



**CONSTRUCTION NOTES:**

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION FOR THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF A CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- A DEVELOPMENT REVIEW ENGINEER SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A DEVELOPMENT REVIEW ENGINEER IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE DEVELOPMENT REVIEW ENGINEER'S ABSENCE. HOWEVER, TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION. FOR INFORMATION CONTACT: DENVER INTER-UTILITY GROUP, 303-534-6700 OR 1-800-922-1987. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER PUBLIC WORKS DIRECTOR, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- ALL PROPOSED STREET CUTS TO EXISTING PAVEMENTS FOR UTILITIES, STORM SEWER OR FOR OTHER PURPOSES ARE LISTED AND REFERENCED BELOW:  
EXAMPLES: WATER TIE-IN SHEET 3 STORM SEWER CONNECTION SHEET 6
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY PERMIT USE APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION."
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR A PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

**ROADWAY NOTES:**

- PAVING SHALL NOT COMMENCE UNTIL A SOILS REPORT AND PAVEMENT DESIGN IS APPROVED BY THE ENGINEERING DIVISION AND SUBGRADE COMPACTION TESTS ARE SUBMITTED TO AND APPROVED BY THE DEVELOPMENT REVIEW ENGINEER.
- STANDARD TOWN OF PARKER HANDICAP RAMPS ARE TO BE CONSTRUCTED AT ALL CURB RETURNS AND AT ALL "T" INTERSECTIONS.
- ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
- ALL ELEVATIONS ARE ON USGS DATUM WITH DATE, RANGE POINT OR MONUMENT SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
- EXCEPT WHERE OTHERWISE PROVIDED FOR IN THESE PLANS AND SPECIFICATIONS, THE COLORADO DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF HIGHWAYS M AND S STANDARDS, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, LATEST EDITION, SHALL APPLY.

**STORM DRAINAGE INFRASTRUCTURE NOTES:**

- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72 (5 CCR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUDED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
- STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
- ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS: • 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC) • 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLY FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
- EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- ALL GROUING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.

**SIGNAGE AND STRIPING NOTES:**

- ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT VERSION OF THE FEDERAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), COLORADO SUPPLEMENTAL MUTCD, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL. FURTHER SPECIFICATIONS AND ILLUSTRATIONS ARE LOCATED IN THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) M AND S STANDARDS.
- A FIELD INSPECTION OF LOCATION AND INSTALLATION OF ALL SIGNS AND MARKINGS SHALL BE PERFORMED BY THE TOWN OF PARKER.
- THE CONTRACTOR INSTALLING SIGNS SHALL BE RESPONSIBLE FOR THE LOCATING AND PROTECTING OF ALL UNDERGROUND UTILITIES.
- TYPE III (LIGHTED) BARRICADES SHALL BE SET AT THE ENDS OF ROADWAYS SEPARATING FINISHED (AND/OR ACCEPTED) AND UNFINISHED CONSTRUCTION AREAS AND SHALL BE MAINTAINED BY THE CONTRACTOR/DEVELOPER. A "ROAD CLOSED AHEAD" AND A "TO BE EXTENDED" WARNING SIGN SHALL BE INSTALLED APPROPRIATELY IN ADVANCE OF THE TYPE III BARRICADES.
- SPECIAL CARE SHALL BE TAKEN IN SIGN LOCATIONS TO ENSURE AN UNOBSTRUCTED VIEW OF EACH SIGN.
- WHERE STOP SIGN CONTROL IS APPROPRIATE, 36 INCH STOP SIGNS SHALL BE USED FOR APPROACHES TO ANY ROADWAY THAT IS CLASSIFIED AS A COLLECTOR OR GREATER.
- A 7 FOOT MINIMUM POST LENGTH SHALL BE MAINTAINED FROM THE BOTTOM OF THE SIGN PANEL TO THE GROUND. THIS REQUIREMENT FOR VERTICAL CLEARANCE IS FOR ALL SIGNS.
- DELINEATION OF ROADWAYS SHALL BE AS SPECIFIED IN THE COLORADO DEPARTMENT OF TRANSPORTATION M AND S STANDARDS.
- RAISED MEDIAN ISLAND NOSES SHALL HAVE R4-7 SIGNS AT EACH END AND A 4"x12" HIGH INTENSITY YELLOW SIGN BLANK LOCATED MIDWAY BETWEEN THE R4-7 SIGN AND FINISHED GRADE ON EACH POST.
- SIGNAGE AND STRIPING HAS BEEN DETERMINED BY INFORMATION AVAILABLE AT THE TIME OF REVIEW. PRIOR TO THE INITIATION OF ANY WARRANTY PERIOD, THE TOWN OF PARKER RESERVES THE RIGHT TO REQUIRE MODIFICATIONS TO EXISTING, OR INSTALLATION OF, ADDITIONAL SIGNAGE AND/OR PAVEMENT MARKING IF IT IS DETERMINED THAT CONDITIONS WARRANT SUCH MODIFICATION ACCORDING TO THE MUTCD OR THE CDOT M AND S STANDARDS. ALL SIGNAGE AND STRIPING SHALL FALL UNDER THE REQUIREMENTS OF THE TWO YEAR WARRANTY PERIOD FOR NEW CONSTRUCTION. ADDITIONALLY, ALL PAVEMENT MARKINGS SHALL NOT LIFT OR PEEL DURING THE FIRST YEAR AFTER INSTALLATION.
- DIAMOND GRADE MATERIAL SHALL BE USED ON ALL STOP SIGNS AND OVERHEAD SIGNS. ALL OTHER ROADSIDE TRAFFIC CONTROL DEVICES SHALL USE HIGH INTENSITY GRADE SHEETING. NO FLUORESCENT YELLOW GREEN SHEETING SHALL BE USED UNLESS SPECIFICALLY APPROVED BY THE TOWN OF PARKER.
- ALL STREET NAME SIGNS SHALL BE HIGH INTENSITY, EXTRUDED, GREEN AND WHITE BLADES. ARTERIAL AND COLLECTOR STREET NAME SIGNS SHALL BE 9 INCH BLADES AND HAVE MIXED CASE LETTERING. LOCAL STREET NAME SIGNS SHALL BE 6 INCHES BLADES WITH MIXED CASE LETTERING.
- ALL LAYOUTS FOR INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- ALL REMOVED SIGNS SHALL BE RETURNED TO THE TOWN OF PARKER.
- CROSSWALKS: SHALL BE CONSTRUCTED USING PREFORMED THERMO-PLASTIC OR AN APPROVED EQUAL. SHALL BE LONGITUDINAL 2 FEET BY 10 FEET (CONTINENTAL) TYPE OR OTHER APPROVED BY THE TOWN OF PARKER. SHALL LINE UP WITH HANDICAP RAMPS. SHALL BE CENTERED WITHIN LANES AND PLACED ON LANE AND CURB LINES SO AS TO AVOID VEHICLE WHEEL PATHS.
- ALL PAVEMENT MARKING MATERIAL (INCLUDING WORDS AND SYMBOLS) SHALL BE AS FOLLOWS: FOR CONCRETE SURFACE: LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC.) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER. WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE METHYL-MYTHACRALATE, PREFORMED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), OR AS SPECIFIED. (SAND OR WATER BLAST CURING COMPOUND PRIOR TO INSTALLATION OF MARKINGS.) FOR ASPHALT SURFACE: LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC.) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER. WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE HOT APPLIED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), METHYL-MYTHACRALATE, OR AS SPECIFIED.
- INSPECTION AND APPROVAL OF STRIPING AND CROSSWALK LAYOUT TO BE DONE BY TOWN OF PARKER DEVELOPMENT REVIEW ENGINEER (CALL 303-840-9546) PRIOR TO APPLICATION OF FINAL STRIPING.

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CALL 811  
TWO WORKING DAYS  
BEFORE YOU DIG  
UNCC 1-800-922-1987  
UTILITY NOTIFICATION  
CENTER OF COLORADO

**BENCHMARK**  
DOUGLAS COUNTY CONTROL POINT KNOWN AS 1.069032, BEING A 3-1/4" ALUMINUM CAP, BEING LOCATED IN THE SOUTHWEST QUARTER OF SECTION 33, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF THE SIXTH PRINCIPAL MERIDIAN, HAVING A PUBLISHED ELEVATION OF 1799.2870 METERS (5903.13 FEET) NAVD '88 DATUM.

**BASIS OF BEARINGS:**  
THE EAST LINE OF THE NORTHEAST QUARTER OF SAID SECTION 9 BEING MONUMENTED AT THE NORTHEAST CORNER OF SAID SECTION 9 BY A 3-1/4" ALUMINUM CAP STAMPED LS 23053 AND AT THE EAST QUARTER CORNER OF SAID SECTION 9 BY A 2-1/2" ALUMINUM CAP STAMPED LS 6935 BEING CONSIDERED TO BEAR SOUTH 00°15'06" EAST, 2648.70 FEET.

PREPARED UNDER THE SUPERVISION OF  
  
BRIAN P. WILSON  
COLORADO P.E. 0050067

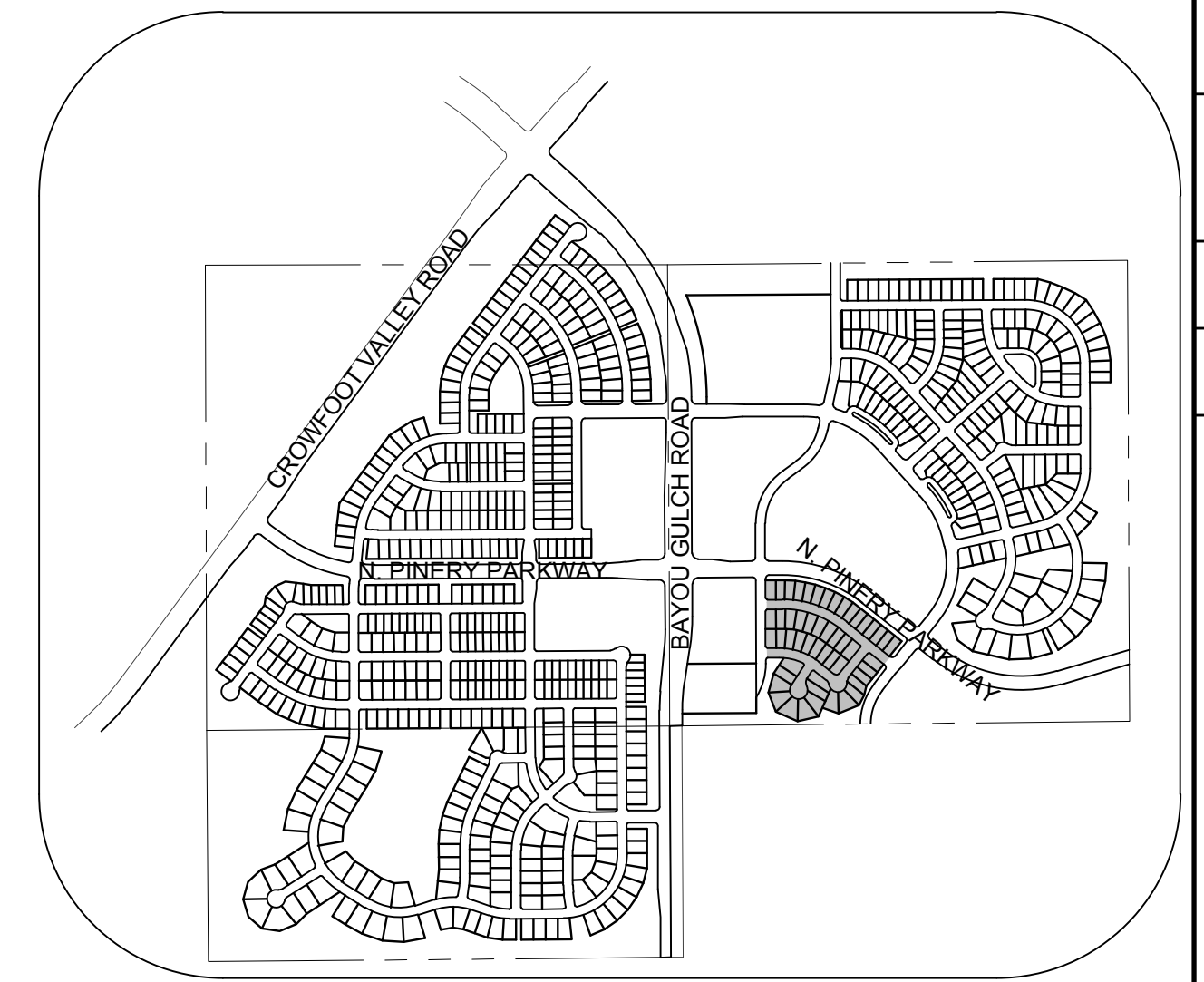
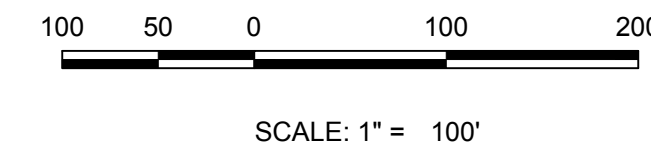
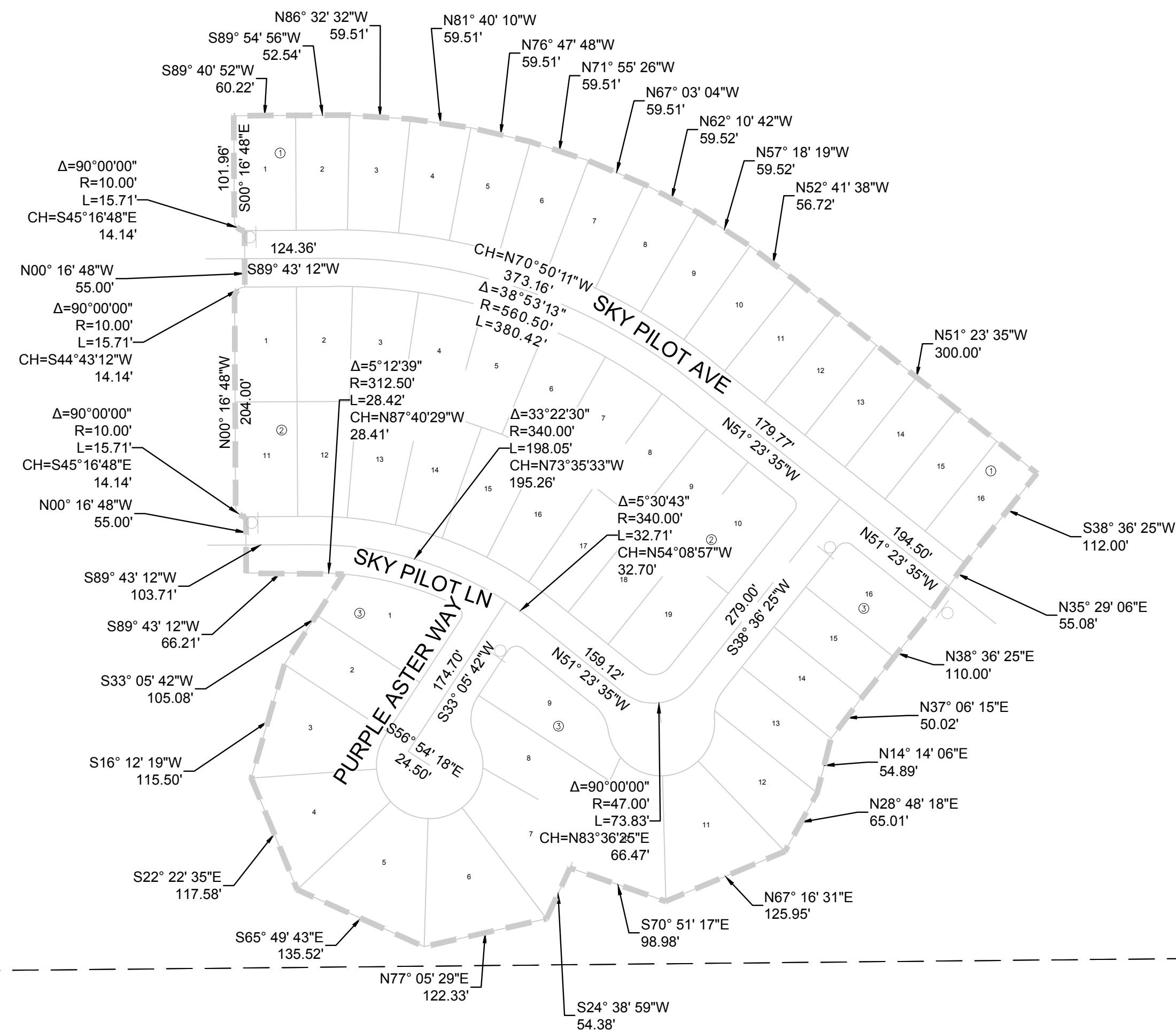
THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGN, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

SHEET NUMBER	2	SCALE:	AS SHOWN	FILE NO.:	8130283701
		DRAWN BY:	JF	CHECKED BY:	JU
TRAILS AT CROWFOOT FILING 13 CONSTRUCTION DRAWINGS TOWN OF PARKER NOTES		HR 935 LLC 7353 South Alton Way CENTENNIAL, CO 80112		10338 E. Dry Creek Rd. Suite 410 Englewood, CO 80110 Tel: (720) 482-9526 Fax: (720) 482-9546	
CVL CONSULTANTS		Revisions		Date	
		No.		Date	
		Appr.		Date	

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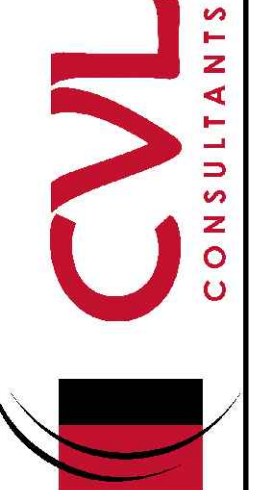
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**KEYMAP**  
N.T.S.

No.	Revisions	Date	Init.	Appr.	Date

10333 E. Dry Creek Rd.  
Suite 410  
Englewood, CO 80150  
Tel: (720) 482-9526  
Fax: (720) 482-9546



**HR 935 LLC**  
7353 South Alton Way  
CENTENNIAL, CO 80112

**TRAILS AT CROWFOOT**  
**FILING 13 CONSTRUCTION DRAWINGS**  
HORIZONTAL CONTROL PLAN

SCALE: AS SHOWN  
FILE NO: 8130283701  
DRAWN BY: TP  
CHECKED BY: JU  
DATE: SEPTEMBER 2017

SHEET NUMBER **3**

**BENCHMARK**  
DOUGLAS COUNTY CONTROL POINT KNOWN AS 1.069032, BEING A 3-1/4" ALUMINUM CAP, BEING LOCATED IN THE SOUTHWEST QUARTER OF SECTION 33, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF THE SIXTH PRINCIPAL MERIDIAN, HAVING A PUBLISHED ELEVATION OF 1799.2870 METERS (5903.13 FEET) NAVD '88 DATUM.

**BASIS OF BEARINGS:**  
THE EAST LINE OF THE NORTHEAST QUARTER OF SAID SECTION 9 BEING MONUMENTED AT THE NORTHEAST CORNER OF SAID SECTION 9 BY A 3-1/4" ALUMINUM CAP STAMPED LS 23053 AND AT THE EAST QUARTER CORNER OF SAID SECTION 9 BY A 2-1/2" ALUMINUM CAP STAMPED LS 6935 BEING CONSIDERED TO BEAR SOUTH 00°15'06" EAST, 2648.70 FEET.

PREPARED UNDER THE SUPERVISION OF

BRIAN P. WILSON  
COLORADO P.E. 0050067

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

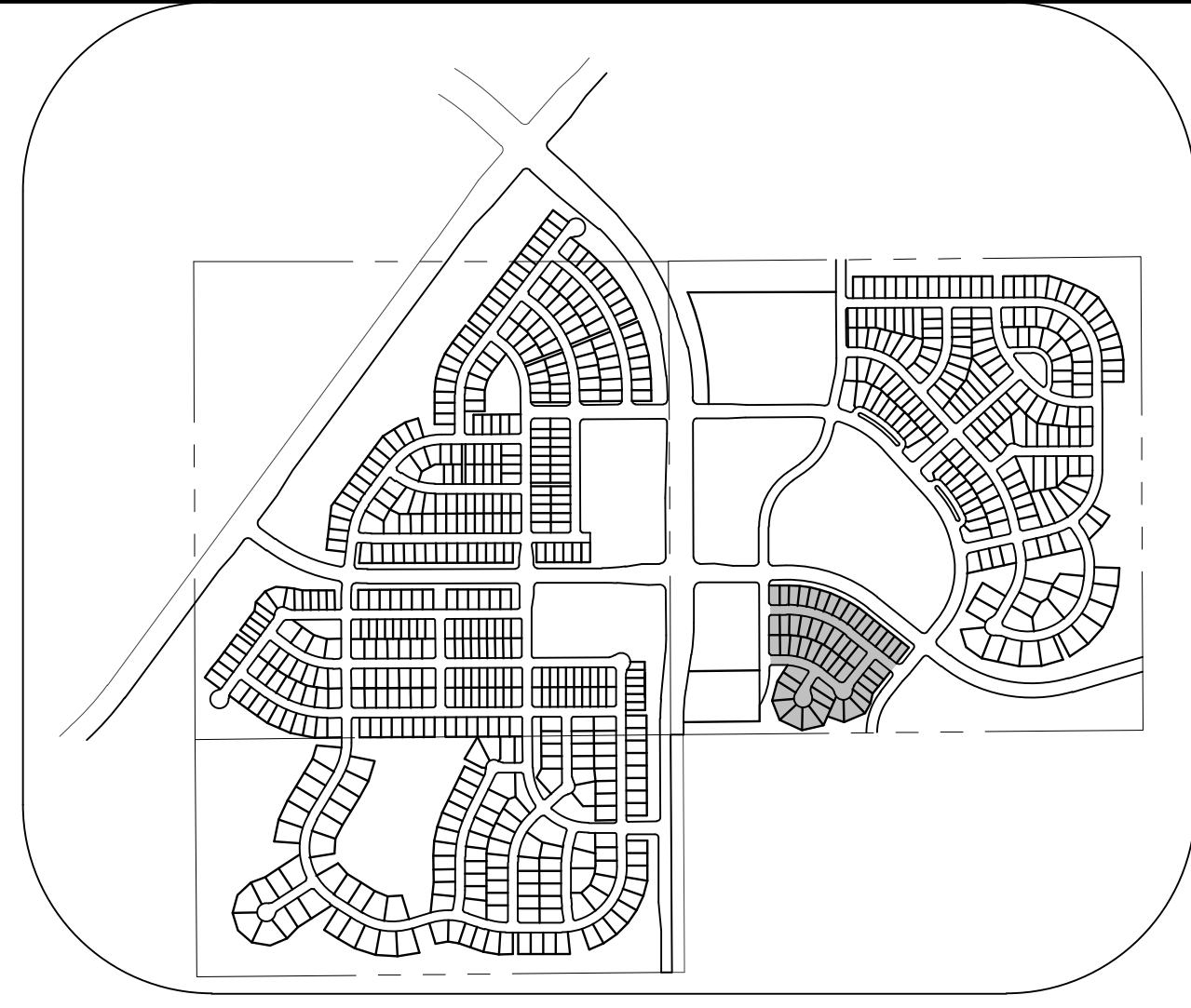
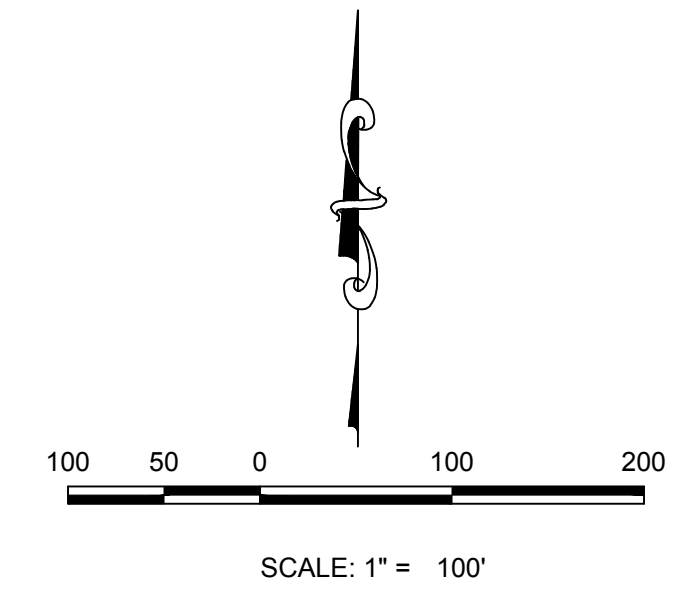
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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_



PROJECT SHEET RANCH/CAD/ENGINEERING SHEET SETS/CD/SIF/DTOWN OF PARKER/F13 BMP PLANS DWG. ANDREW.P. 1/4/2018 7:53 PM

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KEYMAP  
N.T.S.

LEGEND

- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- NO WORK AREA
- EXISTING SURFACE FLOW DIRECTION
- PROPOSED SURFACE FLOW DIRECTION

<p><b>CBMP</b> CONSTRUCTION BEST MANAGEMENT PRACTICES LEGEND 1.0.1</p>	<p><b>CBMP</b> CONSTRUCTION BEST MANAGEMENT PRACTICES LEGEND 1.0.2</p>	<p><b>CBMP</b> CONSTRUCTION BEST MANAGEMENT PRACTICES LEGEND 1.0.3</p>
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- NOTE:
- ALL STREET CUTS SHALL BE CONTROLLED PER THE ROUGH CUT STREET CONTROL DETAIL. SEE BMP DETAILS.
  - TO REDUCE THE POTENTIAL FOR CLOGGING OF DEBRIS GATES, NO STRAW MULCH SHALL BE USED WITHIN THE EXCESS URBAN RUNOFF VOLUME OF A DETENTION BASIN. INSTEAD, EROSION CONTROL BLANKET SHALL BE INSTALLED FOR A WIDTH OF AT LEAST 6 FEET ON BOTH SIDES OF THE CONCRETE LOW FLOW CHANNELS OR UP TO A DEPTH OF 1 FOOT IN SOIL RIPRAP OR BENCHED LOW FLOW CHANNELS. ADDITIONALLY, HYDRAULICALLY APPLIED WOOD FIBER MULCH, STABILIZED FIBER MATRIX, BONDED FIBER MATRIX, OR FLEXIBLE GROWTH MEDIUM SHALL BE USED IN ALL OTHER AREAS WITHIN THE EXCESS URBAN RUNOFF VOLUME OF THE DETENTION BASIN. THE BLANKET AND HYDRAULICALLY APPLIED MULCH SHALL COMPLY WITH THE MATERIALS AND INSTALLATION REQUIREMENTS FOR EROSION CONTROL BLANKETS AS REQUIRED BY THE TOWN'S CBMP PROGRAM, AND WHERE APPLICABLE, THE PRODUCT MANUFACTURER.
  - IT MAY BE NECESSARY TO PROVIDE BACK OF WALK PROTECTION AND BACK OF CURB PROTECTION ONCE HOME BUILDING HAS STARTED.
  - INSTALL ECB IN TREE LAWN AFTER INSTALLATION OF CURB AND GUTTER AND SIDEWALK. TYPICAL THROUGHOUT SUBDIVISION.

**BENCHMARK**  
DOUGLAS COUNTY CONTROL POINT KNOWN AS 1.060032, BEING A 3-1/4" ALUMINUM CAP, BEING MONUMENTED AT THE SOUTHWEST QUARTER OF SECTION 33, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF THE SIXTH PRINCIPAL MERIDIAN, HAVING A PUBLISHED ELEVATION OF 1799.2870 METERS (5903.13 FEET) NAVD '88 DATUM.

**BASIS OF BEARINGS:**  
THE EAST LINE OF THE NORTHEAST QUARTER OF SAID SECTION 9 BEING MONUMENTED AT THE NORTHEAST CORNER OF SAID SECTION 9 BY A 3-1/4" ALUMINUM CAP STAMPED LS 23053 AND AT THE EAST QUARTER CORNER OF SAID SECTION 9 BY A 2-1/2" ALUMINUM CAP STAMPED LS 8935 BEING CONSIDERED TO BEAR SOUTH 00°15'06" EAST, 2648.70 FEET.

