



**GENERAL NOTES FOR CONTRACTOR**

- THE FOLLOWING NOTES ARE PROVIDED TO GIVE DIRECTIONS TO THE CONTRACTOR BY THE ENGINEER OF THE PLANS. THE TOWN ENGINEER'S SIGNATURE ON THESE PLANS DOES NOT CONSTITUTE APPROVAL OF ANY OF THESE NOTES AND THE TOWN WILL NOT BE HELD RESPONSIBLE FOR THEIR ENFORCEMENT.
- THE CONTRACTOR AGREES THAT HE/SHE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOBSITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING: SAFETY OF ALL PERSONS AND PROPERTY, AND THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO INSURE THAT ALL SLOPES, STREETS, UTILITIES, AND STORM SEWERS ARE BUILT IN ACCORDANCE WITH THESE PLANS. IF THERE IS ANY QUESTION REGARDING THESE PLANS OR FIELD STAKES, THE CONTRACTOR SHALL REQUEST AN INTERPRETATION BEFORE DOING ANY WORK BY CALLING THE ENGINEER OF RECORD AT 303-228-2300. THE CONTRACTOR SHALL ALSO TAKE THE NECESSARY STEPS TO PROTECT THE PROJECT AND ADJACENT PROPERTY FROM ANY EROSION AND SILTATION THAT RESULT FROM HIS OPERATIONS BY APPROPRIATE MEANS (SAND BAGS, HAY BALES, TEMPORARY DESILTING BASINS, DIKES, SHORING, ETC.) UNTIL SUCH TIME THAT THE PROJECT IS COMPLETED AND ACCEPTED FOR MAINTENANCE BY WHATEVER OWNER, AGENCY, OR ASSOCIATION IS TO BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE.
- EXCEPT AS NOTED HEREON ALL UTILITY SERVICES WITHIN THIS DEVELOPMENT ARE UNDERGROUND INSTALLATIONS. THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES PRIOR TO STARTING WORK NEAR THEIR FACILITIES, AND SHALL COORDINATE HIS WORK WITH COMPANY REPRESENTATIVES. FOR UTILITY MARK-OUT SERVICE, CALL 811.
- THE EXISTENCE AND LOCATION OF UNDERGROUND UTILITIES OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED FROM A SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO OTHER EXISTING UTILITIES EXCEPT AS SHOWN ON THESE PLANS. NO REPRESENTATION IS MADE AS TO THE ACCURACY OR COMPLETENESS OF SAID UTILITY INFORMATION. THE CONTRACTOR IS REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN HEREON AND ANY OTHERS NOT OF RECORD OR NOT SHOWN ON THESE PLANS. ALL DAMAGES THERE TO CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE APPROPRIATE SPECIFICATIONS AND STANDARDS AT THE EXPENSE OF THE CONTRACTOR.
- LOCATION AND ELEVATION OF EXISTING IMPROVEMENTS TO BE CONNECTED TO SHALL BE CONFIRMED BY FIELD MEASUREMENTS PRIOR TO CONSTRUCTION OF NEW WORK.
- CONTRACTOR SHALL MAKE EXPLORATORY EXCAVATIONS AND LOCATE EXISTING UNDERGROUND FACILITIES SUFFICIENTLY AHEAD OF CONSTRUCTION TO PERMIT REVISIONS TO PLANS IF REVISIONS ARE NECESSARY BECAUSE OF ACTUAL LOCATION OF EXISTING FACILITIES.
- FOR ALL UTILITY TRENCHES, SOILS REPORTS SHALL BE SUBMITTED TO THE ENGINEER OF RECORD BY A QUALIFIED SOILS ENGINEER WHICH CERTIFY THAT TRENCH BACKFILL WAS COMPACTED AS DIRECTED BY THE SOILS ENGINEER IN ACCORDANCE WITH THE ON-SITE EARTHWORK SPECIFICATIONS.
- ANY WORK DONE WITHOUT INSPECTION OR MATERIALS TESTING IS SUBJECT TO REMOVAL OR CORRECTION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVAL OF ANY DAMAGE TO THE EXISTING IMPROVEMENTS TO THE SATISFACTION OF THE ENGINEER.
- PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL SITE CONDITIONS FOR GRADING, DRAINAGE AND UNDERGROUND FACILITIES, INCLUDING LOCATION AND ELEVATION OF EXISTING UNDERGROUND FACILITIES AT CROSSINGS WITH PROPOSED UNDERGROUND FACILITIES. IF CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND SHALL NOT BEGIN CONSTRUCTION UNTIL THE CHANGED CONDITIONS HAVE BEEN EVALUATED.
- THE CONTRACTOR SHALL FAMILIARIZE HIMSELF/HERSELF WITH THE PLANS, THE SOILS AND/OR GEOLOGY REPORTS, AND THE SITE CONDITIONS PRIOR TO COMMENCING WORK.
- SHOULD CONFLICTING INFORMATION BE FOUND ON THE PLANS OR IN THE FIELD, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF RECORD BEFORE PROCEEDING WITH THE WORK IN QUESTION.
- APPROVAL OF THESE PLANS BY THE TOWN DOES NOT AUTHORIZE ANY WORK TO BE PERFORMED UNTIL A PERMIT HAS BEEN ISSUED.
- THE APPROVAL OF THIS PLAN OR ISSUANCE OF A PERMIT BY THE TOWN DOES NOT AUTHORIZE THE CONTRACTOR OR OWNER TO VIOLATE ANY FEDERAL, STATE OR COUNTY LAWS, ORDINANCES, REGULATIONS, OR POLICIES, INCLUDING, BUT NOT LIMITED TO, THE FEDERAL ENDANGERED SPECIES ACT OF 1973 AND AMENDMENTS THERETO (16 USC SECTION 1531 ET SEQ.)
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR SURVEY MONUMENTS AND/OR VERTICAL CONTROL BENCHMARKS WHICH ARE DISTURBED OR DESTROYED BY CONSTRUCTION. A LAND SURVEYOR MUST FIELD LOCATE, REFERENCE, AND/OR PRESERVE ALL HISTORICAL OR CONTROLLING MONUMENTS PRIOR TO ANY EARTHWORK. IF DESTROYED, A LAND SURVEYOR SHALL REPLACE SUCH MONUMENTS WITH APPROPRIATE MONUMENTS. ALL MONUMENTS SHALL BE SET IN ACCORDANCE WITH COLORADO REVISED STATUTES TITLE 12, ARTICLE 25, PART 2. IF ANY VERTICAL CONTROL IS TO BE DISTURBED OR DESTROYED, THE TOWN FIELD SURVEY SECTION MUST BE NOTIFIED IN WRITING AT LEAST 3 DAYS PRIOR TO THE CONSTRUCTION. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE COST OF REPLACING ANY VERTICAL CONTROL BENCHMARKS DESTROYED BY THE CONSTRUCTION.
- DEVIATIONS FROM THESE SIGNED PLANS WILL NOT BE ALLOWED UNLESS THE TOWN ENGINEER APPROVES A CONSTRUCTION CHANGE OR THE TOWN/AGENCY INSPECTOR REQUIRES THE CHANGE.
- AS-BUILT DRAWINGS MUST BE SUBMITTED TO THE ENGINEER PRIOR TO ACCEPTANCE OF THIS PROJECT BY THE TOWN. CONTACT THE ENGINEER FOR THE REQUIREMENTS OF THE AS-BUILT DRAWINGS.
- THE AREA WHICH IS DEFINED AS A NON GRADING AREA AND WHICH IS NOT TO BE DISTURBED SHALL BE STAKED PRIOR TO START OF THE WORK. THE PERMIT APPLICANT AND ALL OF THEIR REPRESENTATIVES OR CONTRACTORS SHALL COMPLY WITH THE REQUIREMENTS FOR PROTECTION OF THIS AREA AS REQUIRED BY ANY APPLICABLE AGENCY. ISSUANCE OF THE TOWN'S GRADING PERMIT SHALL NOT RELIEVE THE APPLICANT OR ANY OF THEIR REPRESENTATIVES OR CONTRACTORS FROM COMPLYING WITH ANY STATE OR FEDERAL REQUIREMENTS BY AGENCIES INCLUDING BUT NOT LIMITED TO COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT OR COLORADO DIVISION OF WILDLIFE. COMPLIANCE MAY INCLUDE OBTAINING PERMITS, OTHER AUTHORIZATIONS, OR COMPLIANCE WITH MANDATES BY ANY APPLICABLE STATE OR FEDERAL AGENCY.
- EXISTING TOPOGRAPHY SHOWN WAS TAKEN FROM A SURVEY PREPARED BY ENGINEERING SERVICE COMPANY DATED MARCH 20, 2014.
- NOTES AND DETAILS DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL NOTES AND TYPICAL DETAILS. WHERE NO DETAILS ARE GIVEN, CONSTRUCTION SHALL BE AS SHOWN FOR SIMILAR WORK.
- IF AT ANY TIME DURING THE GRADING OPERATION, ANY UNFAVORABLE GEOLOGICAL CONDITIONS ARE ENCOUNTERED, GRADING IN THAT AREA WILL STOP UNTIL APPROVED CORRECTIVE MEASURES ARE OBTAINED.
- STRAIGHT GRADE SHALL BE MAINTAINED BETWEEN CONTOUR LINES AND SPOT ELEVATIONS UNLESS OTHERWISE SHOWN ON THE PLANS.
- ALL DEBRIS AND FOREIGN MATERIAL SHALL BE REMOVED FROM THE SITE AND DISPOSED OF AT APPROVED DISPOSAL SITES. THE CONTRACTOR SHALL OBTAIN NECESSARY PERMITS FOR THE TRANSPORTATION OF MATERIAL TO AND FROM THE SITE.
- DIMENSIONS TO PIPELINES ARE TO CENTERLINE UNLESS OTHERWISE NOTED.
- CONSTRUCTION STAKING FOR IMPROVEMENTS SHOWN IN THESE PLANS SHALL BE PERFORMED BY A LICENSED LAND SURVEYOR.
- ALL DIMENSIONS ARE IN FEET OR DECIMALS THEREOF.
- SPOT GRADES ARE TO FLOWLINE OR FINISH PAVEMENT GRADE UNLESS OTHERWISE NOTED.
- CONTRACTOR TO BE AWARE OF ALL OVERHEAD LINES AT ALL TIMES, SO AS NOT TO DISTURB THEM.
- WATER SHALL BE PROVIDED ONSITE AND USED TO CONTROL DUST DURING DEMOLITION AND CONSTRUCTION OPERATIONS.
- STORM DRAINAGE SYSTEMS SHOWN ON THESE PLANS HAVE BEEN DESIGNED FOR THE FINAL SITE CONDITION AT COMPLETION OF THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ADEQUATE DRAINAGE OF THE SITE DURING INTERIM CONDITIONS OF CONSTRUCTION.
- RETAINING WALLS LOCATED CLOSER TO THE PROPERTY LINE THAN THE HEIGHT OF THE WALL SHALL BE BACKFILLED NOT LATER THAN 10 DAYS AFTER CONSTRUCTION OF THE WALL AND NECESSARY STRUCTURAL SUPPORTING MEMBERS UNLESS RECOMMENDED OTHERWISE BY ENGINEER OF RECORD.

**TOWN OF PARKER CONSTRUCTION NOTES**

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- A PRECONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRECONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- A DEVELOPMENT REVIEW ENGINEER SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A DEVELOPMENT REVIEW ENGINEER IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE DEVELOPMENT REVIEW ENGINEER'S ABSENCE. HOWEVER, TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION. FOR INFORMATION CONTACT: DENVER INTER-UTILITY GROUP, 303-534-6700 OR 1-800-922-1987. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER PUBLIC WORKS DIRECTOR, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- ALL PROPOSED STREET CUTS TO EXISTING PAVEMENTS FOR UTILITIES, STORM SEWER OR FOR OTHER PURPOSES ARE LISTED AND REFERENCED BELOW:  
SANITARY SEWER CONNECTION - N/A  
WATER TIE IN - N/A  
STORM CONNECTION - N/A
- A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT-OF-WAY PERMIT USE APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION."
- COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- PLANS ARE APPROVED FOR PERIOD OF 1 (ONE) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER 1 YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY." FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WQCD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO 80222-1530. ATTENTION: PERMITS AND ENFORCEMENT SECTION. PHONE (303) 692-3500.
- IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE AFOREMENTIONED FACILITIES, OR ANY WATER OF THE UNITED STATES.
- ALL REFERENCES TO BOOKS, PAGES, MAPS, AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDER'S OFFICE.

**TOWN OF PARKER STORM DRAINAGE INFRASTRUCTURE NOTES**

- ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72" (5 CCR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR TO THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUDED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
- STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
- ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED TO MEET TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS:  
6.1. 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE MANHOLES, INLETS, ETC)  
6.2. 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A MANNER THAT THE ENDS ARE FULLY ENTERED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-604-11 SHALL BE USED ON ALL FLARED END SECTION OUTFALLS.
- EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
- PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

**ROADWAY NOTES**

- PAVING SHALL NOT COMMENCE UNTIL A SOILS REPORT AND PAVEMENT DESIGN IS APPROVED BY THE ENGINEERING DIVISION AND SUBGRADE COMPACTION TESTS ARE SUBMITTED TO AND APPROVED BY THE DEVELOPMENT REVIEW ENGINEER.
- STANDARD TOWN OF PARKER HANDICAP RAMPS ARE TO BE CONSTRUCTED AT ALL CURB RETURNS AND AT ALL "T" INTERSECTIONS.
- ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
- ALL ELEVATIONS ARE ON USGS DATUM WITH DATE, RANGE POINT OR MONUMENT SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
- EXCEPT WHERE OTHERWISE PROVIDED FOR IN THESE PLANS AND SPECIFICATIONS, THE COLORADO DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF HIGHWAYS M AND S STANDARDS, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, LATEST EDITION, SHALL APPLY.

