

# O'Reilly AUTO PARTS

## O'REILLY AUTO PARTS PARKER, CO (PK2)

### LOT 1 OF PARKER POINTE SUBDIVISION FILING NO 1

LOCATED IN THE NORTHEAST 1/4 SECTION OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST, OF THE 6TH P.M., TOWN OF PAKER, COUNTY OF DOUGLAS, STATE OF COLORADO

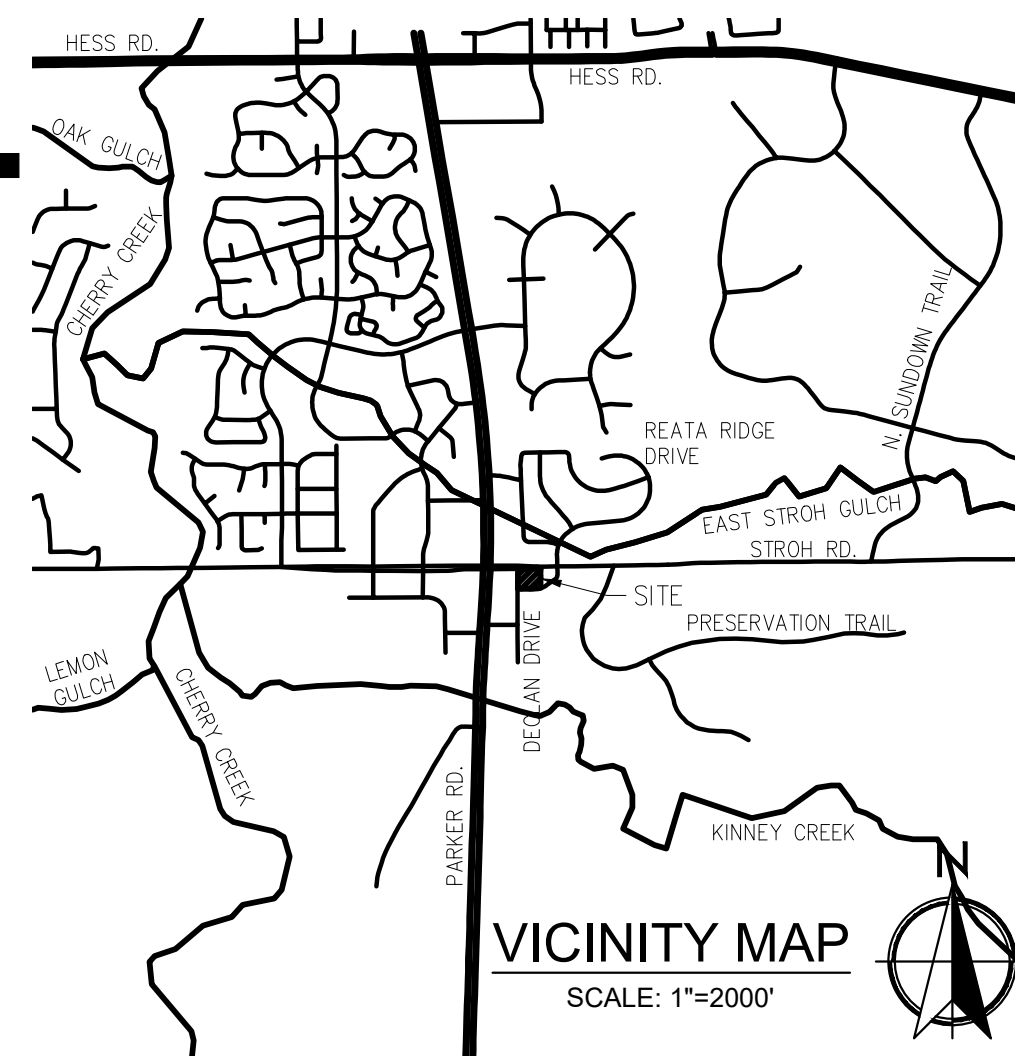
### TOWN OF PARKER CONTACTS

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**FIRE LIFE SAFETY**  
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**ENGINEERING**  
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**PLANNING**  
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### OWNER AND DEVELOPER



O'REILLY AUTO ENTERPRISES, LLC  
CORPORATE OFFICES  
233 SOUTH PATTERSON  
SPRINGFIELD, MISSOURI 65802  
417-862-2674 PHONE

### BIDDING INFORMATION:

REFER TO OWNER'S WEB SITE:  
[HTTP://WWW.OIREILLYPLANROOM.COM](http://www.oireillyplanroom.com)

NOTE: REFER TO CURRENT PROJECTS LIST, LOCATED AT BOTTOM OF SIGN IN PAGE, FOR INVITED GENERAL CONTRACTORS.

ALL SUB-CONTRACTOR BIDS TO BE SUBMITTED TO INVITED GENERAL CONTRACTORS ONLY.

CONTACT OWNER'S DESIGNATED REPRESENTATIVE FOR ADDITIONAL PROJECT INFORMATION.

### CIVIL ENGINEER

**TAIT & ASSOCIATES**  
Since 1964

320 North Lincoln Avenue  
Loveland, CO 80537  
p: 970.613.1447  
www.tait.com

ENGINEERING ENVIRONMENTAL BUILDING LAND  
Santa Ana Sacramento Denver Dallas  
San Luis Obispo Riverside Boise

### ABBREVIATIONS

BACK OF CURB	BC	FLOW LINE	FL
BOTTOM OF PIPE	BOP	FINISHED SURFACE	FS
CURB FACE	CF	INVERT	INV
EXPOSED FOUNDATION	EF	SIDEWALK	SW
EXISTING GROUND	EG	TOP OF CURB	TC
EXISTING SURFACE	ES	TOP OF GRATE	TOG
FINISHED FLOOR	FF	TOP OF PIPE	TOP

### NOTE TO CONTRACTOR

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES, PIPES, AND/OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. THERE MAY BE EXISTING UTILITIES NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL ASCERTAIN THE TRUE VERTICAL AND HORIZONTAL LOCATION OF THOSE UNDERGROUND UTILITIES TO BE USED PRIOR TO CONSTRUCTION AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY PUBLIC OR PRIVATE UTILITIES, SHOWN OR NOT SHOWN HEREON.

### UTILITY PURVEYORS

**WATER/SEWER**  
PARKER WATER AND SANITATION DISTRICT  
LISA SINDALL  
(303) 841-4627

**NATURAL GAS**  
XCEL ENERGY  
(800) 628-2121

**ELECTRIC**  
CORE ELECTRIC COOPERATIVE  
BROOKS KAUFMAN  
(303) 688-3100

**TELEPHONE**  
CENTURYLINK  
(720) 578-5142

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### LAND USE SUMMARY

LOT AREA	0.999 AC / 43,516 SF
PROPOSED USE	COMMERCIAL RETAIL
DWELLING UNITS	NONE
TOTAL BUILDINGS	1
BUILDING GFA	8,000 SF

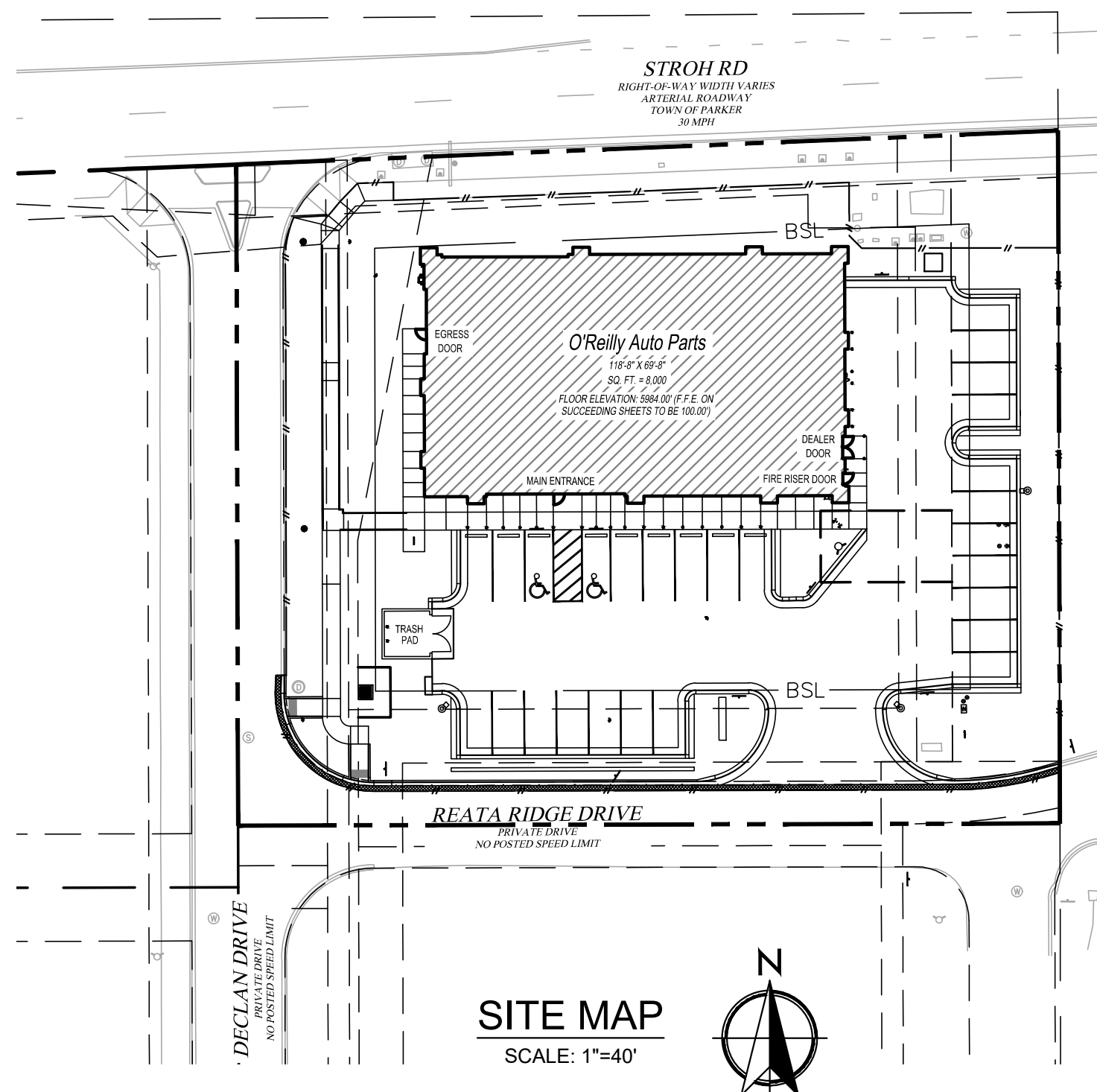
SETBACKS:	
REQUIRED	25' BUILDING SETBACK 6'/10' LANDSCAPE BUFFER
PROVIDED	25' BUILDING SETBACK 6'/10' LANDSCAPE BUFFER

PARKING:	
CALCULATION	1 SPACE PER 300 SF GFA
TOTAL REQUIRED	27
TOTAL PROVIDED	27
ACCESSIBLE REQUIRED	2
ACCESSIBLE PROVIDED	2
BICYCLE PARKING:	
CALCULATION	2 PER 10,000 SF GFA
REQUIRED	2
PROVIDED	2

PERCENTAGE LANDSCAPE REFER TO LANDSCAPE PLANS

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.



### GENERAL NOTES

A. **CONTRACT ADMINISTRATION (LIMITED SERVICE):** THE ARCHITECT AND/OR ENGINEER OF RECORD INDICATED HEREIN ARE PROVIDING A LIMITED SERVICE AND NOT PROVIDING CONTRACT ADMINISTRATION SERVICES FOR THIS PROJECT WHICH IS THE RESPONSIBILITY OF OTHERS. THE ARCHITECT AND/OR ENGINEER ARE NOT RESPONSIBLE FOR FIELD ACTIVITIES ON THIS PROJECT WITHOUT DIRECT INSPECTION OF THE WORK IN PROGRESS. IF FIELD CONDITIONS ARE UNCOVERED THAT REQUIRE A CHANGE OR ADDITIONAL INFORMATION, THE ARCHITECT AND/OR ENGINEER DOES NOT DELEGATE AUTHORITY TO ANYONE ELSE FOR DETERMINING THE MEANING OF PLANS OR SPECIFICATIONS AS AUTHENTICATED HEREIN.

B. **QUALITY STANDARDS AND BUILDING CODES:** CONTRACTORS SHALL BE RESPONSIBLE FOR KNOWING THE QUALITY AND PUBLIC SAFETY REGULATIONS SET FORTH IN THE GOVERNING CODES AND OTHER APPLICABLE REGULATIONS OF LOCAL AND STATE AGENCIES HAVING JURISDICTION WHICH GOVERN EACH CONTRACTOR'S WORK.

C. **EXISTING CONDITIONS:** FIELD VERIFY EXISTING CONDITIONS BY DETAILED INSPECTION PRIOR TO SUBMITTING BID AND BEGINNING WORK. NOTIFY THE ARCHITECT AND/OR ENGINEER OF RECORD IF EXISTING CONDITIONS DEVIATE SUBSTANTIALLY FROM THOSE INDICATED HEREIN.

### QUALITY CONTROL

- REFER TO PROJECT MANUAL, SECTION 01 45 16 - QUALITY CONTROL PROCEDURES, FOR ADDITIONAL REQUIREMENTS.
- REFER TO STRUCTURAL DRAWINGS FOR SCHEDULE OF SPECIAL INSPECTIONS, IF INDICATED.
- WHERE REQUIRED, THE OWNER SHALL PROVIDE THE SERVICES OF AN INDEPENDENT TESTING LABORATORY TO PERFORM TESTING AND SPECIAL INSPECTIONS INDICATED.

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### BENCHMARK

DOUGLAS COUNTY BENCHMARK 1.060032, EL=5,903.17 FEET (NAVD88) - 3 1/4" INCH ALUMINUM CAP (DOUGLAS COUNTY GIS SECONDARY CONTROL MONUMENT 1.060032) AT THE SOUTHEAST CORNER OF THE PARKING LOT OF STROH RANCH SOCCER FIELDS, ON THE NORTH SIDE OF STROH RD, EAST OF THE INTERSECTION OF STORH ROAD AND N. MOTSENBOCKER ROAD, CITY OF PARKER, COLORADO.

### BASIS OF BEARING

BEARINGS SHOWN HEREON ARE BASED ON THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST OF THE 6TH P.M., MONUMENTED AS SHOWN ON THIS SURVEY PLAT AS BEARING NORTH 89°33'14" EAST, WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO.

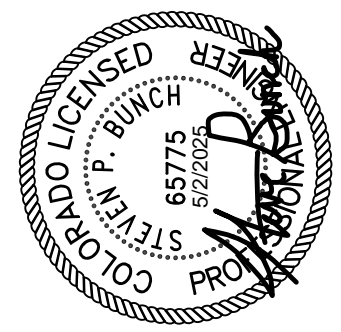
### UNAUTHORIZED CHANGES & USES

THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES OF THESE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THESE PLANS PRIOR TO CONSTRUCTION. CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF DESIGN PROFESSIONAL.



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Loveland, CO 80537  
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ENGINEERING ENVIRONMENTAL BUILDING LAND  
Santa Ana Sacramento Denver Boise  
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Since 1964

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PROJECT: LOT 1 PARKER POINTE SUBDIVISION FILING 1

NEW O'REILLY AUTO PARTS STORE  
STROH RD  
PARKER, CO #2

COVER SHEET

### EARTHWORK SUMMARY

ESTIMATED CUT:	661 CUBIC YARDS
ESTIMATED FILL:	495 CUBIC YARDS
ESTIMATED NET:	166 CUBIC YARDS (EXPORT)
ESTIMATED OVER-EXCAVATION:	985 CUBIC YARDS
STEEPEST PROPOSED SLOPE:	3:1
TOTAL DISTURBANCE:	35,105 SF (0.81 AC)
EXISTING IMPERVIOUS AREA:	6,754 SF (0.16 AC)
PROPOSED IMPERVIOUS AREA:	30,270 SF (0.70 AC)
AREA OF LAND DISTURBANCE:	35,105 SF (0.81 AC)
TREATED BY WATER QUALITY CONTROL MEASURES	

NOTE:  
EARTHWORK QUANTITIES ARE CALCULATED USING AUTOCAD CIVIL 3D TO COMPARE THE DIFFERENCE BETWEEN THE EXISTING SURFACE (CREATED FROM THE SURVEY POINTS PROVIDED BY TAIT & ASSOCIATES) AND THE PROPOSED SURFACE. QUANTITIES ARE RAW NUMBERS TO BE USED FOR PERMIT PURPOSES ONLY. THESE FIGURES TO NOT INCLUDE REMEDIAL QUANTITIES, BULKING OR SHRINKAGE FACTORS. CONTRACTOR SHALL VERIFY QUANTITIES FOR CONSTRUCTION BID PURPOSES.

OVER-EXCAVATION QUANTITY ESTIMATED USING 2' OVER-EXCAVATION AT THE BUILDING FOUNDATION AND 8 INCH OVER-EXCAVATION FOR ALL OTHER PAVED SURFACES. SEE GEOTECHNICAL REPORT FOR DETAILS.

**O'Reilly AUTO PARTS**  
CORPORATE OFFICES  
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SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

COMM # 4884

DATE: 02/14/2025

REVISION

DATE: 05/02/2025

## TOWN GENERAL NOTES

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- THE TOWN INSPECTOR SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A TOWN INSPECTOR IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE TOWN INSPECTOR'S ABSENCE. HOWEVER, THE TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION IN ACCORDANCE WITH ALL STATE AND LOCAL LAWS. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER DIRECTOR OF ENGINEERING/PUBLIC WORKS, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY USE PERMIT APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION."
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR. SUCH RESTORATION MAY BE REQUIRED TO BE COMPLETED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE FOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

**CAUTION:**  
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## STORM DRAINAGE INFRASTRUCTURE NOTES

- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT. WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72" (5 CCR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUDED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
- STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
- ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:
  - 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC)
  - 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
- EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS AND STRUCTURES. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
- PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

### BENCHMARK

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### UNAUTHORIZED CHANGES & USES

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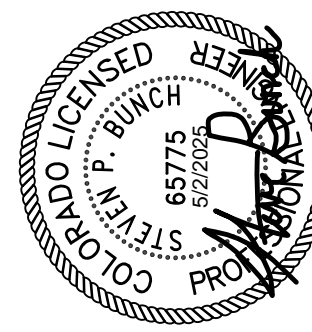
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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORK

DATE

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PROJECT:  
NEW O'REILLY AUTO PARTS STORE  
STROH RD  
PARKER, CO #2

**O'Reilly** AUTO PARTS

CORPORATE OFFICES  
233 SOUTH PATTERSON  
SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

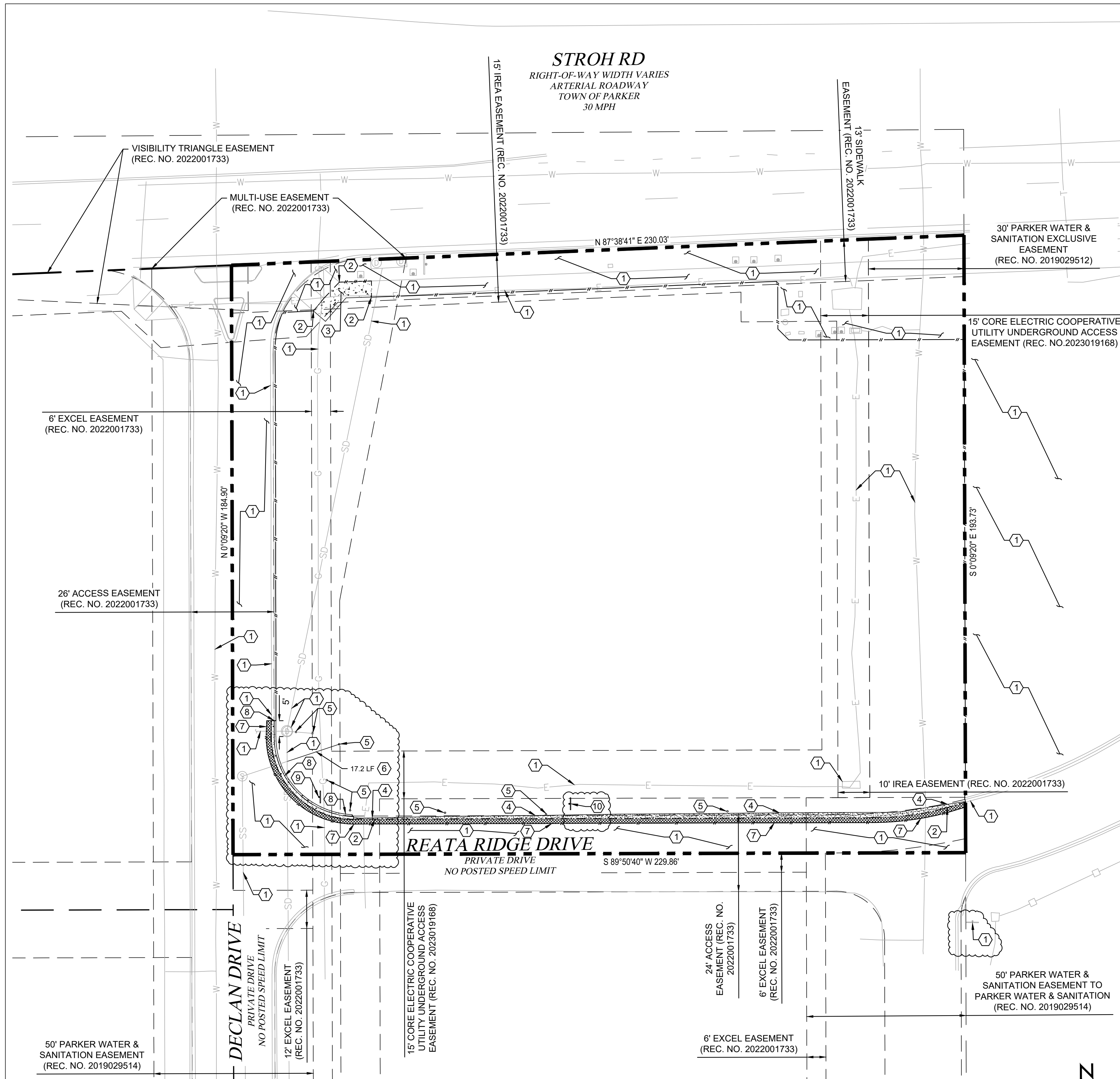
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DATE: 02/14/2025  
REVISION  
DATE: 05/02/2025

CT1.2

TIMOTHY M. GUILLOT  
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Fax: 417.862.3265  
e-mail: architect@esterfyschneider.com

Steven P. Bunch, PE 65775



### GENERAL NOTES

- (A) REFER TO PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS.
- (B) SITE CONDITIONS BASED UPON SURVEY PROVIDED BY OWNER. FIELD VERIFY EXISTING CONDITIONS BY DETAILED INSPECTION PRIOR TO SUBMITTING BID AND BEGINNING CONSTRUCTION. NOTIFY ARCHITECT IF EXISTING CONDITIONS DEVIATE SUBSTANTIALLY FROM THOSE INDICATED HEREIN.
- (C) REMOVE ALL EXISTING SITE CONSTRUCTION AND DELETERIOUS MATERIALS UNLESS OTHERWISE NOTED.
- (D) EXISTING CONSTRUCTION TO REMAIN TO BE PROTECTED AS REQUIRED. REPLACE AND/OR REPAIR EXISTING CONSTRUCTION DAMAGED DUE TO CONTRACTOR ACTIVITIES.
- (E) ALL DEMOLITION MATERIALS SHALL BECOME PROPERTY OF CONTRACTOR FOR PROPER DISPOSAL UNLESS OTHERWISE NOTED.
- (F) EXISTING UNDERGROUND INSTALLATION SUCH AS WATER MAINS, GAS MAINS, SEWERS, TELEPHONE LINES, POWER LINES, AND BURIED STRUCTURES IN THE VICINITY OF THE WORK ARE INDICATED ON THE DRAWINGS ONLY TO THE EXTENT SUCH INFORMATION HAS BEEN MADE AVAILABLE. THERE IS NO GUARANTEE AS TO THE ACCURACY OR COMPLETENESS OF SUCH INFORMATION AND ALL RESPONSIBILITY FOR THE ACCURACY AND COMPLETENESS IS EXPRESSLY DISCLAIMED. GENERALLY SERVICE CONNECTIONS ARE NOT INDICATED ON THE DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE FOR DISCOVERY OF EXISTING UNDERGROUND INSTALLATIONS IN ADVANCE OF EXCAVATING OR TRENCHING BY CONTRACTING LOCAL UTILITIES.

### KEY NOTES

- (1) PROTECT EXISTING IMPROVEMENTS IN PLACE.
- (2) SAWCUT EXISTING PAVEMENT. SIDEWALK SAWCUT SHALL BE MADE AT THE NEAREST JOINT.
- (3) REMOVE EXISTING CONCRETE SIDEWALK.
- (4) REMOVE EXISTING ASPHALT PAVEMENT.
- (5) REMOVE EXISTING POST.
- (6) REMOVE EXISTING 6" PVC SANITARY SEWER SERVICE LINE AS SHOWN.
- (7) 1.5' WIDE, 2" DEEP MILL OF EXISTING ASPHALT PAVEMENT.
- (8) REMOVE EXISTING CURB AND GUTTER.
- (9) APPROXIMATE LOCATION OF EXISTING "STOP" SIGN. RELOCATE SIGN TO LOCATION AS SHOWN ON SHEET C2.1.
- (10) APPROXIMATE LOCATION OF EXISTING "NO PARKING FIRE LANE" SIGN. RELOCATE SIGN TO LOCATION AS SHOWN ON SHEET C2.1.

### SYMBOLS LEGEND

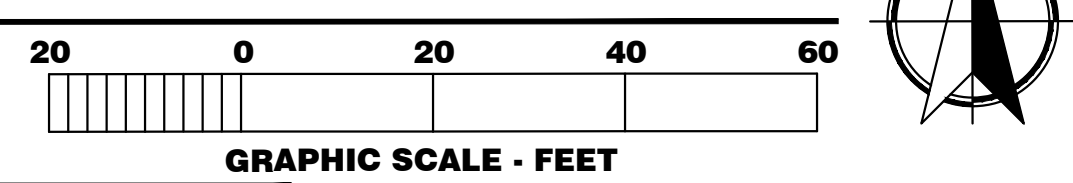
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  - EXISTING EASEMENT
  - PROPOSED EASEMENT
  - ROAD CENTERLINE
  - LIMIT OF DISTURBANCE
  - PROPOSED SAWCUT
  - IMPROVEMENTS TO BE REMOVED
  - IMPROVEMENTS TO REMAIN
  - ▨ AREA OF ASPHALT REMOVAL
  - ▨ AREA OF CONCRETE REMOVAL
  - ▨ AREA OF MILL & OVERLAY

### ENVIRONMENTAL GENERAL NOTES

- (A) AN ENVIRONMENTAL ANALYSIS HAS BEEN PERFORMED ON THE EXISTING SITE. REFER TO PROJECT MANUAL.
- (B) IF THIS PROJECT CONTAINS HAZARDOUS MATERIALS, CONTRACTOR TO PROVIDE WORK REQUIRED FOR PROPER REMOVAL, HANDLING, AND DISPOSAL PER ENVIRONMENTAL ANALYSIS RECOMMENDATIONS.

## 1 DEMOLITION PLAN

D1.1 SCALE: 1" = 20'-0"



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### BENCHMARK

DOUGLAS COUNTY BENCHMARK 1.060032, EL=5,903.17 FEET (NAVD88) - 3 1/4" INCH ALUMINUM CAP (DOUGLAS COUNTY GIS SECONDARY CONTROL MONUMENT 1.060032) AT THE SOUTHEAST CORNER OF THE PARKING LOT OF STROH RANCH SOCCER FIELDS, ON THE NORTH SIDE OF STROH RD, EAST OF THE INTERSECTION OF STORH ROAD AND N. MOTSENBOCKER ROAD, CITY OF PARKER, COLORADO.

### BASIS OF BEARING

BEARINGS SHOWN HEREON ARE BASED ON THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST OF THE 6TH P.M., MONUMENTED AS SHOWN ON THIS SURVEY PLAT AS BEARING NORTH 89°33'14" EAST, WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO.

### UNAUTHORIZED CHANGES & USES

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STROH RD  
PARKER, CO #2

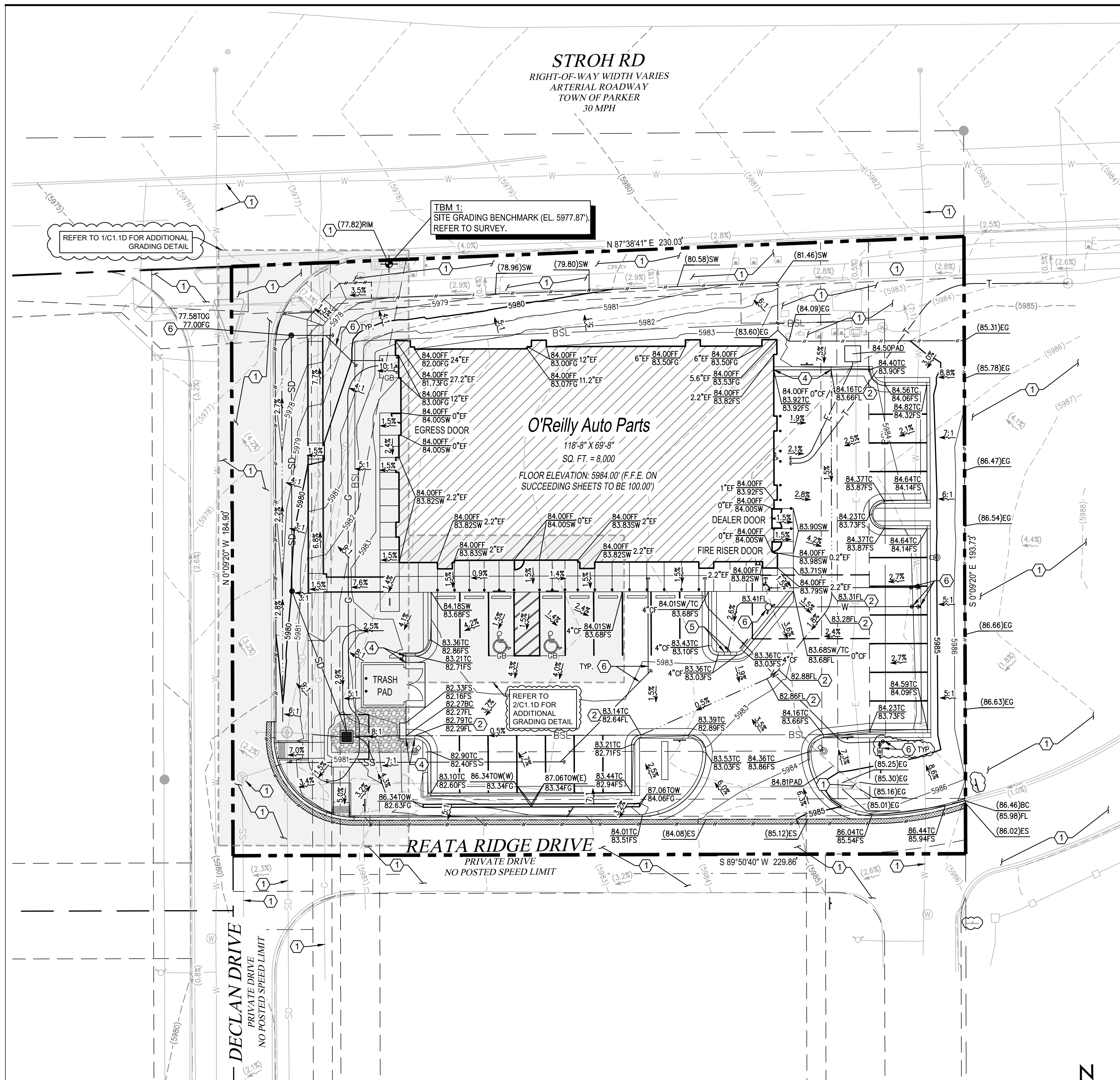
**DEMOLITION PLAN**

**O'Reilly AUTO PARTS**

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**1 SITE GRADING PLAN**  
**C1.1** SCALE: 1" = 20'-0"

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**KEY NOTES**

- 1 PROTECT IN PLACE.
- 2 SWALE IN PAVEMENT.
- 3 SWALE IN LANDSCAPING.
- 4 TRANSITION TO 0" CURB OVER DISTANCE AS SHOWN.
- 5 2" CURB CUT PER DETAIL 9/C2.2.
- 6 REFER TO SHEET C3.1 FOR STORM AND UTILITY IMPROVEMENT DETAILS.
- 7 INSTALL 18" DEEP D50 = 6" RIPRAP PER DETAIL 10/2.2.

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- C FIELD VERIFY SANITARY SEWER SERVICE CONNECTION INVERT PRIOR TO ESTABLISHING FINAL FINISH FLOOR ELEVATION. NOTIFY ENGINEER IF EXISTING CONDITIONS REQUIRE REVISED FINISH FLOOR ELEVATION TO PROVIDE PROPER DRAINAGE.
- D ACCESSIBLE PARKING AREAS TO SLOPE 2% MAXIMUM IN ALL DIRECTIONS. DESIGNATED ACCESSIBLE ROUTE TO SLOPE 5% MAXIMUM IN DIRECTION OF TRAVEL WITH 2% MAXIMUM CROSS SLOPE.

**SITE EXCAVATION REQUIREMENTS**

- A A GEOTECHNICAL ANALYSIS HAS BEEN PERFORMED ON THIS SITE. REFER TO PROJECT MANUAL.
- B FOLLOW GEOTECHNICAL ANALYSIS RECOMMENDATIONS FOR SITE EXCAVATION REQUIREMENTS.
- C REFER TO STRUCTURAL DRAWINGS FOR BUILDING EXCAVATION REQUIREMENTS.

**ADA NOTE**

- ALL ACCESSIBLE PATHS OF TRAVEL ARE TO ABIDE BY THE FOLLOWING PARAMETERS:
- CROSS-SLOPE NOT TO EXCEED 2%
  - LONGITUDINAL SLOPE NOT TO EXCEED 5%
  - LONGITUDINAL RAMP SLOPE NOT TO EXCEED 8.33%
  - RAMP LANDINGS NOT TO EXCEED 2% (LANDING PLACEMENT PER RAMP DETAILS)
  - ELEVATION DIFFERENCE BETWEEN RAMP LANDINGS SHALL NOT EXCEED 30 INCHES.
  - LOWER RAIL SHALL BE PROVIDED ON RAMP RAILING THAT WILL PREVENT AN OBJECT 4 INCHES AND LARGER FROM PASSING UNDER RAILING.
  - PATHWAYS EXCEEDING THESE STANDARDS WILL BE REPLACED AT THE CONTRACTORS EXPENSE

**SYMBOL LEGEND**

NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.

	RIGHT-OF-WAY
	O'REILLY PROPERTY LINE
	ADJACENT PROPERTY LINE
	EXISTING EASEMENT
	PROPOSED EASEMENT
	BSL - BUILDING SETBACK LINE
	ROAD CENTERLINE
	LIMIT OF DISTURBANCE
	PROPOSED SAWCUT
	PROPOSED WATER LINE
	PROPOSED SEWER LINE
	PROPOSED ELECTRIC LINE
	PROPOSED GAS LINE
	PROPOSED TELEPHONE LINE
	PROPOSED RIDGE
	PROPOSED GRADE BREAK
	PROPOSED FLOWLINE
	PROPOSED SIGN
	PROPOSED WHEEL STOP
	PROPOSED BOLLARD
	PROPOSED FDC
	PROPOSED FIRE HYDRANT
	PROPOSED GATE VALVE
	PROPOSED THRUST BLOCK
	PROPOSED CLEANOUT
	PROPOSED LIGHT POLE
	EXISTING ELEVATION
	PROPOSED ELEVATION
	EXISTING CONTOURS
	PROPOSED CONTOURS
	EXISTING SLOPE
	PROPOSED SLOPE
	PROPOSED RIPRAP

**ABBREVIATIONS**

BC	BACK OF CURB
CF	CURB FACE
EF	EXPOSED FOUNDATION
EG	EXISTING GROUND
ES	EXISTING SURFACE
FF	FINISHED FLOOR
FL	FLOW LINE
FS	FINISHED SURFACE
INV	INVERT
SW	TOP OF CURB
TC	TOP OF GRATE
TOG	TOP OF WALL
TOW	TOP OF WALL

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**LOT 1 PARKER POINTE SUBDIVISION FILING 1**

PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
**STROTH RD**  
**PARKER, CO #2**  
**SITE GRADING PLAN**

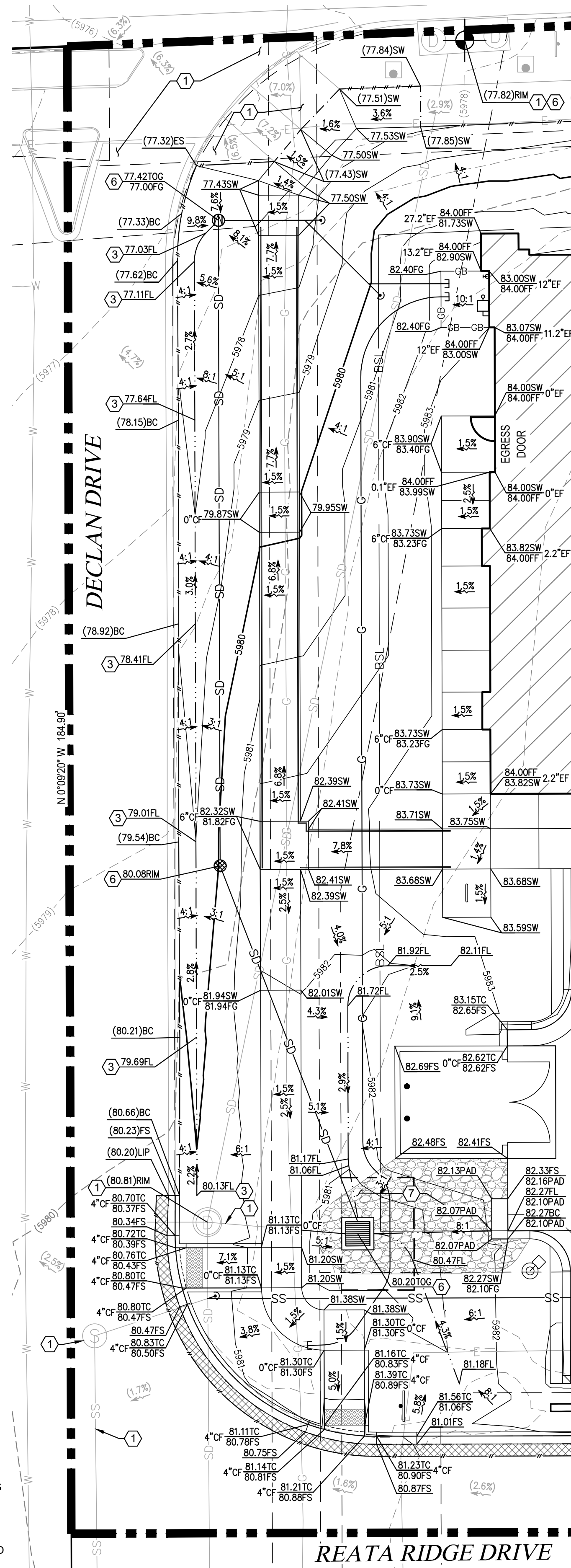
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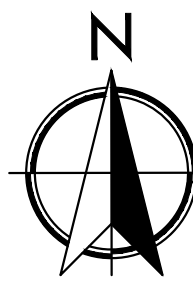
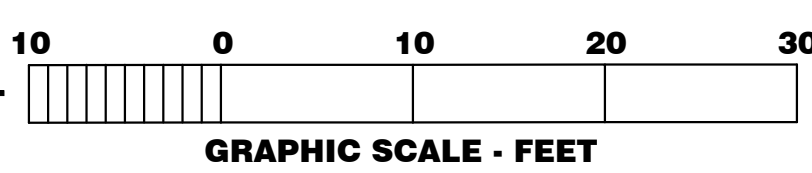


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**1 WEST GRADING DETAIL**

C1.1D SCALE: 1" = 10'-0"

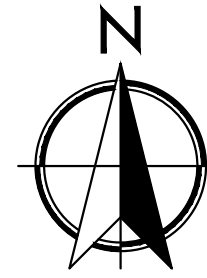
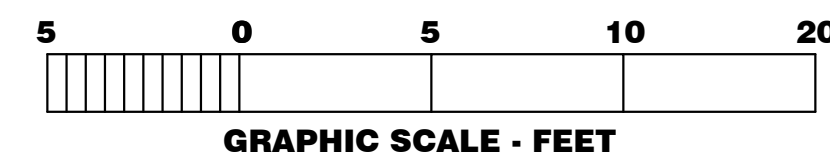
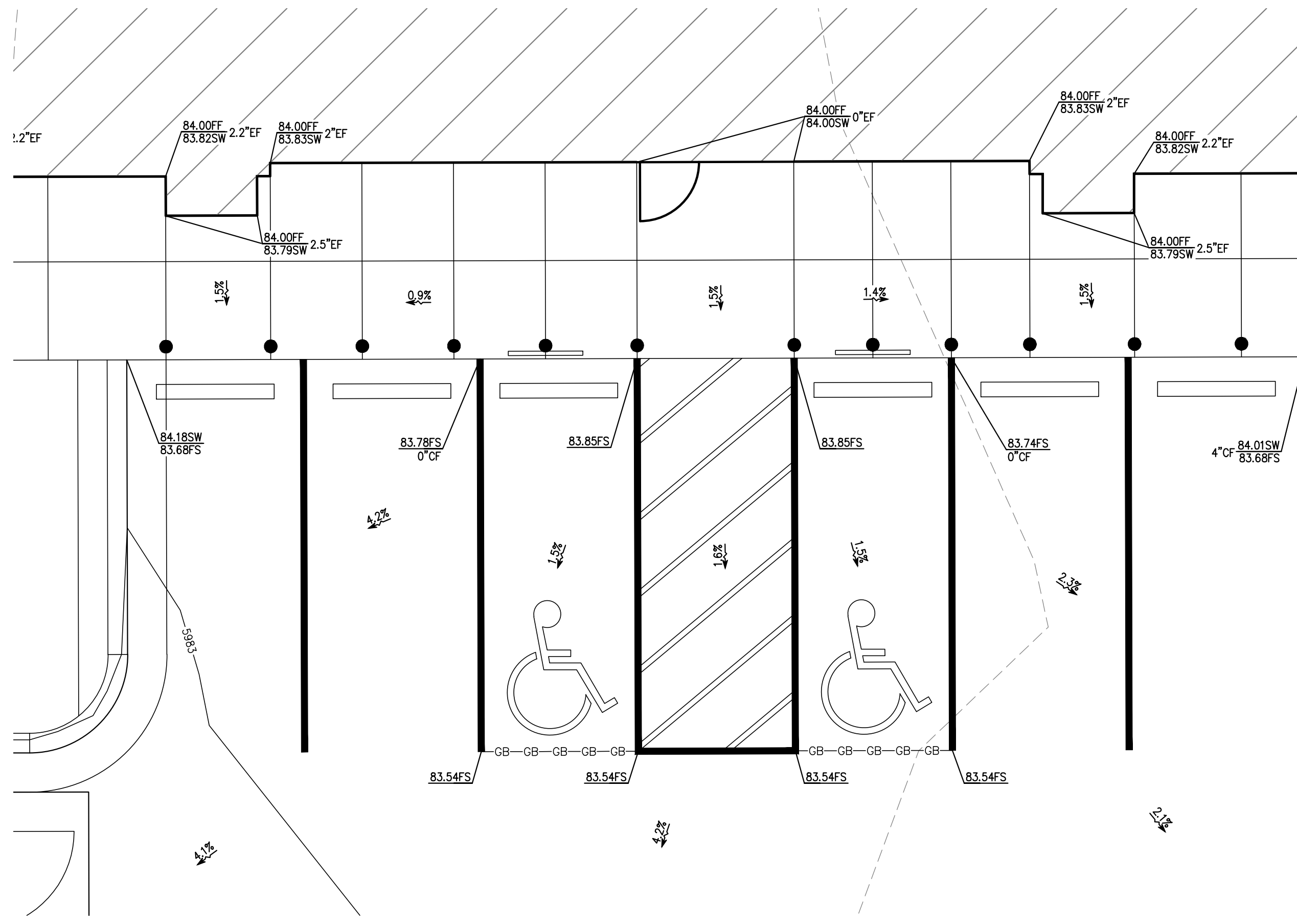