PREPARED UNDER THE SUPERVISION OF  
  
BRIAN P. WILSON  
COLORADO P.E. 0050067

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGN, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

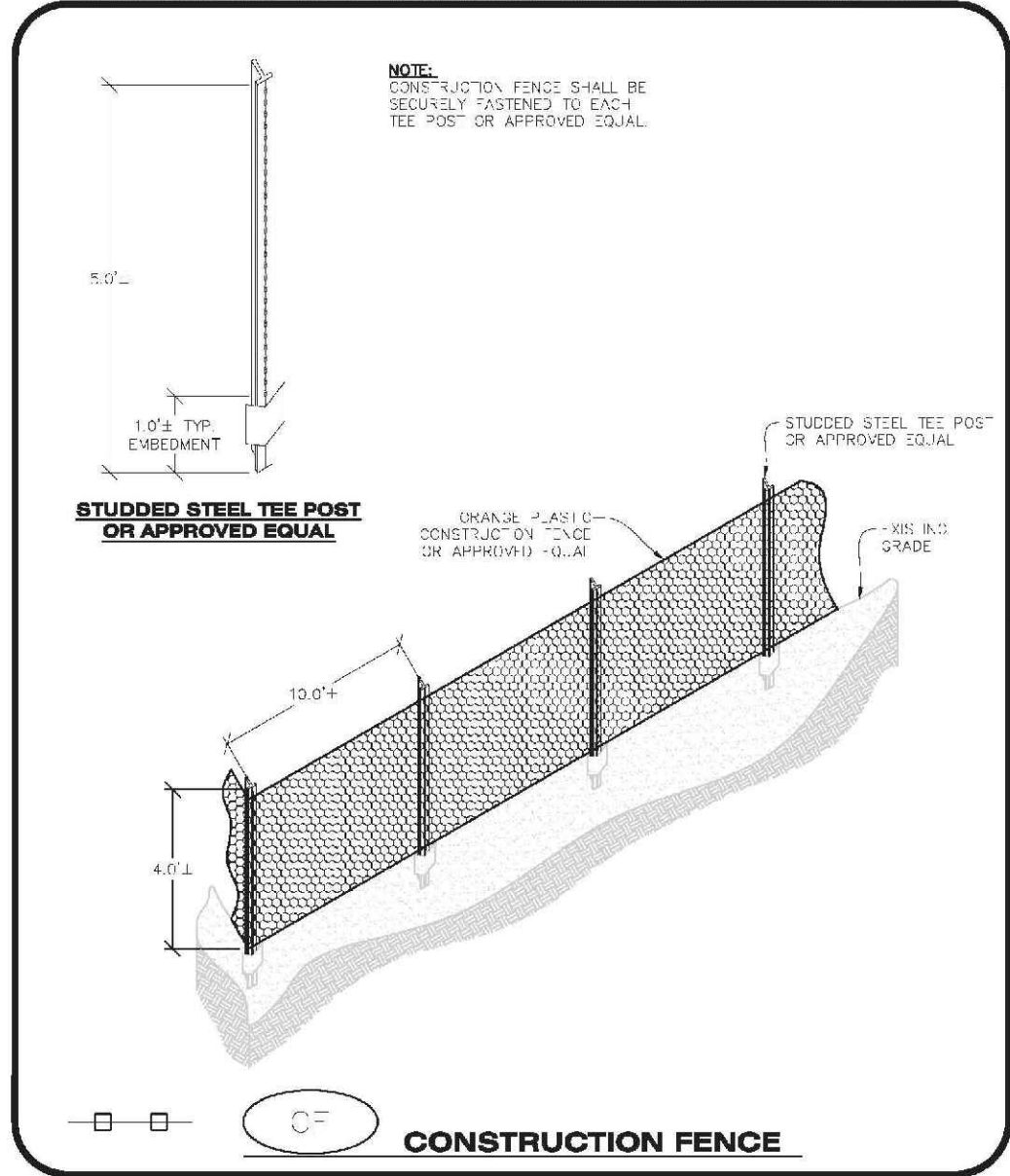
10333 E. Dry Creek Rd. Suite 240 Englewood, CO 80112 Tel: (720) 482-9526 Fax: (720) 482-9546	No.	Revisions	Date	Appr.	Date
HR 935 LLC 7363 South Allon Way CENTENNIAL, CO 80112	<b>TRAILS AT CROWFOOT</b> <b>FILING 13 CONSTRUCTION DRAWINGS</b> INTERIM & FINAL CBMP PLAN				
SCALE: ASP	AS SHOWN	FILE NO: 8130283701			
DRAWN BY: ASP	CHECKED BY: JU	DATE: SEPTEMBER 2017			
SHEET NUMBER	<b>5</b>				

**BEST MANAGEMENT DETAILS**

- GRADING, DRILLING, CLEARING, EXCAVATING, BACK-FILLING, SOIL STRIPPING, SOIL IMPORTING EXPORTING OR ANY OTHER FORM OF SOIL DISTURBANCE SHALL NOT COMMENCE UNTIL A TOWN OF PARKER GRADING/EXCAVATION PERMIT HAS BEEN ISSUED.
- THE TOWN OF PARKER GRADING/EXCAVATION PERMIT IS VALID FOR A PERIOD OF 2 YEARS FROM THE DATE OF ISSUANCE. ANY LAPSE IN PERMIT COVERAGE MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER AND/OR FINES.
- ALL EROSION AND SEDIMENT CONTROL BMPs SHALL BE INSTALLED ACCORDING TO THE CBMP PLAN INSTALLATION NOTES AND DETAILS AND/OR PERMITTED VARIANCES. ALL MATERIALS, WORKMANSHIP AND INSTALLATION METHODS ARE SUBJECT TO INSPECTION BY THE TOWN'S INSPECTOR. THE TOWN OF PARKER RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS, WORKMANSHIP AND/OR INSTALLATION METHODS THAT DO NOT CONFORM TO THE CBMP PLAN AND/OR PERMITTED VARIANCES.
- THE EROSION CONTROL SUPERVISOR SHALL REVISE OR MODIFY THE EROSION AND SEDIMENT CONTROL MEASURES IF IT BECOMES APPARENT THAT THE ORIGINAL PLAN IS INADEQUATE, OR AS A RESULT OF DEFICIENCIES IDENTIFIED DURING INSPECTIONS PERFORMED BY THE TOWN'S INSPECTOR.
- THE INSTALLATION OF ADDITIONAL EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES MAY BE REQUIRED OF THE EROSION CONTROL SUPERVISOR, PROPERTY OWNER, SITE DEVELOPER, CONTRACTOR AND/OR THEIR AUTHORIZED AGENTS AT ANY TIME THROUGHOUT THE DURATION OF THE PROJECT CONSTRUCTION AND/OR SUBSEQUENT REEVALUATION PERIOD.
- THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE SITE REMAINS IN COMPLIANCE WITH THE NOTICE OF NUISANCE REPORTS (A.K.A. CONSTRUCTION SITE RUNOFF CONTROL INSPECTION REPORTS), APPROVED CBMP PLAN(S) AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- THE EROSION CONTROL SUPERVISOR SHALL BE READILY AVAILABLE TO DISCUSS AND CORRECT ANY PROBLEMS THAT MAY ARISE RELATING TO GRADING, EROSION AND SEDIMENT CONTROL.
- IF IT IS ANTICIPATED THAT ALL OR A PORTION OF THE PROJECT WILL OCCUR DURING SEASONS SUSCEPTIBLE TO SNOWFALL, THE USE OF CERTAIN EROSION AND SEDIMENT CONTROL BMPs ADJACENT TO PUBLIC ROADWAYS MAY NEED TO BE CONSIDERED DUE TO THE NATURE OF SNOW REMOVAL OPERATIONS. AN APPROVED ALTERNATIVE MAY BE NECESSARY TO MINIMIZE DAMAGES FROM THESE OPERATIONS. THE TOWN OF PARKER ASSUMES NO RESPONSIBILITY FOR DAMAGE TO ANY BMPs AS A RESULT OF SNOW PLOWING AND SNOW REMOVAL.
- AREAS OF LAND DISTURBANCE EQUAL TO 40 ACRES OR GREATER SHALL NOT BE EXPOSED FOR MORE THAN 30 CONSECUTIVE DAYS WITHOUT TEMPORARY OR PERMANENT STABILIZATION.
- AUTHORIZED EXEMPTIONS MAY BE ALLOWED TO THE 40-ACRE LIMIT FOR REMOVAL AND STORAGE OF CUT MATERIAL WHERE (A) GEOTECHNICAL LIMITATIONS RESTRICT THE USE OF TEMPORARY OR PERMANENT STABILIZATION OF THE STORED MATERIAL (E.G., SWELLING SOILS, ROCK) AND (B) WHEN THE OWNER CAN DEMONSTRATE THAT THE 40-ACRE LIMIT IS PHYSICALLY AND/OR FINANCIALLY IMPRACTICABLE. FOR SUCH EXEMPTIONS, A PHASING AND EARTHWORK QUANTITIES PLAN SHALL BE SUBMITTED TO THE PUBLIC WORKS DEPARTMENT AND APPROVED PRIOR TO THE COMMENCEMENT OF LAND DISTURBANCE ACTIVITIES. SUBMITTAL REQUIREMENTS INCLUDE:
  - PHASING PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
  - EARTHWORK QUANTITY PLAN SHOWING CUT AND FILL VOLUMES AND LOCATIONS FOR EACH PHASE AND PROJECT TOTALS.
  - BMP PLAN SHOWING SPECIFIC EROSION AND SEDIMENT CONTROLS FOR EACH PHASE.
- ANY EROSION AND SEDIMENT CONTROL BMPs THAT ARE DAMAGED OR IN NEED OF MAINTENANCE OR REPLACEMENT SHALL BE CORRECTED AS SOON AS POSSIBLE. IMMEDIATELY IN MOST CASES.
- ALL DEFICIENCIES LISTED ON THE NOTICE OF NUISANCE FORM SHALL BE COMPLETED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES. ALL REQUIRED ACTIONS MUST BE IN THE CORRECTED FORM DURING THE FOLLOW-UP INSPECTION.
- THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TROUBLE CHANNELS, AND/OR OTHER IMPERVIOUS SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEAN BEFORE THE END OF THE WORK DAY.
- BULK STORAGE OF LIQUIDS, PETROLEUM PRODUCTS AND OTHER CHEMICALS SHALL HAVE ADEQUATE PROTECTION SO AS TO CONTAIN ALL SPILLS AND PREVENT ANY SPILLED MATERIAL FROM ENTERING STATE WATERS.
- ALL TRASH RECEPTACLES ON SITE SHALL BE FREE OF HOLES, CRACKS, GAPS, AND/OR OTHER PERMEABLE AREAS THAT MAY ALLOW FOR THE DISCHARGE OF POLLUTANTS.
- ALL TRASH RECEPTACLES ON SITE SHALL BE EMPTIED AT A FREQUENCY AS TO ENSURE THAT THE TRASH REMAINS CONFINED TO THE RECEPTACLE.
- ALL LOOSE TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISCARDED ON A DAILY BASIS.
- ALL PORTABLE TOILETS SHALL BE STAKED DOWN AT ALL TIMES USING U-SHAPED REBAR STAKES. THE PORTABLE TOILETS SHALL ALSO BE PLACED A MINIMUM DISTANCE OF 10 FEET FROM ALL IMPERVIOUS SURFACES, INCLUDING, BUT NOT LIMITED TO STREETS, CURBS, GUTTERS, SIDEWALKS AND PARKING LOTS.
- THE EROSION CONTROL SUPERVISOR SHALL MAINTAIN STRICT ADHERENCE TO THE LIMITS OF CONSTRUCTION AND PROPERTY LIMITS FOR ALL MATERIALS, VEHICLES AND EQUIPMENT. FAILURE TO ABIDE BY THIS REQUIREMENT MAY RESULT IN THE ISSUANCE OF A STOP WORK ORDER.
- ALL CONSTRUCTION TRAFFIC MUST ENTER AND EXIT THE SITE THROUGH THE APPROVED ACCESS POINT(S). A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT ALL APPROVED ACCESS POINTS TO THE SITE. EXCEPTIONS MAY BE CONSIDERED FOR CONSTRUCTION ACTIVITY OCCURRING IMMEDIATELY ADJACENT TO PAVED AREAS AND WHERE ALTERNATIVE BMPs ARE IMPLEMENTED. SUCH ACTIVITY MAY INCLUDE, BUT NOT BE LIMITED TO: RESIDENTIAL CONSTRUCTION, UTILITY CONSTRUCTION, ETC.
- NO PERMANENT SLOPES GREATER THAN 3:1 ARE ALLOWED.
- ALL PERMANENT SLOPES STEEPER THAN 4:1 (HORIZONTAL TO VERTICAL) SHALL REQUIRE EROSION CONTROL BLANKETS(T). TEMPORARY SLOPES IN TEMPORARY SEDIMENT BASINS THAT ARE STEEPER THAN 4:1 MAY REQUIRE EROSION CONTROL BLANKETS(T).
- THE EROSION CONTROL SUPERVISOR SHALL BE RESPONSIBLE FOR CORRECTING ANY ADVERSE IMPACTS THAT OCCUR TO NEIGHBORING PROPERTIES. THE EROSION CONTROL SUPERVISOR MUST OBTAIN PERMISSION FROM LAND OWNERS PRIOR TO ENTERING SUCH PROPERTY.
- A WATER SOURCE SHALL BE AVAILABLE ON-SITE DURING CONSTRUCTION ACTIVITIES, AND UTILIZED TO MINIMIZE FUGITIVE DUST. ALTERNATIVE BMPs MAY BE REQUIRED IF INITIAL ATTEMPTS TO SUPPRESS DUST ARE UNSUCCESSFUL.
- ALL CHEMICAL OR HAZARDOUS MATERIAL SPILLS, INCLUDING CONCRETE WASHOUT WATER, WHICH MAY ENTER WATERS OF THE STATE OF COLORADO, WHICH INCLUDES BUT ARE NOT LIMITED TO, SURFACE WATER, GROUND WATER, DIRT GULLIES OR STORM SEWERS LEADING TO SURFACE WATER, SHALL BE IMMEDIATELY REPORTED TO THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT PER 25-B-601, AND THE TOWN OF PARKER. RELEASES OF PETROLEUM PRODUCTS AND CERTAIN HAZARDOUS SUBSTANCES LISTED UNDER THE FEDERAL CLEAN WATER ACT (40 CFR PART 117) MUST BE REPORTED TO THE NATIONAL RESPONSE CENTER AND THE CORNER THAT POSES AN IMMEDIATE SAFETY HAZARD SHALL BE REPORTED TO 911.
- THE CLEANING OF CONCRETE TRUCKS AND EQUIPMENT IS RESTRICTED TO THE APPROVED CONCRETE WASHOUT LOCATION ON THE JOB SITE. CONCRETE WASH WATER SHALL NOT BE DISCHARGED TO STATE WATERS OR STORM SEWER SYSTEMS.
- VEHICLE AND EQUIPMENT DECONTAMINATION IS PROHIBITED ON THE JOB SITE.
- ALL DEWATERING ON SITE SHALL BE COORDINATED WITH THE TOWN'S INSPECTOR. A STATE PERMIT IT MAY BE REQUIRED FOR DEWATERING. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR OBTAINING AND ADHERING TO ALL APPLICABLE PERMITS.
- HYDRAULIC SEEDING AND/OR HYDRAULIC MULCHING ARE ONLY ALLOWED IN AREAS UNDER TEMPORARY OR PERMANENT IRRIGATION OR FOR THE PURPOSE OF TEMPORARY SOIL STABILIZATION.
- APPLICABLE CONSTRUCTION BMPs SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL ALL LANDSCAPING HAS BEEN INSTALLED AND THE DESIRABLE VEGETATION HAS REACHED A POINT IN WHICH EROSION AND SEDIMENTATION IS NO LONGER A CONCERN AS DETERMINED BY THE TOWN'S INSPECTOR.

**CONSTRUCTION FENCE**

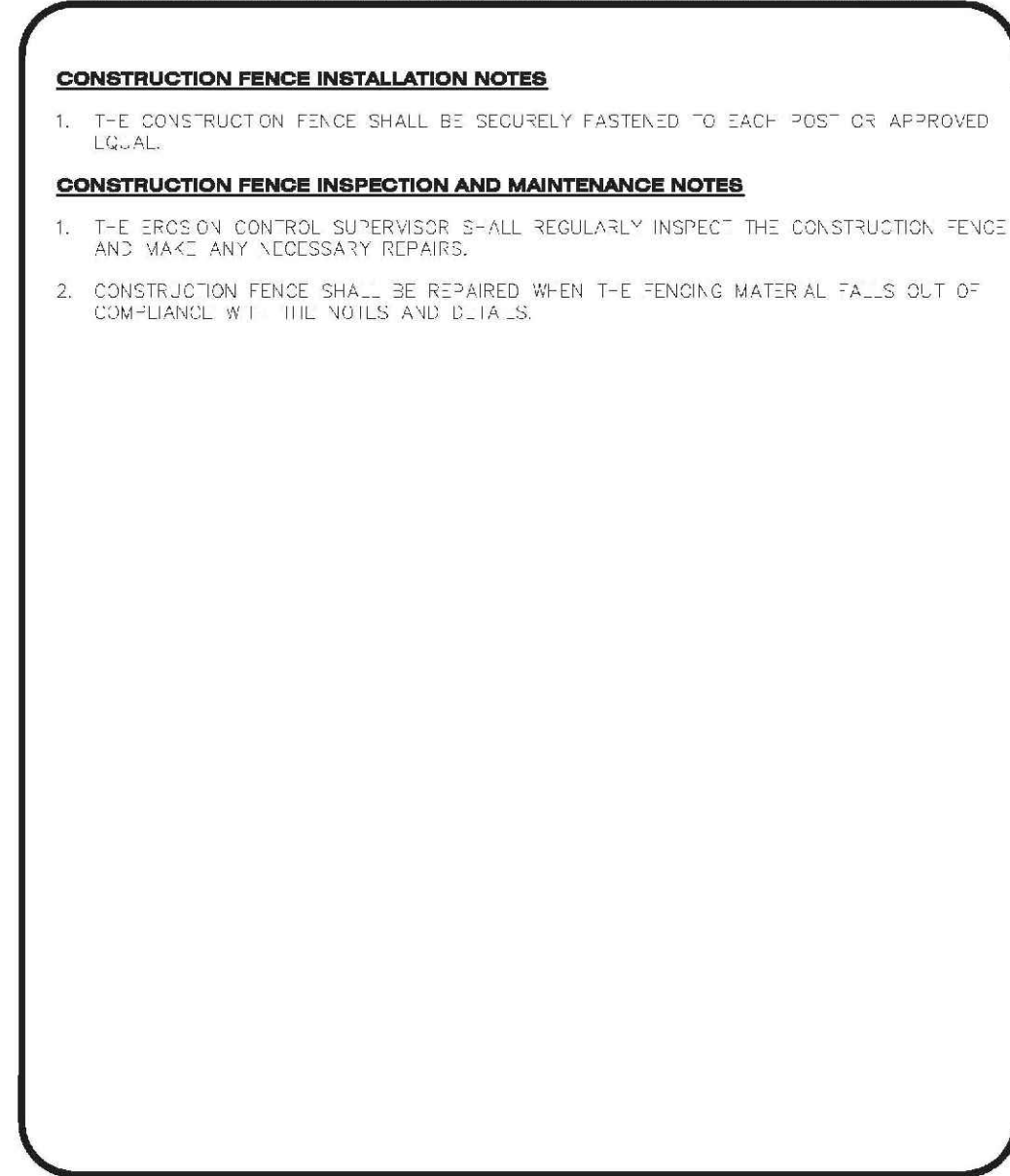
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**CBMP**

CONSTRUCTION BEST MANAGEMENT PRACTICES

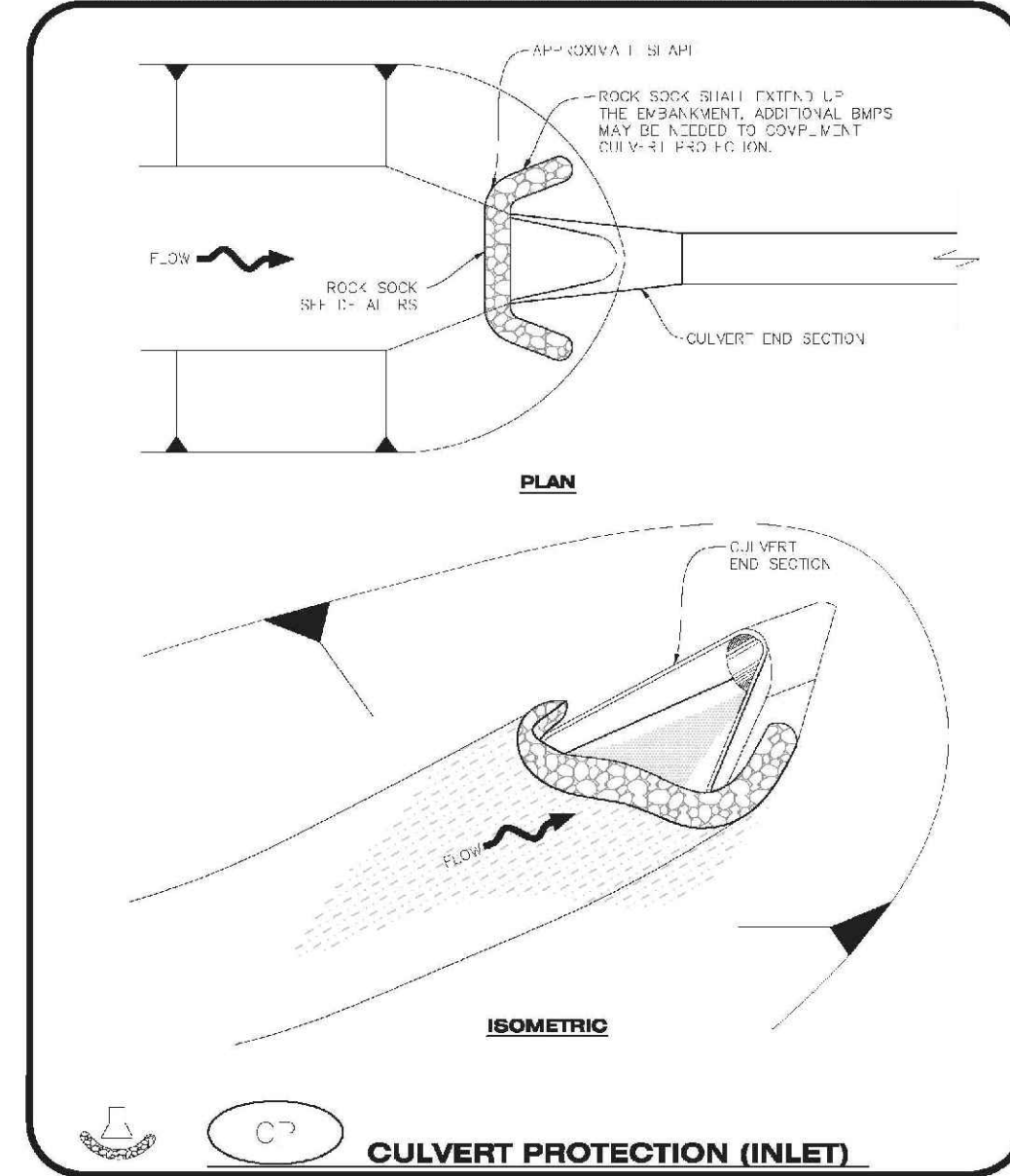
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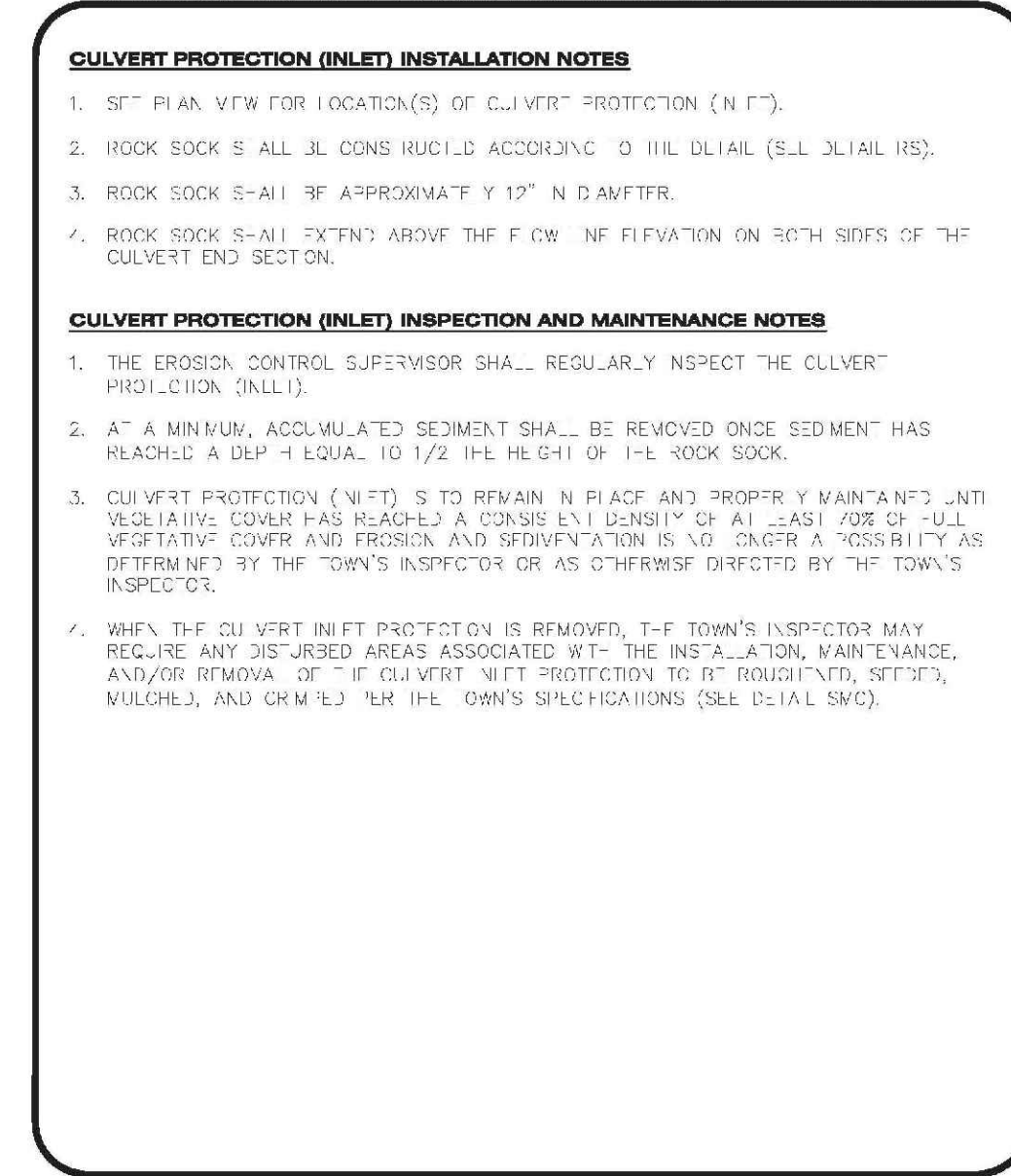
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CONSTRUCTION BEST MANAGEMENT PRACTICES

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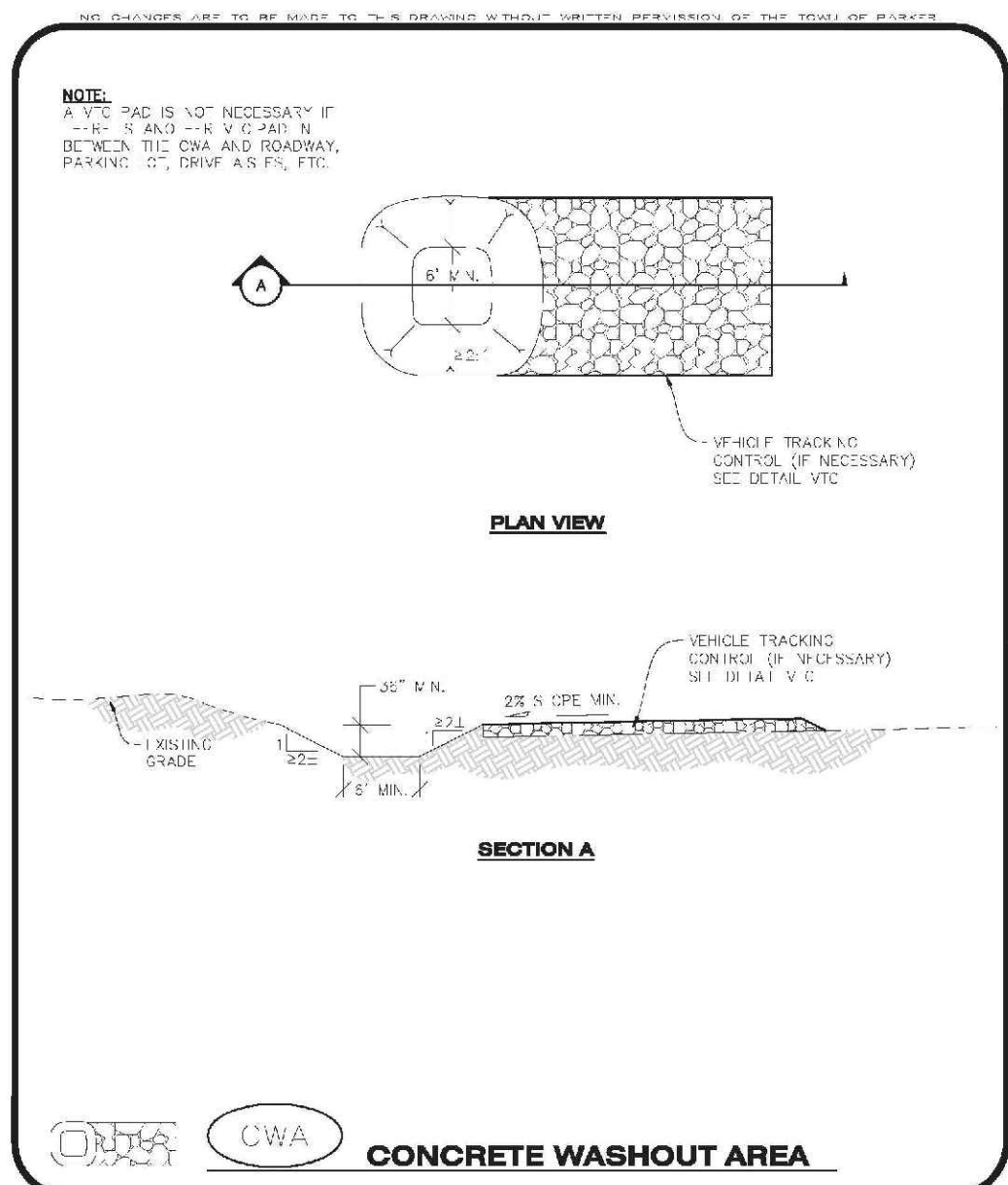
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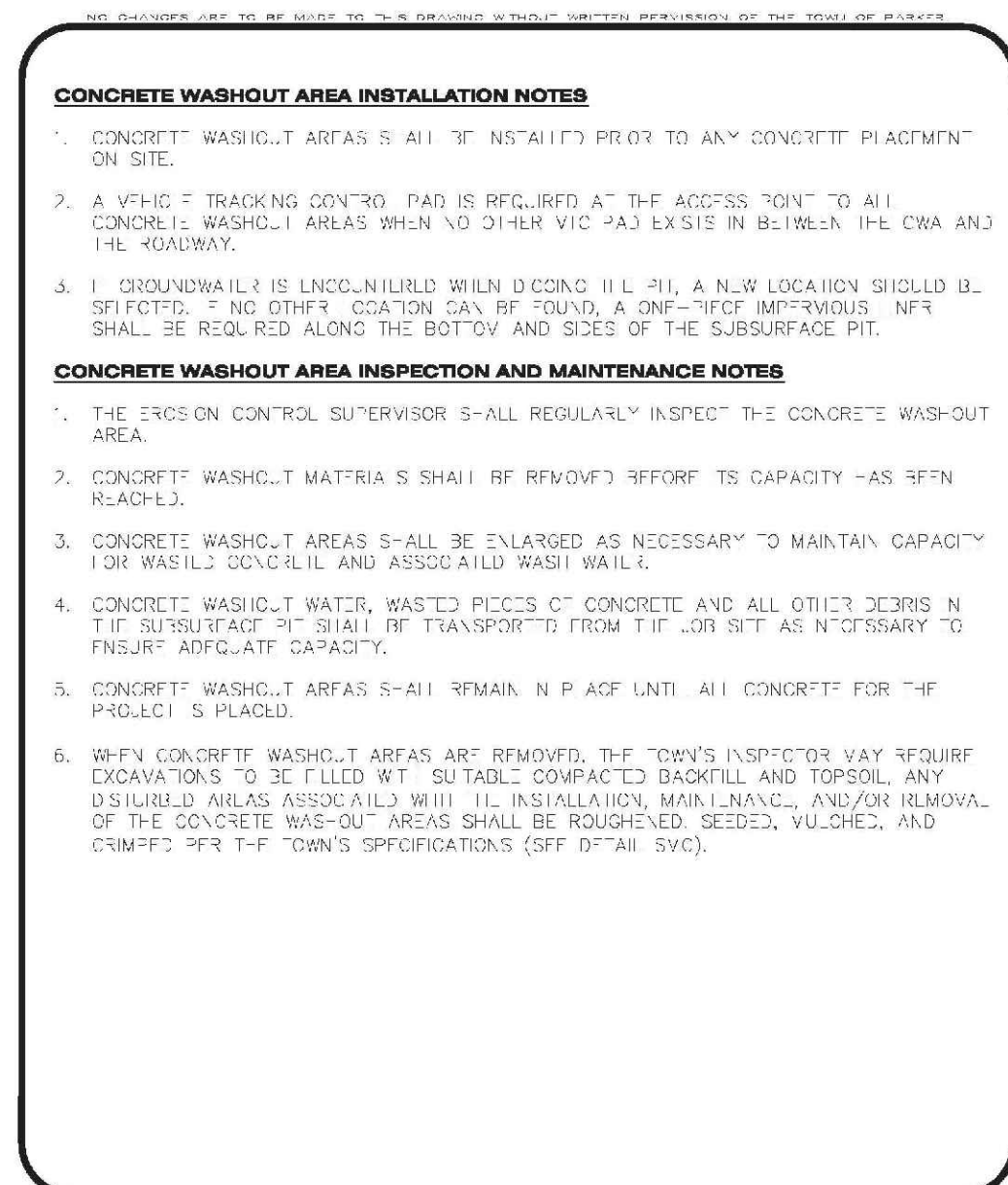
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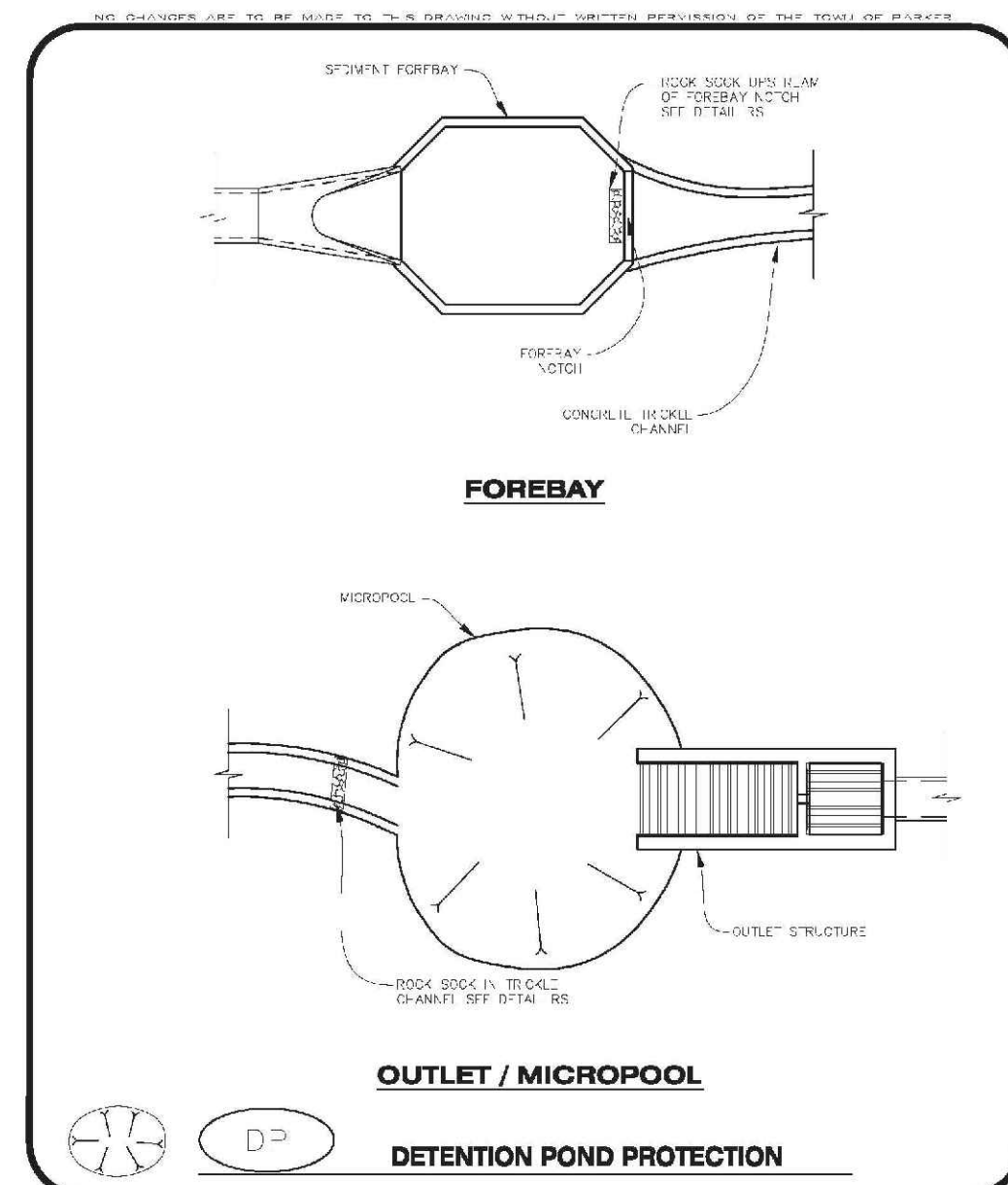
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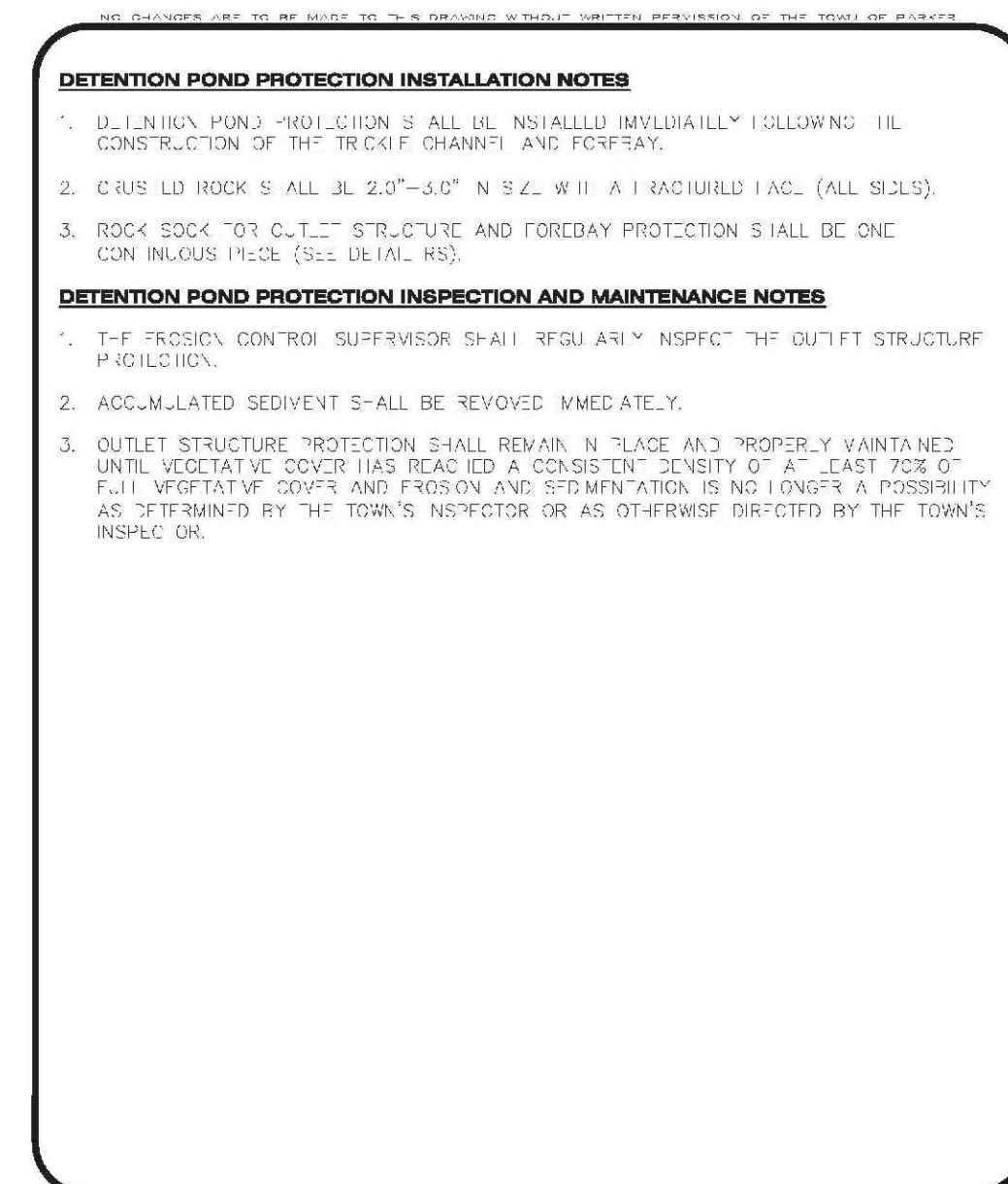
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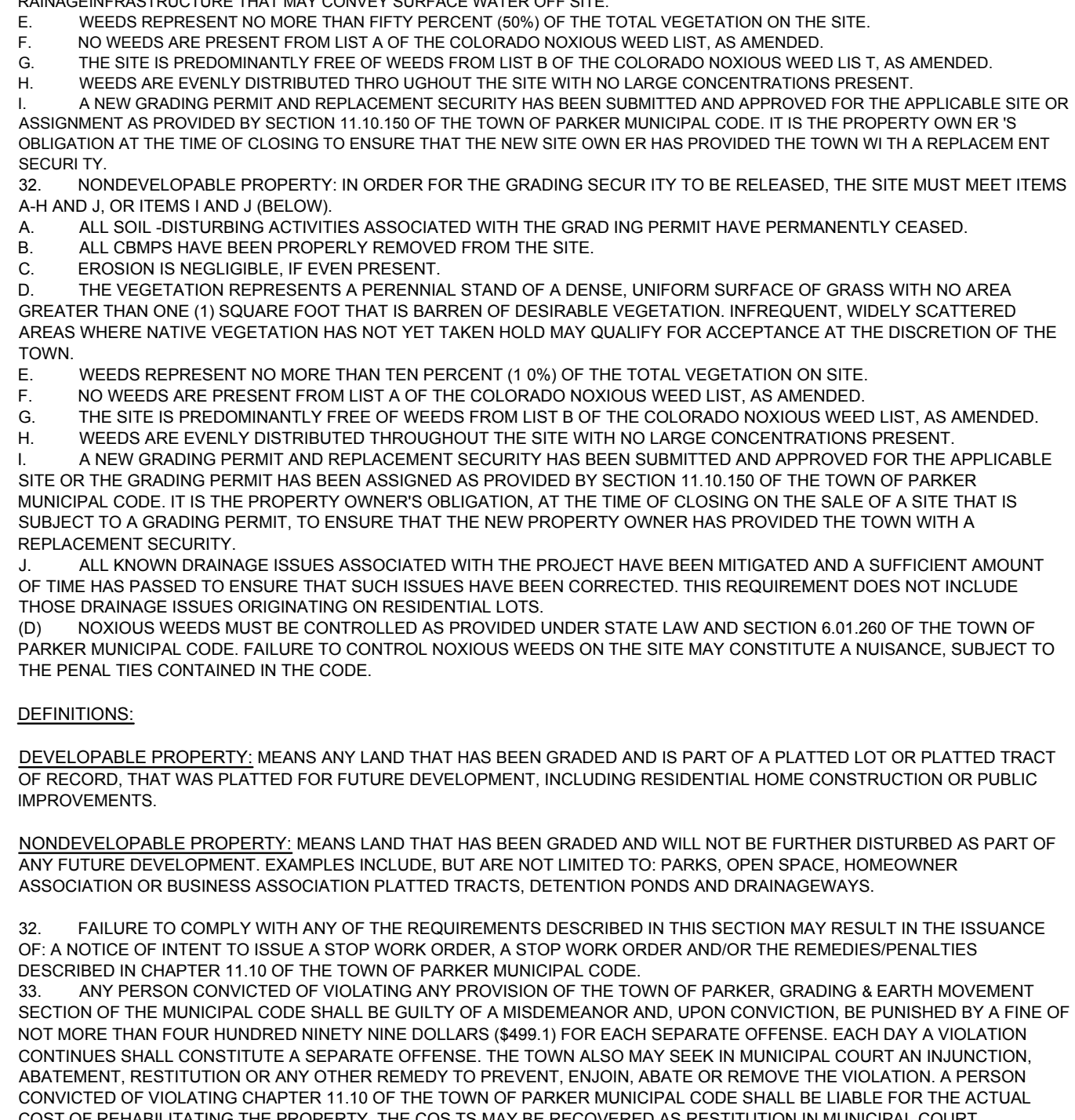
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**CBMP**

