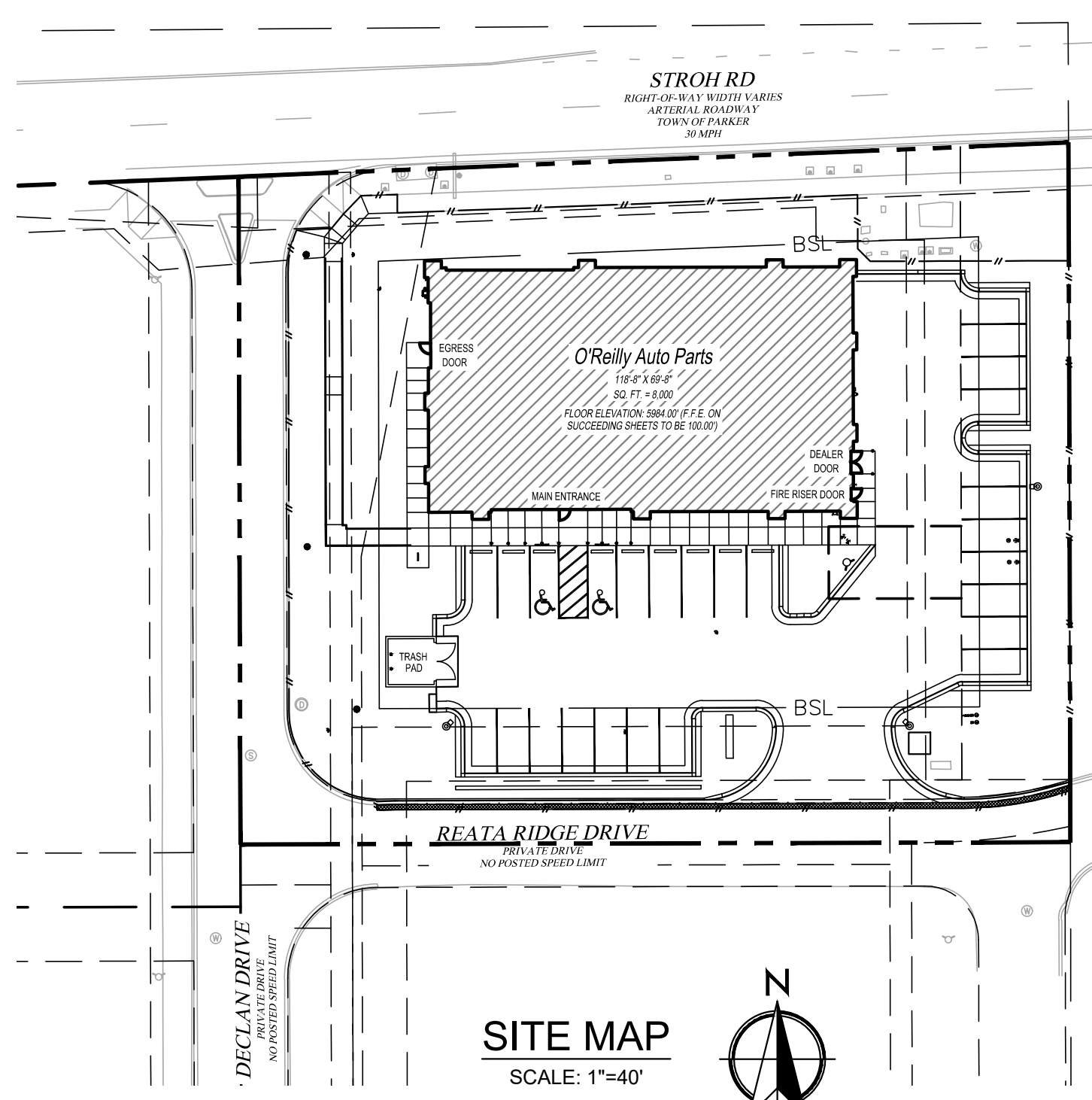
BEARINGS SHOWN HEREON ARE BASED ON THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST OF THE 6TH P.M., MONUMENTED AS SHOWN ON THIS SURVEY PLAT AS BEARING NORTH 89°33'14" EAST, WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO.

BENCHMARK

DOUGLAS COUNTY BENCHMARK 1.060032, EL=5,903.17 FEET (NAV88) - 3 1/4" INCH ALUMINUM CAP (DOUGLAS COUNTY GIS SECONDARY CONTROL MONUMENT 1.060032) AT THE SOUTHEAST CORNER OF THE PARKING LOT OF STROH RANCH SOCCER FIELDS, ON THE NORTH SIDE OF STROH RD, EAST OF THE INTERSECTION OF STROH ROAD AND N. MOTSENBOCKER ROAD, CITY OF PARKER, COLORADO.

UNAUTHORIZED CHANGES & USES

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SITE MAP
SCALE: 1"=40'

GENERAL NOTES

A. **CONTRACT ADMINISTRATION (LIMITED SERVICE):** THE ARCHITECT AND/OR ENGINEER OF RECORD INDICATED HEREIN ARE PROVIDING A LIMITED SERVICE AND NOT PROVIDING CONTRACT ADMINISTRATION SERVICES FOR THIS PROJECT WHICH IS THE RESPONSIBILITY OF OTHERS. THE ARCHITECT AND/OR ENGINEER ARE NOT RESPONSIBLE FOR FIELD ACTIVITIES ON THIS PROJECT WITHOUT DIRECT INSPECTION OF THE WORK IN PROGRESS. IF FIELD CONDITIONS ARE UNCOVERED THAT REQUIRE A CHANGE OR ADDITIONAL INFORMATION, THE ARCHITECT AND/OR ENGINEER DOES NOT DELEGATE AUTHORITY TO ANYONE ELSE FOR DETERMINING THE MEANING OF PLANS OR SPECIFICATIONS AS AUTHENTICATED HEREIN.

B. **QUALITY STANDARDS AND BUILDING CODES:** CONTRACTORS SHALL BE RESPONSIBLE FOR KNOWING THE QUALITY AND PUBLIC SAFETY REGULATIONS SET FORTH IN THE GOVERNING CODES AND OTHER APPLICABLE REGULATIONS OF LOCAL AND STATE AGENCIES HAVING JURISDICTION WHICH GOVERN EACH CONTRACTOR'S WORK.

C. **EXISTING CONDITIONS:** FIELD VERIFY EXISTING CONDITIONS BY DETAILED INSPECTION PRIOR TO SUBMITTING BID AND BEGINNING WORK. NOTIFY THE ARCHITECT AND/OR ENGINEER OF RECORD IF EXISTING CONDITIONS DEVIATE SUBSTANTIALLY FROM THOSE INDICATED HEREIN.

QUALITY CONTROL

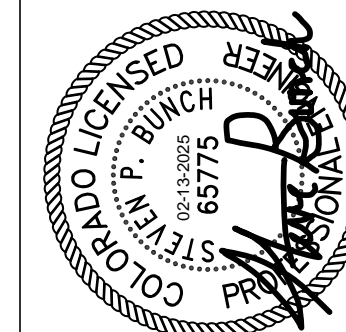
- REFER TO PROJECT MANUAL, SECTION 01 45 16 - QUALITY CONTROL PROCEDURES, FOR ADDITIONAL REQUIREMENTS.
- REFER TO STRUCTURAL DRAWINGS FOR SCHEDULE OF SPECIAL INSPECTIONS, IF INDICATED.
- WHERE REQUIRED, THE OWNER SHALL PROVIDE THE SERVICES OF AN INDEPENDENT TESTING LABORATORY TO PERFORM TESTING AND SPECIAL INSPECTIONS INDICATED.

CAUTION:
INFORMATION ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.



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PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROH RD
PARKER, CO #2

COVER SHEET

O'REILLY AUTO PARTS

CORPORATE OFFICES
233 SOUTH PATTERSON
SPRINGFIELD, MISSOURI 65802
(417) 862-2674 TELEPHONE

COMM # 4884
DATE: 02/14/2025
REVISION
DATE:

CT1.1

TIMOTHY M. GUILLOT
ARCHITECT
1736 East Sunshine, Suite 417
Springfield, Missouri 65804
417-862-0558
Fax: 417-862-3265
e-mail: architect@esterlyschneider.com

TOWN GENERAL NOTES

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- THE TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE TOWN INSPECTOR'S ABSENCE. HOWEVER, THE TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER DIRECTOR OF ENGINEERING/PUBLIC WORKS, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY USE PERMIT APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION." COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR. SUCH RESTORATION MAY BE REQUIRED TO BE COMPLETED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

Include current Town Storm Drainage Infrastructure notes provided in the SDECM on the Town's website (<http://parkeronline.org/577/Storm-Drainage-and-Environment-al-Criteria>)

TAIT Response: Added requested notes

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BASIS OF BEARING

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NOTE TO CONTRACTOR

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES, PIPES, AND/OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. THERE MAY BE EXISTING UTILITIES NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL ASCERTAIN THE TRUE VERTICAL AND HORIZONTAL LOCATION OF THOSE UNDERGROUND UTILITIES TO BE USED PRIOR TO CONSTRUCTION AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY PUBLIC OR PRIVATE UTILITIES, SHOWN OR NOT SHOWN HEREON.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORK

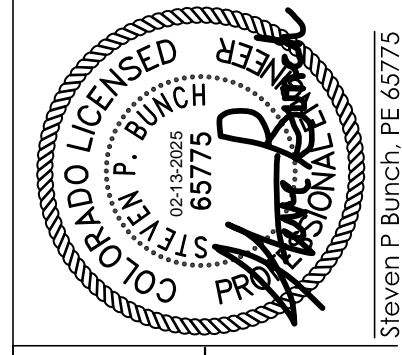
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Since 1914



Steven P. Bunch, PE 65715

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LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROH RD
PARKER, CO #2

GENERAL NOTES



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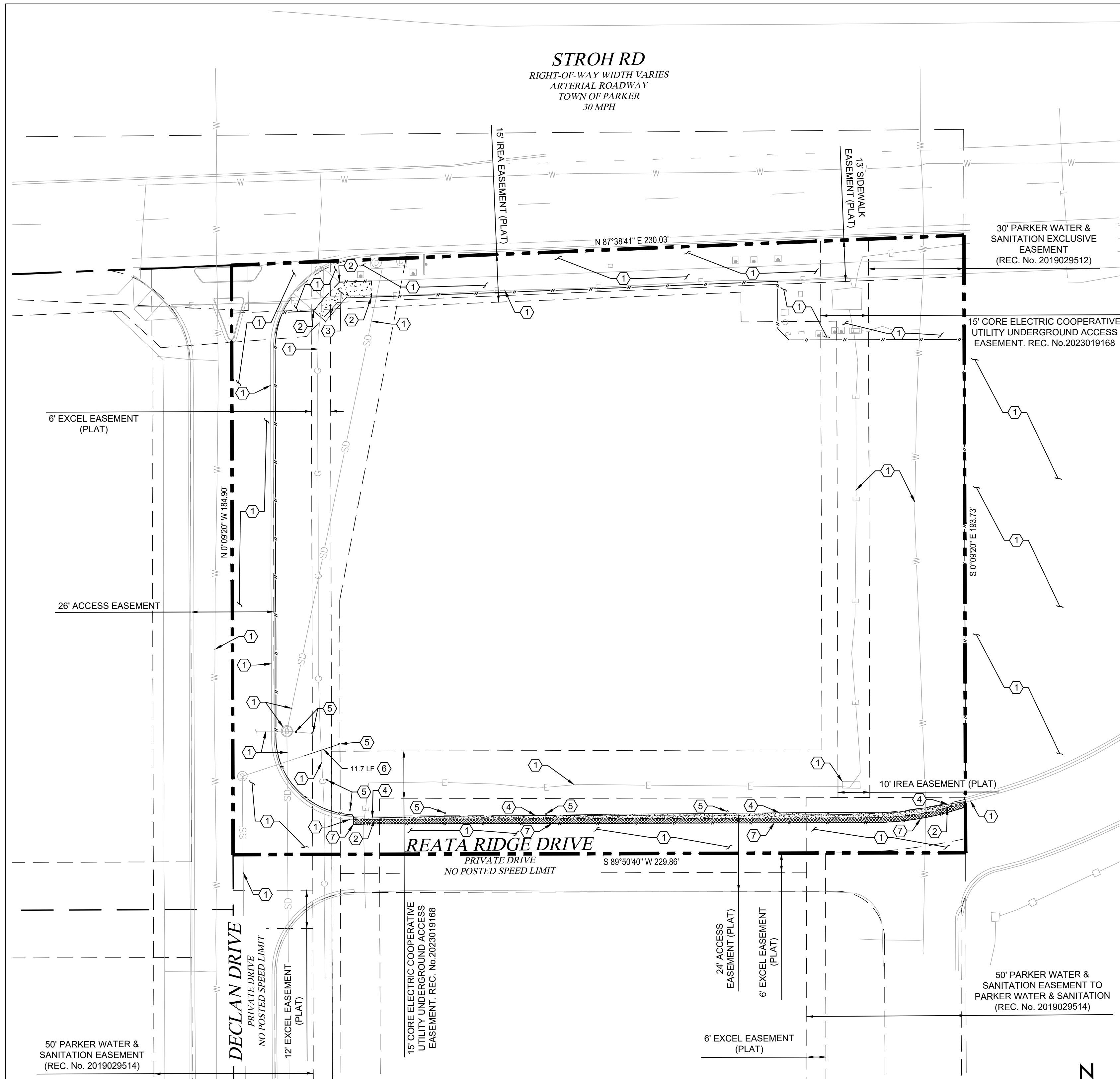
COMM # 4884

DATE: 02/14/2025

REVISION

DATE:

CT1.2



GENERAL NOTES

- (A) REFER TO PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS.
- (B) SITE CONDITIONS BASED UPON SURVEY PROVIDED BY OWNER. FIELD VERIFY EXISTING CONDITIONS BY DETAILED INSPECTION PRIOR TO SUBMITTING BID AND BEGINNING CONSTRUCTION. NOTIFY ARCHITECT IF EXISTING CONDITIONS DEVIATE SUBSTANTIALLY FROM THOSE INDICATED HEREIN.
- (C) REMOVE ALL EXISTING SITE CONSTRUCTION AND DELETERIOUS MATERIALS UNLESS OTHERWISE NOTED.
- (D) EXISTING CONSTRUCTION TO REMAIN TO BE PROTECTED AS REQUIRED. REPLACE AND/OR REPAIR EXISTING CONSTRUCTION DAMAGED DUE TO CONTRACTOR ACTIVITIES.
- (E) ALL DEMOLITION MATERIALS SHALL BECOME PROPERTY OF CONTRACTOR FOR PROPER DISPOSAL UNLESS OTHERWISE NOTED.
- (F) EXISTING UNDERGROUND INSTALLATION SUCH AS WATER MAINS, GAS MAINS, SEWERS, TELEPHONE LINES, POWER LINES, AND BURIED STRUCTURES IN THE VICINITY OF THE WORK ARE INDICATED ON THE DRAWINGS ONLY TO THE EXTENT SUCH INFORMATION HAS BEEN MADE AVAILABLE. THERE IS NO GUARANTEE AS TO THE ACCURACY OR COMPLETENESS OF SUCH INFORMATION AND ALL RESPONSIBILITY FOR THE ACCURACY AND COMPLETENESS IS EXPRESSLY DISCLAIMED. GENERALLY SERVICE CONNECTIONS ARE NOT INDICATED ON THE DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE FOR DISCOVERY OF EXISTING UNDERGROUND INSTALLATIONS IN ADVANCE OF EXCAVATING OR TRENCHING BY CONTRACTING LOCAL UTILITIES.

SYMBOLS LEGEND

- NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.
- RIGHT-OF-WAY
 - O'REILLY PROPERTY LINE
 - ADJACENT PROPERTY LINE
 - EXISTING EASEMENT
 - PROPOSED EASEMENT
 - ROAD CENTERLINE
 - LIMIT OF DISTURBANCE
 - PROPOSED SAWCUT
 - IMPROVEMENTS TO BE REMOVED
 - IMPROVEMENTS TO REMAIN
 - AREA OF ASPHALT REMOVAL
 - AREA OF CONCRETE REMOVAL
 - AREA OF MILL & OVERLAY

KEY NOTES

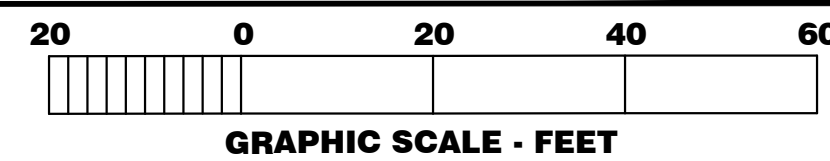
- (1) PROTECT EXISTING IMPROVEMENTS IN PLACE.
- (2) SAWCUT EXISTING PAVEMENT. SIDEWALK SAWCUT SHALL BE MADE AT THE NEAREST JOINT.
- (3) REMOVE EXISTING CONCRETE SIDEWALK.
- (4) REMOVE EXISTING ASPHALT PAVEMENT.
- (5) REMOVE EXISTING POST.
- (6) REMOVE EXISTING 6" PVC SANITARY SEWER SERVICE LINE AS SHOWN.
- (7) 1.5" WIDE, 2" DEEP MILL OF EXISTING ASPHALT PAVEMENT.