**TOWN OF PARKER SIGNAGE AND STRIPING NOTES**

- ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT VERSION OF THE FEDERAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), COLORADO SUPPLEMENTAL MUTCD, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL. FURTHER SPECIFICATIONS AND ILLUSTRATIONS ARE LOCATED IN THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) M AND S STANDARDS.
- A FIELD INSPECTION OF LOCATION AND INSTALLATION OF ALL SIGNS AND MARKINGS SHALL BE PERFORMED BY THE TOWN OF PARKER.
- THE CONTRACTOR INSTALLING SIGNS SHALL BE RESPONSIBLE FOR THE LOCATING AND PROTECTING OF ALL UNDERGROUND UTILITIES.
- TYPE III (LIGHTED) BARRICADES SHALL BE SET AT THE ENDS OF ROADWAYS SEPARATING FINISHED (AND/OR ACCEPTED) AND UNFINISHED CONSTRUCTION AREAS AND SHALL BE MAINTAINED BY THE CONTRACTOR/DEVELOPER. A "ROAD CLOSED AHEAD" AND "TO BE EXTENDED" WARNING SIGNS SHALL BE INSTALLED APPROPRIATELY IN ADVANCE OF THE TYPE III BARRICADES.
- SPECIAL CARE SHALL BE TAKEN IN SIGN LOCATIONS TO ENSURE AN UNOBSTRUCTED VIEW OF EACH SIGN.
- WHERE STOP SIGN CONTROL IS APPROPRIATE, 36 INCH STOP SIGNS SHALL BE USED FOR APPROACHES TO ANY ROADWAY THAT IS CLASSIFIED AS A COLLECTOR OR GREATER.
- A 7 FOOT MINIMUM POST LENGTH SHALL BE MAINTAINED FROM THE BOTTOM OF THE SIGN PANEL TO THE GROUND. THIS REQUIREMENT FOR VERTICAL CLEARANCE IS FOR ALL SIGNS.
- DELINEATION OF ROADWAYS SHALL BE AS SPECIFIED IN THE COLORADO DEPARTMENT OF TRANSPORTATION M AND S STANDARDS.
- RAISED MEDIAN ISLAND NOSES SHALL HAVE R4-7 SIGNS AT EACH END AND A 4"x12" HIGH INTENSITY YELLOW SIGN BLANK LOCATED MIDWAY BETWEEN THE R4-7 SIGN AND FINISHED GRADE ON EACH POST.
- SIGNAGE AND STRIPING HAS BEEN DETERMINED BY INFORMATION AVAILABLE AT THE TIME OF REVIEW. PRIOR TO THE INITIATION OF ANY WARRANTY PERIOD, THE TOWN OF PARKER RESERVES THE RIGHT TO REQUIRE MODIFICATIONS TO EXISTING, OR INSTALLATION OF, ADDITIONAL SIGNAGE AND/OR PAVEMENT MARKING IF IT IS DETERMINED THAT CONDITIONS WARRANT SUCH MODIFICATION ACCORDING TO THE MUTCD OR THE CDOT M AND S STANDARDS. ALL SIGNAGE AND STRIPING SHALL FALL UNDER THE REQUIREMENTS OF THE TWO YEAR WARRANTY PERIOD FOR NEW CONSTRUCTION. ADDITIONALLY, ALL PAVEMENT MARKINGS SHALL NOT LIFT OR PEEL DURING THE FIRST YEAR AFTER INSTALLATION.
- DIAMOND GRADE MATERIAL SHALL BE USED ON ALL STOP SIGNS AND OVERHEAD SIGNS. ALL OTHER ROADSIDE TRAFFIC CONTROL DEVICES SHALL USE HIGH INTENSITY GRADE SHEETING. NO FLUORESCENT YELLOW GREEN SHEETING SHALL BE USED UNLESS SPECIFICALLY APPROVED BY THE TOWN OF PARKER.
- ALL STREET NAME SIGNS SHALL BE HIGH INTENSITY, EXTRUDED, GREEN AND WHITE BLADES. ARTERIAL AND COLLECTOR STREET NAME SIGNS SHALL BE 9 INCH BLADES AND HAVE MIXED CASE LETTERING. LOCAL STREET NAME SIGNS SHALL BE 6 INCHES BLADES WITH MIXED CASE LETTERING.
- ALL LAYOUTS FOR INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- ALL REMOVED SIGNS SHALL BE RETURNED TO THE TOWN OF PARKER.
- CROSSWALKS:  
•• SHALL BE CONSTRUCTED USING PREFORMED THERMO-PLASTIC OR AN APPROVED EQUAL.  
•• SHALL BE LONGITUDINAL 2 FEET BY 10 FEET (CONTINENTAL) TYPE OR OTHER APPROVED BY THE TOWN OF PARKER.  
•• SHALL LINE UP WITH HANDICAP RAMPS.  
•• SHALL BE CENTERED WITHIN LANES AND PLACED ON LANE AND CURB LINES SO AS TO AVOID VEHICLE WHEEL PATHS.
- ALL PAVEMENT MARKING MATERIAL (INCLUDING WORDS AND SYMBOLS) SHALL BE AS FOLLOWS:  
FOR CONCRETE SURFACE:  
LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC.) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER. WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE METHYL-MYTHACRALATE, PREFORMED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), OR AS SPECIFIED.  
(SAND OR WATER BLAST CURING COMPOUND PRIOR TO INSTALLATION OF MARKINGS.)  
FOR ASPHALT SURFACE:  
LONG LINE MARKINGS (CHANNELIZATION LINES, SKIPS, ETC.) SHALL BE EPOXY PAINT. OTHER PAINT MAY BE USED FOR TEMPORARY APPLICATION ONLY WHEN APPROVED BY THE TOWN OF PARKER. WORDS, SYMBOLS, AND CROSSWALKS/STOP BARS SHALL BE HOT APPLIED THERMOPLASTIC (90 MIL), PREFORMED PLASTIC (90 MIL), INLAYED TAPE (STAMARK), METHYL-MYTHACRALATE, OR AS SPECIFIED.
- INSPECTION AND APPROVAL OF STRIPING AND CROSSWALK LAYOUT TO BE DONE BY TOWN OF PARKER DEVELOPMENT REVIEW ENGINEER (CALL 303-840-9546) PRIOR TO APPLICATION OF FINAL STRIPING.

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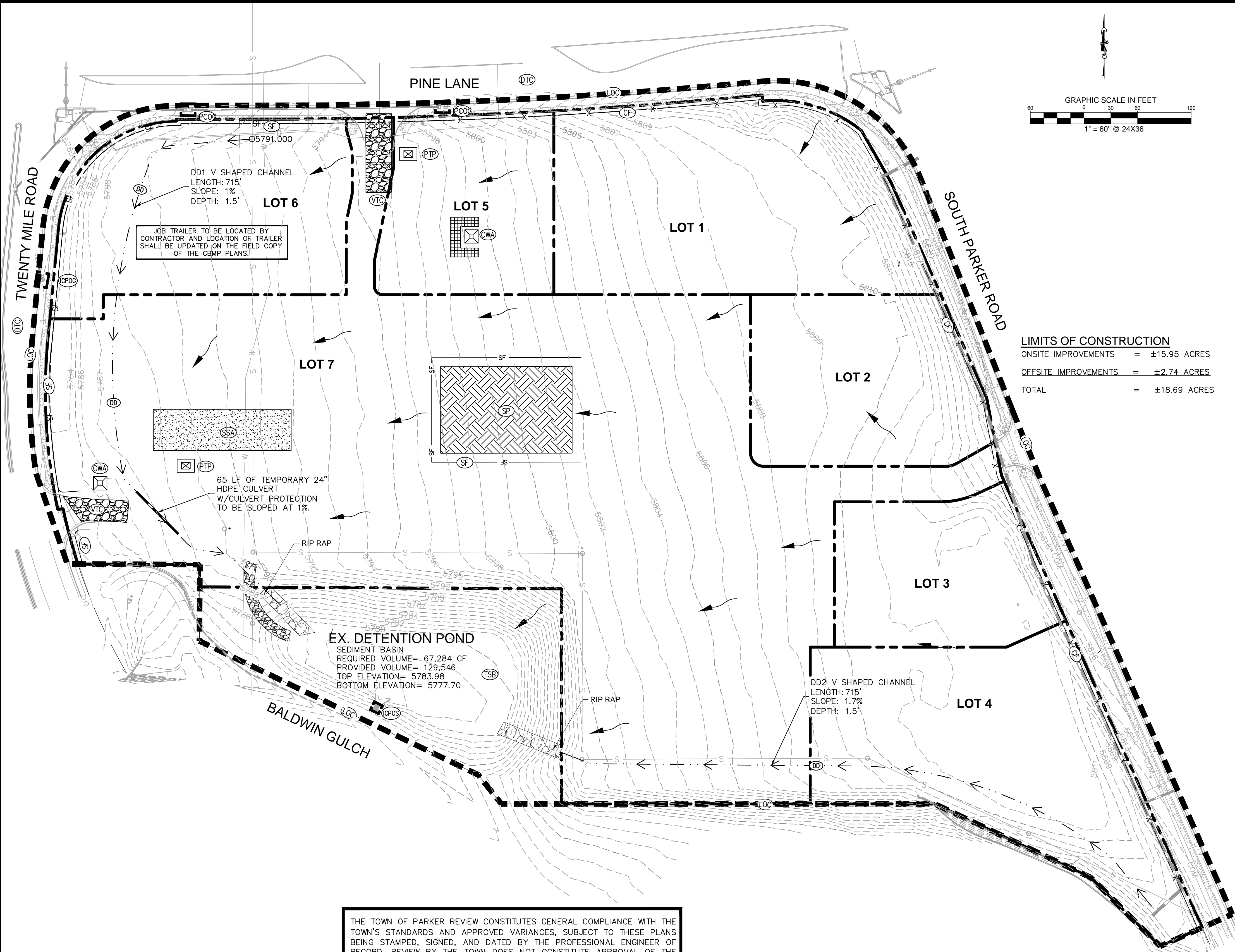
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**LEGEND\***

- EXISTING PROPERTY LINE
- (LOC) LIMITS OF CONSTRUCTION
- X-X- (CF) CONSTRUCTION FENCE
- SF- (SF) SILT FENCE
- [SSA] (SSA) STABILIZED STAGING AREA
- [CWA] (CWA) CONCRETE WASHOUT
- [VTC] (VTC) VEHICLE TRACKING CONTROL
- [SP] (SP) SOIL STOCKPILE
- [PTP] (PTP) PORTABLE TOILET PROTECTION
- FLOW ARROW
- [DTC] (DTC) DEBRIS TRASH CONTROL
- [IFCOS] (IFCOS) INLET PROTECTION, CURB ON SUMP
- [DD] (DD) DIVERSION DITCH/BERM
- [MWP] (MWP) MASONRY WORK PROTECTION
- [IFCOS] (IFCOS) INLET PROTECTION, CURB ON GRADE

**LIMITS OF CONSTRUCTION**

ONSITE IMPROVEMENTS = ±15.95 ACRES  
 OFFSITE IMPROVEMENTS = ±2.74 ACRES  
 TOTAL = ±18.69 ACRES

\*SEE SHEET C3.2 - CBMP DETAILS FOR COMPLETE LEGEND

- NOTES**
- THE INTENT OF THIS PLAN IS TO IDENTIFY THE EROSION CONTROL PRACTICES RECOMMENDED. THE CONTRACTOR SHALL REFERENCE ADDITIONAL CONSTRUCTION PLANS FOR DEMOLITION OF EXISTING AND CONSTRUCTION OF PROPOSED IMPROVEMENTS.
  - ADJACENT STREETS AND SIDEWALK SHALL BE KEPT CLEAN AND FREE OF SEDIMENT AND/OR DEBRIS AT ALL TIMES. CONTRACTOR SHALL PERFORM STREET SWEEPING AT ALL TIMES DURING ACTIVE TRACKING AND AT A MINIMUM ON A DAILY BASIS AT THE END OF EACH CONSTRUCTION DAY.
  - TEMPORARY STABILIZATION (TS) SHALL BE IMPLEMENTED WITHIN THE DISTURBED PORTIONS OF THE PROJECT SITE NO LATER THAN 14 DAYS FOLLOWING THE CEASE OF CONSTRUCTION ACTIVITIES WITHIN THE DISTURBED AREAS.
  - PERMANENT STABILIZATION (PS) MAY BE USED WITHIN AREAS OF TEMPORARY STABILIZATION (TS) AT THE CONTRACTOR'S DISCRETION. STABILIZATION SHALL BE APPLIED IN ACCORDANCE WITH APPLICABLE TEMPORARY STABILIZATION SEQUENCING REQUIREMENTS.
  - CONTRACTOR SHALL UTILIZE ROLLED EROSION CONTROL PRODUCTS ON ALL SLOPES 4H:1V OR GREATER TO ACHIEVE REQUIRED STABILIZATION.
  - CONTRACTOR SHALL MAINTAIN ACCEPTABLE EROSION CONTROL PRACTICES WITHIN THE ANTICIPATED LIMITS OF CONSTRUCTION IDENTIFIED HEREIN. BEST MANAGEMENT PRACTICES AND STABILIZATION SHALL BE COMPLETED AS IDENTIFIED HEREIN IN ACCORDANCE WITH OWNER REQUIREMENTS.
  - ALL WORK IN E. COTTONWOOD DR. ROW REQUIRES A ROW PERMIT FROM THE TOWN OF PARKER.
  - CONTRACTOR SHALL REFER TO THE APPROVED GEOTECHNICAL REPORT FOR OVEREXCAVATION REQUIREMENTS AND ADDITIONAL INFORMATION.
  - SILT FENCE TO BE INSTALLED PRIOR TO COMMENCEMENT OF ONSITE GRADING AND CONSTRUCTION ACTIVITIES.
  - DEMOLITION, REMOVAL AND SOIL TREATMENT SHALL BE IN ACCORDANCE WITH THE GEOTECHNICAL ENGINEER RECOMMENDATIONS.
  - CONTRACTOR TO NOTE PROXIMITY OF EXISTING IMPROVEMENTS ADJACENT TO THE SITE AND PROVIDE NECESSARY MEASURES TO PROTECT ALL FACILITIES AND STRUCTURES IN PLACE.
  - CONTRACTOR SHALL MAINTAIN STABILIZED STAGING AREA (SSA), VEHICLE TRACKING CONTROL (VTC), AND CONCRETE WASHOUT AREA (CWA) AT THE CONSTRUCTION ENTRANCE AT ALL TIMES. CONTRACTOR SHALL UPDATE THE EROSION CONTROL PLAN IN THE FIELD TO INDICATE THE LOCATION OF THE SSA, VTC, AND CWA BMPs AS EXCAVATION SEQUENCING DICTATES.
  - CONTRACTOR SHALL OBTAIN R.O.W. PERMITS FOR ANY R.O.W. CLOSURES.
  - TO REDUCE THE POTENTIAL FOR CLOGGING OF DEBRIS GRATES, NO STRAW MULCH SHALL BE USED WITHIN THE EXCESS URBAN RUNOFF VOLUME OF A DETENTION BASIN. INSTEAD, EROSION CONTROL BLANKET SHALL BE INSTALLED FOR A WIDTH OF AT LEAST 6- FEET ON BOTH SIDES OF THE CONCRETE LOW FLOW CHANNELS OR UP TO A DEPTH OF 1-FOOT IN SOIL RIPRAP OR BENCHED LOW FLOW CHANNELS. ADDITIONALLY, HYDRAULICALLY-APPLIED WOOD FIBER MULCH, STABILIZED FIBER MATRIX, BONDED FIBER MATRIX, OR FLEXIBLE GROWTH MEDIUM SHALL BE USED IN ALL OTHER AREAS WITHIN THE EXCESS URBAN RUNOFF VOLUME OF THE DETENTION BASIN. THE BLANKET AND HYDRAULICALLY-APPLIED MULCH SHALL COMPLY WITH THE MATERIALS AND INSTALLATION REQUIREMENTS FOR EROSION CONTROL BLANKETS AS REQUIRED BY THE TOWN'S CBMP PROGRAM, AND WHERE APPLICABLE, THE PRODUCT MANUFACTURER.
  - LOT PROTECTION IS REQUIRED ON ALL COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
  - CONTRACTOR SHALL TEMPORARILY IRRIGATE ALL AREAS OF PERMANENT VEGETATION THAT WILL NOT BE SERVED BY PERMANENT IRRIGATION.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NOTE: REFER TO SHEET C3.11 FOR BMP CALCULATIONS.