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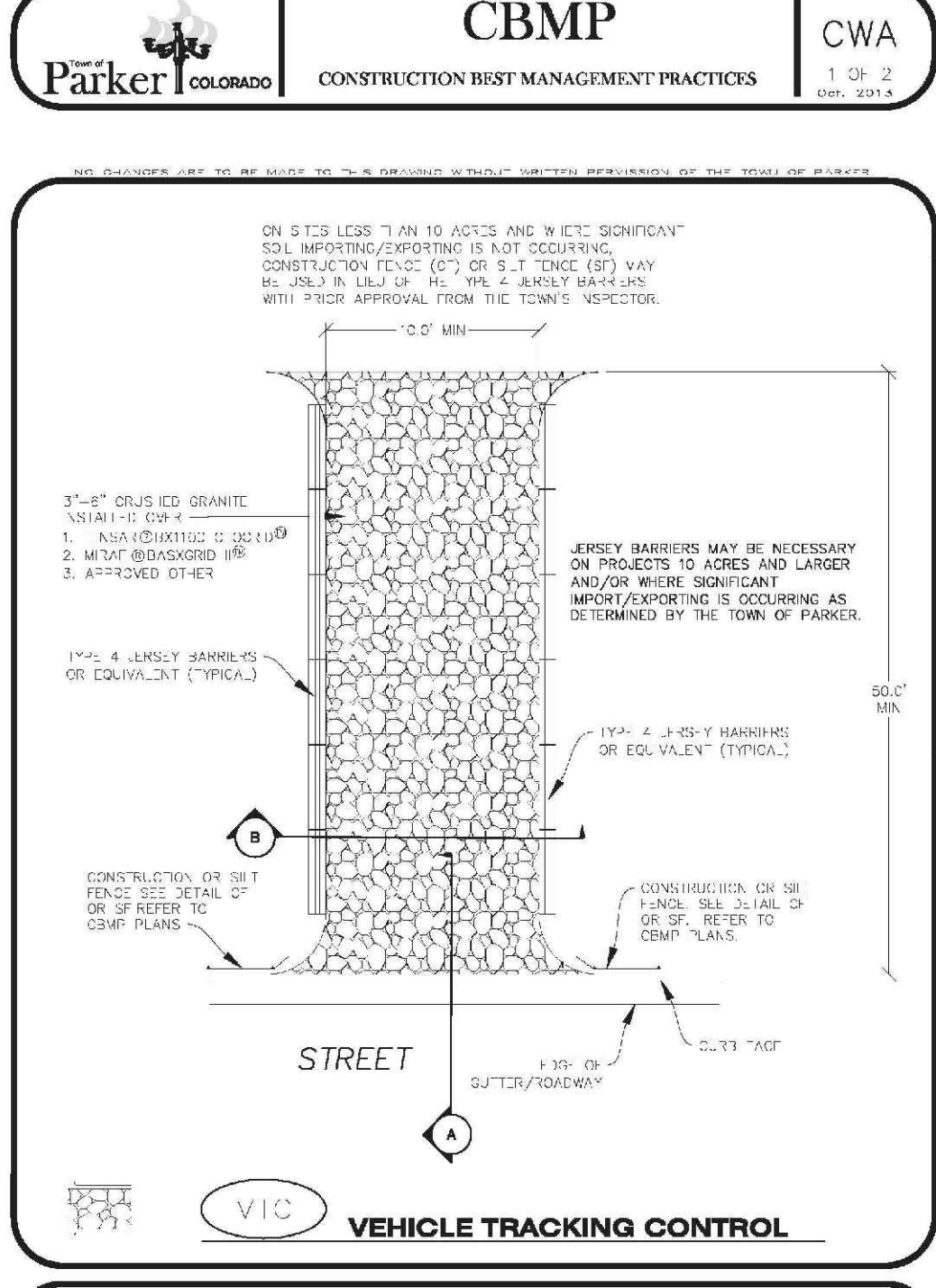
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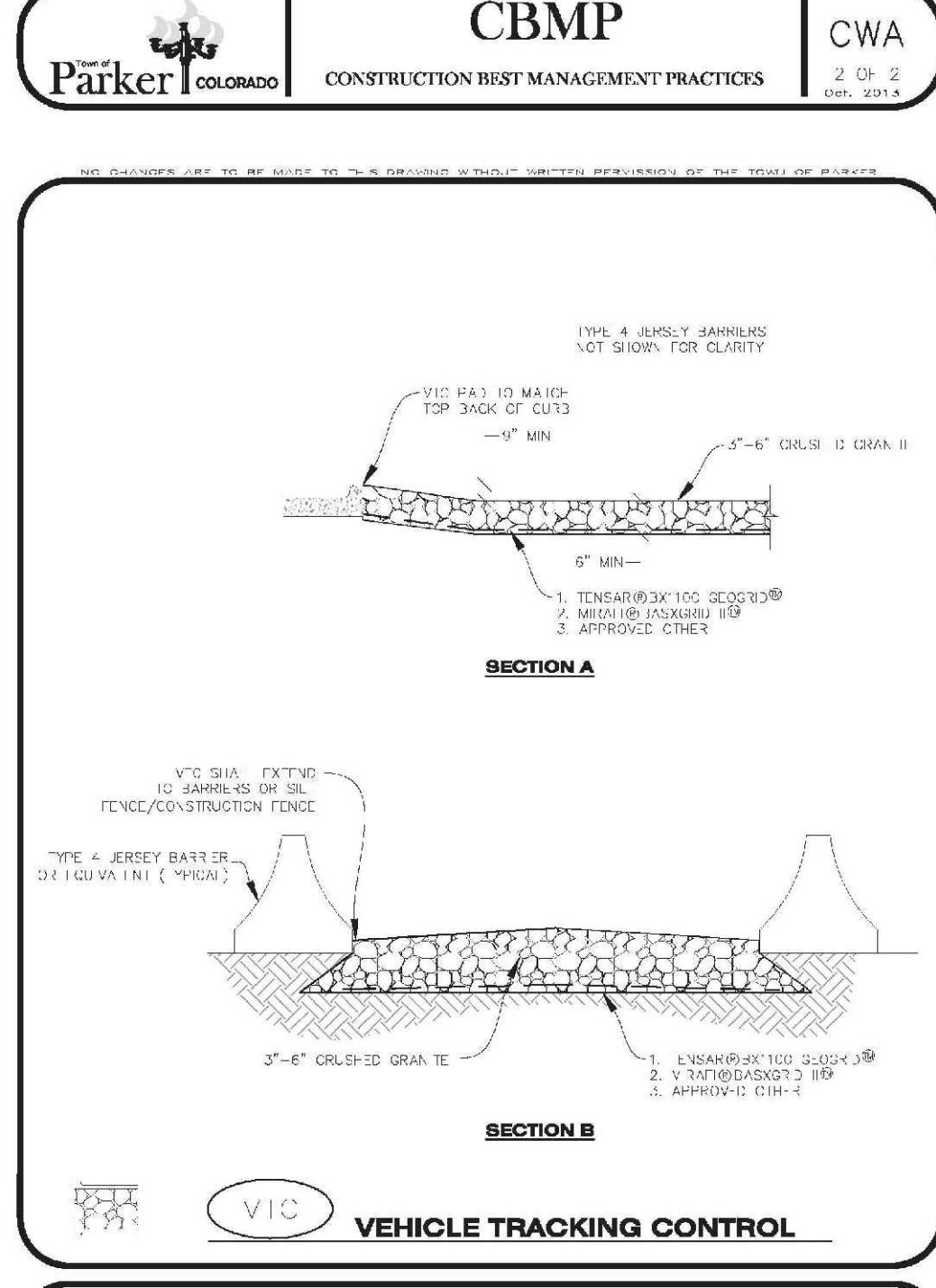
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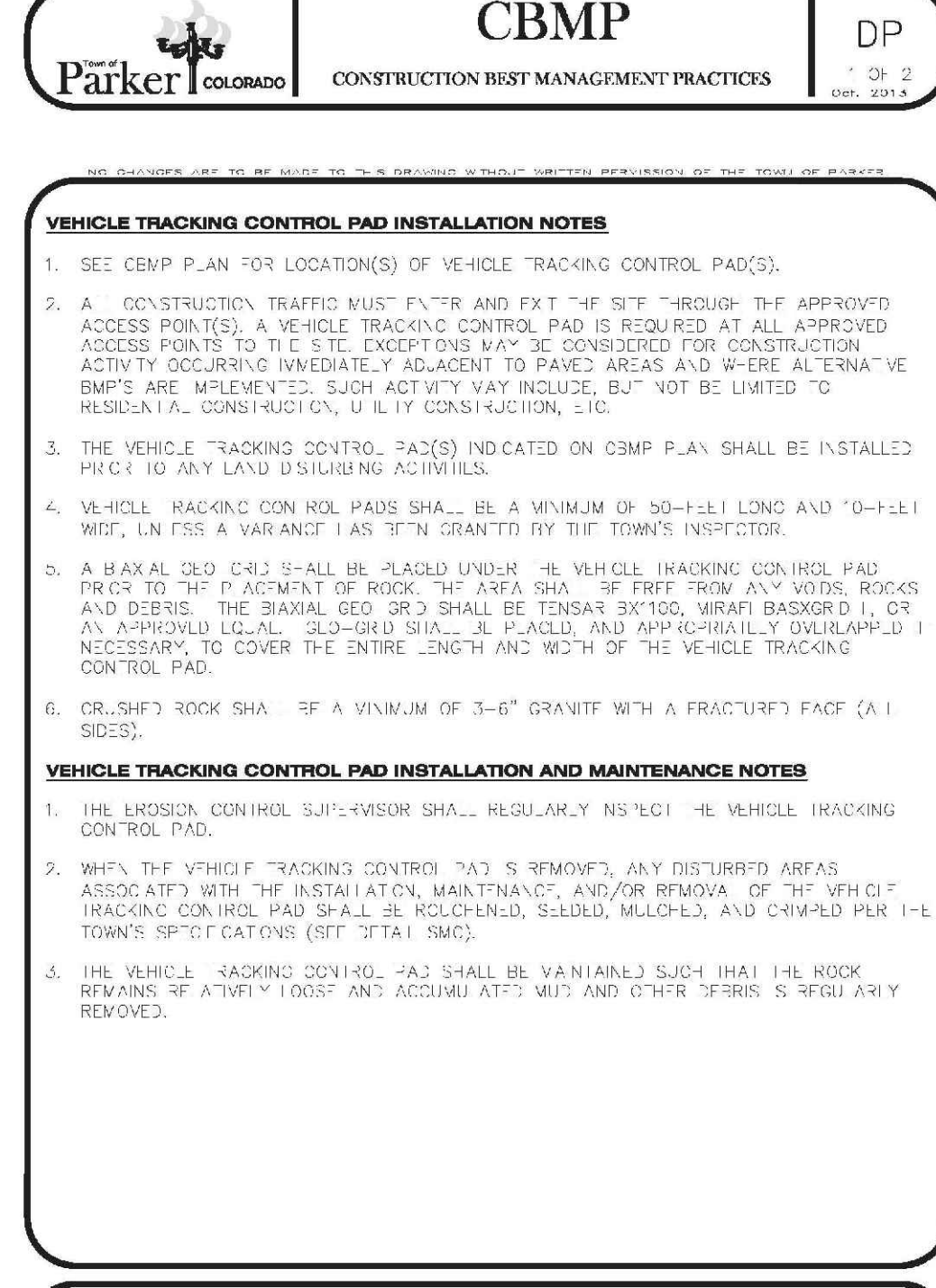
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CONSTRUCTION BEST MANAGEMENT PRACTICES

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**CBMP**

CONSTRUCTION BEST MANAGEMENT PRACTICES

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**CBMP**

CONSTRUCTION BEST MANAGEMENT PRACTICES

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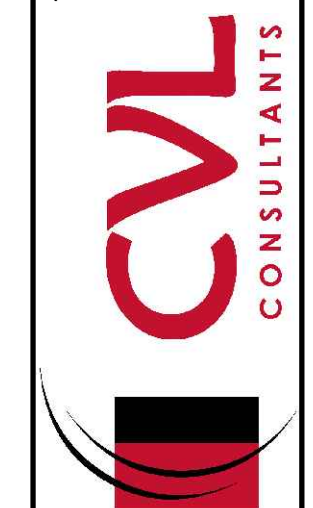
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							6				

TRAILS AT CROWFOOT  
 FILING 13 CONSTRUCTION DRAWINGS  
 BMP DETAILS

HR 935 LLC  
 7353 South Alton Way  
 CENTENNIAL, CO 80112

10338 E. Dry Creek Rd.  
 Suite 410  
 Englewood, CO 80150  
 Tel: (720) 482-9526  
 Fax: (720) 482-9546



**DEBRIS AND TRASH CONTROL**

**NOTE:**

1. THE EROSION CONTROL SUPERVISOR IS RESPONSIBLE FOR ENSURING THAT ALL STREETS, CURBS, GUTTERS, SIDEWALKS, DRIVEWAYS, PARKING LOTS, ALLEYS, TRUCK CHANNELS, AND OTHER IMPROVED SURFACES IMPACTED BY CONSTRUCTION ACTIVITIES ARE THOROUGHLY CLEANED THROUGHOUT THE DAY IF THEY BECOME SOILED. THESE AREAS MUST ALSO BE THOROUGHLY CLEANED AT THE END OF THE WORK DAY TO INCLUDE ALL CURB, TRASH AND LITTER ASSOCIATED WITH THE PROJECT MUST BE REMOVED AND PROPERLY DISPOSED ON A DAILY BASIS.

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**DEBRIS CONTROL NOTES:**

1. A COMBINATION OF SURFACE SCANNING AND SWEEPING MAY BE NECESSARY TO PROPERLY CLEAN THE AREAS.
2. ALL CHYMEI OILS AND STAINS ON THE SITE SHALL BE CLEANED TO THE MAXIMUM EX EXTENT POSSIBLE. IN SOME CASES IT MAY BE NECESSARY TO USE PRESSURIZED WATER AND A VACUUM TRUCK.
3. ON-SITE PERSONNEL, INCLUDING DRIVERS, ETC., SHOULD BE EDUCATED ON THE NEED FOR CONTINUOUS EFFORTS AND TRANS-CONTO.

**EROSION CONTROL BLANKET (SLOPE)**

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REV. 02/14

**EROSION CONTROL BLANKET (CHANNEL)**

**NOTE:**

1. WOOD STAKES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATION. IF MANUFACTURER'S RECOMMENDATION IS NOT AVAILABLE, THEN WOOD STAKES SHALL BE 2" DIA. AND 18" LONG. STAKES SHALL NOT BE USED FOR EROSION CONTROL BLANKETS ON SLOPES.

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**EROSION CONTROL BLANKET (CHANNEL)**

**NOTE:**

1. WOOD STAKES SHALL BE INSTALLED PER MANUFACTURER'S RECOMMENDATION. IF MANUFACTURER'S RECOMMENDATION IS NOT AVAILABLE, THEN WOOD STAKES SHALL BE 2" DIA. AND 18" LONG. STAKES SHALL NOT BE USED FOR EROSION CONTROL BLANKETS ON SLOPES.

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**EROSION CONTROL BLANKET INSTALLATION NOTES**

1. SET THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.
2. EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE THOROUGHLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THIS CALCULATION, THE EROSION CONTROL BLANKET OR RIFRAP MAY BE NECESSARY IN THE EROSION CONTROL BLANKETS.
3. INSTALLATION PRIOR TO BLANKET INSTALLATION, EROSION CONTROL BLANKETS SHALL BE THOROUGHLY CLEANED AND FREE OF ANY GAPS, HOLES, WEBS, ROCKS, STAKES OR OTHER UNDESIRABLE DEBRIS.
4. EROSION CONTROL BLANKETS SHALL BE INSTALLED ACCORDING TO THE DETAIL DRAWINGS.
5. ANY DAMAGED OR WEAVING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.
6. ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE LAYER, STRAW OR EXCELSDOR.

MANUFACTURER	PRODUCT NAME
AGRI-MANUFACTURING	ST150
APPROVED EQUAL	APPROVED EQUAL

**EROSION CONTROL BLANKET MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

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**AREA INLET PROTECTION INSTALLATION NOTES**

1. SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.
2. THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS IN PAVEMENT OR ANY LAND DISTURBING ACTIVITIES ON UNPAVED AREAS. THE INSTALLATION OF NEW INLETS, AN INTERMEDIATE INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.
2. AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
3. WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION SHALL BE REVEGETATED, SEEDING, MULCHED, AND COMPLETED PER THE TOWN'S INSPECTOR'S GUIDANCE (SEE DETAIL SMC).
4. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

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**AREA INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

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**AREA INLET PROTECTION FOR AREA INLETS IN PAVEMENT**

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**AREA INLET PROTECTION, CURB ON-GRADE, TYPE R INLET**

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**TUBULAR TRAFFIC MARKER DETAIL**

1 OF 2  
REV. 02/14

**INLET PROTECTION, CURB ON-GRADE INSTALLATION NOTES**

1. SET CBMP PLAN FOR LOCATION(S) OF ON-GRADE INLET PROTECTION.
2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
3. ROCK SOCK FOR ON-GRADE INLET PROTECTION SHALL BE ONE CONTINUOUS PIECE.
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, WELD TO KILL ROCK TO PREVENT SPINNING OUT.
5. ROCK SOCK SHALL BE PLACED 5.0' UPHILL OF THE INLET OPENING.
6. TUBULAR MARKERS SHALL BE A MINIMUM OF 3.0' HIGH WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
7. THE CURB INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE TUBULAR MARKER INSTALLATION OF THE INLET ON ROADWAYS ADJACENT TO THE INLET.

**ON-GRADE INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ON-GRADE INLET PROTECTION.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
4. ON-GRADE INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

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REV. 02/14

**INLET PROTECTION, CURB ON SUMP, TYPE R INLET**

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REV. 02/14

**CURB INLET PROTECTION INSTALLATION NOTES**

1. SET CBMP PLAN FOR LOCATION(S) OF CURB INLET PROTECTION.
2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
3. ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPINNING OUT.
5. ROCK SOCK SHALL BE 3.0' HIGH WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
6. TUBULAR MARKERS SHALL BE A MINIMUM OF 36" IN HEIGHT WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.
7. THE CURB INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES ON UNPAVED AREAS. THE INSTALLATION OF NEW INLETS, AN INTERMEDIATE INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**CURB INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CURB INLET PROTECTION.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
4. CURB INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

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REV. 02/14

**LOT PROTECTION**

1 OF 2  
REV. 02/14

**EROSION CONTROL BLANKET FOR LOT PROTECTION INSTALLATION NOTES**

1. ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION INSTALLED IN THE TOWN SHALL MEET THE FOLLOWING MINIMUM REQUIREMENTS:
2. ALL EROSION CONTROL BLANKETS FOR LOT PROTECTION SHALL BE INSTALLED ACCORDING TO THE DETAIL DRAWINGS, WITH THE FOLLOWING ALLOWANCES AND ADDITIONS:
  - THE TOWN WILL ALLOW THE USE OF BIODEGRADABLE EROSION CONTROL BLANKETS IF THEY ARE AT LEAST 18" HIGH AND MADE OF UNPAVED AREAS.
  - ALL EROSION CONTROL BLANKET JOINTS (SEAMS AND ENDS) MUST OVERLAP THE ADJACENT BLANKET BY A MINIMUM OF 6" WITH THE DOWN-SLOPE EDGE OF THE ADJACENT BLANKET BEING PLACED ON TOP OF THE DOWN-SLOPE EDGE OF THE ADJACENT BLANKET.
  - ALL JOINTS (SEAMS AND ENDS) OF THE EROSION CONTROL BLANKETS SHALL BE NEEDED TO BE REVEGETATED INTO THE GROUND ASSURING THE SITE CONDITIONS WILL NOT CAUSE EROSION BEHIND THE BLANKETS. THESE ASSUMPTIONS WILL BE THE RESPONSIBILITY OF THE EROSION CONTROL SUPERVISOR. ON OCCASION, THE TOWN'S INSPECTOR MAY REQUEST REVEGETATION BASED UPON SITE CONDITIONS.
  - TOPSOIL PLACEMENT, SEEDING, AND MULCHING WILL NOT BE REQUIRED ON THE LOTS PRIOR TO THE INSTALLATION OF THE EROSION CONTROL BLANKET.
3. ANY DAMAGED OR EXCESS STAPLES SHALL BE REMOVED FROM THE SITE UPON CONSTRUCTION INSTALLATION.
4. THE EROSION CONTROL BLANKETS REQUIRED AS PART OF THE LOT PROTECTION (LP) REQUIREMENT MUST BE INSTALLED ON ALL UNPAVED AREAS WITHIN EACH RESIDENTIAL, COMMERCIAL, AND COMMUNITY LOT PRIOR TO THE ISSUANCE OF A CERTIFICATE OF OCCUPANCY / TEMPORARY CERTIFICATE OF OCCUPANCY.

**EROSION CONTROL BLANKET FOR LOT PROTECTION MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKET FOR LOT PROTECTION.
2. EROSION CONTROL BLANKETS FOR LOT PROTECTION ARE INTENDED TO REMAIN IN PLACE AND MAINTAINED UNTIL LANDSCAPING IS INSTALLED.