ENVIRONMENTAL GENERAL NOTES

- (A) AN ENVIRONMENTAL ANALYSIS HAS BEEN PERFORMED ON THE EXISTING SITE. REFER TO PROJECT MANUAL.
- (B) IF THIS PROJECT CONTAINS HAZARDOUS MATERIALS, CONTRACTOR TO PROVIDE WORK REQUIRED FOR PROPER REMOVAL, HANDLING, AND DISPOSAL PER ENVIRONMENTAL ANALYSIS RECOMMENDATIONS.

1 DEMOLITION PLAN

D1.1 SCALE: 1" = 20'-0"



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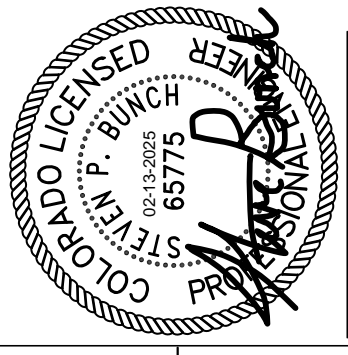
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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORK

DATE

PREPARED UNDER THE SUPERVISION OF TAIT & ASSOCIATES, INC.



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Santa Ana Sacramento Denver
San Luis Obispo Riverside Boise

LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROTH RD
PARKER, CO #2
DEMOLITION PLAN

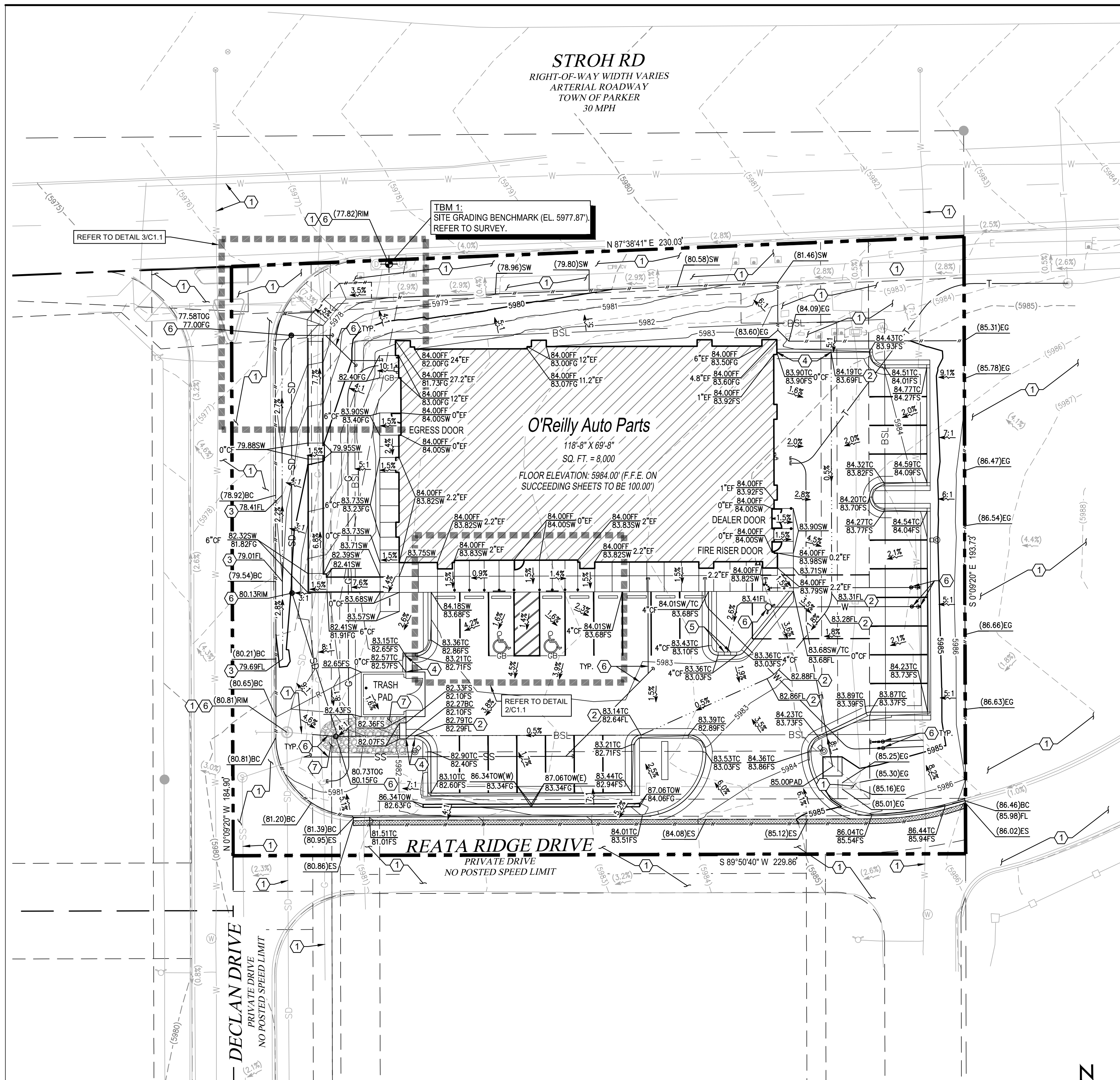


COMM # 4884
DATE: 02/14/2025
REVISION
DATE:

D1.1



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1 SITE GRADING PLAN
C1.1 SCALE: 1" = 20'-0"

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BENCHMARK
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- KEY NOTES**
- PROTECT IN PLACE.
 - SWALE IN PAVEMENT.
 - SWALE IN LANDSCAPING.
 - TRANSITION TO 0" CURB OVER DISTANCE AS SHOWN.
 - 2" CURB CUT PER DETAIL 9/C2.2.
 - REFER TO SHEET C3.1 FOR STORM AND UTILITY IMPROVEMENT DETAILS.
 - INSTALL 18" DEEP D50 = 6" RIPRAP PER DETAIL 10/2.2.

- GENERAL NOTES**
- REFER TO PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS.
 - SITE CONDITIONS BASED UPON SURVEY PROVIDED BY OWNER. FIELD VERIFY EXISTING CONDITIONS BY DETAILED INSPECTION PRIOR TO SUBMITTING BID AND BEGINNING CONSTRUCTION. NOTIFY ENGINEER IF EXISTING CONDITIONS DEVIATE SUBSTANTIALLY FROM THOSE INDICATED HEREIN.
 - FIELD VERIFY SANITARY SEWER SERVICE CONNECTION INVERT PRIOR TO ESTABLISHING FINAL FLOOR ELEVATION. NOTIFY ENGINEER IF EXISTING CONDITIONS REQUIRE REVISED FINISH FLOOR ELEVATION TO PROVIDE PROPER DRAINAGE.
 - ACCESSIBLE PARKING AREAS TO SLOPE 2% MAXIMUM IN ALL DIRECTIONS. DESIGNATED ACCESSIBLE ROUTE TO SLOPE 5% MAXIMUM IN DIRECTION OF TRAVEL WITH 2% MAXIMUM CROSS SLOPE.

- SITE EXCAVATION REQUIREMENTS**
- A GEOTECHNICAL ANALYSIS HAS BEEN PERFORMED ON THIS SITE. REFER TO PROJECT MANUAL.
 - FOLLOW GEOTECHNICAL ANALYSIS RECOMMENDATIONS FOR SITE EXCAVATION REQUIREMENTS.
 - REFER TO STRUCTURAL DRAWINGS FOR BUILDING EXCAVATION REQUIREMENTS.

- ADA NOTE**
- ALL ACCESSIBLE PATHS OF TRAVEL ARE TO ABIDE BY THE FOLLOWING PARAMETERS:
- CROSS-SLOPE NOT TO EXCEED 2%
 - LONGITUDINAL SLOPE NOT TO EXCEED 5%
 - LONGITUDINAL RAMP SLOPE NOT TO EXCEED 8.33%
 - RAMP LANDINGS NOT TO EXCEED 2% (LANDING PLACEMENT PER RAMP DETAILS)
 - ELEVATION DIFFERENCE BETWEEN RAMP LANDINGS SHALL NOT EXCEED 30 INCHES.
 - LOWER RAIL SHALL BE PROVIDED ON RAMP RAILING THAT WILL PREVENT AN OBJECT 4 INCHES AND LARGER FROM PASSING UNDER RAILING.
 - PATHWAYS EXCEEDING THESE STANDARDS WILL BE REPLACED AT THE CONTRACTORS EXPENSE

SYMBOL LEGEND

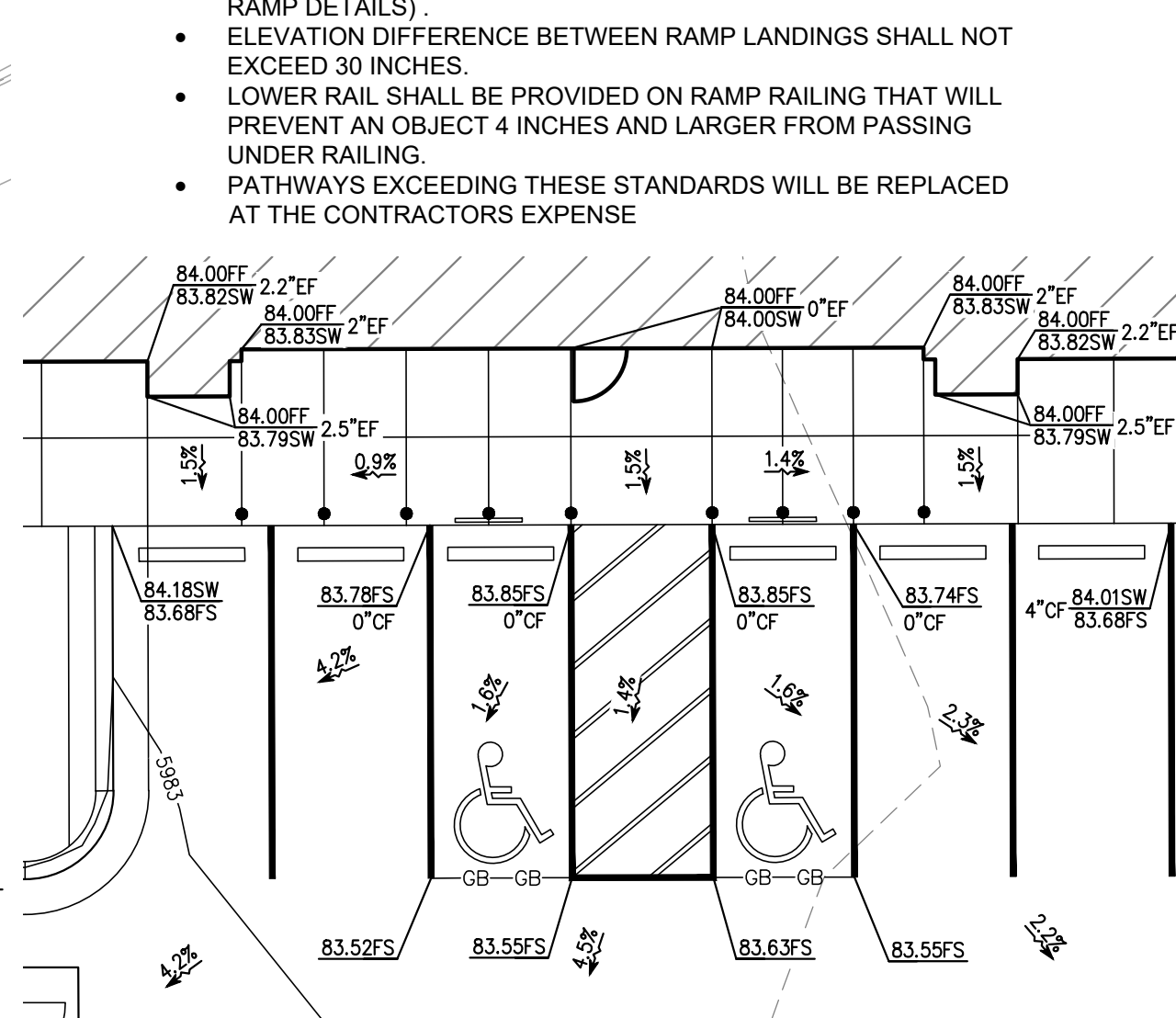
NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.

(---)	RIGHT-OF-WAY
(---)	O'REILLY PROPERTY LINE
(---)	ADJACENT PROPERTY LINE
(---)	EXISTING EASEMENT
(---)	PROPOSED EASEMENT
(---)	BSL - BUILDING SETBACK LINE
(---)	ROAD CENTERLINE
(---)	LIMIT OF DISTURBANCE
(---)	PROPOSED SAWCUT
(---)	PROPOSED WATER LINE
(---)	PROPOSED SEWER LINE
(---)	PROPOSED ELECTRIC LINE
(---)	PROPOSED GAS LINE
(---)	PROPOSED TELEPHONE LINE
(---)	PROPOSED RIDGE
(---)	PROPOSED GRADE BREAK
(---)	PROPOSED FLOWLINE
(---)	PROPOSED SIGN
(---)	PROPOSED WHEEL STOP
(---)	PROPOSED BOLLARD
(---)	PROPOSED FDC
(---)	PROPOSED FIRE HYDRANT
(---)	PROPOSED GATE VALVE
(---)	PROPOSED THRUST BLOCK
(---)	PROPOSED CLEANOUT
(---)	PROPOSED LIGHT POLE
(---)	EXISTING ELEVATION
(---)	PROPOSED ELEVATION
(---)	EXISTING CONTOURS
(---)	PROPOSED CONTOURS
(---)	EXISTING SLOPE
(---)	PROPOSED SLOPE
(---)	PROPOSED RIPRAP

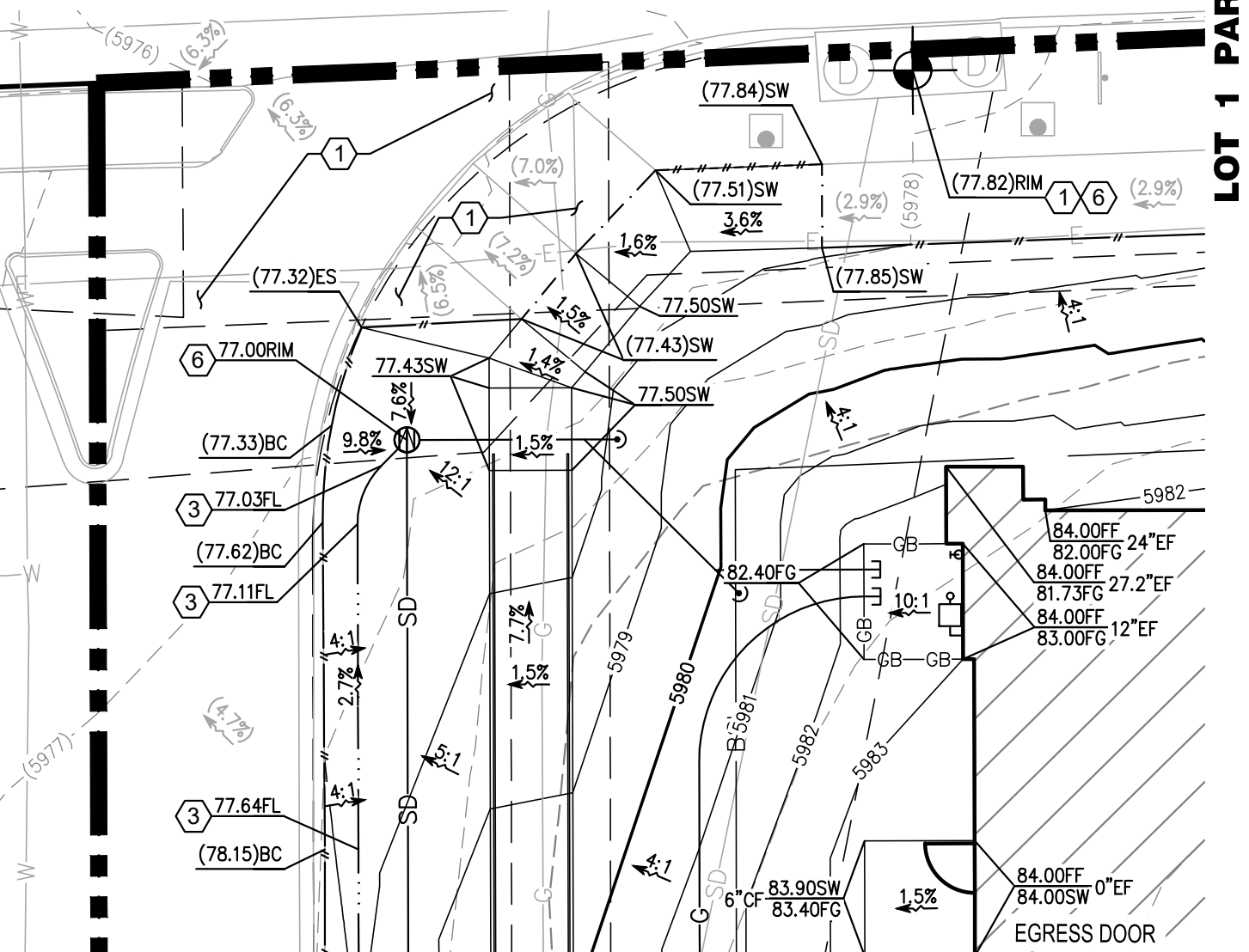
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2 ACCESSIBLE PARKING DETAIL
C1.1 SCALE: 1" = 10'-0"