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**2 ACCESSIBLE PARKING DETAIL**

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- ROAD CENTERLINE
- LIMIT OF DISTURBANCE
- PROPOSED SAWCUT
- W — PROPOSED WATER LINE
- SS — PROPOSED SEWER LINE
- E — PROPOSED ELECTRIC LINE
- G — PROPOSED GAS LINE
- T — PROPOSED TELEPHONE LINE
- R — R — PROPOSED RIDGE
- GB — GB — PROPOSED GRADE BREAK
- PROPOSED FLOWLINE
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- PROPOSED FDC
- PROPOSED FIRE HYDRANT
- PROPOSED GATE VALVE
- PROPOSED THRUST BLOCK
- PROPOSED CLEANOUT
- PROPOSED LIGHT POLE
- (40.00)XX — EXISTING ELEVATION
- 40.00XX — PROPOSED ELEVATION
- (XXXX) — (XXXX) — EXISTING CONTOURS
- (XXXX) — (XXXX) — PROPOSED CONTOURS
- (X.XX) — EXISTING SLOPE
- X.XX — PROPOSED SLOPE
- PROPOSED RIPRAP

- ABBREVIATIONS**
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  - CF — CURB FACE
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  - FS — FINISHED SURFACE
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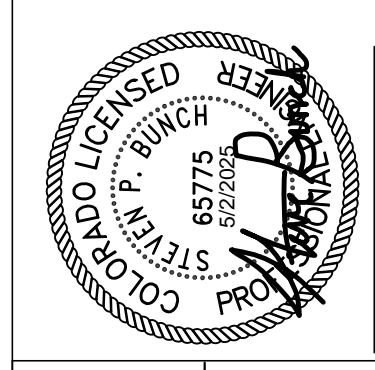
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San Luis Obispo Riverside Boise



LOT 1 PARKER POINTE SUBDIVISION FILING 1

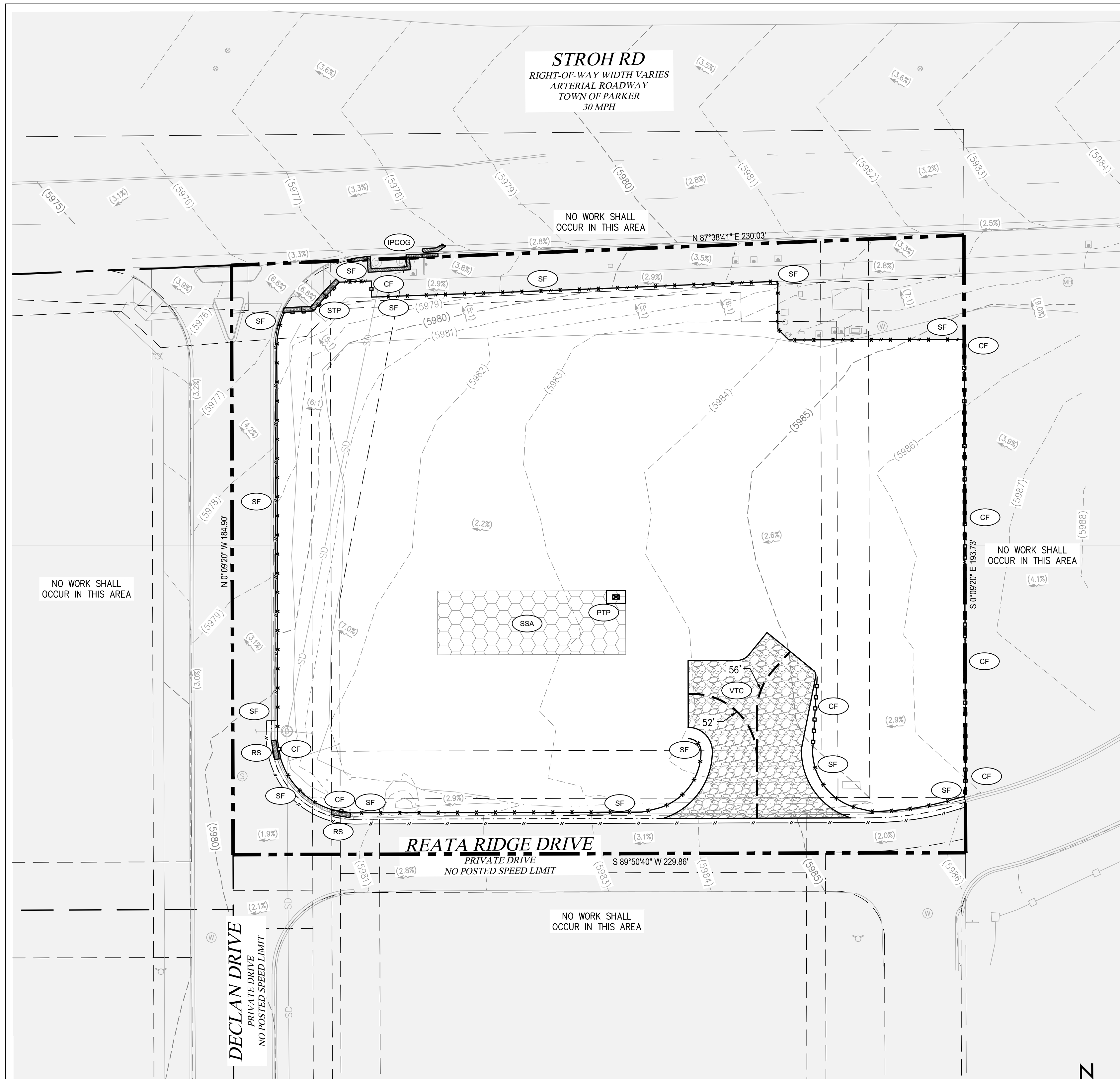
PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
**STROH RD**  
**PARKER, CO #2**



COMM # 4884  
DATE: 05/02/2025  
REVISION  
DATE:

C1.1D

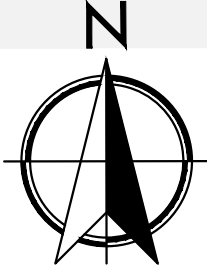
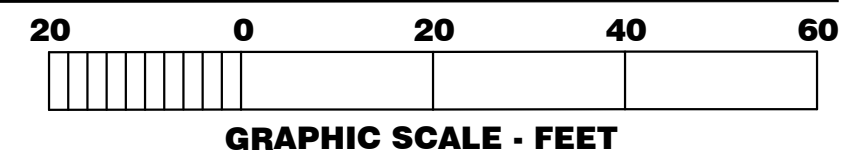
**TIMOTHY M. GUILLOT**  
**ARCHITECT**  
1736 East Sunshine, Suite 417  
Springfield, Missouri 65804  
417.862.0558  
Fax: 417.862.3265  
e-mail: architect@esterlyschneider.com



**1 INITIAL SITE EROSION CONTROL PLAN**

**C1.2** SCALE: 1" = 20'-0"

THIS PLAN HAS BEEN REVISED TO TRANSFER INTERIM/FINAL EROSION CONTROL MEASURES TO A NEW SHEET - SHEET C1.3. ADDITIONAL GENERAL REVISIONS HAVE BEEN MADE TO COMPLY WITH TOWN OF PARKER REQUIREMENTS.



EROSION CONTROL LEGEND		
TITLE	KEY	SYMBOL
AREA OF NO WORK		[Grey Shaded Area]
CONCRETE WASHOUT AREA	CWA	[CWA Symbol]
CONSTRUCTION FENCE	CF	[CF Symbol]
DEBRIS TRASH CONTROL	DTC	[DTC Symbol]
EROSION CONTROL BLANKET ON SLOPES GREATER THAN 3:1 OR WHERE SPECIFIED ON PLAN	ECB	[ECB Symbol]
INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT	IPAN	[IPAN Symbol]
INLET PROTECTION, CURB ON-GRADE, TYPE R INLET	IPCOG	[IPCOG Symbol]
MASONRY WORK PROTECTION	MWP	[MWP Symbol]
PORTABLE TOILET PROTECTION	PTP	[PTP Symbol]
ROCK SOCK	RS	[RS Symbol]
SEDIMENT CONTROL LOG	SCL	[SCL Symbol]
SEEDING, MULCHING AND CRIMPING	SMC	[SMC Symbol]
SIDEWALK TRANSITION PROTECTION	STP	[STP Symbol]
SILT FENCE	SF	[SF Symbol]
STABILIZED STAGING AREA	SSA	[SSA Symbol]
SURFACE ROUGHENING	SR	[SR Symbol]
VEHICLE TRACKING CONTROL	VTC	[VTC Symbol]

- ### GENERAL PHASING NOTES
- PHASE 1:(PRE-CONSTRUCTION)**
- INSTALLATION OF EROSION CONTROL SILT FENCE.
  - DEMOLITION OF EXISTING SITE IMPROVEMENTS.
  - REMOVAL OF EXISTING PAVEMENT ON THE SITE.
- SILT FENCE**
- CONSTRUCTION VEHICLE TRACKING CONTROL
  - STABILIZED STAGING AREA
  - SANITARY FACILITIES
  - ROCK SOCKS
- PHASE 2:(DURING GRADING, AND PAVING)**
- INSTALLATION OF ALL STORM WATER DRAINAGE IMPROVEMENTS.
  - ROUGH GRADING.
  - CONSTRUCTION OF NEW SITE IMPROVEMENTS.
  - FINAL GRADING.
  - CONCRETE WASHOUT AREA PRIOR TO ANY CONCRETE WORK
  - SEEDING AND MULCHING STOCKPILES OVER 7-DAYS IDLE
  - INLET PROTECTION FOR PROPOSED ONCE INSTALLED
- PHASE 3:(POST PAVING)**
- PLACEMENT OF FINAL LANDSCAPING ITEMS AND SOD.
  - REMOVAL OF EROSION CONTROL FENCE.
  - FINAL LANDSCAPE INSTALLATION PER LANDSCAPE PLANS
  - REMOVE REMAINING EROSION CONTROL BMP'S ONCE STABILIZED

- ### EROSION GENERAL NOTES
- THE VTC PAD FOR A CWA DOES NOT NEED TO CONFORM TO THE FORMAL VTC DETAIL.
  - THE TRUE LOCATION OF THE CWA MAY BE DETERMINED BY THE TOWN AND THE ECS.
  - LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN COMPLETION OF LANDSCAPING IS NOT POSSIBLE AND PRIOR TO ISSUANCE OF A TEMPORARY CERTIFICATE OF OCCUPANCY.
  - THE TRUE LOCATION OF THE PORTABLE TOILET PROTECTION (PTP) MAY BE DETERMINED BY THE TOWN AND THE ECS.
  - MASONRY WORK PROTECTION IS REQUIRED FOR CONSTRUCTION OF BUILDINGS.
  - ANY ON-SITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN OF PARKER PRIOR TO INSTALLATION.
  - REFER TO SHEET C1.4 FOR COMPLETE LIST OF EROSION CONTROL GENERAL NOTES.

PREPARED UNDER THE SUPERVISION OF TAIT & ASSOCIATES, INC.

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 Fax: 417.862.3265  
 e-mail: architect@esteryschneider.com

**LOT 1 PARKER POINTE SUBDIVISION FILING 1**

PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
**STROTH RD**  
**PARKER, CO #2**

**INITIAL SITE EROSION CONTROL PLAN**

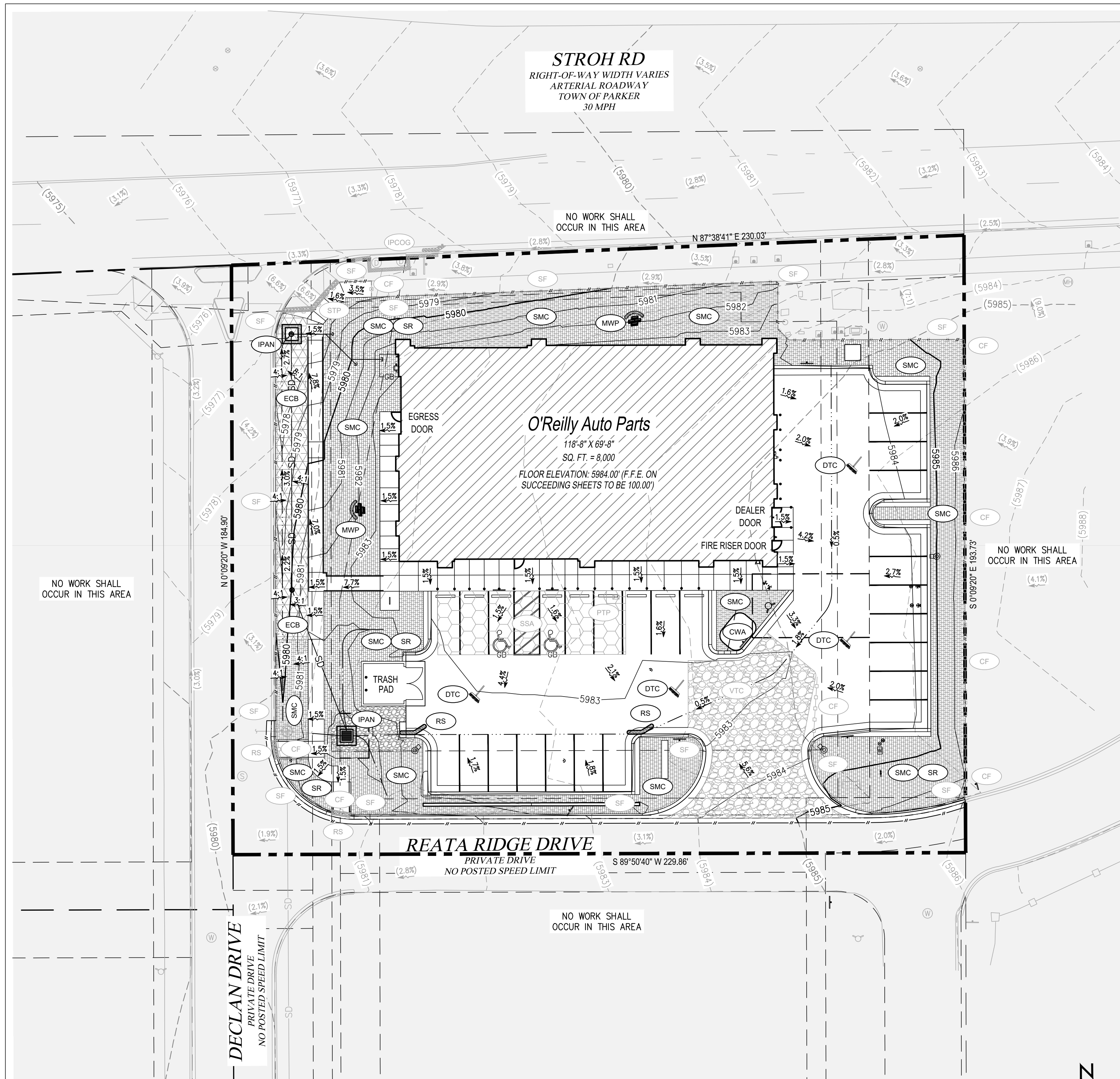
**O'Reilly AUTO PARTS**

CORPORATE OFFICES  
 233 SOUTH PATTERSON  
 SPRINGFIELD, MISSOURI 65802  
 (417) 862-2674 TELEPHONE

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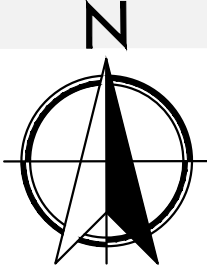
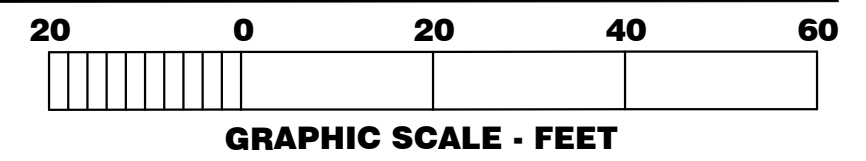
THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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**1 FINAL SITE EROSION CONTROL PLAN**

**C1.3** SCALE: 1" = 20'-0"



### EROSION CONTROL LEGEND

TITLE	KEY	SYMBOL
AREA OF NO WORK		
CONCRETE WASHOUT AREA	(CWA)	
CONSTRUCTION FENCE	(CF)	
DEBRIS TRASH CONTROL	(DTC)	
EROSION CONTROL BLANKET ON SLOPES GREATER THAN 3:1 OR WHERE SPECIFIED ON PLAN	(ECB)	
INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT	(IPAN)	
INLET PROTECTION, CURB ON-GRADE, TYPE R INLET	(IPCOG)	
MASONRY WORK PROTECTION	(MWP)	
PORTABLE TOILET PROTECTION	(PTP)	
ROCK SOCK	(RS)	
SEDIMENT CONTROL LOG	(SCL)	
SEEDING, MULCHING AND CRIMPING	(SMC)	
SIDEWALK TRANSITION PROTECTION	(STP)	
SILT FENCE	(SF)	
STABILIZED STAGING AREA	(SSA)	
SURFACE ROUGHENING	(SR)	
VEHICLE TRACKING CONTROL	(VTC)	

- ### GENERAL PHASING NOTES
- PHASE 1:(PRE-CONSTRUCTION)**
- INSTALLATION OF EROSION CONTROL SILT FENCE.
  - DEMOLITION OF EXISTING SITE IMPROVEMENTS.
  - REMOVAL OF EXISTING PAVEMENT ON THE SITE.
  - SILT FENCE
  - CONSTRUCTION VEHICLE TRACKING CONTROL
  - STABILIZED STAGING AREA
  - SANITARY FACILITIES
  - ROCK SOCKS
- PHASE 2:(DURING GRADING, AND PAVING)**
- INSTALLATION OF ALL STORM WATER DRAINAGE IMPROVEMENTS.
  - ROUGH GRADING.
  - CONSTRUCTION OF NEW SITE IMPROVEMENTS.
  - FINAL GRADING.
  - CONCRETE WASHOUT AREA
  - SEEDING AND MULCHING
  - STOCKPILES OVER 7-DAYS IDLE
  - INLET PROTECTION FOR PROPOSED ONCE INSTALLED
- PHASE 3:(POST PAVING)**
- PLACEMENT OF FINAL LANDSCAPING ITEMS AND SOD.
  - REMOVAL OF EROSION CONTROL FENCE.
  - FINAL LANDSCAPE INSTALLATION PER LANDSCAPE PLANS
  - REMOVE REMAINING EROSION CONTROL BMP'S ONCE STABILIZED

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  - ANY ON-SITE BULK FUEL STORAGE REQUIRES A FIRE LIFE SAFETY PERMIT FROM THE TOWN OF PARKER PRIOR TO INSTALLATION.
  - REFER TO SHEET C1.4 FOR COMPLETE LIST OF EROSION CONTROL GENERAL NOTES.

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Sacramento Denver  
San Luis Obispo Riverside Boise

Steven P. Bunch, PE 6575

LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
**STROTH RD**  
**PARKER, CO #2**

**FINAL SITE EROSION CONTROL PLAN**

**TIMOTHY M. GUILLOT**  
**ARCHITECT**

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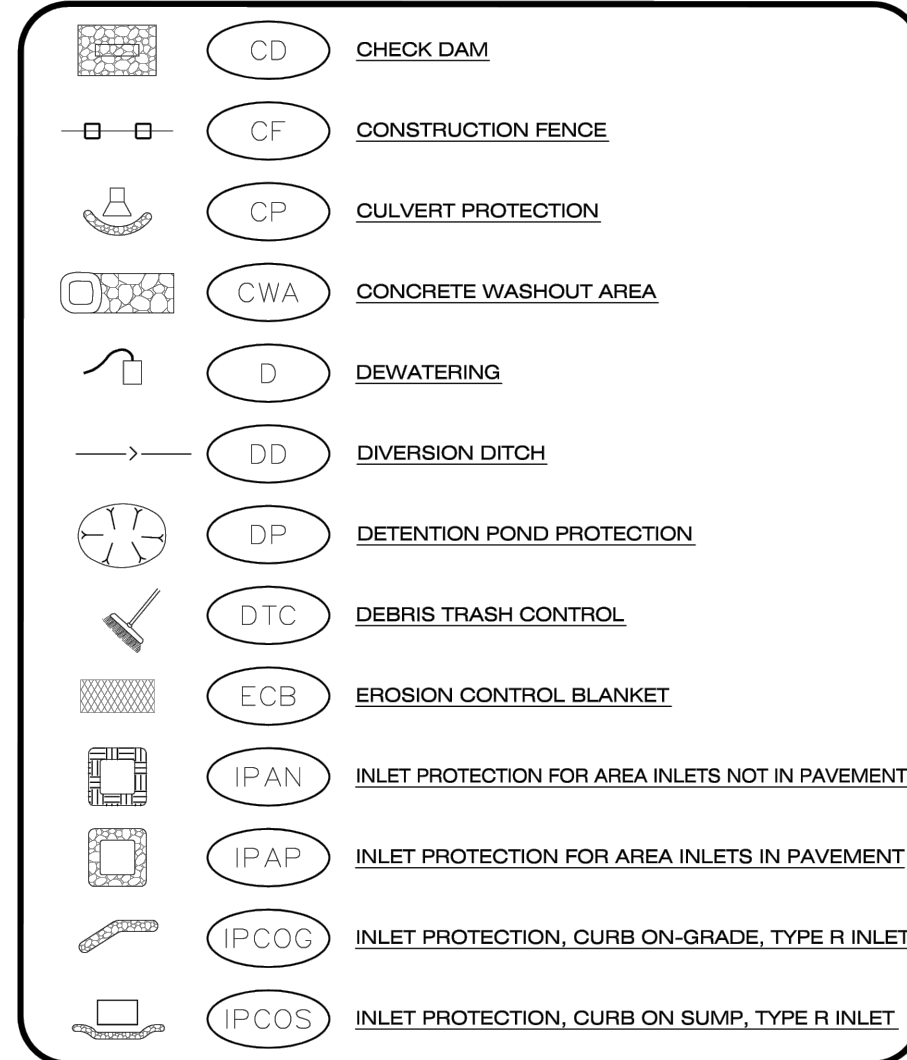
TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORK

DATE

C1.3

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**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **LEGEND** 1 OF 3 Oct. 2012

25. ALL CHEMICAL OR HAZARDOUS MATERIAL SPILLS, INCLUDING CONCRETE WASHOUT WATER, WHICH MAY ENTER WATER OF THE STATE OF COLORADO, WHICH INCLUDES BUT IS NOT LIMITED TO SURFACE WATER, GROUND WATER, DRY GULCHES OR STORM SEWERS LEADING TO SURFACE WATER, SHALL BE IMMEDIATELY REPORTED TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE) PER 25-6-601, AND THE TOWN OF PARKER, RELEASES OF PETROLEUM PRODUCTS AND CERTAIN HAZARDOUS SUBSTANCES LISTED UNDER THE FEDERAL CLEAN WATER ACT (40 CFR PART 116) MUST BE REPORTED TO THE NATIONAL RESPONSE CENTER AND THE CDPHE. SPILLS THAT POSE AN IMMEDIATE SAFETY HAZARD SHALL BE REPORTED TO 911.

26. THE CLEANING OF CONCRETE TRUCKS AND EQUIPMENT IS RESTRICTED TO THE APPROVED CONCRETE WASHOUT LOCATION ON THE JOB SITE. CONCRETE WASH WATER SHALL NOT BE DISCHARGED TO STATE WATER OR STORM SEWER SYSTEMS.

27. VEHICLE AND EQUIPMENT DEGRASSING IS PROHIBITED ON THE JOB SITE.

28. ALL DEWATERING ON SITE SHALL BE COORDINATED WITH THE TOWN'S INSPECTOR. A STATE PERMIT MAY BE REQUIRED FOR DEWATERING. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR OBTAINING AND ADHERING TO ALL APPLICABLE PERMITS.

29. HYDRAULIC SEEDING AND/OR HYDRAULIC MULCHING ARE ONLY ALLOWED IN AREAS UNDER TEMPORARY OR PERMANENT IRRIGATION OR FOR THE PURPOSE OF TEMPORARY SOIL STABILIZATION.

30. APPLICABLE CONSTRUCTION BMPs SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL ALL LANDSCAPING HAS BEEN INSTALLED AND THE DESIRABLE VEGETATION HAS REACHED A POINT IN WHICH EROSION AND SEDIMENTATION IS NO LONGER A CONCERN AS DETERMINED BY THE TOWN'S INSPECTOR.

31. GRADING SECURITY RELEASE REQUIREMENTS:

- 1) DEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H OR ITEM I (BELOW).
- A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
- B. UNIFORM PERENNIAL VEGETATION COVER HAS BEEN ESTABLISHED WITH AN INDIVIDUAL PLANT DENSITY OF AT LEAST SEVENTY PERCENT (70%) OF PRE-DISTURBANCE LEVELS.
- C. ALL CBMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
- D. IF ANY EROSION IS PRESENT, IT IS INSIGNIFICANT AND IS NOT LEAVING THE SITE AND/OR LEADING INTO ANY ON-SITE DRAINAGE INFRASTRUCTURE THAT MAY CONVERT SURFACE WATER OFF SITE.
- E. WEEDS REPRESENT NO MORE THAN FIFTY PERCENT (50%) OF THE TOTAL VEGETATION ON THE SITE.
- F. NO WEEDS ARE PRESENT FROM LIST A OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
- G. THE SITE IS PREDOMINANTLY FREE OF WEEDS FROM LIST B OF THE COLORADO NOXIOUS WEED LIST, AS AMENDED.
- H. WEEDS ARE EVENLY DISTRIBUTED THROUGHOUT THE SITE WITH NO LARGE CONCENTRATIONS PRESENT.
- I. A NEW GRADING PERMIT AND REPLACEMENT SECURITY HAS BEEN SUBMITTED AND APPROVED BY THE TOWN OF PARKER MUNICIPAL CODE. IT IS THE PROPERTY OWNER'S OBLIGATION AT THE TIME OF CLOSING ON THE SALE OF A SITE THAT IS SUBJECT TO A GRADING PERMIT TO ENSURE THAT THE NEW PROPERTY OWNER HAS PROVIDED THE TOWN WITH A REPLACEMENT SECURITY.
- J. ALL KNOWN DRAINAGE ISSUES ASSOCIATED WITH THE PROJECT HAVE BEEN MITIGATED AND A SUFFICIENT AMOUNT OF TIME HAS PASSED TO ENSURE THAT SUCH ISSUES HAVE BEEN CORRECTED. THE REQUIREMENT DOES NOT INCLUDE THOSE DRAINAGE ISSUES ORIGINATING ON RESIDENTIAL LOTS.

(K) NOXIOUS WEEDS MUST BE CONTROLLED AS PROVIDED UNDER STATE LAW AND SECTION 6.01.263 OF THE TOWN OF PARKER MUNICIPAL CODE. FAILURE TO CONTROL NOXIOUS WEEDS ON THE SITE MAY CONSTITUTE A NUISANCE, SUBJECT TO THE PENALTIES CONTAINED IN THE CODE.

**DEFINITIONS:**

**DEVELOPABLE PROPERTY** MEANS ANY LAND THAT HAS BEEN GRADED AND IS PART OF A PLATTED LOT OR PLATTED TRACT OF RECORD, THAT WAS PLATTED FOR FUTURE DEVELOPMENT, INCLUDING RESIDENTIAL, HOME CONSTRUCTION OR PUBLIC IMPROVEMENTS.

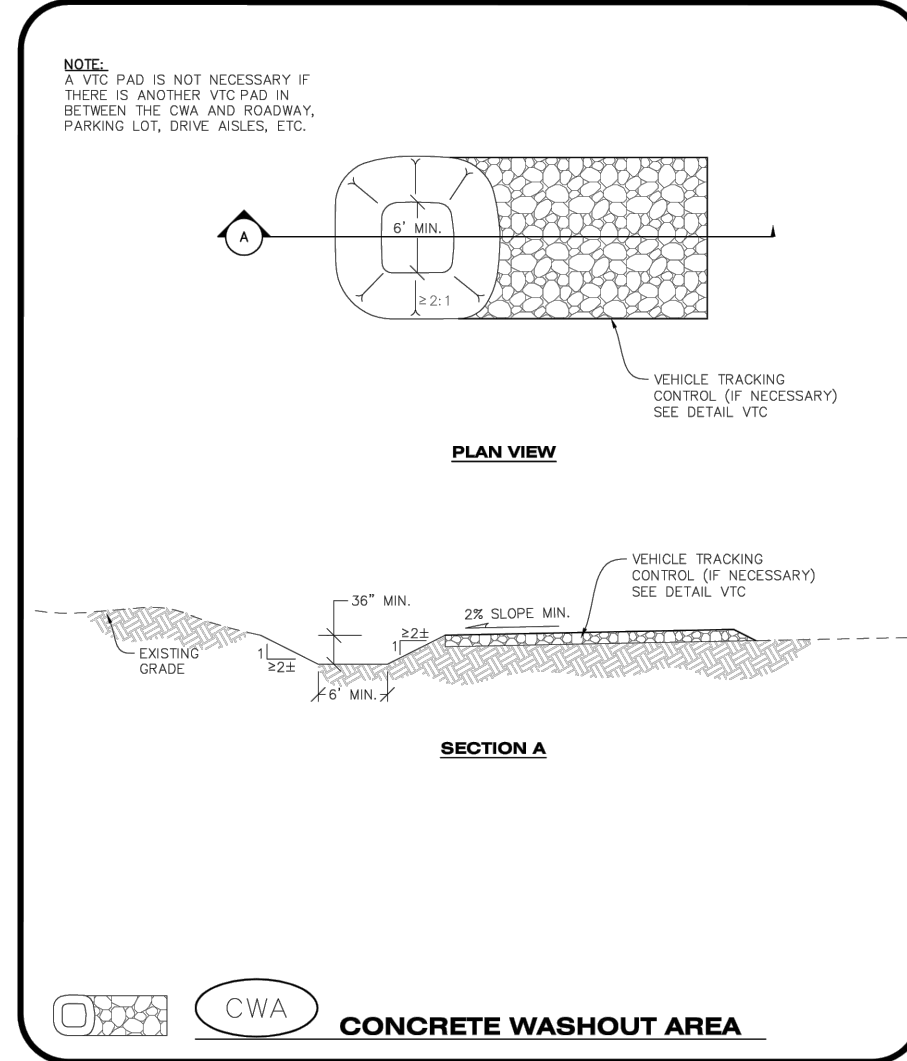
**NONDEVELOPABLE PROPERTY** MEANS LAND THAT HAS BEEN GRADED AND WILL NOT BE FURTHER DEVELOPED. THIS INCLUDES BUT IS NOT LIMITED TO PARKS, OPEN SPACE, HOMEOWNERS' ASSOCIATION OR BUSINESS ASSOCIATION PLATTED TRACTS, RESTORATION PLOTS AND DRAINAGE WAYS.

32. FAILURE TO COMPLY WITH ANY OF THE REQUIREMENTS DESCRIBED IN THIS SECTION MAY RESULT IN THE ISSUANCE OF A NOTICE OF VIOLATION, A STOP WORK ORDER, A STOP WORK ORDER AND/OR THE REMEDIES/PENALTIES DESCRIBED IN CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE.

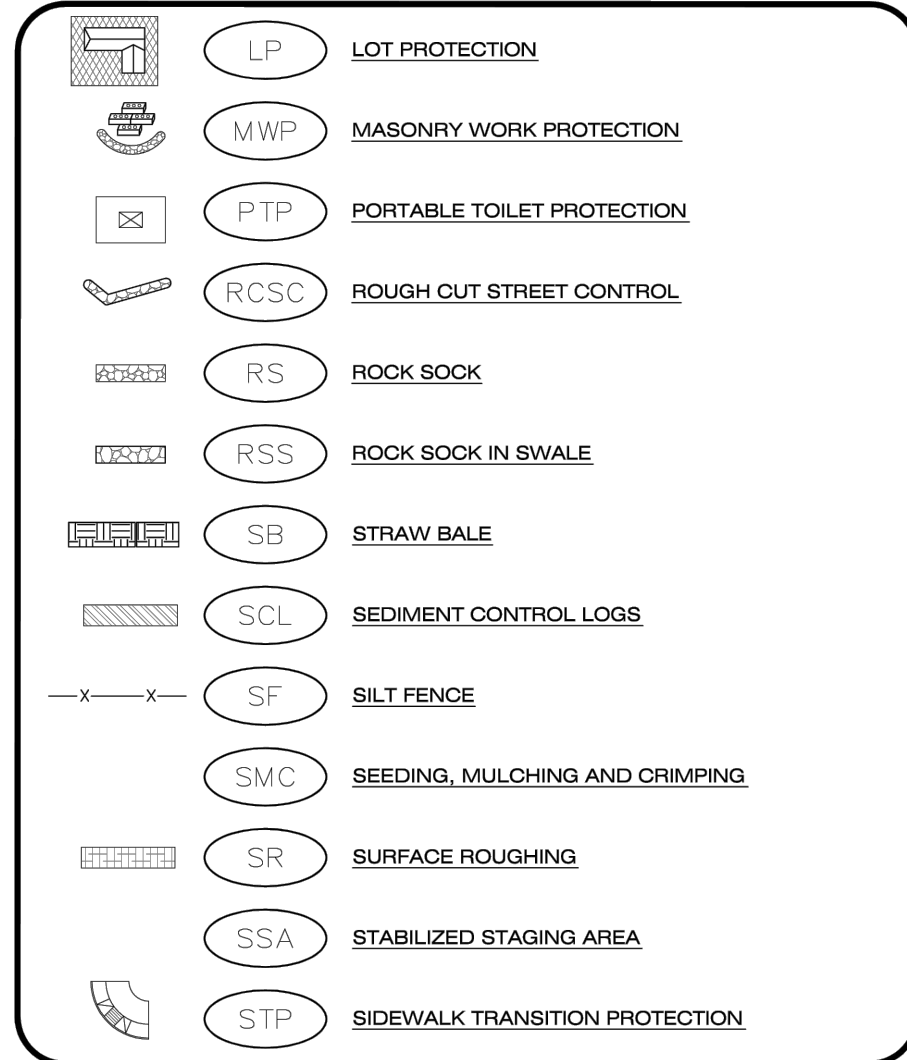
33. ANY PERSON CONVICTED OF VIOLATING ANY PROVISION OF THE TOWN OF PARKER, GRADING & EARTH MOVEMENT SECTION OF THE MUNICIPAL CODE SHALL BE QUALIFIED TO BE RE-EVALUATED AND A FINE OR CONVICTION, BE PUNISHED BY A FINE OF NOT MORE THAN FOUR HUNDRED NINETY NINE DOLLARS (\$499) FOR EACH VIOLATION. EACH DAY A VIOLATION CONTINUES SHALL CONSTITUTE A SEPARATE OFFENSE. THE TOWN ALSO MAY SEEK AN INJUNCTION, ABATEMENT, RESTITUTION OR ANY OTHER REMEDY TO PREVENT, STOP, ABATE OR REMOVE THE VIOLATION. A PERSON CONVICTED OF VIOLATING CHAPTER 11.10 OF THE TOWN OF PARKER MUNICIPAL CODE SHALL BE ELIGIBLE FOR THE RESTRICTION OF RE-EVALUATING THE PROPERTY IF THE CODE MAY BE REVOKED.

34. THE TOWN OF PARKER RESERVES THE RIGHT TO ALLOW MODIFICATIONS AND SUBSTITUTIONS TO THE CBMP NOTES AND DETAILS WHEN SUCH MODIFICATIONS AND SUBSTITUTIONS OFFER THE SAME LEVEL OF PROTECTION AS THE STANDARD REQUIREMENTS BASED UPON THE SPECIFIC SITUATION, AS DETERMINED BY TOWN STAFF. DUE TO THE INDEFINITE AND REGULARITY OF SUCH MODIFICATIONS AND SUBSTITUTIONS, THE APPROVAL OF SUCH VARIATIONS MAY NOT BE DOCUMENTED BY TOWN STAFF.

**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 1 OF 4 Oct. 2012



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CWA** 1 OF 2 Oct. 2012



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **LEGEND** 2 OF 3 Oct. 2012

2) NONDEVELOPABLE PROPERTY: IN ORDER FOR THE GRADING SECURITY TO BE RELEASED, THE SITE MUST MEET ITEMS A-H AND J, OR ITEMS I AND J (BELOW).

- A. ALL SOIL-DISTURBING ACTIVITIES ASSOCIATED WITH THE GRADING PERMIT HAVE PERMANENTLY CEASED.
- B. ALL CBMPs HAVE BEEN PROPERLY REMOVED FROM THE SITE.
- C. UNIFORM PERENNIAL VEGETATION COVER HAS BEEN ESTABLISHED WITH AN INDIVIDUAL PLANT DENSITY OF AT LEAST SEVENTY PERCENT (70%) OF PRE-DISTURBANCE LEVELS.
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**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 2 OF 4 Oct. 2012

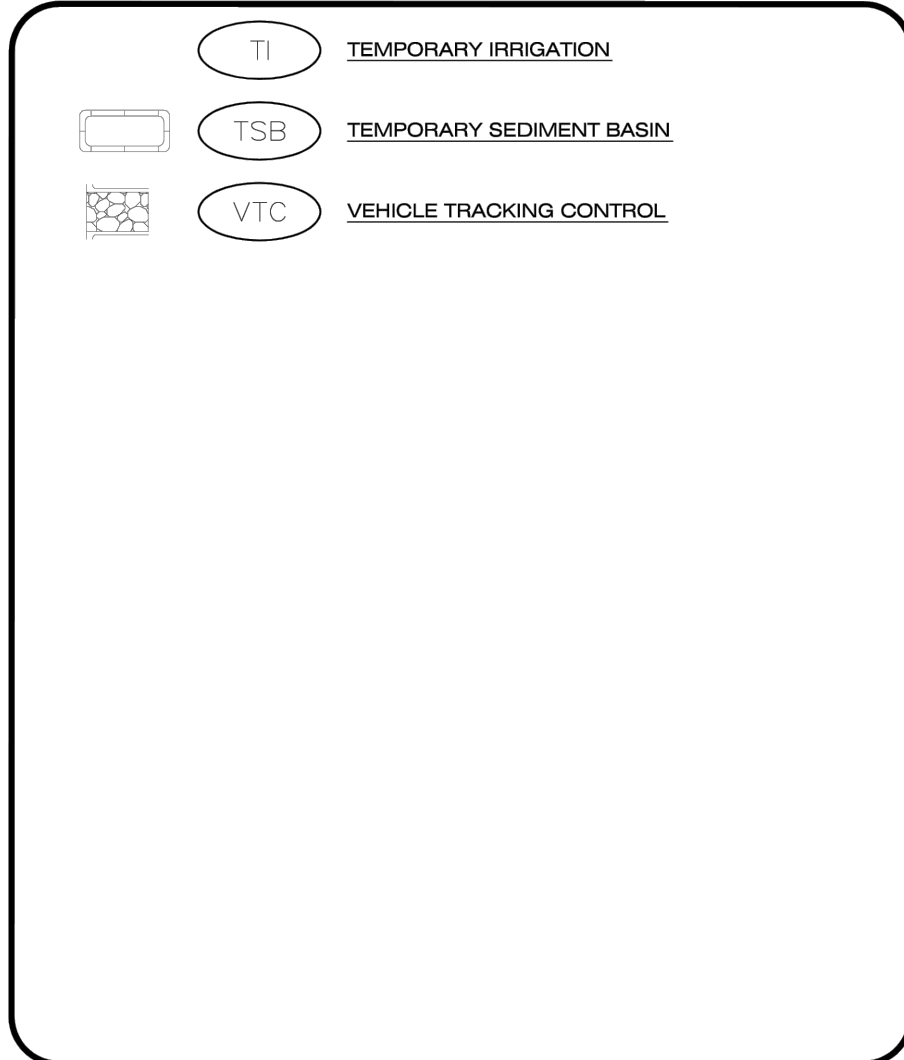
**CONCRETE WASHOUT AREA INSTALLATION NOTES**

1. CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
2. A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
3. IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.