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REV. 02/14

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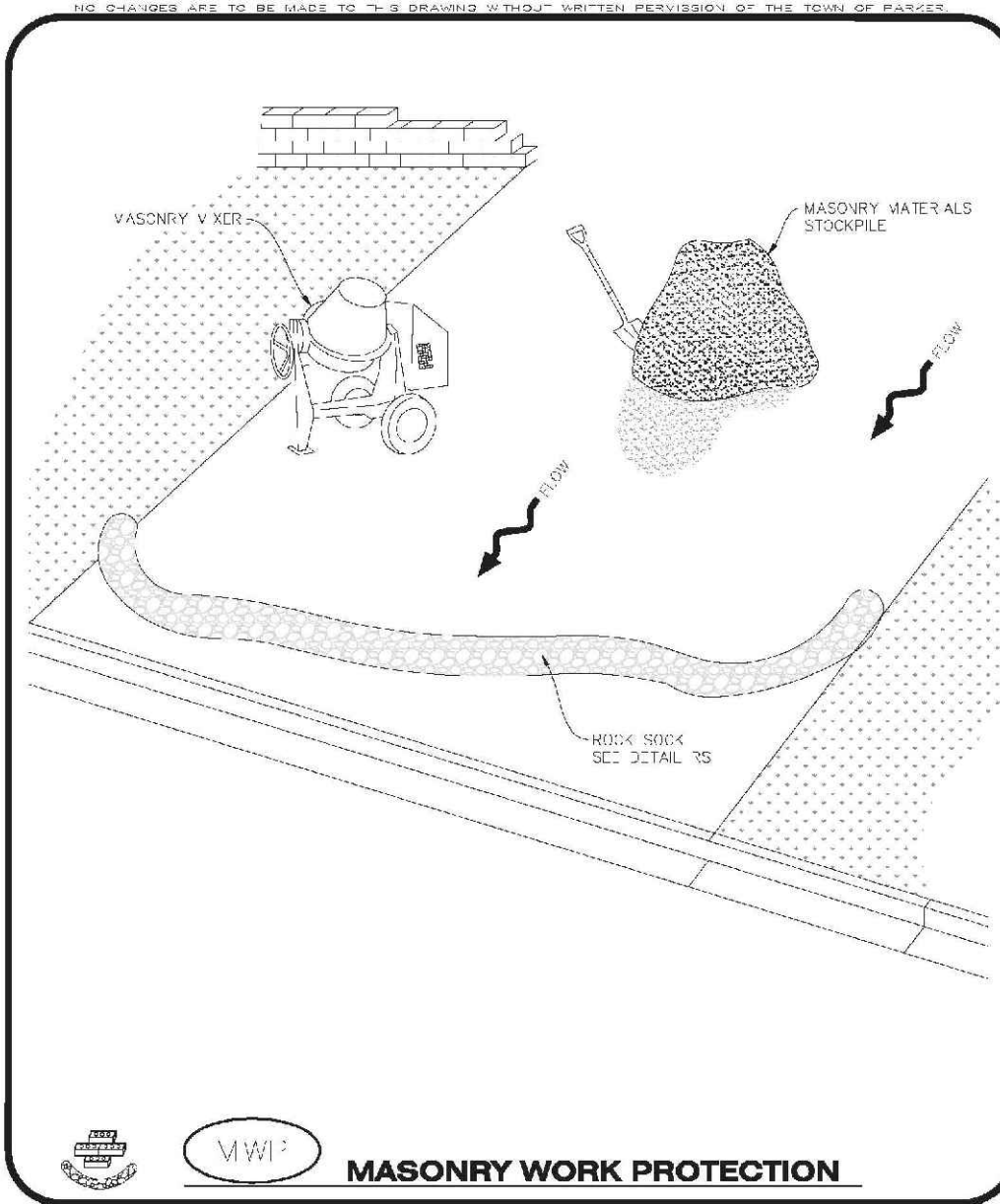
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TRAILS AT CROWFOOT  
FILING 13 CONSTRUCTION DRAWINGS  
BMP DETAILS

SHEET NUMBER	7
DRAWN BY:	JF
CHECKED BY:	JU
DATE:	SEPTEMBER 2017
SCALE:	AS SHOWN
FILE NO.:	8130283701

No.	Revisions	Date	Init.	Appr.	Date



**MASONRY WORK PROTECTION**

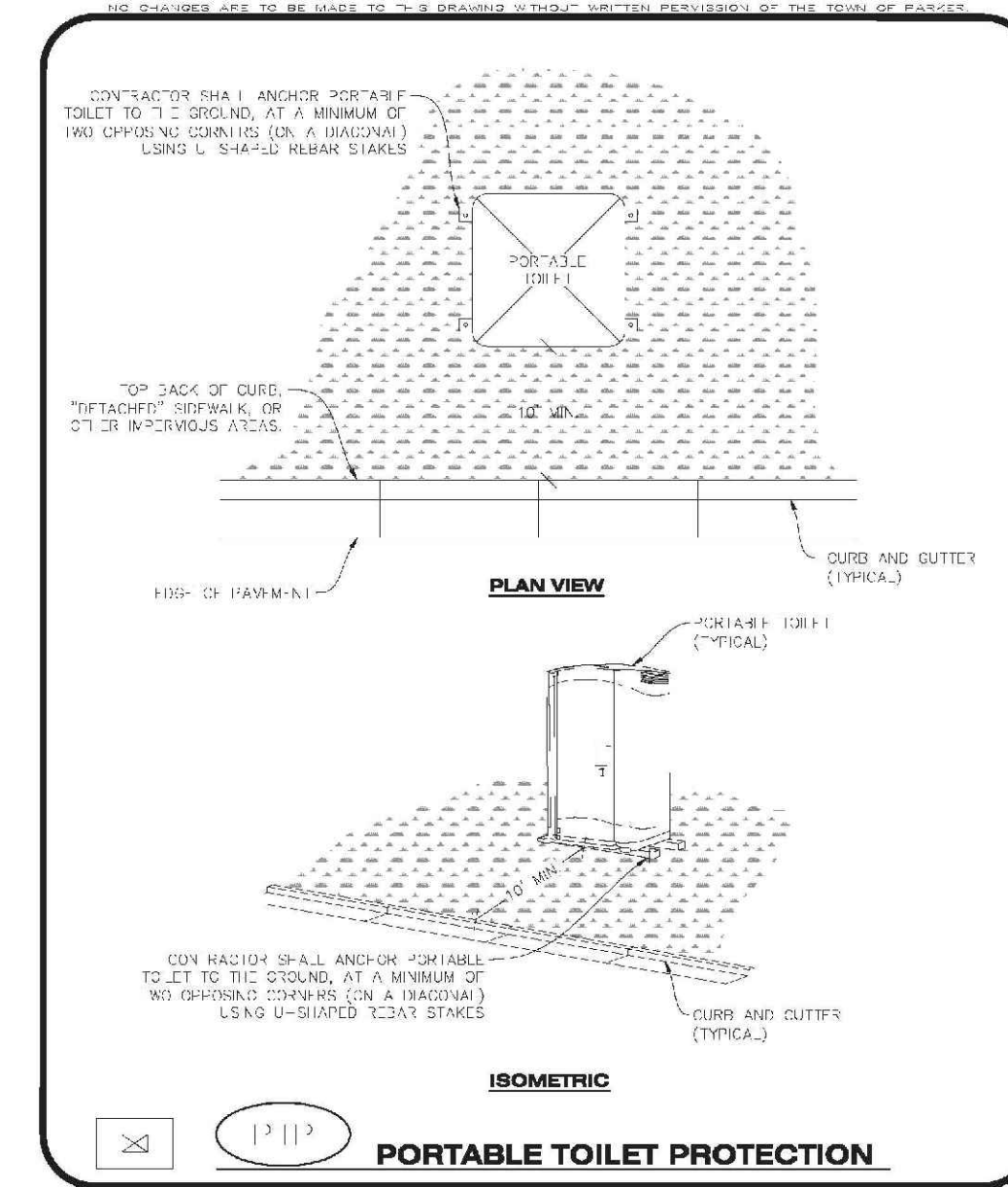
**MASONRY WORK PROTECTION INSTALLATION NOTES**

1. MASONRY WORK PROTECTION SHALL BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
2. A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
3. CRUSHED ROCK SHALL BE 2.0' x 3.0' N SIZE WITH A FRACTURED FACE (ALL SIDES).
4. ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAP-UP JOINTS (SEE DETAIL 10).
5. ROCK SOCK SHALL BE CONSTRUCTED USING CHAIN WIRE OR OTHER APPROVED MATERIAL, SHALL BE 4.0' HIGH FROM FINISHED GROUND.

**MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE PROJECT CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
2. ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASTE COLLECTION AREA.
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.

**CBMP** MWP  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014



**PORTABLE TOILET PROTECTION**

**CBMP** PTP  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014

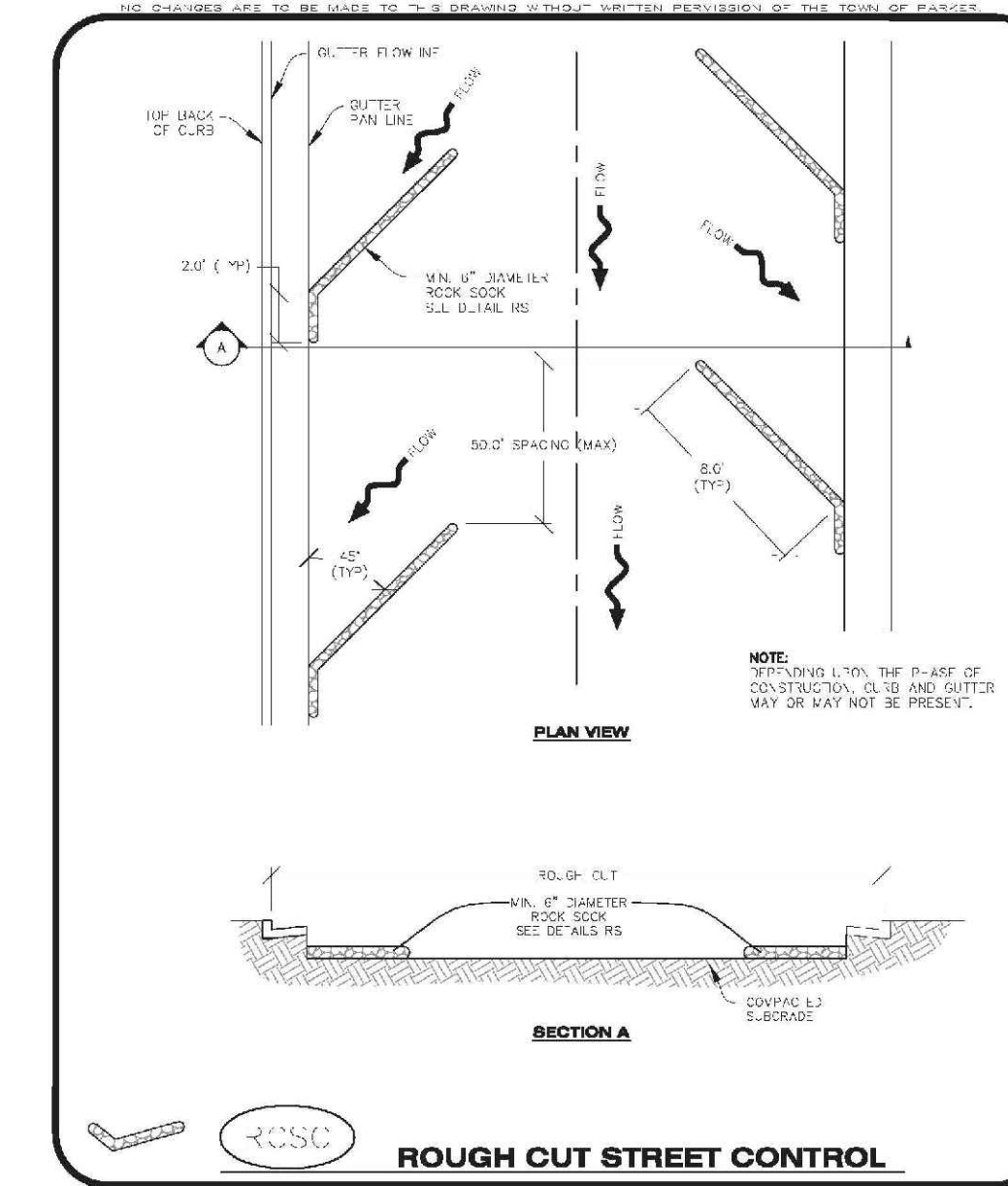
**PORTABLE TOILET PROTECTION INSTALLATION NOTES**

1. PORTABLE TOILETS SHALL BE PLACED A MINIMUM OF 10.0' BEHIND ALL CURBS, SIDEWALKS, AND OTHER IMPERVIOUS AREAS.
2. ALL PORTABLE TOILETS MUST BE CIRCUMFERENCED WITH U-SHAPED STAKES.
3. PORTABLE TOILETS SHALL BE SECURELY ANCHORED TO THE GROUND USING U-SHAPED STAKES.
4. U-SHAPED STAKE STAKES SHALL BE POSITIONED ON AT LEAST 2 OPPOSITE DIRECTION CORNERS.

**PORTABLE TOILET PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE PORTABLE TOILET PROTECTION.
2. PORTABLE TOILETS SHALL BE SECURED AT THE NEAREST IMPERVIOUS CURB OR WALL TO THE POSSIBILITY OF OVERTURN.
3. WHEN THE PORTABLE TOILETS ARE BEING USED, ANY DISBURSED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE TOILETS MAY NEED TO BE LANDSCAPED OR ROCK-CURED, SEEDED, MULCHED, AND GRATED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL 5M).
4. PORTABLE TOILETS THAT ARE NOT CONSISTENTLY MAINTAINED ACCORDANCE WITH THESE REQUIREMENTS MAY NEED TO BE DISBURSED IMMEDIATELY IN ORDER TO INCREASE COMPLIANCE AND REDUCE THE CHANCE OF A SPILL.

**CBMP** PTP  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014



**ROUGH CUT STREET CONTROL**

**CBMP** RCSC  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 2  
REV. 2014

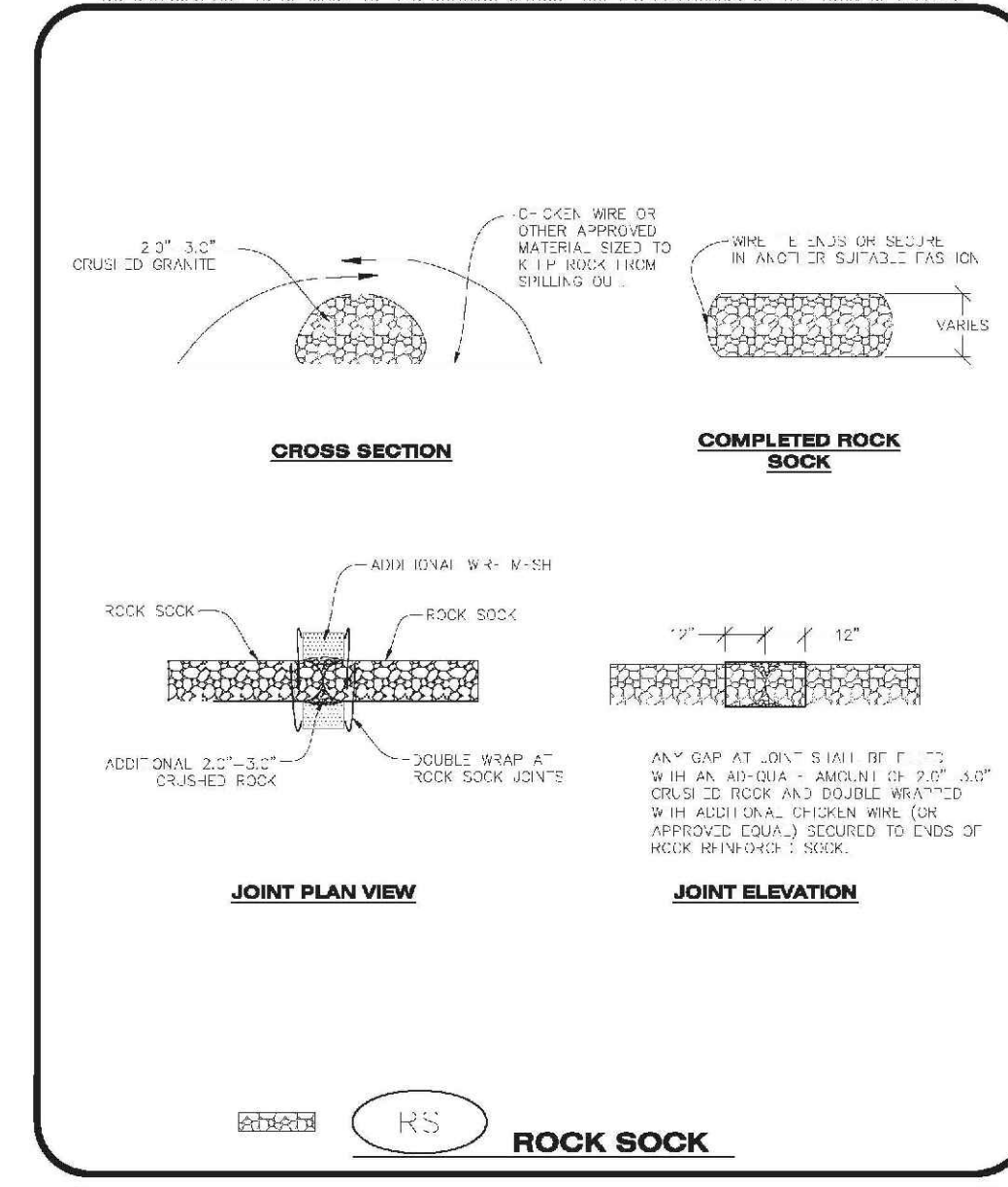
**ROUGH CUT STREET CONTROL INSTALLATION NOTES**

1. SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
2. THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.

**ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES**

1. THE PROJECT CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
3. ROCK SOCKS SHALL BE REPLACED OR ALL REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
4. ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUBGRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE ROCK SOCKS SHOULD BE REMOVED IN ACCORDANCE WITH THE TOWN'S SPECIFICATIONS.

**CBMP** RCSC  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014



**ROCK SOCK**

**CBMP** RS  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014

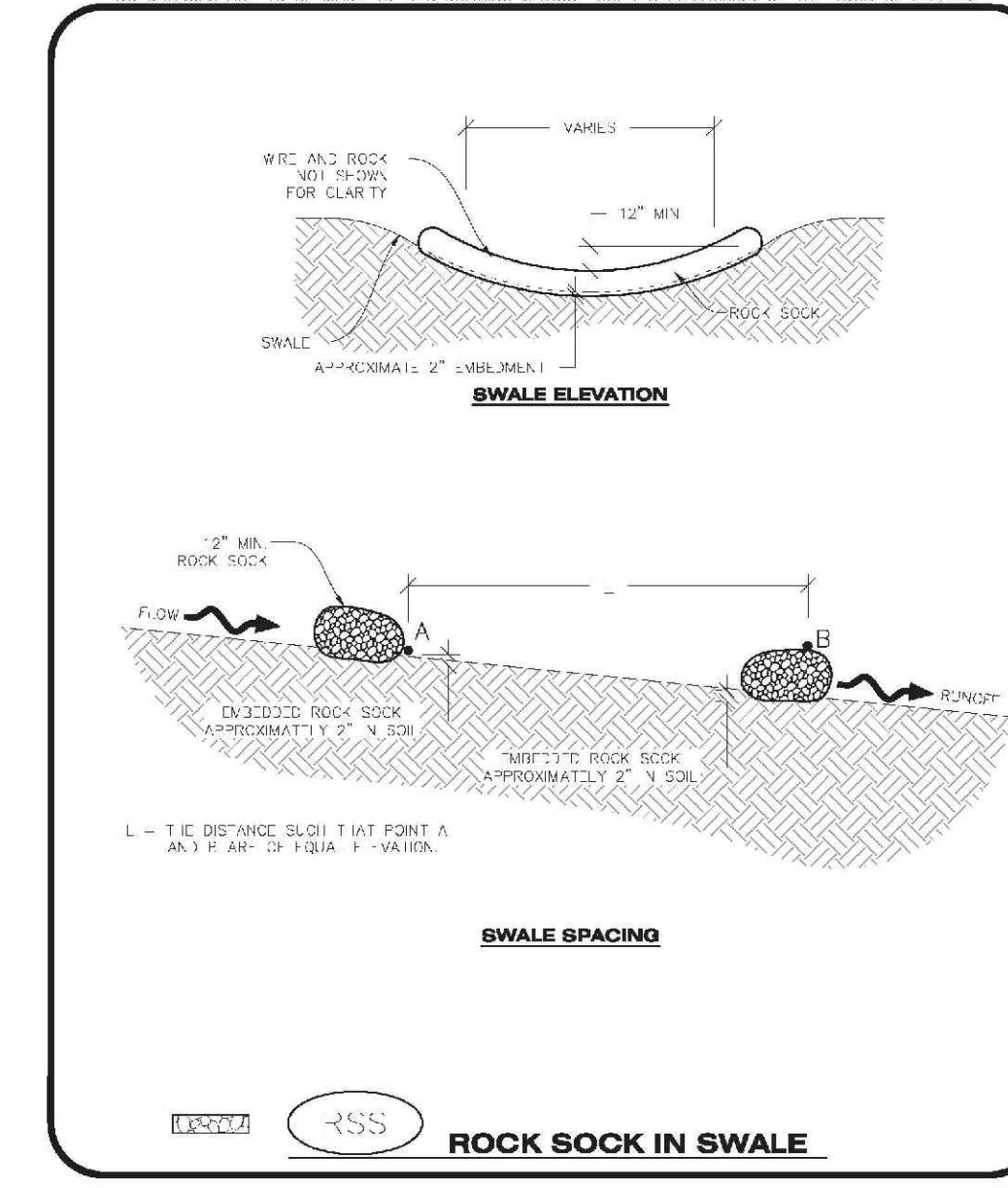
**ROCK SOCK INSTALLATION NOTES**

1. SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
2. CRUSHED ROCK SHALL BE APPROXIMATELY 2.0' x 3.0' GRAVEL SIZE WITH A FRACTURED FACE (ALL SIDES).
3. ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAP-UP JOINTS (SEE DETAIL 10).
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHAIN WIRE OR OTHER APPROVED MATERIAL, SHALL BE 4.0' HIGH FROM FINISHED GROUND.
5. MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
6. TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCAL JURISDICTION TO PREVENT TRAFFIC FROM STRAYING INTO THE WORK AREA.

**ROCK SOCK INSPECTION AND MAINTENANCE NOTES**

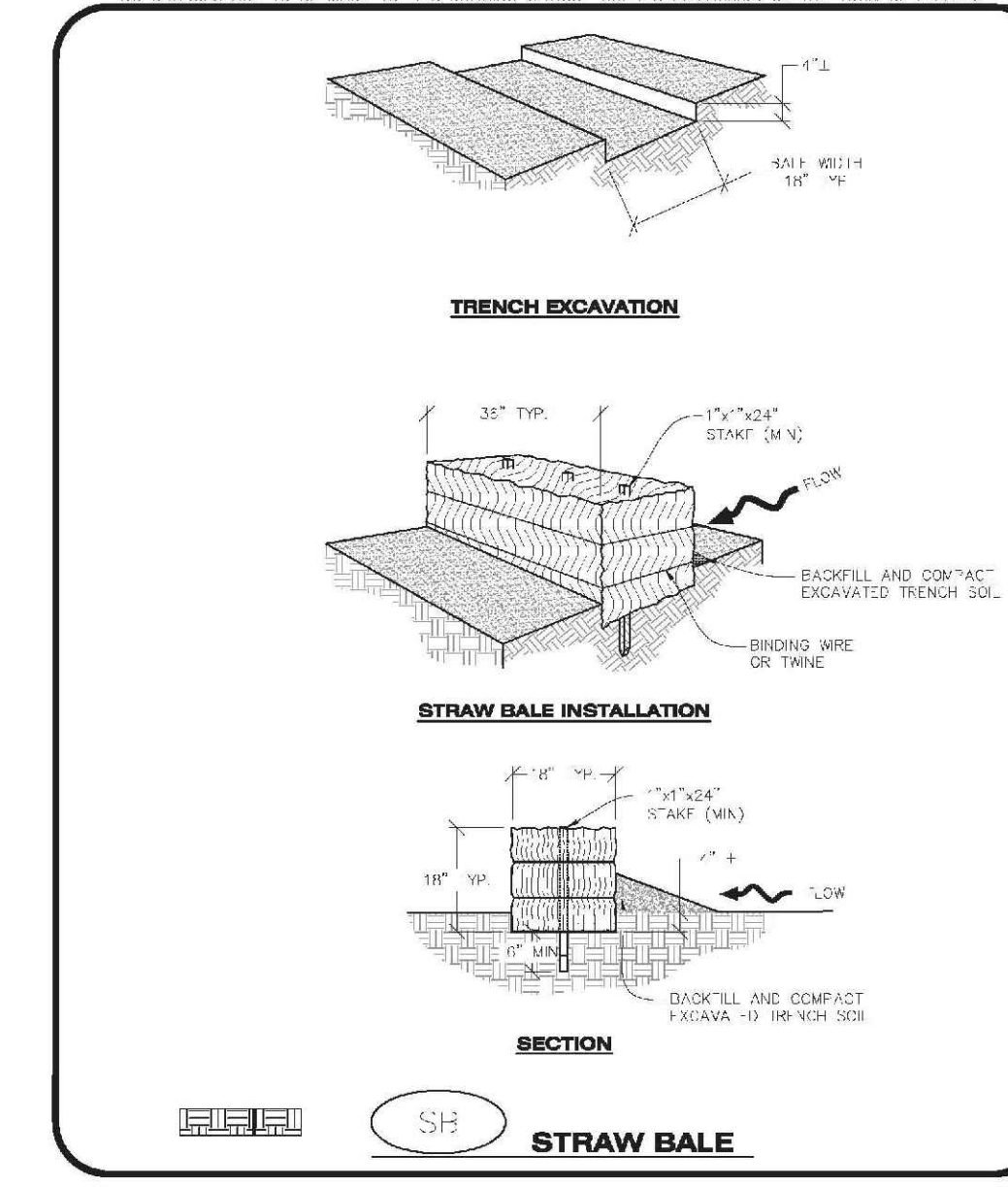
1. THE PROJECT CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
2. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
3. ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** RS  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014



**ROCK SOCK IN SWALE**

**CBMP** RSS  
CONSTRUCTION BEST MANAGEMENT PRACTICES 1 OF 1  
REV. 2014



**STRAW BALE**

**CBMP** SB  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014

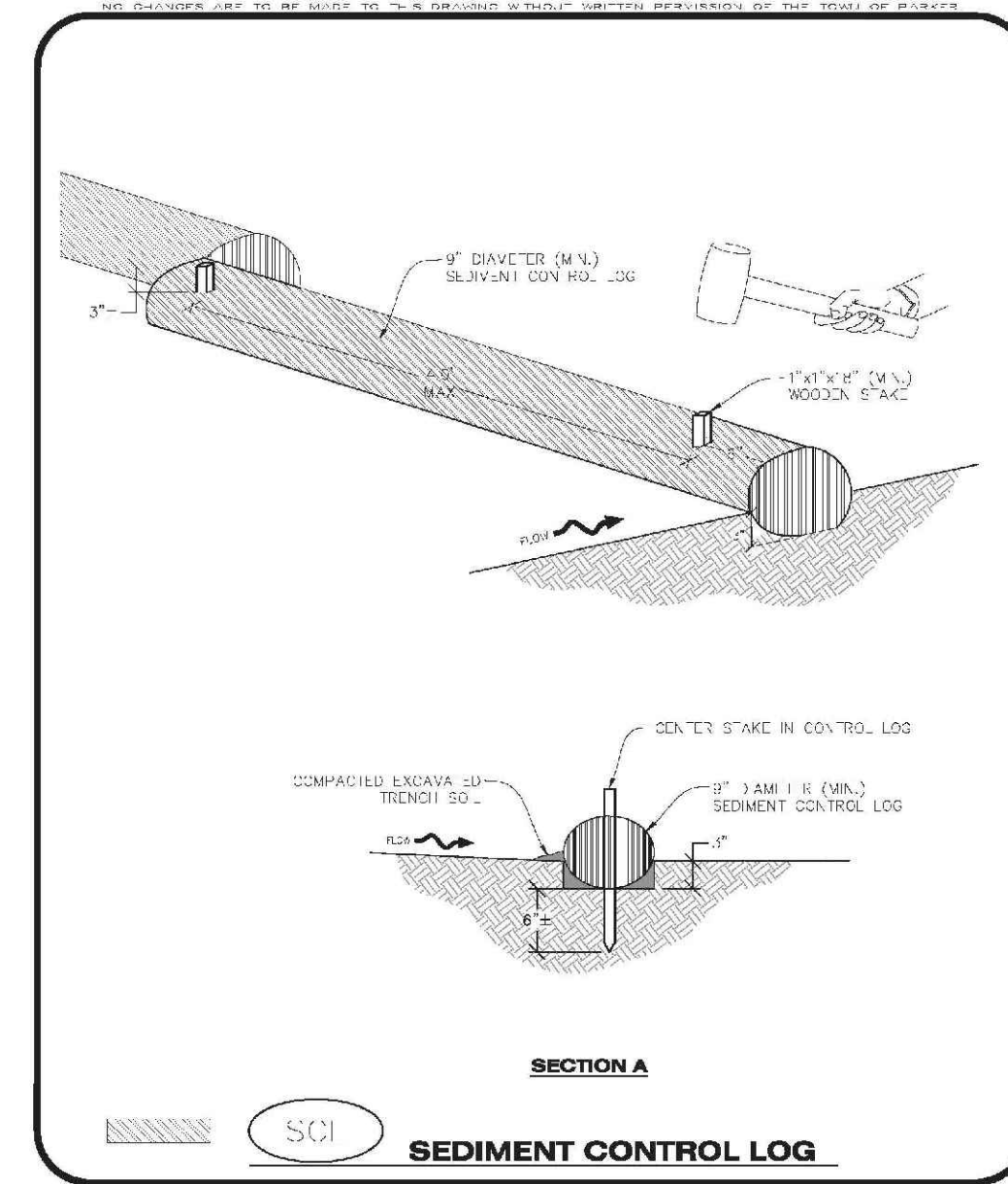
**STRAW BALE INSTALLATION NOTES**

1. SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
2. TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36" x 18" x 18".
3. TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1" x 4" x 24".
4. WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
5. STRAW BALES SHALL BE STAGED AND POSITIONED ACCORDING TO DETAILS.

**STRAW BALE INSPECTION AND MAINTENANCE NOTES**

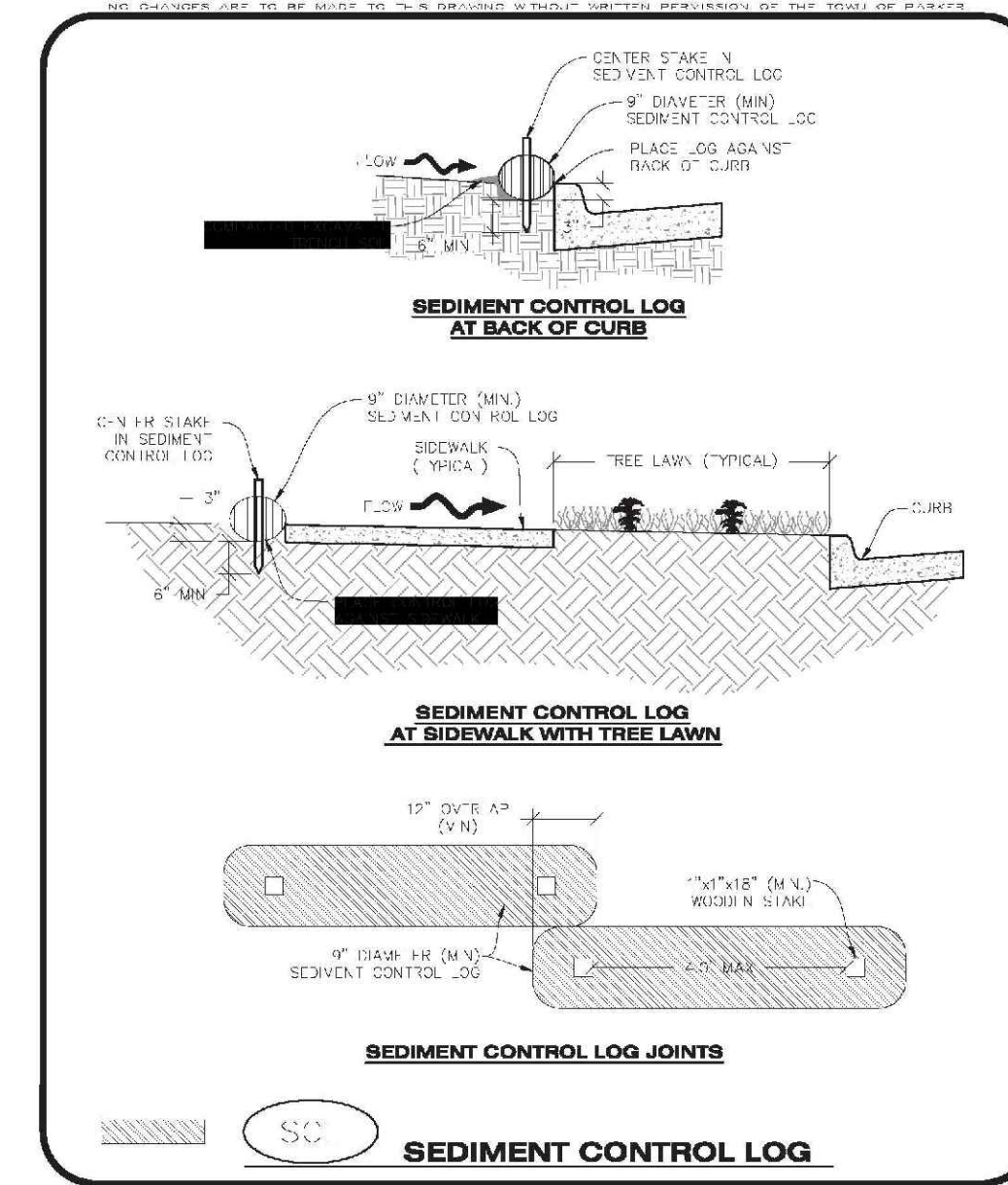
1. THE PROJECT CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
3. STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
4. STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
5. WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE REVEGETATED, SEEDED, MULCHED, AND GRATED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL 5M).