3 GRADING DETAIL
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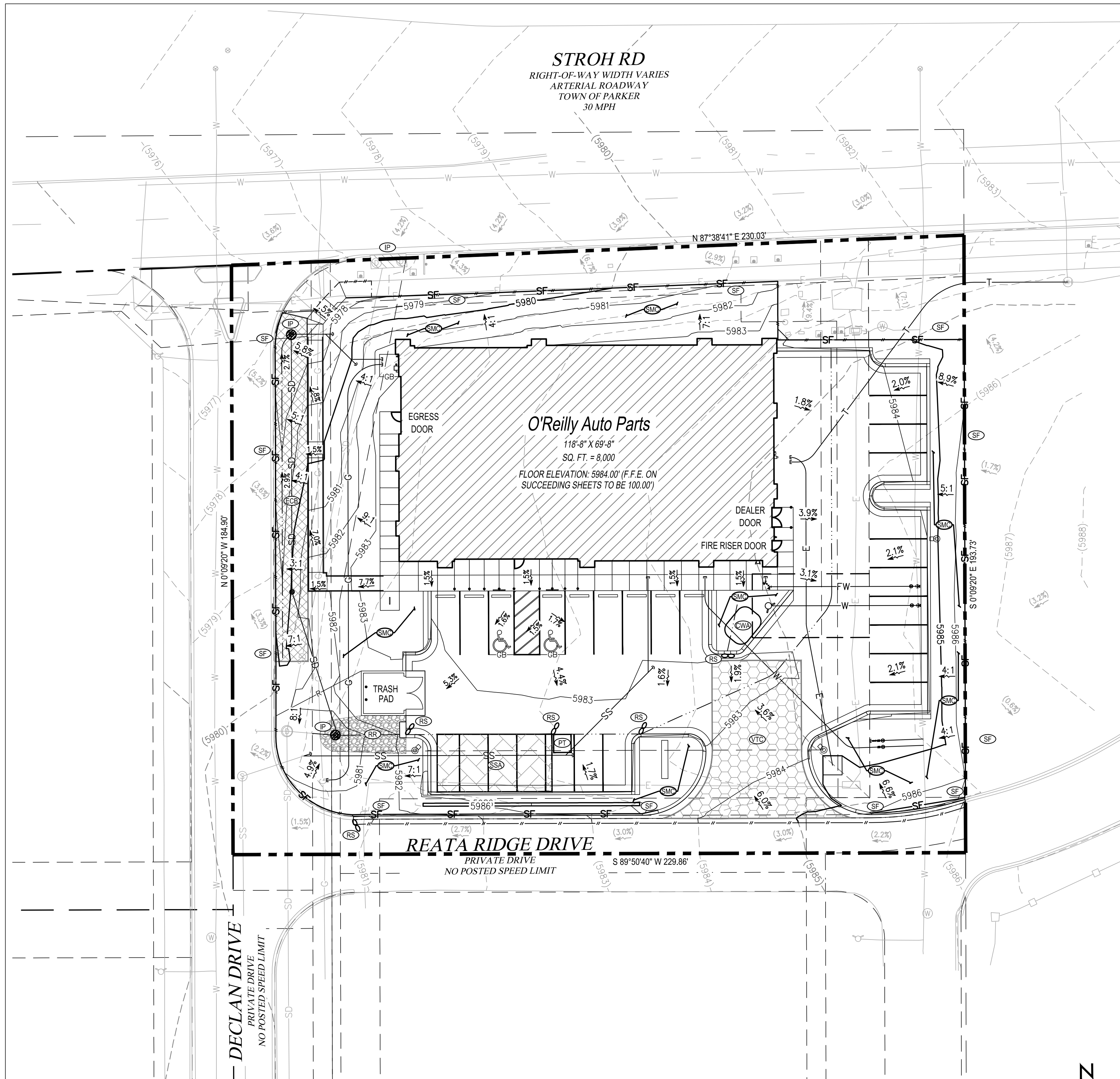
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PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROTH RD
PARKER, CO #2
SITE GRADING PLAN

O'Reilly AUTO PARTS
 CORPORATE OFFICES
 233 SOUTH PATTERSON
 SPRINGFIELD, MISSOURI 65802
 (417) 862-2674 TELEPHONE

COMM # 4884
 DATE: 02/14/2025
 REVISION
 DATE:



1 SITE EROSION CONTROL PLAN
C1.2 SCALE: 1" = 20'-0"



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GENERAL PHASING NOTES

- PHASE 1 (PRE-CONSTRUCTION)**
- INSTALLATION OF EROSION CONTROL SILT FENCE.
 - DEMOLITION OF EXISTING SITE IMPROVEMENTS.
 - REMOVAL OF EXISTING PAVEMENT ON THE SITE.
 - SILT FENCE
 - CONSTRUCTION VEHICLE TRACKING CONTROL
 - STABILIZED STAGING AREA
 - SANITARY FACILITIES
 - ROCK SOCKS
- PHASE 2 (DURING GRADING, AND PAVING)**
- INSTALLATION OF ALL STORM WATER DRAINAGE IMPROVEMENTS.
 - ROUGH GRADING.
 - CONSTRUCTION OF NEW SITE IMPROVEMENTS.
 - FINAL GRADING.
 - CONCRETE WASHOUT AREA
 - PRIOR TO ANY CONCRETE WORK
 - SEEDING AND MULCHING
 - STOCKPILES OVER 7-DAYS IDLE
 - INLET PROTECTION FOR PROPOSED ONCE INSTALLED
- PHASE 3 (POST PAVING)**
- PLACEMENT OF FINAL LANDSCAPING ITEMS AND SOD.
 - REMOVAL OF EROSION CONTROL FENCE.
 - FINAL LANDSCAPE INSTALLATION PER LANDSCAPE PLANS
 - REMOVE REMAINING EROSION CONTROL BMP'S ONCE STABILIZED

SITE EXCAVATION REQUIREMENTS

- CONSTRUCTION EGRESS SHALL BE 25' MINIMUM WIDTH AND 30' MINIMUM LENGTH AND 6" MIN THICKNESS OF STONE (1.5"-3.5") COARSE AGGREGATE WITH GEOTEXTILE UNDERLINER. REFER TO CITY OR STATE FOR ANY ADDITIONAL REQUIREMENTS.
- THE EXIT SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH 1.5-3.5 INCH STONE, AS CONDITIONS DEMAND, AND REPAIR AND/OR CLEAN OUT OF ANY STRUCTURE USED TO TRAP SEDIMENT. ALL MATERIALS SPILLED, DROPPED, WASHED, OR TRACKED FROM VEHICLES OR SITE ONTO ROADWAYS OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY.
- WHEELS MUST BE CLEANED TO REMOVE MUD PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA WITH CRUSHED STONE WHICH DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN LOCATED PER CITY OR STATE REQUIREMENTS.

EROSION CONTROL LEGEND

TITLE	KEY	SYMBOL
CONCRETE WASHOUT AREA	WTA	
PORTABLE TOILET	PT	
VEHICLE TRACKING CONTROL	VTG	
ROCK SOCK	RS	
STABILIZED STAGING AREA	SSA	
SILT FENCE	SF	
RIPRAP	RR	
DRAIN INLET PROTECTION	IF	
SEEDING, MULCHING, AND CRIMPING	SMC	
EROSION CONTROL BLANKET ON SLOPES GREATER THAN 3:1 OR WHERE SPECIFIED ON PLAN	ECB	

CONSTRUCTION EGRESS NOTES

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- (B) FOLLOW GEOTECHNICAL ANALYSIS RECOMMENDATIONS FOR SITE EXCAVATION REQUIREMENTS.
- (C) REFER TO STRUCTURAL DRAWINGS FOR BUILDING EXCAVATION REQUIREMENTS.

EROSION CONTROL & MAINTENANCE PLAN NOTES

- RETAIN FLOATABLE WIND BLOWN MATERIALS ON SITE BY STORING ALL TRASH AND BUILDING MATERIAL WASTE IN ENCLOSURES UNTIL PROPER DISPOSAL AT OFF-SITE FACILITIES. CHECK ADJACENT AREAS DAILY AND PICK UP CONSTRUCTION WASTE MATERIALS AND DEBRIS THAT HAVE BLOWN OR WASHED OFF SITE.
- PERMANENTLY STABILIZE ALL SURFACE AREA WITHIN AND ADJACENT TO THIS SITE THAT IS DISTURBED BY VEHICLES, GRADING AND OTHER CONSTRUCTION FOR THE PROPOSED FACILITY. STABILIZATION IS OBTAINED WHEN THE DISTURBED SURFACE IS COVERED WITH STRUCTURES, PAVING AND OR PERENNIAL VEGETATION HAVING A UNIFORM COVERAGE DENSITY OF AT LEAST 70%. STABILIZATION OF ALL DISTURBED AREA IS REQUIRED BEFORE TERMINATING MAINTENANCE AND REMOVAL OF EROSION CONTROL MEASURES.
- CONTRACTORS SHALL INSPECT POLLUTION CONTROL MEASURES AT LEAST ONCE EVERY 14 DAYS AND WITHIN 24 HOURS AFTER A STORM EVENT OF 1/2 INCH OR GREATER. DAMAGED MEASURES THAT PROVE TO BE INEFFECTIVE SHALL BE REPLACED WITH MORE EFFECTIVE MEASURES OR ADDITIONAL MEASURES WITHIN SEVEN DAYS. REPEATED FAILURE OF A CONTROL MEASURE REQUIRES INSTALLATION OF A MORE SUITABLE DEVICE TO PREVENT DISCHARGE OF POLLUTANTS FROM THE CONSTRUCTION SITE.
- INSTALLATION OF ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY THE CITY CODE. CONTRACTOR TO VERIFY REQUIREMENTS PRIOR TO BEGINNING ANY WORK ON PROJECT SITE.
- CARE SHALL BE TAKEN TO MINIMIZE THE ENCROACHMENT OF SEDIMENT INTO ALL STORM DRAIN APPURTENANCES, PUBLIC STREETS, AND ONTO PRIVATE PROPERTY UNTIL IMPERVIOUS MATERIAL (ROAD/PARKING AREA SURFACE) IS APPLIED OR UNTIL PROPOSED LANDSCAPE HAS BEEN ESTABLISHED.
- REFER TO SITE DETAILS FOR SILT FENCE CONSTRUCTION.
- ALL GRASS SLOPES WHICH EXCEED 3:1 (H:V) SHALL UTILIZE CONTECH CONSTRUCTION PRODUCTS PERMANENT TURF REINFORCEMENT MATS C35 OR APPROVED EQUAL. MATS SHALL BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS AND STANDARDS. CONTRACTOR SHALL COORDINATE INSTALLATION INSPECTION WITH MANUFACTURER.

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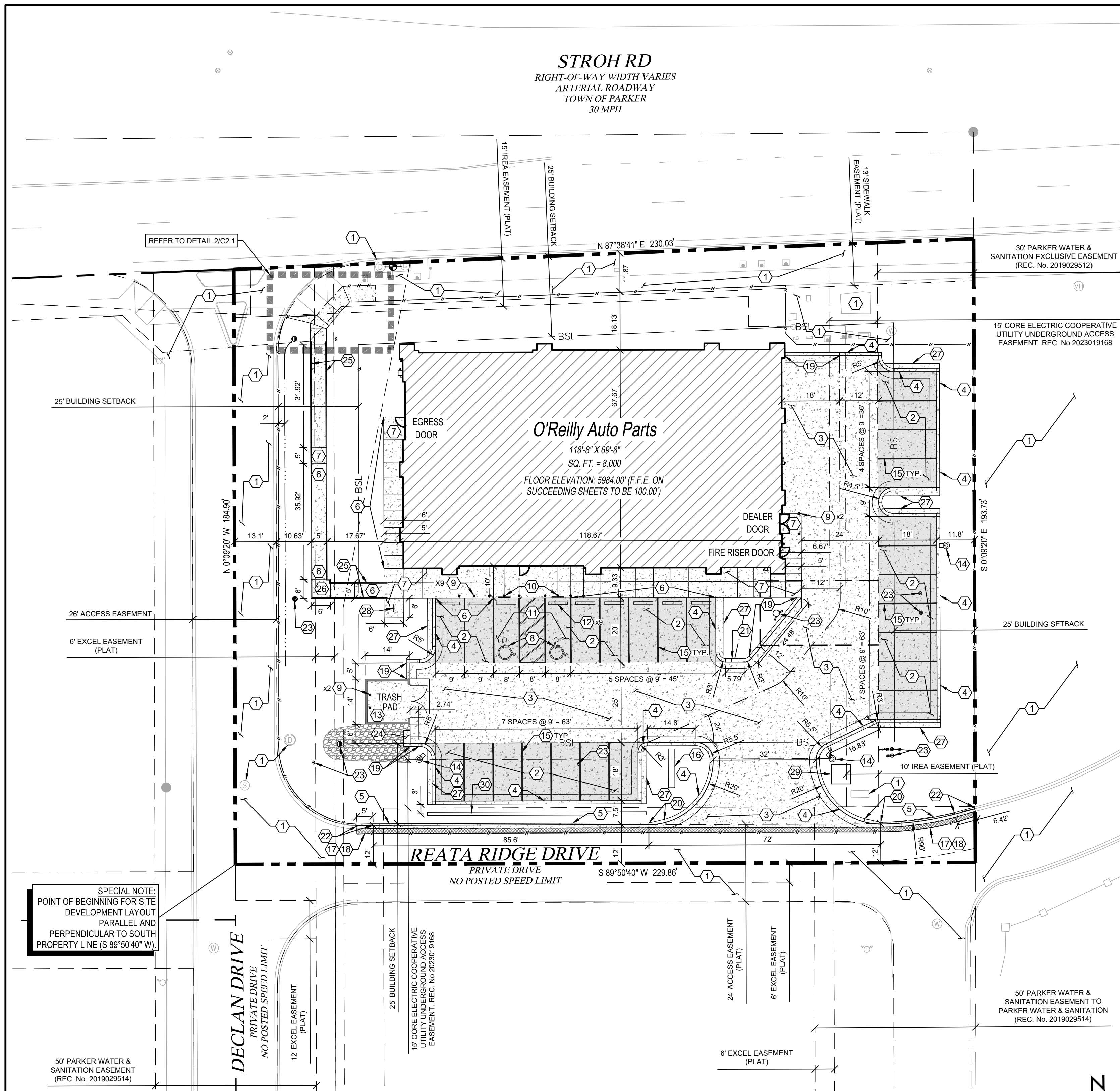
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PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROH RD
PARKER, CO #2

SITE EROSION CONTROL PLAN

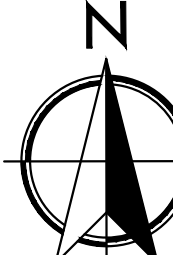
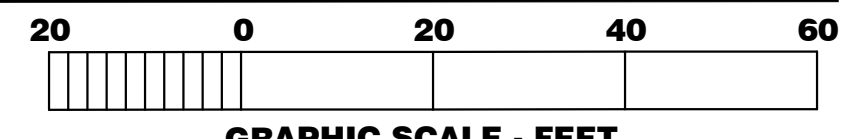
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 (417) 862-2674 TELEPHONE

COMM #	4884
DATE:	02/14/2025
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1 SITE DEVELOPMENT PLAN

C2.1 SCALE: 1" = 20'-0"



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**Know what's below
Call before you dig.**

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ZONING CODE

ZONING CLASSIFICATION:	MIXED-USE COMMUNITY
PROPERTY AREA:	43,516 SF (0.999 AC)
PROPOSED BUILDING AREA:	8,000 SF
PROPOSED PARKING PAVEMENT:	12,900 SF
PROPOSED SIDEWALK PAVEMENT:	2,250 SF
PARKING SUMMARY	
PARKING FORMULA:	1 SP. PER 300 SF GFA
SPACE SIZE:	9' x 20' AT BUILDING
	9' x 18' ALL OTHER LOCATIONS
SPACES REQUIRED:	27
SPACES PROVIDED:	27
H.C. SPACES PROVIDED:	2