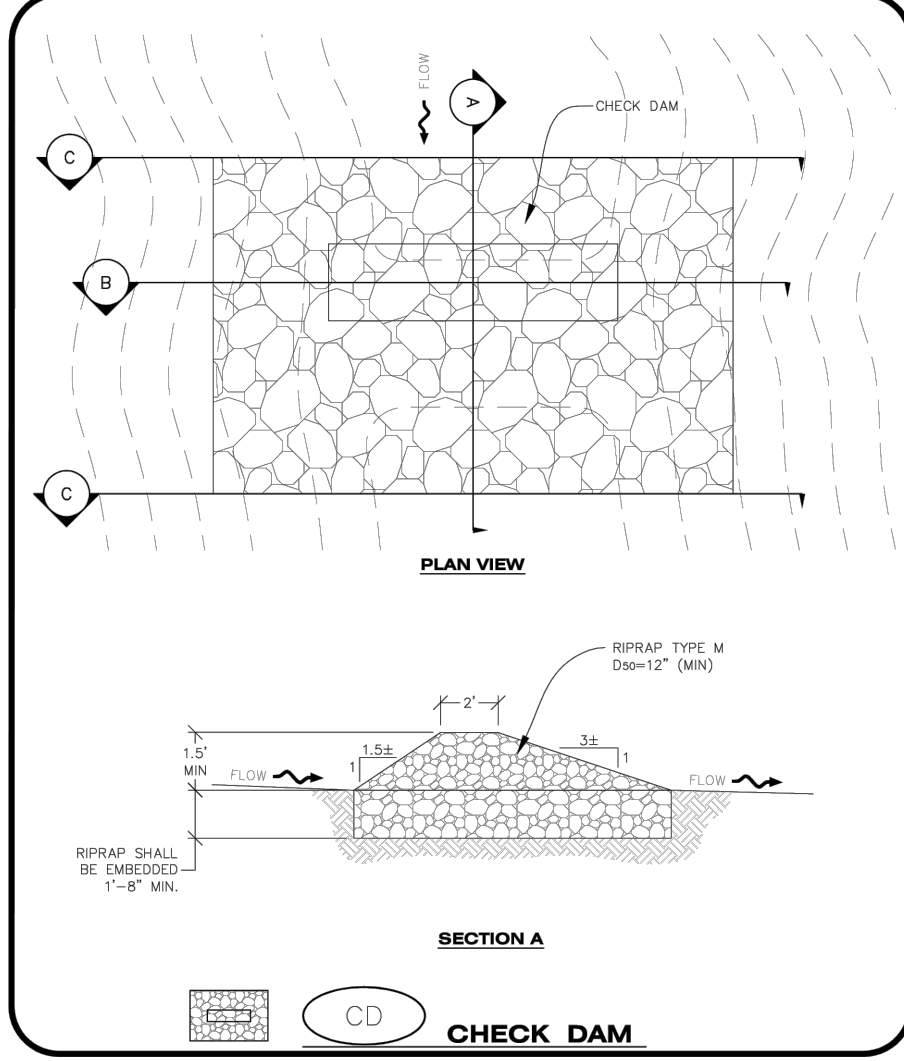
**CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
2. CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
3. CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
4. CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
5. CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
6. WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDING, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

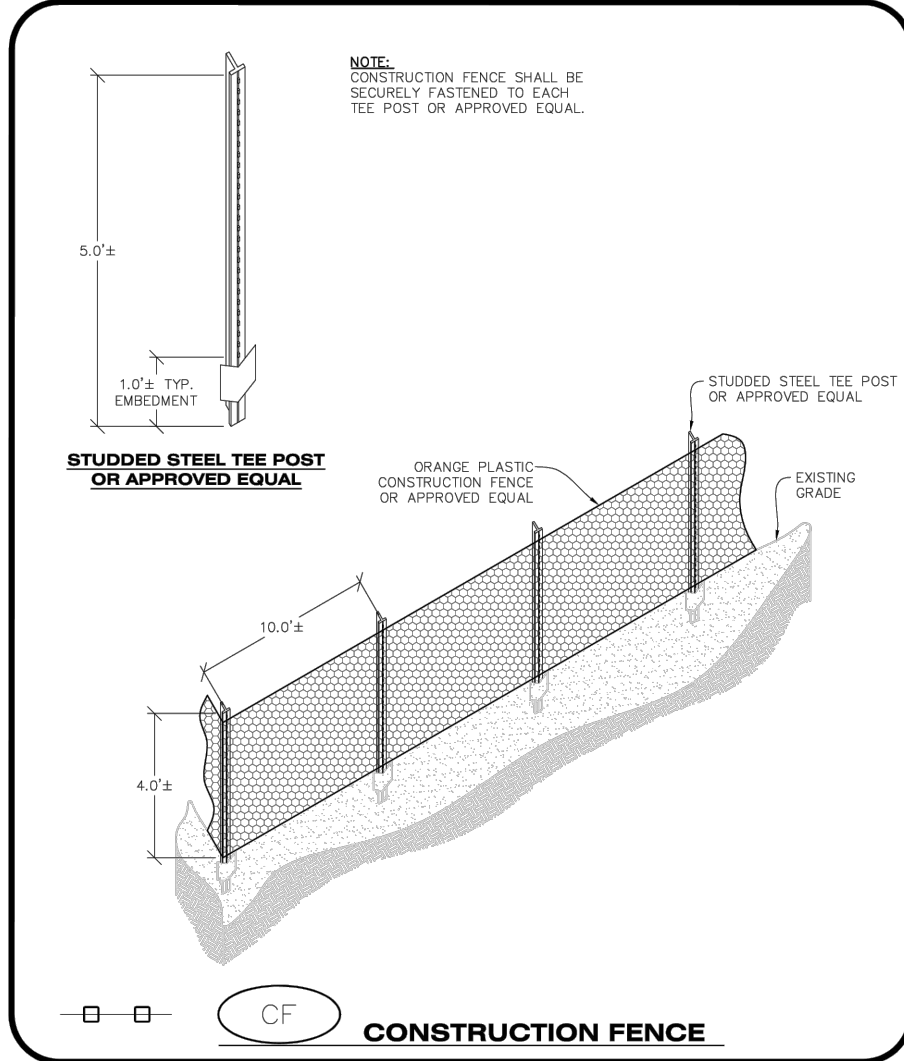
**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CWA** 2 OF 2 Oct. 2012



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **LEGEND** 3 OF 3 Oct. 2012



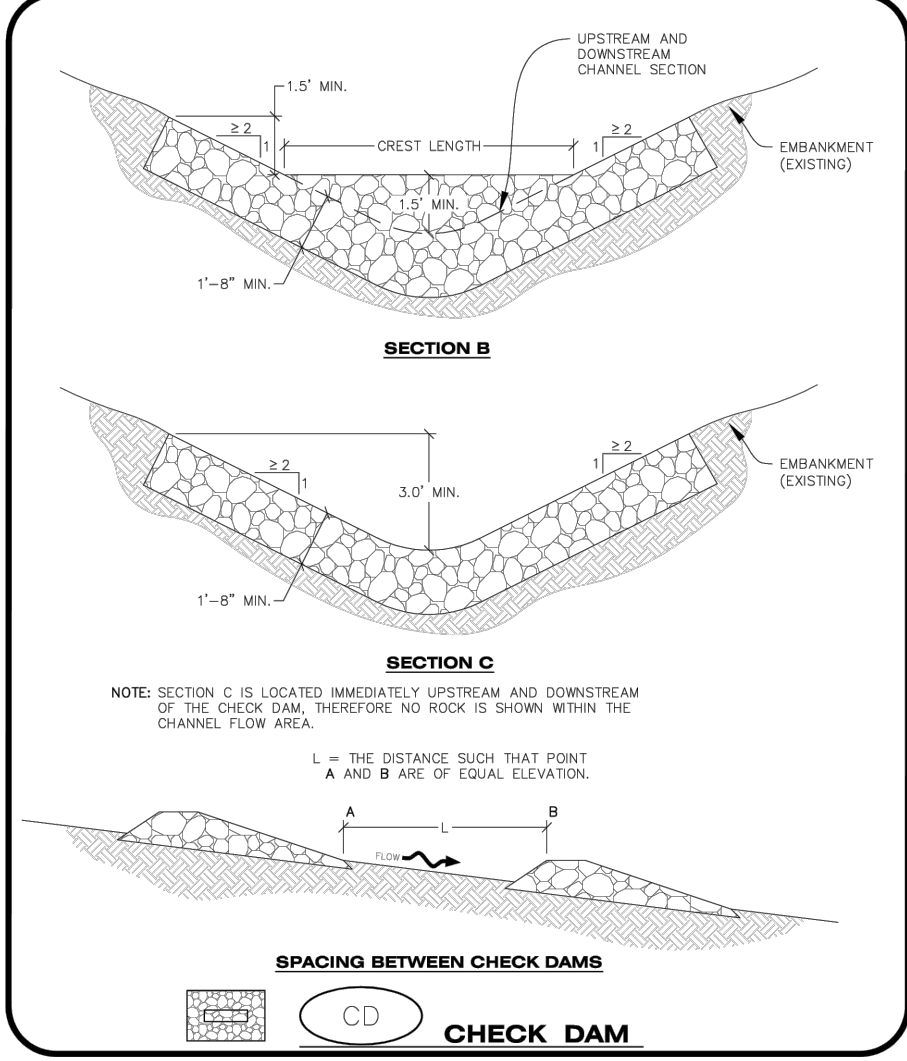
**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CD** 1 OF 3 Oct. 2012



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CF** 1 OF 2 Oct. 2012

1. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
2. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.
3. ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE CBMP PLAN INSTALLATION NOTES AND DETAILS AND/OR PERMITTED VARIANCES. ALL MATERIALS, WORKMANSHIP AND INSTALLATION METHODS ARE SUBJECT TO INSPECTION BY THE TOWN'S INSPECTOR. THE TOWN OF PARKER RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS, WORKMANSHIP AND/OR INSTALLATION METHODS THAT DO NOT CONFORM TO THE CBMP PLAN AND/OR PERMITTED VARIANCES.
4. THE EROSION CONTROL SUPERVISOR SHALL REVISE OR MODIFY THE EROSION AND SEDIMENT CONTROL MEASURES IF IT BECOMES APPARENT THAT THE ORIGINAL PLAN IS INADEQUATE, OR AS A RESULT OF DEFICIENCIES IDENTIFIED DURING INSPECTIONS PERFORMED BY THE TOWN'S INSPECTOR.
5. THE INSTALLATION OF ADDITIONAL EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs) MAY BE REQUIRED IF THE EROSION CONTROL SUPERVISOR, PROPERTY OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS AT ANY TIME THROUGHOUT THE DURATION OF THE PROJECT CONSTRUCTION AND/OR SUBSEQUENT REVEGETATION PERIOD.
6. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE SITE REMAINS IN COMPLIANCE WITH THE NOTICE OF NUISANCE REPORTS (S.A.O. CONSTRUCTION SITE RUNOFF CONTROL INSPECTION REPORTS), APPROVED CBMP PLANS(S) AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
7. THE EROSION CONTROL SUPERVISOR SHALL BE READY AVAILABLE TO DISCUSS AND CORRECT ANY PROBLEMS THAT MAY ARISE RELATING TO GRADING, EROSION AND SEDIMENT CONTROL.
8. IF IT IS ANTICIPATED THAT ALL OR A PORTION OF THE PROJECT WILL OCCUR DURING SEASONAL OPERATIONS, AN APPROVED ALTERNATIVE MAY BE NECESSARY TO MINIMIZE DAMAGE FROM THESE OPERATIONS. THE TOWN OF PARKER RESERVES NO RESPONSIBILITY FOR DAMAGE TO ANY BMPs AS A RESULT OF SNOW FLOWING AND SNOW REMOVAL.
9. AREAS OF LAND DISTURBANCE EQUAL TO 40 ACRES OR GREATER SHALL NOT BE EXPOSED FOR MORE THAN 30 CONSECUTIVE DAYS WITHOUT TEMPORARY OR PERMANENT STABILIZATION.
10. AUTHORIZED EXEMPTIONS MAY BE ALLOWED TO THE 40-ACRE LIMIT FOR REMOVAL AND STORAGE OF CUT MATERIAL WHERE (A) GEOTECHNICAL LIMITATIONS RESTRICT THE USE OF TEMPORARY OR PERMANENT STABILIZATION OF THE STORED MATERIAL (E.G. SQUELCHING SOILS, ROCK) AND (B) WHEN THE OWNER CAN DEMONSTRATE THAT THE 40-ACRE LIMIT IS PHYSICALLY AND/OR FINANCIALLY IMPRACTICABLE FOR SITES GRANTED THIS EXEMPTION. A PHASING AND EARTHWORK QUANTITIES PLAN SHALL BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT AND APPROVED PRIOR TO THE COMMENCEMENT OF LAND EXPOSURE ACTIVITIES. SUFFICIENT REQUIREMENTS INCLUDE:
  - (i) PHASING PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
  - (ii) EARTHWORK QUANTITY PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
  - (iii) BMP PLAN SHOWING SPECIFIC EROSION AND SEDIMENT CONTROLS FOR EACH PHASE.

**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 1 OF 4 Oct. 2012



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CD** 2 OF 3 Oct. 2012

**CONSTRUCTION FENCE INSTALLATION NOTES**

1. THE CONSTRUCTION FENCE SHALL BE SECURELY FASTENED TO EACH POST OR APPROVED EQUAL.

**CONSTRUCTION FENCE INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONSTRUCTION FENCE AND MAKE ANY NECESSARY REPAIRS.
2. CONSTRUCTION FENCE SHALL BE REPAIRED WHEN THE FENCING MATERIAL FALLS OUT OF COMPLIANCE WITH THE NOTES AND DETAILS.

**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CF** 2 OF 2 Oct. 2012

11. ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
12. ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.
13. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.
14. BULK STORAGE STRUCTURES FOR PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION TO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.
15. ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.
16. ALL TRASH RECEPTACLES ON SITE SHALL BE EQUIPPED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONTAINED TO THE RECEPTACLE.
17. ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISPOSED ON A DAILY BASIS.
18. ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS, CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.
19. THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT FAILURE TO ABIDE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.
20. ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO FAVORED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.
21. NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.
22. ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL BLANKETS, TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS.
23. THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.
24. A WATER SOURCE SHALL BE AVAILABLE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FLOTTING DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.

**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **GEN NOTES** 2 OF 4 Oct. 2012

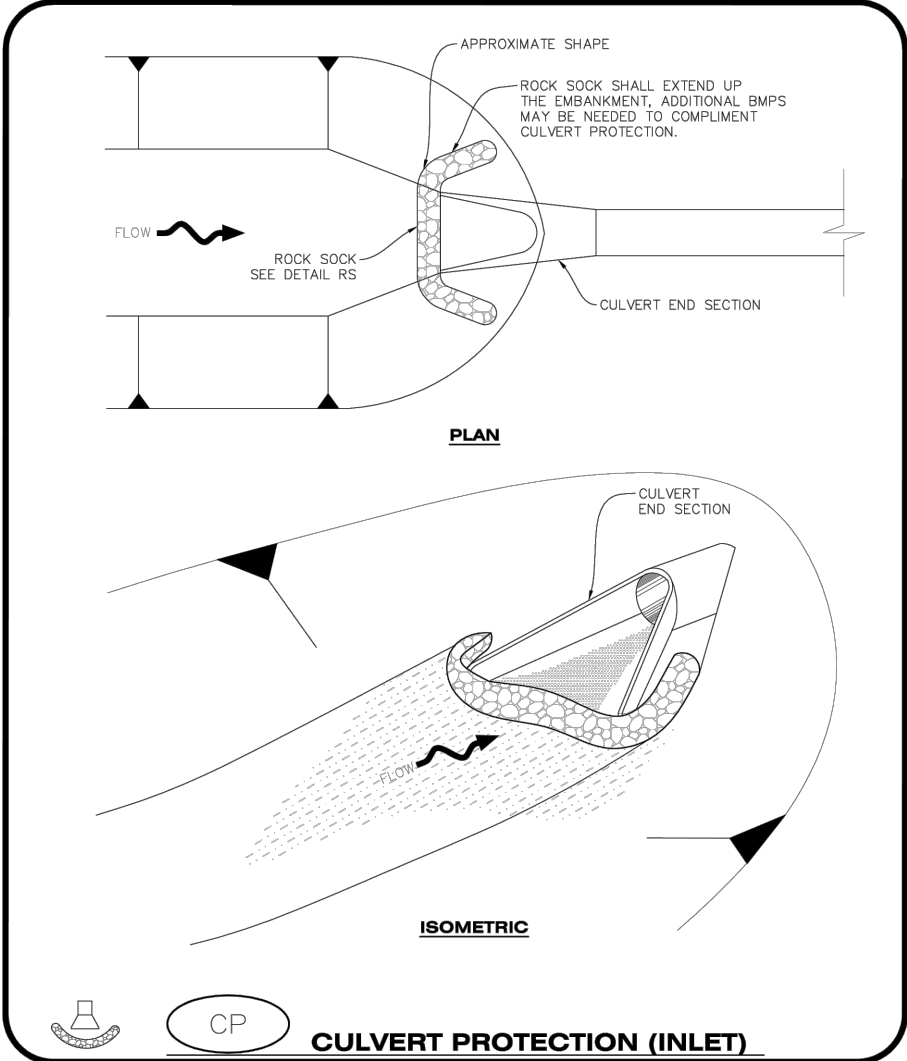
**CHECK DAM INSTALLATION NOTES**

1. SEE PLAN VIEW FOR LOCATION(S) OF CHECK DAMS.
2. CHECK DAMS SHOWN ON CBMP PLAN SHALL BE INSTALLED WHEN DIRECTED BY THE TOWN'S INSPECTOR.
3. RIPPAP UTILIZED FOR CHECK DAMS SHALL HAVE A D50 MEDIAN STONE SIZE OF 12".
4. RIPPAP PAD SHALL BE TRENCHED INTO THE GROUND A MINIMUM OF 1'-8".
5. THE MAXIMUM SPACING BETWEEN CHECK DAMS SHOULD BE SUCH THAT THE BOTTOM OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM AS SHOWN IN THE DETAIL.

**CHECK DAM INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CHECK DAMS.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE CREST OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
3. CHECK DAMS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. WHEN CHECK DAMS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED TOPSOIL AND ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CHECK DAMS BE ROUGHENED, SEEDING, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
5. IN SOME INSTANCES, CHECK DAMS MAY REMAIN IN PLACE PERMANENTLY.

**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CD** 3 OF 3 Oct. 2012



**Parker COLORADO** **CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CP** 1 OF 2 Oct. 2012

PREPARED UNDER THE SUPERVISION OF TAIT & ASSOCIATES, INC.

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PROJECT: LOT 1 PARKER POINTE SUBDIVISION FILING 1

NEW O'REILLY AUTO PARTS STORE  
STROH RD  
PARKER, CO #2

EROSION CONTROL DETAILS

O'Reilly AUTO PARTS

CORPORATE OFFICES  
233 SOUTH PATTERSON  
SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

COMM # 4884  
DATE: 05/02/2025  
REVISION  
DATE:

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY OR CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

**CULVERT PROTECTION (INLET) INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF CULVERT PROTECTION (INLET).
- ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
- ROCK SOCK SHALL BE APPROXIMATELY 12" IN DIAMETER.
- ROCK SOCK SHALL EXTEND ABOVE THE FLOW LINE ELEVATION ON BOTH SIDES OF THE CULVERT END SECTION.

**CULVERT PROTECTION (INLET) INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CULVERT PROTECTION (INLET).
- AT A MINIMUM, ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE ROCK SOCK.
- CULVERT PROTECTION (INLET) IS TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE CULVERT INLET PROTECTION IS REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CULVERT INLET PROTECTION TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**Parker COLORADO** **CBMP** **CP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
04th, 2012

**NOTE:**

- THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRICKLE CHANNELS, AND/OR OTHER IMPROVED SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY. FURTHERMORE, ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.

**Parker COLORADO** **CBMP** **DTC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
04th, 2012

**DEBRIS CONTROL NOTES:**

- A COMBINATION OF SURFACE SCRAPING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THESE AREAS.
- ALL CHEMICAL SPILLS AND/OR STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EXTENT PRACTICABLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VAC-TRUCK.
- ON-SITE PERSONNEL, DELIVERY DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUAL DEBRIS AND TRASH CONTROL.

**Parker COLORADO** **CBMP** **DTC**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
04th, 2012

**Parker COLORADO** **CBMP** **DP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
04th, 2012

**DETENTION POND PROTECTION INSTALLATION NOTES**

- DETENTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).

**DETENTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
- ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
- OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**Parker COLORADO** **CBMP** **DP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
04th, 2012

**NOTE:**

IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE APPROPRIATE MEASURES TO PROTECT PUMPING CAPACITY AND MINIMIZE SEDIMENT. USE OF A PLASTIC 5 GALLON BUCKET IS SUGGESTED BUT NOT REQUIRED. THE CONTRACTOR SHALL NOT HOLD THE TOWN LIABLE FOR DAMAGE TO PUMPING EQUIPMENT REGARDLESS OF METHODS USED.

**Parker COLORADO** **CBMP** **D**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
04th, 2012

**DEWATERING INSTALLATION NOTES**

- IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

**DEWATERING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

**Parker COLORADO** **CBMP** **D**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
04th, 2012

**NOTE:**

DIVERSION DITCH GEOMETRY AND REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM ASSOCIATED CBMP PLANS VIA THE DESIGN ENGINEER. IF THE INFORMATION IS NOT PROVIDED ON THE CBMP PLANS, THE CONTRACTOR SHALL NOTIFY THE TOWN PRIOR TO CONSTRUCTION.

**Parker COLORADO** **CBMP** **DD**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
04th, 2012

**DIVERSION DITCH INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
- A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
- ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
- THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
- THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

**DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
- DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
- DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

**Parker COLORADO** **CBMP** **DD**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
04th, 2012

**NOTE:**

WHEN NECESSARY, ALL SEEDING SHALL BE COMPLETED PRIOR TO THE PLACEMENT OF EROSION CONTROL BLANKETS. PLEASE SEE DETAIL SMC FOR SEEDING, MULCHING, AND CRIMPING REQUIREMENTS.

**Parker COLORADO** **CBMP** **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 3  
04th, 2012

**NOTE:**

- WOOD STAKES SHALL BE INSTALLED SUCH THAT ONLY 1" IS EXPOSED FROM THE GROUND.
- WHEN NECESSARY, ALL SEEDING SHALL BE COMPLETED PRIOR TO THE PLACEMENT OF EROSION CONTROL BLANKETS. PLEASE SEE DETAIL SMC FOR SEEDING, MULCHING, AND CRIMPING REQUIREMENTS.

**Parker COLORADO** **CBMP** **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 3  
04th, 2012

**EROSION CONTROL BLANKET INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
- EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THESE CALCULATIONS, TUB REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.
- IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.
- EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.
- ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
- ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELISOR.

MANUFACTURER	PRODUCT NAME
NOVA AMERICAN GREEN	ES150
APPROVED EQUAL	APPROVED EQUAL

**EROSION CONTROL BLANKET MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

**Parker COLORADO** **CBMP** **ECB**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 3 OF 3  
04th, 2012

**AREA INLET PROTECTION INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
- THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
- AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

**Parker COLORADO** **CBMP** **IPA**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 1  
04th, 2012

**Parker COLORADO** **CBMP** **IPAP**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 1  
04th, 2012

**AREA INLET PROTECTION INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
- THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
- AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

**Parker COLORADO** **CBMP** **IPA**  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 1  
04th, 2012

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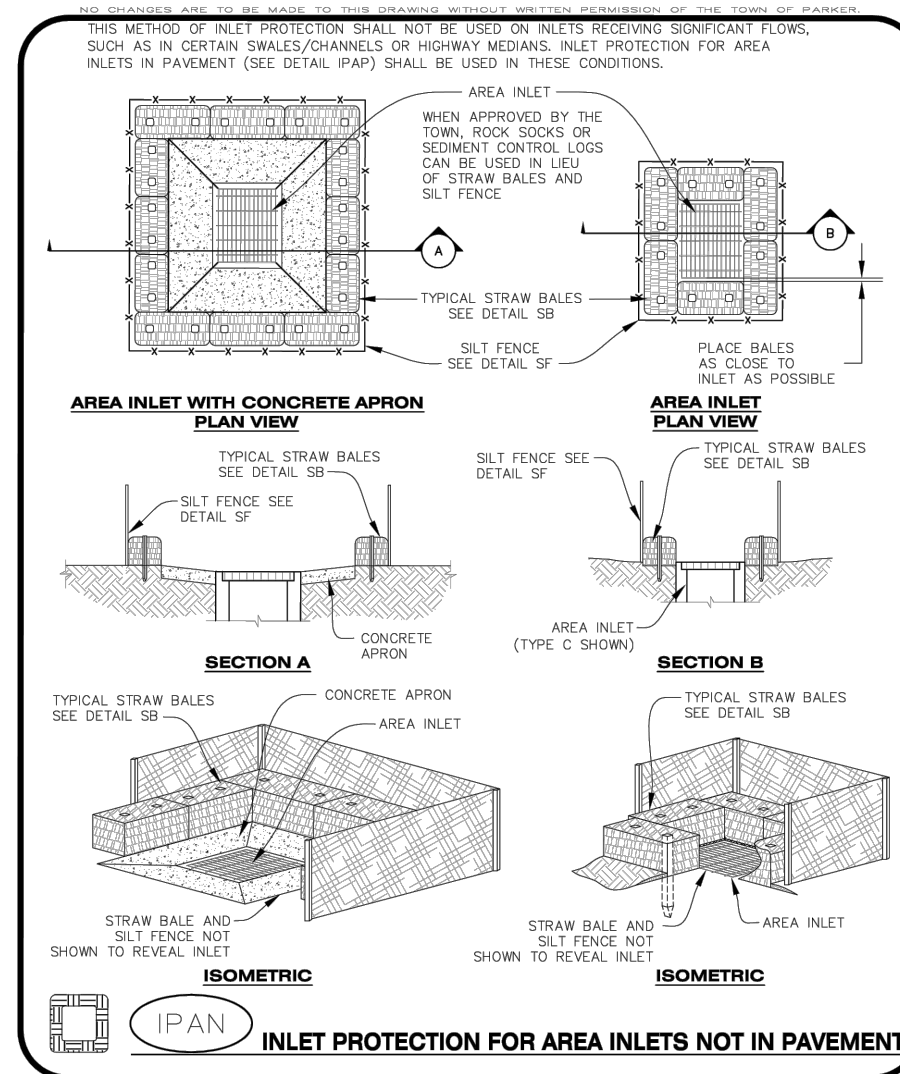
LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
**STROH RD**  
**PARKER, CO #2**

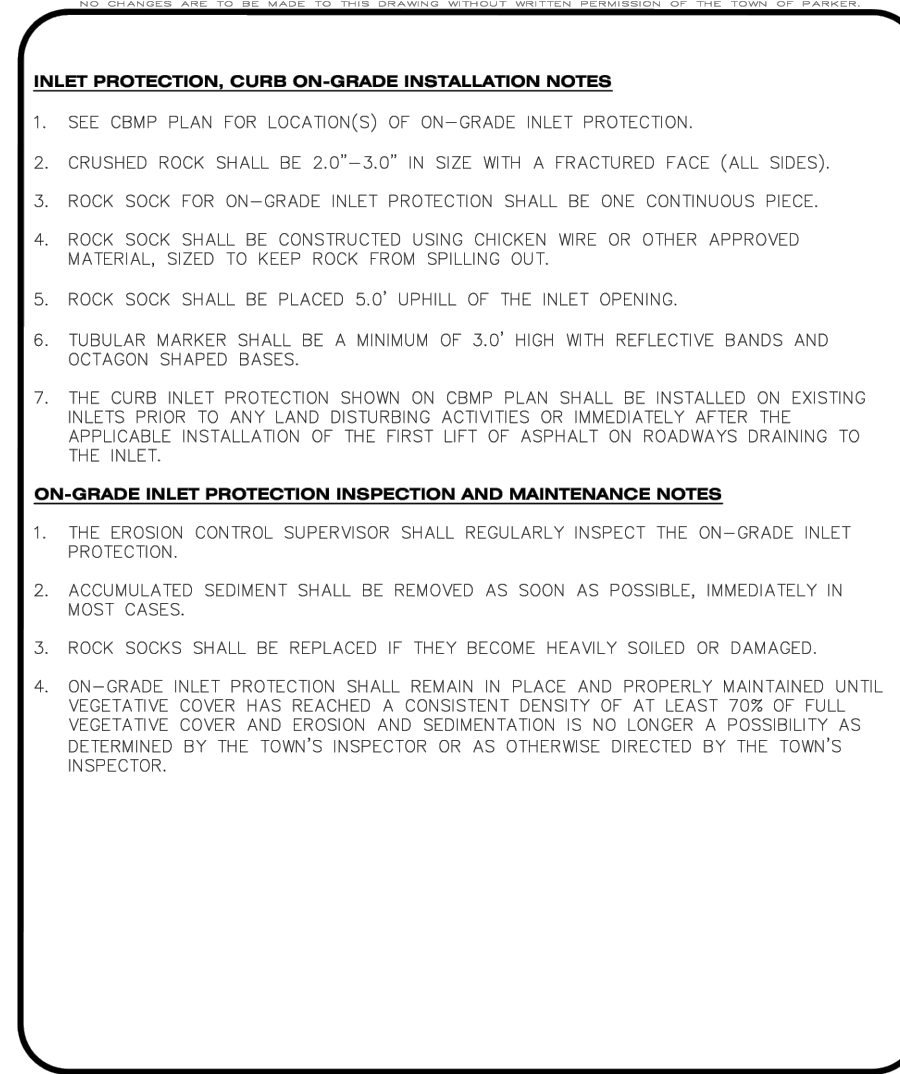
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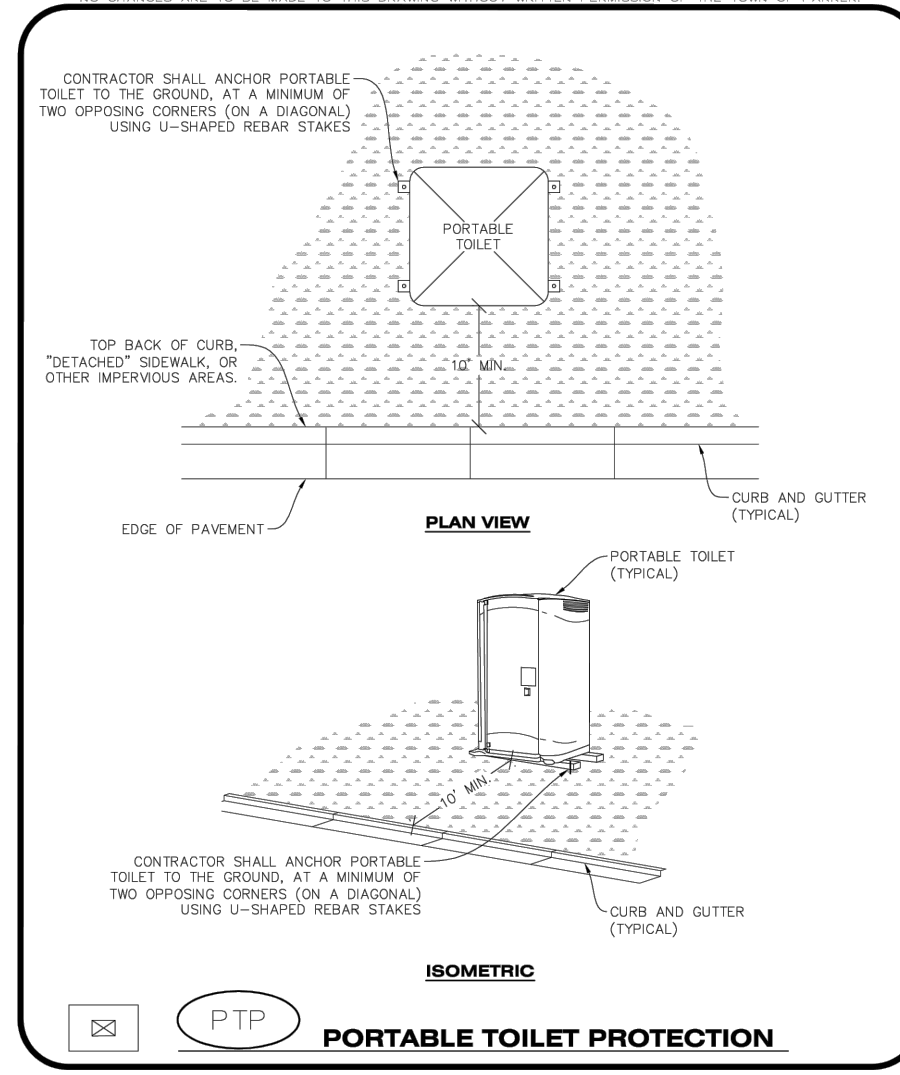
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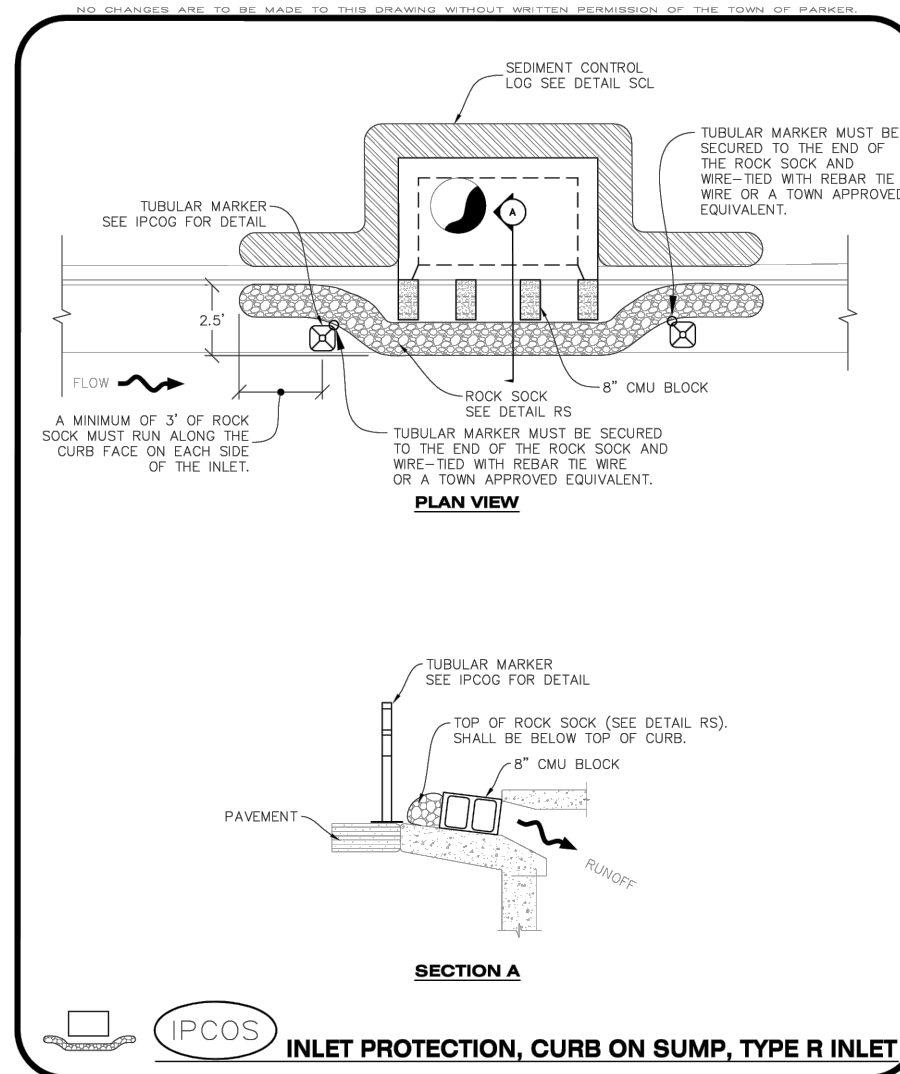
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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**IPAN** 1 OF 1  
Oct. 2012



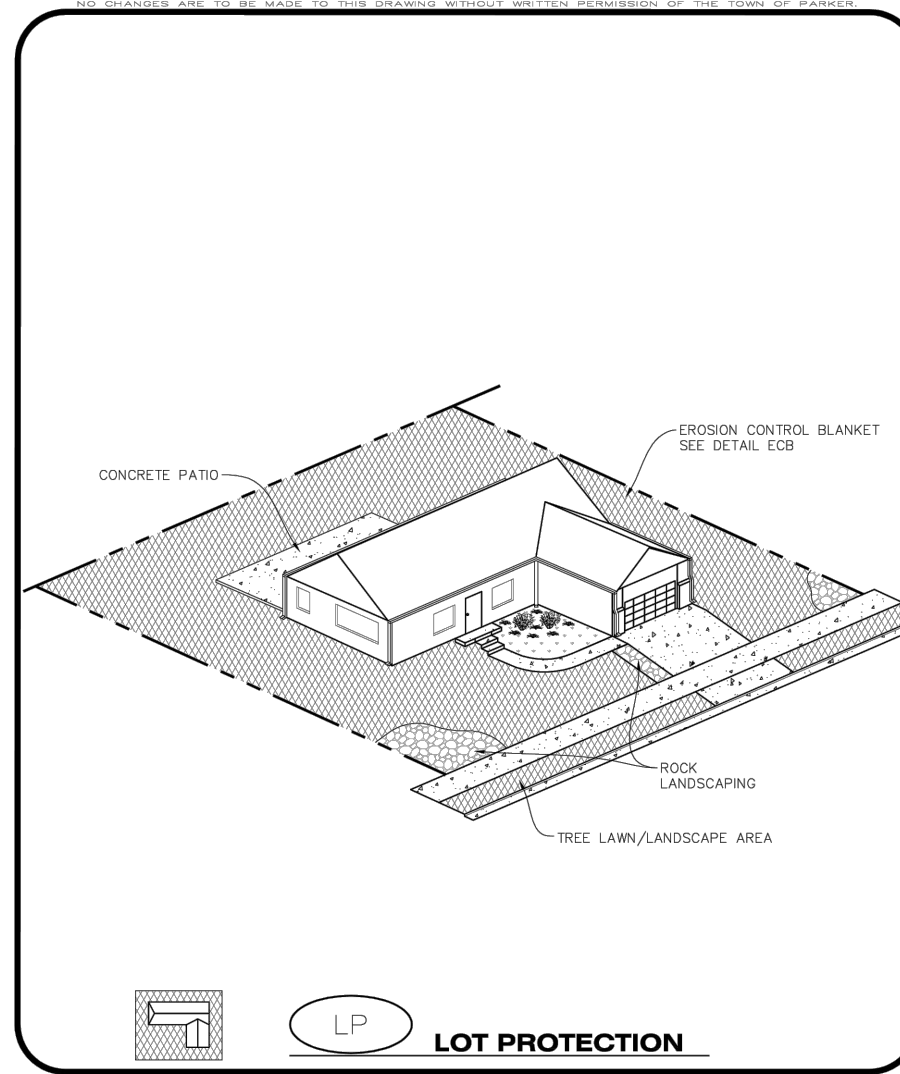
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Oct. 2012



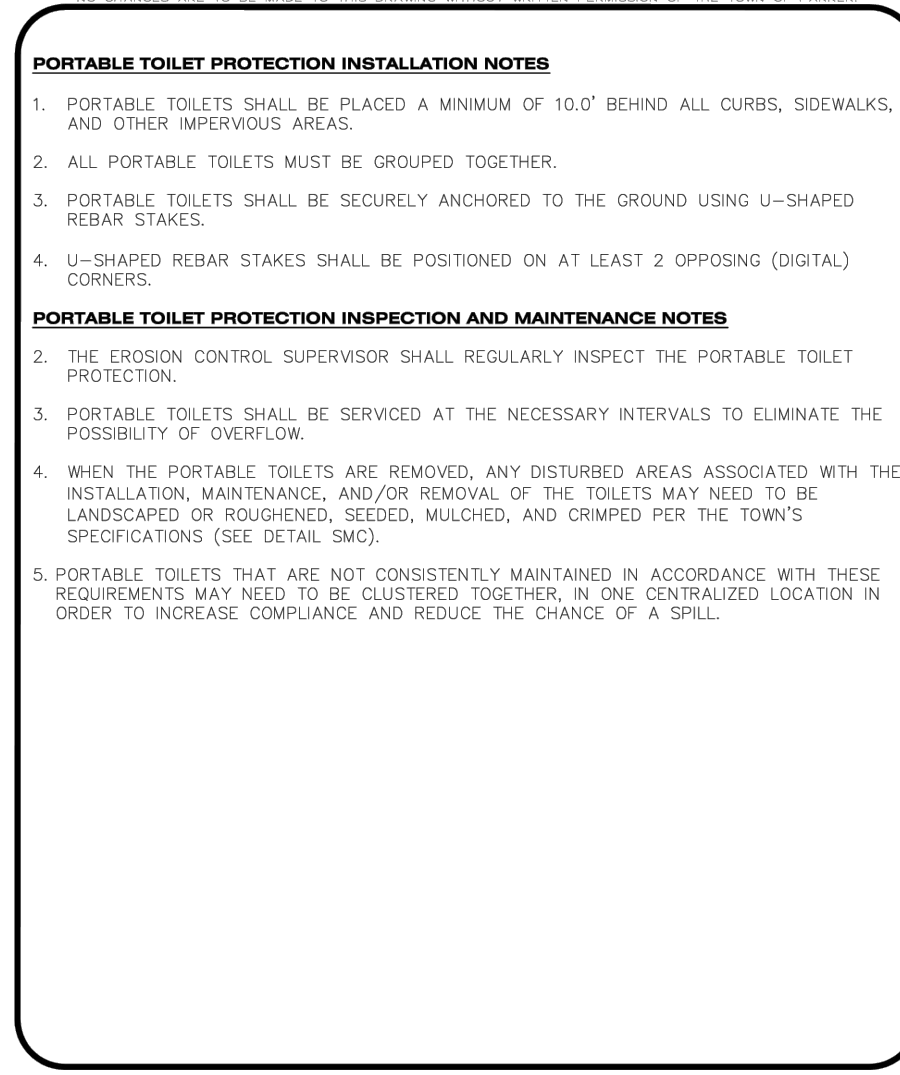
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Oct. 2012



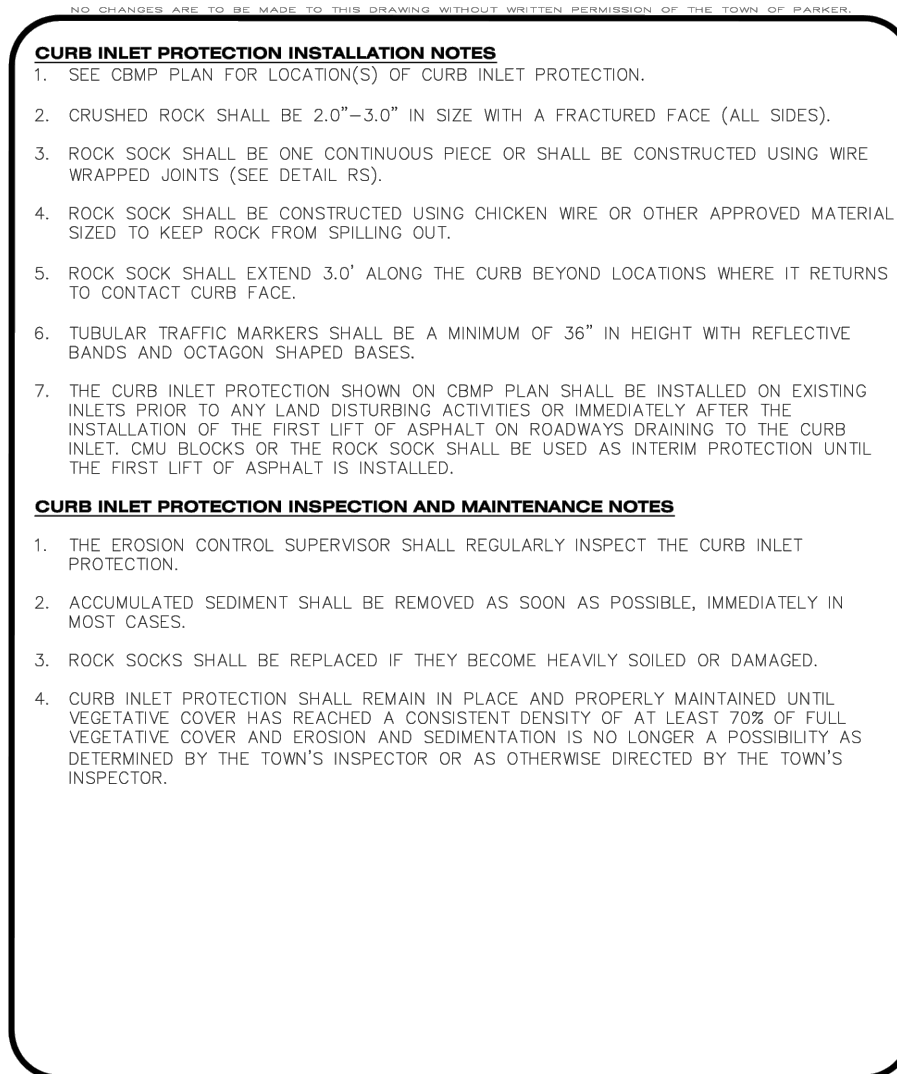
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Oct. 2012