NO.	REVISION	BY	DATE

**Kimley-Horn**

2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
 CBMP PLAN (INITIAL)

PRELIMINARY  
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**Kimley-Horn**  
 Kimley-Horn and Associates, Inc.

PROJECT NO.  
096502001

DRAWING NAME  
096502001EC\_IP

**811** Know what's below.  
Call before you dig.

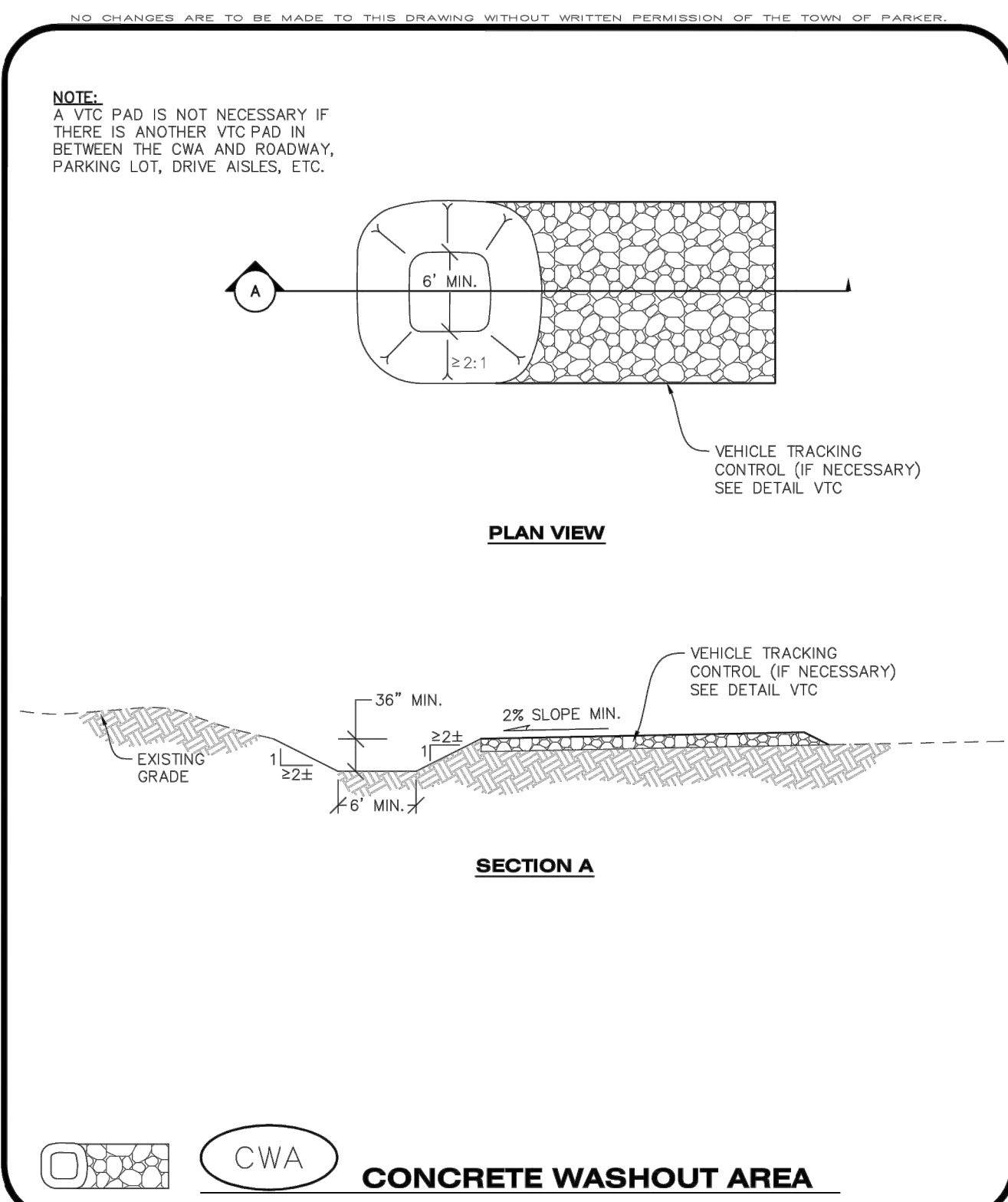
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**CBMP** | **CWA**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

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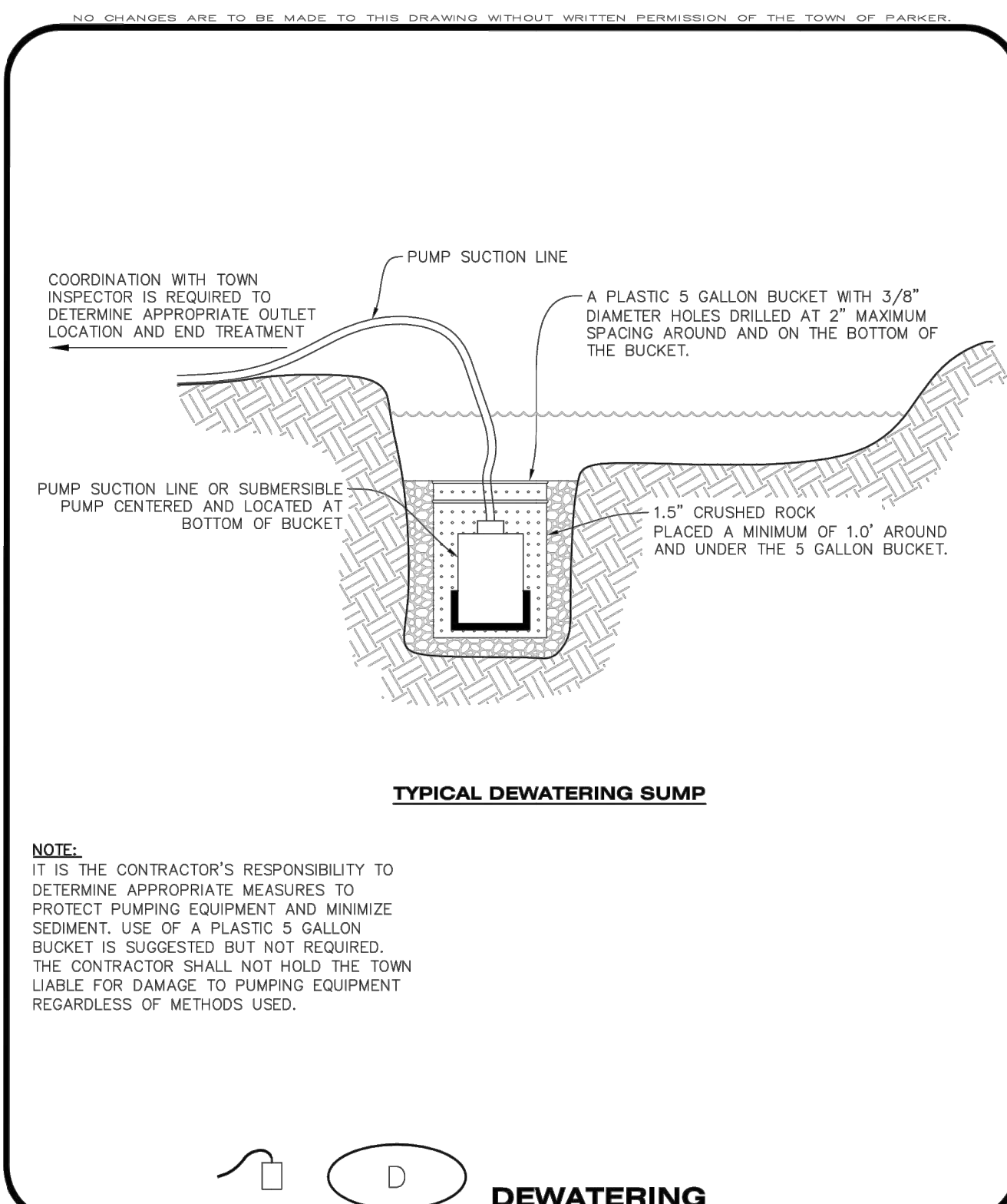
**CONCRETE WASHOUT AREA INSTALLATION NOTES**

1. CONCRETE WASHOUT AREAS SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
2. A VEHICLE TRACKING CONTROL PAD IS REQUIRED AT THE ACCESS POINT TO ALL CONCRETE WASHOUT AREAS WHEN NO OTHER VTC PAD EXISTS IN BETWEEN THE CWA AND THE ROADWAY.
3. IF GROUNDWATER IS ENCOUNTERED WHEN DIGGING THE PIT, A NEW LOCATION SHOULD BE SELECTED. IF NO OTHER LOCATION CAN BE FOUND, A ONE-PIECE IMPERVIOUS LINER SHALL BE REQUIRED ALONG THE BOTTOM AND SIDES OF THE SUBSURFACE PIT.

**CONCRETE WASHOUT AREA INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CONCRETE WASHOUT AREA.
2. CONCRETE WASHOUT MATERIALS SHALL BE REMOVED BEFORE ITS CAPACITY HAS BEEN REACHED.
3. CONCRETE WASHOUT AREAS SHALL BE ENLARGED AS NECESSARY TO MAINTAIN CAPACITY FOR WASTED CONCRETE AND ASSOCIATED WASH WATER.
4. CONCRETE WASHOUT WATER, WASTED PIECES OF CONCRETE AND ALL OTHER DEBRIS IN THE SUBSURFACE PIT SHALL BE TRANSPORTED FROM THE JOB SITE AS NECESSARY TO ENSURE ADEQUATE CAPACITY.
5. CONCRETE WASHOUT AREAS SHALL REMAIN IN PLACE UNTIL ALL CONCRETE FOR THE PROJECT IS PLACED.
6. WHEN CONCRETE WASHOUT AREAS ARE REMOVED, THE TOWN'S INSPECTOR MAY REQUIRE EXCAVATIONS TO BE FILLED WITH SUITABLE COMPACTED BACKFILL AND TOPSOIL, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE CONCRETE WASHOUT AREAS SHALL BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**CBMP** | **CWA**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013



**CBMP** | **D**  
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Oct. 2013

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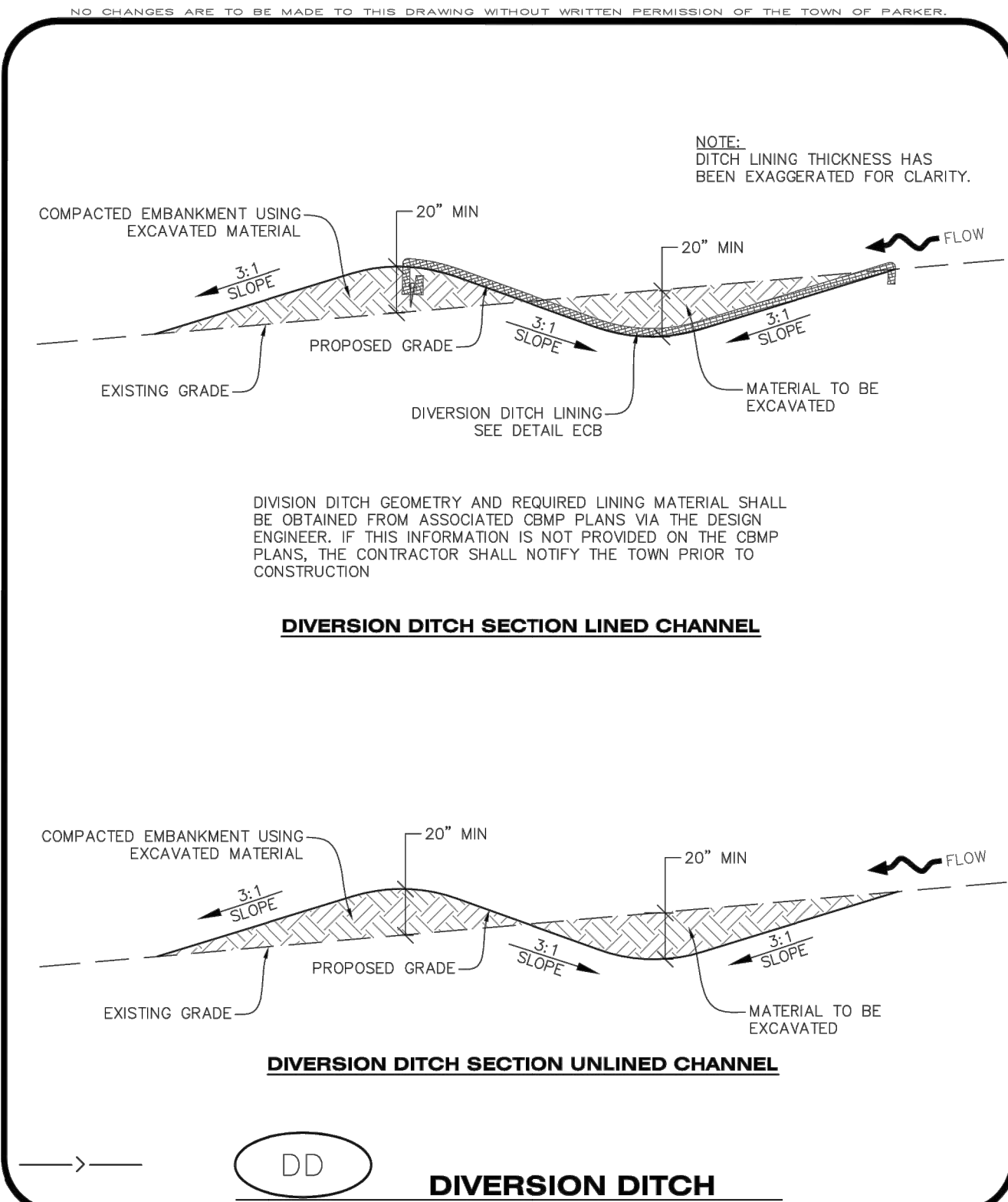
**DEWATERING INSTALLATION NOTES**

1. IT IS THE EROSION CONTROL SUPERVISOR'S RESPONSIBILITY TO ENSURE THAT ALL DEWATERING IS PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT (CDPHE).