KEY NOTES

- 1 PROTECT EXISTING IMPROVEMENTS IN PLACE.
- 2 CONCRETE PAVING (STANDARD DUTY) AT PARKING AREAS. REFER TO DETAIL 2/C2.2
- 3 CONCRETE PAVING (HEAVY DUTY) AT DRIVE AREAS, DRIVE APRONS, FIRE LANES, AND REFUSE AREA. REFER TO DETAIL 1/C2.2
- 4 INSTALL CONCRETE CURB PER DETAIL 3/C2.2.
- 5 INSTALL TOWN OF PARKER MEDIAN CURB AND GUTTER SECTION PER DETAIL 13 ON SHEET C2.2.
- 6 INSTALL CONCRETE SIDEWALK CURB PER DETAIL 4/C2.2. REFER TO STRUCTURAL DRAWINGS FOR LAYOUT OF CONTROL AND EXPANSION JOINTS ADJACENT TO BUILDING PERIMETER.
- 7 INSTALL CONCRETE SIDEWALK OR DOOR LANDING PER DETAIL 5/C2.2. REFER TO STRUCTURAL DRAWINGS FOR LAYOUT ADJACENT TO BUILDING PERIMETER.
- 8 INSTALL ACCESSIBLE PARKING SYMBOL PAVEMENT STRIPING PER DETAIL 6/C2.2.
- 9 INSTALL STEEL BOLLARD PER DETAIL 7/C2.2. PROVIDE (2) AT REFUSE CONTAINER AREA, (2) AT SECTIONAL OVERHEAD FREIGHT DOOR AND PROVIDE (9) AT FRONT ENTRY. REFER TO STRUCTURAL DRAWINGS FOR BOLLARDS ADJACENT TO BUILDING PERIMETER.
- 10 INSTALL ACCESSIBLE PARKING SIGN PER DETAIL 8/C2.2.
- 11 ACCESSIBLE PARKING SPACE WITH ACCESS AISLE (ADA COMPLIANT) TO SLOPE 2% MAXIMUM IN ALL DIRECTIONS. PROVIDE PAVEMENT MARKINGS TO MATCH STRIPING COLOR UNLESS OTHERWISE REQUIRED TO COMPLY WITH GOVERNING ACCESSIBILITY REGULATIONS.
- 12 INSTALL CONCRETE BUMPER BLOCK (8" W X 5" H X 6'-0" L) ANCHORED TO PAVING WITH (2) 1"-6" LONG #4 REBAR.
- 13 REFUSE ENCLOSURE. REFER TO 1/C2.3.
- 14 PARKING LOT LIGHTING SHOWN FOR REFERENCE ONLY. REFER TO SHEET US1 FOR MORE DETAIL.
- 15 INSTALL 4" WIDE PAVEMENT STRIPING AS SHOWN USING HIGHWAY MARKING PAINT - YELLOW (2 COATS).
- 16 SITE SIGN SHOWN FOR REFERENCE ONLY. REFER TO SHEET SG1.1 FOR CONSTRUCTION TYPE AND LOCATION. SIGN IS OWNER FURNISHED AND INSTALLED (REFER TO SCOPE OF WORK SCHEDULE). REFER TO SHEET US1 FOR ADDITIONAL REQUIREMENTS.
- 17 LIMITS OF NEW PAVING. MATCH EXISTING PAVEMENT TRANSITION ELEVATIONS. AT TRANSITIONS OF NEW CONCRETE TO EXISTING CONCRETE PAVEMENT, EPOXY DOWEL 6" MINIMUM INTO EXISTING WITH #4 X 1'-6" SMOOTH DOWELS MATCHING NEW PAVEMENT REINFORCEMENT SPACING. PROVIDE SEALANT AT JOINTS.
- 18 1.5" WIDE, 2" DEEP GRADE SX ASPHALT MILL & OVERLAY PER GEOTECHNICAL RECOMMENDATIONS.
- 19 TRANSITION TO 0" CURB OVER DISTANCE AS SHOWN.
- 20 5' TRANSITION FROM 6" WIDE CURB TO 1' WIDE CURB.
- 21 2' CURB CUT PER DETAIL 9/C2.2.
- 22 TRANSITION TO EXISTING 6" CURB AND GUTTER.
- 23 REFER TO SHEET C3.1 FOR STORM AND UTILITY IMPROVEMENT DETAILS.
- 24 INSTALL 5' X 2' CONCRETE SPLASH PAD RECESSED 2" MIN. FROM PARKING ACCESS AISLE PAVEMENT. REFER TO GRADING PLAN FOR ELEVATIONS.
- 25 INSTALL SIDEWALK RAMP RAILING PER DETAIL 14/C2.2.
- 26 5' X 5' CLEAR SPACE AT RAMP CHANGE IN DIRECTION.
- 27 INSTALL 1.5" WIDE, 4" THICK CONCRETE STRIP ATTACHED TO PARKING STALL BACK OF CURB.
- 28 INSTALL INVERTED U BIKE RACK. SEE DETAIL 11/C2.2.
- 29 INSTALL TRANSFORMER PAD PER CORE ELECTRIC STANDARDS.
- 30 INSTALL PARKING LOT SCREENING WALL. REFER TO STRUCTURAL FOR MORE DETAIL.

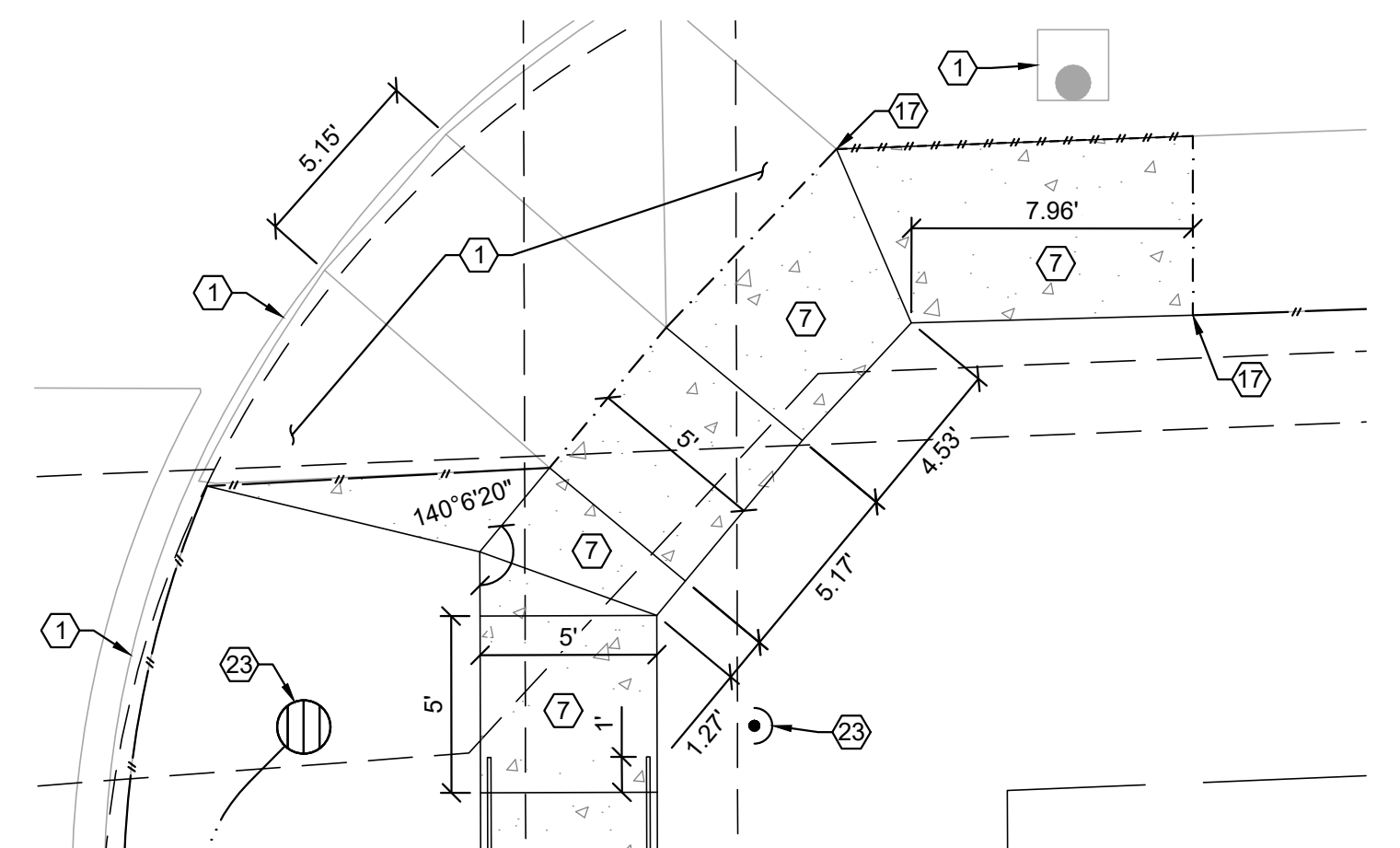
GENERAL NOTES

- A REFER TO PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS. ALL ON SITE CONCRETE TO BE 4,500 PSI.
- B SITE DIMENSIONS TO FACE OF CONCRETE FOUNDATION, SIDEWALK, CURB GUTTER LINE, PROPERTY LINE, OR CENTER LINE OF STRIPING UNLESS OTHERWISE NOTED.
- C COORDINATE WORK WITH OTHER SITE RELATED DEVELOPMENT DRAWINGS.
- D REFER TO STRUCTURAL DRAWINGS FOR BUILDING DIMENSIONS AND LAYOUT OF SIDEWALKS ADJACENT TO BUILDING PERIMETER.
- E PRIOR TO INSTALLATION, CONTRACTOR TO VERIFY LOCATIONS OF LIGHT POLES, LANDSCAPING, AND UTILITIES DO NOT CONFLICT WITH SITE SIGN LOCATION SHOWN. IF CONFLICT IS DISCOVERED, CONTRACTOR TO NOTIFY OWNERS REPRESENTATIVE PRIOR TO PROCEEDING WITH THE WORK.

SYMBOLS LEGEND

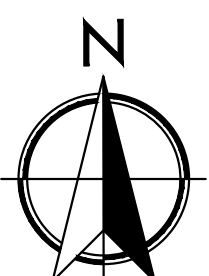
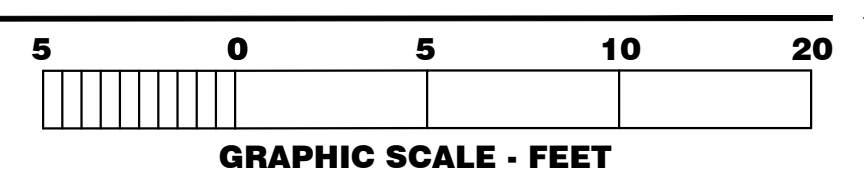
NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.

	RIGHT-OF-WAY
	O'REILLY PROPERTY LINE
	ADJACENT PROPERTY LINE
	EXISTING EASEMENT
	PROPOSED EASEMENT
	BSL - BUILDING SETBACK LINE
	ROAD CENTERLINE
	LIMIT OF DISTURBANCE
	PROPOSED SAWCUT
	PROPOSED FLOWLINE
	PROPOSED SIGN
	PROPOSED WHEEL STOP
	PROPOSED BOLLARD
	PROPOSED FDC
	PROPOSED FIRE HYDRANT
	PROPOSED GATE VALVE
	PROPOSED CLEANOUT
	PROPOSED LIGHT POLE
	PROPOSED BUILDING FOOTPRINT
	PROPOSED LIGHT DUTY CONCRETE
	PROPOSED HEAVY DUTY CONCRETE
	PROPOSED MILL & OVERLAY
	PROPOSED RIPRAP



2 CURB RAMP TIE-IN DETAIL

C2.1 SCALE: 1" = 5'-0"



NOTE TO CONTRACTOR

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES, PIPES, AND/OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. THERE MAY BE EXISTING UTILITIES NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL ASCERTAIN THE TRUE VERTICAL AND HORIZONTAL LOCATION OF THOSE UNDERGROUND UTILITIES TO BE USED PRIOR TO CONSTRUCTION AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY PUBLIC OR PRIVATE UTILITIES, SHOWN OR NOT SHOWN HEREON.

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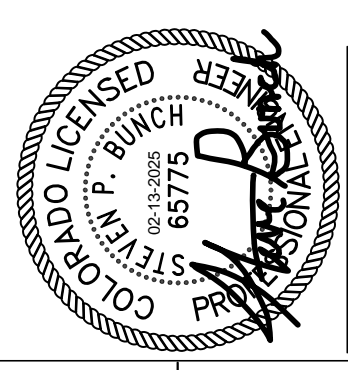
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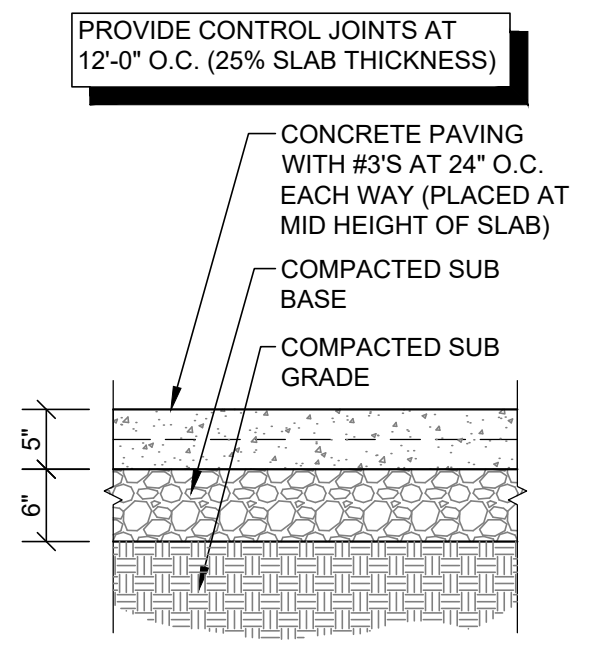
LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROTH RD
PARKER, CO #2
SITE DEVELOPMENT PLAN

O'Reilly AUTO PARTS

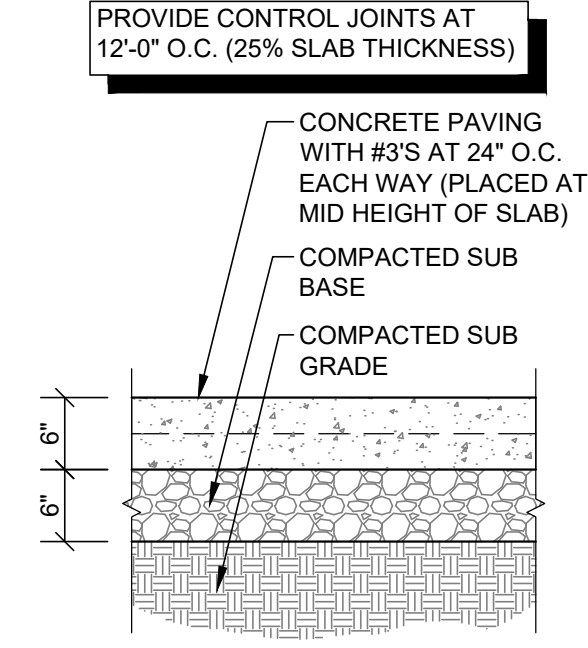
CORPORATE OFFICES
233 SOUTH PATTERSON
SPRINGFIELD, MISSOURI 65802
(417) 862-2674 TELEPHONE

COMM #	4884
DATE:	02/14/2025
REVISION	
DATE:	



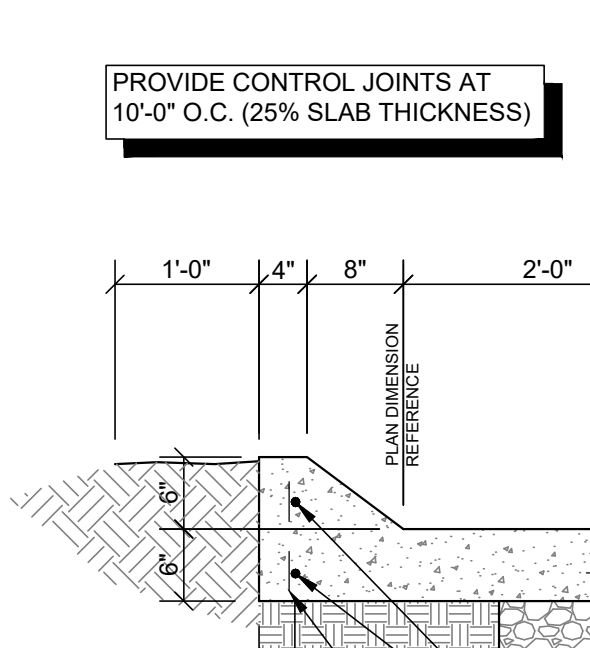
**PAVEMENT DESIGN IS BASED ON GEOTECHNICAL REPORT FROM GEG PROVIDED BY THE OWNER DATED 10/30/2024. CONTRACTOR IS RESPONSIBLE TO FULLY READ AND FOLLOW THE RECOMMENDATIONS OF THE PROJECT GEOTECHNICAL REPORT.

1 CONCRETE (STANDARD DUTY) PAVING SECTION
C2.2 SCALE: 3/4" = 1'-0"



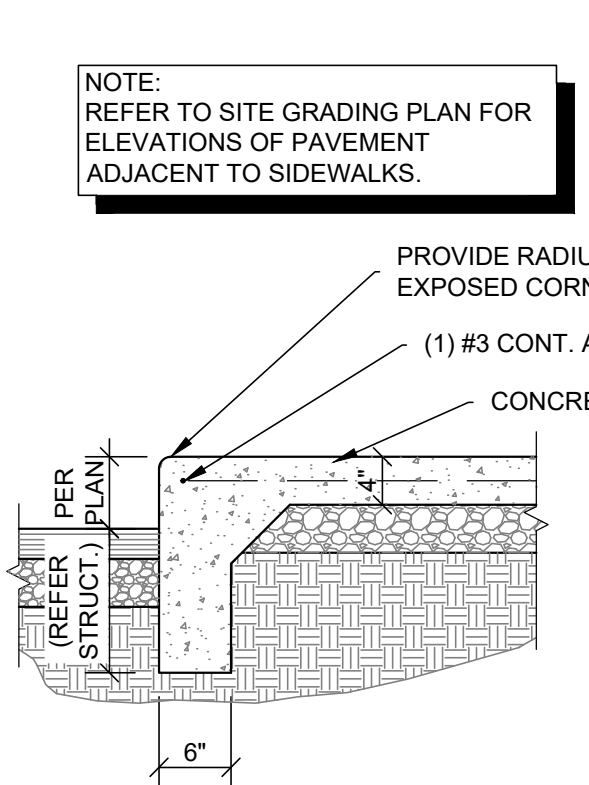
**PAVEMENT DESIGN IS BASED ON GEOTECHNICAL REPORT FROM GEG PROVIDED BY THE OWNER DATED 10/30/2024. CONTRACTOR IS RESPONSIBLE TO FULLY READ AND FOLLOW THE RECOMMENDATIONS OF THE PROJECT GEOTECHNICAL REPORT.