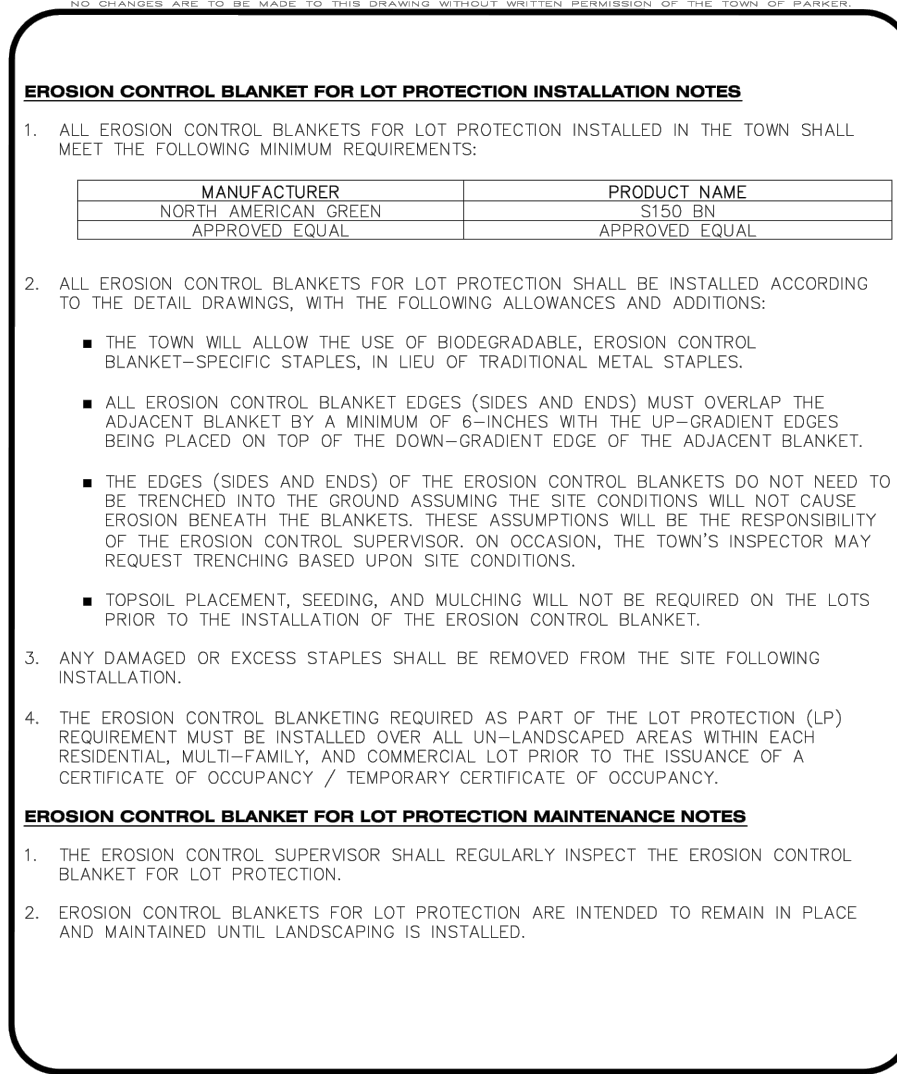
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Oct. 2012



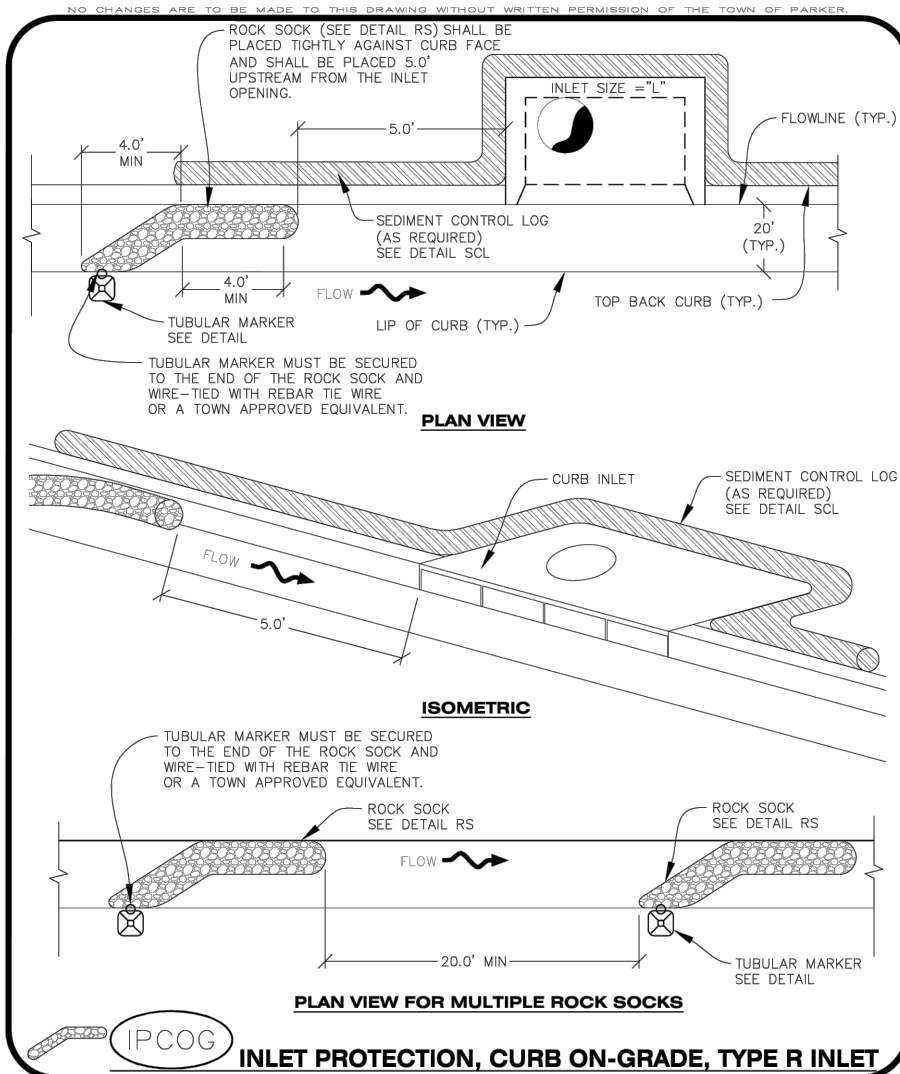
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Oct. 2012



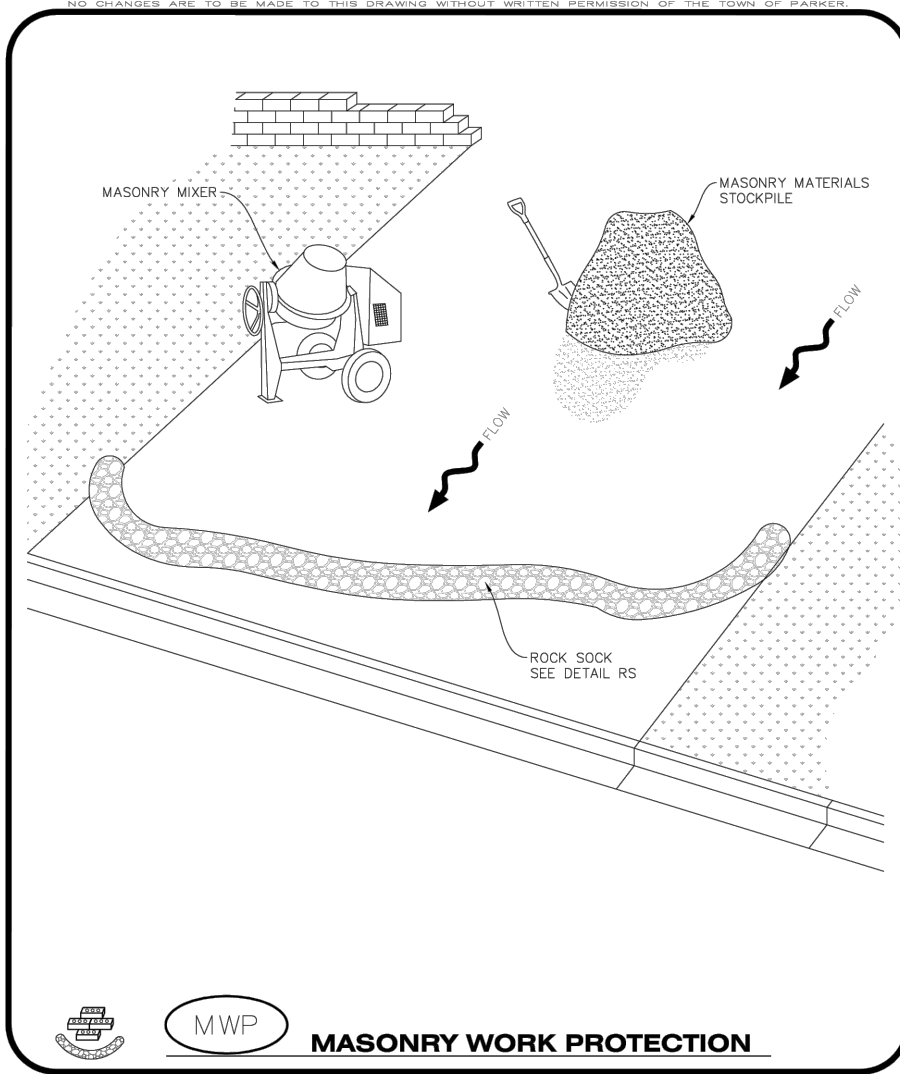
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Oct. 2012



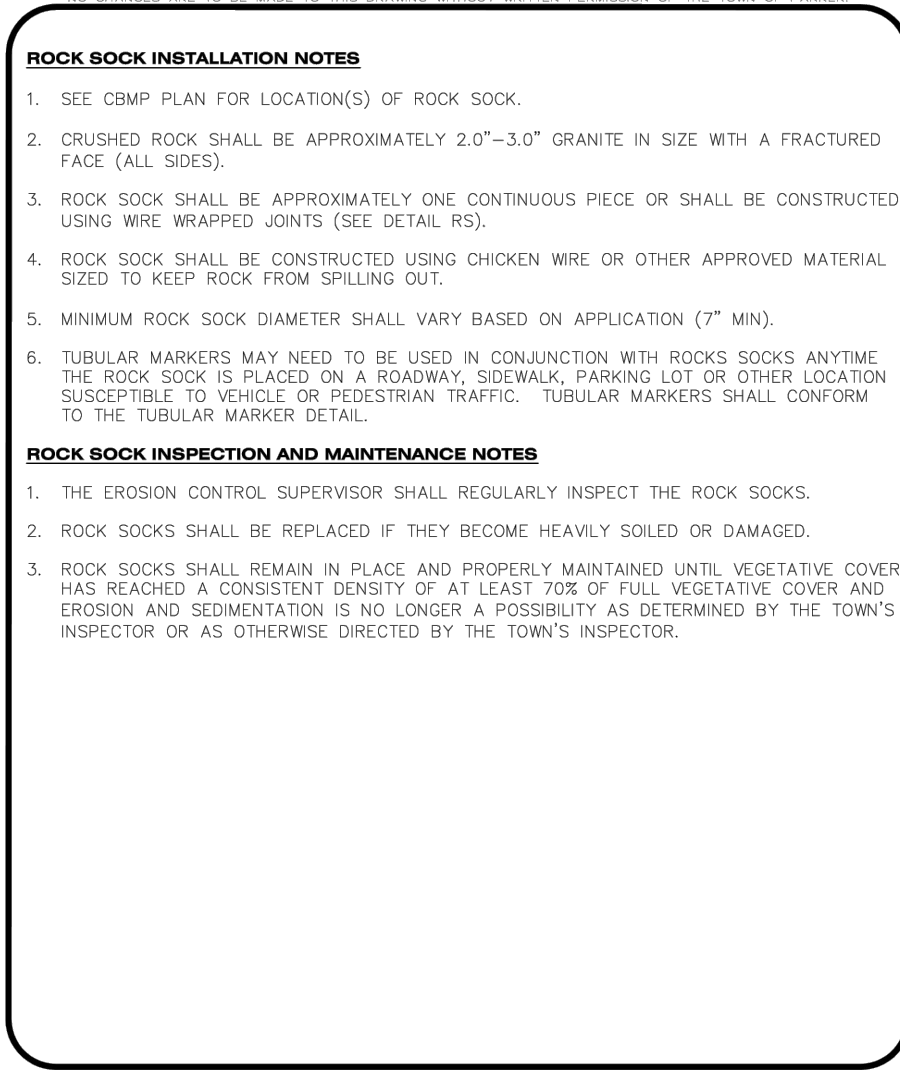
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Oct. 2012



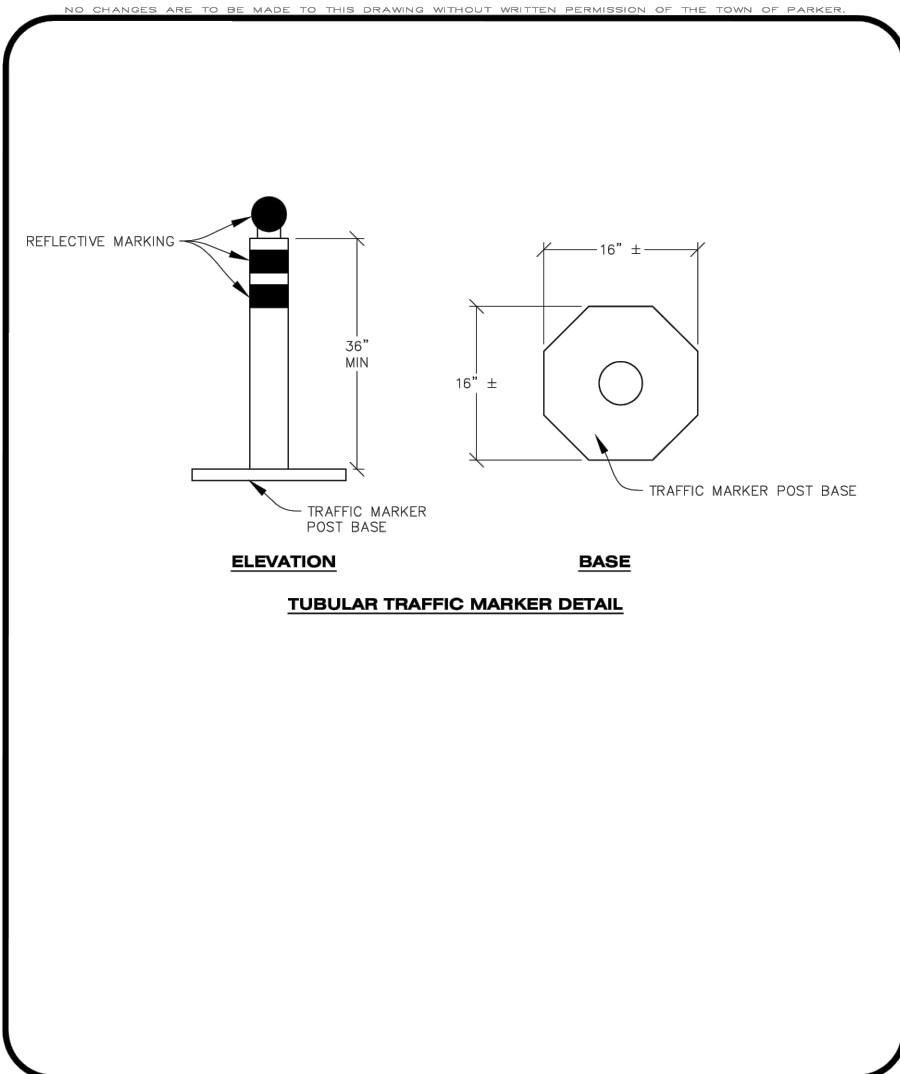
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Oct. 2012



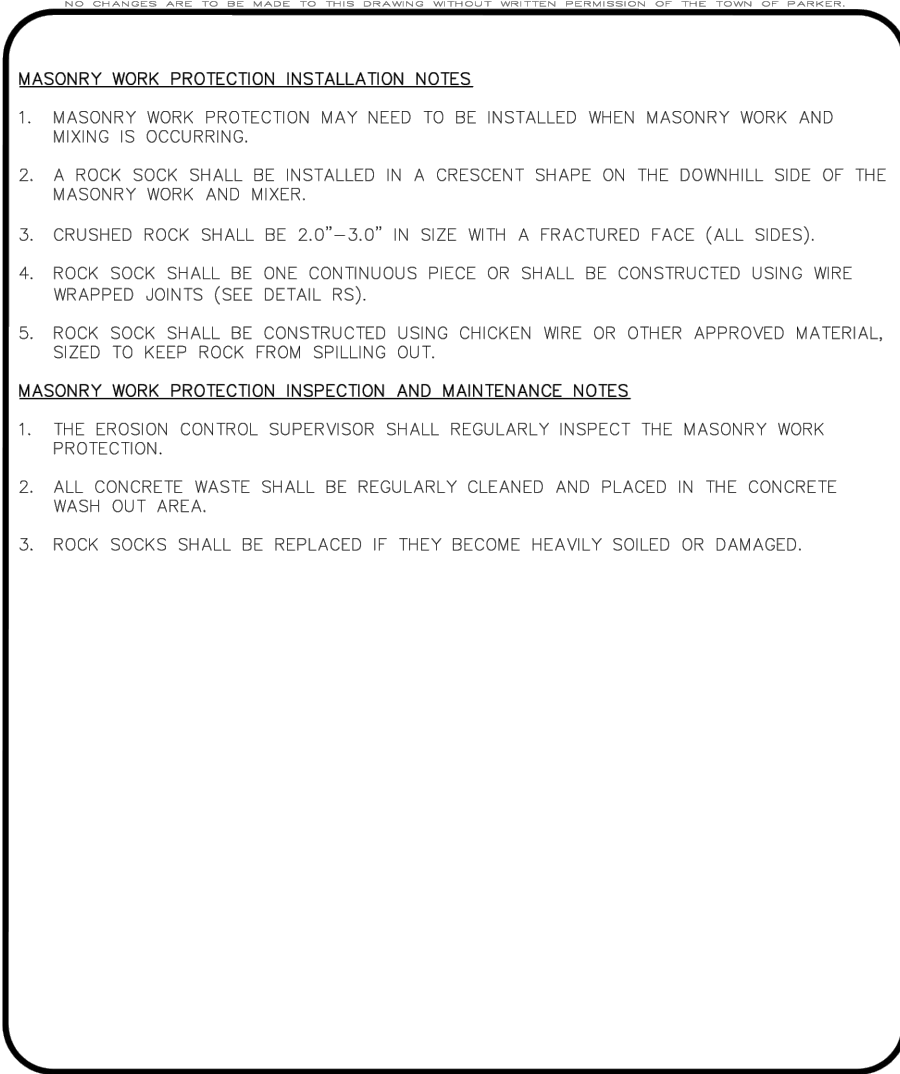
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Oct. 2012



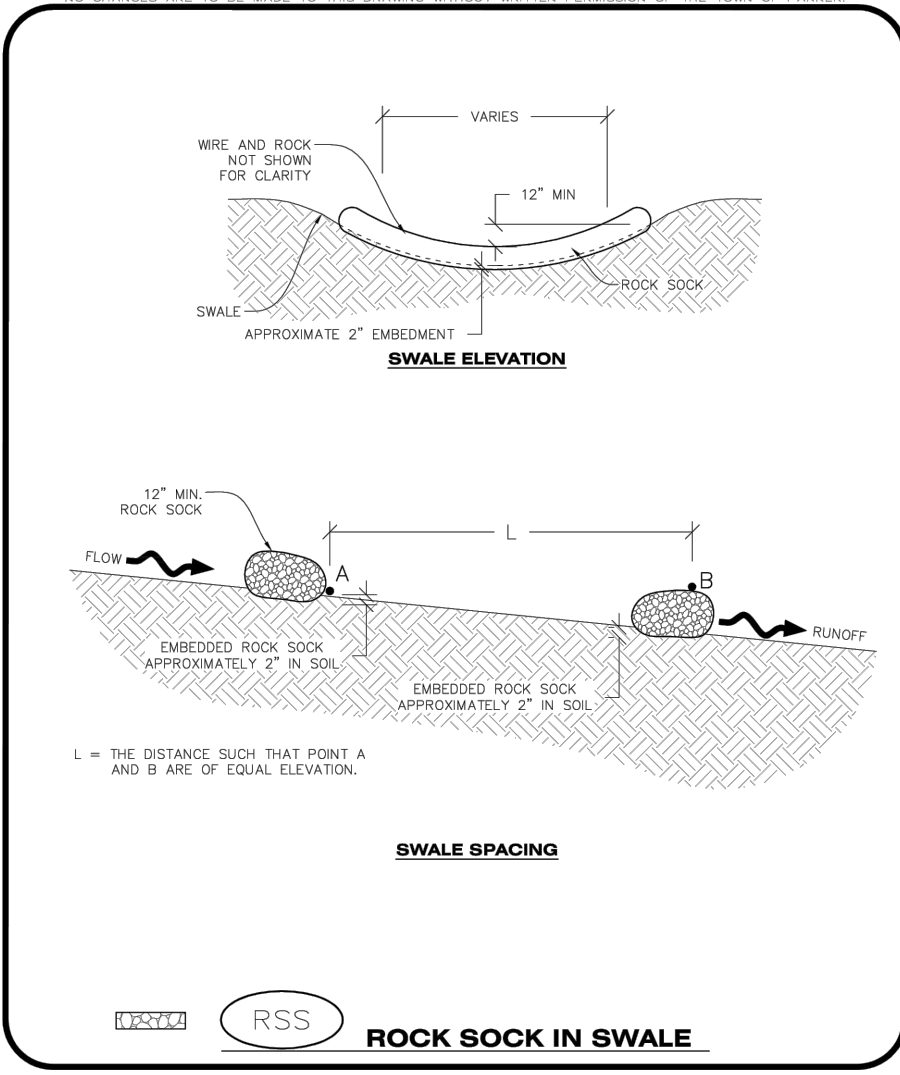
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**MWP** 2 OF 2  
Oct. 2012



**RSS** ROCK SOCK IN SWALE  
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**RSS** 1 OF 1  
Oct. 2012



**IPAN** INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT  
**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**IPAN** 2 OF 3  
Oct. 2012



**IPCOS** INLET PROTECTION, CURB ON SUMP, TYPE R INLET  
**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**IPCOS** 3 OF 3  
Oct. 2012

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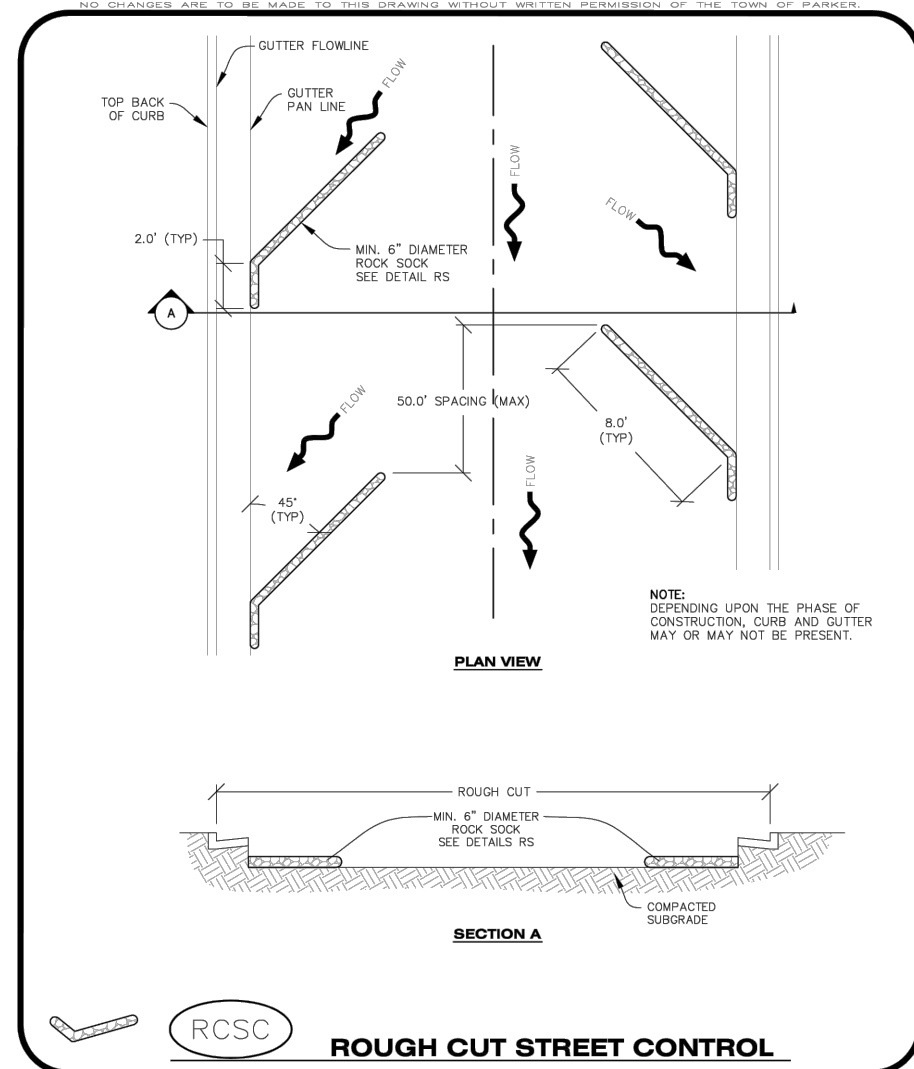
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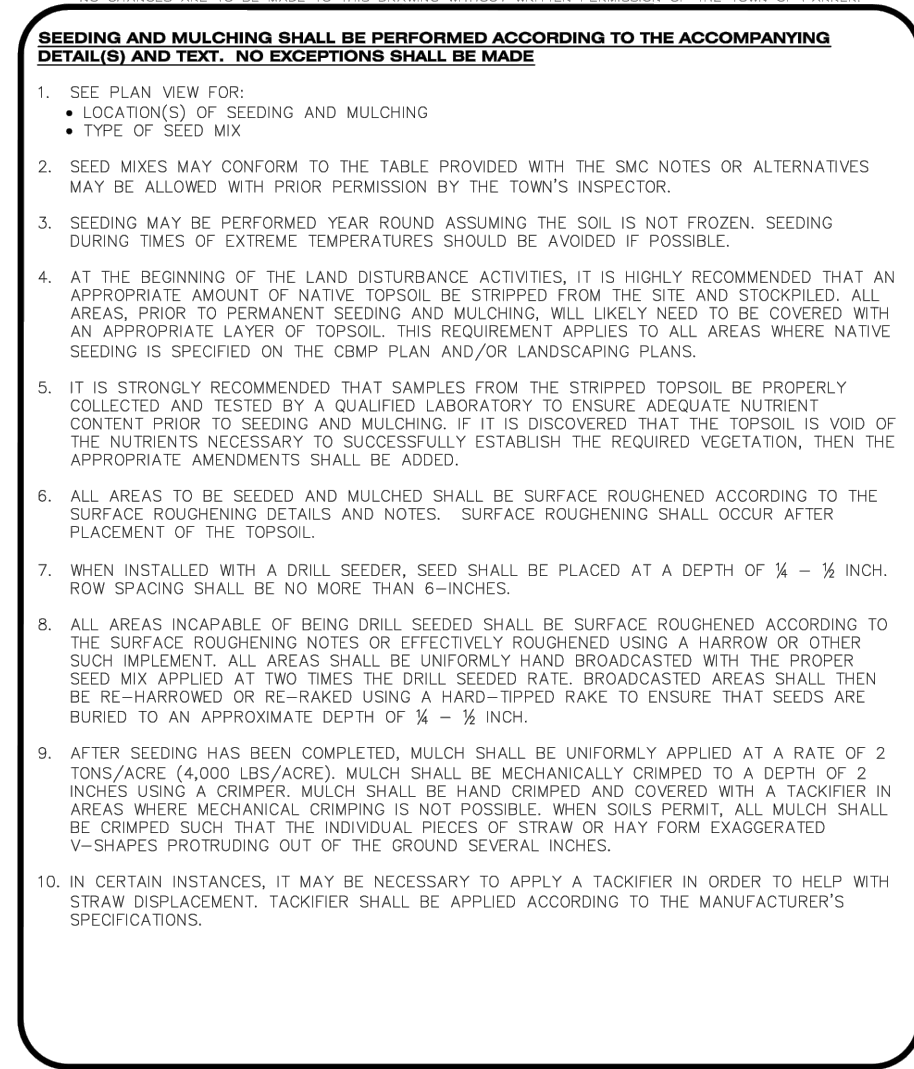
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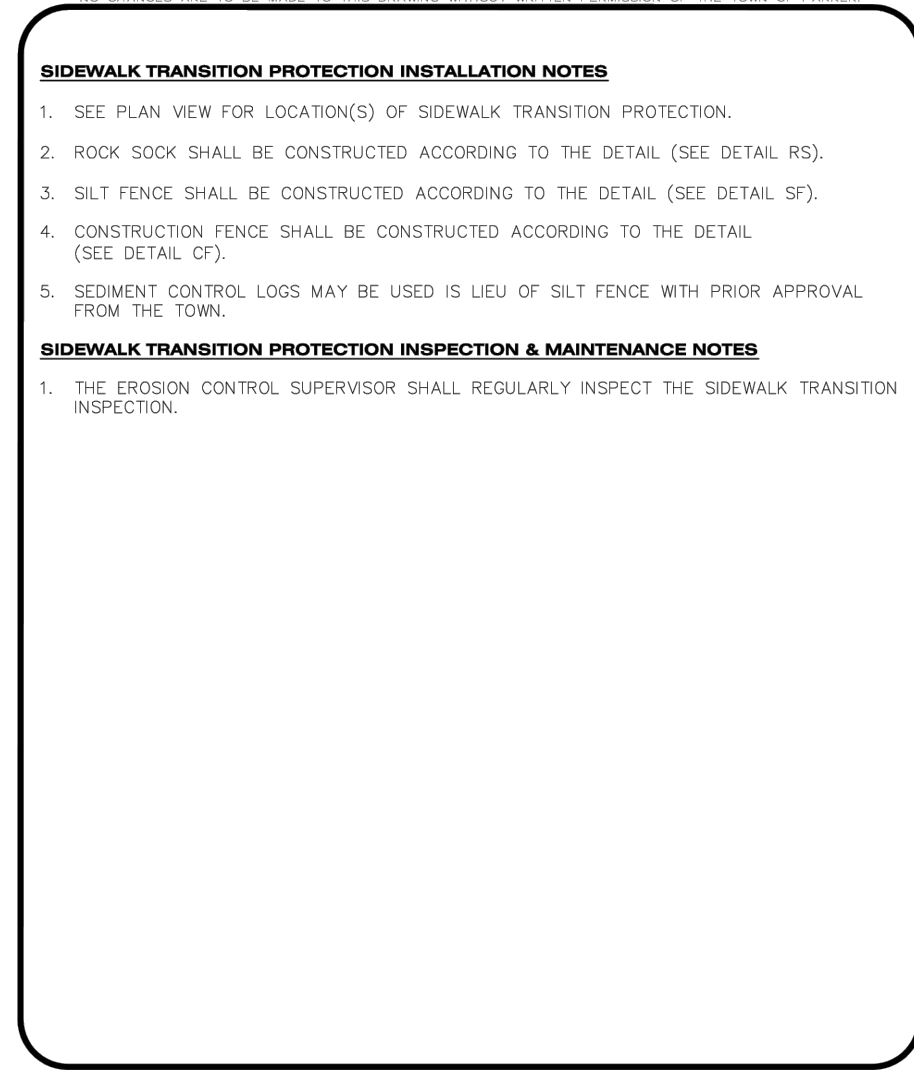
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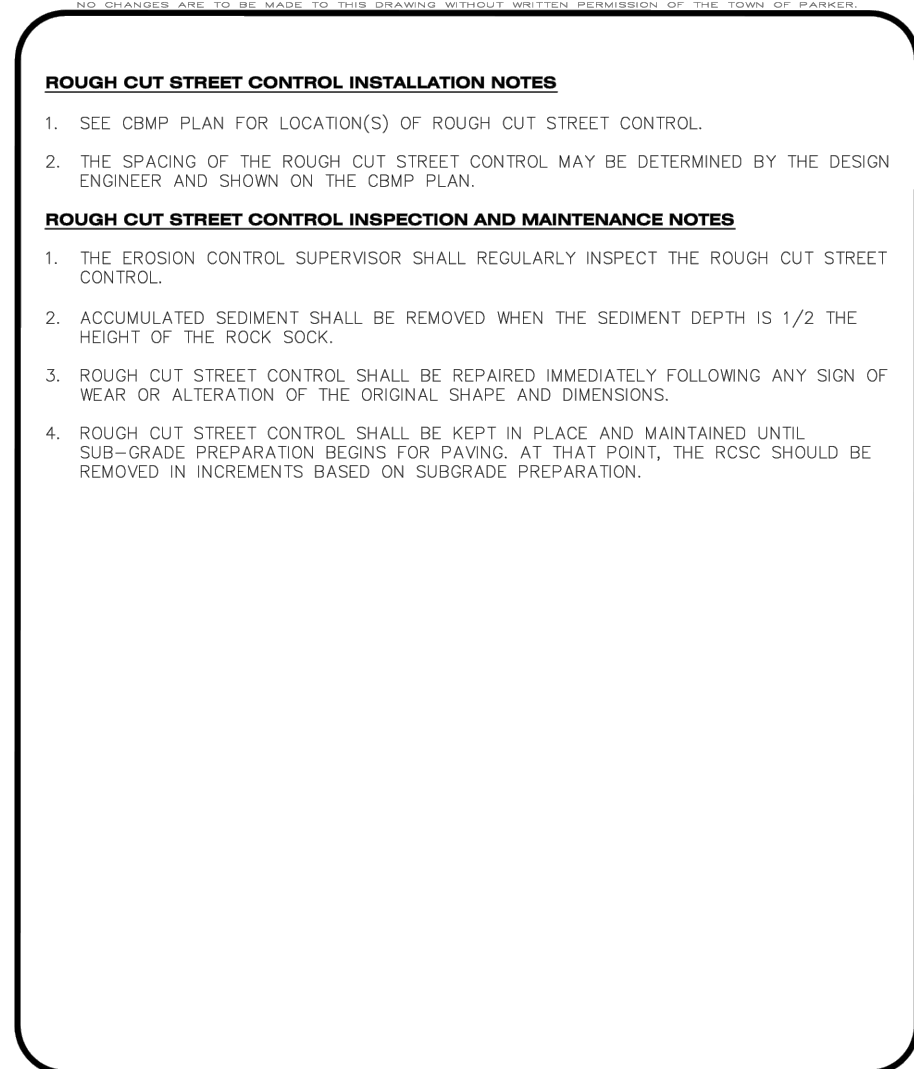
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 Parker COLORADO CONSTRUCTION BEST MANAGEMENT PRACTICES  
 CBMP  
 1 OF 2  
 04/1/2012