**CBMP** SB  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 2  
REV. 2014



**SEDIMENT CONTROL LOG**

**CBMP** SCL  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 3  
REV. 2014



**SEDIMENT CONTROL LOG**

**CBMP** SCL  
CONSTRUCTION BEST MANAGEMENT PRACTICES 2 OF 3  
REV. 2014

**SEDIMENT CONTROL LOG INSTALLATION NOTES**

1. SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT CONTROL LOGS.
2. ALL SEDIMENT CONTROL LOGS SHALL BE INSTALLED UPSTREAM OF ALL CURBS, SIDEWALKS, AND OTHER IMPERVIOUS AREAS.
3. SEDIMENT CONTROL LOGS SHALL BE INSTALLED WITH A MINIMUM OF 10.0' FROM THE CURB OR SIDEWALK TO THE GROUND.
4. SEDIMENT CONTROL LOGS SHALL BE INSTALLED WITH A MINIMUM OF 10.0' FROM THE CURB OR SIDEWALK TO THE GROUND.
5. EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH AND PROPERLY COMPACTED.
6. ANCHOR TRENCH SHALL BE RELATIVELY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT OF THE SEDIMENT CONTROL LOG.
7. ALL SEDIMENT CONTROL LOGS SHALL BE PLACED 3" (APPROX.) BELOW THE GROUND AND PULLED TIGHT ON BOTH ENDS TO REMOVE ANY CURVES OR SAGS.
8. THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS RELATIVELY FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED AGAINST THE GROUND AND SEDIMENT CONTROL LOG USING A SHOVEL OR SIMILAR DEVICE.
9. SEDIMENT CONTROL LOG STAKES SHALL BE MADE OF WOOD AND SECURELY ANCHOR TO THE SOIL IN PLACE.
10. STAKES SHALL BE PLACED ON 4.0' CENTERS AND EMBEDDED APPROXIMATELY 6" INTO THE GROUND. STAKES THAT ARE BROKEN PRIOR TO OR DURING INSTALLATION SHALL BE REPLACED.
11. SEDIMENT CONTROL LOGS SHALL OVERLAP A MINIMUM OF 12". THE OVERLAPPING SHALL OCCUR ON THE UPHILL SIDE OF THE LOGS.
12. SEDIMENT CONTROL LOGS SHALL BE STAGED WITHIN 6" FROM EACH END.
13. SEDIMENT CONTROL LOGS SHALL BE INSTALLED WITH CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN THE SEDIMENT CONTROL LOG AND THE CURB OR SIDEWALK. GAPS (LESS THAN 2") MAY BE REPAIRED IN ANY ANGLES WHERE SOILS DO NOT OCCUR.

**SEDIMENT CONTROL LOG INSPECTION AND MAINTENANCE NOTES**

1. THE PROJECT CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEDIMENT CONTROL LOGS.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE LOGS.
3. SEDIMENT CONTROL LOGS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. SEDIMENT CONTROL LOGS SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR OR DAMAGE THAT WOULD PREVENT THE SEDIMENT CONTROL LOG FROM FUNCTIONING AS DESIGNED.
5. WHEN THE SEDIMENT CONTROL LOGS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SEDIMENT CONTROL LOGS MAY NEED TO BE REVEGETATED, SEEDED, MULCHED, AND GRATED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL 5M).

**CBMP** SCL  
CONSTRUCTION BEST MANAGEMENT PRACTICES 3 OF 3  
REV. 2014

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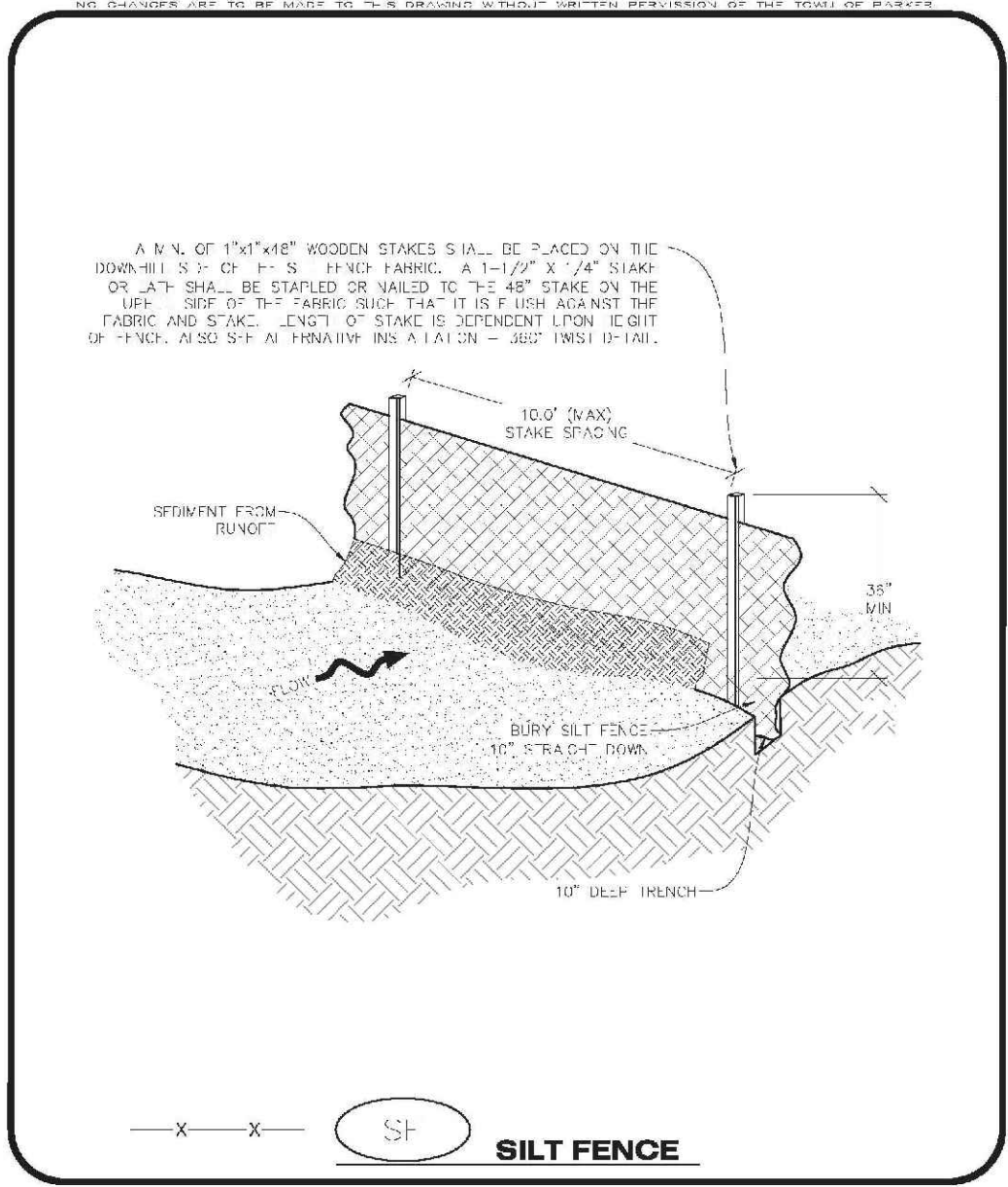
SHEET NUMBER	DRAWN BY: JF	SCALE: AS SHOWN	FILE NO: 8130283701	DATE: SEPTEMBER 2017	Revisions	Appr.	Date
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8	JF	AS SHOWN	8130283701	SEPTEMBER 2017			

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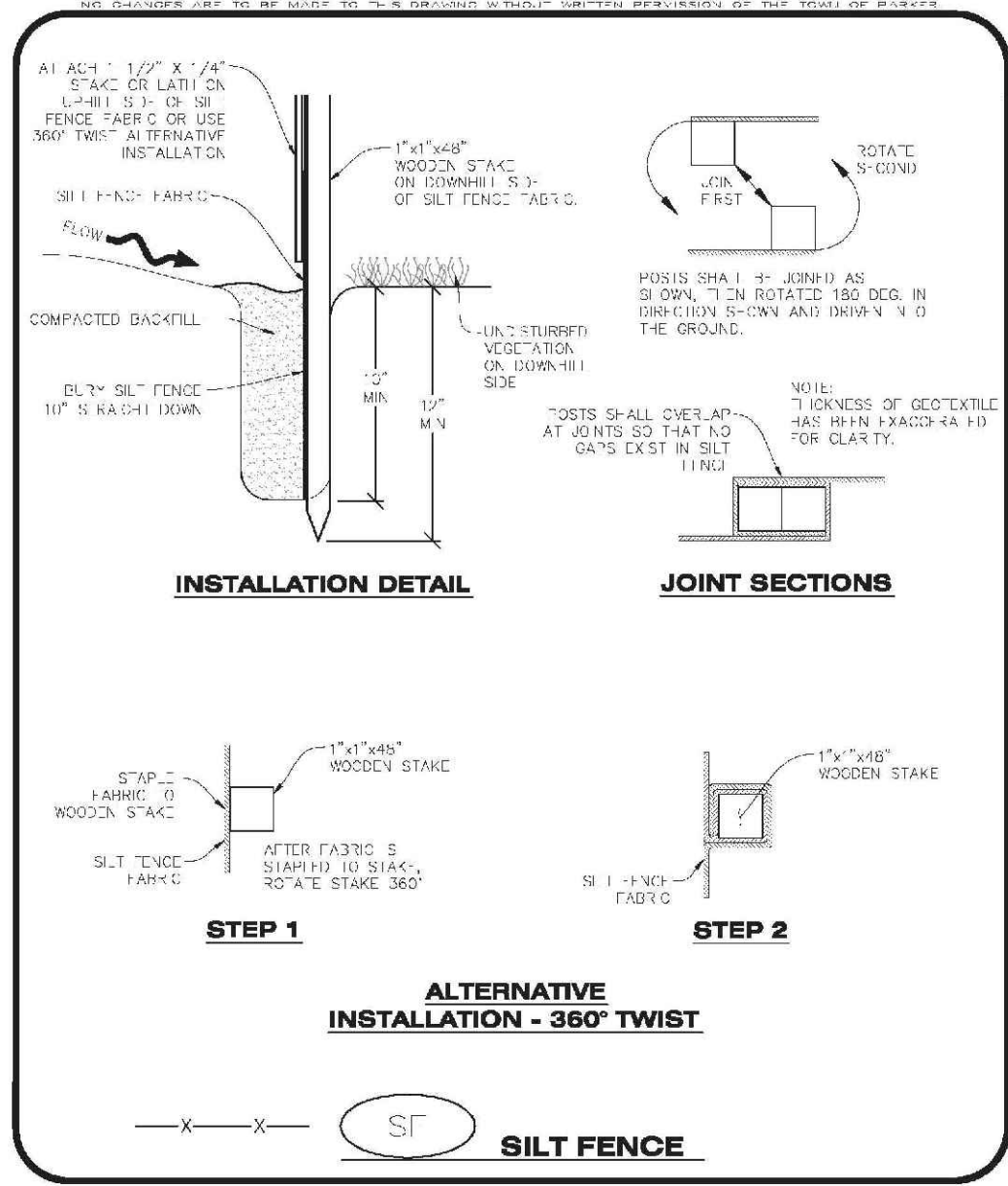
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**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SF 4 OF 4  
REV. 2014



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SF 2 OF 4  
REV. 2014

**SILT FENCE INSTALLATION NOTES**

- SFF BMP PLAN FOR LOCATION(S) OF SILT FENCE.
- ALL SILT FENCE SHALL BE INSTALLED IN GOOD CONDITION AND FREE OF ANY DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- A UNIFORM "O" DEEP ANCHOR TRENCH SHALL BE EXCAVATED USING A TRENCHER.
- A 10" DEEP ANCHOR SILL SHALL BE INSTALLED USING A 5" DIA. SLEND W/PIPE.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UP-HILL SIDE OF THE ANCHOR TRENCH.
- ANCHOR TRENCH SHALL BE GENERALLY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT OF THE SILT FENCE.
- ALL ANCHOR TRENCH SHALL BE THOROUGHLY BACKFILLED WITH SILT THAT IS GENERALLY FREE OF ROCKS AND DEBRIS.
- ALL EXCAVATED MATERIAL SHALL BE PLACED ON THE UP-DRAINING SIDE OF THE SILT FENCE.
- STAKES SHALL BE POSITIONED ON THE DOWN-HILL SIDE OF THE SILT FENCE FABRIC AND PLACED ON TOP OF CURBS OR LESS STAKES SHALL BE EMBEDDED A MINIMUM OF 12" INTO THE GROUND. A WOODEN LATH SHALL BE ATTACHED TO THE DOWN-HILL SIDE OF THE STAKE FOR ADDED STABILITY AND SUPPORT. THE LATH SHALL HAVE THE FOLLOWING DIMENSIONS: 1" X 4" X 4'.
- SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD NOT BE SIGNIFICANT SAGGING ALONG ANY PORTION OF THE SILT FENCE AFTER IT HAS BEEN ANCHORED TO THE STAKES.
- SILT FENCE FABRIC SHALL BE EMBEDDED IN THE STAKES AND LATHS USING STAPLES OR NAILS OF AN APPROPRIATE LENGTH. STAPLES AND NAILS SHOULD BE PLACED ALONG THE LATH TO ENSURE PROPER ATTACHMENT.
- SILT FENCE FABRIC SHALL MEET THE FOLLOWING MANDATORY REQUIREMENTS:

PROPERTIES	TEST METHOD	MANDATORY REQUIREMENTS
CHAR. TENSILE STRENGTH	ASTM D 4242	≥ 124 LBS
MULTI-TENSILE STRENGTH	ASTM D 3786	≥ 300 LBS
TENSILE STRENGTH	ASTM D 4832	≥ 60 LBS
TEAR RESISTANCE	ASTM D 2553	≥ 60 LBS
UV RESISTANCE	ASTM D 2355	≥ 80% AT 500 HOURS OF UV EXPOSURE
LOW RATE	ASTM D 4481	≥ 10 GR / IN <sup>2</sup> / HR

- AN ORIGINAL PHYSICAL SPECIFICATION SHALL SHOW THE SILT FENCE MANUFACTURER'S ALL GC MADE AVAILABLE AT THE REQUEST OF THE TOWN'S INSPECTOR. THE PRODUCT IDENTIFICATION SHALL PROVIDE THE RESULTS OF THE TESTS LISTED ABOVE.
- SILT FENCE JOINTS SHALL BE CONNECTED ACCORDING TO THE ATTACHED DRAWING.
- SILT FENCE THAT IS INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN CONCRETE AND THE SILT FENCE. EROSION CONTROL MEASURING DEVICES BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS GAP IS OCCURRING.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SF 3 OF 4  
REV. 2014

**SILT FENCE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SILT FENCE.
- ACCUMULATED SEDIMENT SHALL BE REMOVED REGULARLY.
- SILT FENCE SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND PROXIMAL SOILS ARE NO LONGER POSSIBLY AS DIRECTLY IMPACTED BY THE TOWN'S INSPECTION.
- SILT FENCE SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR AND/OR DAMAGE.
- WHEN THE SILT FENCE IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SILT FENCE MAY NEED TO BE RELOTTED, SEEDING, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE JULIAN SMC).

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SF 4 OF 4  
REV. 2014

**SEEDING AND MULCHING SHALL BE PERFORMED ACCORDING TO THE ACCOMPANYING DETAIL(S) AND TEXT. NO EXCEPTIONS SHALL BE MADE.**

- SILT PLAN V.V. OR:
  - LOCATION(S) OF SEEDING AND MULCHING
  - TYP. OF SEED MIX
- SEED MIXES MAY CONFORM TO THE TABLE PROVIDED WITH THE SMC NOTES OR ALTERNATIVES MAY BE ALLOWED WITH PRIOR PERMISSION BY THE TOWN'S INSPECTOR.
- SEEDING MAY BE PERFORMED YEAR-ROUND ASSUMING THE SOIL IS NOT FROZEN AND DURING TIMES OF EXTREME TEMPERATURES SHOULD BE AVOIDED.
- IF THE SEEDING OF THE TOWN'S INSPECTION IS NOT COMPLETED BY THE TOWN'S INSPECTOR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETING THE SEEDING AND MULCHING. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEEDING AND MULCHING OF ALL AREAS THAT ARE NOT SEEDING AND MULCHING BY THE TOWN'S INSPECTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEEDING AND MULCHING OF ALL AREAS THAT ARE NOT SEEDING AND MULCHING BY THE TOWN'S INSPECTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEEDING AND MULCHING OF ALL AREAS THAT ARE NOT SEEDING AND MULCHING BY THE TOWN'S INSPECTOR.
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- ALL AREAS NEARBY TO BEING DRILL SEEDING SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING NOTES OR EFFECTIVELY ROUGHENED USING A HARROW OR OTHER SUCH IMPLEMENT. ALL AREAS SHALL BE UNIFORMLY HAND BROADCASTED WITH THE PROPER SEED MIX APPLIED AT THE TIMES THE DRILL SEEDING RATE BROADCASTED AREAS SHALL THEN BE RE-HARROWED OR RE RAKED USING A HAND TYPED RAKE TO ENSURE THAT SEEDS ARE BURIED TO AN APPROXIMATE DEPTH OF 1/2" TO 1".
- AFTER SEEDING HAS BEEN COMPLETED, MULCH SHALL BE UNIFORMLY APPLIED AT A RATE OF 2 TONS/ACRE (2000 LBS/ACRE). MULCH SHALL BE MANUALLY GRADDED TO A DEPTH OF 2" TO 3" USING A DRUMMER. MULCH SHALL BE HAND CRIMPED AND COVERED WITH A TACKLER IN AREAS WHERE MECHANICAL CRIMPING IS NOT POSSIBLE. WHEN SOILS PERMIT, ALL MULCH SHALL BE CRIMPED SUCH THAT THE INDIVIDUAL PIECES OF STAWK FORM A TIGHT MAT OF SHAPES PROTRUDING OUT OF THE GROUND SEVERAL INCHES.
- IN CERTAIN INSTANCES, IT MAY BE NECESSARY TO APPLY A TACKLER IN ORDER TO HELP WITH STAWK CRIMPING. TACKLER SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SMC 1 OF 3  
REV. 2014

**SEEDING AND MULCHING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEEDING AND MULCHING.
- ANY SEEDING AND MULCHING AREAS THAT BECOME DAMAGED SHALL BE REPAIRED WITHIN THE TIME FRAME SPECIFIED BY THE TOWN'S INSPECTOR.

**WEED MANAGEMENT**

- ALL WEEDS SHALL BE REMOVED BY COMMERCIAL SERVICES. APPLICATIONS CONDUCTED BY THE COLORADO DEPARTMENT OF AGRICULTURE AS QUALIFIED APPLICATORS. THE CONTRACTOR SHALL FURNISH DOCUMENTATION OF SUCH WEEDING PRIOR TO HERBICIDE APPLICATION.
- HERBICIDE APPLICATIONS SHALL BE SUCH THAT PLANT GROWTH OUTSIDE THE DESIGNATED TREATMENT AREAS WILL NOT BE DAMAGED. ALL DAMAGE CAUSED BY IMPROPER HERBICIDE APPLICATION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- HERBICIDE SHALL BE APPLIED DURING THE APPROPRIATE SEASONS, WHEN TARGET PLANTS ARE ACTIVELY GROWING.
- IF THE WEEDS ARE NOT CONTROLLED BY HERBICIDES, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SEEDING AND MULCHING OF ALL AREAS THAT ARE NOT SEEDING AND MULCHING BY THE TOWN'S INSPECTOR.
- HERBICIDE TREATMENTS SHALL CONTINUE AT AN APPROPRIATE RATE UNTIL IT IS EVIDENT THAT WEED GROWTH PRESENCE AND GROWTH IS MINIMAL AND MAY BE CONTROLLED THROUGH MOWING AND ANNUAL HERBICIDE APPLICATIONS.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SMC 2 OF 3  
REV. 2014

**TOWN OF PARKER, SEED MIX 1**

20% ARIZONA BLUEGRASS  
10% EASTWIND WHEATGRASS  
10% SLENDER WHEATGRASS  
10% ANNUAL KYRGRASS  
10% BLUEGRASS  
10% BLUEGRASS  
10% CANADIAN BLUEGRASS  
OR BLUEGRASS

**SEEDING RATE:**  
DRILL: 25 LBS/ACRE  
BROADCAST: 50 LBS/ACRE

**TOWN OF PARKER, SEED MIX 2**

20% ARIZONA BLUEGRASS  
10% SODAR STREAMBANK WHEATGRASS  
10% SLENDER WHEATGRASS  
10% BLUEGRASS  
10% BLUEGRASS  
10% CANADIAN BLUEGRASS  
OR BLUEGRASS

**SEEDING RATE:**  
DRILL: 25 LBS/ACRE  
BROADCAST: 50 LBS/ACRE

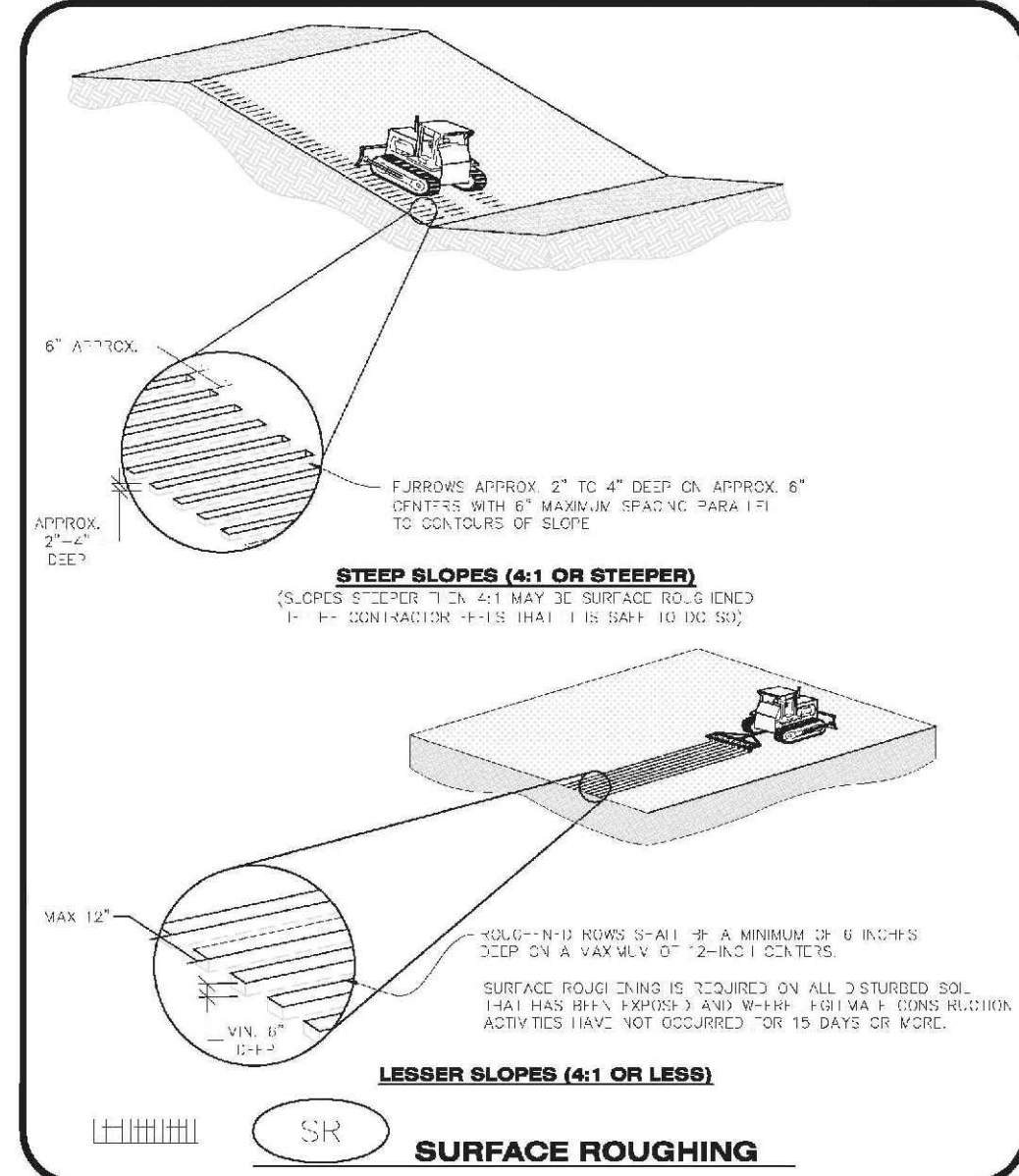
**TOWN OF PARKER, SEED MIX 3 (LOW-GROWTH MIX)**

20% CRITAM CRISTED WHEATGRASS  
20% SLENDER WHEATGRASS  
10% ANNUAL KYRGRASS  
10% BLUEGRASS  
10% BLUEGRASS

**SEEDING RATE:**  
DRILL: 25 LBS/ACRE  
BROADCAST: 50 LBS/ACRE

**SEED MIX 4:**  
OTHER SEED MIXES APPROVED BY THE TOWN OF PARKER

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SMC 3 OF 3  
REV. 2014



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SR 1 OF 2  
REV. 2014

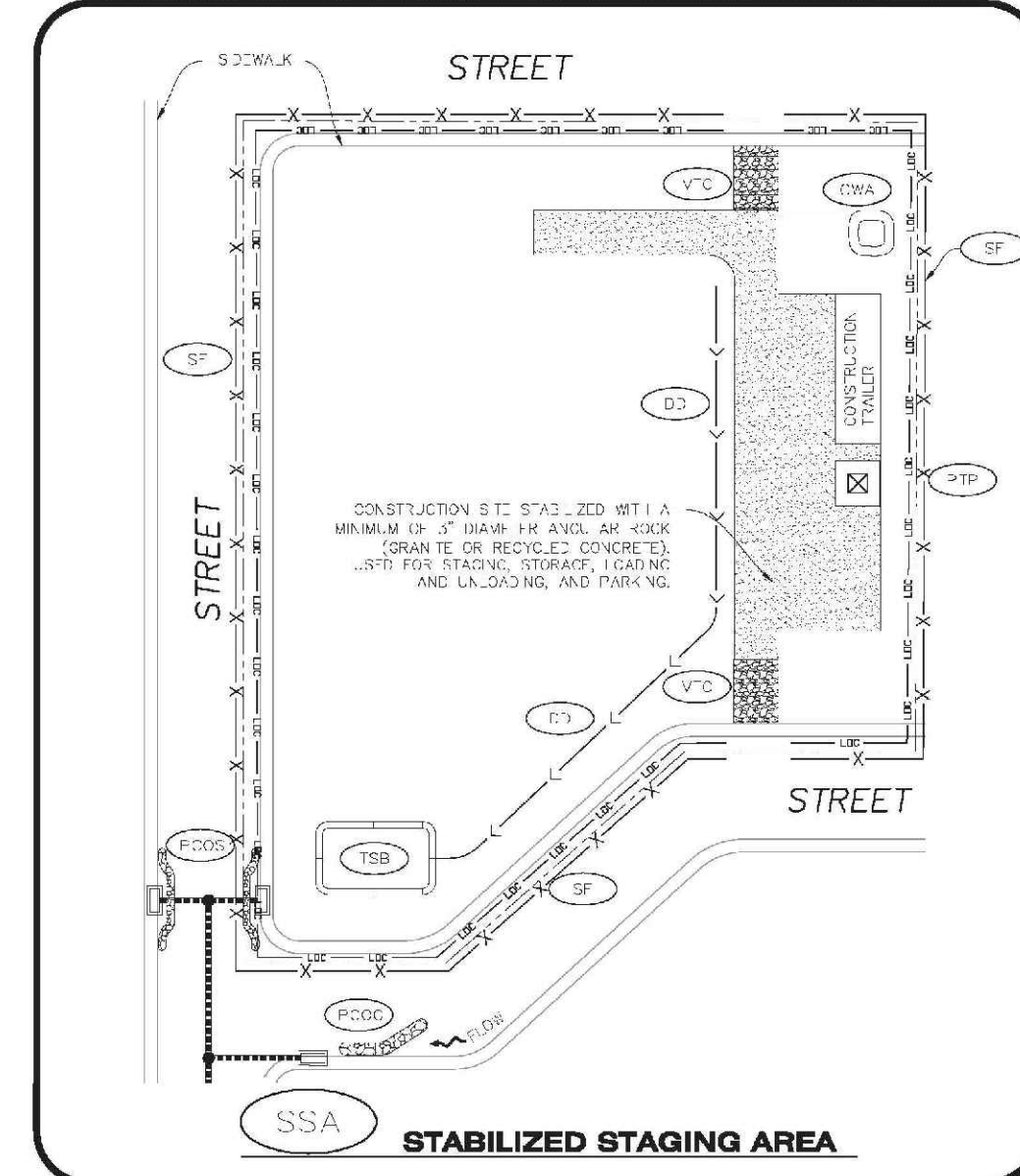
**SURFACE ROUGHENING INSTALLATION NOTES**

- SEE BMP PLAN FOR LOCATION(S) OF SURFACE ROUGHENING.
- DISTURBED AREAS THAT REMAIN INACTIVE FOR 90 DAYS OR MORE MUST RECEIVE SURFACE ROUGHENING OR ANOTHER APPROVED BMP FROM THE TOWN'S INSPECTOR.
- FOR STEEP SLOPES (3% OR STEEPER), IT IS ACCEPTABLE TO "RAKE" THE SLOPES, ACCORDING TO THE BMP PLAN.
- SCHEDULED OR REQUIRED STABILIZATION MAY BE COMPLETED BY THE PERMITEE TO A LOW FOR SPECIAL CONSIDERATIONS SUCH AS STABILIZING ACCESS AREAS AND AREAS IN CLOSE PROXIMITY TO CONTINUING CONSTRUCTION.

**SURFACE ROUGHENING INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SURFACE ROUGHENING.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SR 2 OF 2  
REV. 2014



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SSA 1 OF 2  
REV. 2014

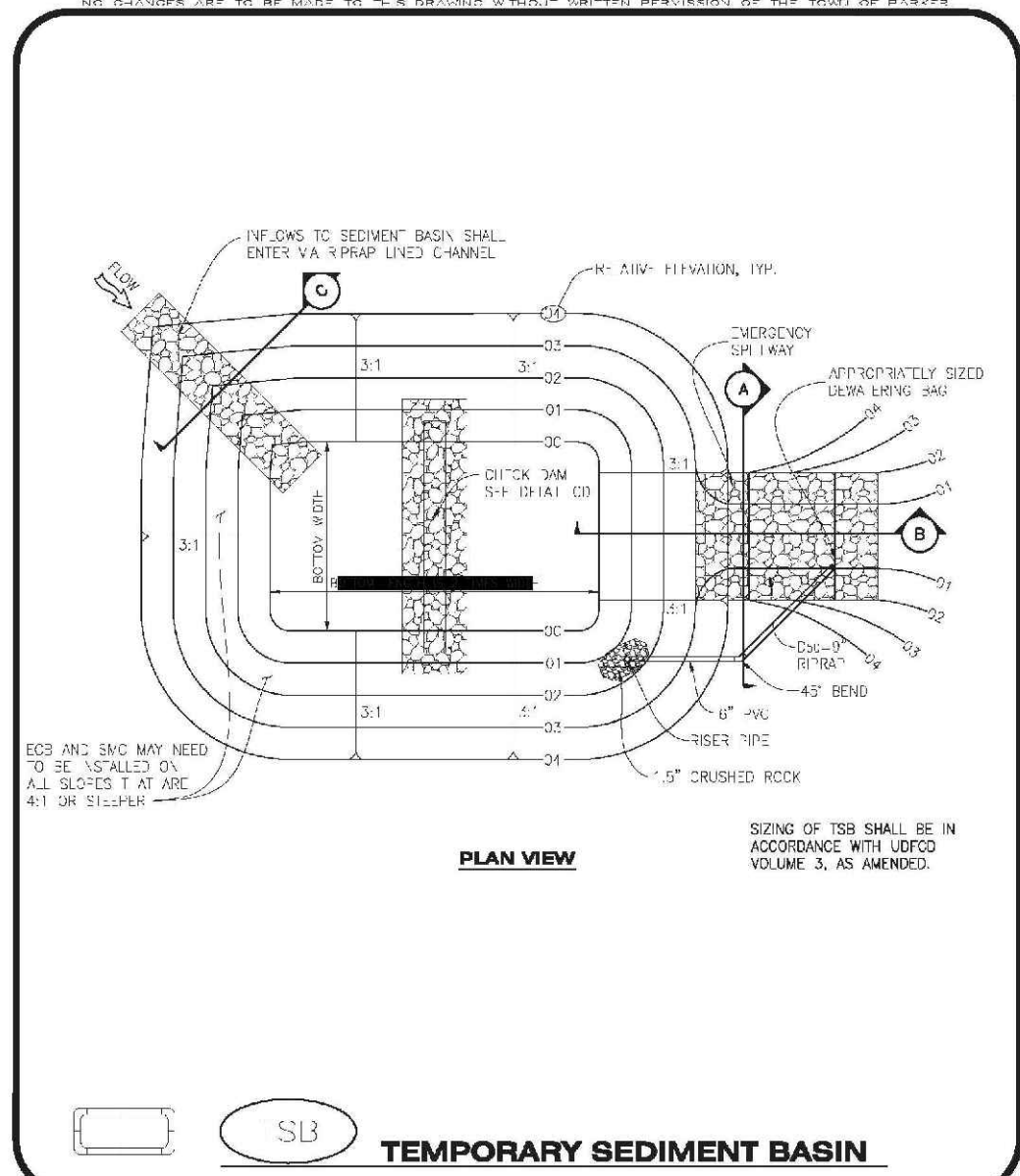
**STABILIZED STAGING AREA INSTALLATION NOTES**

- SFF BMP PLAN FOR LOCATION OF STAGING AREA. CONTRACTOR MAY MODIFY LOCATION AND SIZE OF STABILIZED STAGING AREA WITH TOWN APPROVAL.
- STABILIZED STAGING AREA SHALL BE LARGE ENOUGH TO FULLY CONTAIN PARKING, STORAGE, AND LOAD OPERATIONS.
- THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM OF 3" DIAMETER OF ANGULAR ROCK (GRANITE OR RECYCLED CONCRETE).
- SSA FOR SMALLER SITES MAY NOT BE PRACTICAL IN THESE AND SIMILAR SITUATIONS, VARIATIONS MAY BE PERMITTED BY THE TOWN.