2 CONCRETE (HEAVY DUTY) PAVING SECTION
C2.2 SCALE: 3/4" = 1'-0"

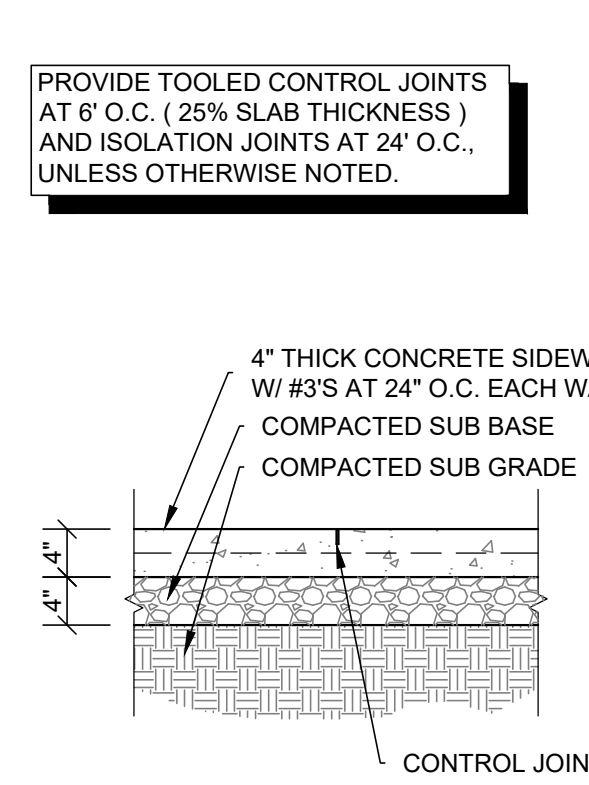


NOTE: CONTRACTOR'S OPTION TO POUR INTEGRAL (MONOLITHIC) CURB AND PAVING. WHERE PAVING POURED SEPARATE, PROVIDE KEY-WAY OR #4 X 1'-0" DOWELS AT 48" O.C.

3 CONCRETE CURB SECTION
C2.2 SCALE: 3/4" = 1'-0"

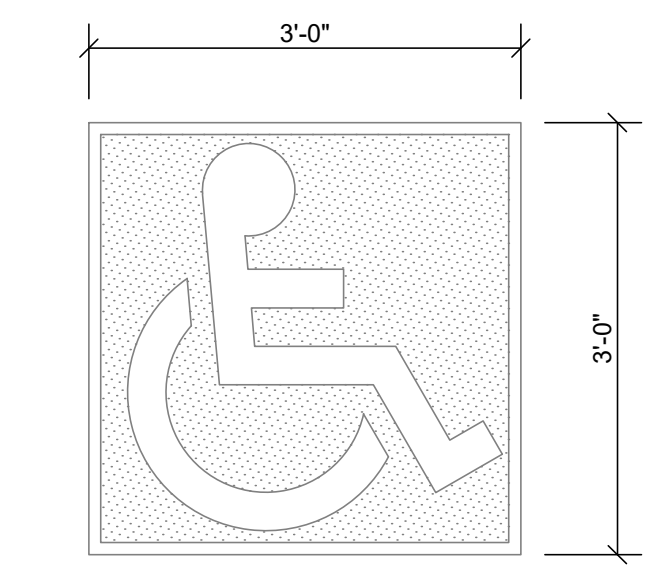


4 CONCRETE SIDEWALK CURB SECTION
C2.2 SCALE: 3/4" = 1'-0"

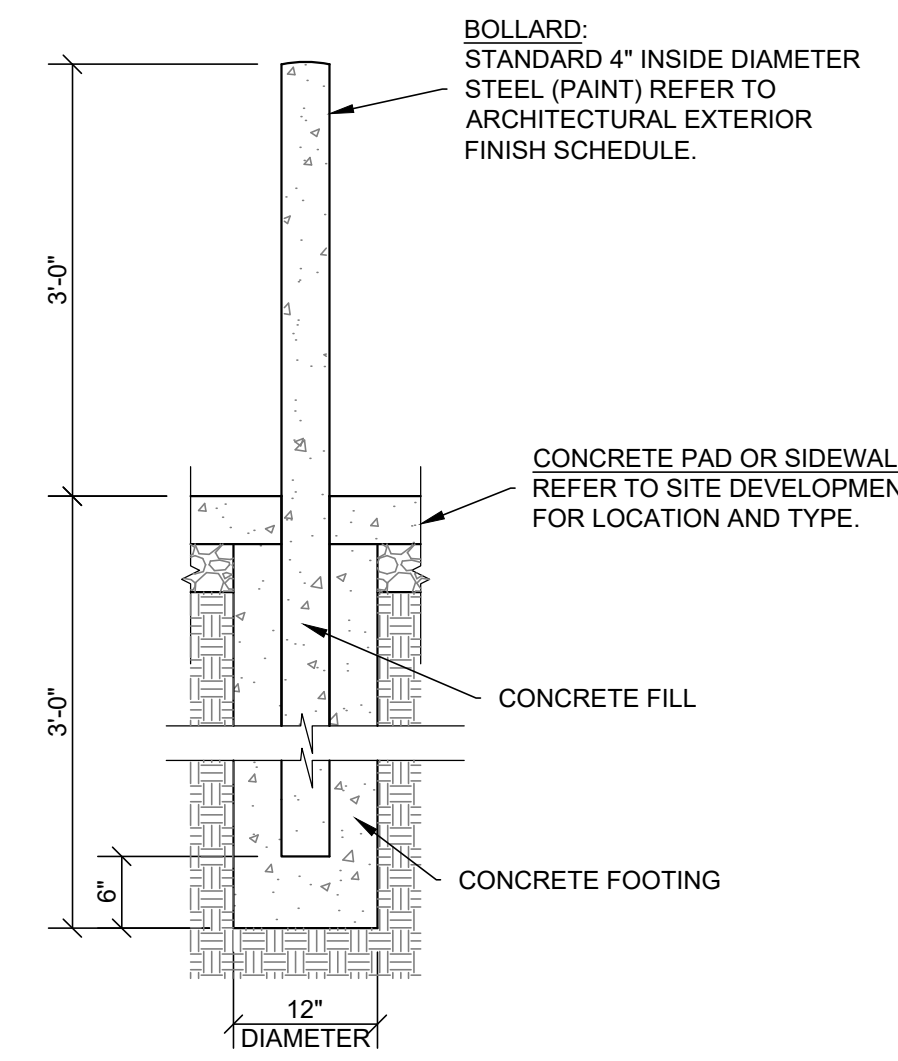


5 CONCRETE SIDEWALK SECTION
C2.2 SCALE: 3/4" = 1'-0"

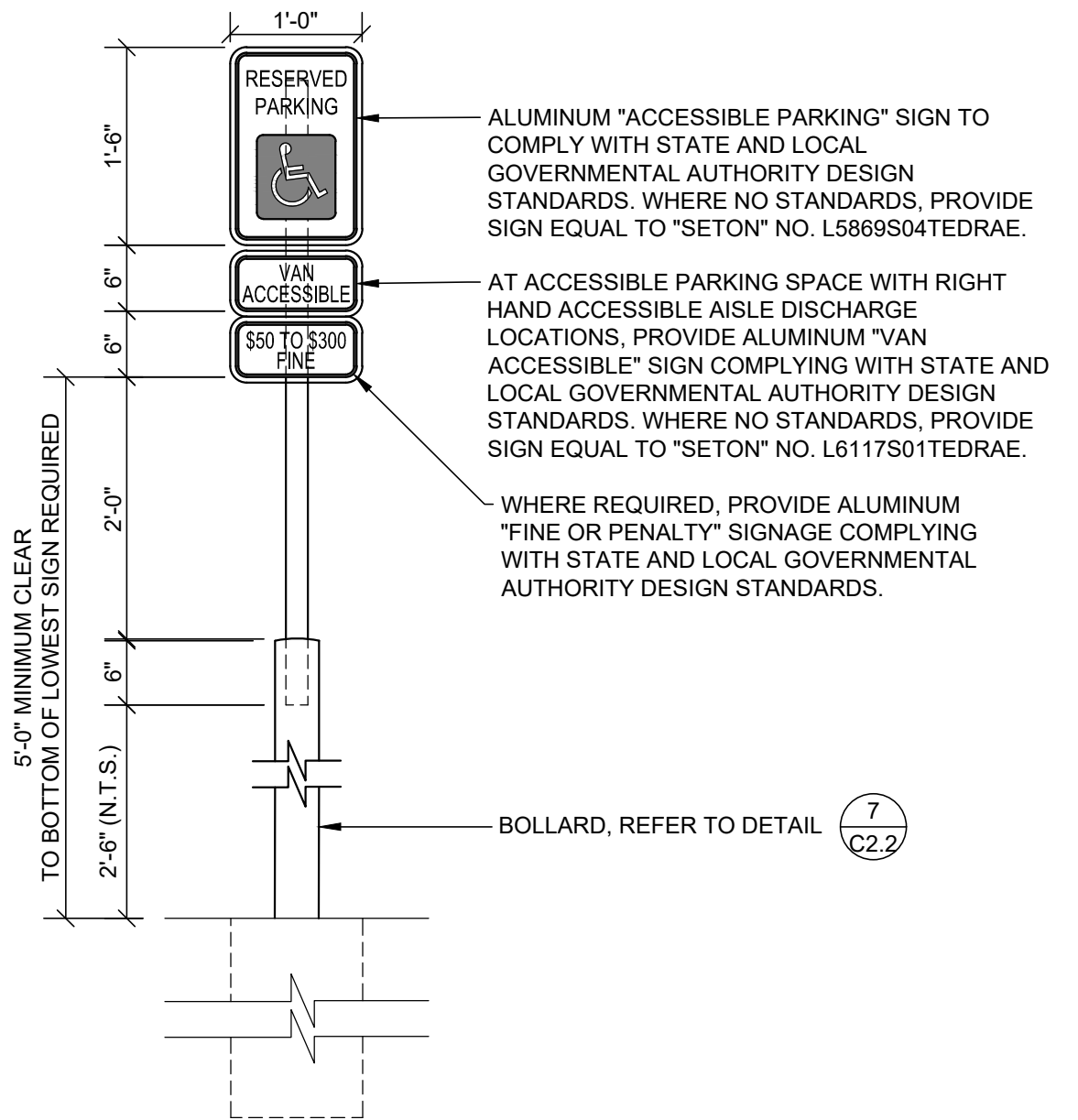
WHITE SYMBOL OF ACCESSIBILITY TO BE 36" HIGH BY 36" WIDE ON A BLUE BACKGROUND. (USE HIGHWAY GRADE MARKING PAINT)



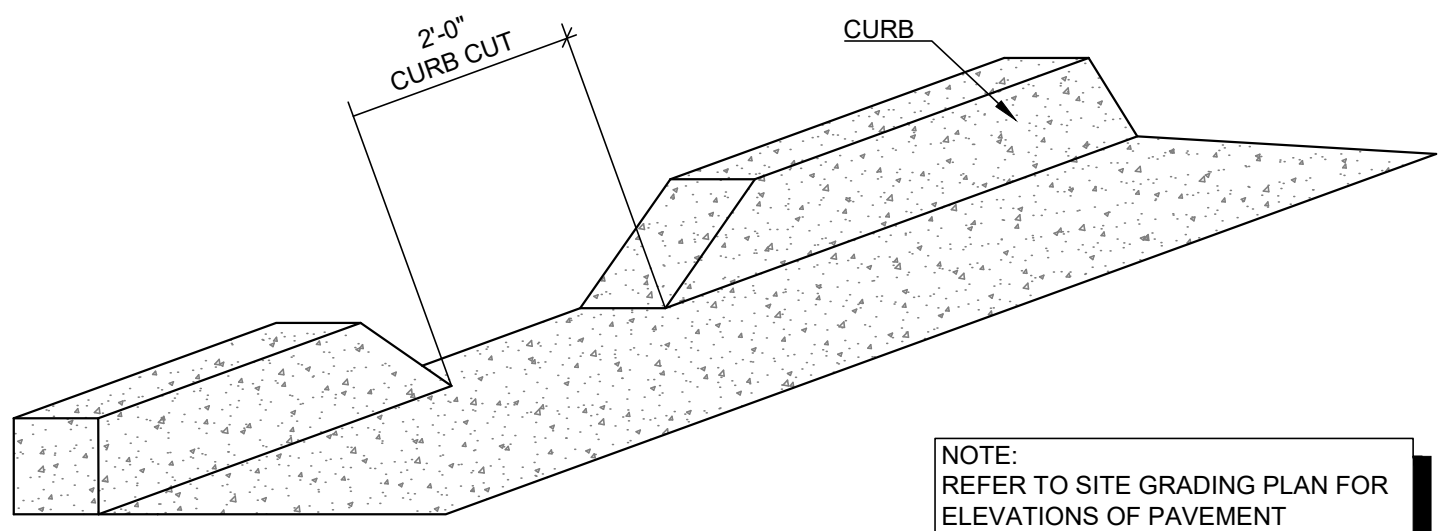
6 ACCESSIBLE PARKING PAVEMENT STRIPING DETAIL
C2.2 SCALE: 3/4" = 1'-0"



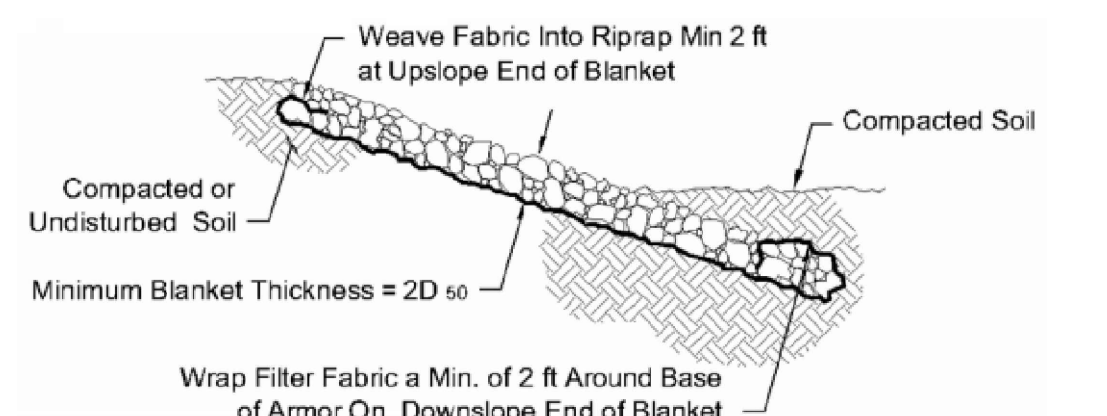
7 STEEL BOLLARD SECTION
C2.2 SCALE: 3/4" = 1'-0"



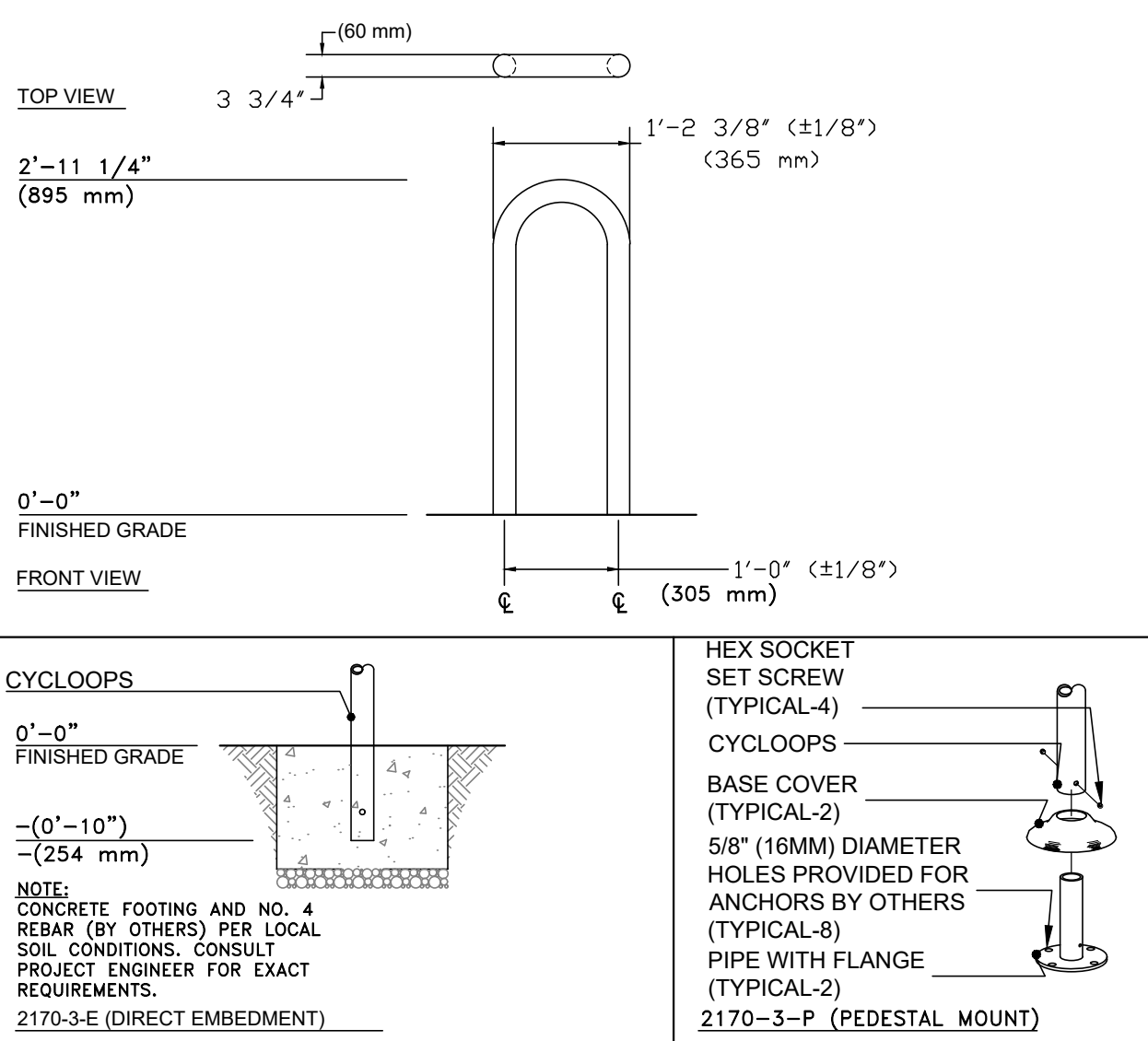
8 HANDICAP PARKING SIGN DETAIL
C2.2 SCALE: 3/4" = 1'-0"