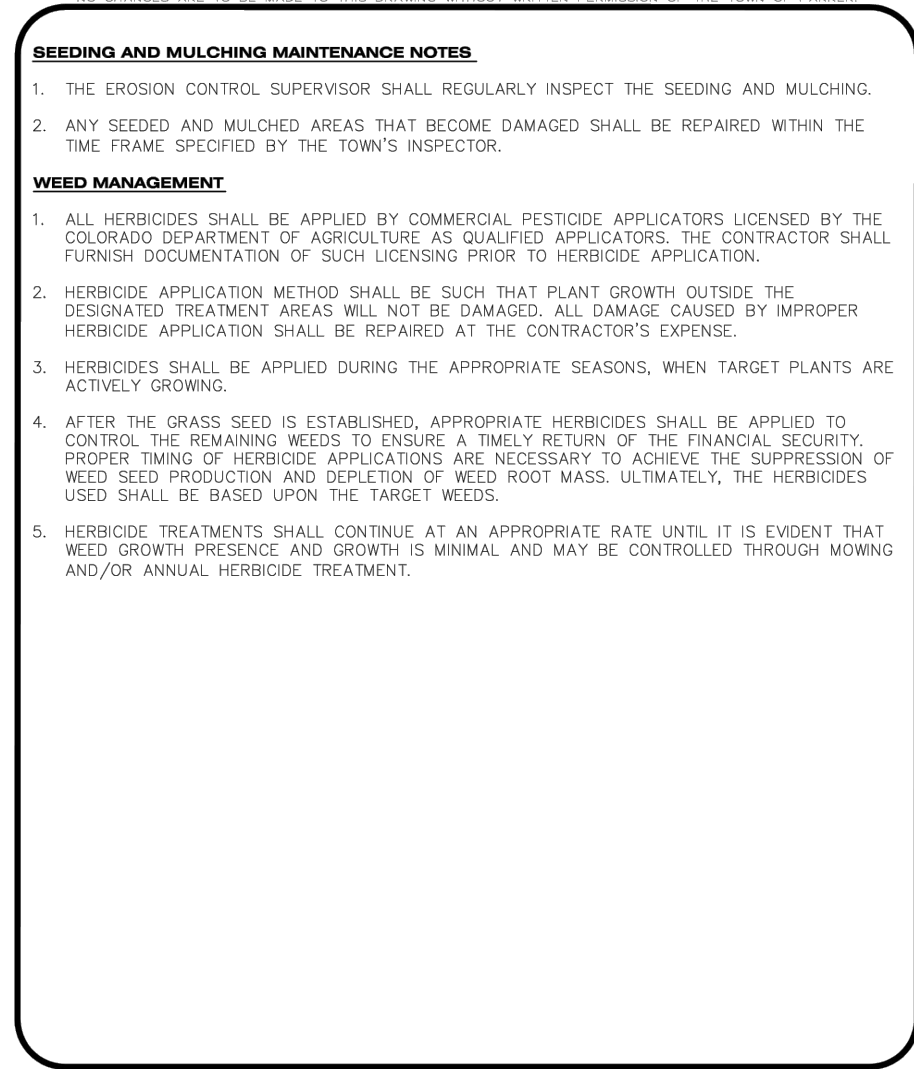
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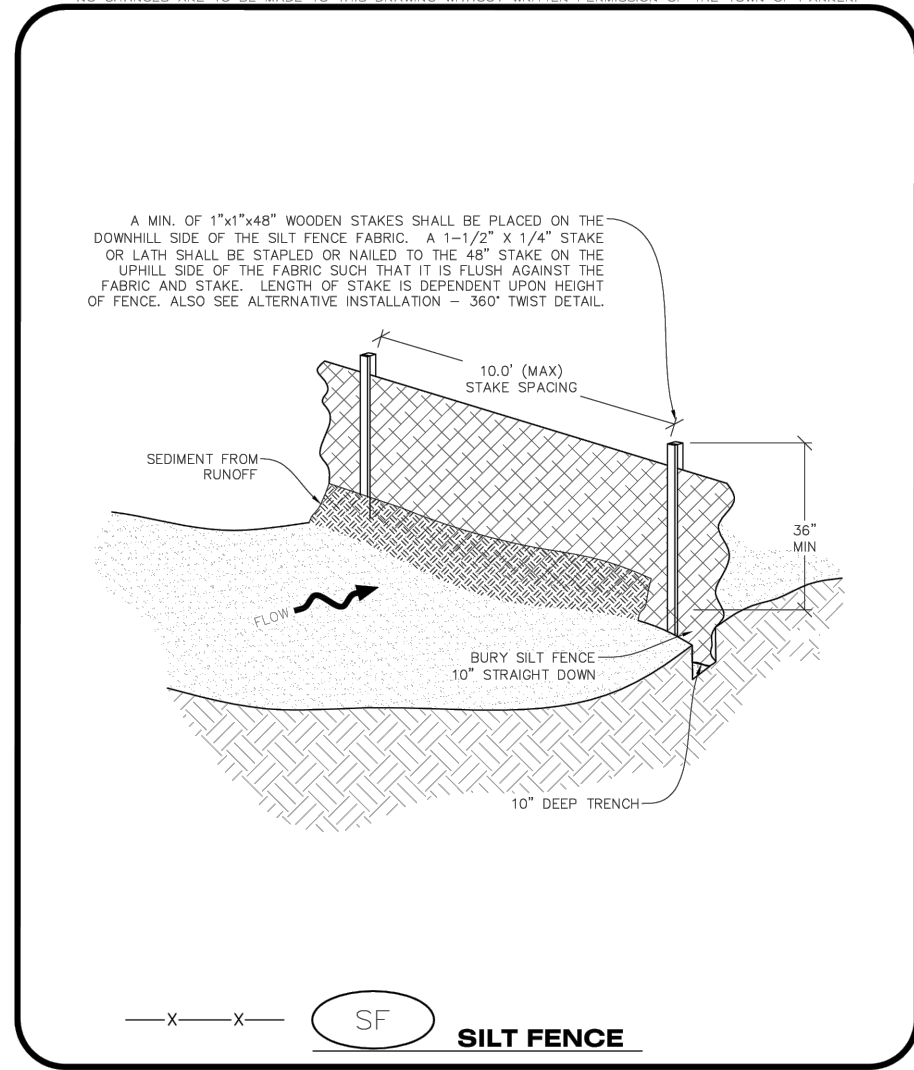
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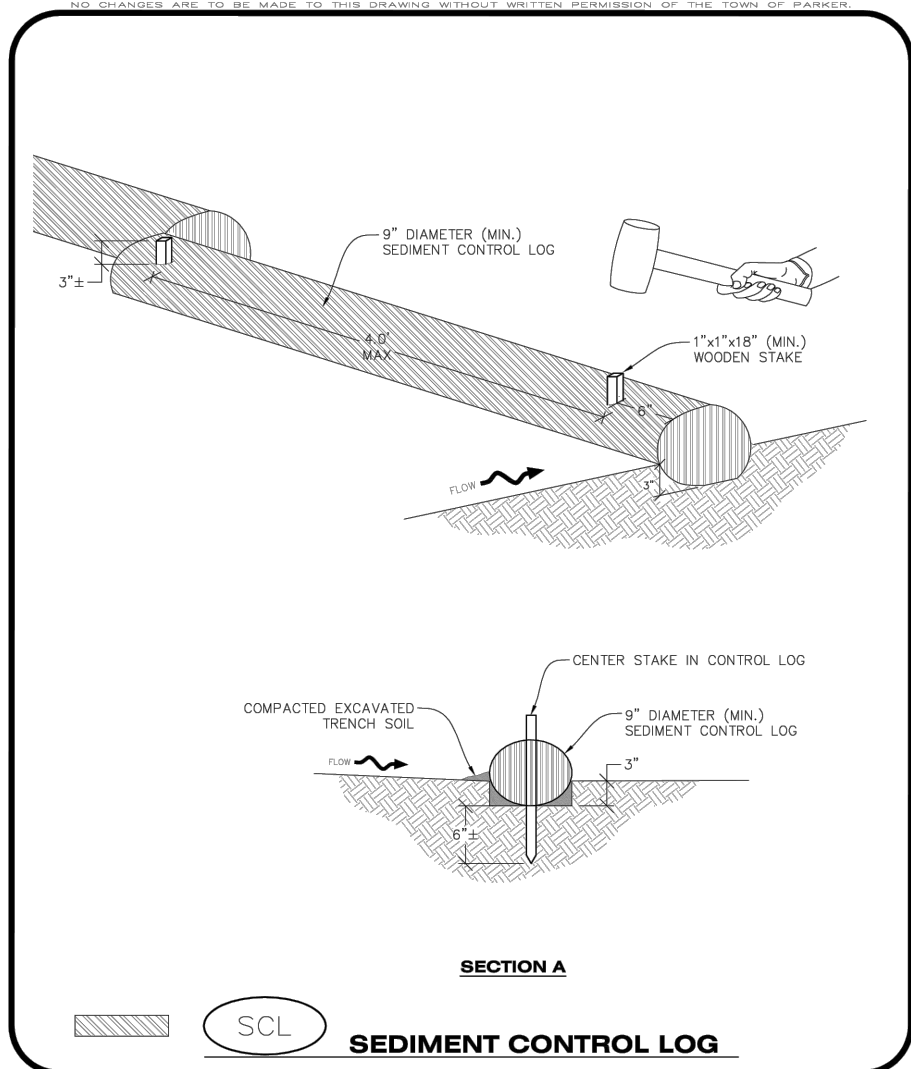
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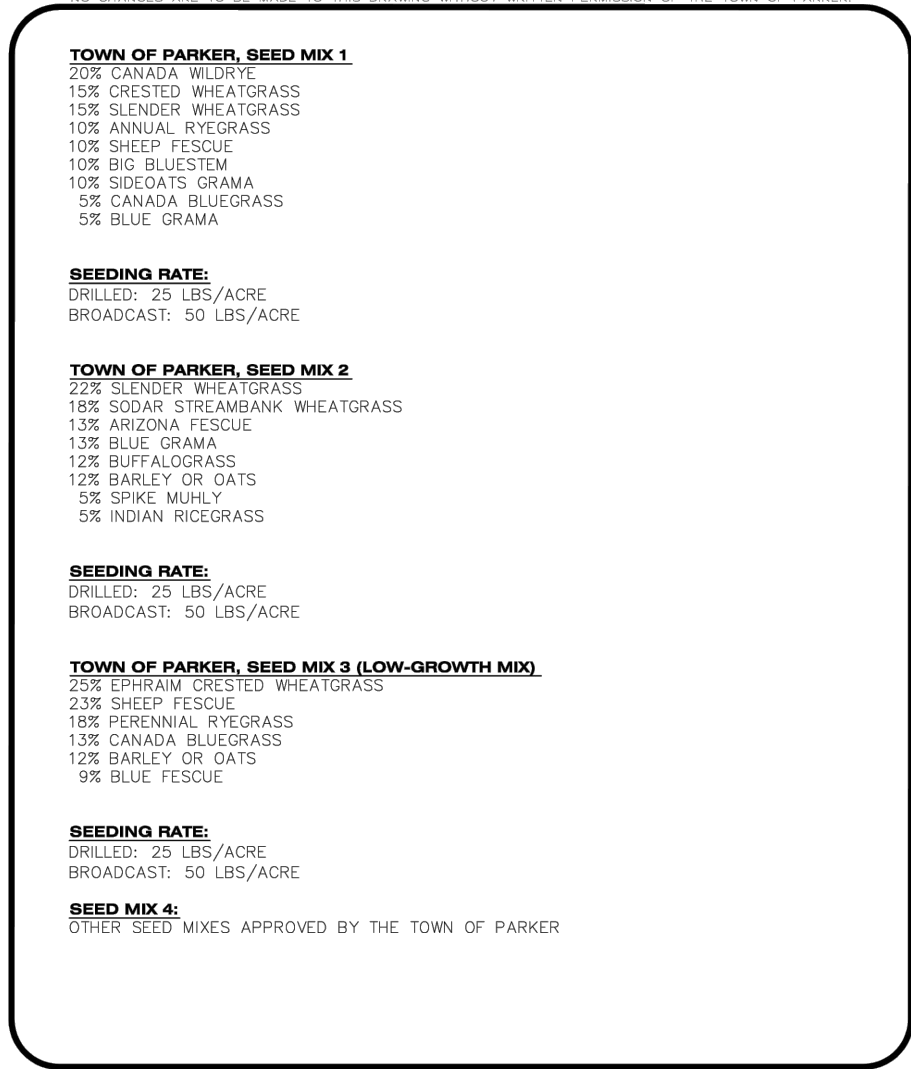
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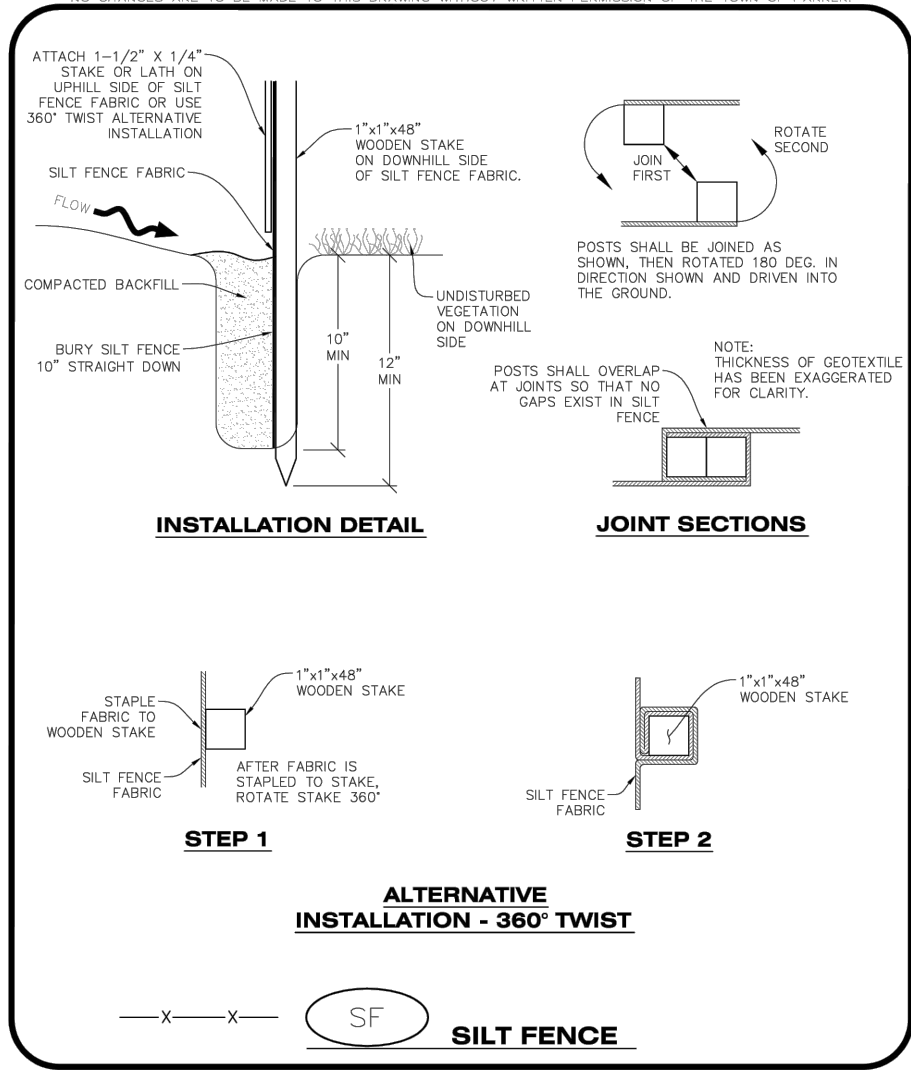
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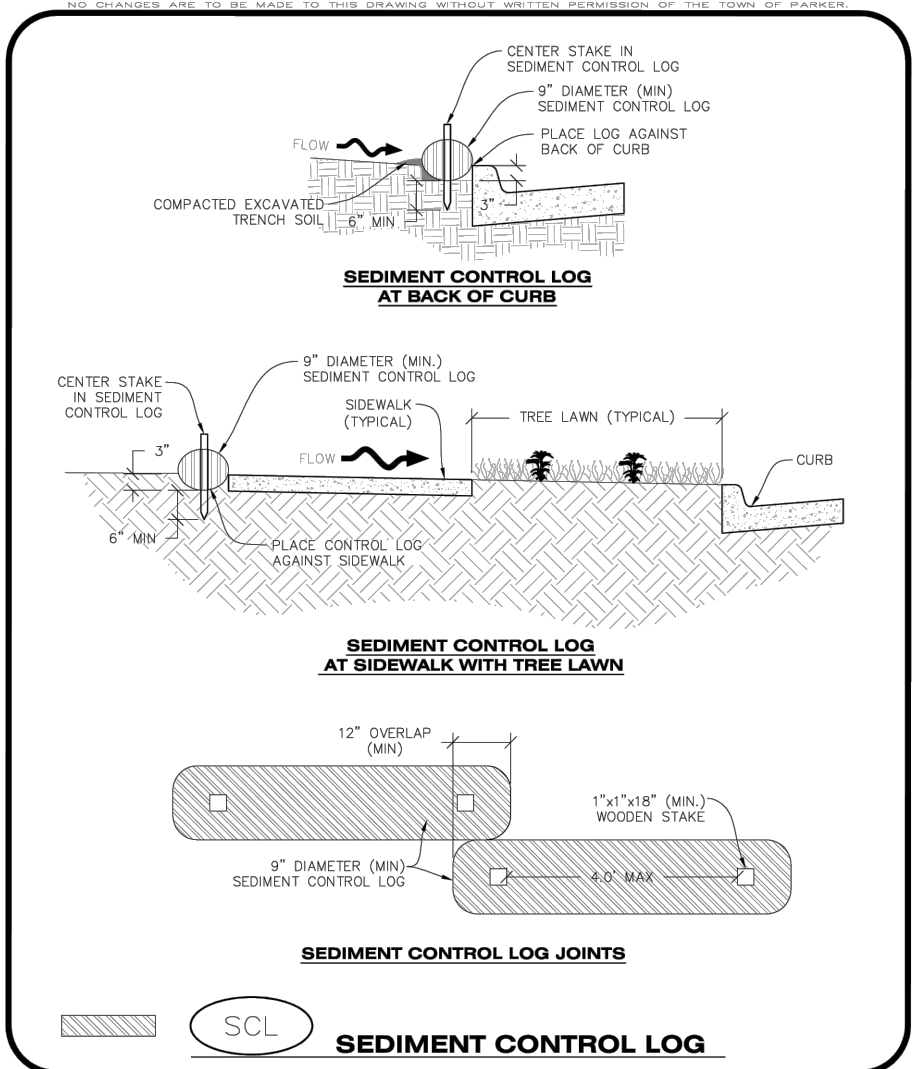
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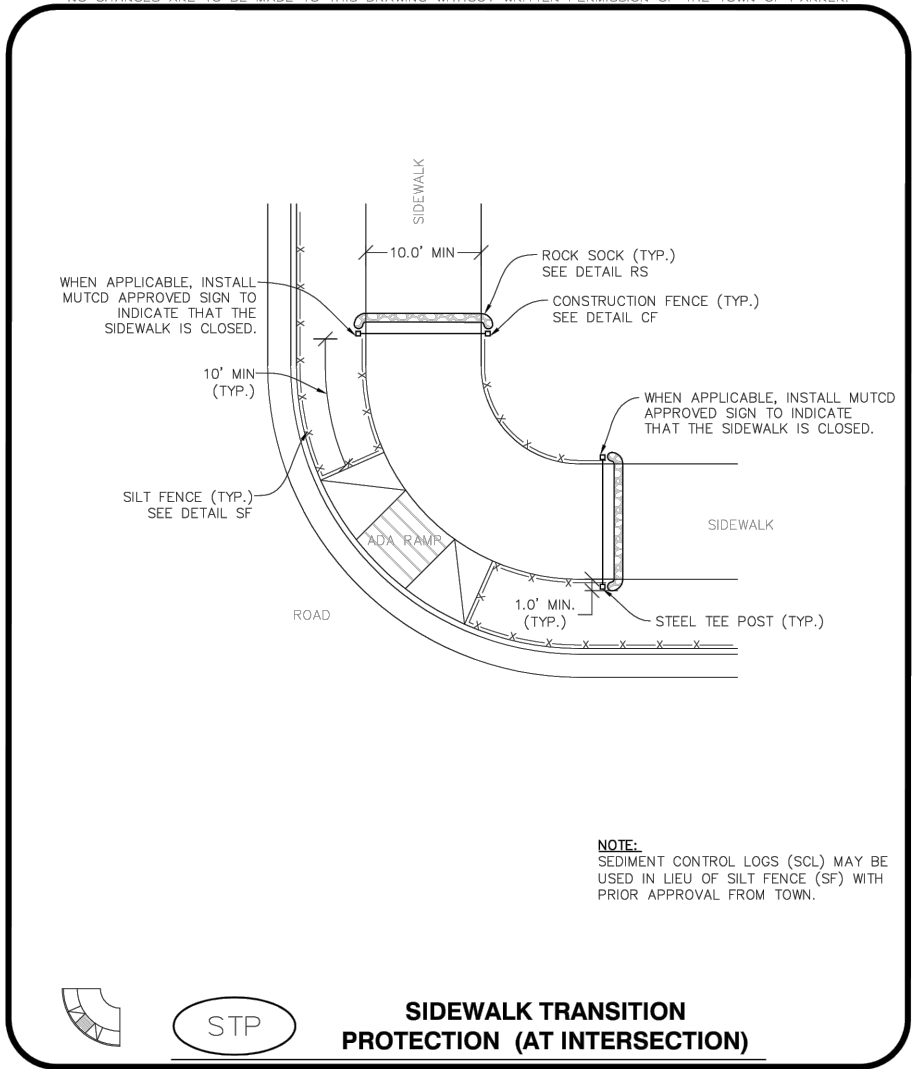
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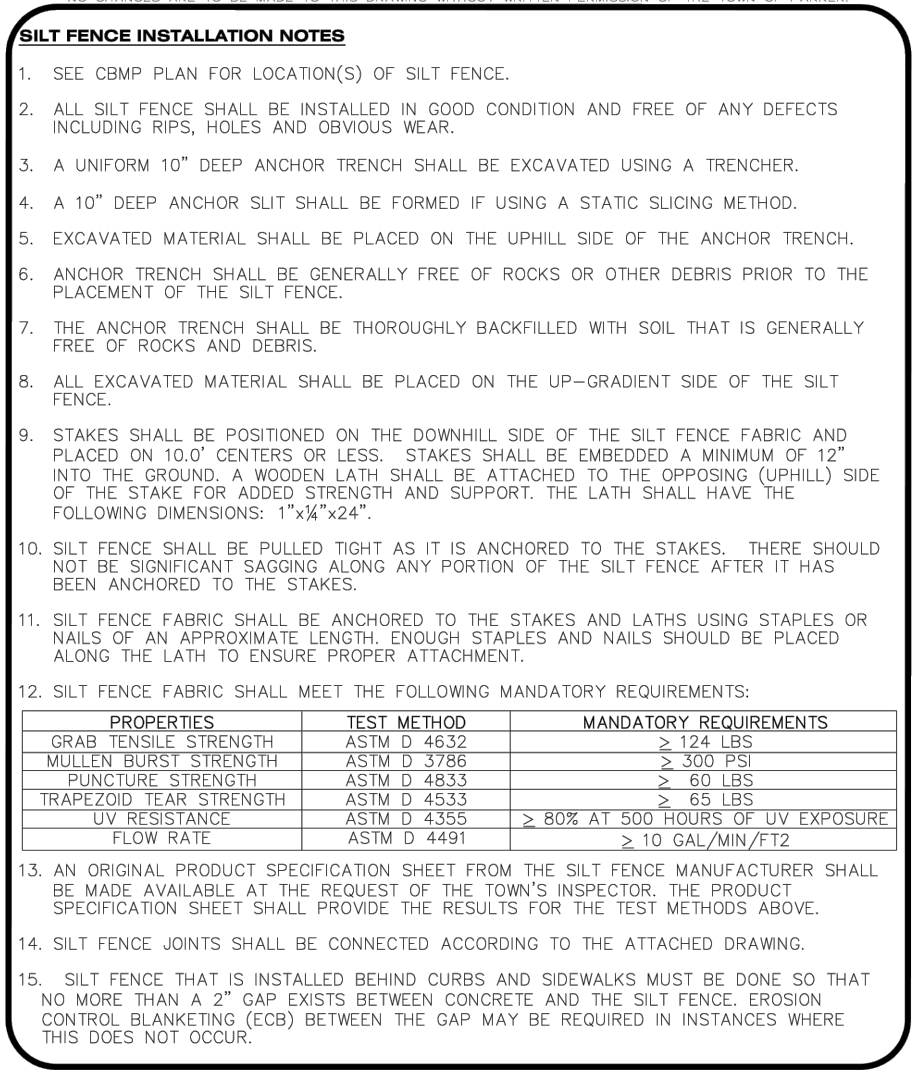
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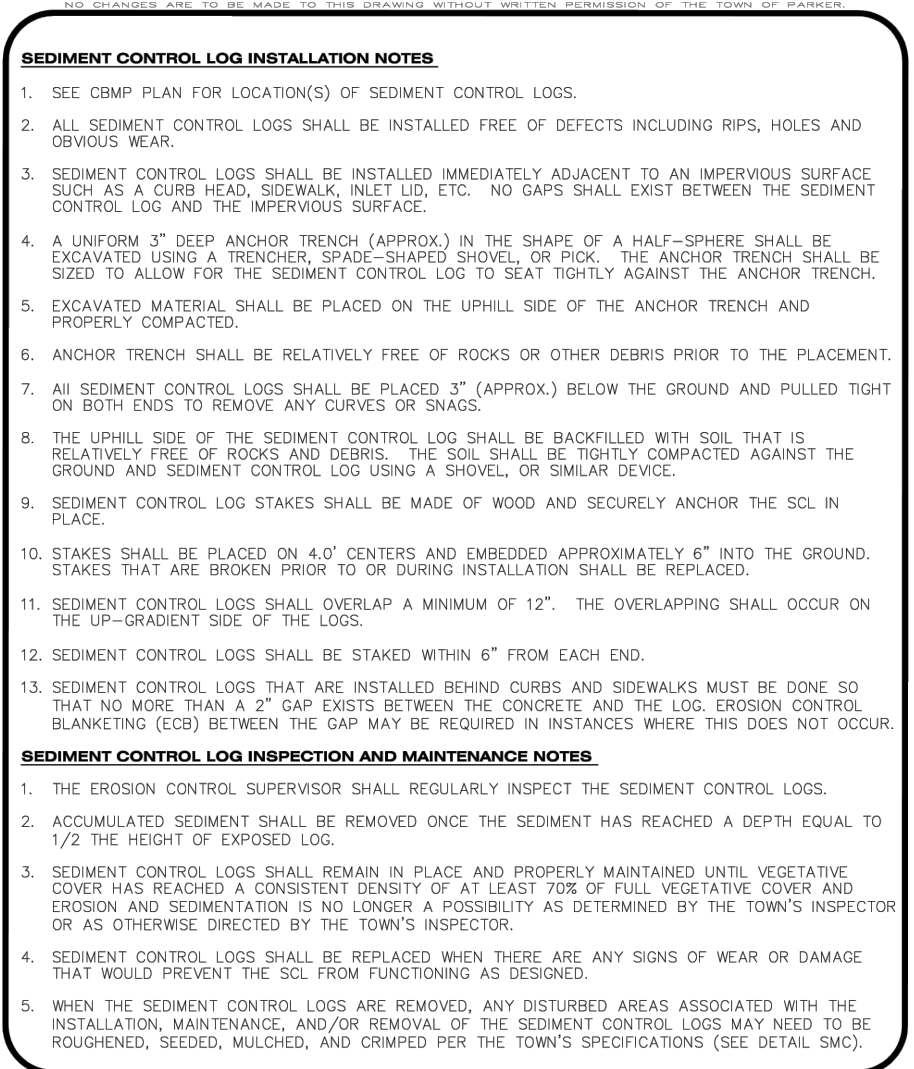
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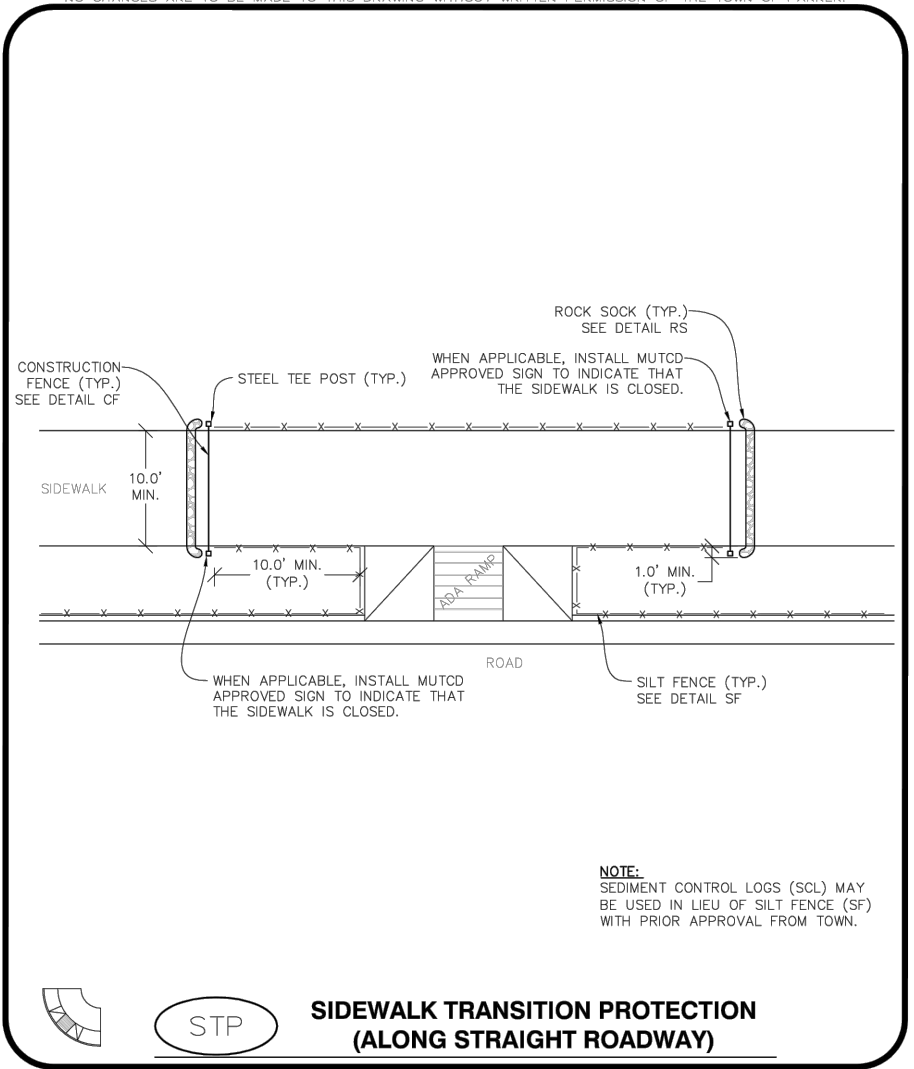
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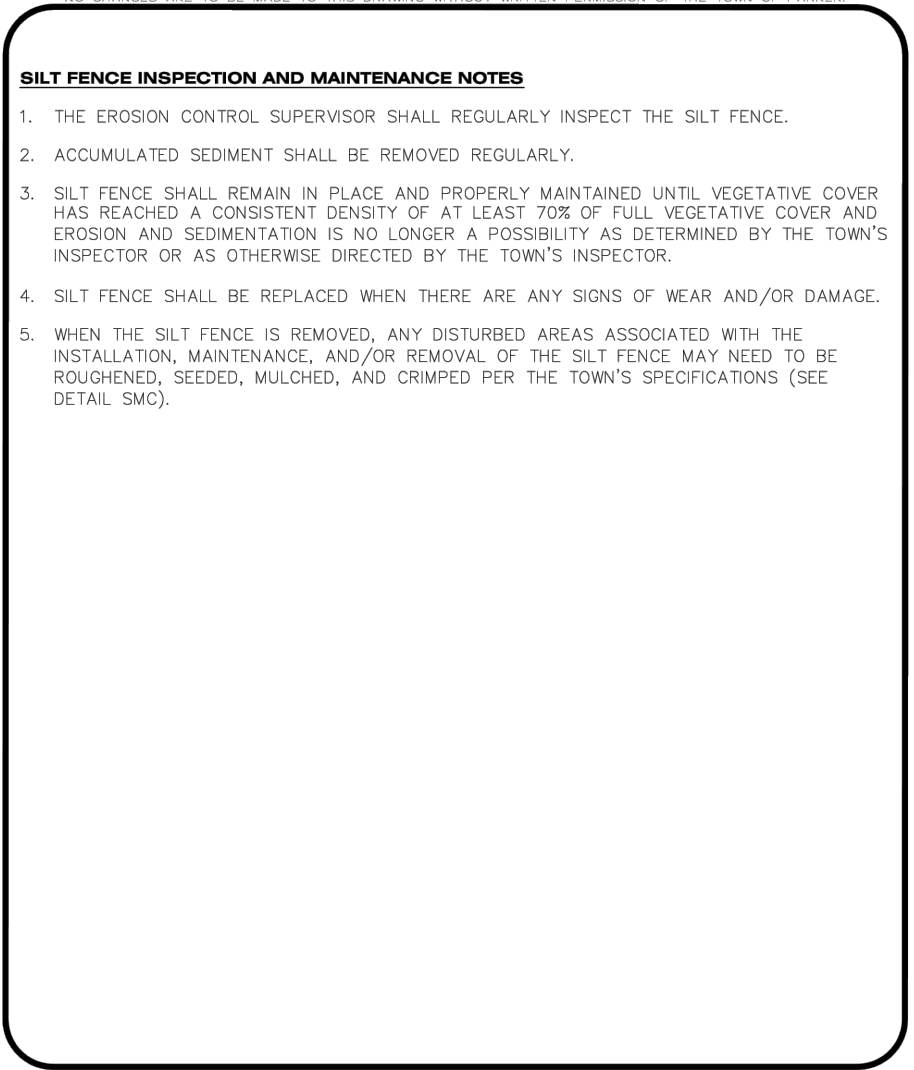
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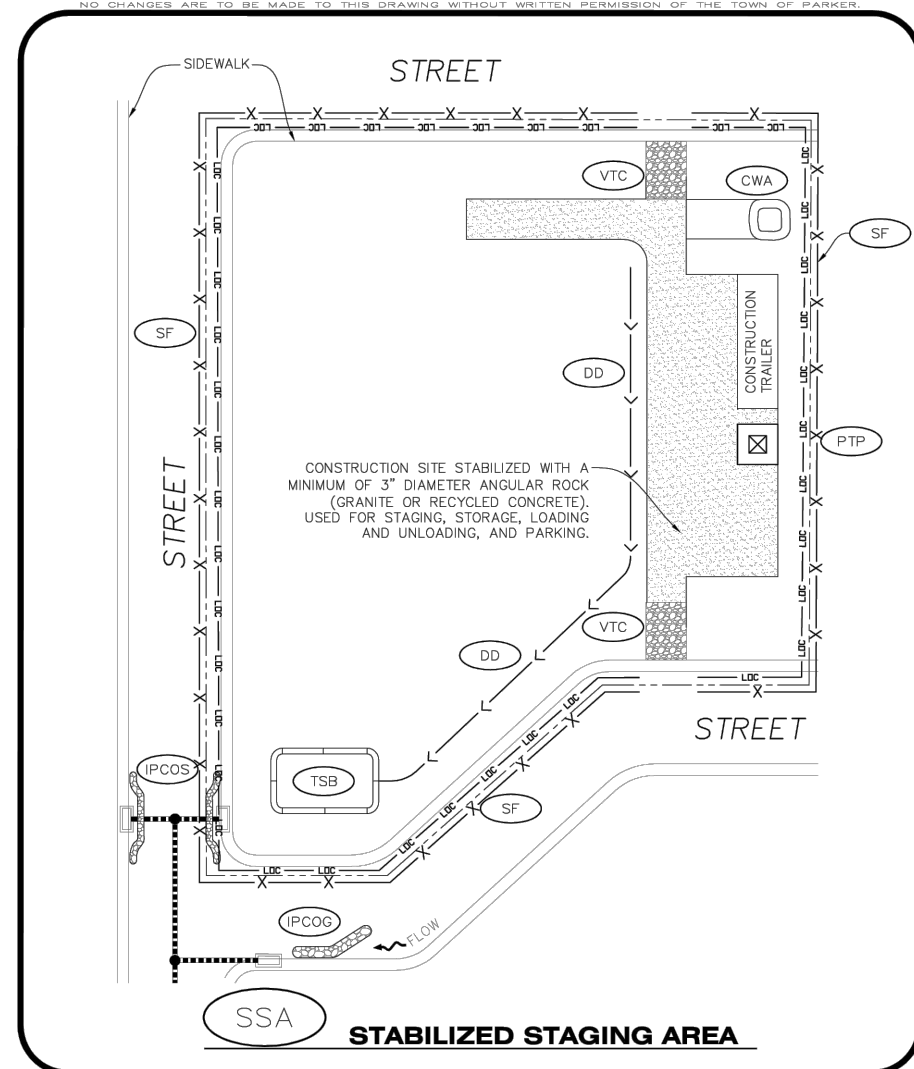
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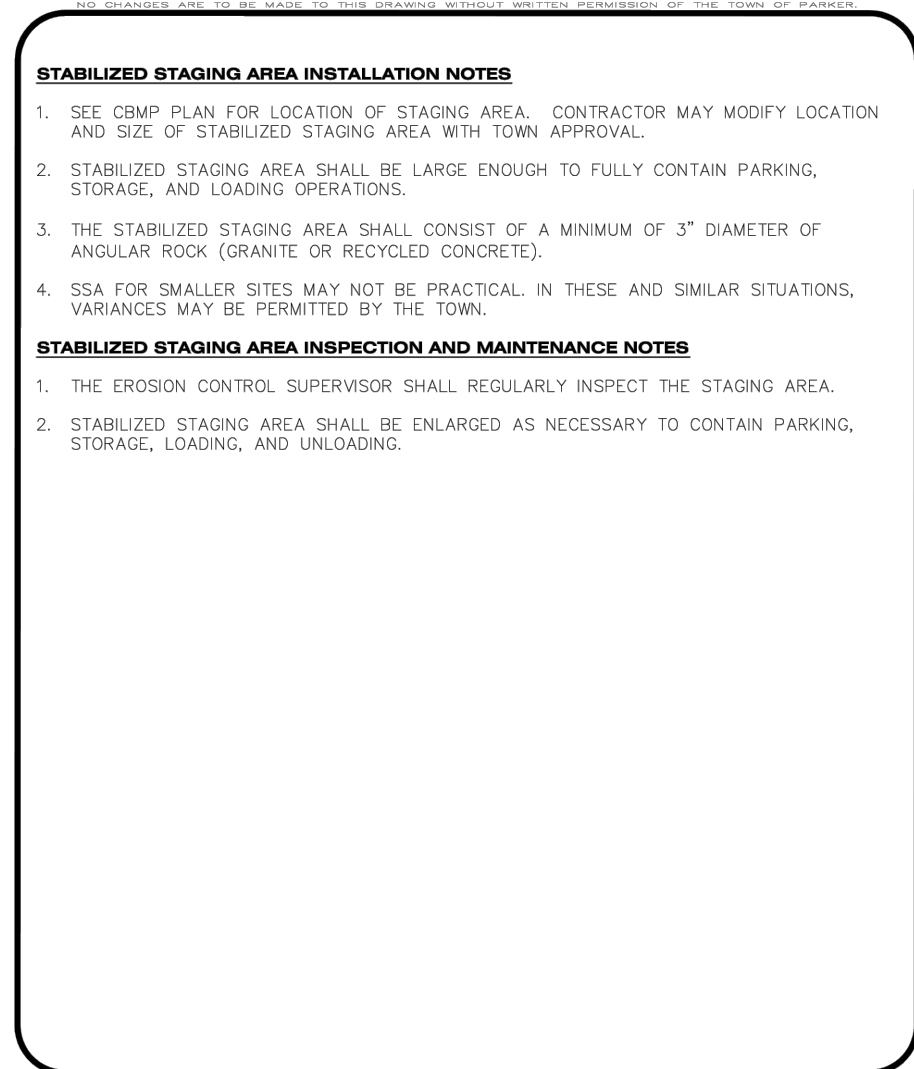
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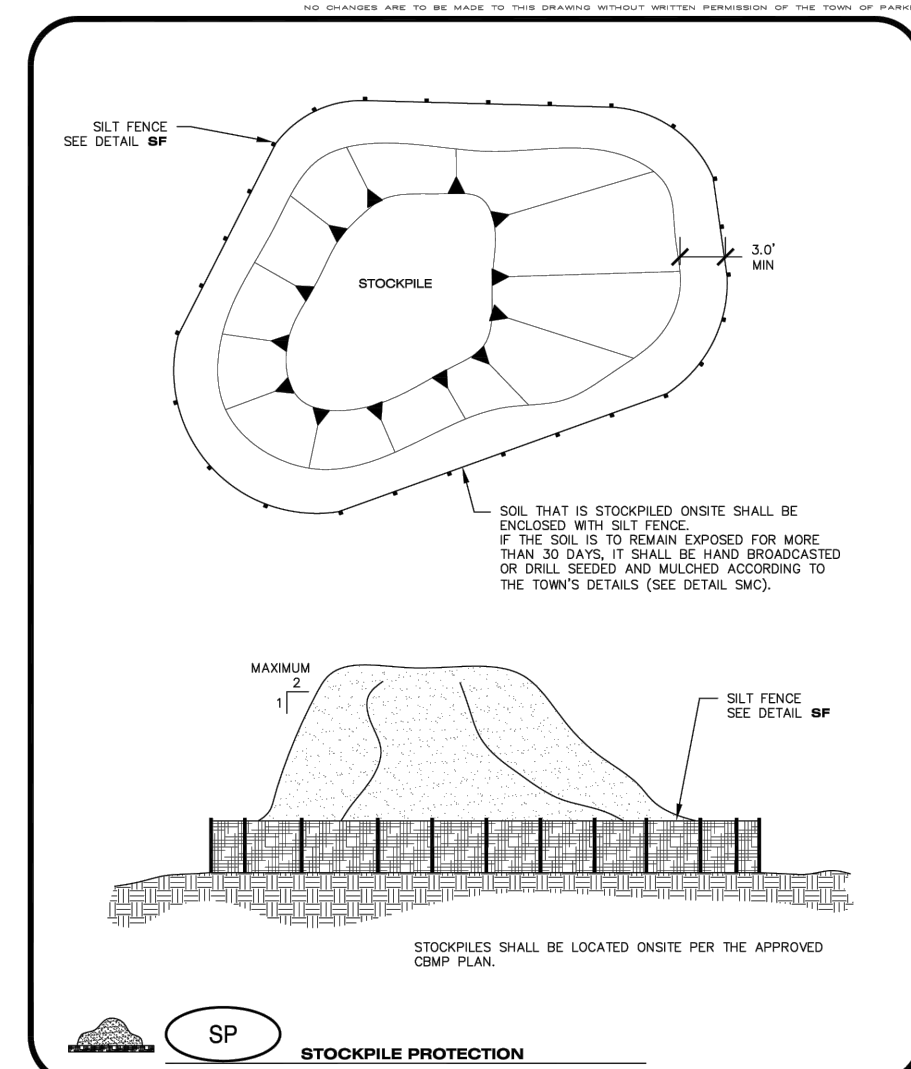
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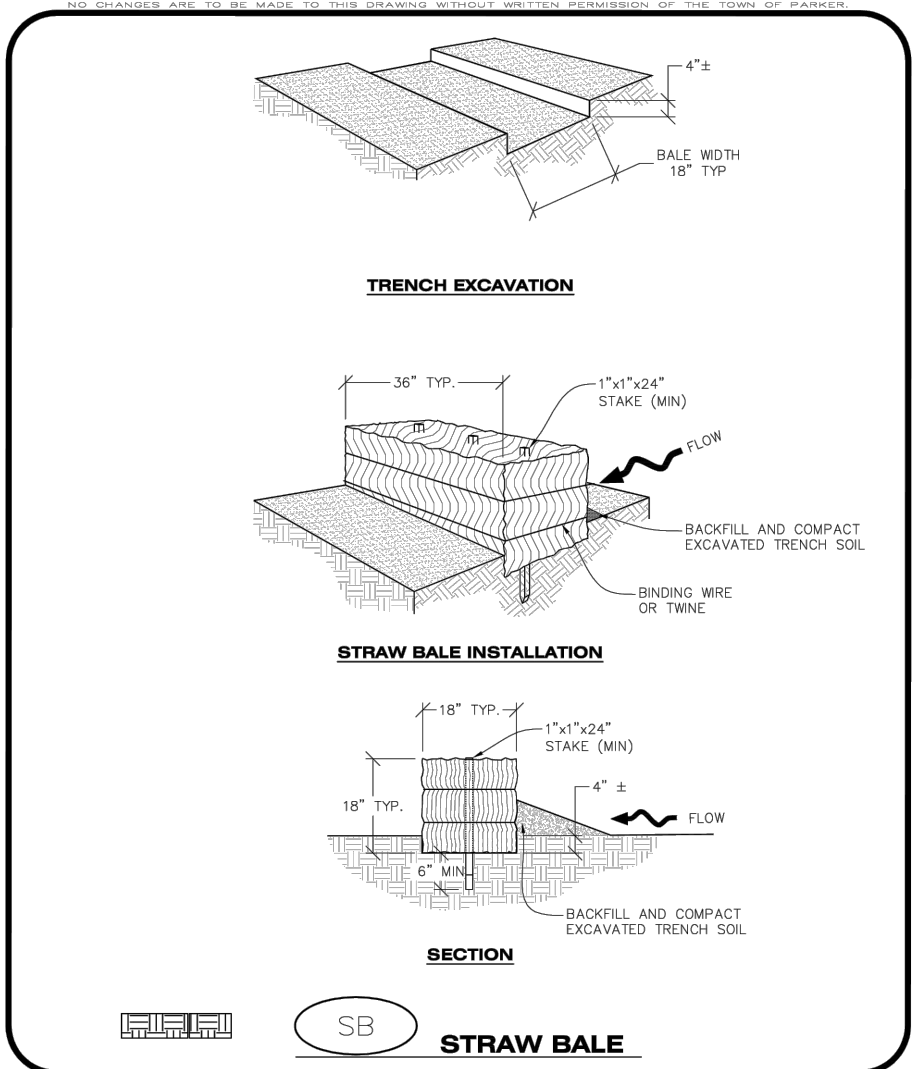
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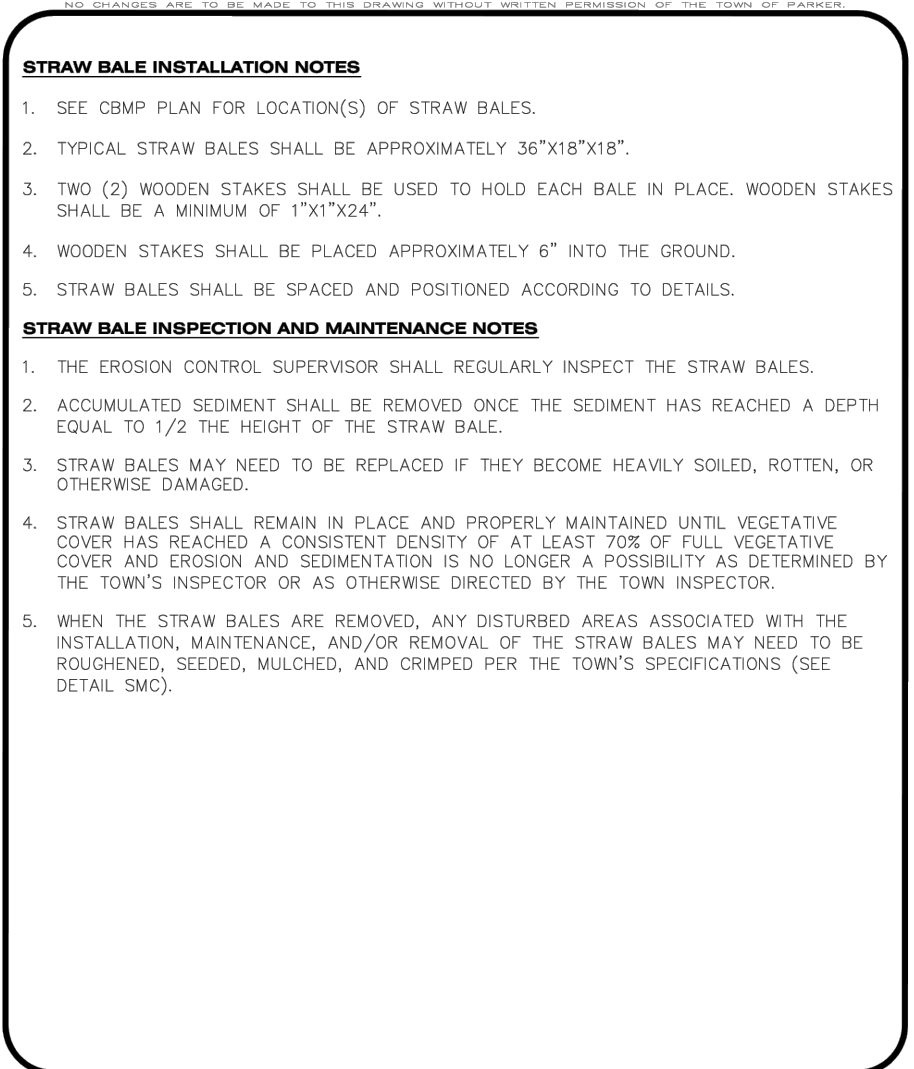
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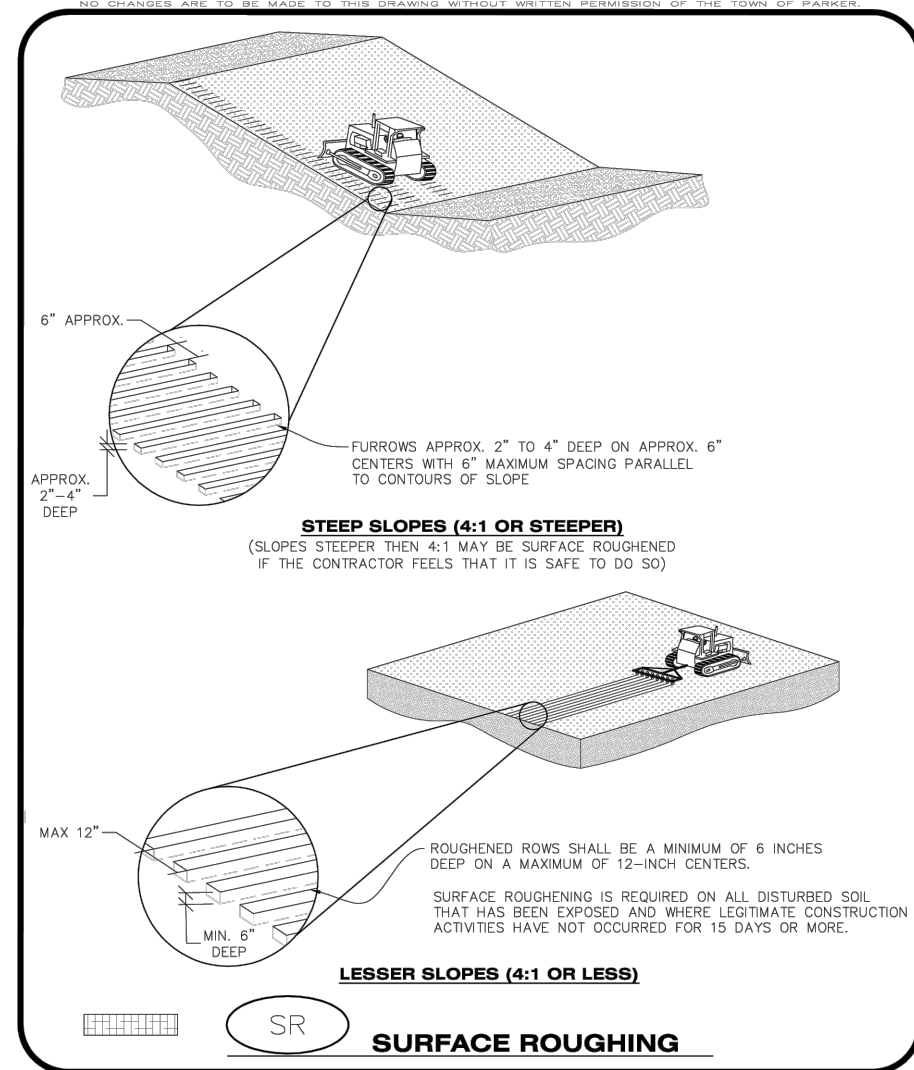
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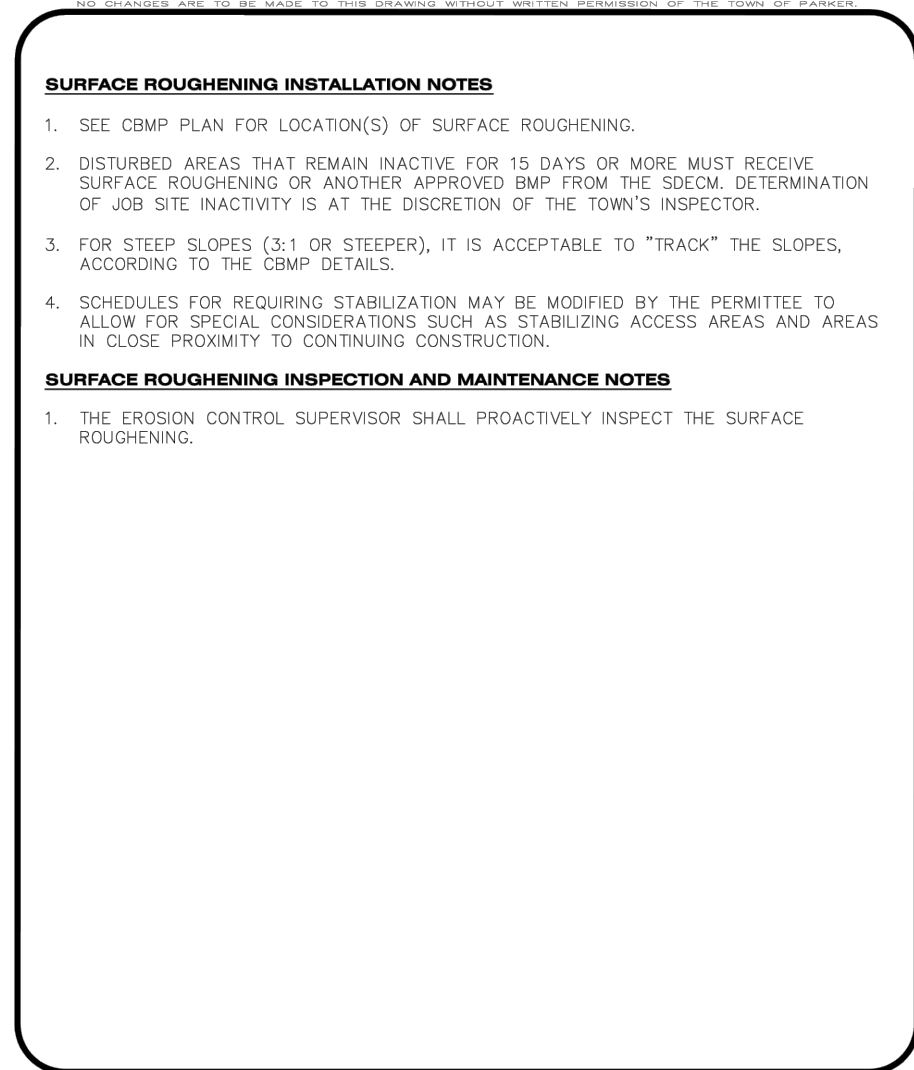
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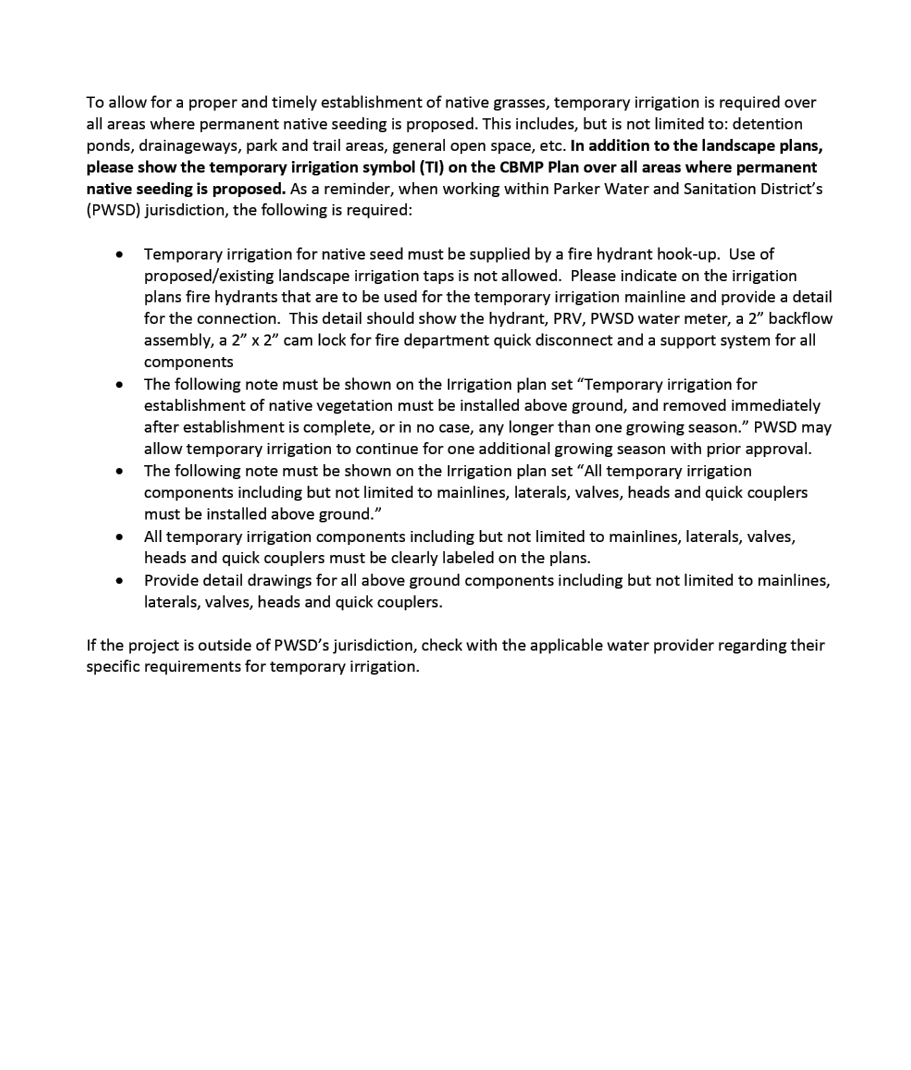
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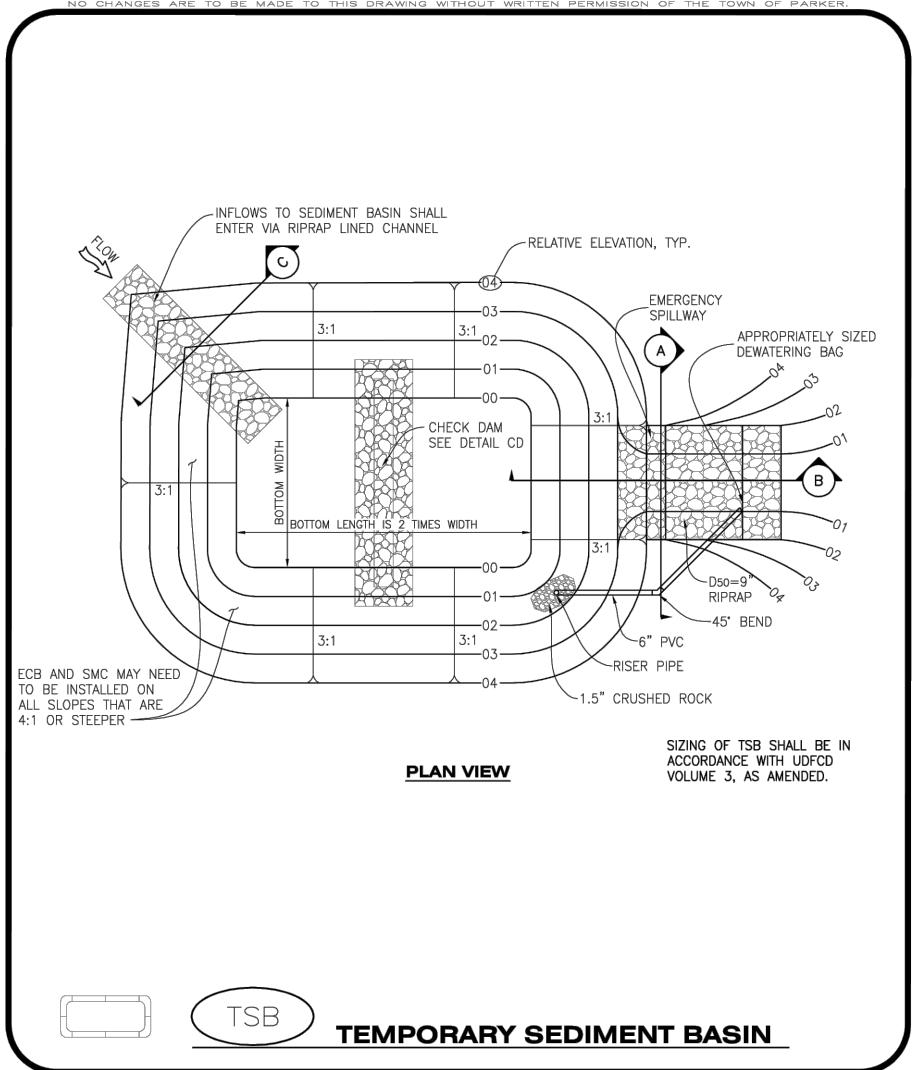
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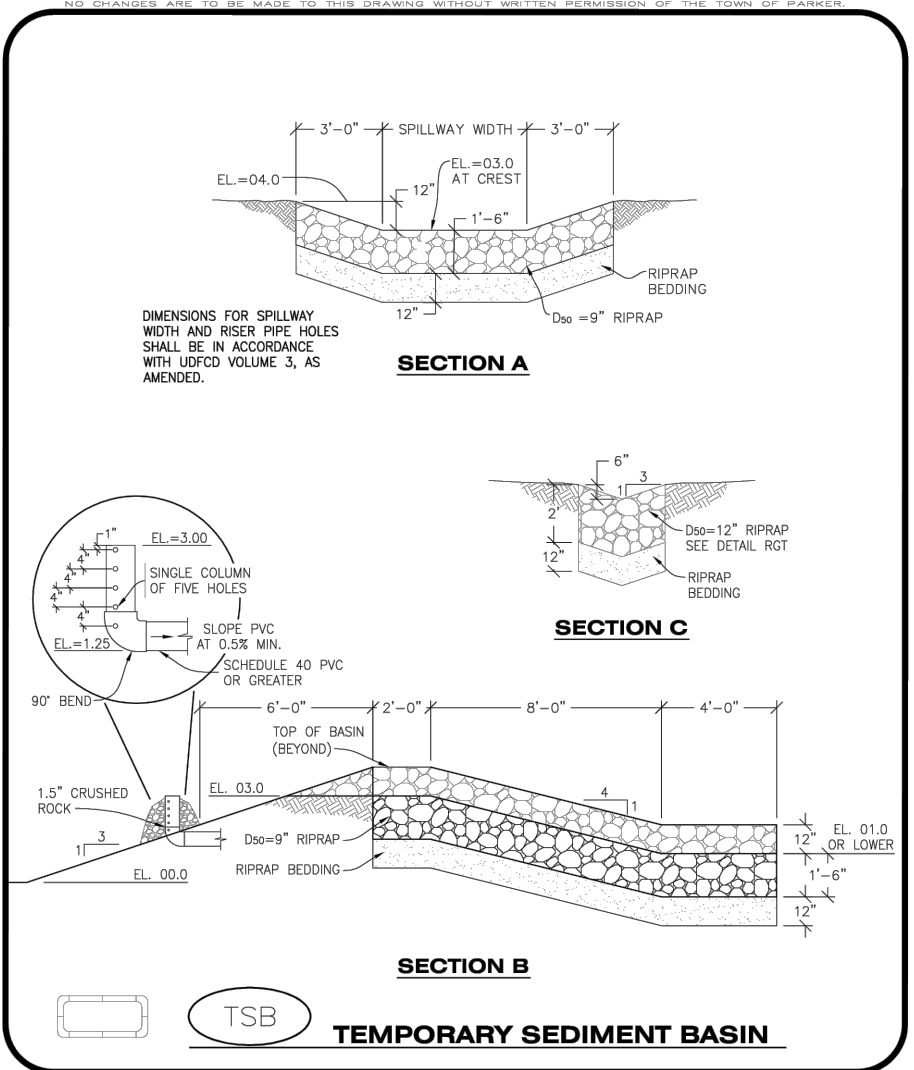
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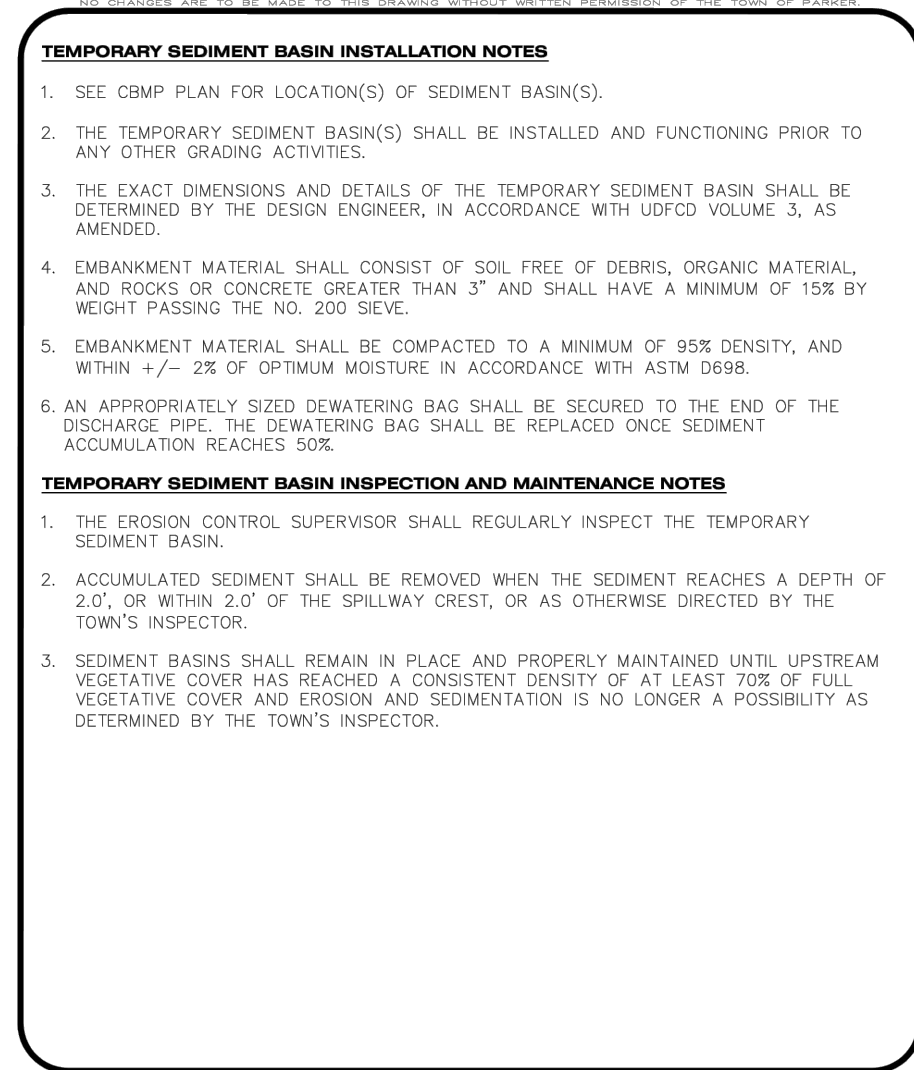
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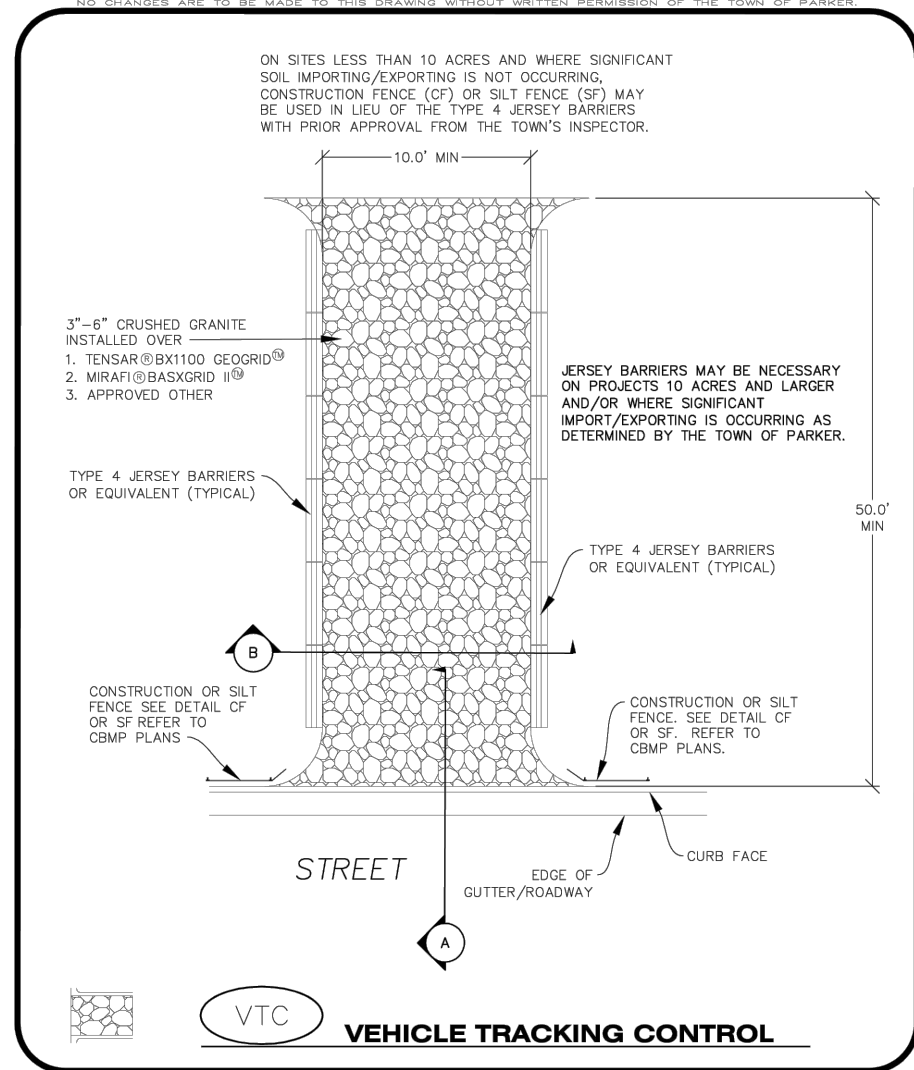
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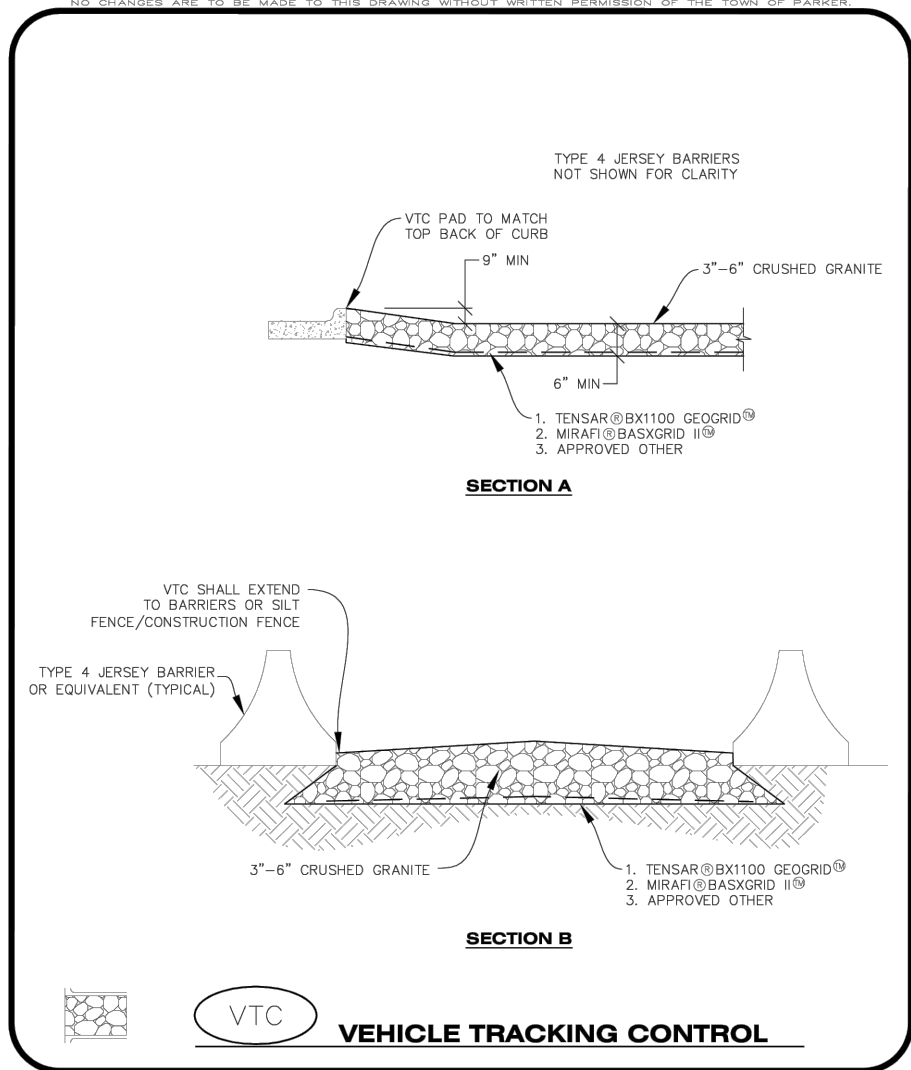
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**TSB** 3 OF 3  
REV. 2012