**DEWATERING MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL INSPECT THE DEWATERING OPERATION TO ENSURE THAT THE DISCHARGE WATER IS DRAINING TO THE PROPER LOCATION(S) AND PERFORM ANY NECESSARY REPAIRS OR MAINTENANCE ON A FREQUENT BASIS.

**CBMP** | **D**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013



**CBMP** | **DD**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

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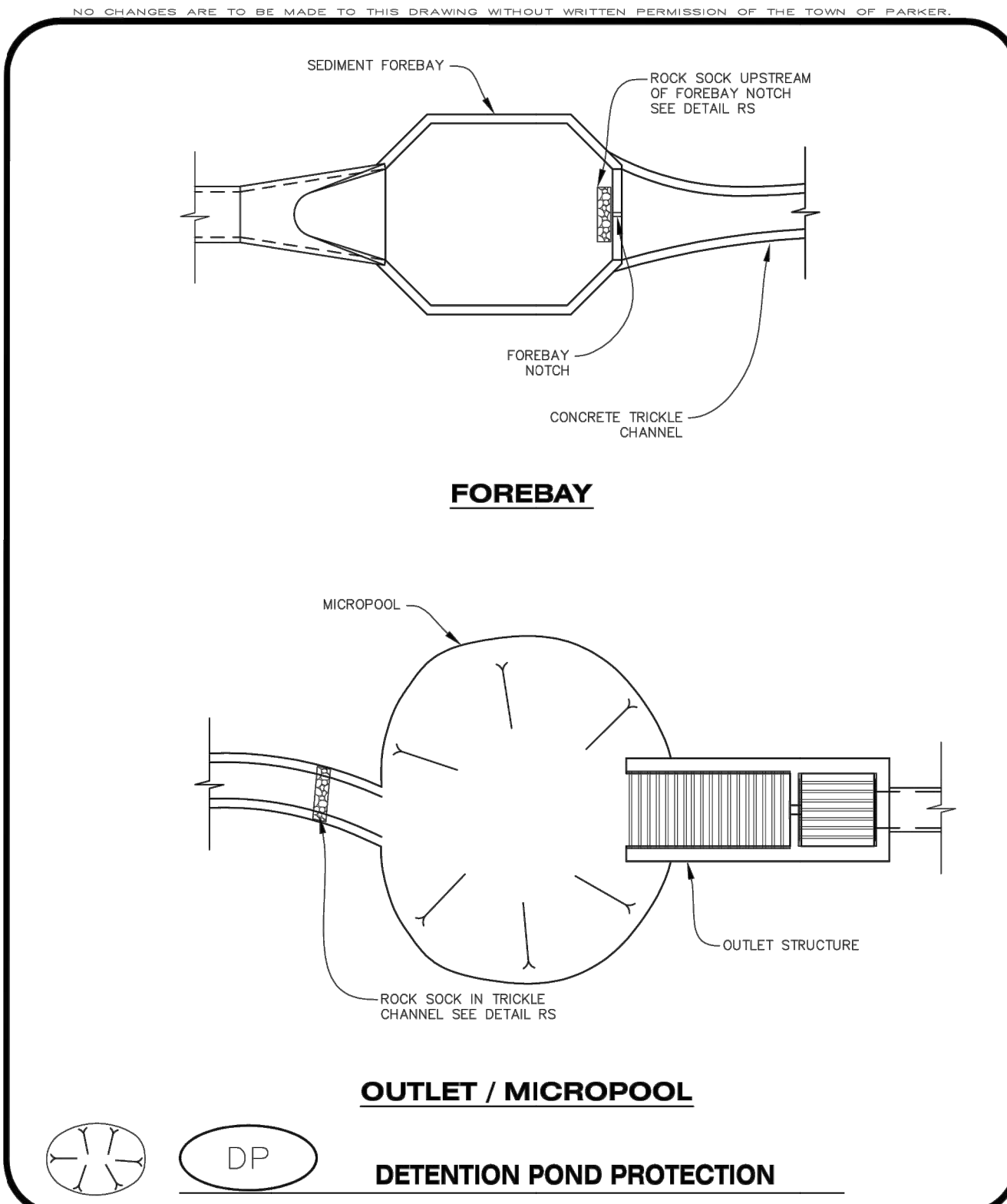
**DIVERSION DITCH INSTALLATION NOTES**

1. SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE DIVERSION DITCHES.
2. A PLASTIC LINER, RIPRAP, OR EROSION CONTROL BLANKET MAY BE NECESSARY TO PROTECT THE DIVERSION DITCH. THE REQUIRED LINING MATERIAL SHALL BE OBTAINED FROM THE CBMP PLANS VIA THE DESIGN ENGINEER.
3. ALL MATERIAL EXCAVATED FROM THE DITCH MAY BE USED TO CONSTRUCT THE BERM ON THE DOWNHILL SIDE OF THE DITCH.
4. THE DIVERSION DITCH SHALL BE A MINIMUM OF 20" DEEP WITH APPROX. 3:1 SIDE SLOPES. THE ADJACENT BERM SHALL BE A MINIMUM OF 20" IN HEIGHT WITH APPROX. 3:1 SIDE SLOPES. ALL EMBANKMENTS SHALL BE FIRMLY COMPACTED.
5. THE DISCHARGE FROM THE DIVERSION DITCH SHALL BE DIRECTED TOWARDS AN APPROPRIATELY SIZED TEMPORARY SEDIMENT BASIN OR OTHER APPROVED AREA.

**DIVERSION DITCH INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE DIVERSION DITCH.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 (10") THE CREST HEIGHT.
3. DIVERSION DITCHES SHALL BE RE-GRADED FOLLOWING THE SIGNS OF MODERATE OR MORE SOIL EROSION OR ANY DAMAGE.
4. DIVERSION DITCHES ARE TO REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR. ALTERNATIVELY, THE DIVERSION DITCHES MAY BE REMOVED WHEN THE SITE'S TOPOGRAPHY CHANGES SUCH THAT SIGNIFICANT RUNOFF IS NO LONGER POSSIBLE. IN SOME INSTANCES, THE DIVERSION DITCHES MAY REMAIN IN PLACE PERMANENTLY.

**CBMP** | **DD**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013



**CBMP** | **DP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

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**DETENTION POND PROTECTION INSTALLATION NOTES**

1. DETENTION POND PROTECTION SHALL BE INSTALLED IMMEDIATELY FOLLOWING THE CONSTRUCTION OF THE TRICKLE CHANNEL AND FOREBAY.
2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
3. ROCK SOCK FOR OUTLET STRUCTURE AND FOREBAY PROTECTION SHALL BE ONE CONTINUOUS PIECE (SEE DETAIL RS).

**DETENTION POND PROTECTION INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE OUTLET STRUCTURE PROTECTION.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED IMMEDIATELY.
3. OUTLET STRUCTURE PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** | **DP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
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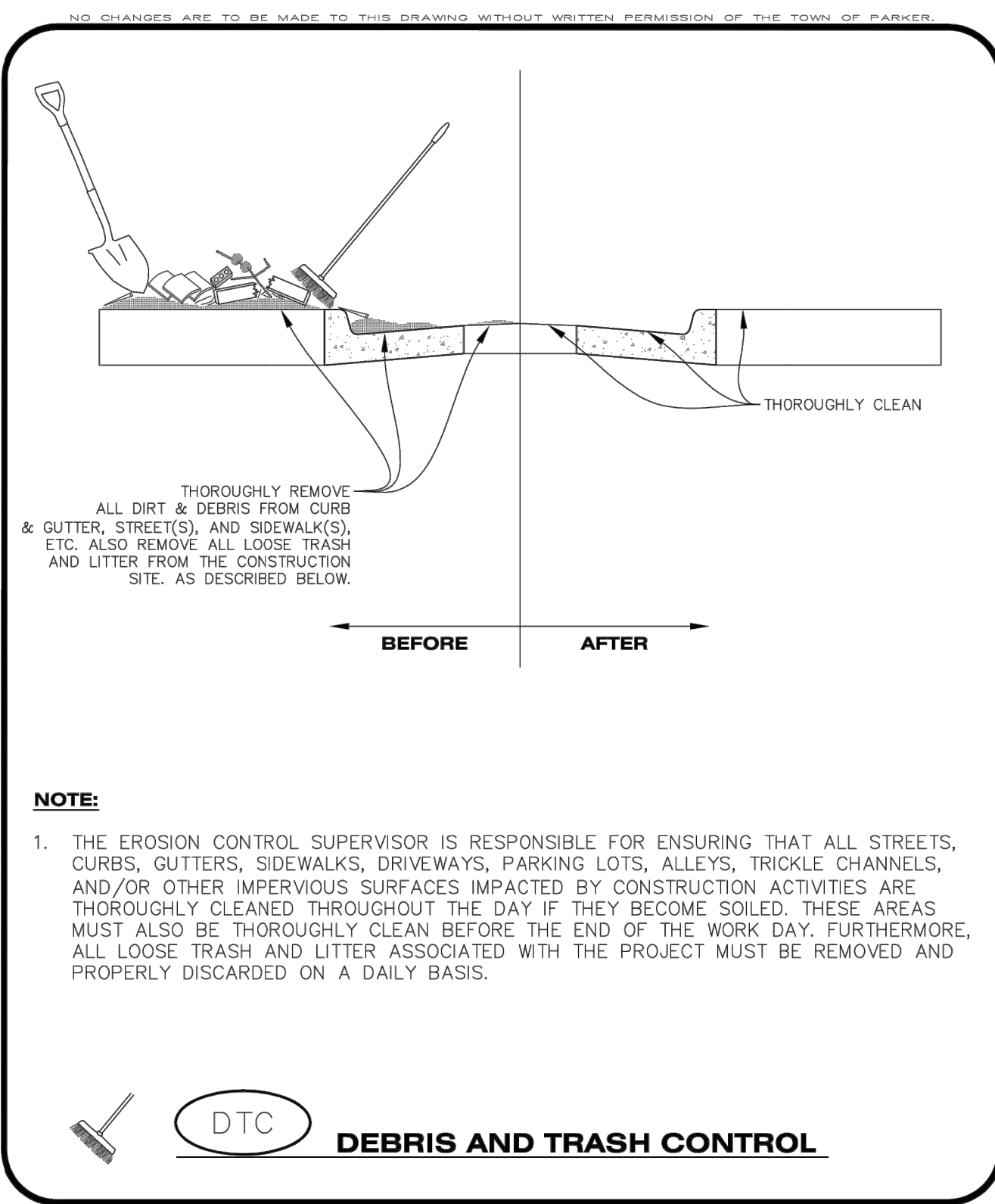
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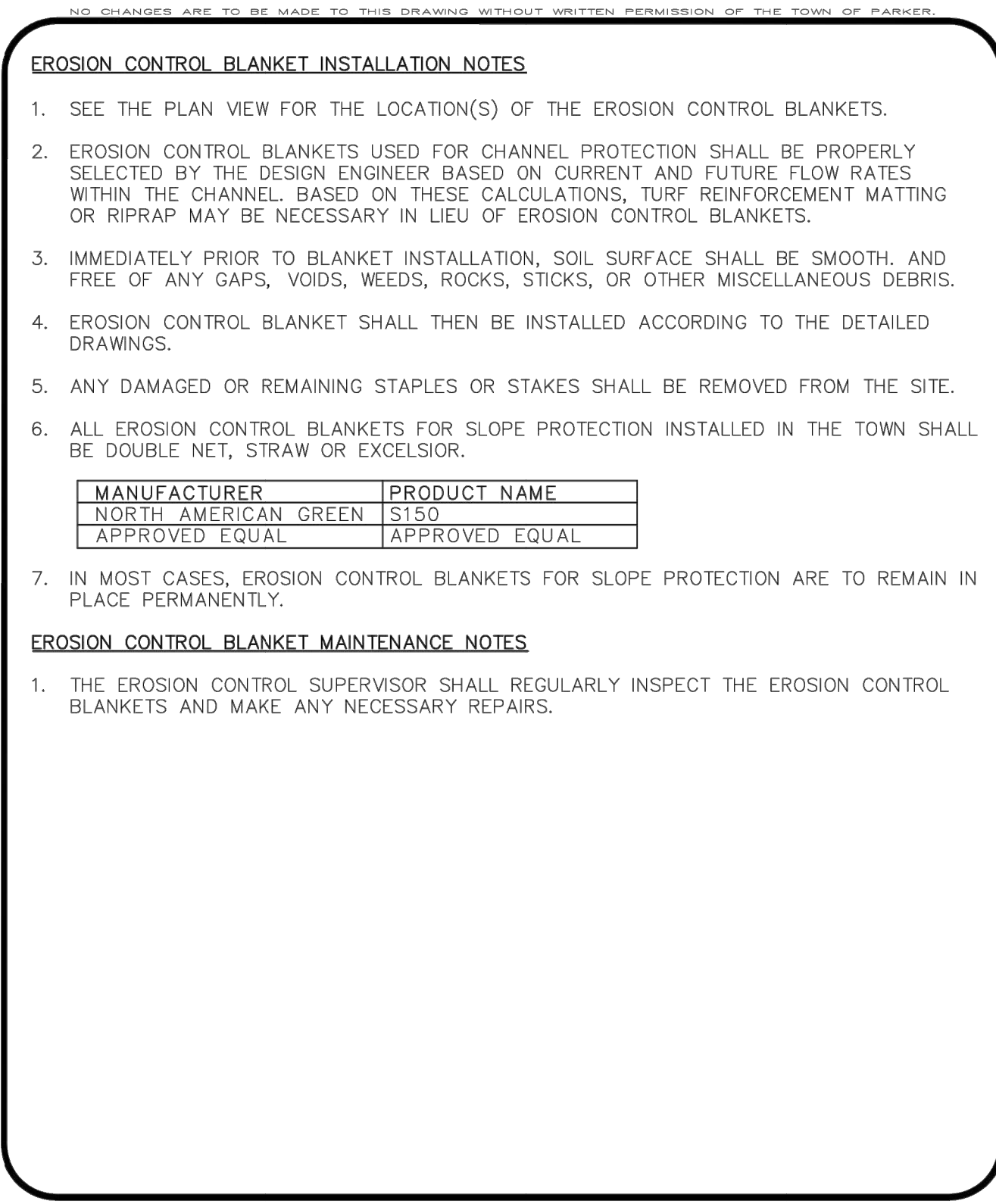
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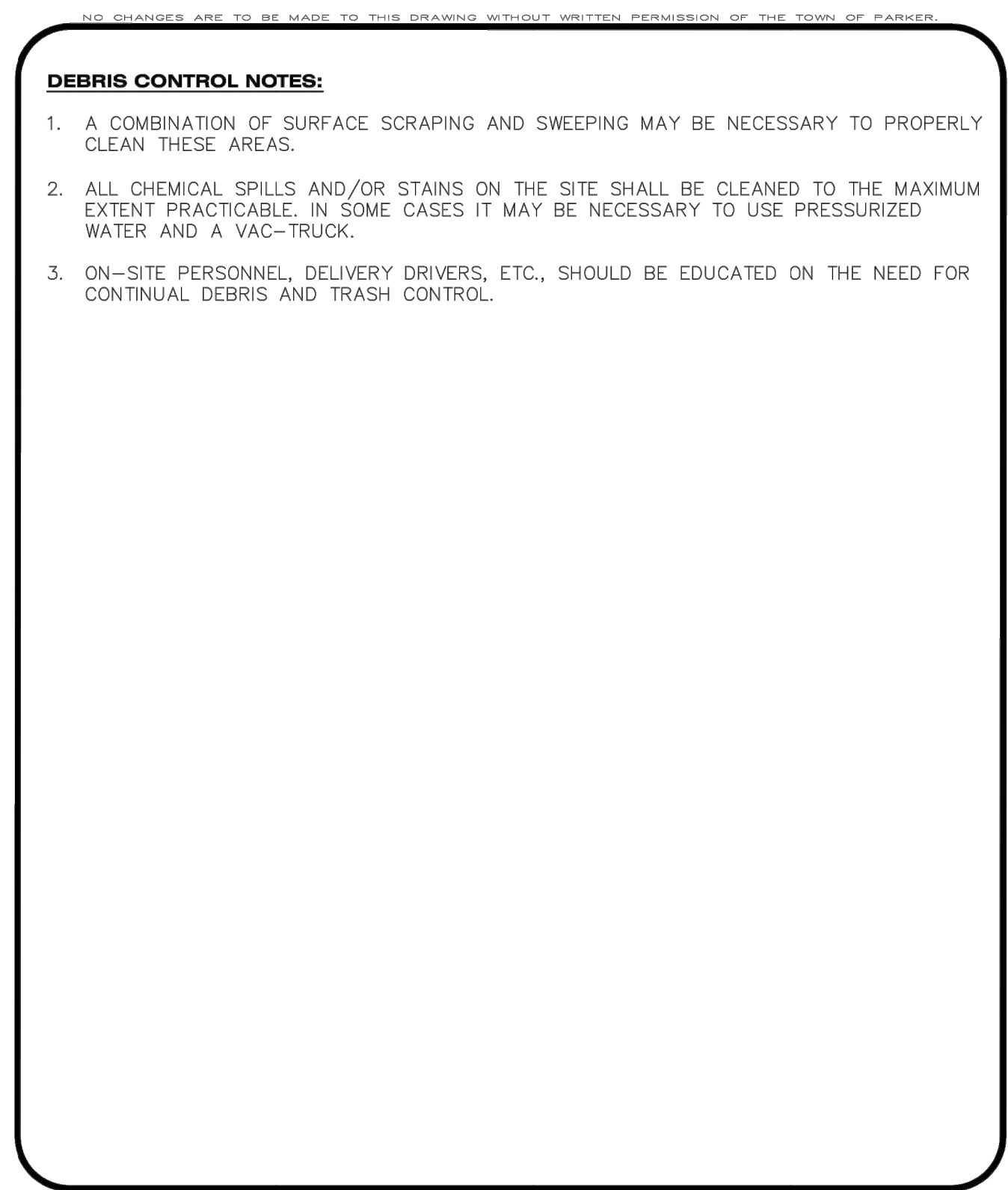