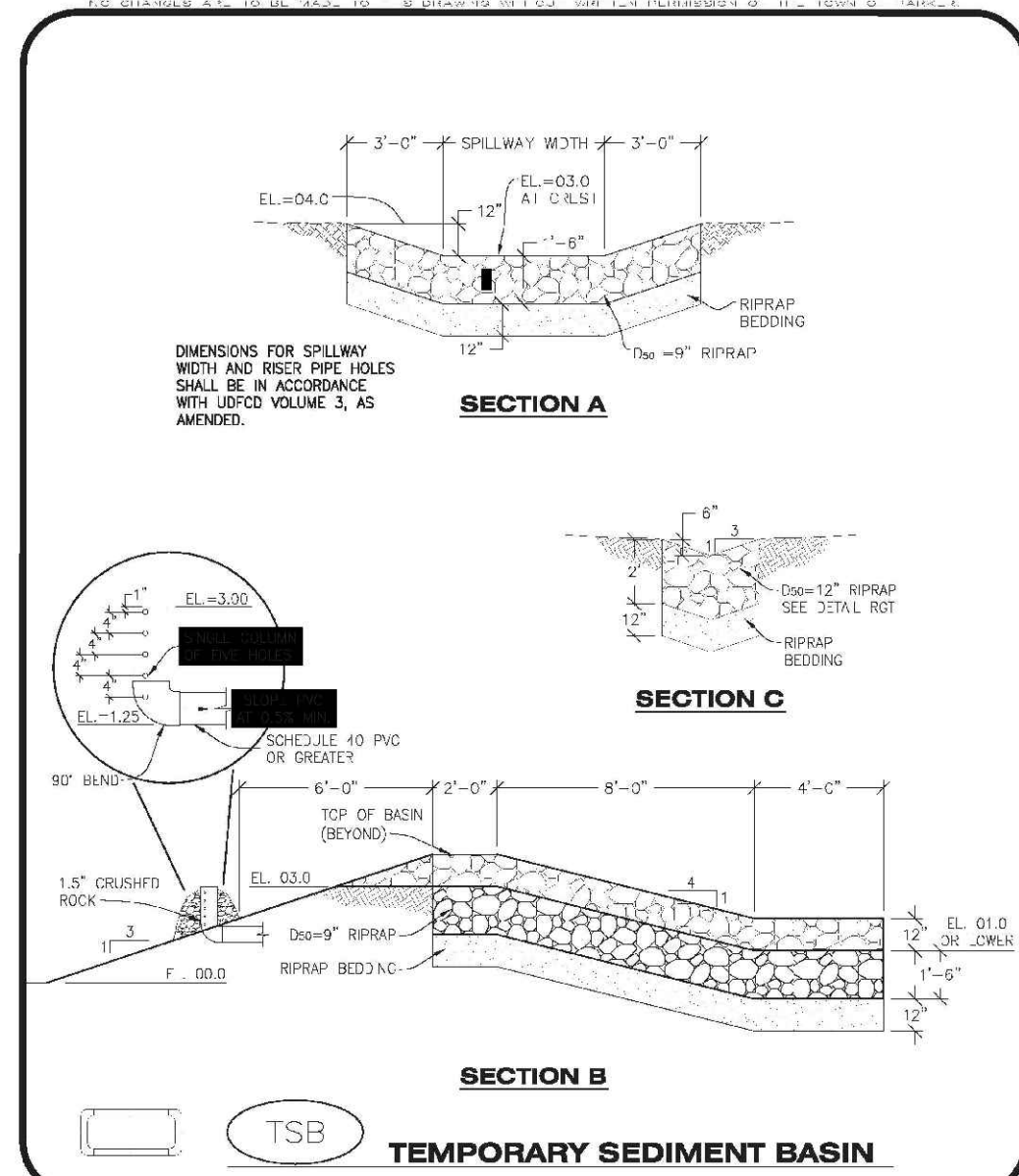
**STABILIZED STAGING AREA INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STAGING AREA.
- STABILIZED STAGING AREA SHALL BE MAINTAINED AS NECESSARY TO CONTAIN PARKING, STORAGE, LOADING, AND UNLOADING.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
SSA 2 OF 2  
REV. 2014



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
TSB 1 OF 3  
REV. 2014



**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
TSB 2 OF 3  
REV. 2014

**TEMPORARY SEDIMENT BASIN INSTALLATION NOTES**

- SFF BMP PLAN FOR LOCATION(S) OF SEDIMENT BASIN(S).
- THE TEMPORARY SEDIMENT BASIN(S) SHALL BE INSTALLED AND FUNCTIONING PRIOR TO ANY OTHER GRADING ACTIVITIES.
- THE SLOPE, DIMENSIONS AND DETAILS OF THE TEMPORARY SEDIMENT BASIN SHALL BE DETERMINED BY THE TOWN'S INSPECTOR, IN ACCORDANCE WITH THE SMC NOTES, AS APPLICABLE.
- EMBANKMENT MATERIAL SHALL CONSIST OF SOIL FREE OF DEBRIS, ORGANIC MATERIAL, AND ROCKS OF CONCRETE OR OTHER MATERIALS. A MINIMUM OF 10% BY WEIGHT PASSING THE NO. 200 SIEVE.
- EMBANKMENT MATERIAL SHALL BE COMPACTED TO A MINIMUM OF 90% DENSITY, AND WITHIN +/- 2% OF OPTIMUM MOISTURE IN ACCORDANCE WITH ASTM D698.
- AN APPROXIMATELY 1/2" DIA. LIME BAG SHALL BE PLACED AT THE END OF THE DISCHARGE PIPE. THE BATTERING RAG SHALL BE REPAIRED DURING STOPPAGE ACCUMULATION RACHES EDGE.

**TEMPORARY SEDIMENT BASIN INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE TEMPORARY SEDIMENT BASIN.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT REACHES A DEPTH OF 2.0', OR WITHIN 2.0' OF THE SPILLWAY CREST, OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTION.
- SEDIMENT BASINS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND PROXIMAL SOILS ARE NO LONGER POSSIBLY AS DIRECTLY IMPACTED BY THE TOWN'S INSPECTION.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
TSB 3 OF 3  
REV. 2014

10338 E. Dry Creek Rd.  
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**CVL CONSULTANTS**

HR 935 LLC  
7353 South Alton Way  
CENTENNIAL, CO 80112

TRAILS AT CROWFOOT  
FILING 13 CONSTRUCTION DRAWINGS  
BMP DETAILS

SCALE: AS SHOWN  
DRAWN BY: JF  
CHECKED BY: JJ  
DATE: SEPTEMBER 2017

FILE NO: 8130283701

SHEET NUMBER 9

No.	Revisions	Date	Appr.	Date

N:\PROJECTS\BIRCH-CREEK-ENGINEERING\SHEET SET\CD\SHEET 13\TOWN OF PARKER\12 BMP DETAILS.DWG, BRUNAK, 11/20/18 7:54 PM

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CD CHECK DAM**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CD** 1 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CD CHECK DAM**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CD** 2 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**CHECK DAM INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF CHECK DAMS.
- CHECK DAMS SHOWN ON CBMP PLAN SHALL BE INSTALLED WHEN DIRECTED BY THE TOWN'S INSPECTOR.
- RIPRAP UTILIZED FOR CHECK DAMS SHALL HAVE A D50 MEDIAN STONE SIZE OF 12".
- RIPRAP PAD SHALL BE TRENCHED INTO THE GROUND A MINIMUM OF 1'-8".
- THE MAXIMUM SPACING BETWEEN CHECK DAMS SHOULD BE SUCH THAT THE BOTTOM OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM AS SHOWN IN THE DETAIL.

**CHECK DAM INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CHECK DAMS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE CREST OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- CHECK DAMS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- WHEN CHECK DAMS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED TOPSOIL AND ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CHECK DAMS BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMO).
- IN SOME INSTANCES, CHECK DAMS MAY REMAIN IN PLACE PERMANENTLY.

**CD CHECK DAM**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **CD** 3 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**D DEWATERING**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **D** 1 OF 2 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**DEWATERING INSTALLATION NOTES**

- IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

**DEWATERING MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

**D DEWATERING**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **D** 2 OF 2 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**DD DIVERSION DITCH**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DD** 1 OF 2 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**DIVERSION DITCH INSTALLATION NOTES**

- SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
- A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
- ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
- THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
- THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

**DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
- DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
- DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

**DD DIVERSION DITCH**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **DD** 2 OF 2 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**STP SIDEWALK TRANSITION PROTECTION (AT INTERSECTION)**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **STP** 1 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**STP SIDEWALK TRANSITION PROTECTION (ALONG STRAIGHT ROADWAY)**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **STP** 2 OF 3 Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SIDEWALK TRANSITION PROTECTION INSTALLATION NOTES**

- SEE PLAN VIEW FOR LOCATION(S) OF SIDEWALK TRANSITION PROTECTION.
- ROCK SOCK SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL RS).
- SILT FENCE SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL SF).
- CONSTRUCTION FENCE SHALL BE CONSTRUCTED ACCORDING TO THE DETAIL (SEE DETAIL CF).
- SEDIMENT CONTROL LOGS MAY BE USED IN LIEU OF SILT FENCE WITH PRIOR APPROVAL FROM THE TOWN.

**SIDEWALK TRANSITION PROTECTION INSPECTION & MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SIDEWALK TRANSITION INSPECTION.

**STP SIDEWALK TRANSITION PROTECTION**

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES **STP** 3 OF 3 Oct. 2013

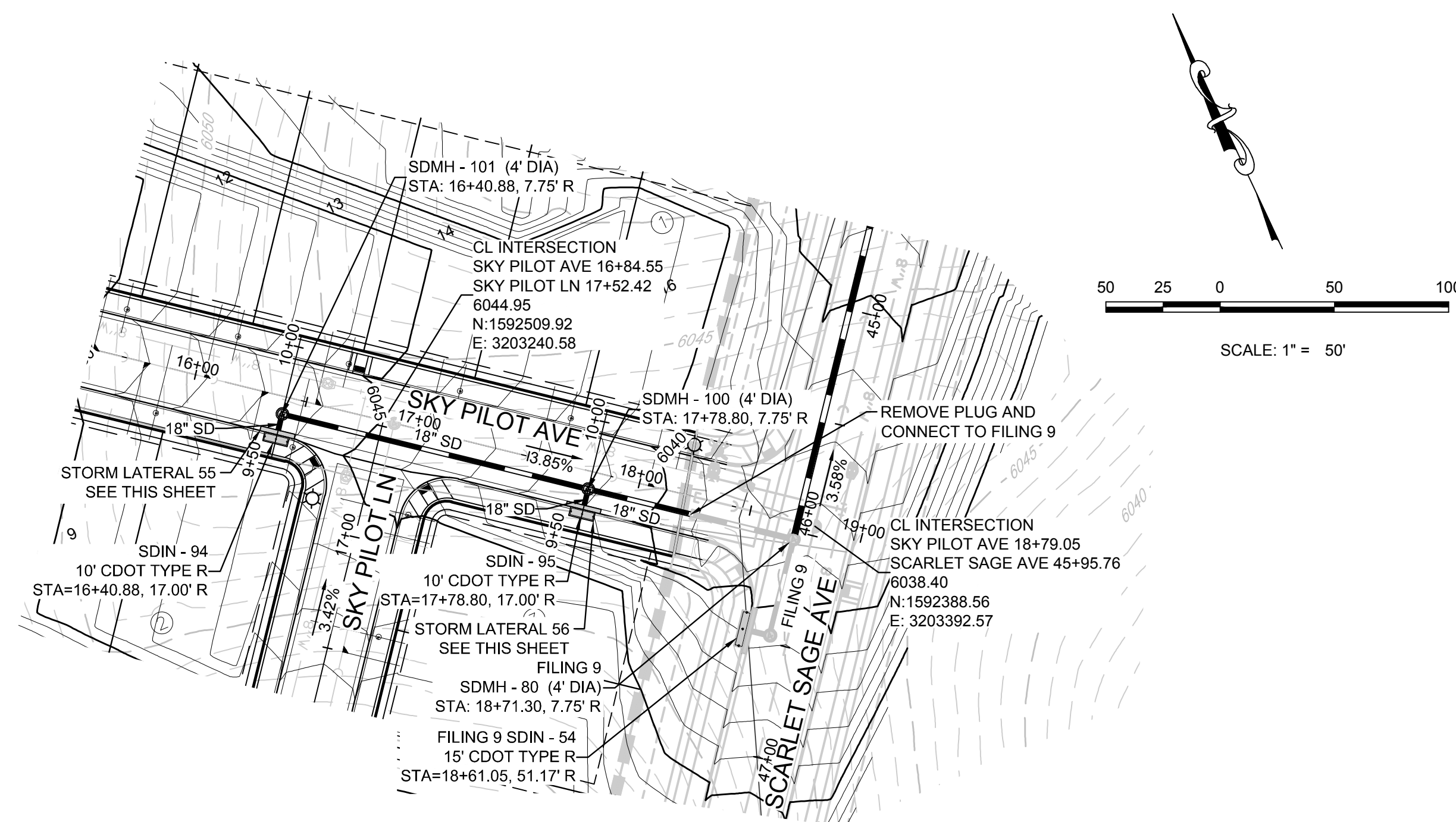
SHEET NUMBER	DRAWN BY: JF	SCALE: AS SHOWN	FILE NO: 8130283701	DATE: SEPTEMBER 2017	Revisions	Appr.	Date
				CHECKED BY: JJ	INIT.		
10	TRAILS AT CROWFOOT FILING 13 CONSTRUCTION DRAWINGS BMP DETAILS			10333 E. Dry Creek Rd. Suite 110 Englewood, CO 80110 Tel: (720) 482-9526 Fax: (720) 482-9546			
	HR 935 LLC 7353 South Alton Way CENTENNIAL, CO 80112			<b>CVL</b> CONSULTANTS			



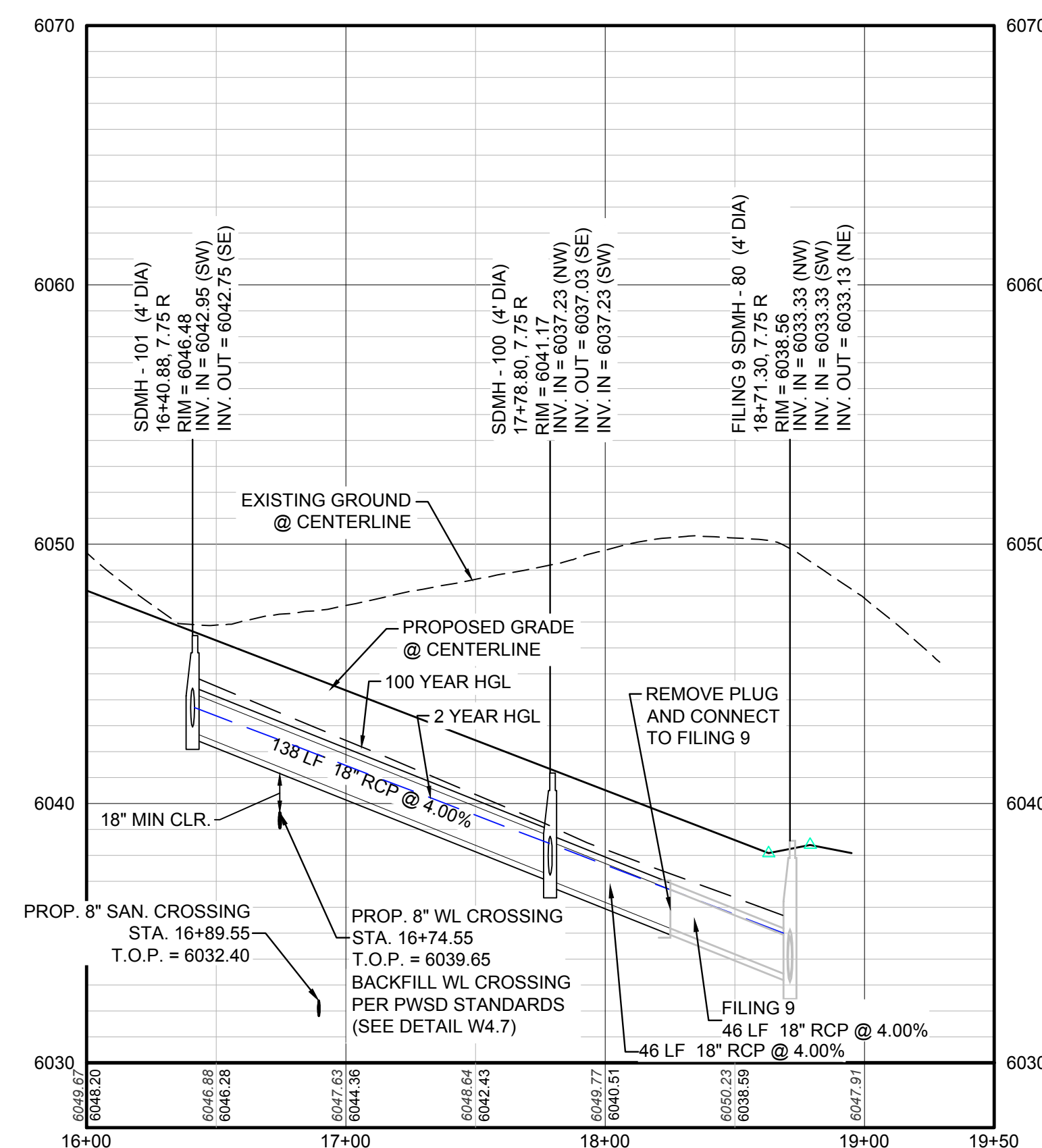






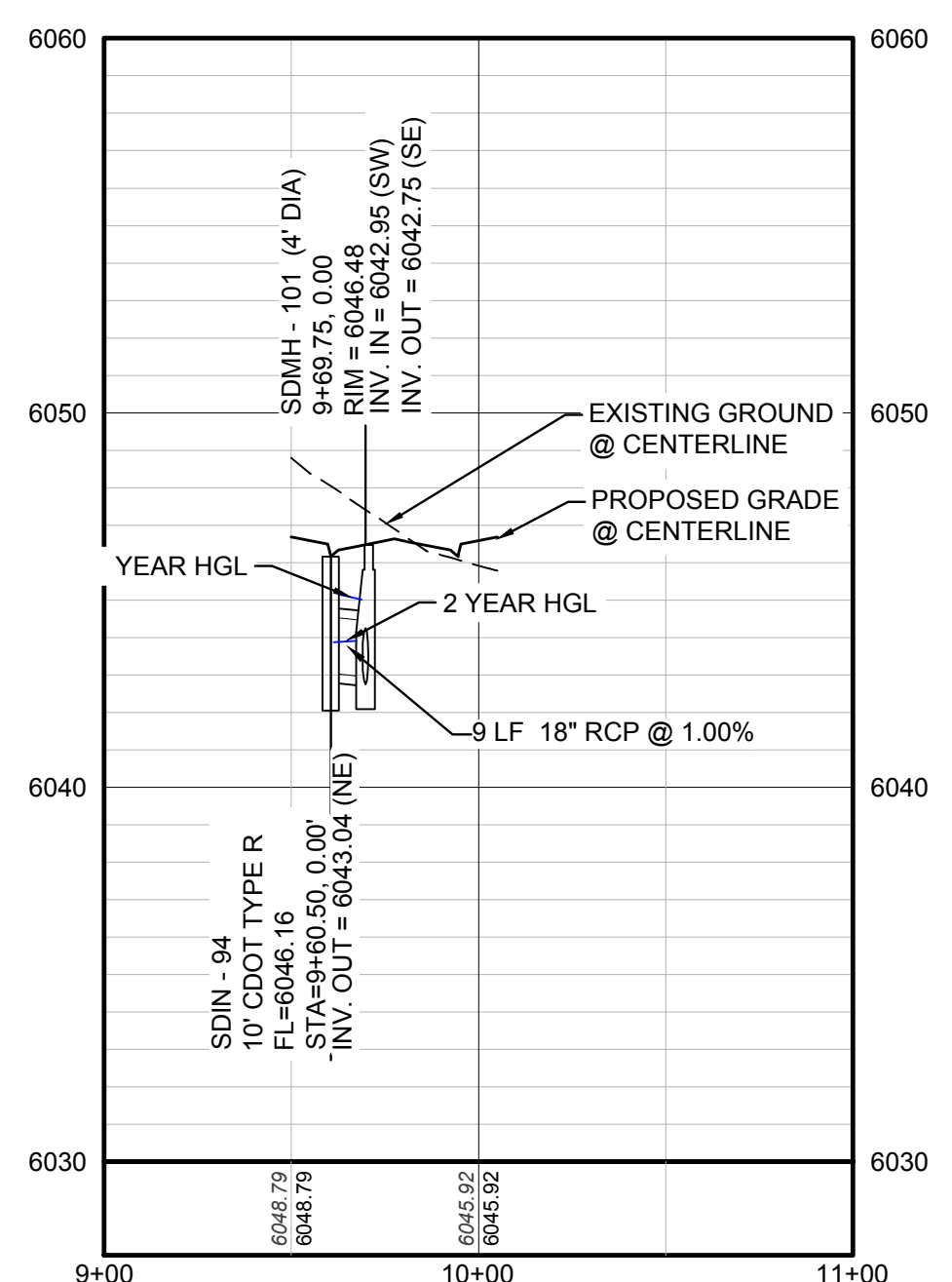


PLAN: SKY PILOT AVE STA: 16+00.00 TO 19+50.00  
HORIZONTAL SCALE: 1" = 50'



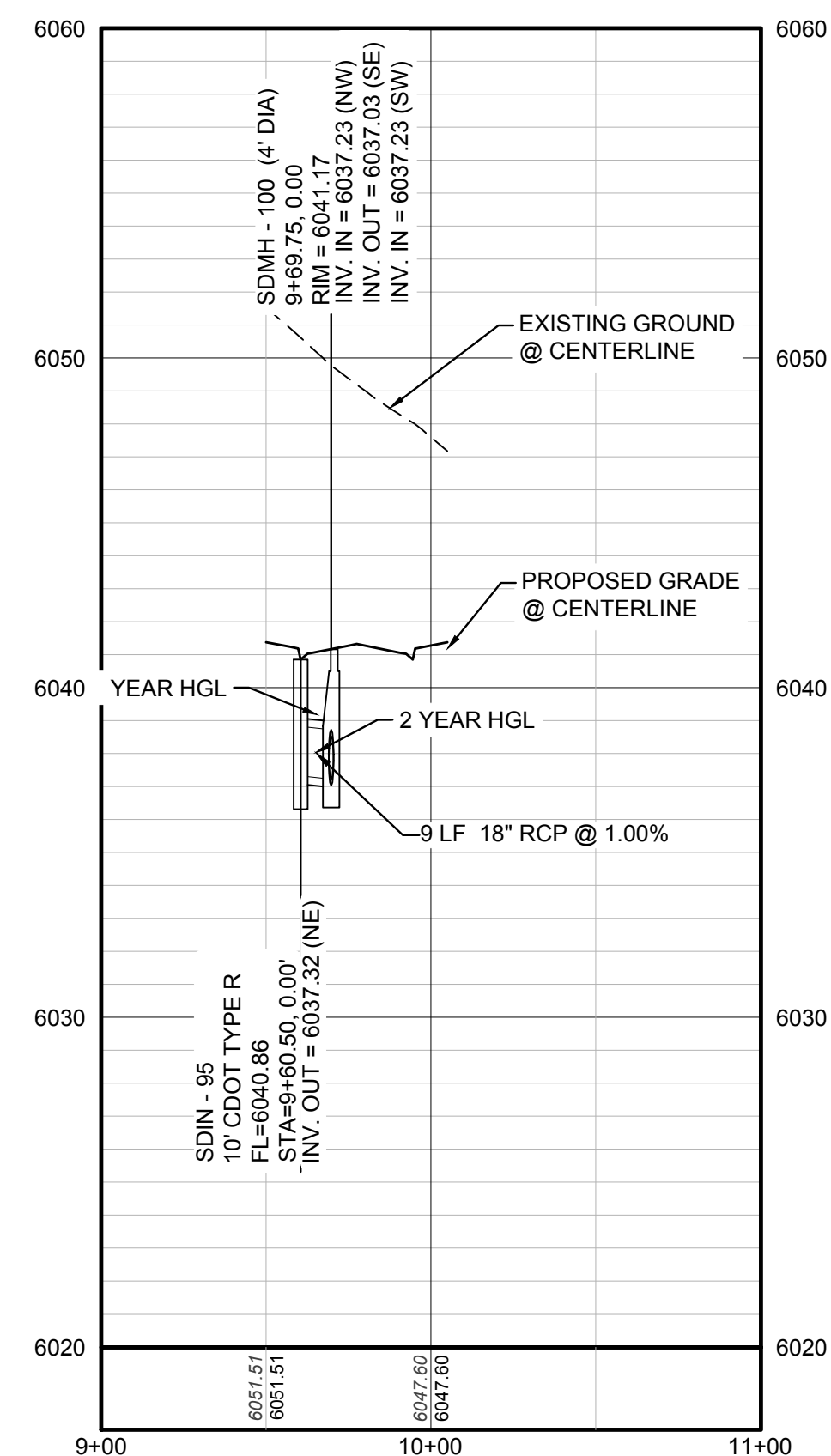
PROFILE: SKY PILOT AVE STA: 16+00.00 TO 19+50.00  
HORIZONTAL: 1" = 50'

STORM LATERAL 55 - SEE THIS SHEET

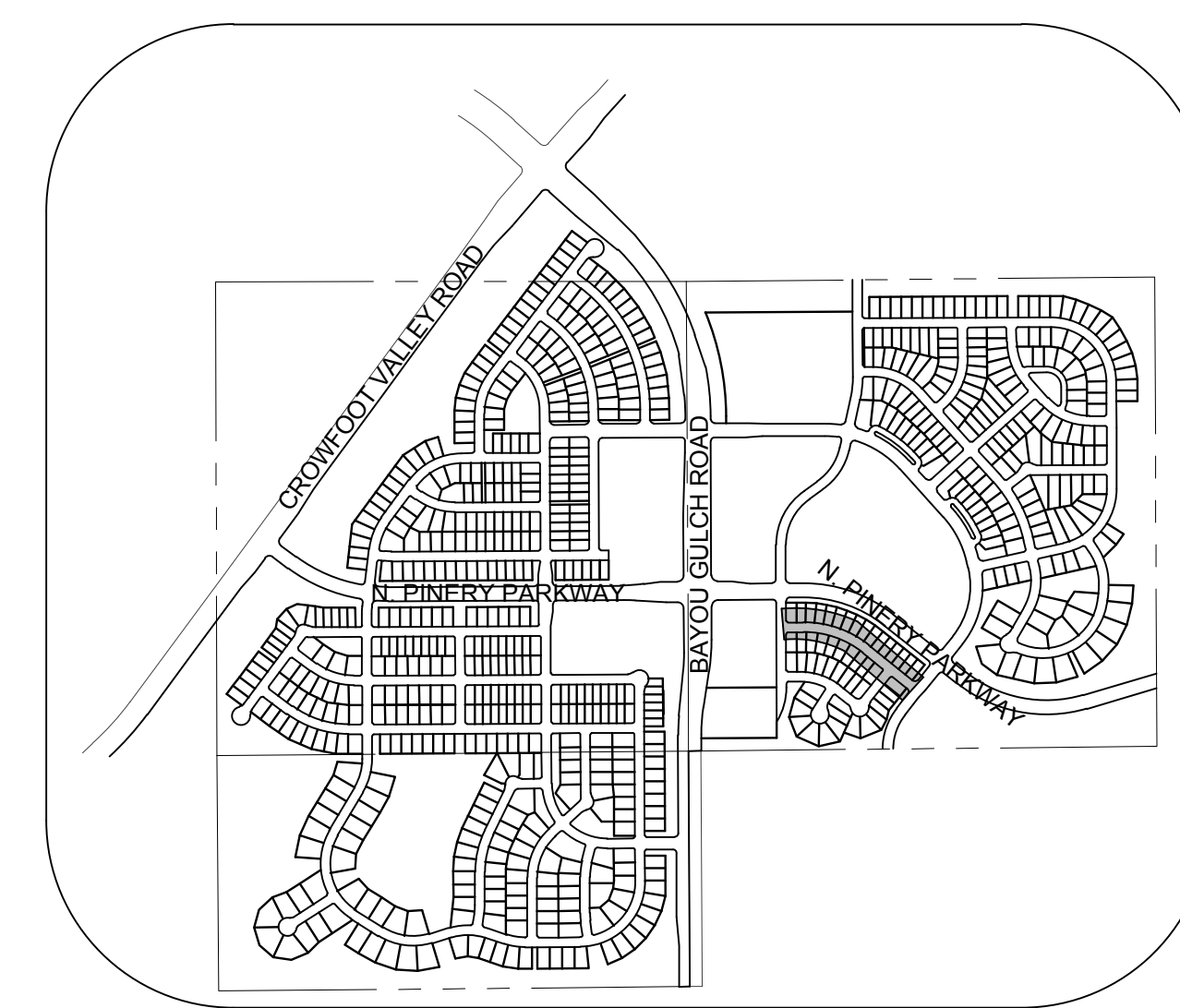


PROFILE: STORM LATERAL 55 STA: 9+00.00 TO 11+00.00  
HORIZONTAL: 1" = 50'  
VERTICAL: 1" = 5'

STORM LATERAL 56 - SEE THIS SHEET



PROFILE: STORM LATERAL 56 STA: 9+00.00 TO 11+00.00  
HORIZONTAL: 1" = 50'  
VERTICAL: 1" = 5'



KEYMAP  
N.T.S.  
LEGEND

②	BLOCK NUMBER	△	PROPOSED RANGE POINT
Ⓐ	LOT TYPE	---	CENTERLINE
6	LOT NUMBER	---	RIGHT-OF-WAY
⊕	PROPOSED BUTTERFLY VALVE	---	PROPERTY LINE
⌋	PROPOSED CAP WITH END OF LINE BLOWOFF	---	EDGE OF PAVEMENT
⊘	PROPOSED REDUCER	→	PROPOSED DIRECTION OF FLOW
⊕	PROPOSED VALVE	1.0%	PROPOSED SLOPE & DIRECTION
⊕	PROPOSED FIRE HYDRANT	---	EXISTING 5' CONTOUR
⊕	PROPOSED WL FITTING WITH THRUST BLOCK	---	EXISTING 1' CONTOUR
⊕	PROPOSED FLARED END SECTION	---	PROPOSED 5' CONTOUR
⊕	PROPOSED LOW POINT BLOW-OFF	---	PROPOSED 1' CONTOUR
⊕	PROPOSED AIR VALVE	---	PROPOSED STORM DRAIN
⊕	FUTURE PHASE VALVE	---	PROPOSED SEWER LINE WITH MANHOLE
⊕	FUTURE FIRE HYDRANT	---	PROPOSED SEWER LATERAL
⊕	PROPOSED LIGHT POLE	---	PROPOSED WATER LINE
⊕	PROPOSED SIDEWALK	---	PROPOSED WATER LATERAL W/ METER
⊕	PROPOSED SIDEWALK RAMP	---	SECTION LINE
10.00	EXISTING ELEVATION	---	FILING BOUNDARY
10.00	PROPOSED DESIGN ELEVATION	---	EXISTING FIBER OPTIC LINE
⊕	PROPOSED STORM DRAIN INLET	---	EXISTING OVERHEAD POWER
⊕	PROPOSED STORM DRAIN MANHOLE	---	EXISTING TELEPHONE LINE