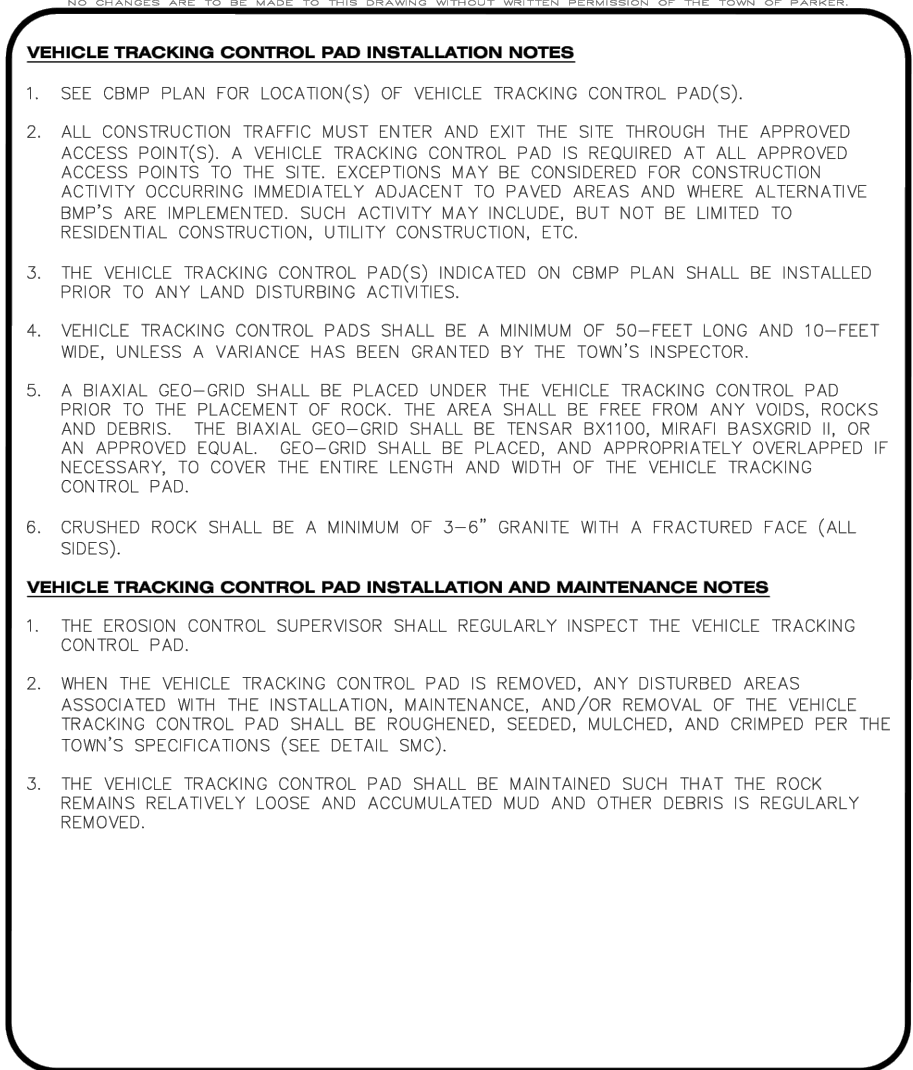
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**TSB** 1 OF 3  
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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
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**VTC** 3 OF 3  
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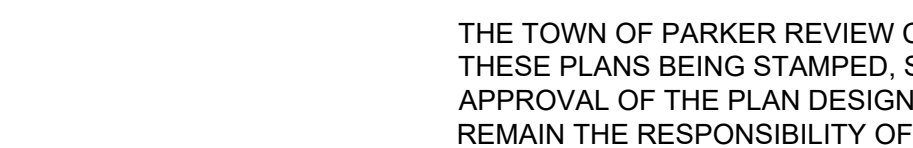
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REV. 2012



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**VTC** 2 OF 3  
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**VTC** 3 OF 3  
REV. 2012



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**VTC** 1 OF 3  
REV. 2012



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**VTC** 2 OF 3  
REV. 2012



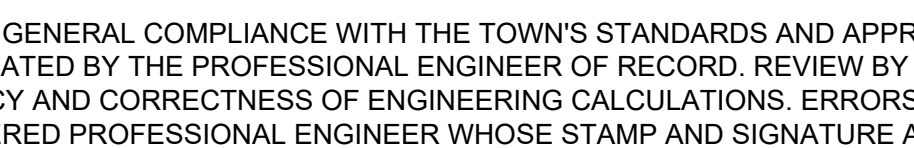
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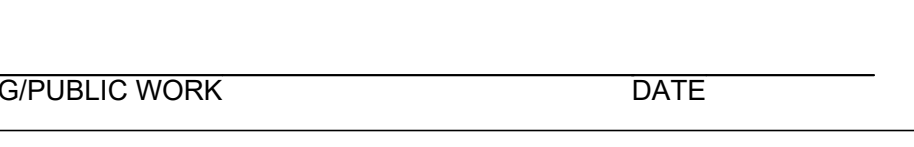
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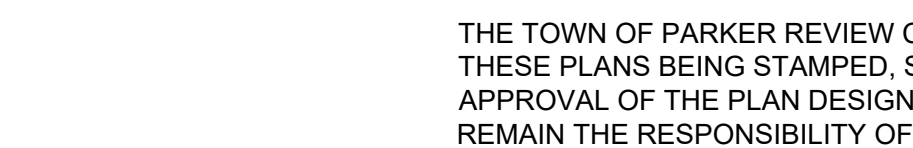
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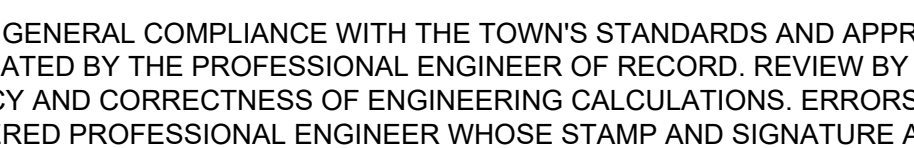
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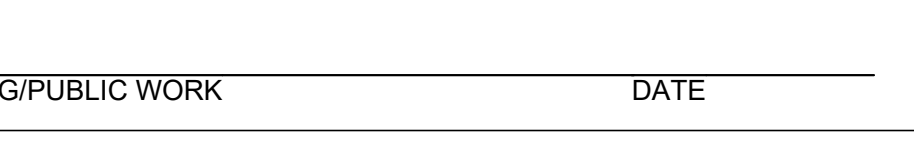
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THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGNED AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY OR CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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PROJECT: **LOT 1 PARKER POINTE SUBDIVISION FILING 1**

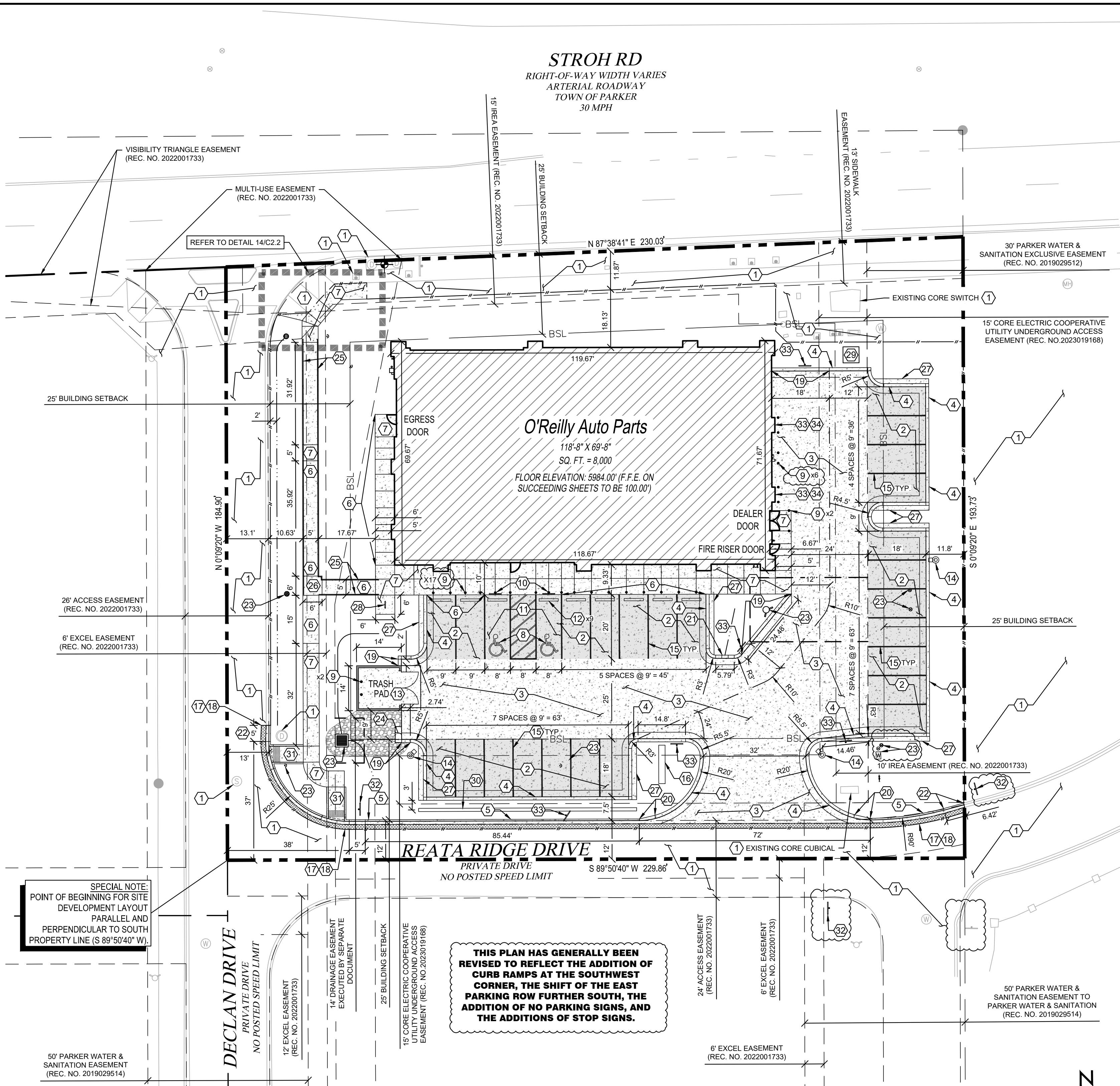
**NEW O'REILLY AUTO PARTS STORE**  
STROH RD  
PARKER, CO #2

**EROSION CONTROL DETAILS**

**O'Reilly AUTO PARTS**

CORPORATE OFFICES  
233 SOUTH PATTERSON  
SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

COMM # 4884  
DATE: 05/02/2025  
REVISION  
DATE:



- ### KEY NOTES
- PROTECT EXISTING IMPROVEMENTS IN PLACE.
  - CONCRETE PAVING (STANDARD DUTY) AT PARKING AREAS. REFER TO DETAIL 2/C2.2
  - CONCRETE PAVING (HEAVY DUTY) AT DRIVE AREAS, DRIVE APRONS, FIRE LANES, AND REFUSE AREA. REFER TO DETAIL 1/C2.2
  - INSTALL CONCRETE CURB PER DETAIL 3/C2.2.
  - INSTALL TOWN OF PARKER MEDIAN CURB AND GUTTER SECTION PER DETAIL 13 ON SHEET C2.4.
  - INSTALL CONCRETE SIDEWALK CURB PER DETAIL 4/C2.2. REFER TO STRUCTURAL DRAWINGS FOR LAYOUT OF CONTROL AND EXPANSION JOINTS ADJACENT TO BUILDING PERIMETER.
  - INSTALL CONCRETE SIDEWALK OR DOOR LANDING PER DETAIL 5/C2.2. REFER TO STRUCTURAL DRAWINGS FOR LAYOUT ADJACENT TO BUILDING PERIMETER.
  - INSTALL ACCESSIBLE PARKING SYMBOL PAVEMENT STRIPING PER DETAIL 6/C2.2.
  - INSTALL STEEL BOLLARD PER DETAIL 7/C2.2. PROVIDE (2) AT REFUSE CONTAINER AREA, (2) AT SECTIONAL OVERHEAD FREIGHT DOOR, (6) ALONG EAST SIDE OF BUILDING AND PROVIDE (17) AT FRONT ENTRY. REFER TO STRUCTURAL DRAWINGS FOR BOLLARDS ADJACENT TO BUILDING PERIMETER.
  - INSTALL ACCESSIBLE PARKING SIGN PER DETAIL 8/C2.2.
  - ACCESSIBLE PARKING SPACE WITH ACCESS AISLE (ADA COMPLIANT) TO SLOPE 2% MAXIMUM IN ALL DIRECTIONS. PROVIDE PAVEMENT MARKINGS TO MATCH STRIPING COLOR UNLESS OTHERWISE REQUIRED TO COMPLY WITH GOVERNING ACCESSIBILITY REGULATIONS.
  - INSTALL CONCRETE BUMPER BLOCK (8" W X 5" H X 6'-0" L) ANCHORED TO PAVING WITH (2) 1'-6" LONG #4 REBAR.
  - REFUSE ENCLOSURE. REFER TO 1/C2.3.
  - PARKING LOT LIGHTING SHOWN FOR REFERENCE ONLY. REFER TO SHEET US1 FOR MORE DETAIL.
  - INSTALL 4" WIDE PAVEMENT STRIPING AS SHOWN USING HIGHWAY MARKING PAINT - YELLOW (2 COATS).
  - SITE SIGN SHOWN FOR REFERENCE ONLY. REFER TO SHEET SG1.1 FOR CONSTRUCTION TYPE AND LOCATION. SIGN IS OWNER FURNISHED AND INSTALLED (REFER TO SCOPE OF WORK SCHEDULE). REFER TO SHEET US1 FOR ADDITIONAL REQUIREMENTS. NOTE: SIGN APPROVAL NOT A PART OF THIS SITE PLAN AND SHALL BE APPROVED THROUGH SEPARATE PERMIT.
  - LIMITS OF NEW PAVING. MATCH EXISTING PAVEMENT TRANSITION ELEVATIONS. AT TRANSITIONS OF NEW CONCRETE TO EXISTING CONCRETE PAVEMENT, EPOXY DOWEL 6" MINIMUM INTO EXISTING WITH #4 X 1'-6" SMOOTH DOWELS MATCHING NEW PAVEMENT REINFORCEMENT SPACING. PROVIDE SEALANT AT JOINTS.
  - 1.5' WIDE, 2" DEEP GRADE SX ASPHALT MILL & OVERLAY PER GEOTECHNICAL RECOMMENDATIONS.
  - TRANSITION TO 0" CURB OVER DISTANCE AS SHOWN.
  - 5' TRANSITION FROM 6" WIDE CURB TO 1' WIDE CURB.
  - 2' CURB CUT PER DETAIL 9/C2.2.
  - TRANSITION TO EXISTING 6" CURB AND GUTTER.
  - REFER TO SHEET C3.1 FOR STORM AND UTILITY IMPROVEMENT DETAILS.
  - INSTALL 5' X 2' CONCRETE SPLASH PAD RECESSED 2" MIN. FROM PARKING ACCESS AISLE PAVEMENT. REFER TO GRADING PLAN FOR ELEVATIONS.
  - INSTALL SIDEWALK RAMP RAILING PER DETAIL 16/C2.2.
  - 5' X 5' CLEAR SPACE AT RAMP CHANGE IN DIRECTION.
  - INSTALL 1.5' WIDE, 4" THICK CONCRETE STRIP ATTACHED TO PARKING STALL BACK OF CURB.
  - INSTALL INVERTED U BIKE RACK. SEE DETAIL 11/C2.2.
  - INSTALL TRANSFORMER PAD PER CORE ELECTRIC STANDARDS.
  - INSTALL PARKING LOT SCREENING WALL. REFER TO STRUCTURAL FOR MORE DETAIL.
  - INSTALL DIRECTIONAL CURB RAMP PER TOWN OF PARKER STANDARD DETAIL 15 ON SHEET C2.4. REFER TO DETAIL 14/C2.2 FOR SPECIFIC CONSTRUCTION DIMENSIONS.
  - INSTALL R1-1 STOP SIGN PER LATEST MUTCD STANDARDS.
  - INSTALL "NO PARKING FIRE LANE" SIGN PER DETAIL ON SHEET C2.4 AND THE LATEST MUTCD STANDARDS.
  - WALL MOUNT "NO PARKING FIRE LANE" SIGN PER DETAIL ON SHEET C2.4 TO BUILDING EXTERIOR PER ARCHITECT RECOMMENDATIONS.

### ZONING CODE

ZONING CLASSIFICATION:	MIXED-USE COMMUNITY
PROPERTY AREA:	43,516 SF (0.999 AC)
PROPOSED BUILDING AREA:	8,000 SF
PROPOSED PARKING PAVEMENT:	12,900 SF
PROPOSED SIDEWALK PAVEMENT:	2,250 SF

PARKING SUMMARY	1 SP. PER 300 SF GFA
PARKING FORMULA:	9' x 20' AT BUILDING
SPACE SIZE:	9' x 18' ALL OTHER LOCATIONS
SPACES REQUIRED:	27
SPACES PROVIDED:	27
H.C. SPACES PROVIDED:	2

BUILDING SETBACK DISTANCE	
NORTH PROPERTY LINE	VARIABLE (25.4' MIN., 31.8' MAX)
EAST PROPERTY LINE	VARIABLE (58.8' MIN., 60.5' MAX)
SOUTH PROPERTY LINE	VARIABLE (59.8' MIN., 92.5' MAX)
WEST PROPERTY LINE	VARIABLE (51.4' MIN., 53.1' MAX)

- ### GENERAL NOTES
- REFER TO PROJECT MANUAL FOR ADDITIONAL REQUIREMENTS. ALL ON SITE CONCRETE TO BE 4,500 PSI.
  - SITE DIMENSIONS TO FACE OF CONCRETE FOUNDATION, SIDEWALK, CURB GUTTER LINE, PROPERTY LINE, OR CENTER LINE OF STRIPING UNLESS OTHERWISE NOTED.
  - COORDINATE WORK WITH OTHER SITE RELATED DEVELOPMENT DRAWINGS.
  - REFER TO STRUCTURAL DRAWINGS FOR BUILDING DIMENSIONS AND LAYOUT OF SIDEWALKS ADJACENT TO BUILDING PERIMETER.
  - PRIOR TO INSTALLATION, CONTRACTOR TO VERIFY LOCATIONS OF LIGHT POLES, LANDSCAPING, AND UTILITIES DO NOT CONFLICT WITH SITE SIGN LOCATION SHOWN. IF CONFLICT IS DISCOVERED, CONTRACTOR TO NOTIFY OWNER'S REPRESENTATIVE PRIOR TO PROCEEDING WITH THE WORK.