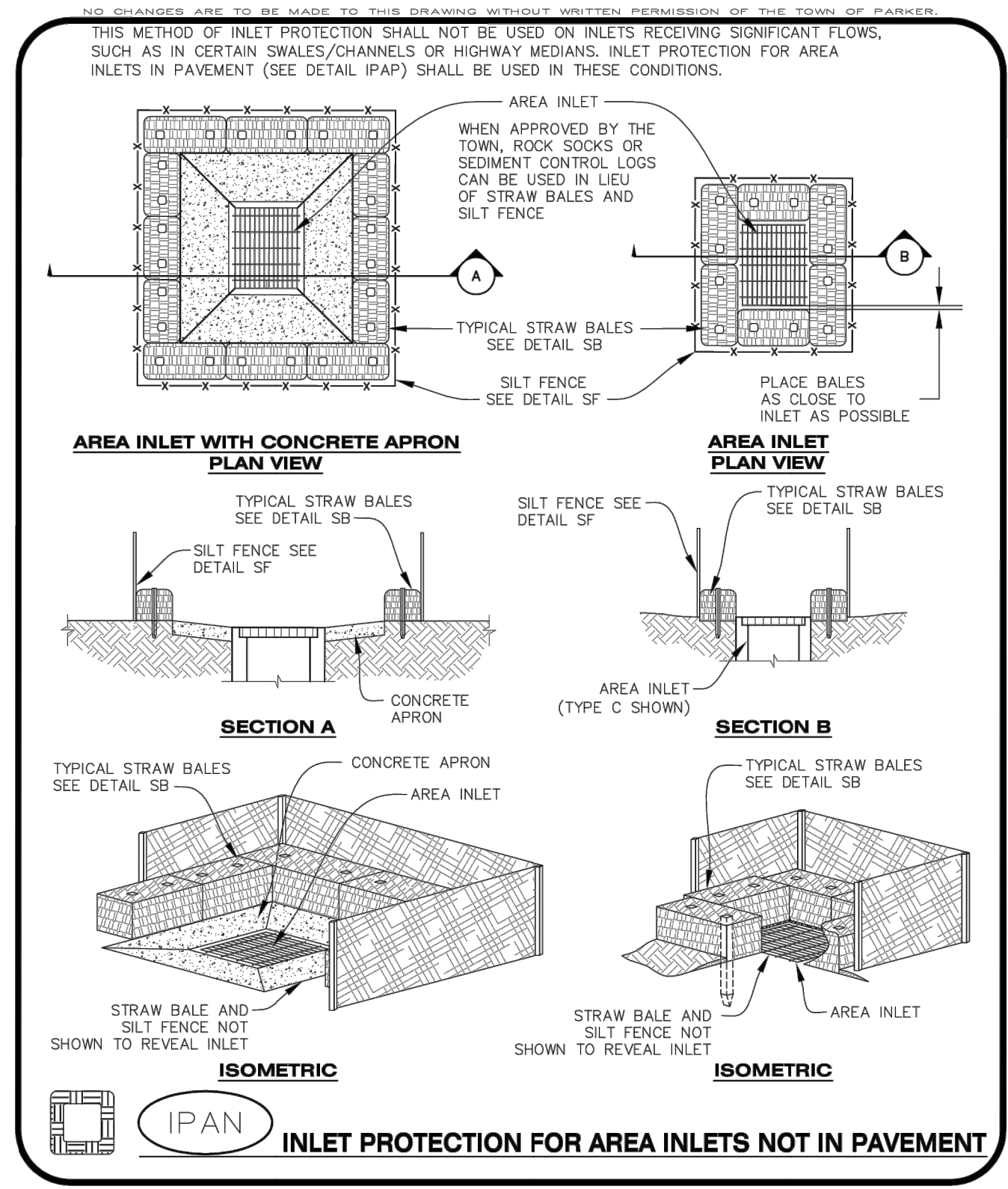
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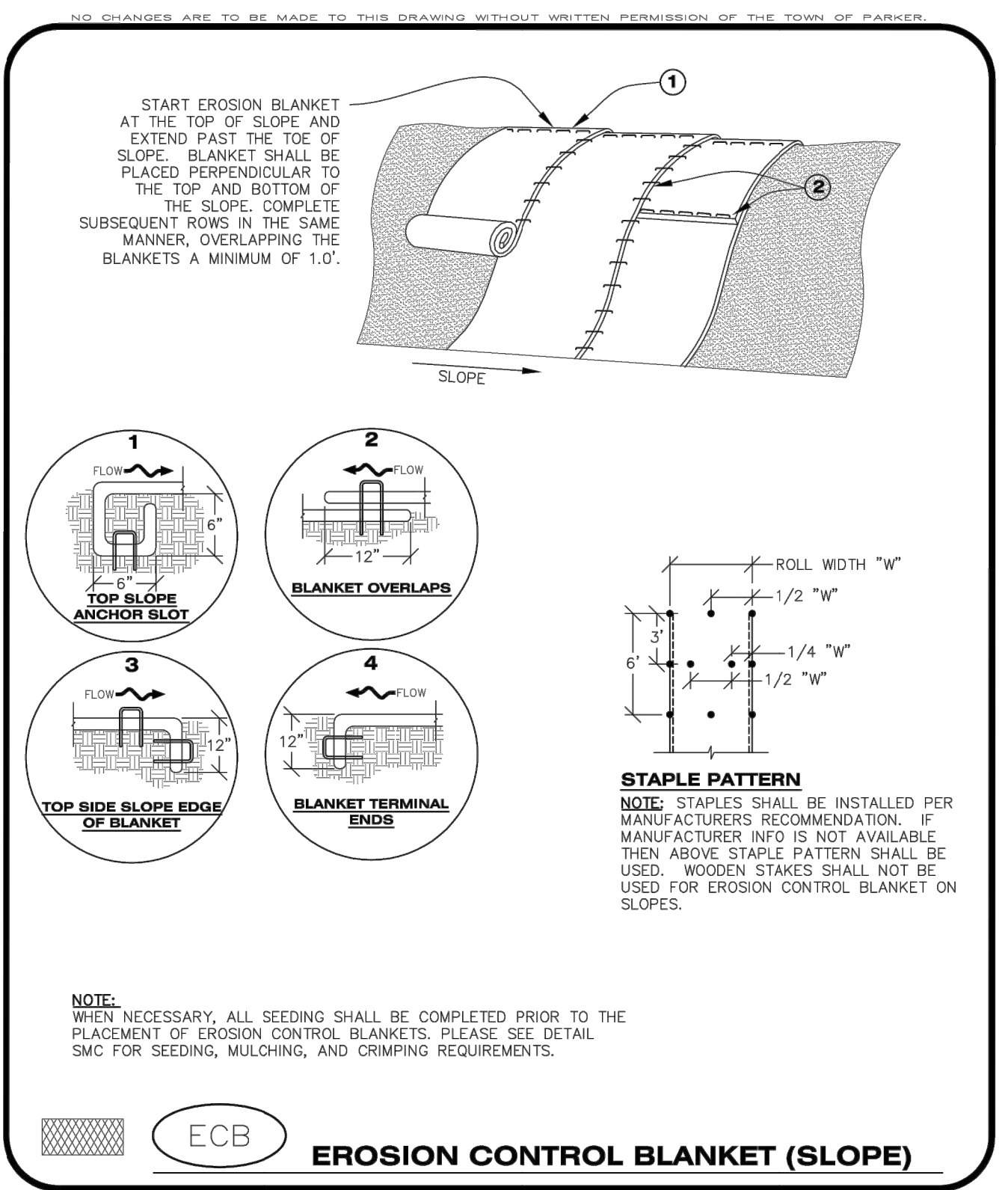
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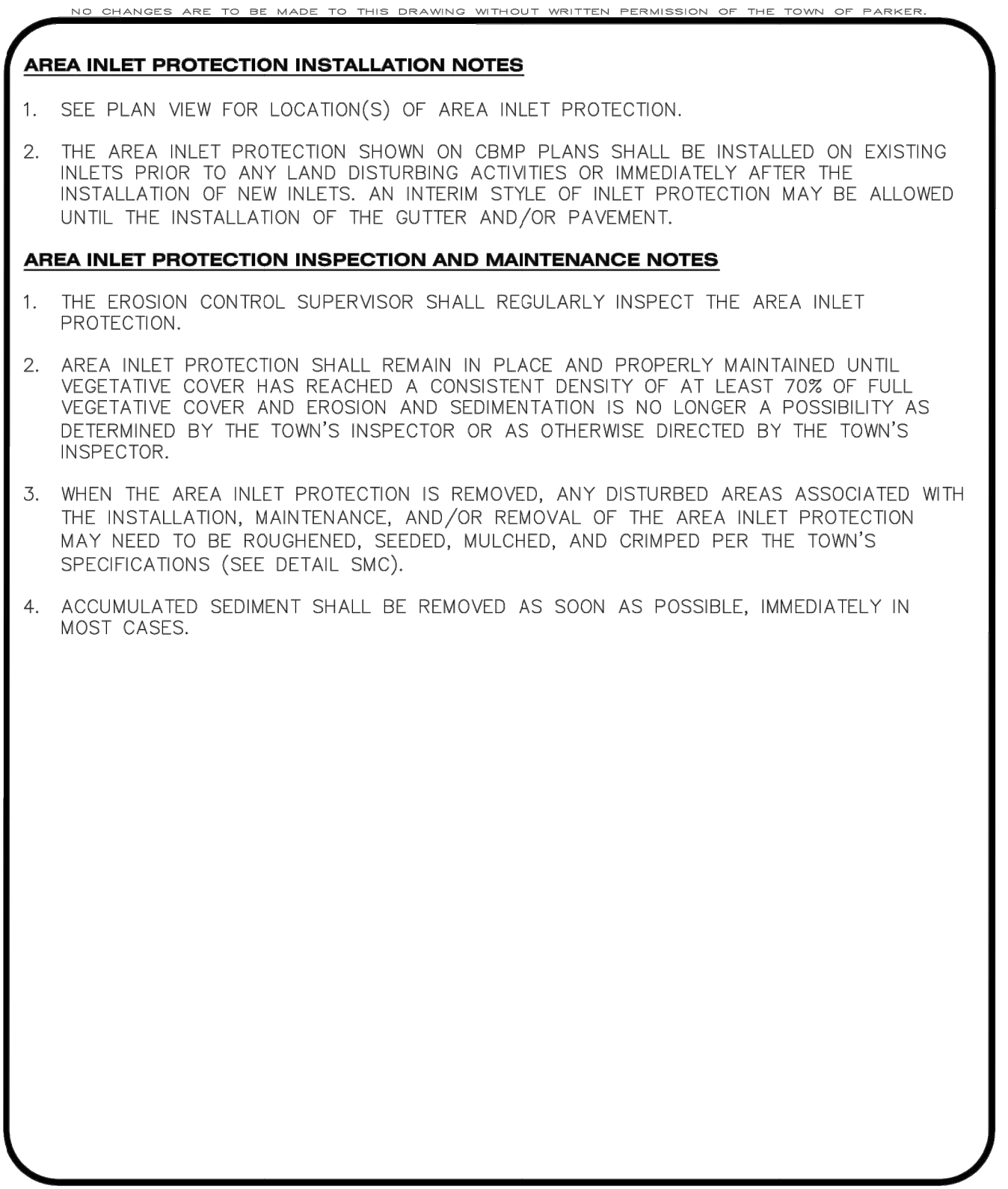
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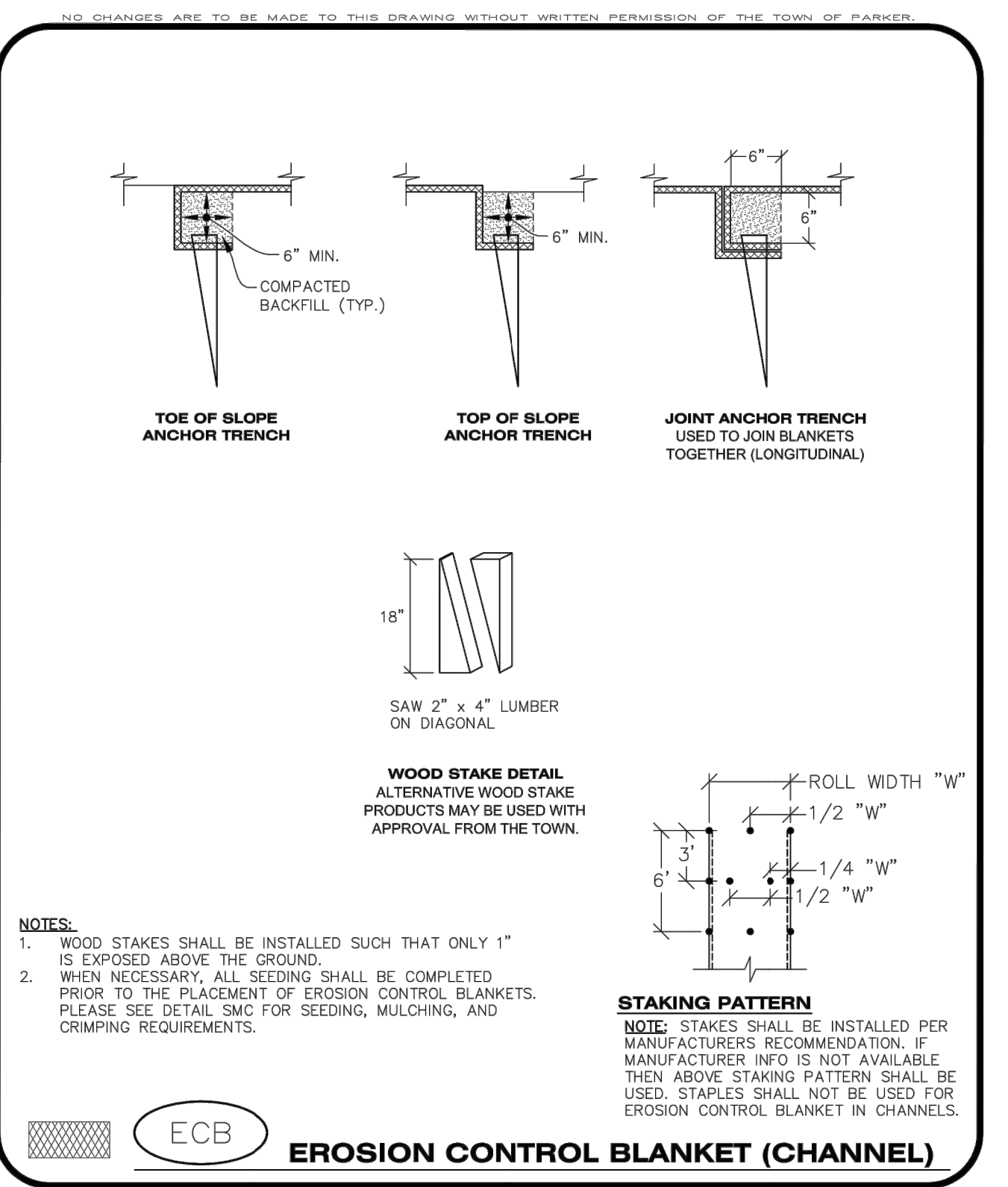
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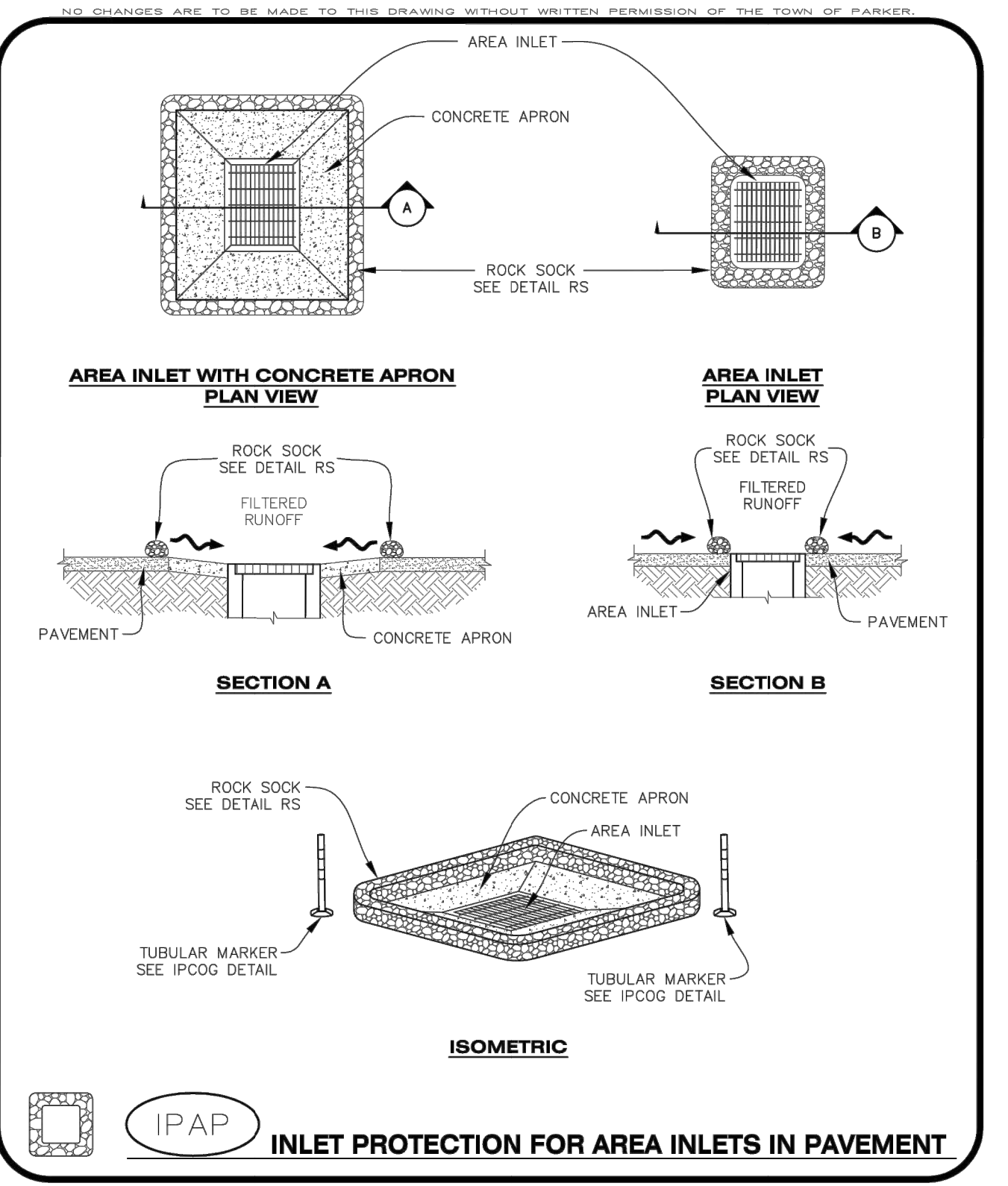
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Oct. 2013