ABBREVIATIONS

AD	ANGLE DIFFERENCE	MH	MANHOLE
AV	AIR VAC RELEASE VALVE	N.T.S.	NOT TO SCALE
BVC	BEGIN VERTICAL CURVE	PVC	POLYVINYL CHLORIDE
BVP	BEGIN VERTICAL PROFILE	PVI	PT. OF VERTICAL INTERSECTION
CBO	CAP WITH BLOW OFF	PVT	POINT OF VERTICAL TANGENT
CRR	CURB RETURN RADIUS	RCBC	REINFORCED CONCRETE BOX CULVERT
ELEVATION	ELEVATION	RCP	REINFORCED CONCRETE PIPE
EP	EDGE OF PAVEMENT	ROW	RIGHT OF WAY
EVC	END VERTICAL CURVE	SC	SANITARY CAP
EVP	END VERTICAL PROFILE	SD	STORM DRAIN
FG	FINISHED GROUND	SL	SECTION LINE
FL	FLOW LINE	SS	SANITARY SEWER
GV	GATE VALVE	STA	STATION
HA	HYDRANT ASSEMBLY	T.O.P.	TOP OF PIPE
HCR	HANDICAP CURB RAMP	UE	UTILITY EASEMENT
HP	HIGH POINT	VC	VERTICAL CURVE
INV	INVERT	WL	WATER LINE
K	CURVATURE COEFFICIENT	WLC	WATER LINE CONNECTION
LF	LINEAR FEET	WSE	WATER SURFACE ELEVATION

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES. SUBJECT TO THESE PLANS BEING STAMPED, SIGN, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

TOWN OF PARKER, DIRECTOR OF ENGINEERING DATE

NO.	REVISIONS	DATE	INIT.	APPR.	DATE

10338 E. Dry Creek Rd.  
Suite 410  
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Fax: (720) 482-9548

**CVL**  
CONSULTANTS

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7353 South Alton Way  
CENTENNIAL, CO 80112

TRAILS AT CROWFOOT  
PRELIMINARY CONSTRUCTION DRAWINGS  
STORM PLAN & PROFILE  
SKY PILOT AVENUE

SCALE: AS SHOWN  
FILE NO: 8130283701  
DRAWN BY: JF  
CHECKED BY: JU  
DATE: OCTOBER 2017

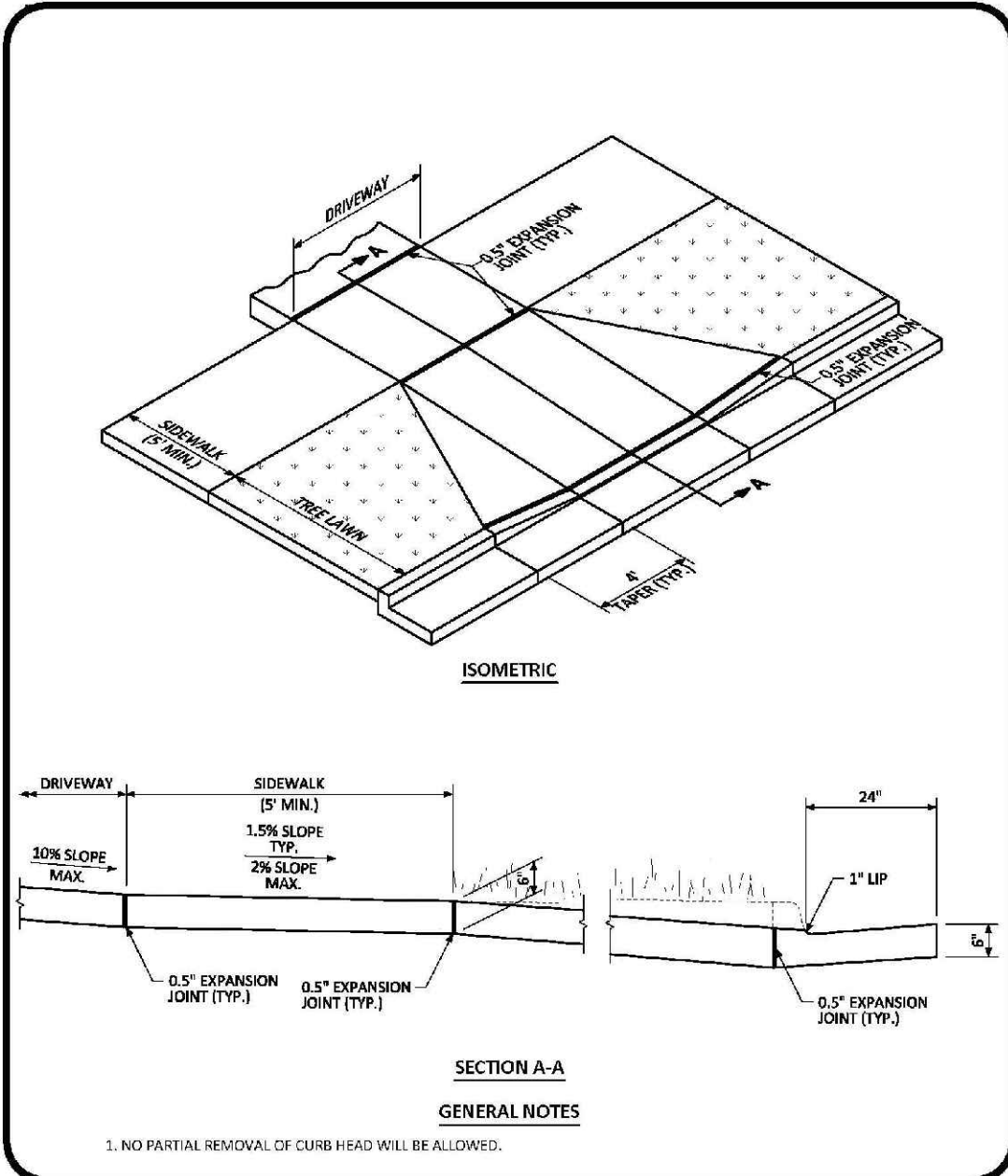
SHEET NUMBER **15**

CALL 811  
TWO WORKING DAYS  
BEFORE YOU DIG  
UNCC 1-800-922-1987

**BENCHMARK**  
THE DOUGLAS COUNTY CONTROL POINT KNOWN AS 1.069032, BEING A 3-1/4" ALUMINUM CAP, BEING LOCATED IN THE SOUTHWEST QUARTER OF SECTION 33, TOWNSHIP 6 SOUTH, RANGE 66 WEST OF THE SIXTH PRINCIPAL MERIDIAN, HAVING A PUBLISHED ELEVATION OF 1799.2870 METERS (5903.13 FEET) NAVD '88 DATUM.

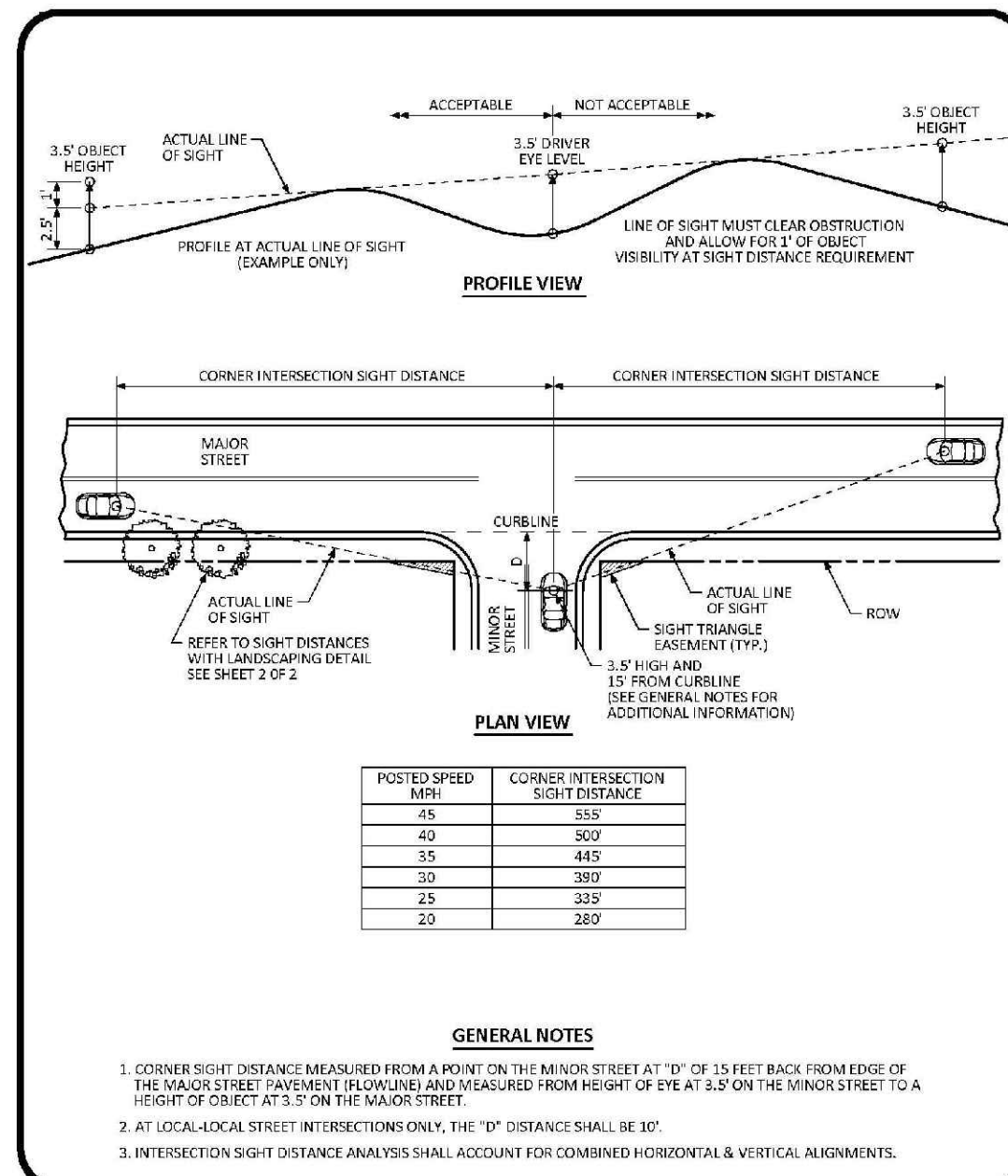
**BASIS OF BEARINGS:**  
THE EAST LINE OF THE NORTHEAST QUARTER OF SAID SECTION 9 BEING MONUMENTED AT THE NORTHEAST CORNER OF SAID SECTION 9 BY A 3-1/4" ALUMINUM CAP STAMPED LS 23053 AND AT THE EAST QUARTER CORNER OF SAID SECTION 9 BY A 2-1/2" ALUMINUM CAP STAMPED LS 6935 BEING CONSIDERED TO BEAR SOUTH 00°15'06" EAST, 2648.70 FEET.





**RESIDENTIAL DRIVEWAY WITH 6" VERTICAL CURB & GUTTER LAYOUT STANDARD DETAIL**

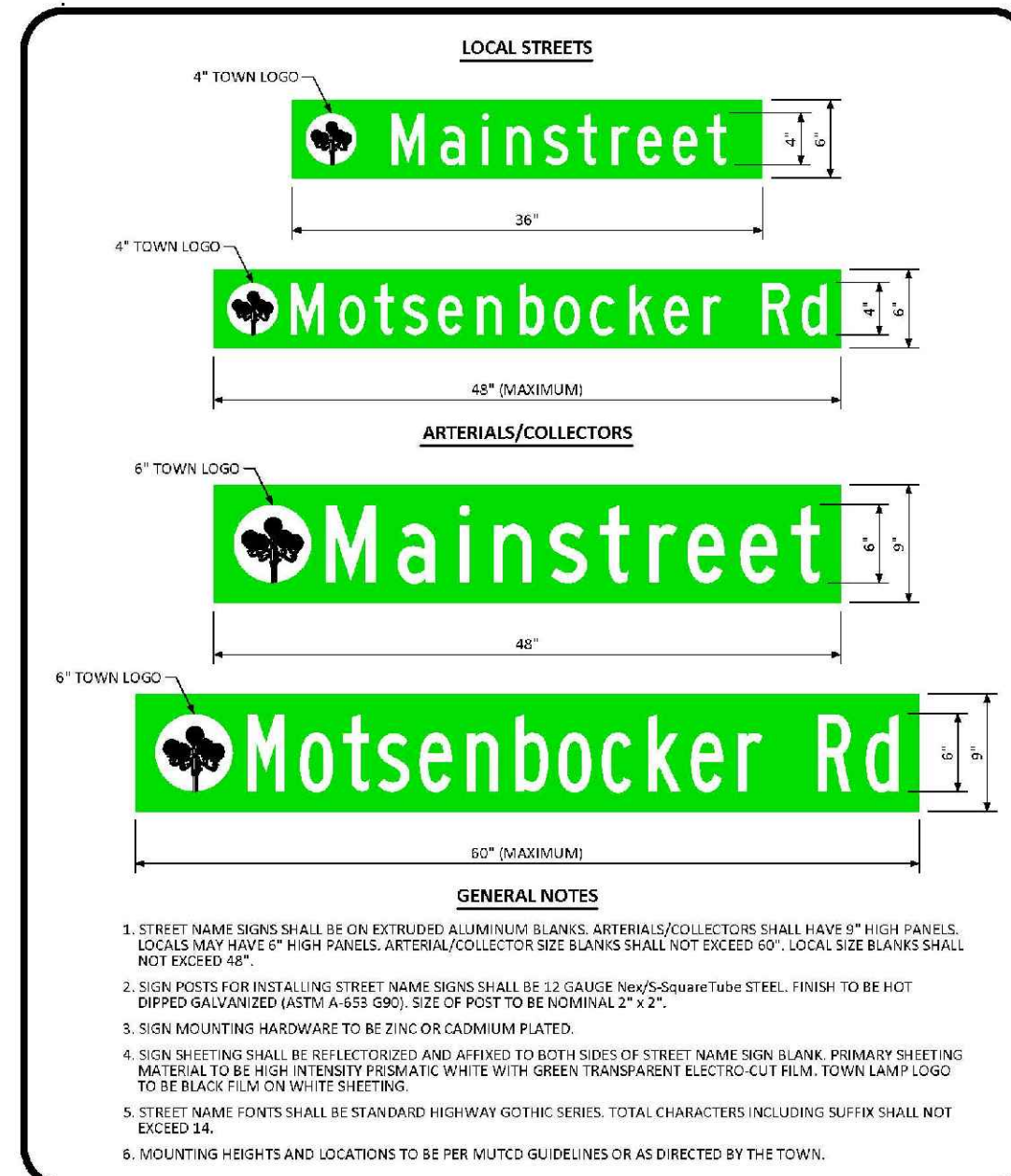
DATE: AUGUST 2014  
 DETAIL: 23  
 1 OF 1



**INTERSECTION SIGHT DISTANCE REQUIREMENTS STANDARD DETAIL**

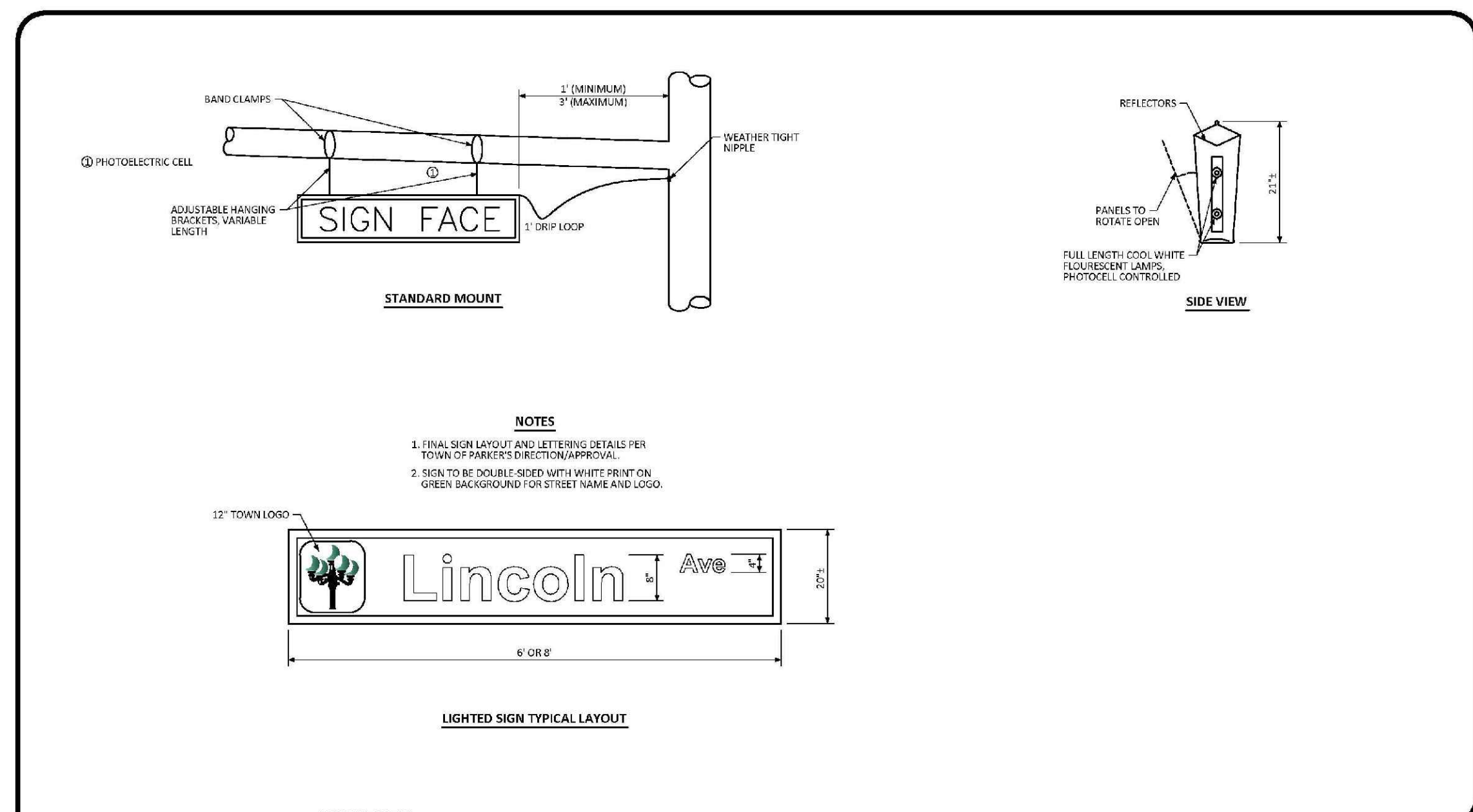
POSTED SPEED (MPS)	CORNER INTERSECTION SIGHT DISTANCE
45	353
40	309
35	265
30	221
25	177
20	133

DATE: AUGUST 2014  
 DETAIL: 25  
 1 OF 1



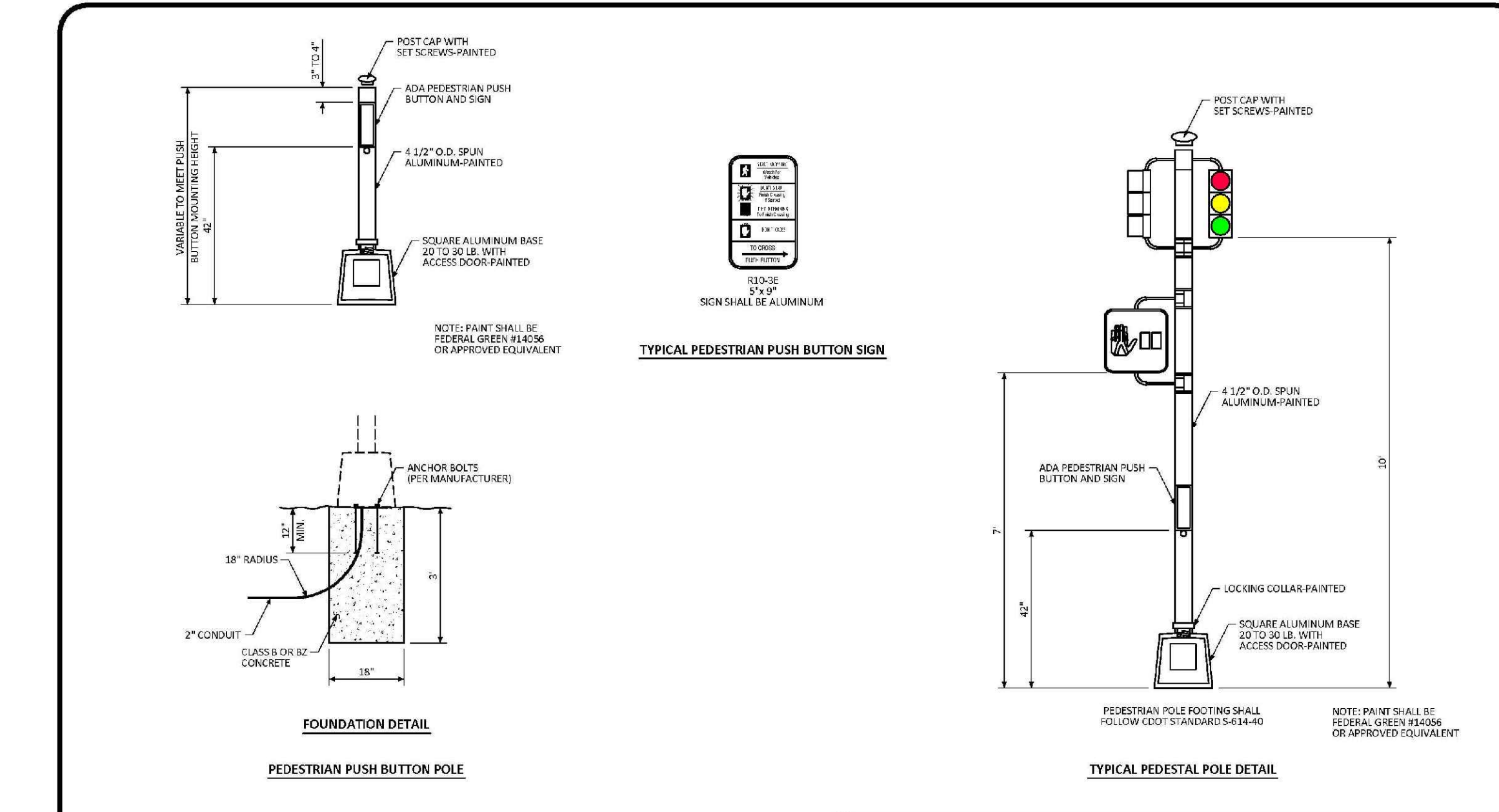
**GROUND MOUNTED STREET NAME PANELS**

DATE: DECEMBER 2016  
 DETAIL: 32  
 1 OF 1



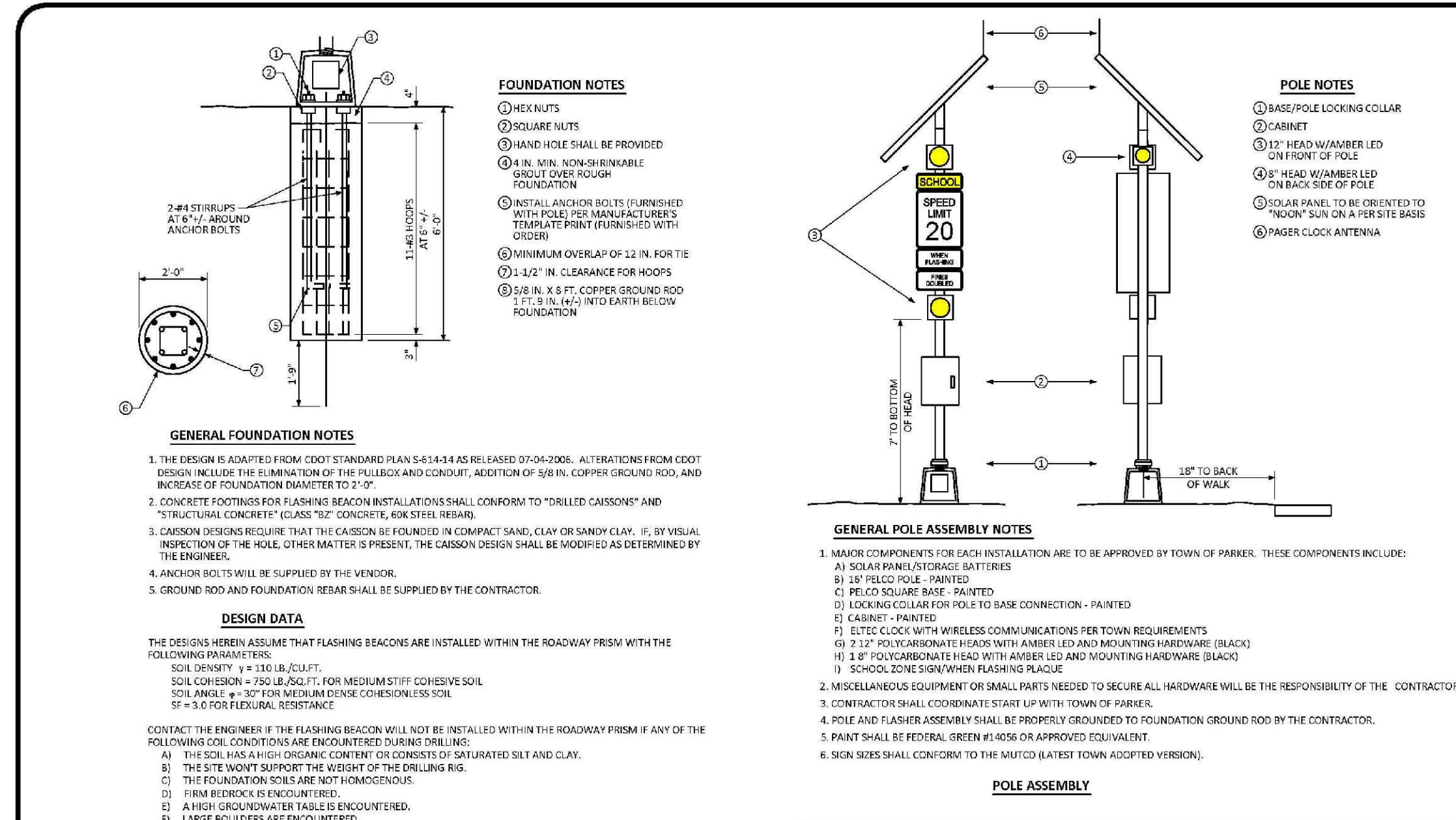
**ILLUMINATED STREET NAME SIGN STANDARD SIGNAL DETAIL**

DATE: NOVEMBER 2012  
 DETAIL: 3  
 1 OF 1



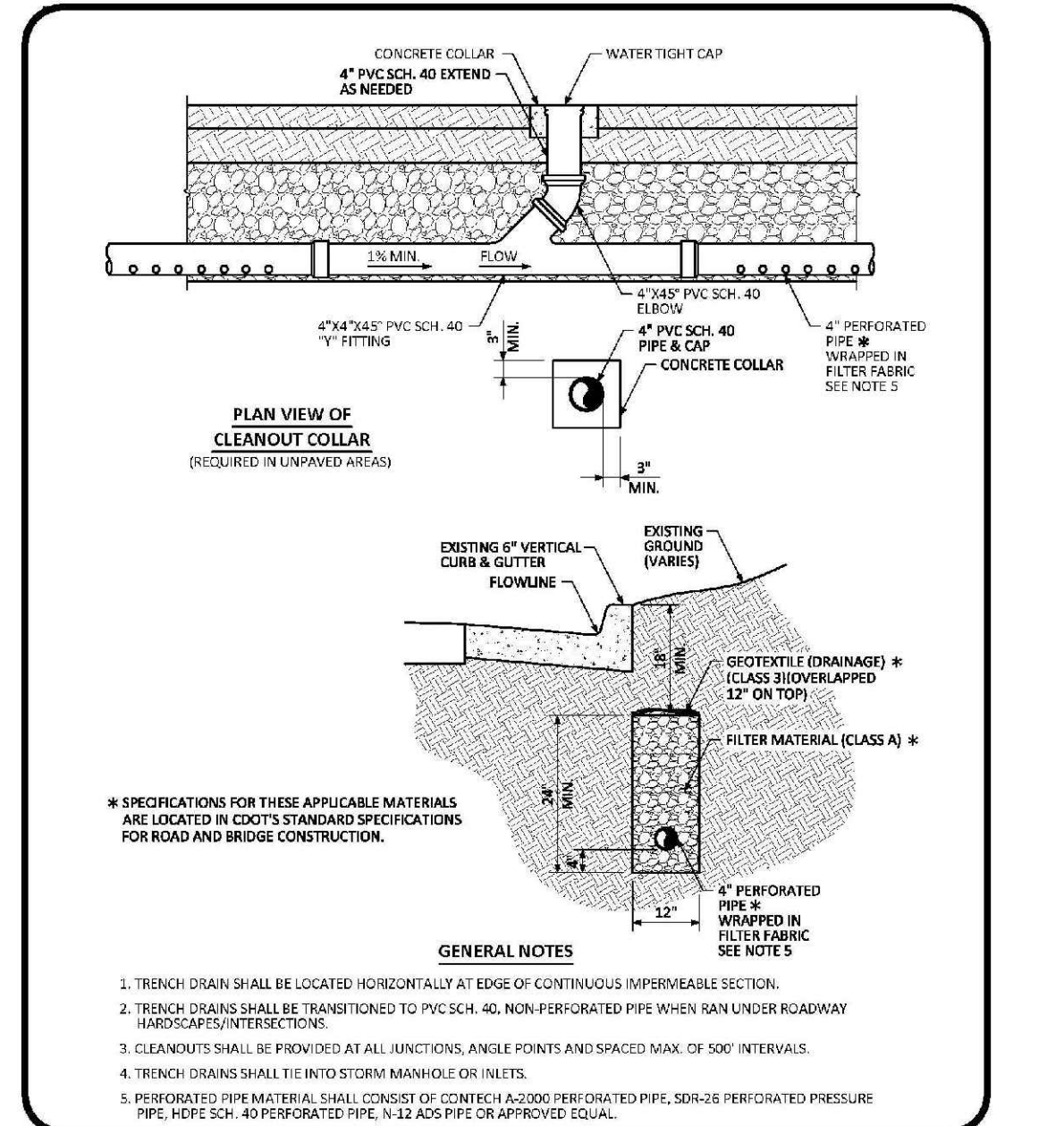
**PEDESTRIAN POLE STANDARD SIGNAL DETAIL**

DATE: NOVEMBER 2012  
 DETAIL: 4  
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**SCHOOL FLASHING BEACON STANDARD SIGNAL DETAIL**

DATE: NOVEMBER 2012  
 DETAIL: 6  
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**TRENCH DRAIN DETAIL SECTION STANDARD DETAIL**