### SYMBOLS LEGEND

NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.

	RIGHT-OF-WAY
	O'REILLY PROPERTY LINE
	ADJACENT PROPERTY LINE
	EXISTING EASEMENT
	PROPOSED EASEMENT
	BSL - BUILDING SETBACK LINE
	ROAD CENTERLINE
	LIMIT OF DISTURBANCE
	PROPOSED SAWCUT
	PROPOSED FLOWLINE
	PROPOSED SIGN
	PROPOSED WHEEL STOP
	PROPOSED BOLLARD
	PROPOSED FDC
	PROPOSED FIRE HYDRANT
	PROPOSED GATE VALVE
	PROPOSED CLEANOUT
	PROPOSED LIGHT POLE
	PROPOSED BUILDING FOOTPRINT
	PROPOSED LIGHT DUTY CONCRETE
	PROPOSED HEAVY DUTY CONCRETE
	PROPOSED MILL & OVERLAY
	PROPOSED RIPRAP

## 1 SITE DEVELOPMENT PLAN

C2.1 SCALE: 1" = 20'-0"

**CAUTION:** INFORMATION ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.



### BENCHMARK

DOUGLAS COUNTY BENCHMARK 1.060032, EL=5,903.17 FEET (NAVD88) - 3 1/4" INCH ALUMINUM CAP (DOUGLAS COUNTY GIS SECONDARY CONTROL MONUMENT 1.060032) AT THE SOUTHEAST CORNER OF THE PARKING LOT OF STROH RANCH SOCCER FIELDS, ON THE NORTH SIDE OF STROH RD, EAST OF THE INTERSECTION OF STROH ROAD AND N. MOTSENBOCKER ROAD, CITY OF PARKER, COLORADO.

### BASIS OF BEARING

BEARINGS SHOWN HEREON ARE BASED ON THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST OF THE 6TH P.M., MONUMENTED AS SHOWN ON THIS SURVEY PLAT AS BEARING NORTH 89°33'14" EAST, WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO.

### UNAUTHORIZED CHANGES & USES

THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES OF THESE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THESE PLANS PRIOR TO CONSTRUCTION. CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF DESIGN PROFESSIONAL.

### NOTE TO CONTRACTOR

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITIES, PIPES, AND/OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF AVAILABLE RECORDS. THERE MAY BE EXISTING UTILITIES NOT SHOWN ON THESE PLANS. THE CONTRACTOR SHALL ASCERTAIN THE TRUE VERTICAL AND HORIZONTAL LOCATION OF THOSE UNDERGROUND UTILITIES TO BE USED PRIOR TO CONSTRUCTION AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY PUBLIC OR PRIVATE UTILITIES, SHOWN OR NOT SHOWN HEREON.

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LOT 1 PARKER POINTE SUBDIVISION FILING 1

**TIMOTHY M. GUILLOT**  
ARCHITECT

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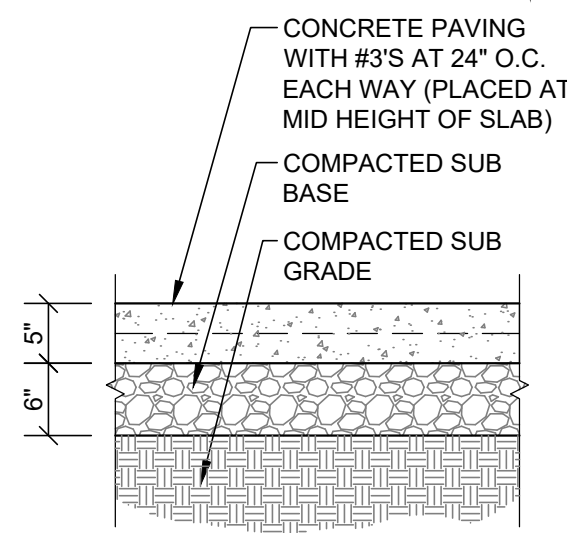
PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
STROH RD  
PARKER, CO #2

**SITE DEVELOPMENT PLAN**

CORPORATE OFFICES  
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SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

COMM #	4884
DATE:	02/14/2025
REVISION	
DATE:	05/02/2025

PROVIDE CONTROL JOINTS AT 12'-0" O.C. (25% SLAB THICKNESS)

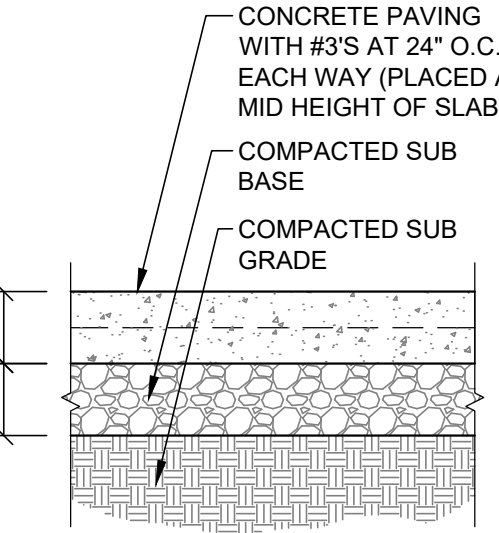


\*\*PAVEMENT DESIGN IS BASED ON GEOTECHNICAL REPORT FROM GEG PROVIDED BY THE OWNER DATED 10/30/2024. CONTRACTOR IS RESPONSIBLE TO FULLY READ AND FOLLOW THE RECOMMENDATIONS OF THE PROJECT GEOTECHNICAL REPORT.

**1 CONCRETE (STANDARD DUTY) PAVING SECTION**

C2.2 SCALE: 3/4" = 1'-0"

PROVIDE CONTROL JOINTS AT 12'-0" O.C. (25% SLAB THICKNESS)

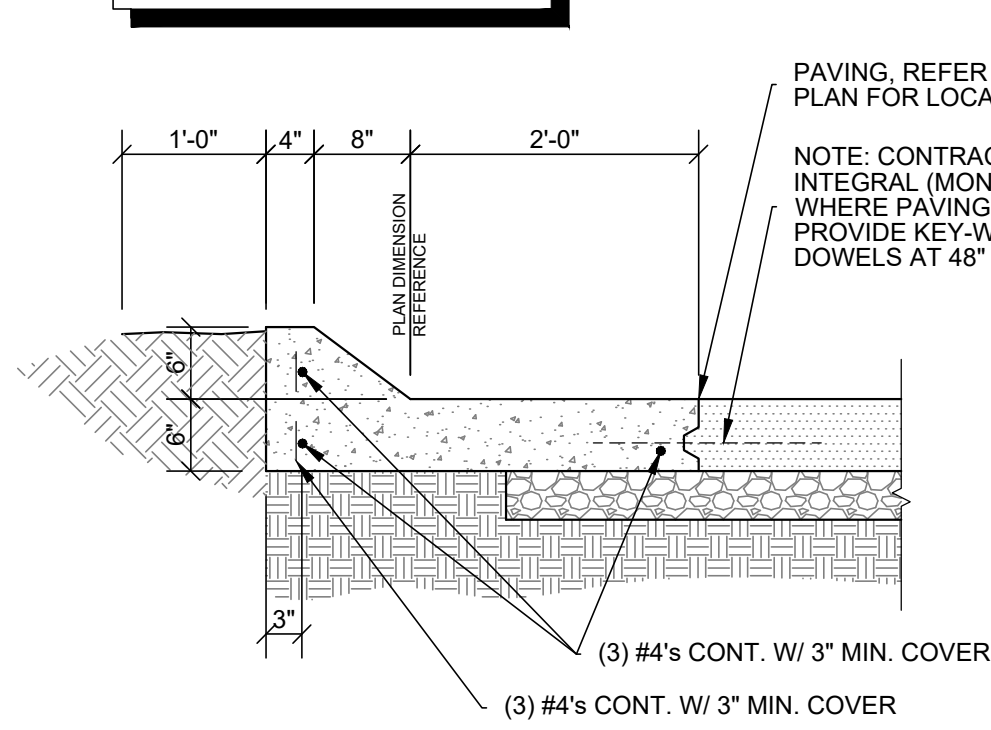


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**2 CONCRETE (HEAVY DUTY) PAVING SECTION**

C2.2 SCALE: 3/4" = 1'-0"

PROVIDE CONTROL JOINTS AT 10'-0" O.C. (25% SLAB THICKNESS)

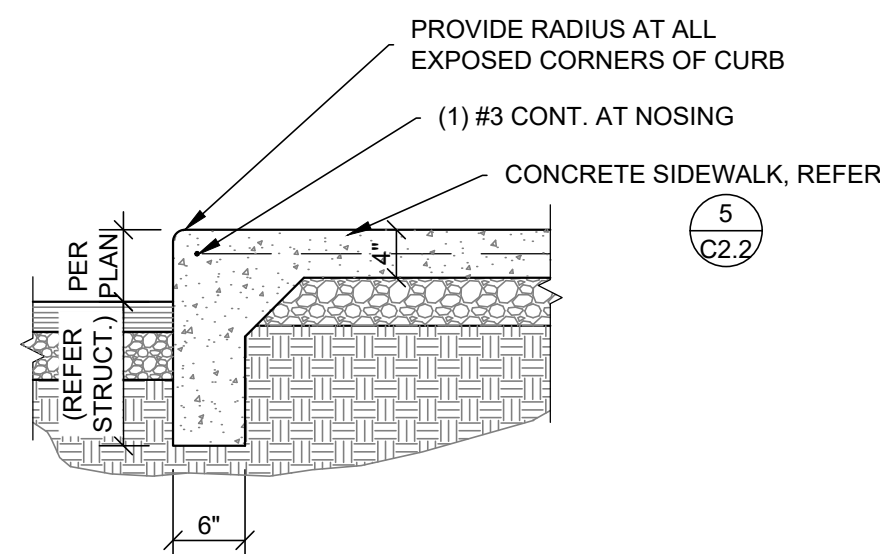


PAVING, REFER TO SITE DEVELOPMENT PLAN FOR LOCATION AND TYPE  
NOTE: CONTRACTOR'S OPTION TO POUR INTEGRAL (MONOLITHIC) CURB AND PAVING, WHERE PAVING POURED SEPARATE, PROVIDE KEY-WAY OR #4 X 1'-0" DOWELS AT 48" O.C.

**3 CONCRETE CURB SECTION**

C2.2 SCALE: 3/4" = 1'-0"

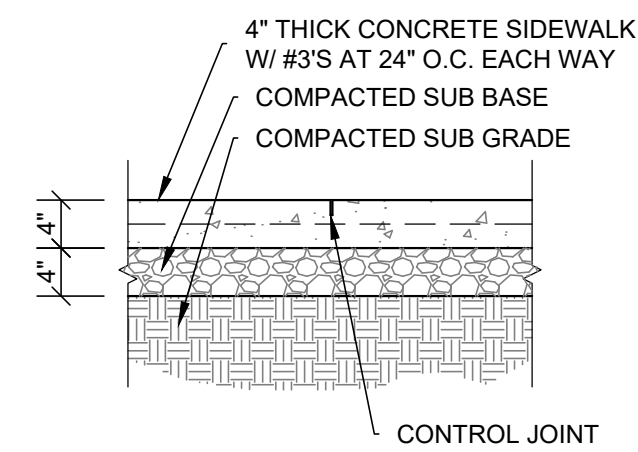
NOTE: REFER TO SITE GRADING PLAN FOR ELEVATIONS OF PAVEMENT ADJACENT TO SIDEWALKS.



**4 CONCRETE SIDEWALK CURB SECTION**

C2.2 SCALE: 3/4" = 1'-0"

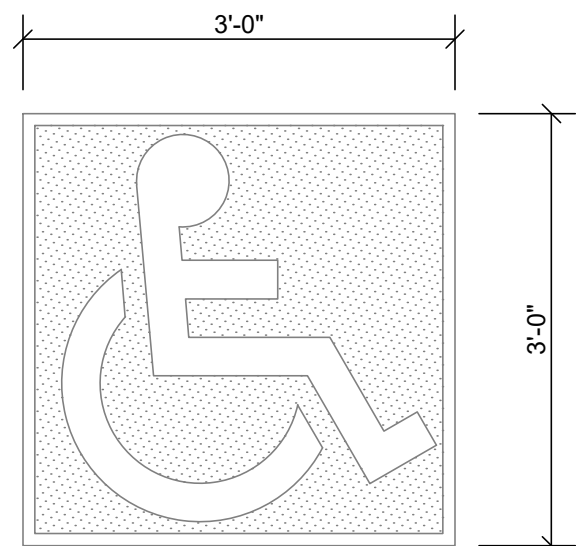
PROVIDE TOOLED CONTROL JOINTS AT 6' O.C. (25% SLAB THICKNESS) AND ISOLATION JOINTS AT 24' O.C., UNLESS OTHERWISE NOTED.



**5 CONCRETE SIDEWALK SECTION**

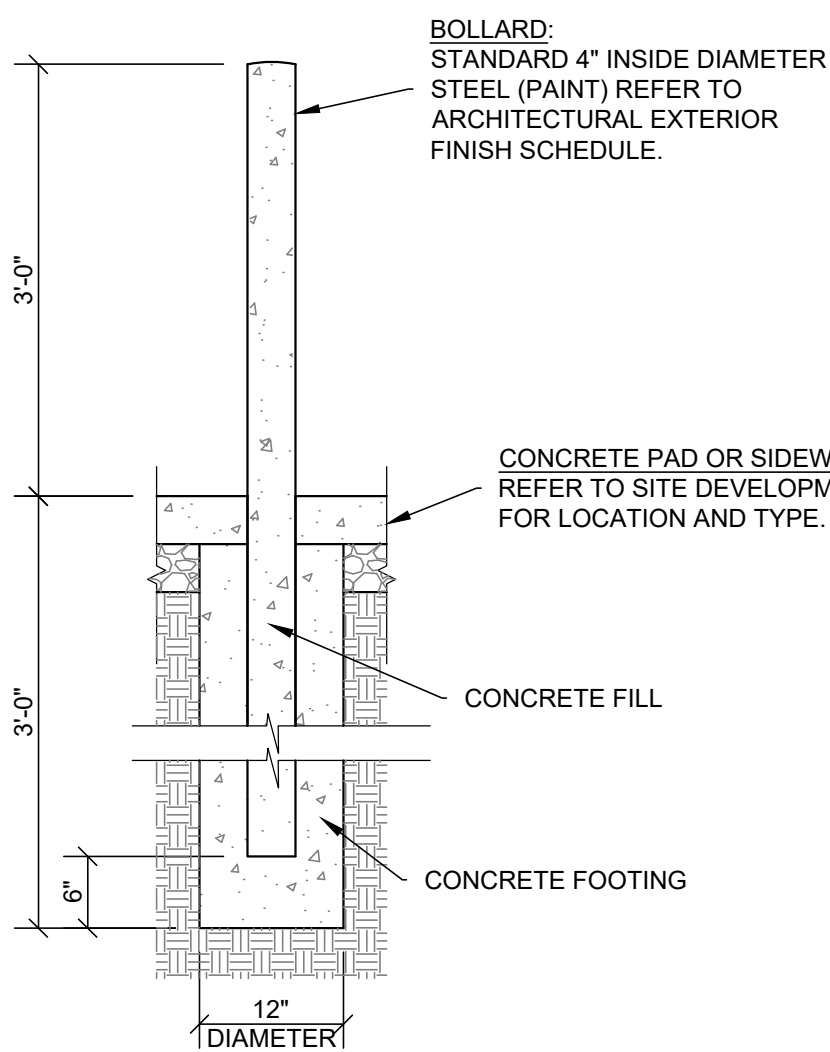
C2.2 SCALE: 3/4" = 1'-0"

WHITE SYMBOL OF ACCESSIBILITY TO BE 36" HIGH BY 36" WIDE ON A BLUE BACKGROUND. (USE HIGHWAY GRADE MARKING PAINT)



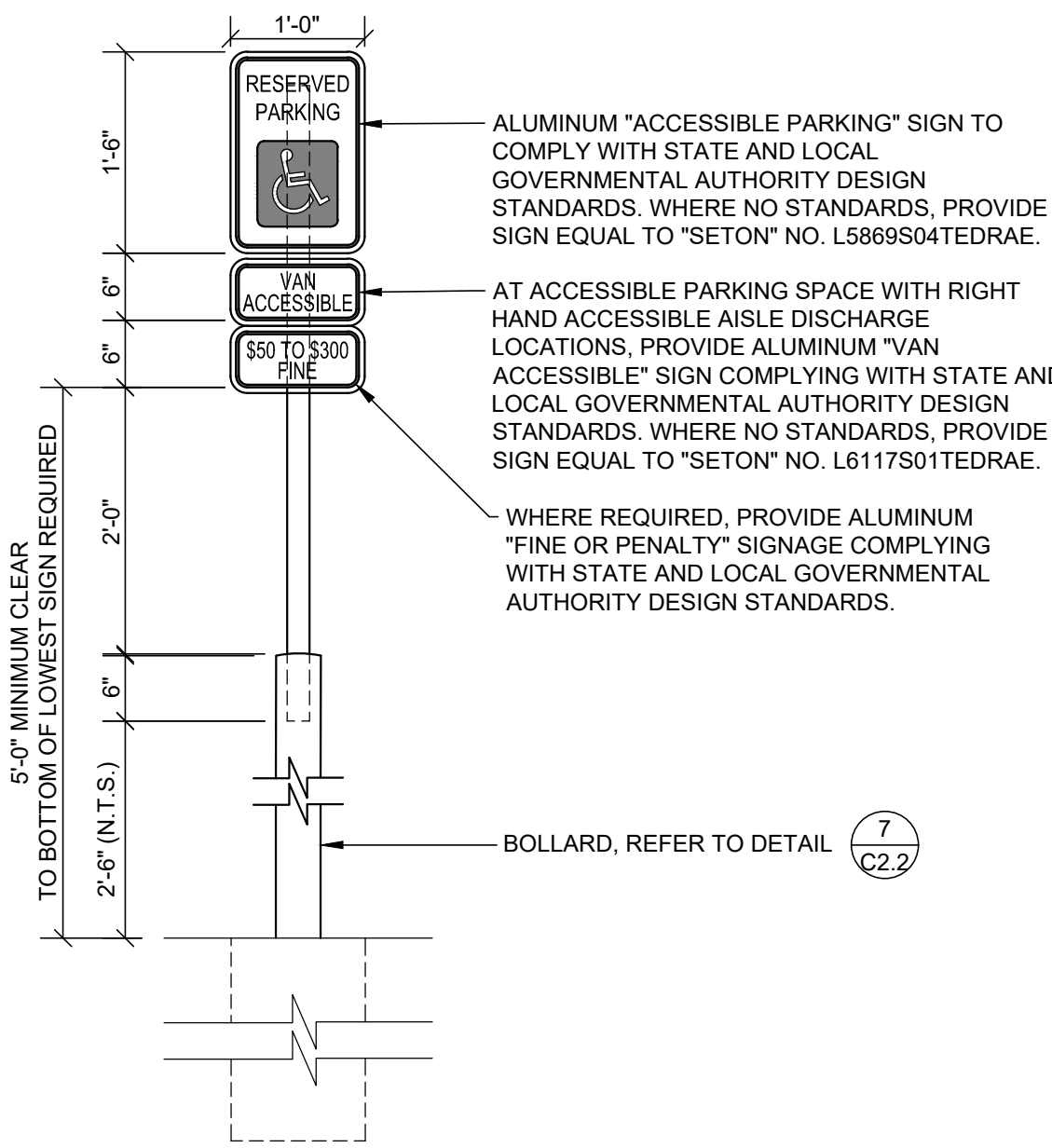
**6 ACCESSIBLE PARKING PAVEMENT STRIPING DETAIL**

C2.2 SCALE: 3/4" = 1'-0"



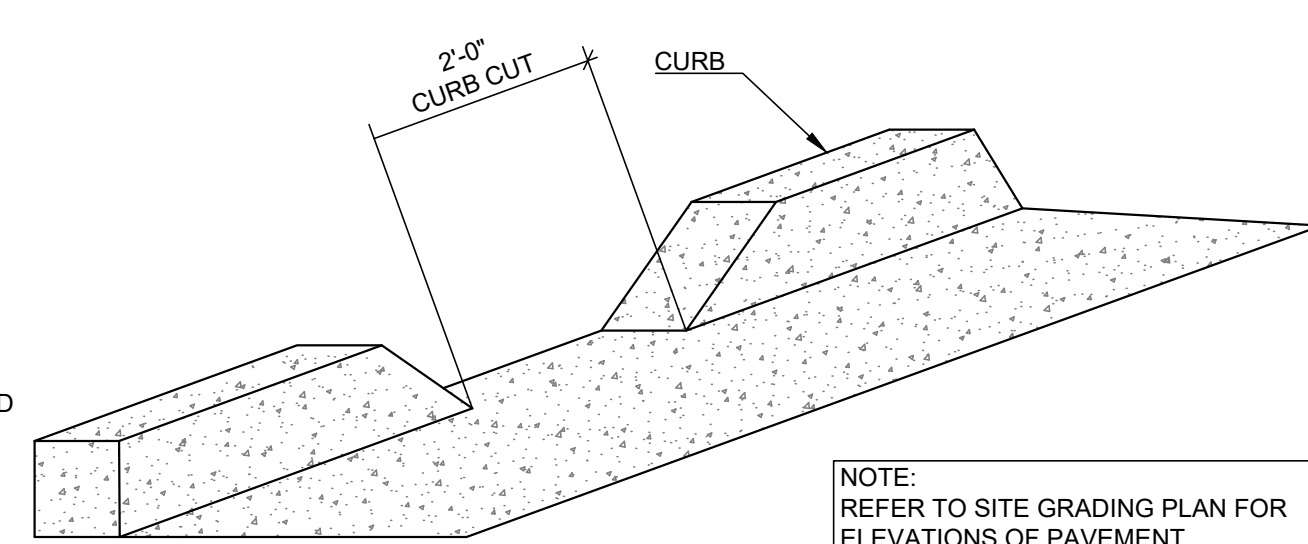
**7 STEEL BOLLARD SECTION**

C2.2 SCALE: 3/4" = 1'-0"



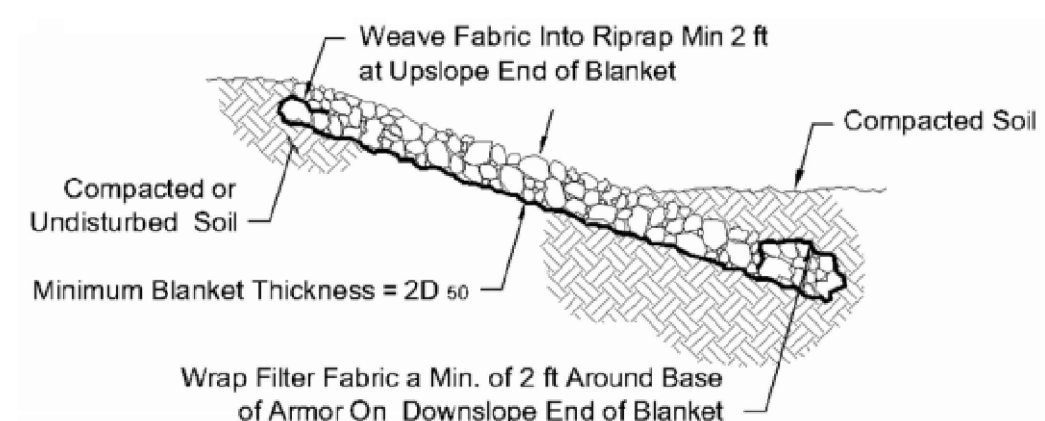
**8 HANDICAP PARKING SIGN DETAIL**

C2.2 SCALE: 3/4" = 1'-0"



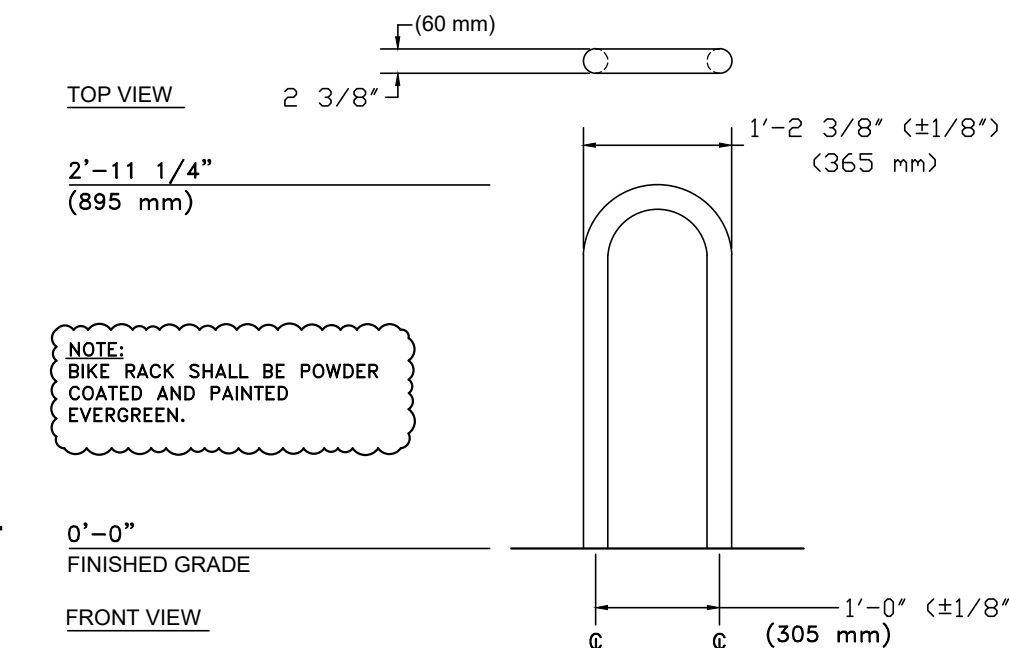
**9 2' WIDE CURB CUT**

C2.2 SCALE: N.T.S.



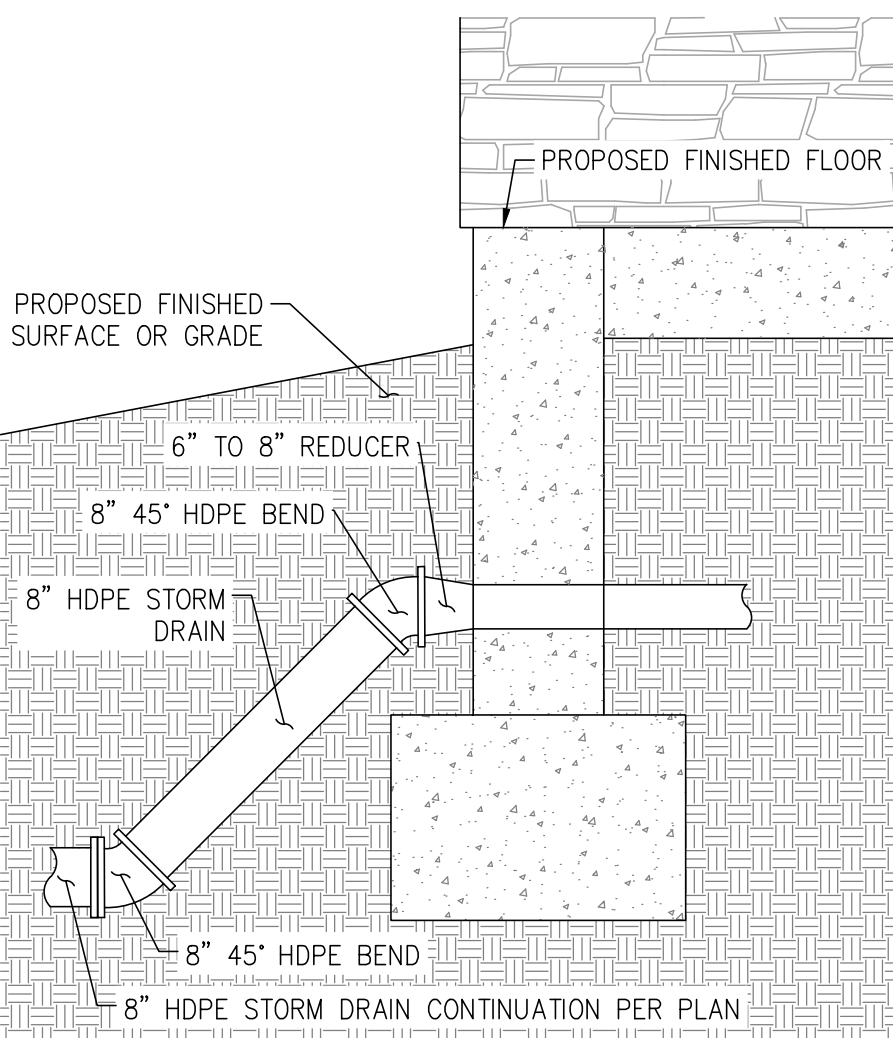
**10 RIPRAP DETAIL**

C2.2 SCALE: N.T.S.



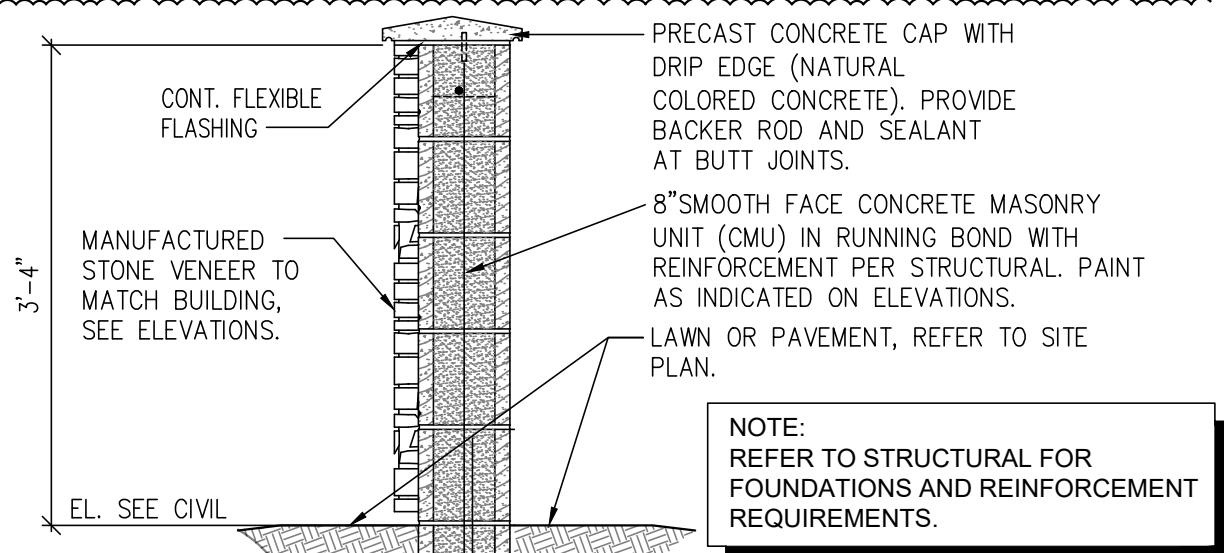
**11 BICYCLE PARKING (INVERTED "U")**

C2.2 SCALE: 3/4" = 1'-0"



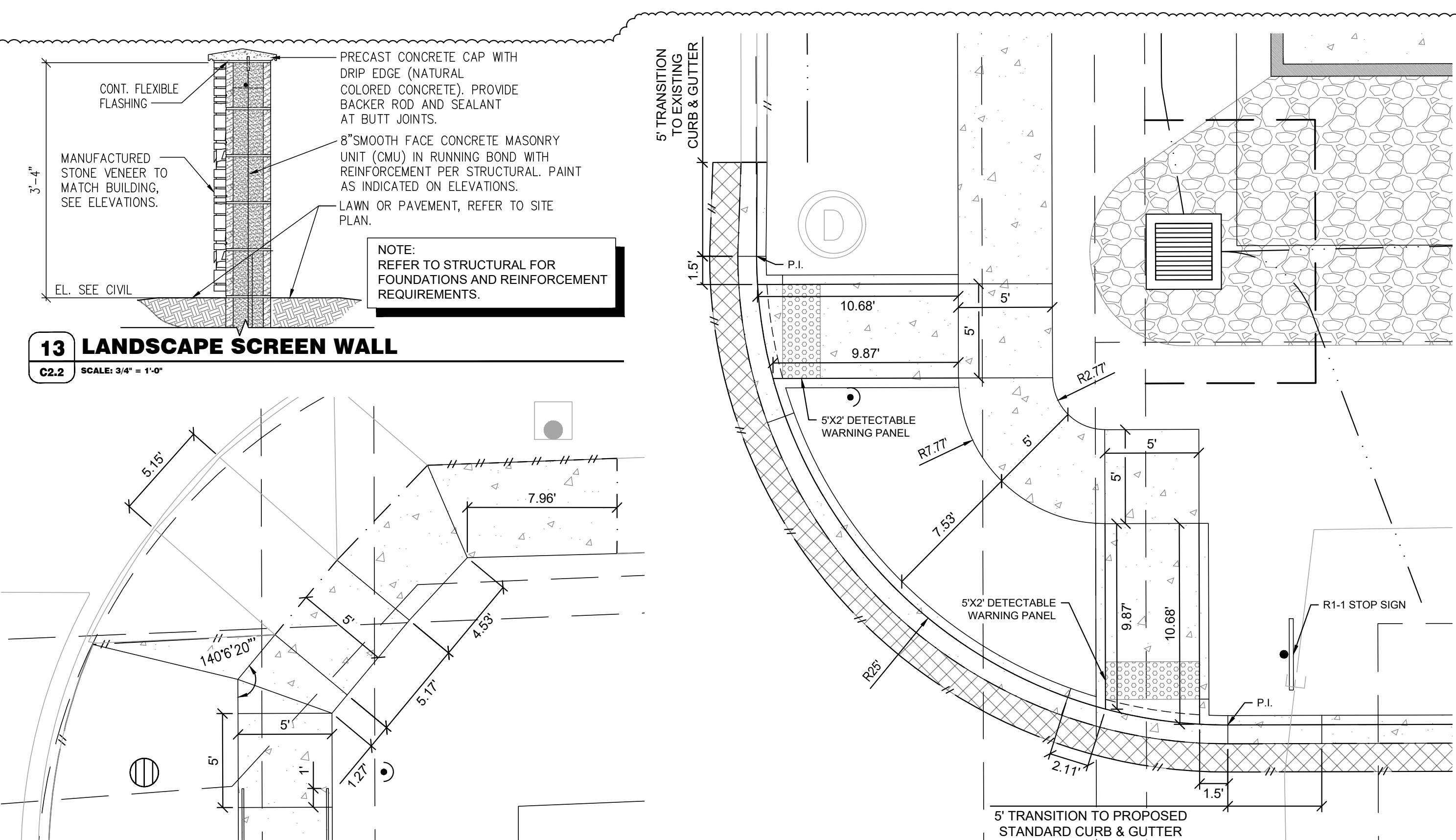
**12 DOWNSPOUT CONNECTION**

C2.2 SCALE: N.T.S.



**13 LANDSCAPE SCREEN WALL**

C2.2 SCALE: 3/4" = 1'-0"

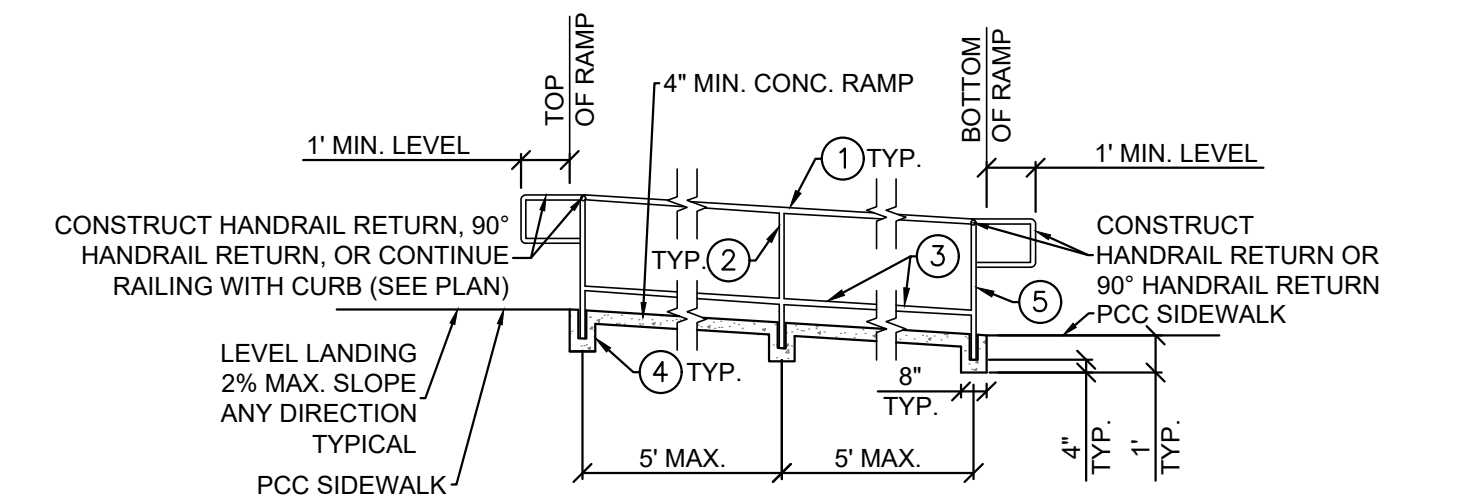


**14 NORTHWEST ACCESSIBLE RAMP TIE-IN DETAIL**

C2.2 SCALE: 1" = 5'

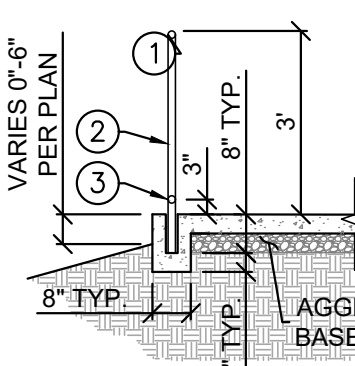
**15 PROPOSED CURB RAMP DIMENSION DETAIL**

C2.2 SCALE: 1" = 5'



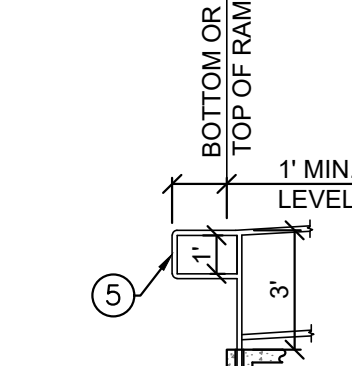
**RAMP HANDRAIL DETAIL**

N.T.S.



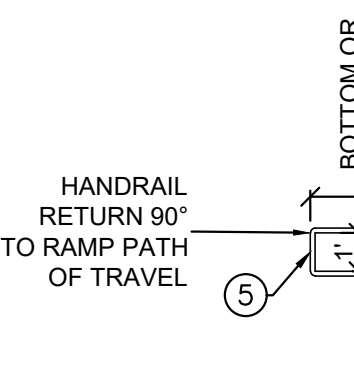
**TYPICAL HANDRAIL SECTION**

N.T.S.



**RAMP HANDRAIL RETURN**

N.T.S.



**RAMP 90° HANDRAIL RETURN**

N.T.S.

**16 HANDRAIL DETAILS**

C2.2 SCALE: N.T.S.

NOTE:  
1. ALL HANDRAILS, POSTS, AND APPURTENANCES TO BE PAINTED "CYBERSPACE" SW7076 (OR EQUAL) PER O'REILLY SPECIFICATIONS AND ARCHITECT RECOMMENDATIONS.  
2. HANDRAIL GRIPPING SURFACES AND ANY SURFACES ADJACENT TO THEM SHALL BE FREE OF SHARP OR ABRASIVE ELEMENTS AND SHALL HAVE ROUNDED EDGES.  
3. HANDRAILS SHALL NOT ROTATE WITHIN THEIR FITTINGS.

**LEGEND:**

- 1 1 1/2" O.D. GALVANIZED STEEL HANDRAIL.
  - 2 1 1/2" O.D. GALVANIZED STEEL POST.
  - 3 1 1/2" O.D. GALVANIZED STEEL GUIDE RAIL.
  - 4 BLOCK OUT 2 1/2" x 2 1/2" x 8" DEEP SQUARE POST OPENING PRIOR TO POURING CONCRETE. FILL WITH NON-SHRINK GROUT AFTER POST IS PLACED, ALIGNED AND LEVELED.
  - 5 1 1/2" STEEL HANDRAIL EXTENSION RETURN. EXTENSION SHALL EXTEND A MINIMUM OF 1' IN DIRECTION OF RAMP. IF EXTENSION WOULD CREATE A HAZARD, THE EXTENSION MAY BE TURNED 90 DEGREES TO THE RUN OF THE RAMP.
- ANY WELDS SHALL BE SLOT OR FILLET WELDS EQUAL TO THICKNESS OF PIPE. WELD ALL JOINTS ALL AROUND.