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CHECKED BY: DLS  
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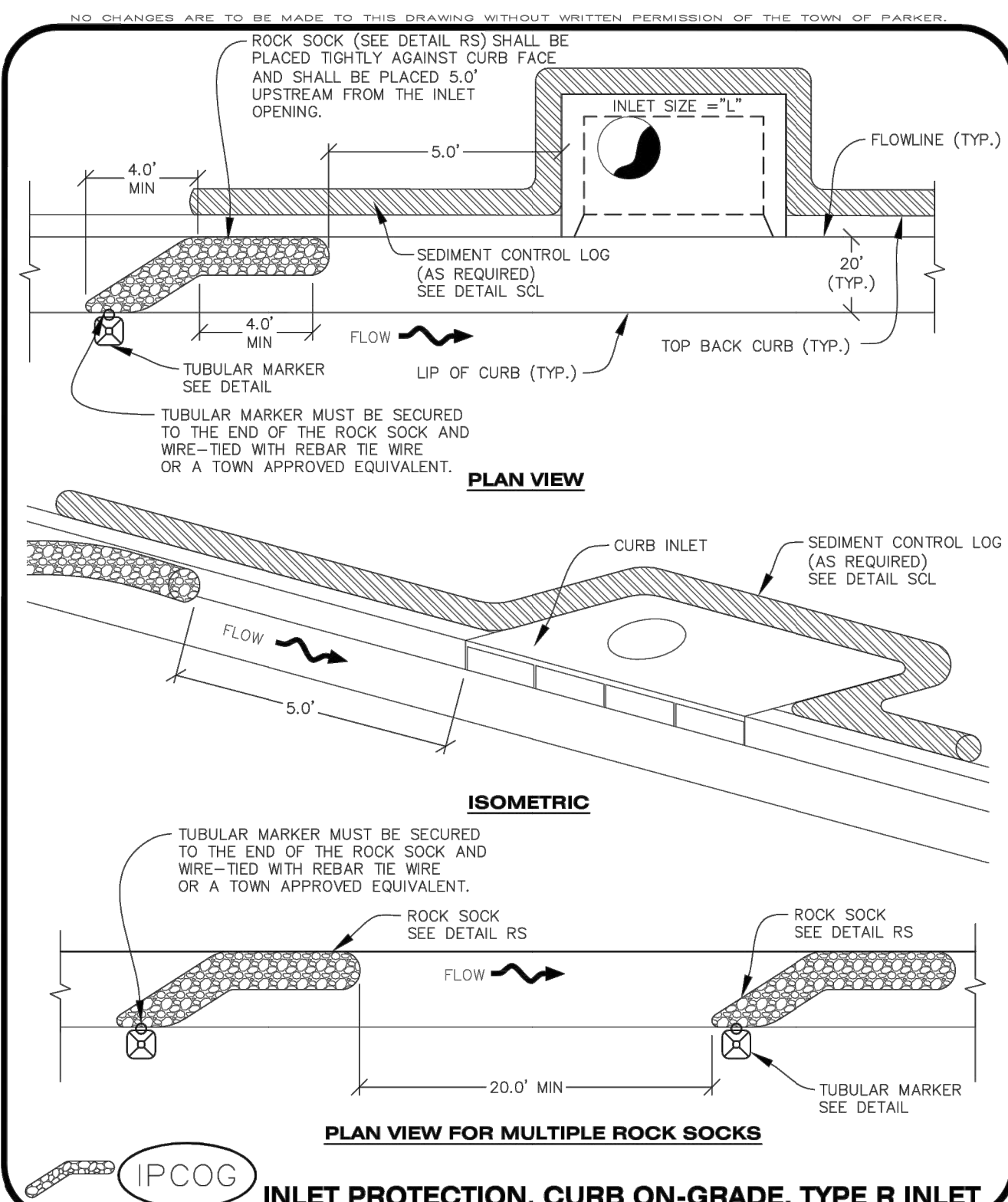
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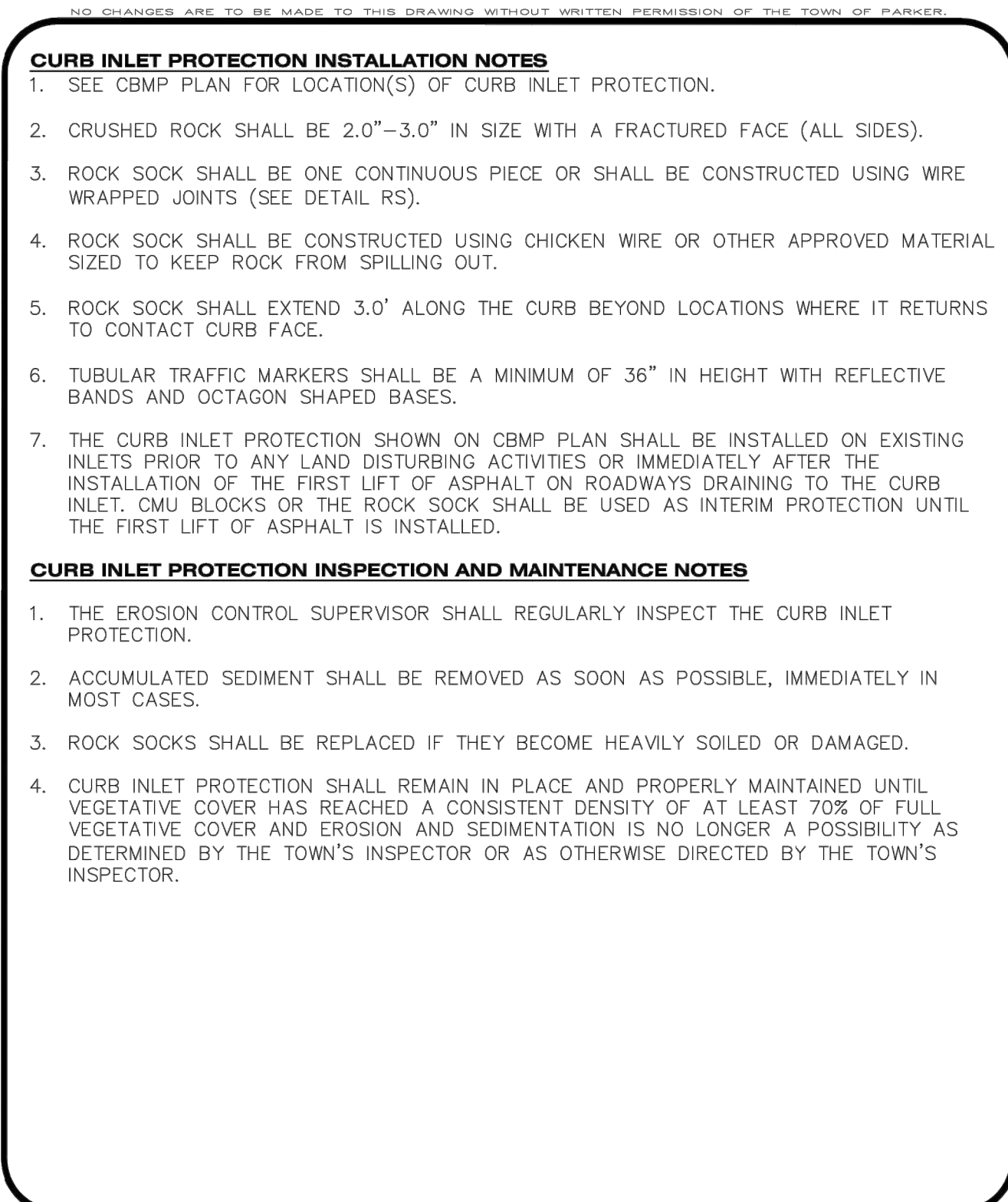