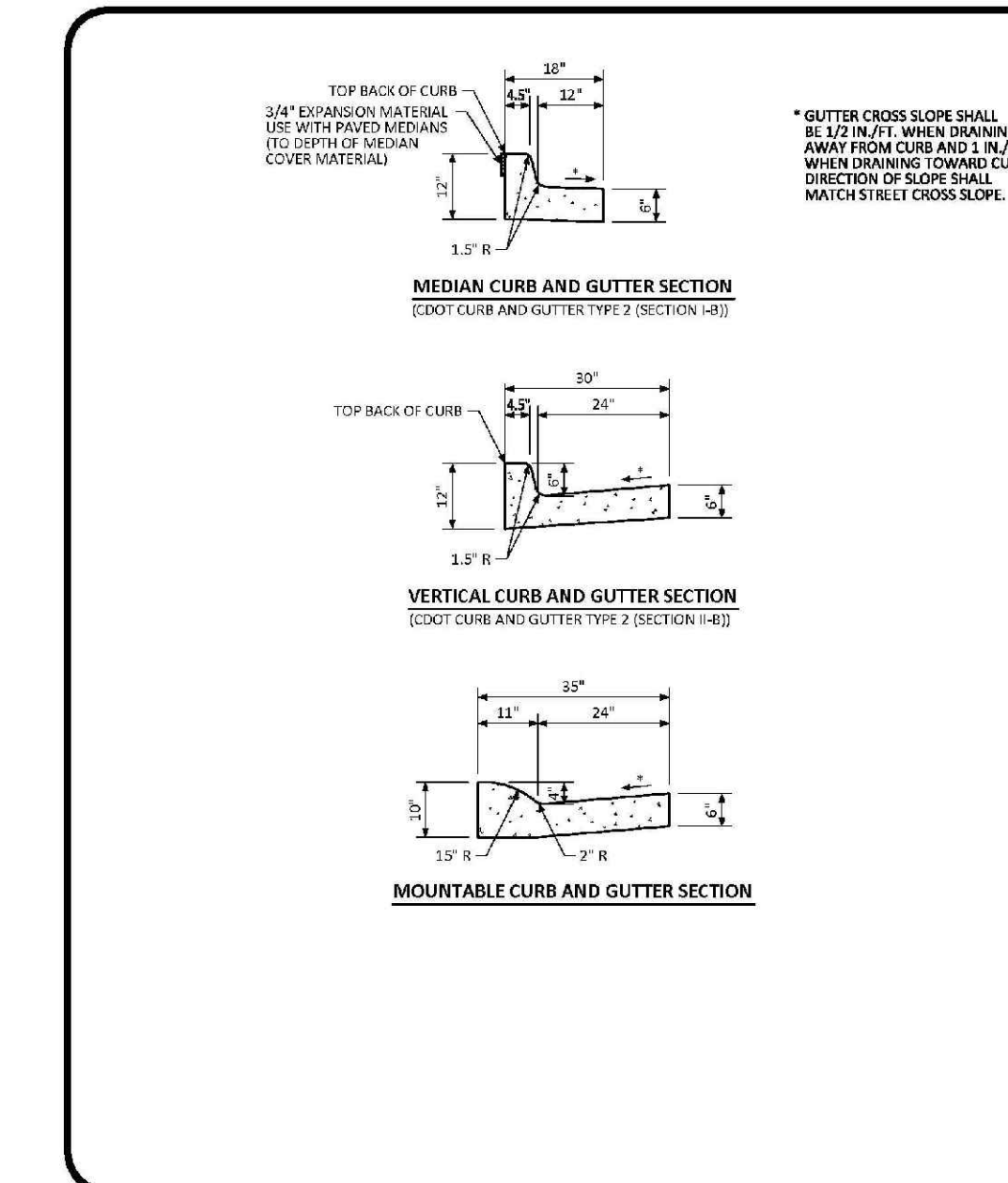
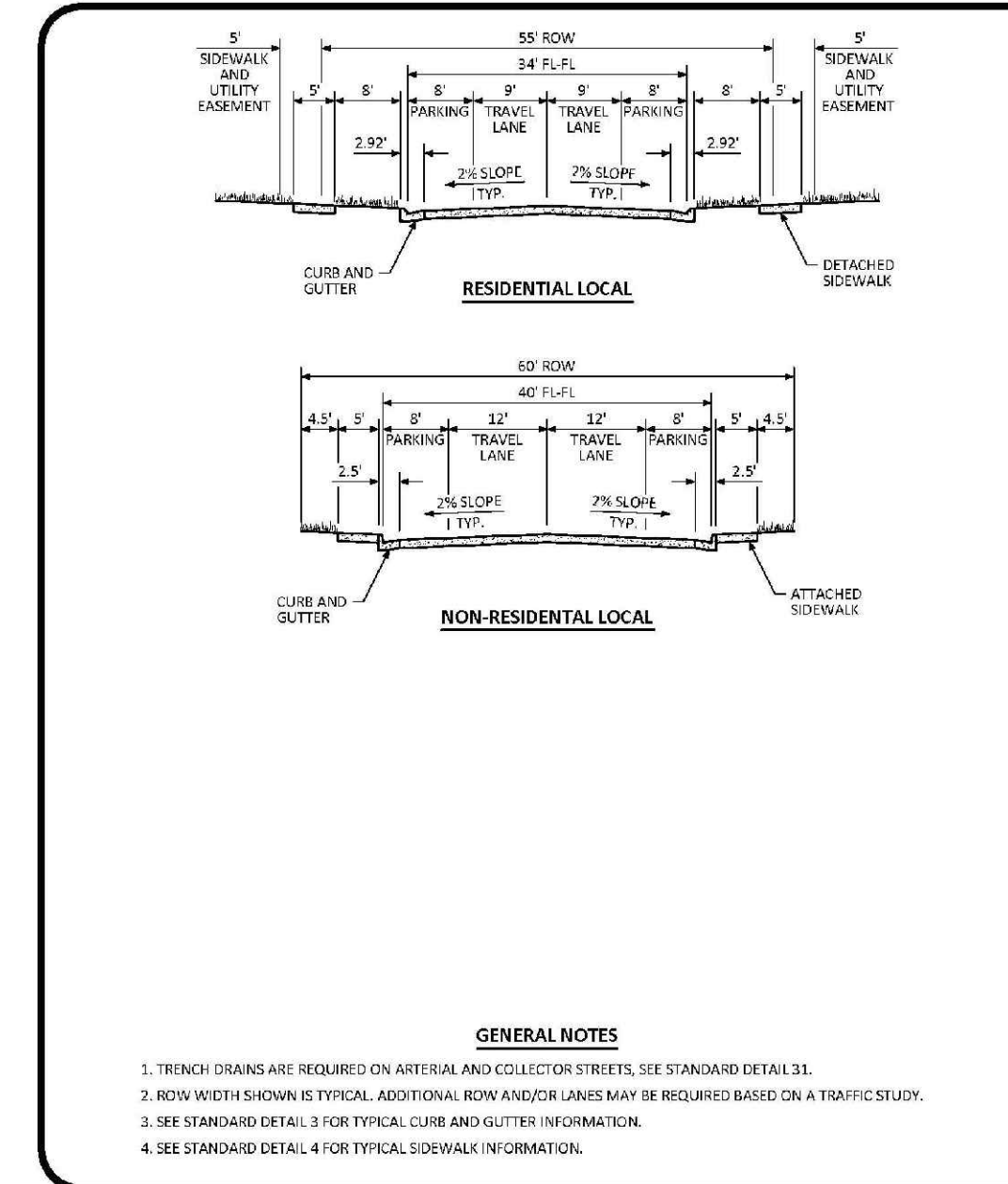
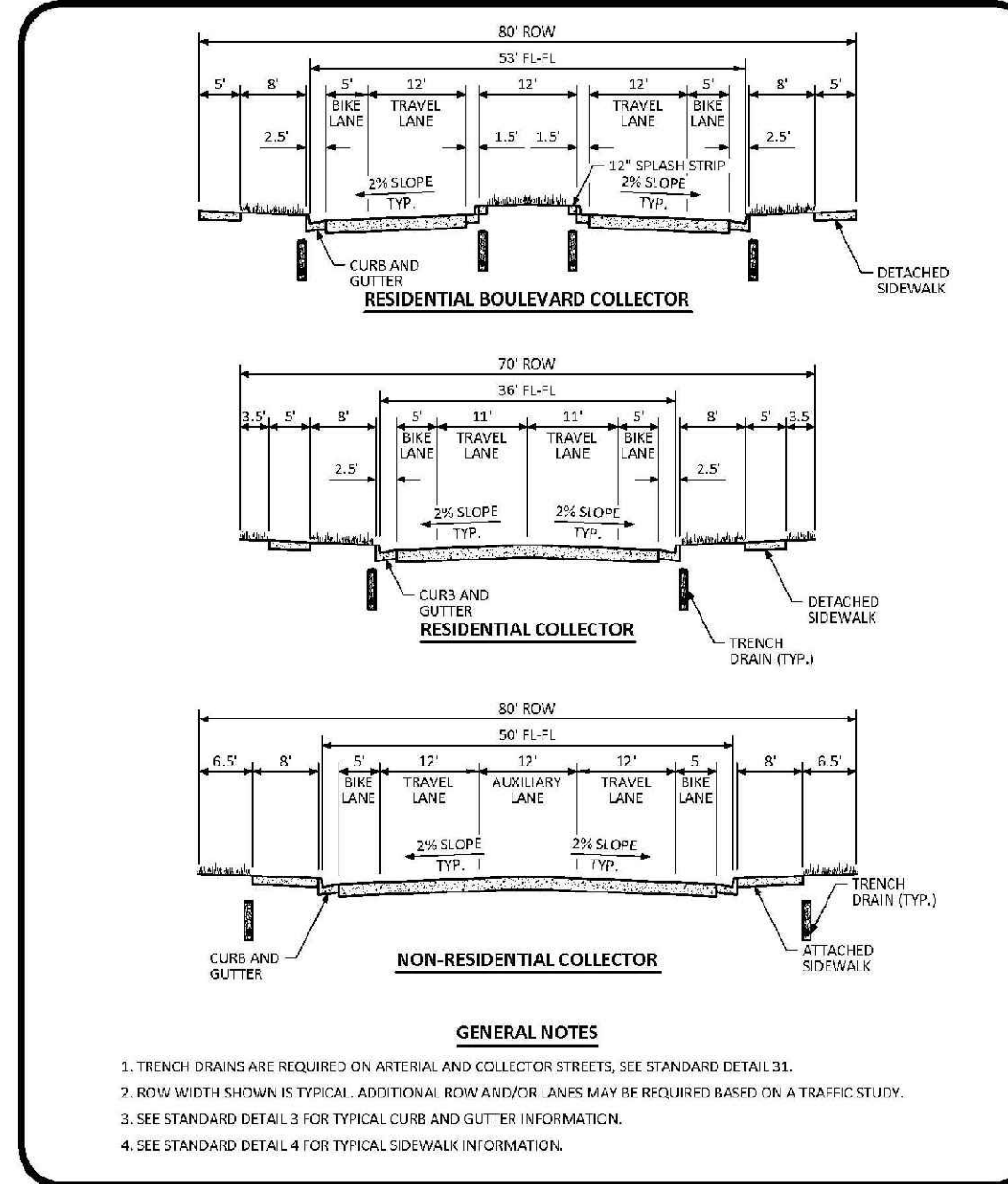
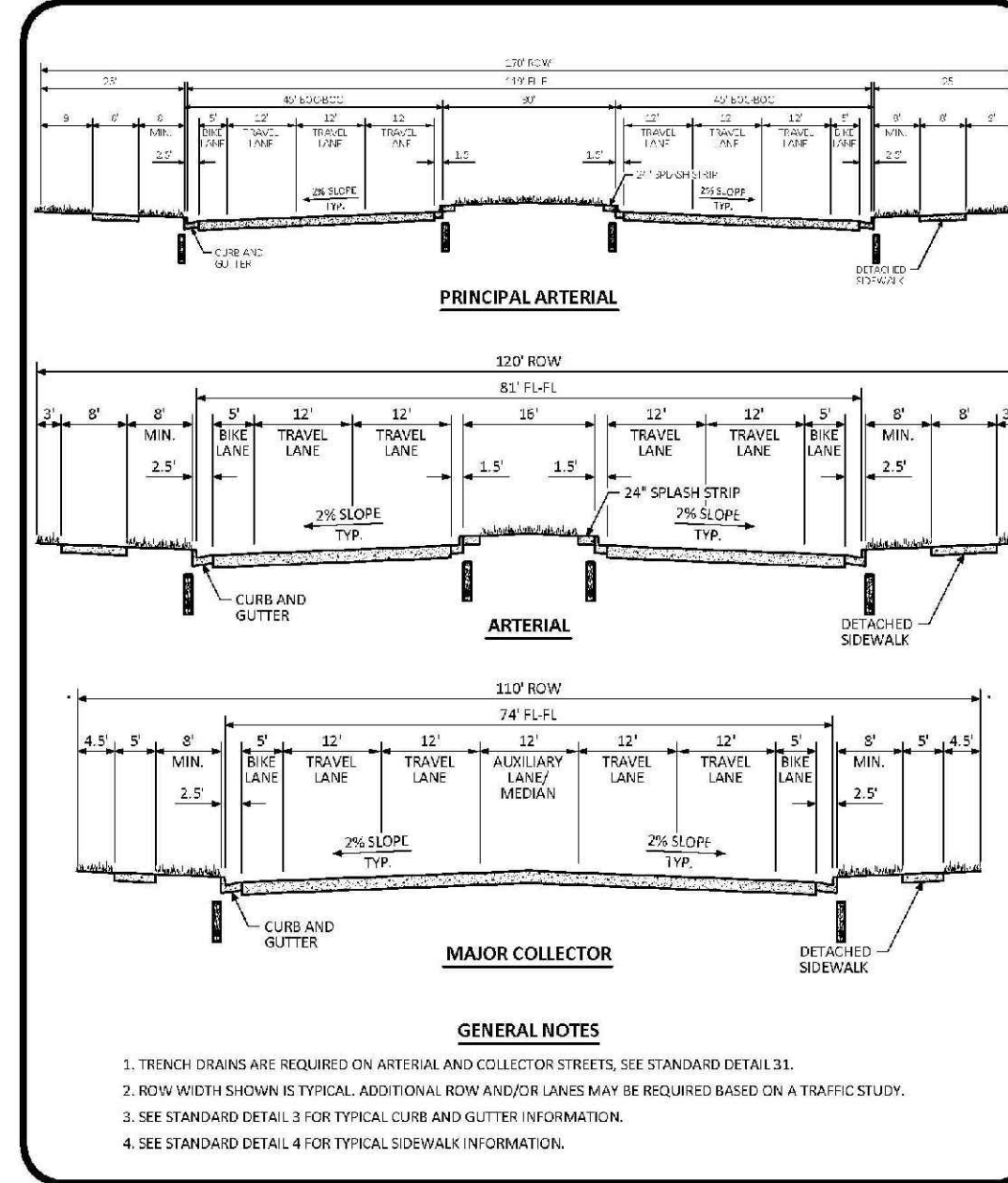
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DRAWN BY: JF	SCALE: AS SHOWN	DATE: SEPTEMBER 2017	FILE NO: 8130283701	SHEET NUMBER: 17	TRAILS AT CROWFOOT FILING 13 CONSTRUCTION DRAWINGS ROADWAY DETAILS	HR 935 LLC 7353 South Alton Way CENTENNIAL, CO 80112		10333 E. Dry Creek Rd. Suite 410 Englewood, Colorado Tel: (720) 482-9526 Fax: (720) 482-9548	Revisions	Appr.	Date
	NO. DATE								NO. DATE	NO. DATE	

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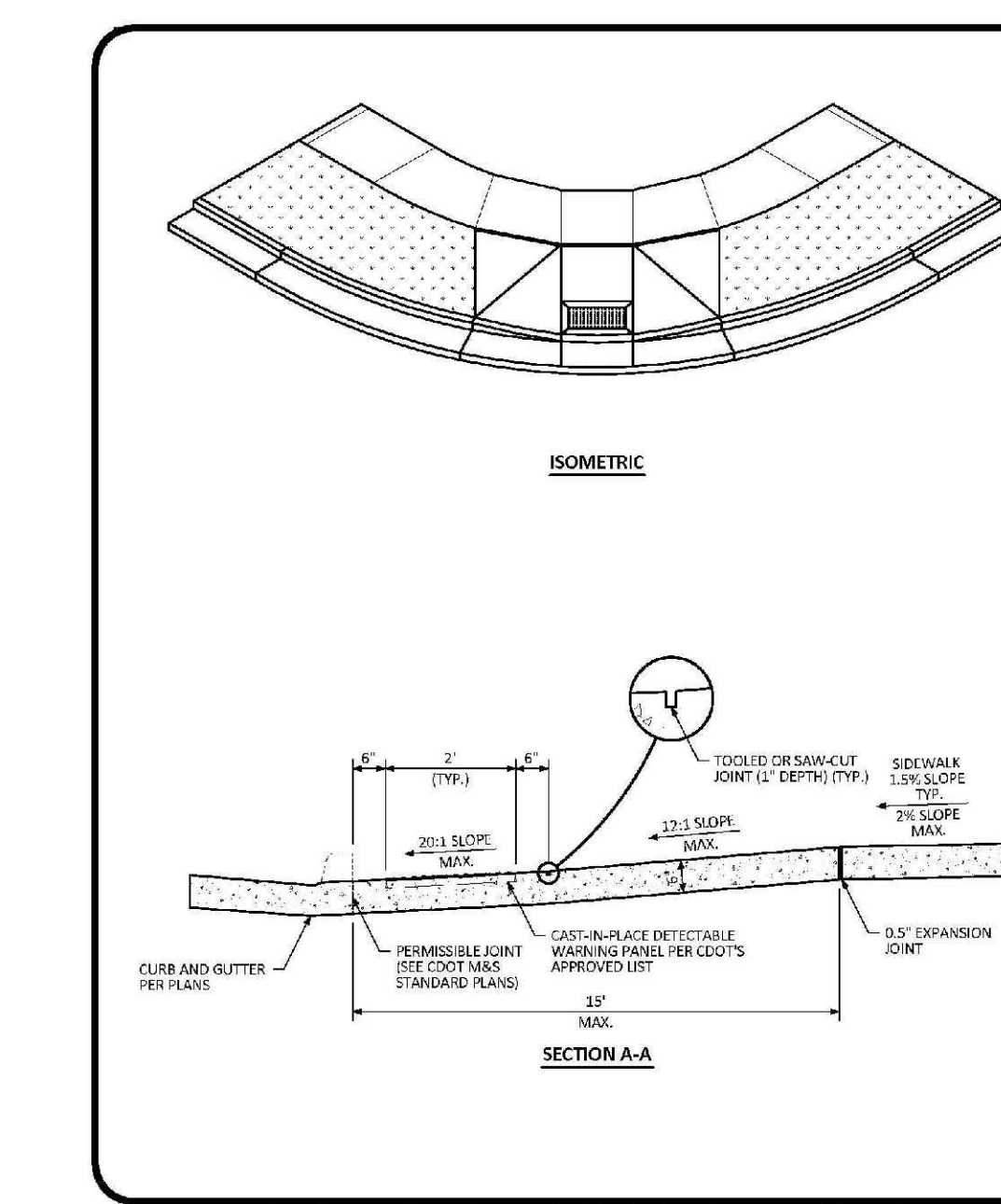
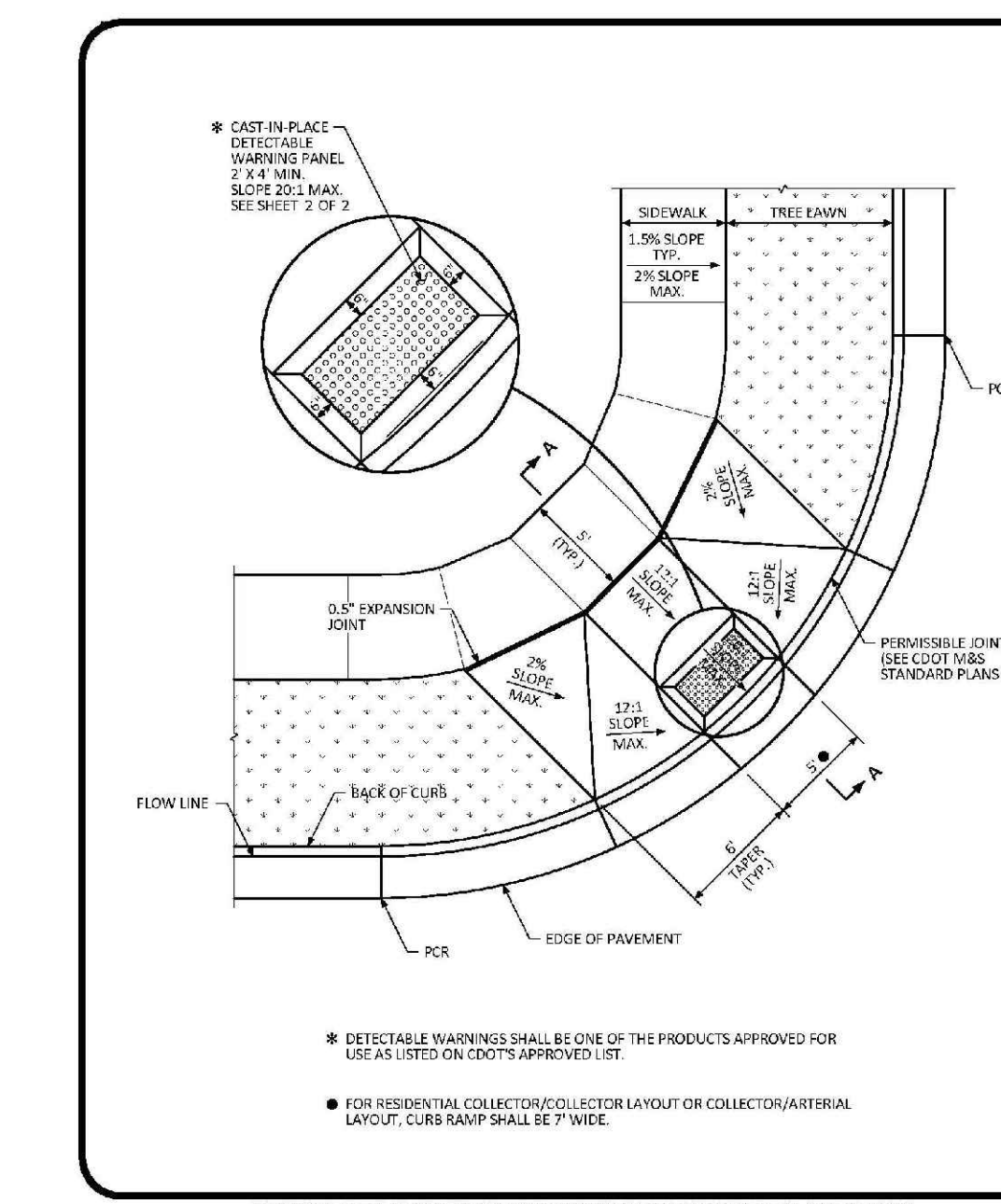
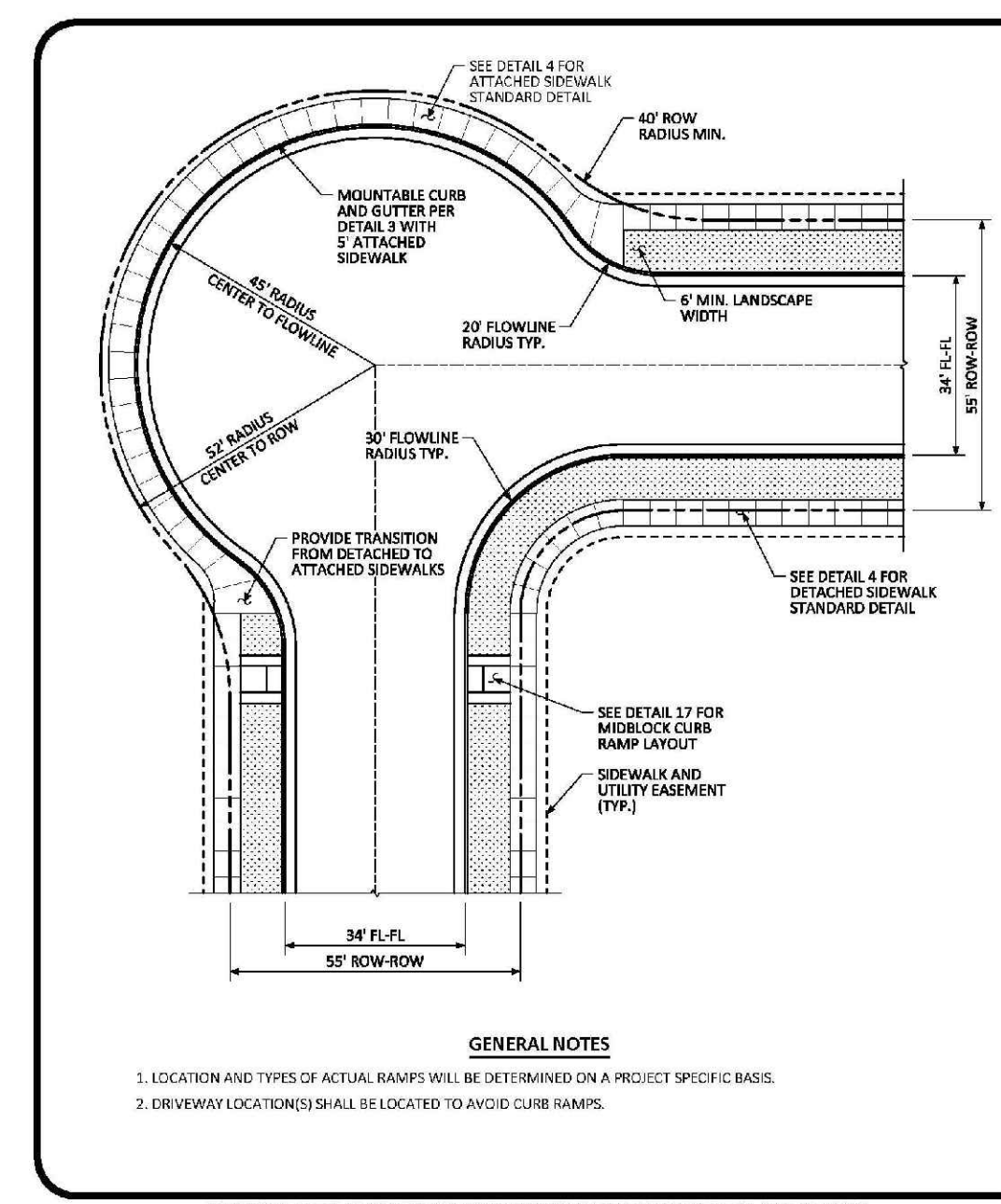
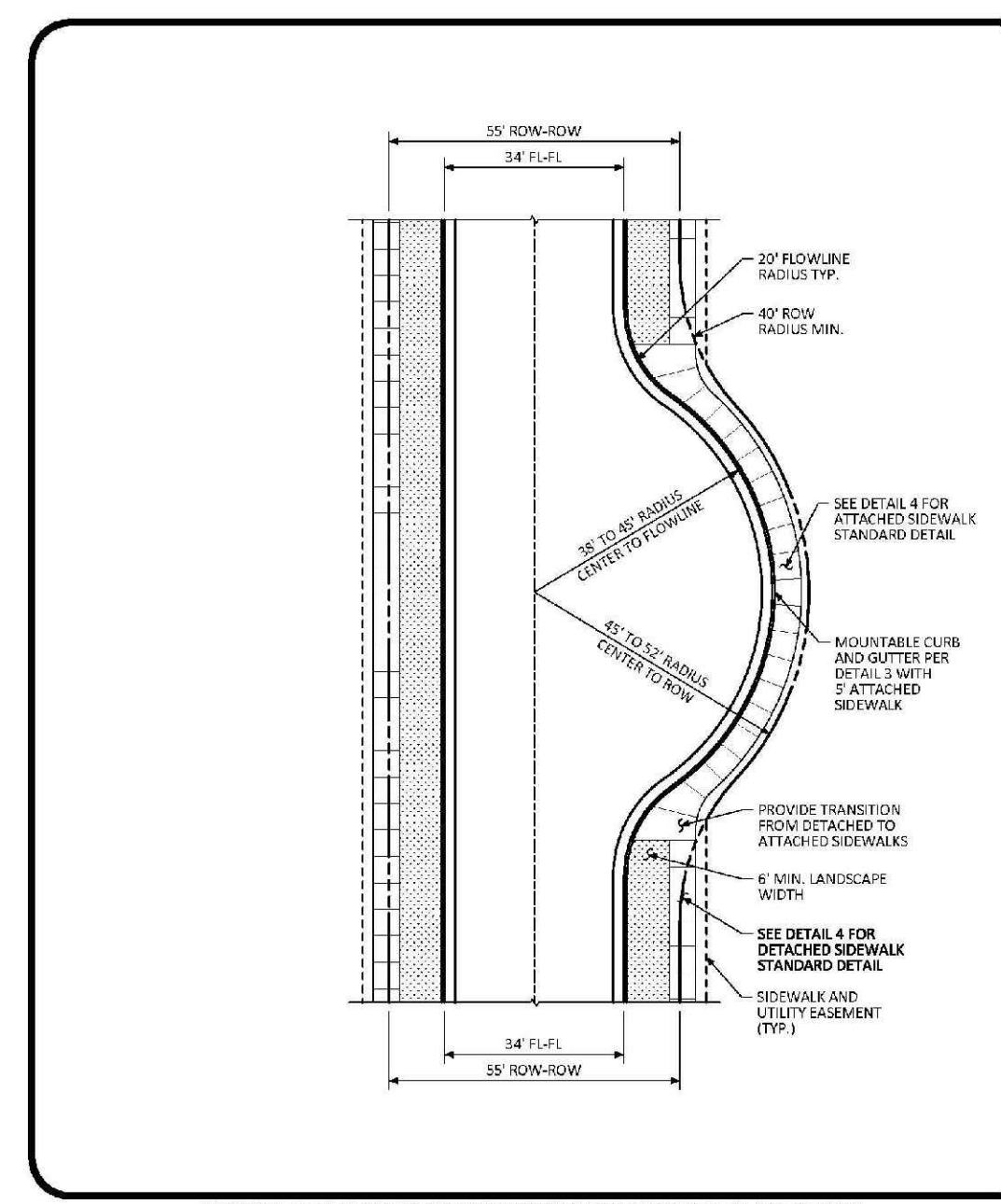
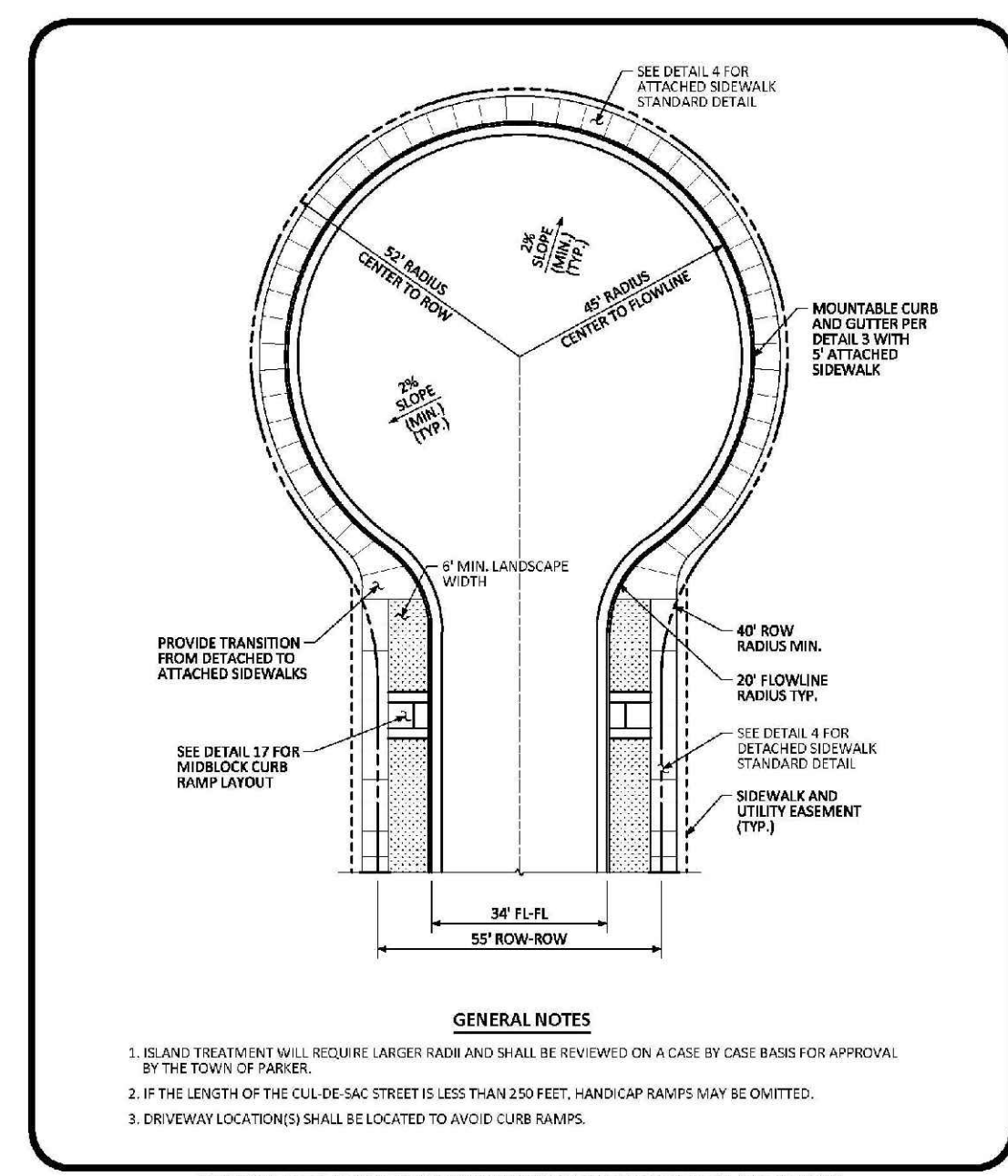
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DATE: JULY 2015  
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**CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL**  
DATE: AUGUST 2014  
DETAIL: 1 OF 1



**RESIDENTIAL LOCAL CUL-DE-SAC TYPICAL LAYOUT STANDARD DETAIL**  
DATE: AUGUST 2014  
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**RESIDENTIAL LOCAL KNUCKLE TYPICAL LAYOUT STANDARD DETAIL**  
DATE: AUGUST 2014  
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DATE: AUGUST 2014  
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			CHECKED BY: JJ						
18	TRAILS AT CROWFOOT FILING 13 CONSTRUCTION DRAWINGS ROADWAY DETAILS								
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10333 E. Dry Creek Rd. Suite 110 Englewood, CO 80110 Tel: (720) 482-9526 Fax: (720) 482-9546									