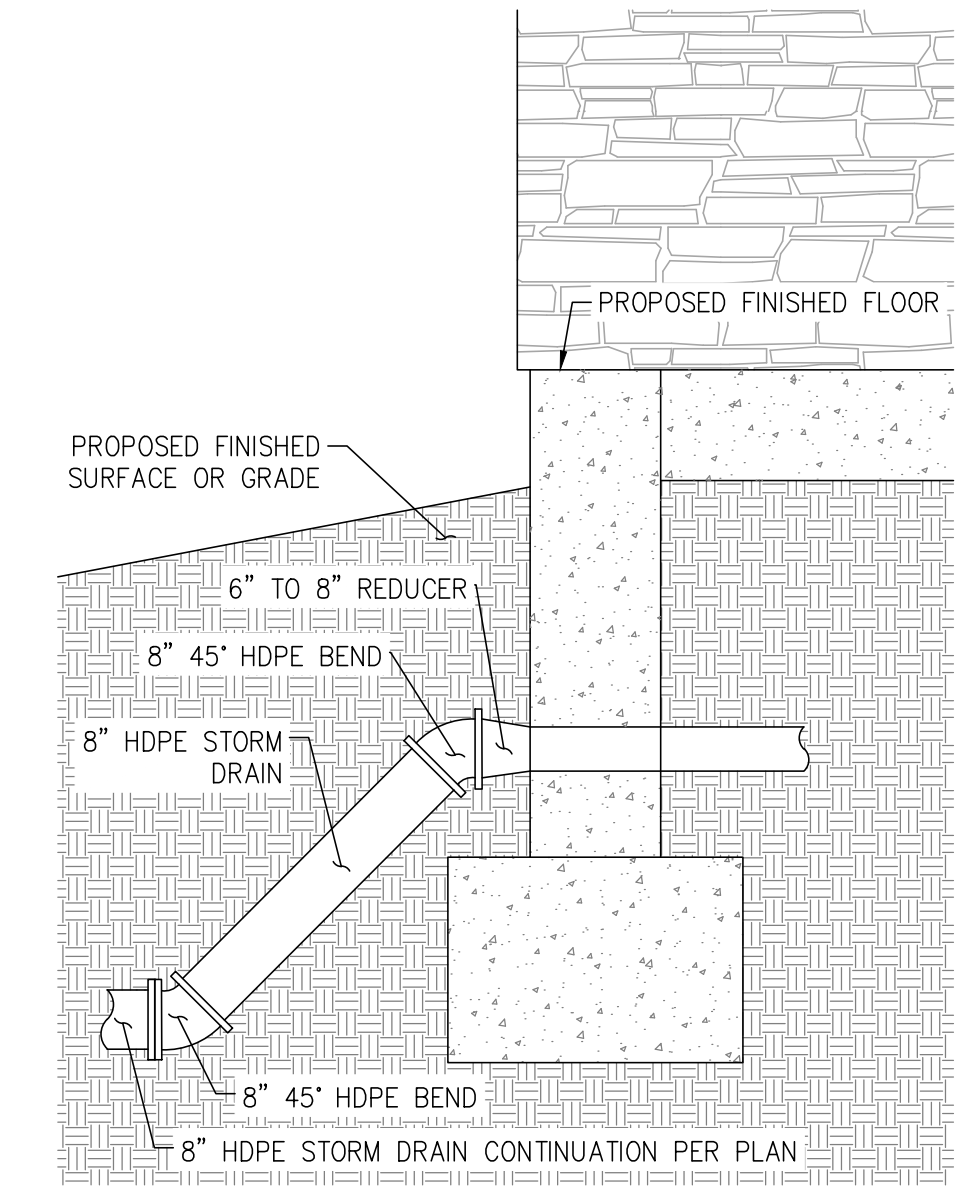
9 2' WIDE CURB CUT
C2.2 SCALE: NTS



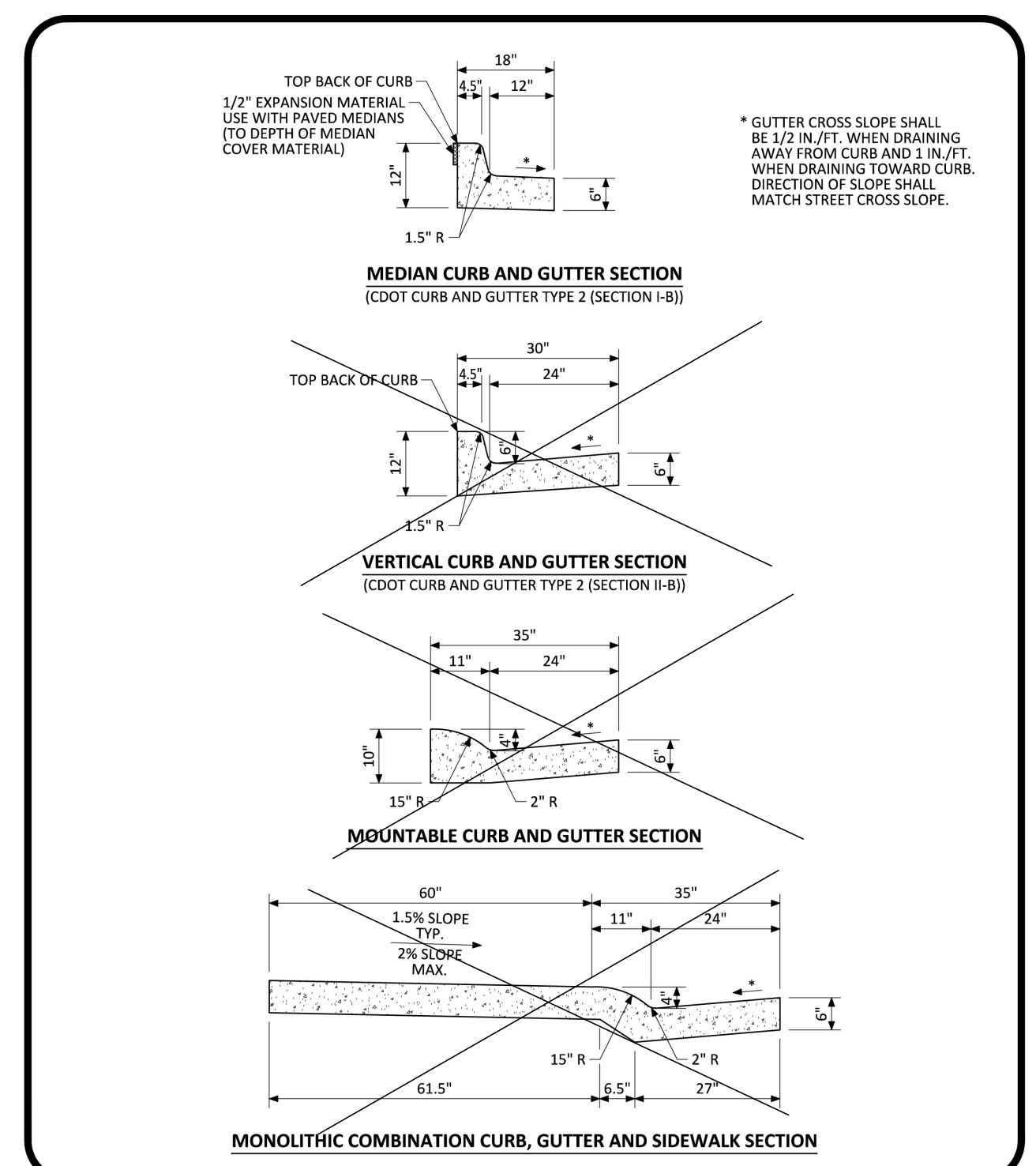
10 RIPRAP DETAIL
C2.2 SCALE: NTS



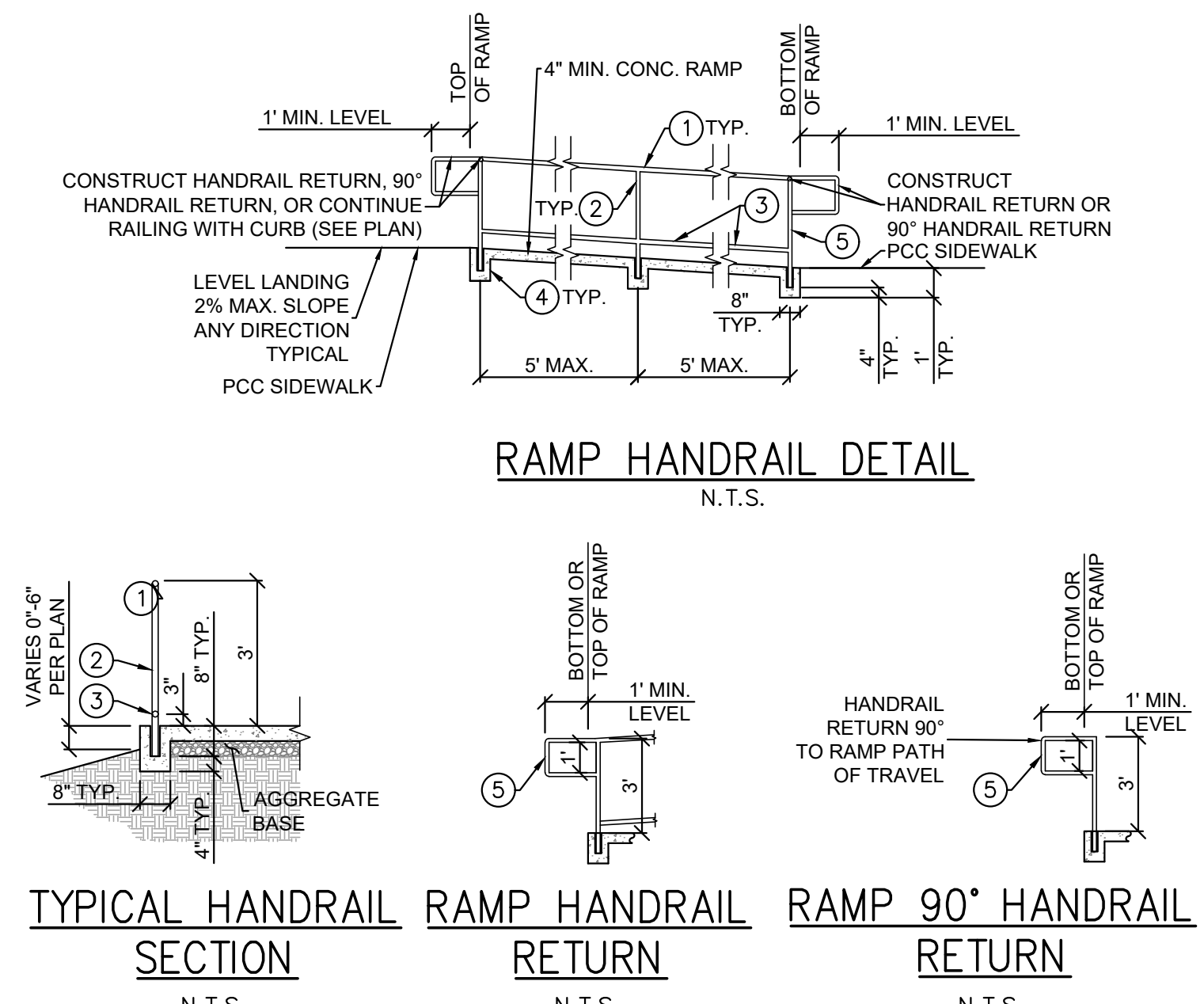
11 BICYCLE PARKING (INVERTED "U")
C2.2 SCALE: 3/4" = 1'-0"



12 DOWNSPOUT CONNECTION
C2.2 SCALE: NTS



13 PARKER MEDIAN CURB AND GUTTER
C2.2 SCALE: NTS



14 HANDRAIL DETAILS
C2.2 SCALE: NTS

NOTE:
1. ALL HANDRAILS, POSTS, AND APPURTENANCES TO BE PAINTED PER O'REILLY SPECIFICATIONS AND ARCHITECT RECOMMENDATIONS.
2. HANDRAIL GRIPPING SURFACES AND ANY SURFACES ADJACENT TO THEM SHALL BE FREE OF SHARP OR ABRASIVE ELEMENTS AND SHALL HAVE ROUNDED EDGES.
3. HANDRAILS SHALL NOT ROTATE WITHIN THEIR FITTINGS.

LEGEND:
① 1 1/2" O.D. STEEL HANDRAIL
② 1 1/2" O.D. STEEL POST
③ 1 1/2" O.D. STEEL GUIDE RAIL
④ BLOCK OUT 2 1/2" x 2 1/2" x 8" DEEP SQUARE POST OPENING PRIOR TO POURING CONCRETE. FILL WITH NON-SHRINK GROUT AFTER POST IS PLACED, ALIGNED AND LEVELED.
⑤ 1 1/2" STEEL HANDRAIL EXTENSION RETURN. EXTENSION SHALL EXTEND A MINIMUM OF 1' IN DIRECTION OF RAMP. IF EXTENSION WOULD CREATE A HAZARD, THE EXTENSION MAY BE TURNED 90 DEGREES TO THE RUN OF THE RAMP.

ANY WELDS SHALL BE SLOT OR FILLET WELDS EQUAL TO THICKNESS OF PIPE. WELD ALL JOINTS ALL AROUND.

PARKER CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL
DATE: NOVEMBER 2020
DETAIL: 3
1 OF 1