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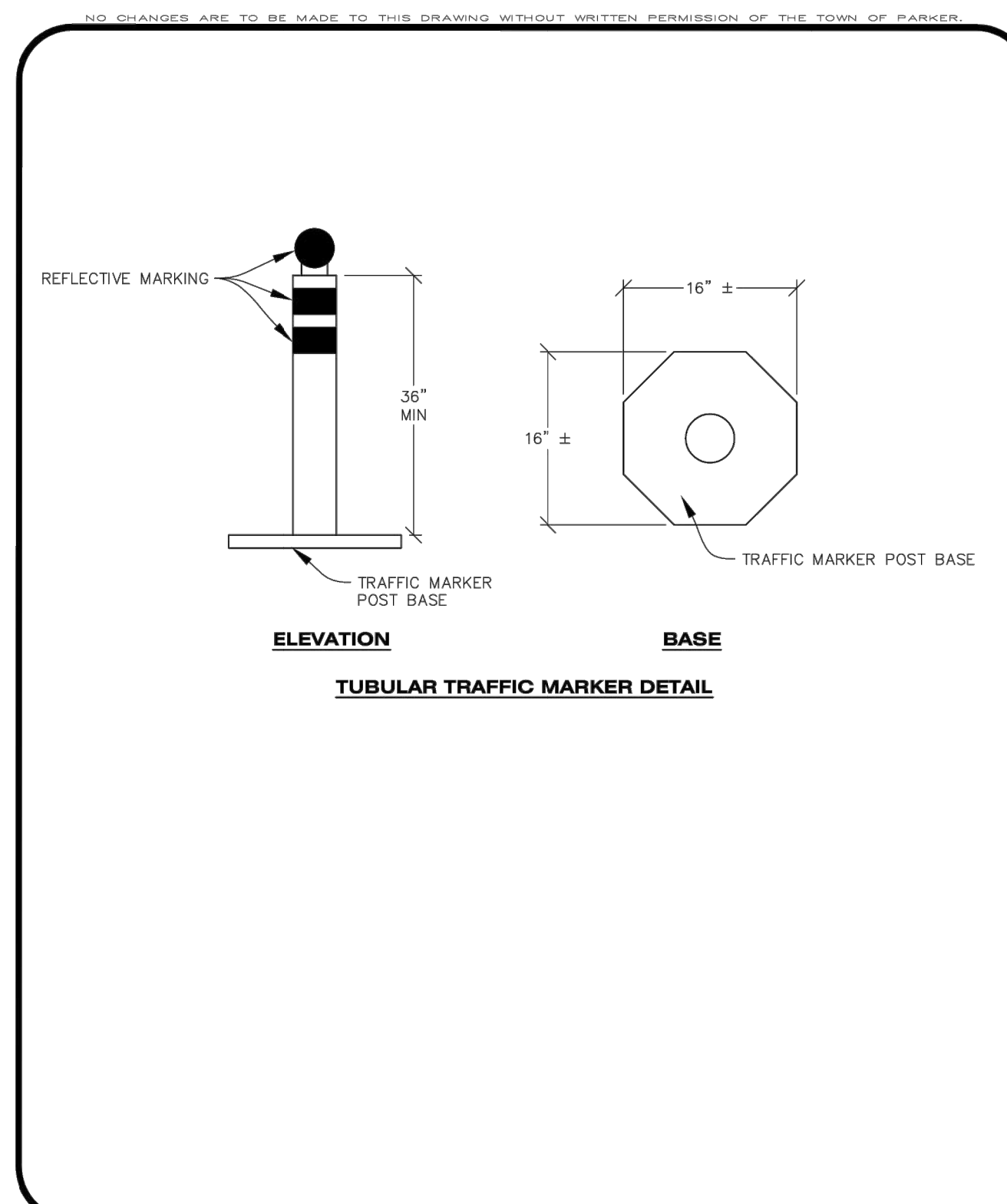
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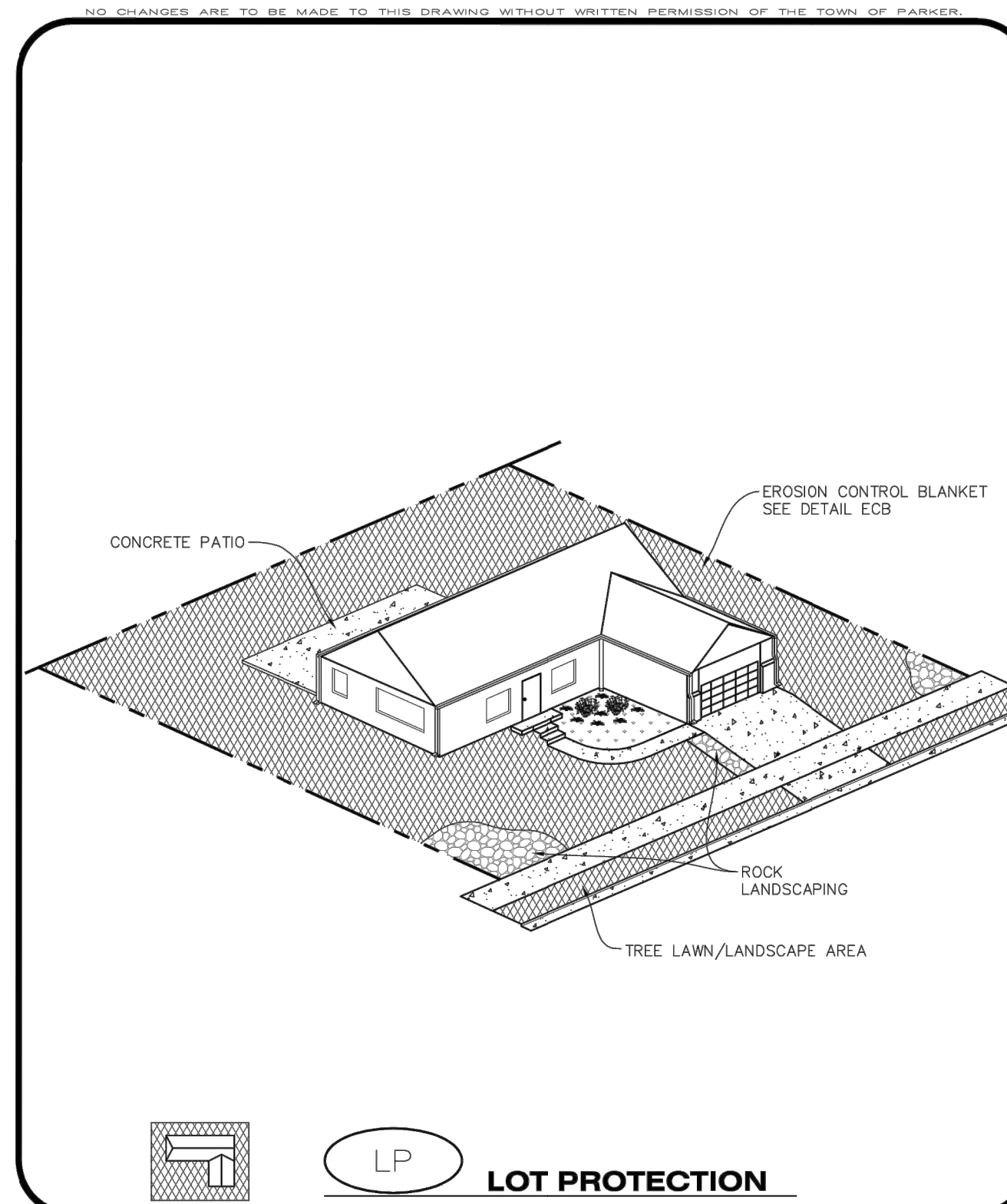
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 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
 Oct. 2013



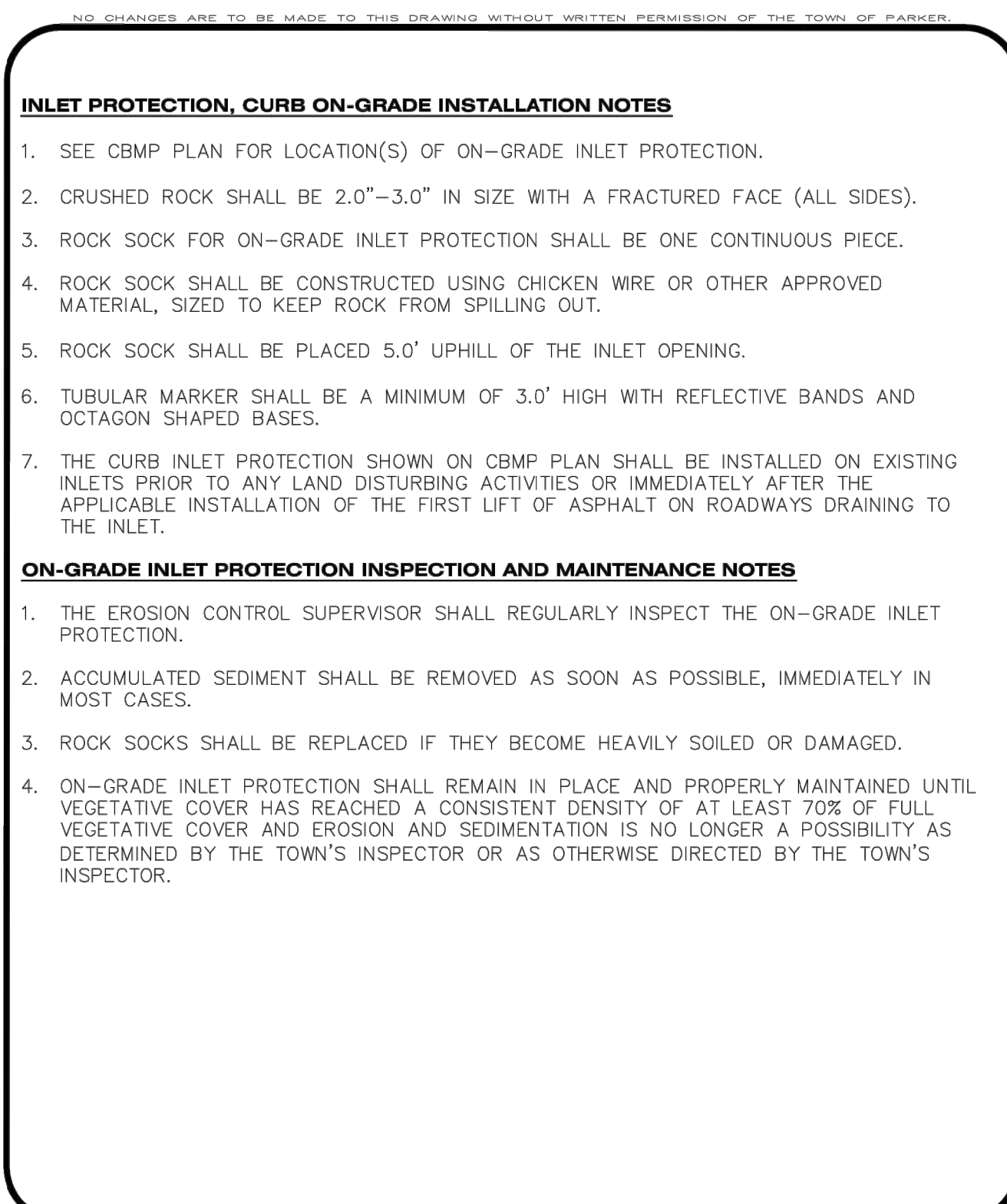
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 Oct. 2013