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Since 1964

LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
STROH RD  
PARKER, CO #2

**TIMOTHY M. GUILLOT**  
ARCHITECT

417.862.6558

Fax: 417.862.3265

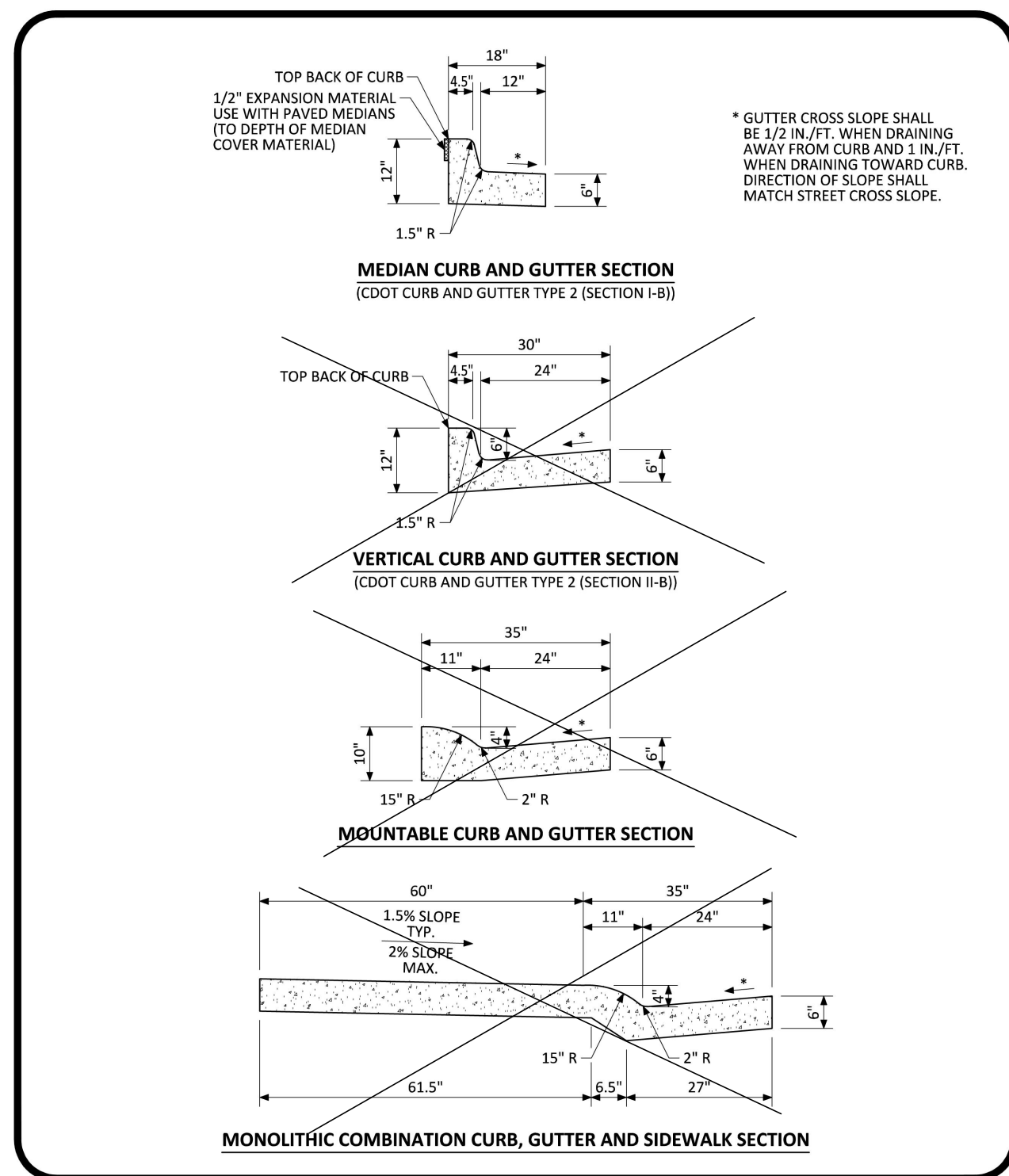
1736 East Sunshine, Suite 417  
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e-mail: architect@estieryschneider.com

**O'Reilly AUTO PARTS**  
CORPORATE OFFICES  
233 SOUTH PATTERSON  
SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

COMM # 4884  
DATE: 02/14/2025  
REVISION  
DATE: 05/02/2025





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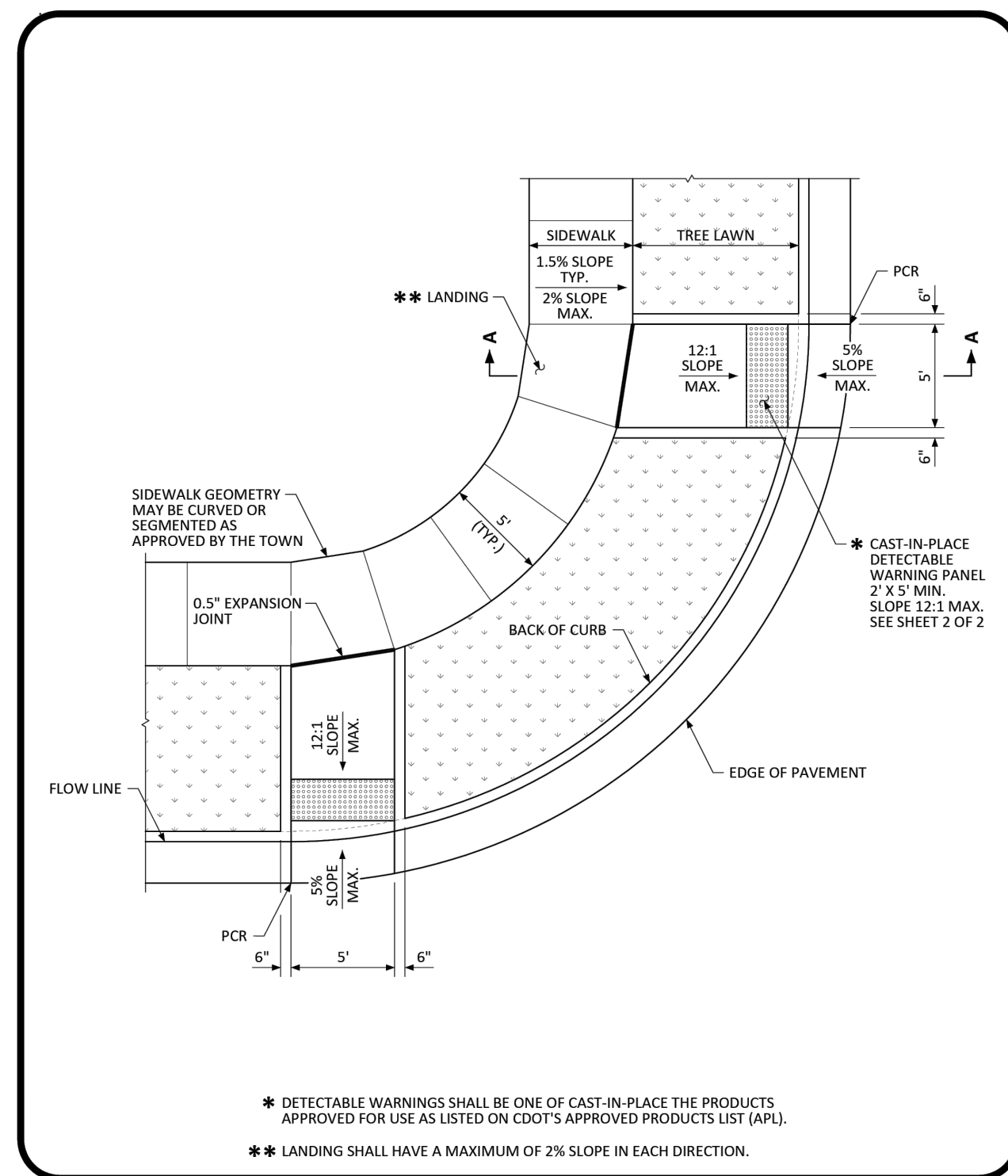
**PARKER COLORADO**

**CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL**

DATE NOVEMBER 2020

DETAIL 3

1 OF 1



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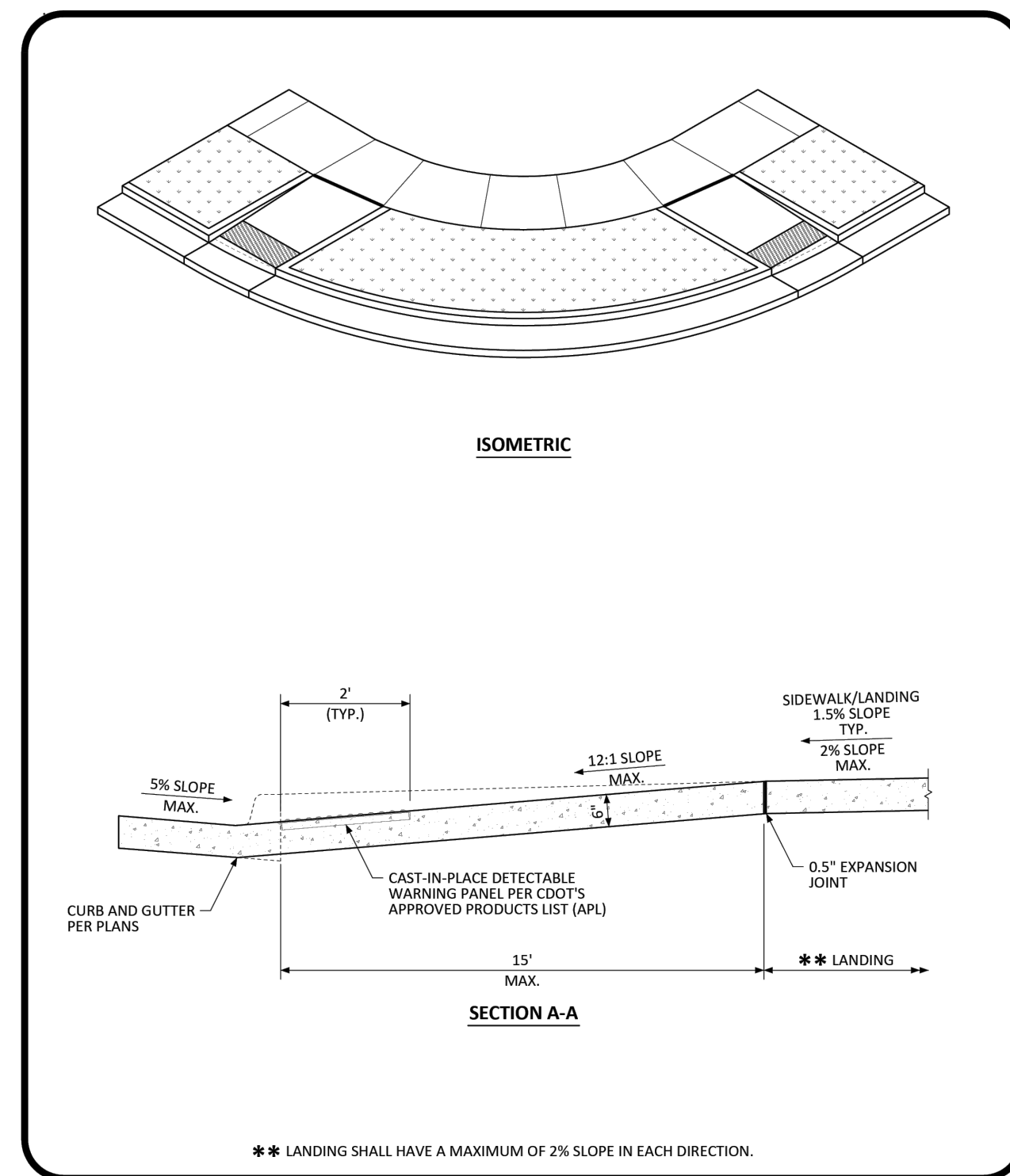
**PARKER COLORADO**

**DIRECTIONAL CURB RAMP LAYOUT STANDARD DETAIL**

DATE NOVEMBER 2020

DETAIL 15

3 OF 4



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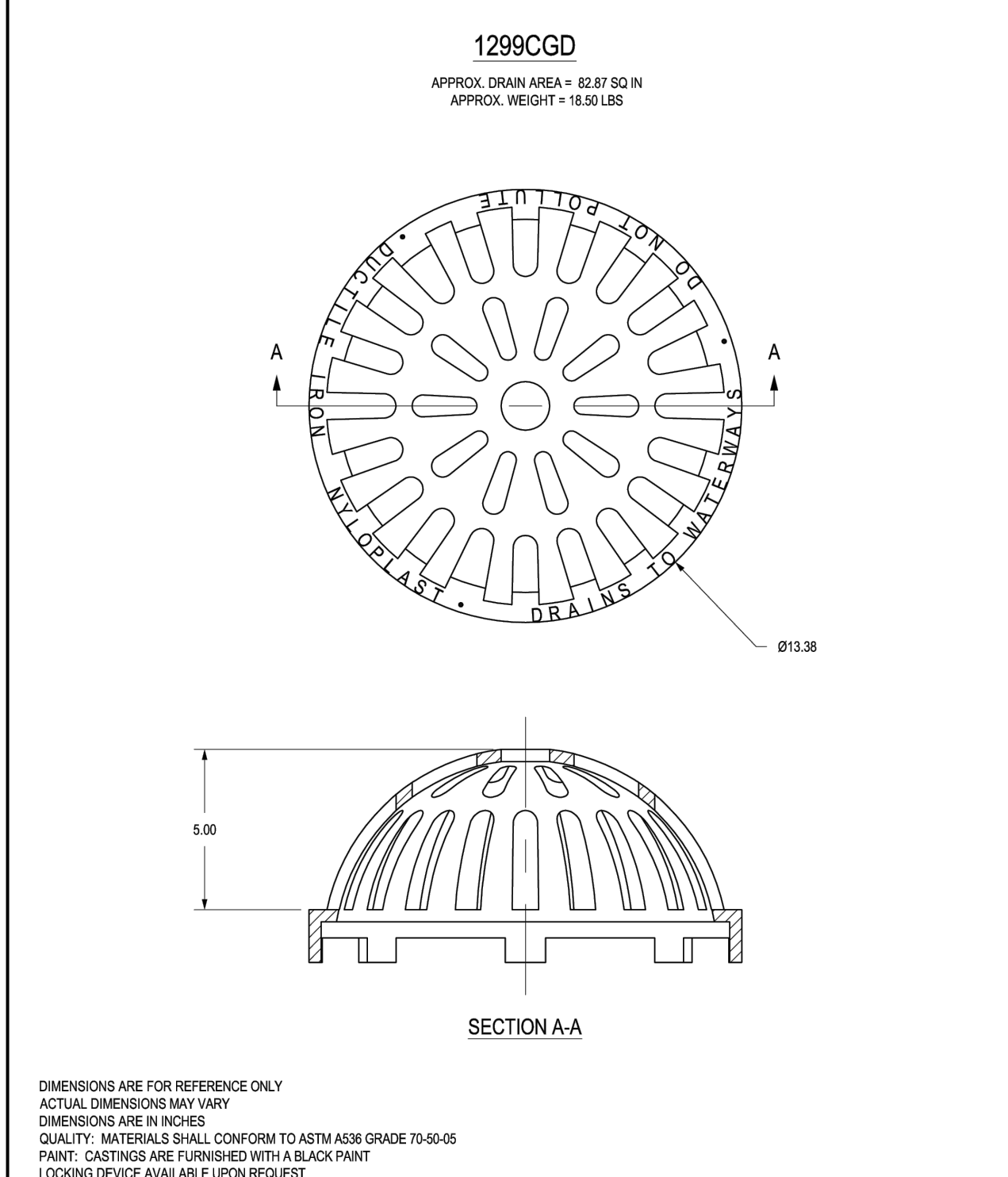
**PARKER COLORADO**

**DIRECTIONAL CURB RAMP LAYOUT STANDARD DETAIL**

DATE NOVEMBER 2020

DETAIL 15

4 OF 4



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DATE	03-06-06	DRAWN BY	AWA	MATERIAL	DUCTILE IRON
DATE	08-30-13	REVIEWED BY	GCA	PROJECT NO.	12 IN DOME GRATE
DWG NO.	700-110-206	SCALE	1:4	SHEET	1 OF 1

310 WYOMING AVE. BOULDER, CO 80501  
PH: (303) 440-2444  
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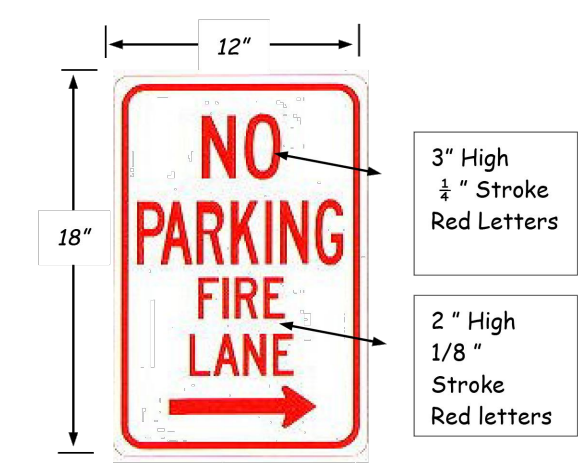
**SOUTH METRO FIRE RESCUE**  
FIRE MARSHAL'S OFFICE



**FIRE LANE SIGN INSTALLATION**

Where the posting of designated "Fire Lane" signage is required, the following information shall serve as the guideline for said installation and shall be in accordance with the International Fire Code and/or the traffic code adopted by the local city or county.

**Fire Lane Sign Dimensions.** NO PARKING - FIRE LANE signs shall be 12 inches wide and 18 inches high.



**Fire Lane Sign Characteristics:** Signs shall be engineer grade reflective sheeting on 0.80 aluminum with red lettering on a white background.

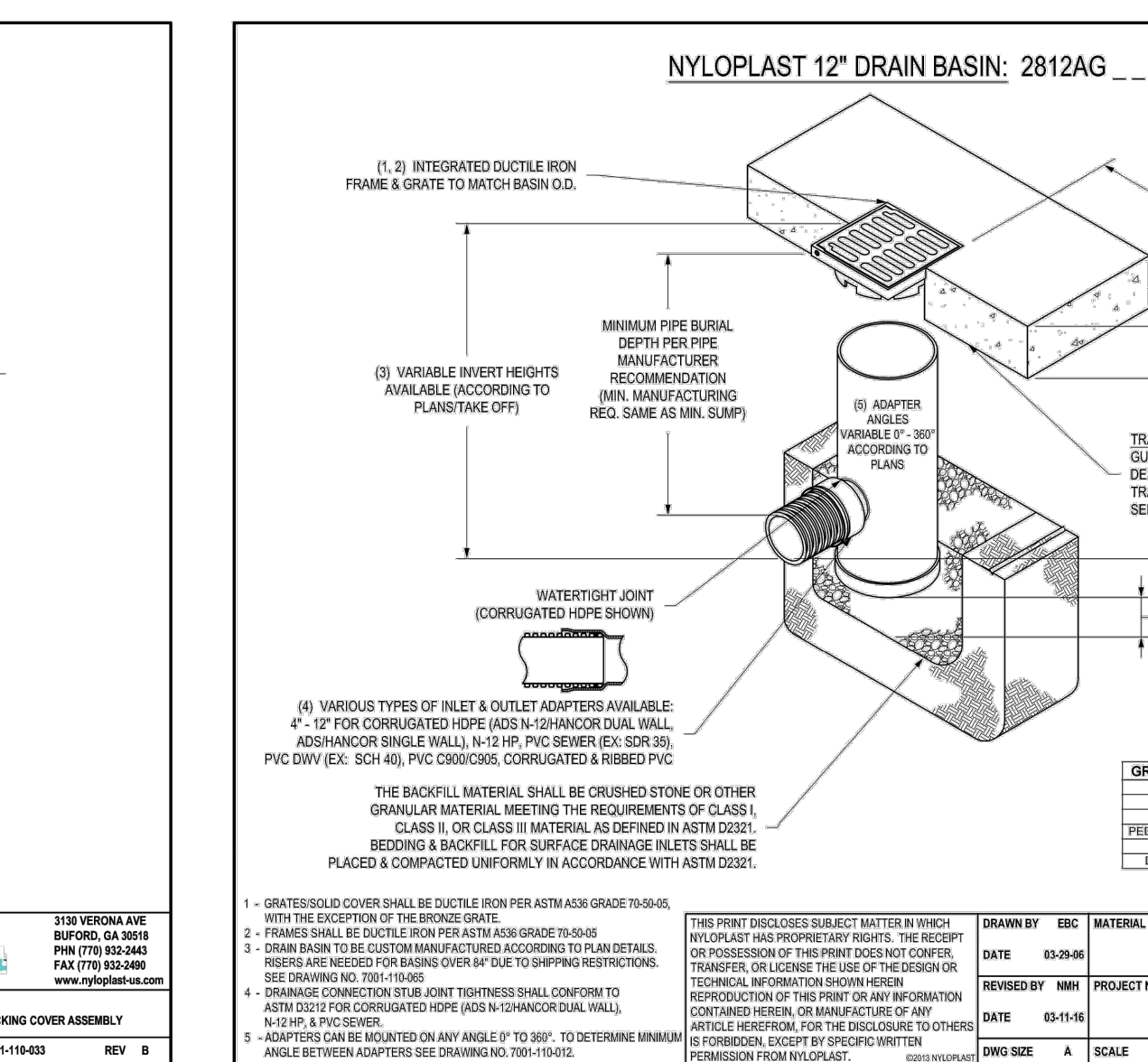
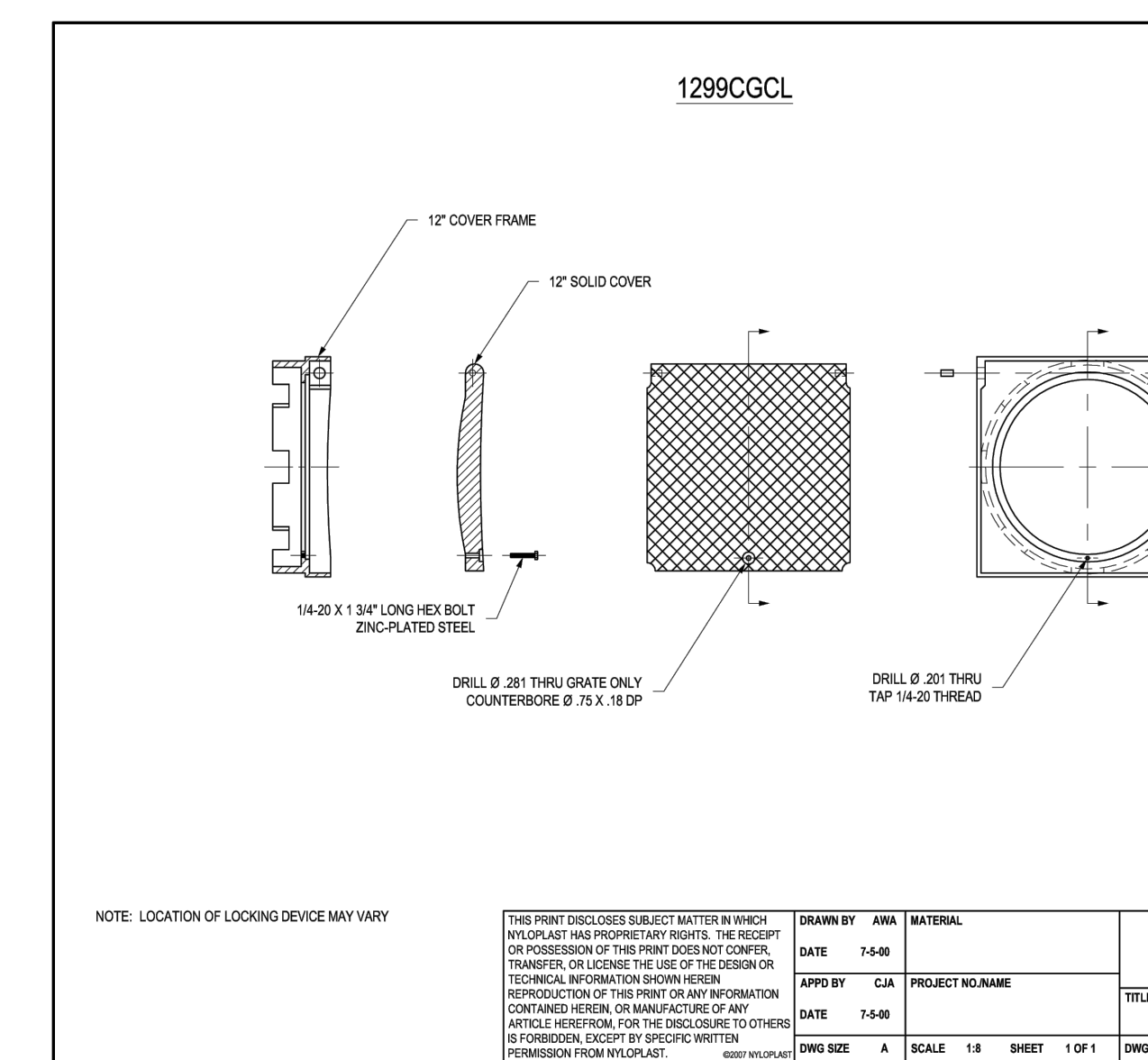
**IFC Section 503.3:** Fire lanes shall be designated on site design documents along with sign locations and details. Fire apparatus access roads shall be signed "NO PARKING FIRE LANE" as follows:

- On both sides of roads less than 26 feet wide;
- On one side of roads at least 26 feet wide and less than 32 feet wide;
- Roads at least 32 feet wide do not need fire lane signs;
- Other locations as determined by the fire department.

**Fire Lane Sign Installation.** NO PARKING - FIRE LANE signs shall be installed as follows:

- Bottom edge of the signs shall be 7 feet above surface (sidewalk) and post shall be placed 24 inches from flow line (curb).
- One sign at the beginning of the restriction (one arrow pointing towards the restriction zone).
- One sign at the end of the restriction (one arrow pointing back into restriction zone).
- One sign at least every 150 ft. within the restricted area (double arrow pointing in each direction, to indicate the continuing restriction).
- Spaced evenly within the restriction (for straight curbs/zones). For example, if the parking restriction zone is 300 ft long, only one double arrow sign is required at a distance of 150 ft. from the beginning of the restriction. If the restriction zone is 400 ft. long, two double arrow signs are required, spaced 100 ft. evenly from each other and from the beginning/ending signs.
- Spaced so that at least one readable sign is visible in front of a parked vehicle from any point along the restriction (for curved curbs/zones and areas that may present visual obstacles.)
- Set at an angle of no fewer than 30 degrees and no more than 45 degrees with the lane of traffic flow visible to approaching traffic.

**Fire Lane Sign Types.** The diagrams below indicate the types of signs that may be used, and the recommended placement.



LOT 1 PARKER POINTE SUBDIVISION FILING 1

PROJECT:  
**NEW O'REILLY AUTO PARTS STORE**  
STROH RD  
PARKER, CO #2

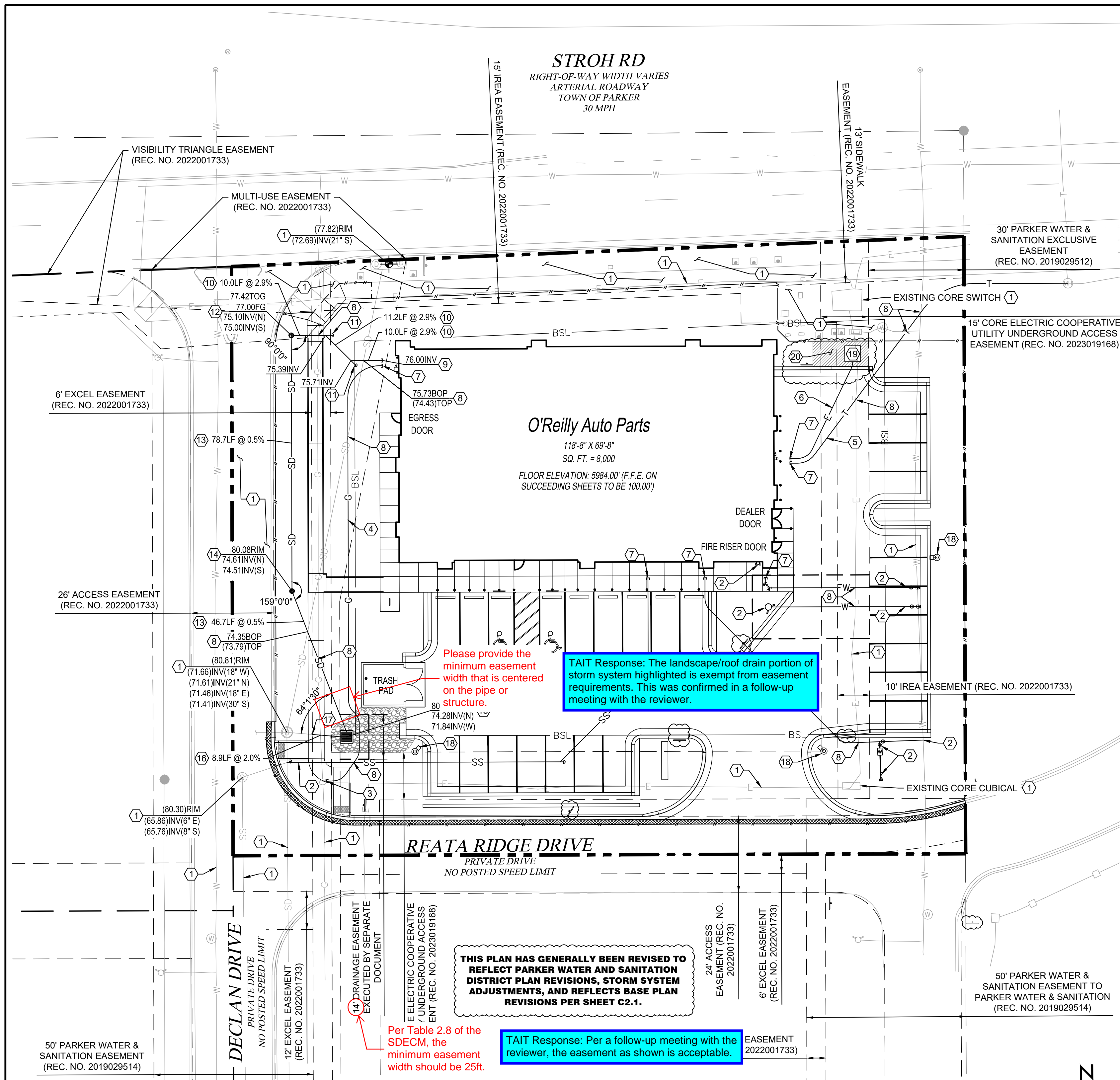
**O'Reilly AUTO PARTS**

CORPORATE OFFICES  
233 SOUTH PATTERSON  
SPRINGFIELD, MISSOURI 65802  
(417) 862-2674 TELEPHONE

COMM # 4884  
DATE: 02/14/2025  
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DATE: 05/02/2025

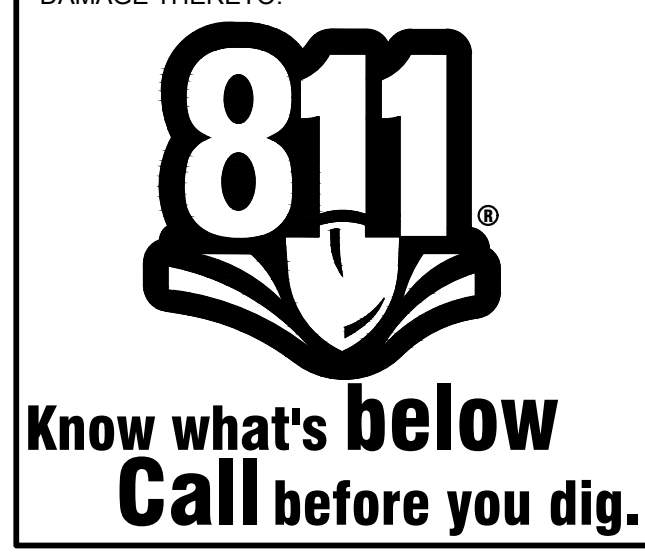
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**1 SITE UTILITY PLAN**  
**C3.1** SCALE: 1" = 20'-0"

**CAUTION:**  
 INFORMATION ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATION AS TO THE TYPE AND LOCATION OF UNDERGROUND AND OTHER UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO.



**BENCHMARK**  
 DOUGLAS COUNTY BENCHMARK 1.060032, EL=5,903.17 FEET (NAVD88) - 3 1/4" INCH ALUMINUM CAP (DOUGLAS COUNTY GIS SECONDARY CONTROL MONUMENT 1.060032) AT THE SOUTHEAST CORNER OF THE PARKING LOT OF STROTH RANCH SOCCER FIELDS, ON THE NORTH SIDE OF STROTH RD, EAST OF THE INTERSECTION OF STROTH ROAD AND N. MOTSENBOCKER ROAD, CITY OF PARKER, COLORADO.

**BASIS OF BEARING**  
 BEARINGS SHOWN HEREON ARE BASED ON THE NORTHERLY LINE OF THE NORTHEAST QUARTER OF SECTION 3, TOWNSHIP 7 SOUTH, RANGE 66 WEST OF THE 6TH P.M., MONUMENTED AS SHOWN ON THIS SURVEY PLAT AS BEARING NORTH 89°33'14" EAST, WITH ALL BEARINGS CONTAINED HEREIN RELATIVE THERETO.

**UNAUTHORIZED CHANGES & USES**  
 THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES OF THESE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THESE PLANS PRIOR TO CONSTRUCTION. CONSTRUCTION CONTRACTOR AGREES THAT IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, CONSTRUCTION CONTRACTOR WILL BE REQUIRED TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THE PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL BE MADE TO APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND CONSTRUCTION CONTRACTOR FURTHER AGREES TO DEFEND, INDEMNIFY AND HOLD DESIGN PROFESSIONAL HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF DESIGN PROFESSIONAL.

THIS PLAN HAS GENERALLY BEEN REVISED TO REFLECT PARKER WATER AND SANITATION DISTRICT PLAN REVISIONS, STORM SYSTEM ADJUSTMENTS, AND REFLECTS BASE PLAN REVISIONS PER SHEET C2.1.

Per Table 2.8 of the SDECM, the minimum easement width should be 25ft.

TAIT Response: Per a follow-up meeting with the reviewer, the easement as shown is acceptable.

TAIT Response: The landscape/roof drain portion of storm system highlighted is exempt from easement requirements. This was confirmed in a follow-up meeting with the reviewer.

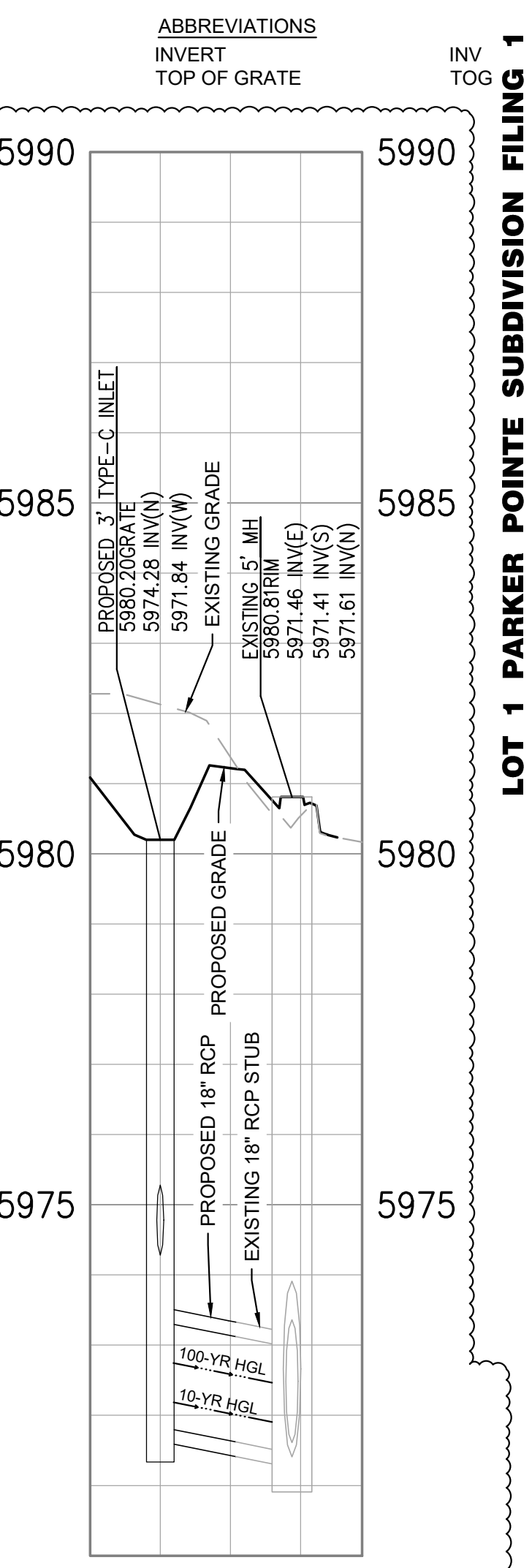
- KEY NOTES**
- PROTECT EXISTING IMPROVEMENTS IN PLACE.
  - REFER TO PARKER WATER AND SANITATION DISTRICT (PWS&D) PLANS FOR WATER AND SANITARY SEWER DETAILS.
  - GAS SERVICE CONNECTION TO EXISTING GAS SERVICE STUB.
  - GAS SERVICE LINE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
  - TELEPHONE LINE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
  - ELECTRIC SERVICE LINE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
  - UTILITY CONNECTION AT BUILDING. REFER TO MEP PLANS FOR CONTINUATION.
  - UTILITY CROSSING. CONTRACTOR TO VERIFY ELEVATIONS AND NOTIFY ENGINEER OF CONFLICT.
  - TRANSITION ROOF DRAIN TO 8" ADS N-12 STORM DRAIN PER DETAIL 12/C2.2.
  - INSTALL 8" ADS N-12 DUAL WALL STORM DRAIN.
  - INSTALL 8" STORM DRAIN CLEANOUT.
  - INSTALL 12" NYLOPLAST DRAIN BASIN WITH DOME GRATE. REFER TO SHEET C2.4 FOR MORE DETAIL.
  - INSTALL 12" ADS N-12 DUAL WALL STORM DRAIN.
  - INSTALL 12" NYLOPLAST DRAIN BASIN WITH SOLID COVER. REFER TO SHEET C2.4 FOR MORE DETAIL.
  - INSTALL 36" CDOT TYPE-C INLET WITH STANDARD GRATE AND 70% MINIMUM OPEN AREA.
  - INSTALL 18" RCP STORM DRAIN.
  - CONNECT PROPOSED 18" RCP STORM DRAIN TO EXISTING 18" RCP STORM DRAIN STUB.
  - LIGHT POLE SHOWN FOR REFERENCE ONLY. REFER TO US1 FOR MORE DETAIL.
  - INSTALL 5' x 5' CONCRETE TRANSFORMER PAD PER CORE ELECTRIC STANDARDS.
  - TRANSFORMER CLEAR AREA. 10' AT FRONT, 1.5' ON SIDES, AND 2.5' AT REAR OF TRANSFORMER.

- LEGEND**
- NOTE: REFER TO SURVEY FOR EXISTING CONDITIONS SYMBOLS LEGEND.
- RIGHT-OF-WAY
  - O'REILLY PROPERTY LINE
  - ADJACENT PROPERTY LINE
  - EXISTING EASEMENT
  - PROPOSED EASEMENT
  - BSL - BUILDING SETBACK LINE
  - ROAD CENTERLINE
  - LIMIT OF DISTURBANCE
  - PROPOSED SAWCUT
  - W - PROPOSED WATER LINE
  - SD - PROPOSED STORM DRAIN LINE
  - SS - PROPOSED SEWER LINE
  - E - PROPOSED ELECTRIC LINE
  - G - PROPOSED GAS LINE
  - T - PROPOSED TELEPHONE LINE
  - PROPOSED SIGN
  - PROPOSED WHEEL STOP
  - PROPOSED BOLLARD
  - PROPOSED FDC
  - PROPOSED FIRE HYDRANT
  - PROPOSED GATE VALVE
  - PROPOSED THRUST BLOCK
  - PROPOSED CLEANOUT
  - PROPOSED LIGHT POLE
  - PROPOSED RIPRAP
  - TRANSFORMER CLEAR AREA

- GENERAL NOTES**
- CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES USING UTILITY CONTACT INFORMATION GIVEN, COORDINATE FOR ALL UTILITY WORK SHOWN. GIVE UTILITY COMPANIES NOTICE OF CONSTRUCTION START DATE AND ESTIMATED COMPLETION DATE.
  - PRIOR TO INSTALLATION, GENERAL CONTRACTOR TO CONFIRM THAT LIGHT POLES, LANDSCAPING AND UTILITIES DO NOT CONFLICT WITH SIGN LOCATION SHOWN. IF ANY POTENTIAL CONFLICT IS DISCOVERED, GENERAL CONTRACTOR TO CONTACT THE O'REILLY PROJECT ADMINISTRATOR BEFORE PROCEEDING
- EXISTING UTILITY NOTES**
- CONTRACTOR TO CONFIRM MINIMUM BURY DEPTH IS MAINTAINED OVER EXISTING UTILITIES.
- EXISTING CONDITIONS NOTE**
- THE CONTRACTOR IS OBLIGATED TO INSPECT FOR EXISTING CONDITIONS/INSTALLATIONS AND AVAILABLE INFORMATION PRIOR TO SUBMITTING A BID. REFER TO SPECIFICATIONS ALSO.
  - EXISTING INSTALLATIONS (SUCH AS WATER MAINS/LINES, GAS MAINS/LINES, SEWER MAINS/LINES, TELEPHONE LINES, POWER LINES, AND UTILITY STRUCTURES IN THE VICINITY OF THE WORK TO BE DONE) ARE INDICATED ON THE DRAWINGS ONLY TO THE EXTENT THAT SUCH INFORMATION HAS BEEN MADE AVAILABLE TO OR DISCOVERED BY THE ENGINEER IN PREPARING THE DRAWINGS. THERE IS NO GUARANTEE AS TO THE ACCURACY OR COMPLETENESS OF SUCH INFORMATION, AND ALL RESPONSIBILITY FOR THE ACCURACY AND COMPLETENESS THEREOF IS EXPRESSLY DISCLAIMED.
  - THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR LOCATING ALL EXISTING INSTALLATIONS.
  - ANY DELAY, ADDITIONAL WORK, FEES OR EXTRA COST TO THE CONTRACTOR CAUSED BY RESULTING FROM DAMAGE TO OR MODIFICATIONS OF EXISTING INSTALLATIONS BY THE CONTRACTOR OR AFFECTED UTILITY COMPANY SHALL NOT CONSTITUTE A CLAIM FOR EXTRA WORK, ADDITIONAL PAYMENT OR DAMAGES.
  - THE CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONSTRUCTION PRIOR TO SUBMITTING HIS BID. NO EXTRAS WILL BE PAID DUE TO UNANTICIPATED EXISTING CONDITIONS / INSTALLATIONS.

**PARKER UTILITY NOTE**

ANY PROPOSED UTILITY CONNECTION WITHIN AN EXISTING ROADWAY WITHIN PUBLIC RIGHT OF WAY WILL REQUIRE A TOWN RIGHT-OF-WAY PERMIT PRIOR TO COMMENCING WORK. THE TOWN REQUIRES CONNECTIONS TO BE BORED TO THE EXTENT POSSIBLE, AND ANY STREET CUT ALLOWED BY THE TOWN WILL BE REQUIRED TO BE PATCHED ACCORDING TO THE TOWN'S CONSTRUCTION STANDARDS AND DETAILS. THE LIMITS OF PATCHING WILL BE DETERMINED BY THE TOWN AT THE TIME OF CONSTRUCTION. THE TOWN OF PARKER DOES NOT ALLOW ROADWAY CLOSURES FOR UTILITY WORK.



**2 18" RCP STORM PROFILE**  
**C3.1** SCALE: 1" = 20'-0" HORIZONTAL 1" = 2'-0" VERTICAL

**ABBREVIATIONS**  
 INV - INVERT  
 TOG - TOP OF GRATE

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**NEW O'REILLY AUTO PARTS STORE**  
 STROTH RD  
 PARKER, CO #2

**LOT 1 PARKER POINTE SUBDIVISION FILING 1**

**SITE UTILITY PLAN**

**O'Reilly AUTO PARTS**

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