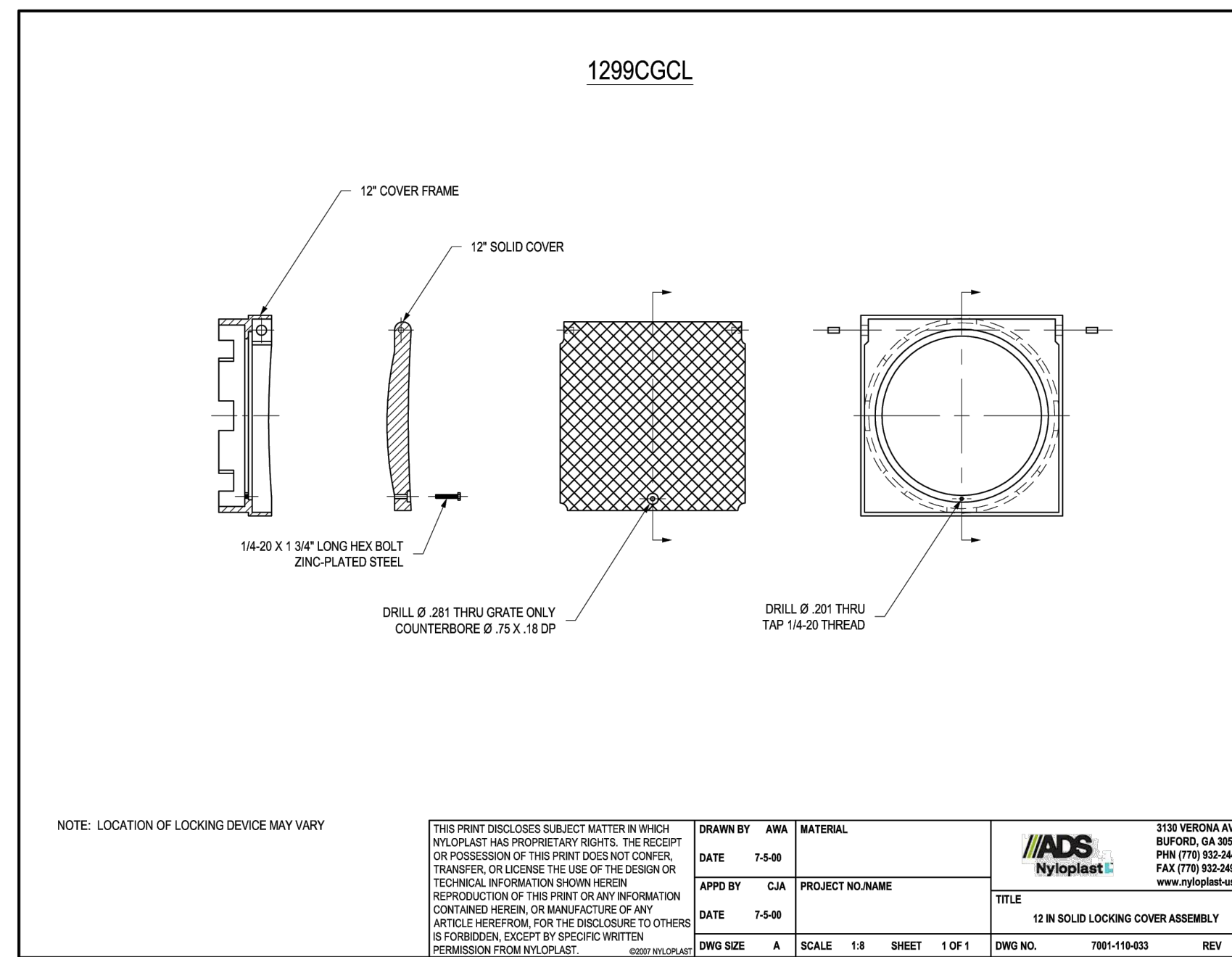
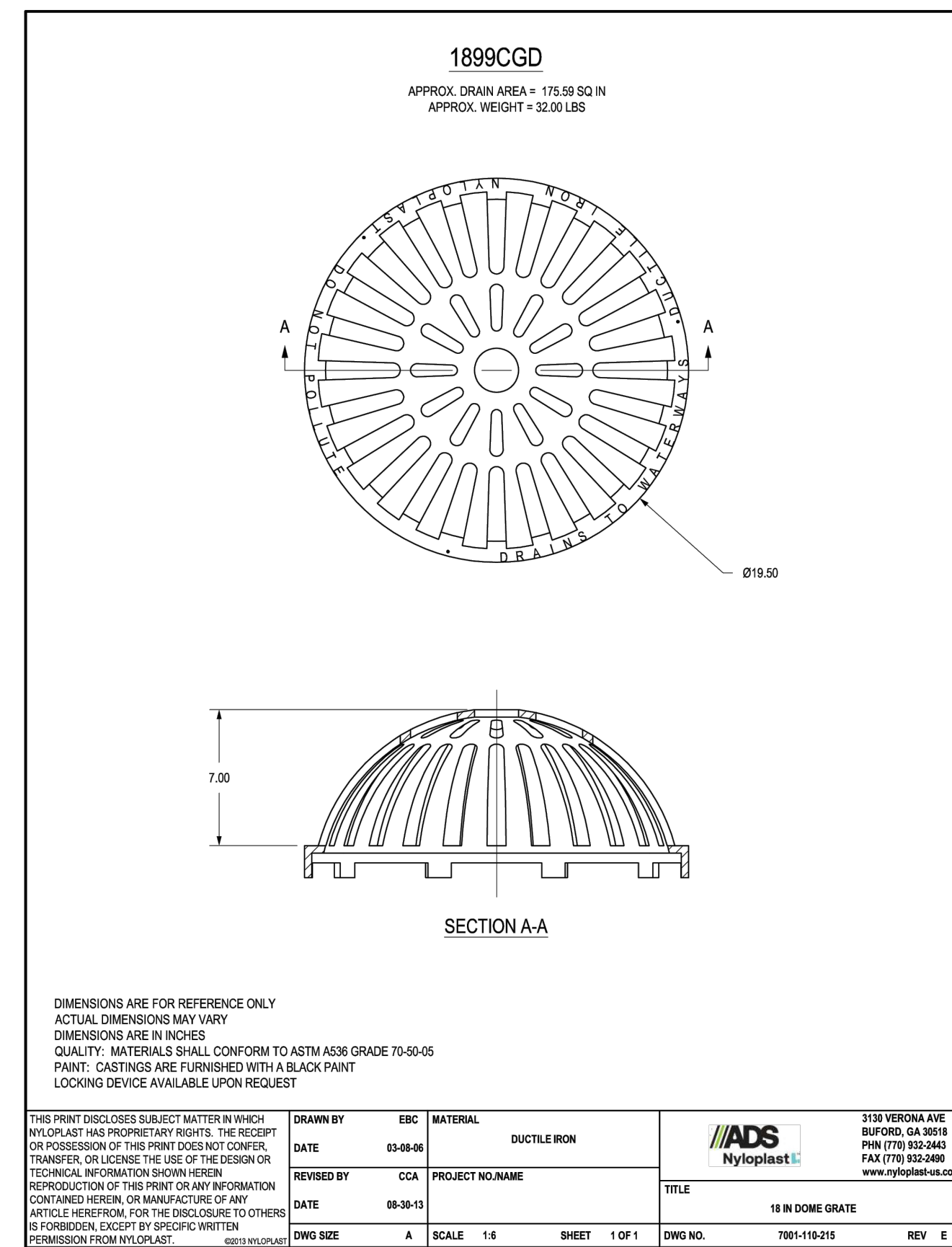
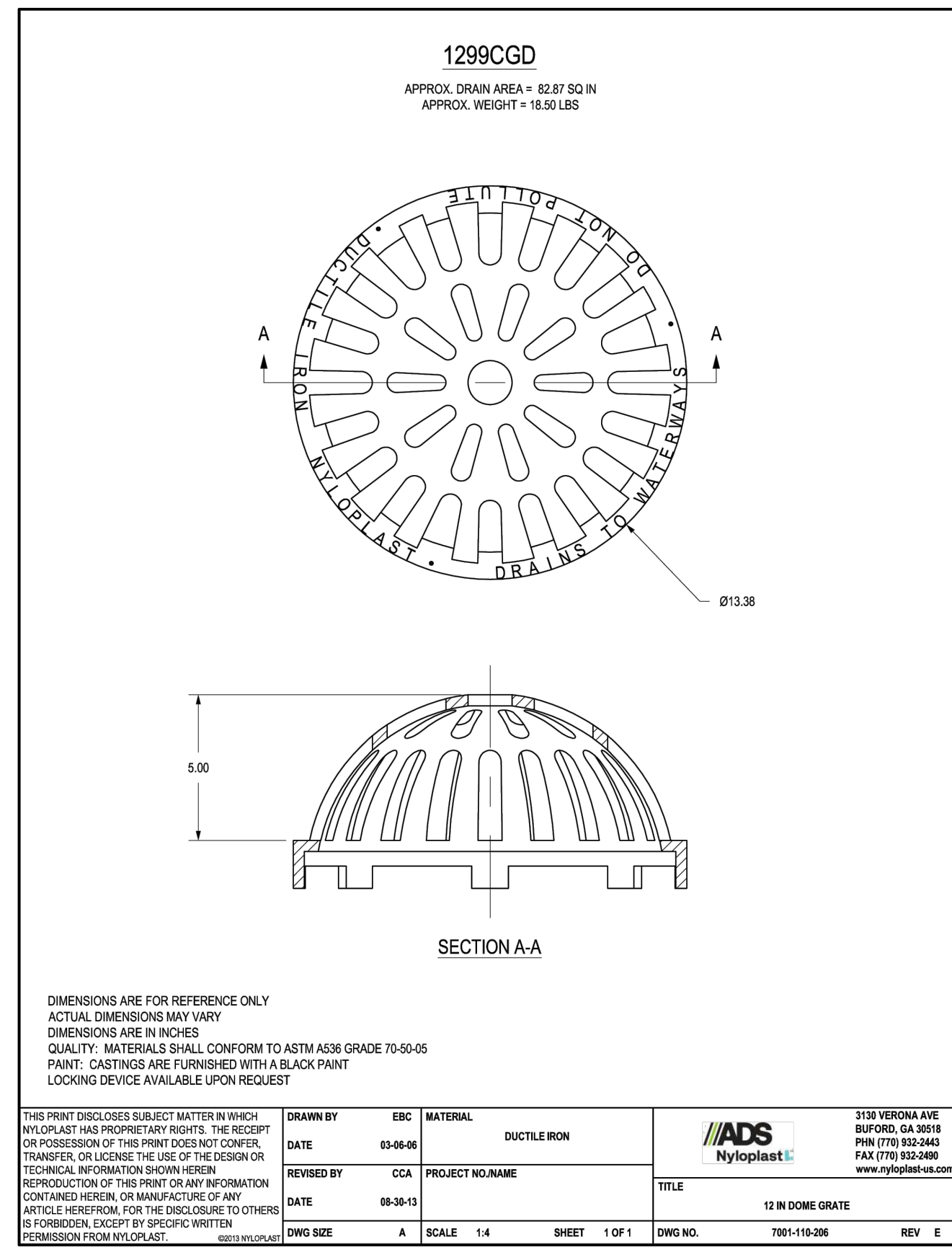
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e-mail: architect@esterfischer.com

LOT 1 PARKER POINTE SUBDIVISION FILING 1
PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROH RD
PARKER, CO #2
SITE DETAILS

O'Reilly AUTO PARTS
CORPORATE OFFICES
233 SOUTH PATTERSON
SPRINGFIELD, MISSOURI 65802
(417) 862-2674 TELEPHONE

COMM # 4884
DATE: 02/14/2025
REVISION
DATE:

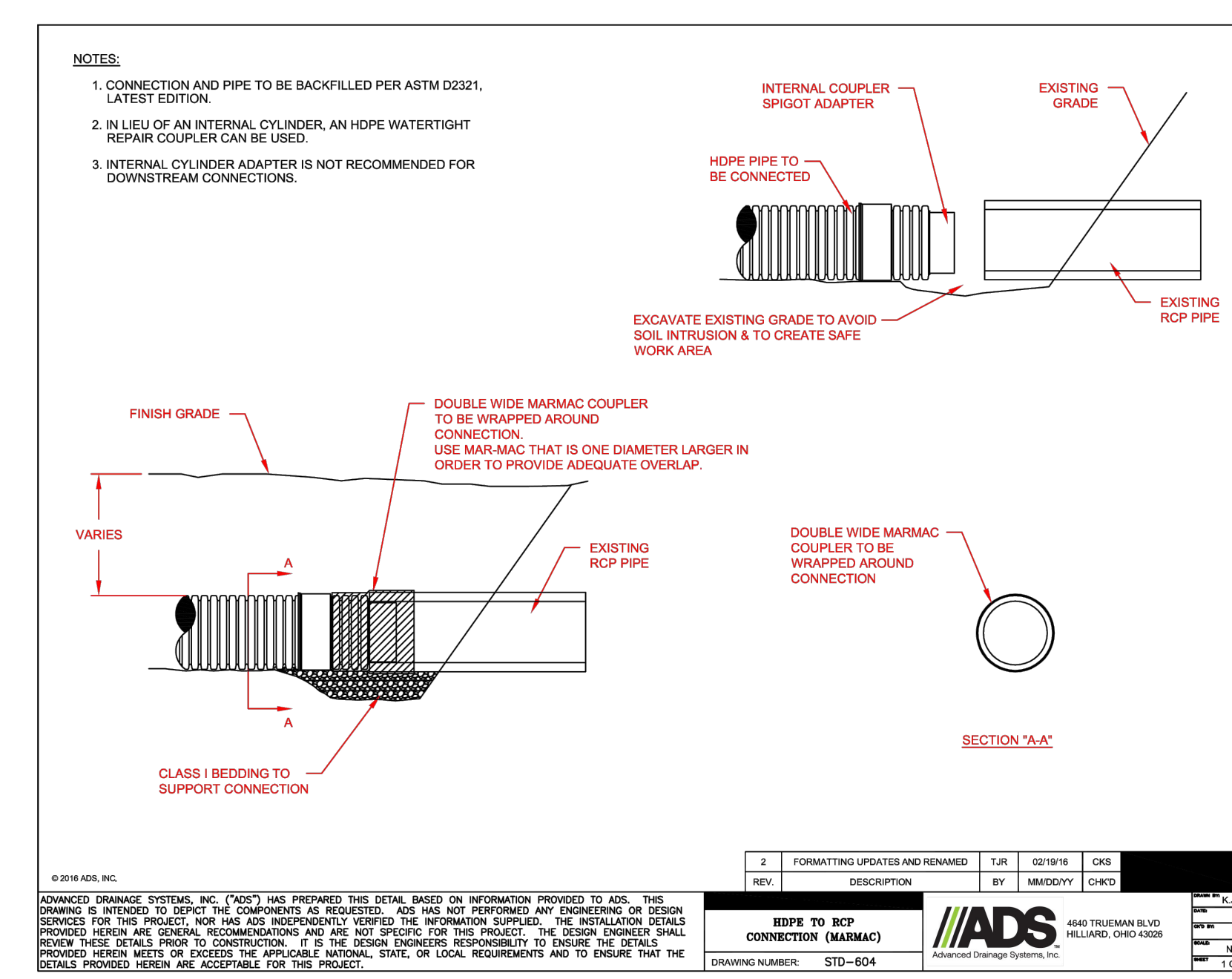
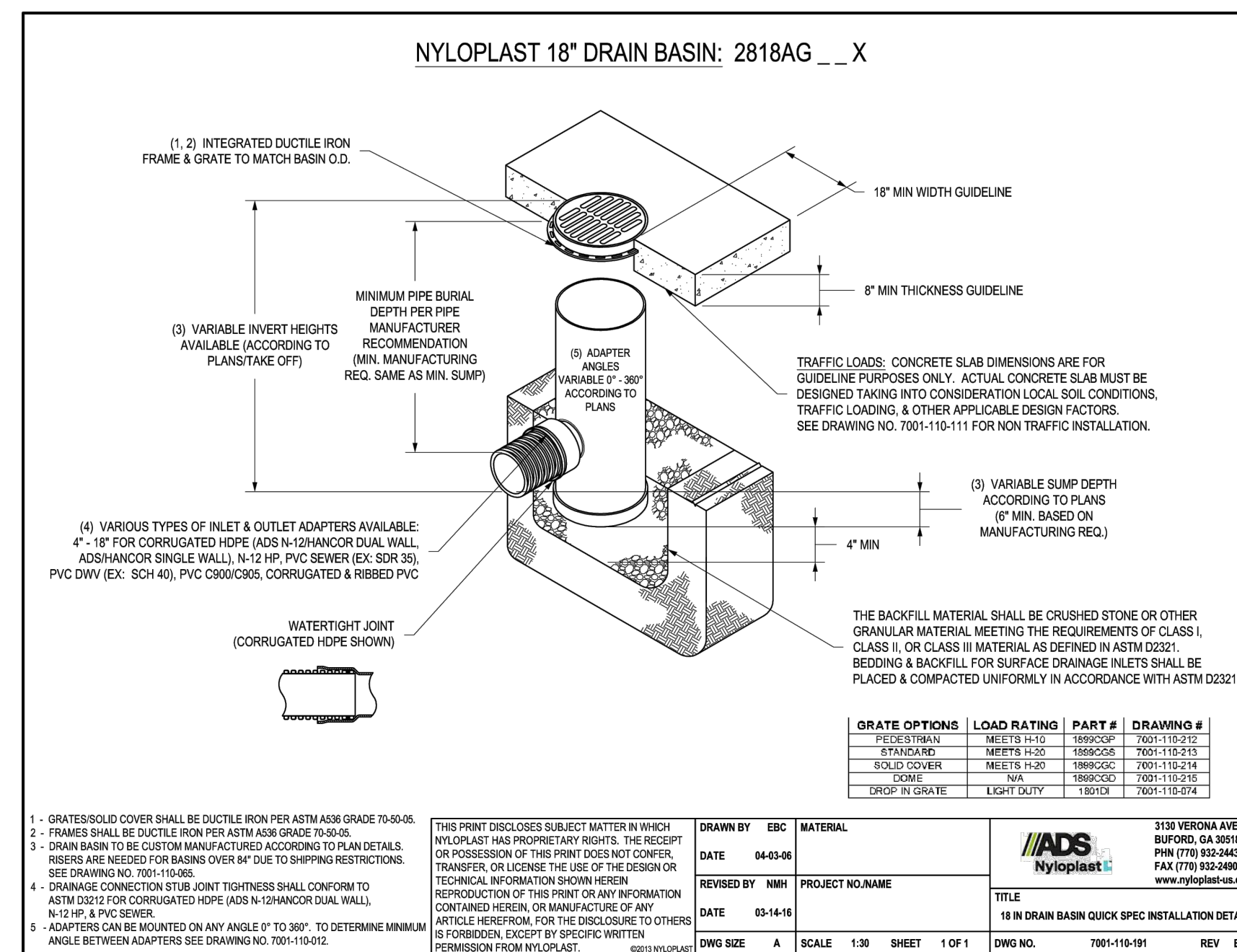
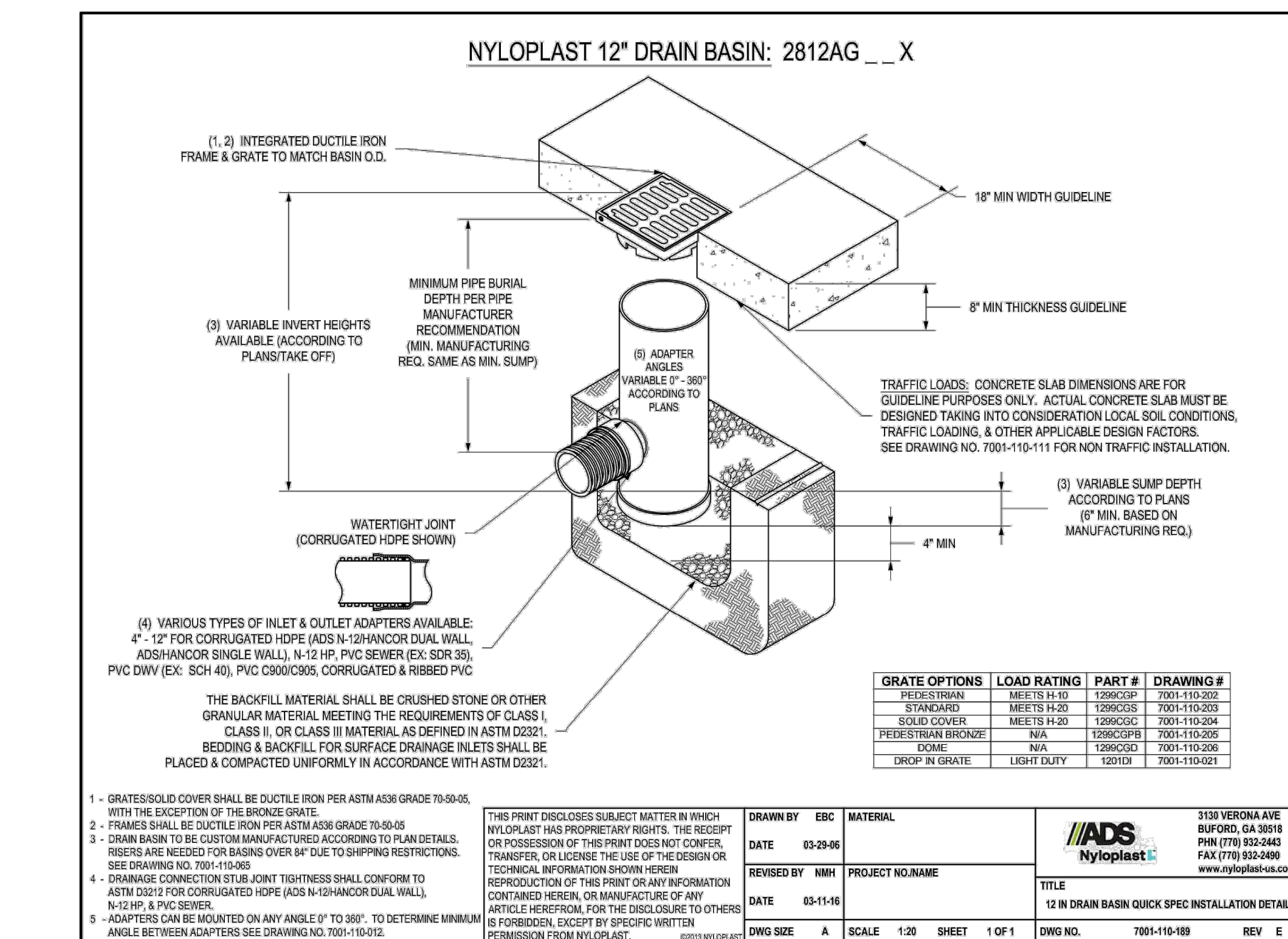


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DRAWN BY	AWA	MATERIAL	DUCTILE IRON
DATE	03-09-06		
REVISOR	CCA	PROJECT NO.	12 IN DOME GRATE
DATE	08-30-13		
DWG SIZE	A	SCALE	1:4
SHEET	1 OF 1	DWG NO.	7001-110-206
REV	E		

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DRAWN BY	AWA	MATERIAL	DUCTILE IRON
DATE	03-09-06		
REVISOR	CCA	PROJECT NO.	18 IN DOME GRATE
DATE	08-30-13		
DWG SIZE	A	SCALE	1:4
SHEET	1 OF 1	DWG NO.	7001-110-215
REV	E		



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LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:
NEW O'REILLY AUTO PARTS STORE
STROH RD
PARKER, CO #2
NYLOPLAST DETAILS

O'Reilly AUTO PARTS

CORPORATE OFFICES
233 SOUTH PATTERSON
SPRINGFIELD, MISSOURI 65802
(417) 862-2674 TELEPHONE

COMM # 4884
DATE: 02/14/2025
REVISION
DATE:

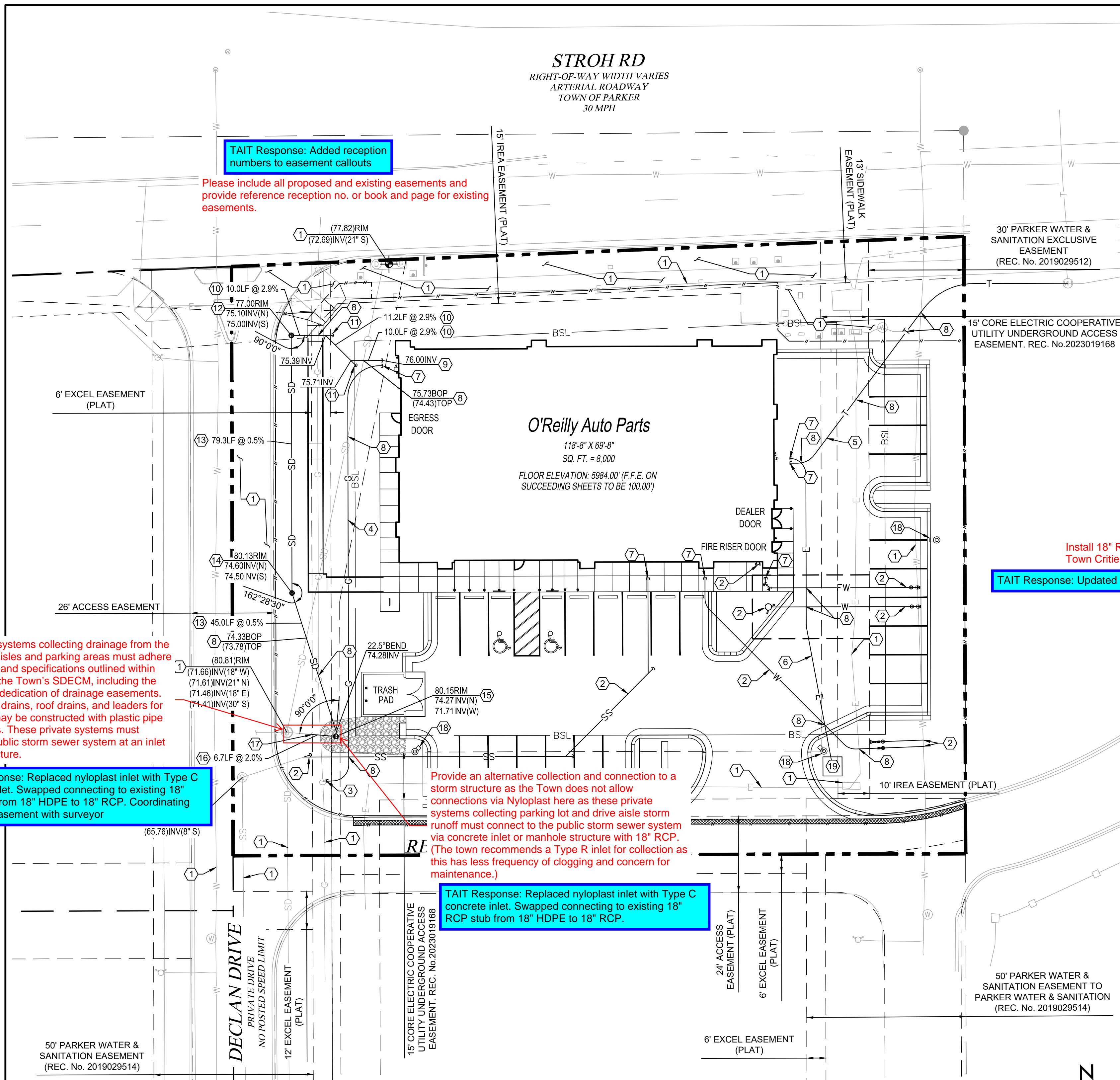
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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORKS

DATE

C2.4



KEY NOTES

- 1 PROTECT EXISTING IMPROVEMENTS IN PLACE.
- 2 REFER TO PARKER WATER AND SANITATION DISTRICT (PWS) PLANS FOR WATER AND SANITARY SEWER DETAILS.
- 3 GAS SERVICE CONNECTION TO EXISTING GAS SERVICE STUB.
- 4 GAS SERVICE LINE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
- 5 TELEPHONE LINE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
- 6 ELECTRIC SERVICE LINE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
- 7 UTILITY CONNECTION AT BUILDING. REFER TO MEP PLANS FOR CONTINUATION.
- 8 UTILITY CROSSING. CONTRACTOR TO VERIFY ELEVATIONS AND NOTIFY ENGINEER OF CONFLICT.
- 9 TRANSITION ROOF DRAIN TO 8" ADS N-12 STORM DRAIN PER DETAIL 12/C2.2.
- 10 INSTALL 8" ADS N-12 DUAL WALL STORM DRAIN.
- 11 INSTALL 8" STORM DRAIN CLEANOUT.
- 12 INSTALL 12" NYLOPLAST DRAIN BASIN WITH DOME GRATE. REFER TO SHEET C2.4 FOR MORE DETAIL.
- 13 INSTALL 12" ADS N-12 DUAL WALL STORM DRAIN.
- 14 INSTALL 12" NYLOPLAST DRAIN BASIN WITH SOLID COVER. REFER TO SHEET C2.4 FOR MORE DETAIL.
- 15 INSTALL 18" NYLOPLAST DRAIN BASIN WITH DOME GRATE. REFER TO SHEET C2.4 FOR MORE DETAIL.
- 16 INSTALL 18" ADS N-12 DUAL WALL STORM DRAIN.
- 17 **INSTALL 18" ADS MARMAC CONNECTION AT CONNECTION TO EXISTING 18" RCP STORM DRAIN STUB. REFER TO SHEET C2.4 FOR MORE DETAIL.**
- 18 LIGHT POLE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
- 19 INSTALL TRANSFORMER PAD PER CORE ELECTRIC STANDARDS.

GENERAL NOTES

1. CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES USING UTILITY CONTACT INFORMATION GIVEN. COORDINATE FOR ALL UTILITY WORK SHOWN. GIVE UTILITY COMPANIES NOTICE OF CONSTRUCTION START DATE AND ESTIMATED COMPLETION DATE.
2. PRIOR TO INSTALLATION, GENERAL CONTRACTOR TO CONFIRM THAT LIGHT POLES, LANDSCAPING AND UTILITIES DO NOT CONFLICT WITH SIGN LOCATION SHOWN. IF ANY POTENTIAL CONFLICT IS DISCOVERED, GENERAL CONTRACTOR TO CONTACT THE O'REILLY PROJECT ADMINISTRATOR BEFORE.

EXISTING UTILITY NOTES

CONTRACTOR TO CONFIRM MINIMUM BURY DEPTH IS MAINTAINED OVER EXISTING UTILITIES.

EXISTING CONDITIONS NOTE

1. THE CONTRACTOR IS OBLIGATED TO INSPECT FOR EXISTING CONDITIONS/INSTALLATIONS AND AVAILABLE INFORMATION PRIOR TO SUBMITTING A BID, REFER TO SPECIFICATIONS ALSO.
2. EXISTING INSTALLATIONS (SUCH AS WATER MAINS/LINES, GAS MAINS/LINES, SEWER MAINS/LINES, TELEPHONE LINES, POWER LINES, AND UTILITY STRUCTURES IN THE VICINITY OF THE WORK TO BE DONE) ARE INDICATED ON THE DRAWINGS ONLY TO THE EXTENT THAT SUCH INFORMATION HAS BEEN MADE AVAILABLE TO OR DISCOVERED BY THE ENGINEER IN PREPARING THE DRAWINGS. THERE IS NO GUARANTEE AS TO THE ACCURACY OR COMPLETENESS OF SUCH INFORMATION, AND ALL RESPONSIBILITY FOR THE ACCURACY AND COMPLETENESS THEREOF IS EXPRESSLY DISCLAIMED.
3. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATING ALL EXISTING INSTALLATIONS.
4. ANY DELAY, ADDITIONAL WORK, FEES OR EXTRA COST TO THE CONTRACTOR CAUSED BY RESULTING FROM DAMAGE TO OR MODIFICATIONS OF EXISTING INSTALLATIONS BY THE CONTRACTOR OR AFFECTED UTILITY COMPANY SHALL NOT CONSTITUTE A CLAIM FOR EXTRA WORK, ADDITIONAL PAYMENT OR DAMAGES.
5. THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONSTRUCTION PRIOR TO SUBMITTING HIS BID. NO EXTRAS WILL BE PAID DUE TO UNANTICIPATED EXISTING CONDITIONS / INSTALLATIONS.

LEGEND

NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.

---	RIGHT-OF-WAY
---	O'REILLY PROPERTY LINE
---	ADJACENT PROPERTY LINE
---	EXISTING EASEMENT
---	PROPOSED EASEMENT
---	BSL - BUILDING SETBACK LINE
---	ROAD CENTERLINE
---	LIMIT OF DISTURBANCE
---	PROPOSED SAWCUT
---	PROPOSED WATER LINE
---	PROPOSED SEWER LINE
---	PROPOSED ELECTRIC LINE
---	PROPOSED TELEPHONE LINE
---	PROPOSED SIGN
---	PROPOSED WHEEL STOP
---	PROPOSED BOLLARD
---	PROPOSED FDC
---	PROPOSED FIRE HYDRANT
---	PROPOSED GATE VALVE
---	PROPOSED THRUST BLOCK
---	PROPOSED CLEANOUT
---	PROPOSED LIGHT POLE
---	PROPOSED RIPRAP

Provide the following note on the Overall Utility Plan: "ANY PROPOSED UTILITY CONNECTION WITHIN AN EXISTING ROADWAY WITHIN PUBLIC RIGHT OF WAY WILL REQUIRE A TOWN RIGHT-OF-WAY PERMIT PRIOR TO COMMENCING WORK. THE TOWN REQUIRES CONNECTIONS TO BE BORED TO THE EXTENT POSSIBLE, AND ANY STREET CUT ALLOWED BY THE TOWN WILL BE REQUIRED TO BE PATCHED ACCORDING TO THE TOWN'S CONSTRUCTION STANDARDS AND DETAILS. THE LIMITS OF PATCHING WILL BE DETERMINED BY THE TOWN AT THE TIME OF CONSTRUCTION. THE TOWN OF PARKER DOES NOT ALLOW ROADWAY CLOSURES FOR UTILITY WORK."

TAIT Response: Added note

TAIT Response: Updated per request

TAIT Response: Added reception numbers to easement callouts

Please include all proposed and existing easements and provide reference reception no. or book and page for existing easements.

All storm sewer systems collecting drainage from the proposed drive aisles and parking areas must adhere to the standards and specifications outlined within Section 6.3.3 of the Town's SDECM, including the use of RCP and dedication of drainage easements. Only Landscape drains, roof drains, and leaders for those systems may be constructed with plastic pipe and catch basins. These private systems must connect to the public storm sewer system at an inlet or manhole structure.

TAIT Response: Replaced nyloplast inlet with Type C concrete inlet. Swapped connecting to existing 18" RCP stub from 18" HDPE to 18" RCP. Coordinating drainage easement with surveyor

Provide an alternative collection and connection to a storm structure as the Town does not allow connections via Nyloplast here as these private systems collecting parking lot and drive aisle storm runoff must connect to the public storm sewer system via concrete inlet or manhole structure with 18" RCP. (The town recommends a Type R inlet for collection as this has less frequency of clogging and concern for maintenance.)

TAIT Response: Replaced nyloplast inlet with Type C concrete inlet. Swapped connecting to existing 18" RCP stub from 18" HDPE to 18" RCP.

1 SITE UTILITY PLAN

C3.1 SCALE: 1" = 20'-0"

CAUTION:
INFORMATION ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.



BENCHMARK

DOUGLAS COUNTY BENCHMARK 1.060032, EL.=5,903.17 FEET (NAVVD88) - 3 1/4" INCH ALUMINUM CAP (DOUGLAS COUNTY GIS SECONDARY CONTROL MONUMENT 1.060032) AT THE SOUTHEAST CORNER OF THE PARKING LOT OF STORH RANCH SOCCER FIELDS, ON THE NORTH SIDE OF STORH RD, EAST OF THE INTERSECTION OF STORH ROAD AND N. MOTSENBOCKER ROAD, CITY OF PARKER, COLORADO.

BASIS OF BEARING

BEARINGS SHOWN HEREON ARE BASED ON THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST OF THE 6TH P.M., MONUMENTED AS SHOWN ON THIS SURVEY PLAT AS BEARING NORTH 89°33'14" EAST, WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO.

UNAUTHORIZED CHANGES & USES

THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES OF THESE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THESE PLANS PRIOR TO CONSTRUCTION. CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF DESIGN PROFESSIONAL.

NOTE TO CONTRACTOR

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES, PIPES, AND/OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. THERE MAY BE EXISTING UTILITIES NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL ASCERTAIN THE TRUE VERTICAL AND HORIZONTAL LOCATION OF THOSE UNDERGROUND UTILITIES TO BE USED PRIOR TO CONSTRUCTION AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY PUBLIC OR PRIVATE UTILITIES, SHOWN OR NOT SHOWN HEREON.

TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORK

DATE

PREPARED UNDER THE SUPERVISION OF TAIT & ASSOCIATES, INC.

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ENGINEERING ENVIRONMENTAL BUILDING LAND
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PROJECT:
NEW O'REILLY AUTO PARTS STORE
STORH RD
PARKER, CO #2
SITE UTILITY PLAN

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COMM # 4884
DATE: 02/14/2025
REVISION
DATE:

C3.1