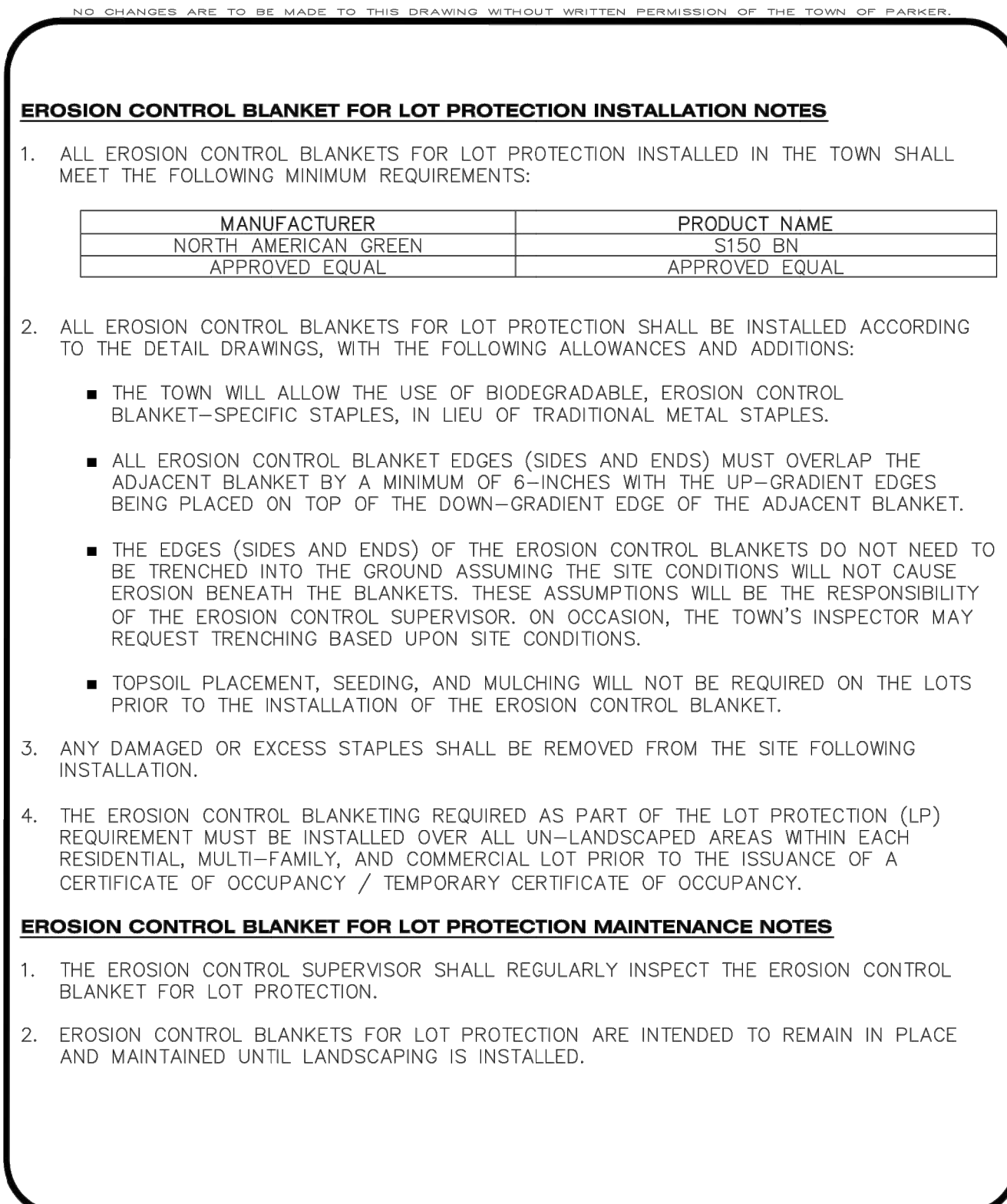
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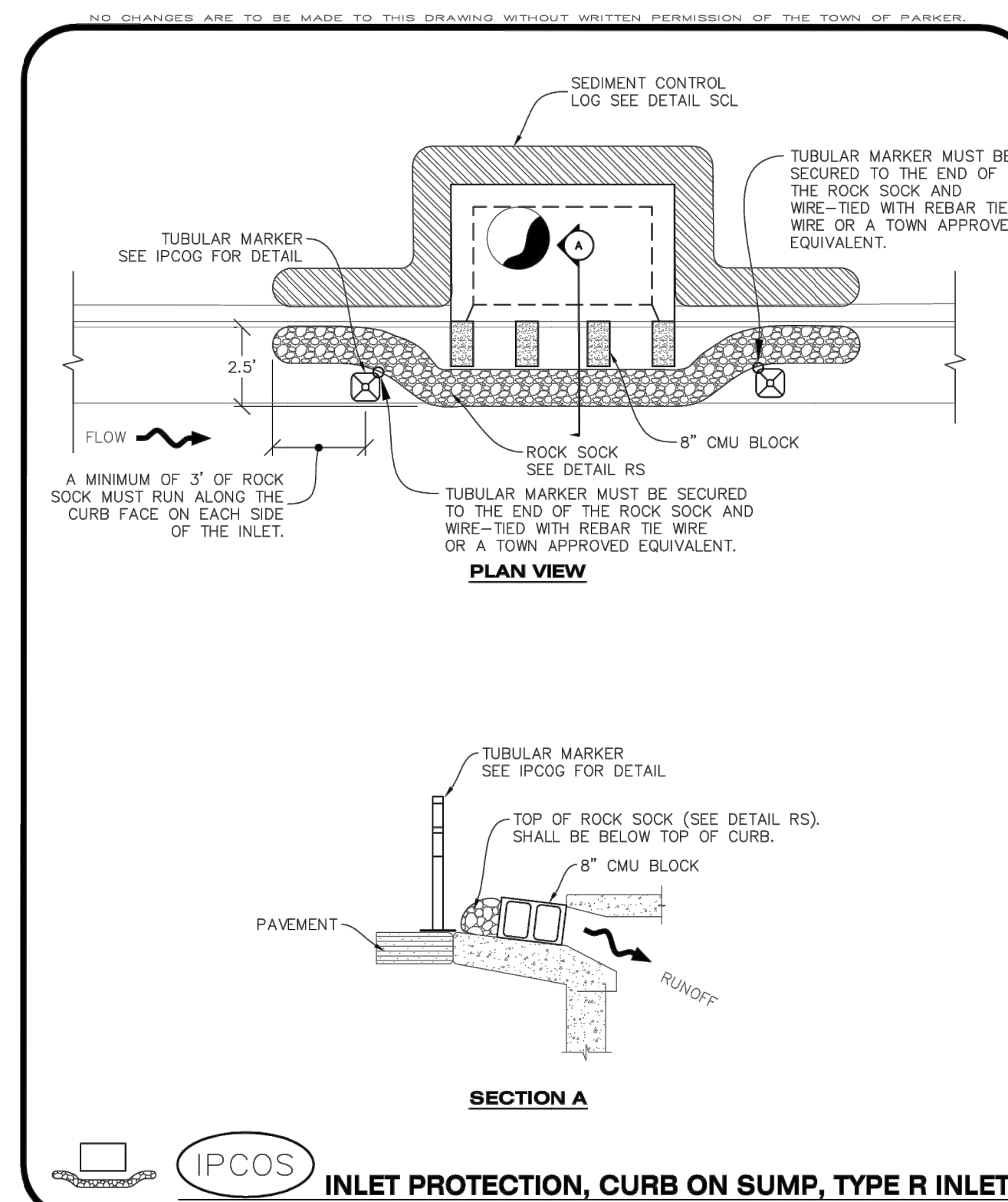
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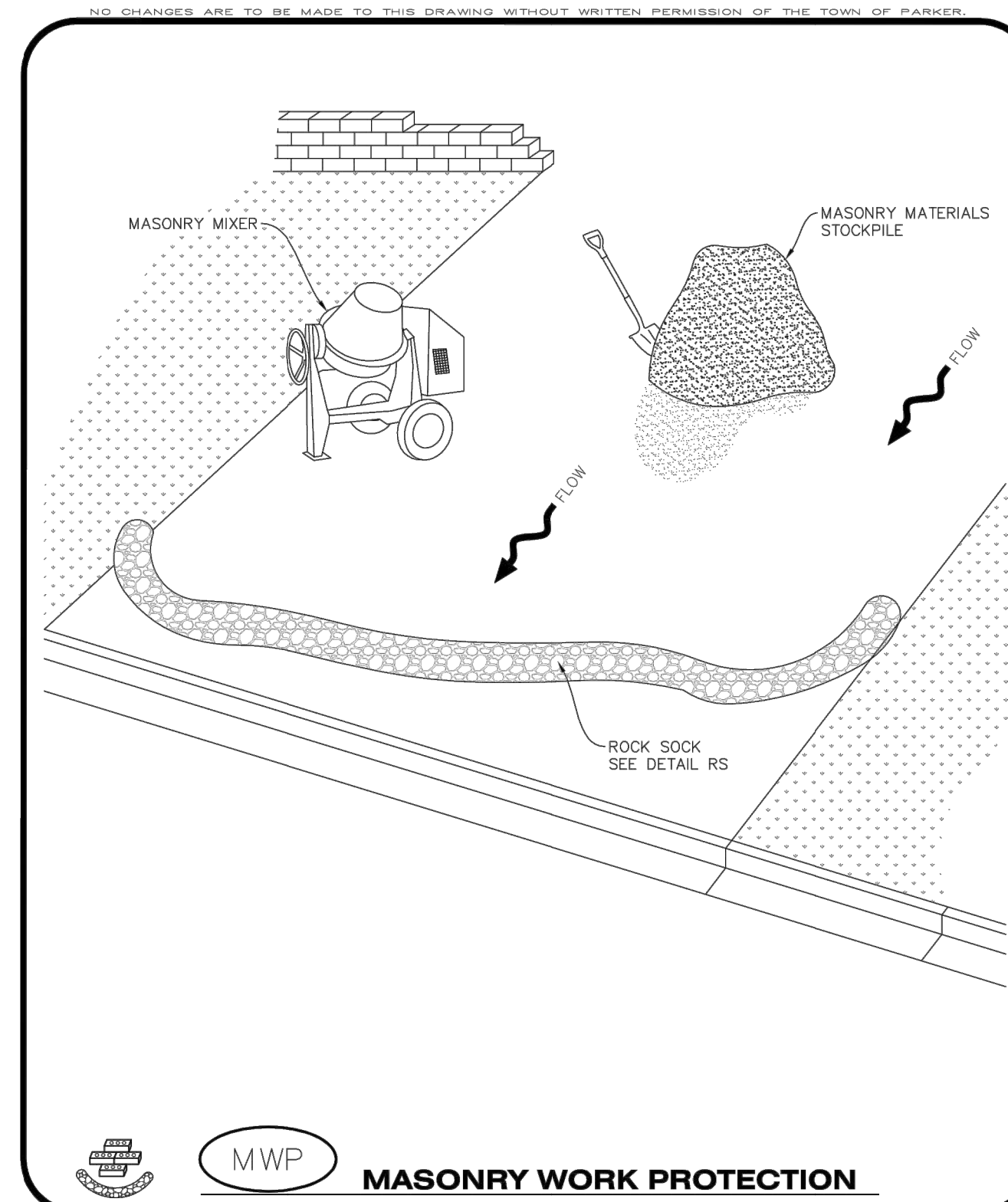
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 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
 Oct. 2013



**CBMP** | **LP**  
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 Oct. 2013



**CBMP** | **IPCOS**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013



**CBMP** | **MWP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
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 CHECKED BY: DLS  
 DATE: 11/14/19

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 CONSTRUCTION DOCUMENTS  
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**MASONRY WORK PROTECTION INSTALLATION NOTES**

- MASONRY WORK PROTECTION MAY NEED TO BE INSTALLED WHEN MASONRY WORK AND MIXING IS OCCURRING.
- A ROCK SOCK SHALL BE INSTALLED IN A CRESCENT SHAPE ON THE DOWNHILL SIDE OF THE MASONRY WORK AND MIXER.
- CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.

**MASONRY WORK PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE MASONRY WORK PROTECTION.
- ALL CONCRETE WASTE SHALL BE REGULARLY CLEANED AND PLACED IN THE CONCRETE WASH OUT AREA.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.

**CBMP** | **MWP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

CONTRACTOR SHALL ANCHOR PORTABLE TOILET TO THE GROUND, AT A MINIMUM OF TWO OPPOSING CORNERS (ON A DIAGONAL) USING U-SHAPED REBAR STAKES

**PORTABLE TOILET PROTECTION**

**CBMP** | **PTP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**PORTABLE TOILET PROTECTION INSTALLATION NOTES**

- PORTABLE TOILETS SHALL BE PLACED A MINIMUM OF 10.0' BEHIND ALL CURBS, SIDEWALKS, AND OTHER IMPERVIOUS AREAS.
- ALL PORTABLE TOILETS MUST BE GROUPED TOGETHER.
- PORTABLE TOILETS SHALL BE SECURELY ANCHORED TO THE GROUND USING U-SHAPED REBAR STAKES.
- U-SHAPED REBAR STAKES SHALL BE POSITIONED ON AT LEAST 2 OPPOSING (DIGITAL) CORNERS.

**PORTABLE TOILET PROTECTION INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE PORTABLE TOILET PROTECTION.
- PORTABLE TOILETS SHALL BE SERVICED AT THE NECESSARY INTERVALS TO ELIMINATE THE POSSIBILITY OF OVERFLOW.
- WHEN THE PORTABLE TOILETS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE TOILETS MAY NEED TO BE LANDSCAPED OR ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).
- PORTABLE TOILETS THAT ARE NOT CONSISTENTLY MAINTAINED IN ACCORDANCE WITH THESE REQUIREMENTS MAY NEED TO BE CLUSTERED TOGETHER, IN ONE CENTRALIZED LOCATION IN ORDER TO INCREASE COMPLIANCE AND REDUCE THE CHANCE OF A SPILL.

**CBMP** | **PTP**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**ROUGH CUT STREET CONTROL**

**CBMP** | **RCSC**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**ROUGH CUT STREET CONTROL INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROUGH CUT STREET CONTROL.
- THE SPACING OF THE ROUGH CUT STREET CONTROL MAY BE DETERMINED BY THE DESIGN ENGINEER AND SHOWN ON THE CBMP PLAN.

**ROUGH CUT STREET CONTROL INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROUGH CUT STREET CONTROL.
- ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN THE SEDIMENT DEPTH IS 1/2 THE HEIGHT OF THE ROCK SOCK.
- ROUGH CUT STREET CONTROL SHALL BE REPAIRED IMMEDIATELY FOLLOWING ANY SIGN OF WEAR OR ALTERATION OF THE ORIGINAL SHAPE AND DIMENSIONS.
- ROUGH CUT STREET CONTROL SHALL BE KEPT IN PLACE AND MAINTAINED UNTIL SUB-GRADE PREPARATION BEGINS FOR PAVING. AT THAT POINT, THE RCSC SHOULD BE REMOVED IN INCREMENTS BASED ON SUBGRADE PREPARATION.

**CBMP** | **RCSC**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**ROCK SOCK**

**CBMP** | **RS**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**ROCK SOCK INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF ROCK SOCK.
- CRUSHED ROCK SHALL BE APPROXIMATELY 2.0"-3.0" GRANITE IN SIZE WITH A FRACTURED FACE (ALL SIDES).
- ROCK SOCK SHALL BE APPROXIMATELY ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).
- ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.
- MINIMUM ROCK SOCK DIAMETER SHALL VARY BASED ON APPLICATION (7" MIN).
- TUBULAR MARKERS MAY NEED TO BE USED IN CONJUNCTION WITH ROCK SOCKS ANYTIME THE ROCK SOCK IS PLACED ON A ROADWAY, SIDEWALK, PARKING LOT OR OTHER LOCATION SUSCEPTIBLE TO VEHICLE OR PEDESTRIAN TRAFFIC. TUBULAR MARKERS SHALL CONFORM TO THE TUBULAR MARKER DETAIL.

**ROCK SOCK INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ROCK SOCKS.
- ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.
- ROCK SOCKS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CBMP** | **RS**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**ROCK SOCK IN SWALE**

**CBMP** | **RSS**  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 1  
Oct. 2013

NO.	REVISION	BY	DATE

**Kimley-Horn**  
 2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**CBMP DETAILS**

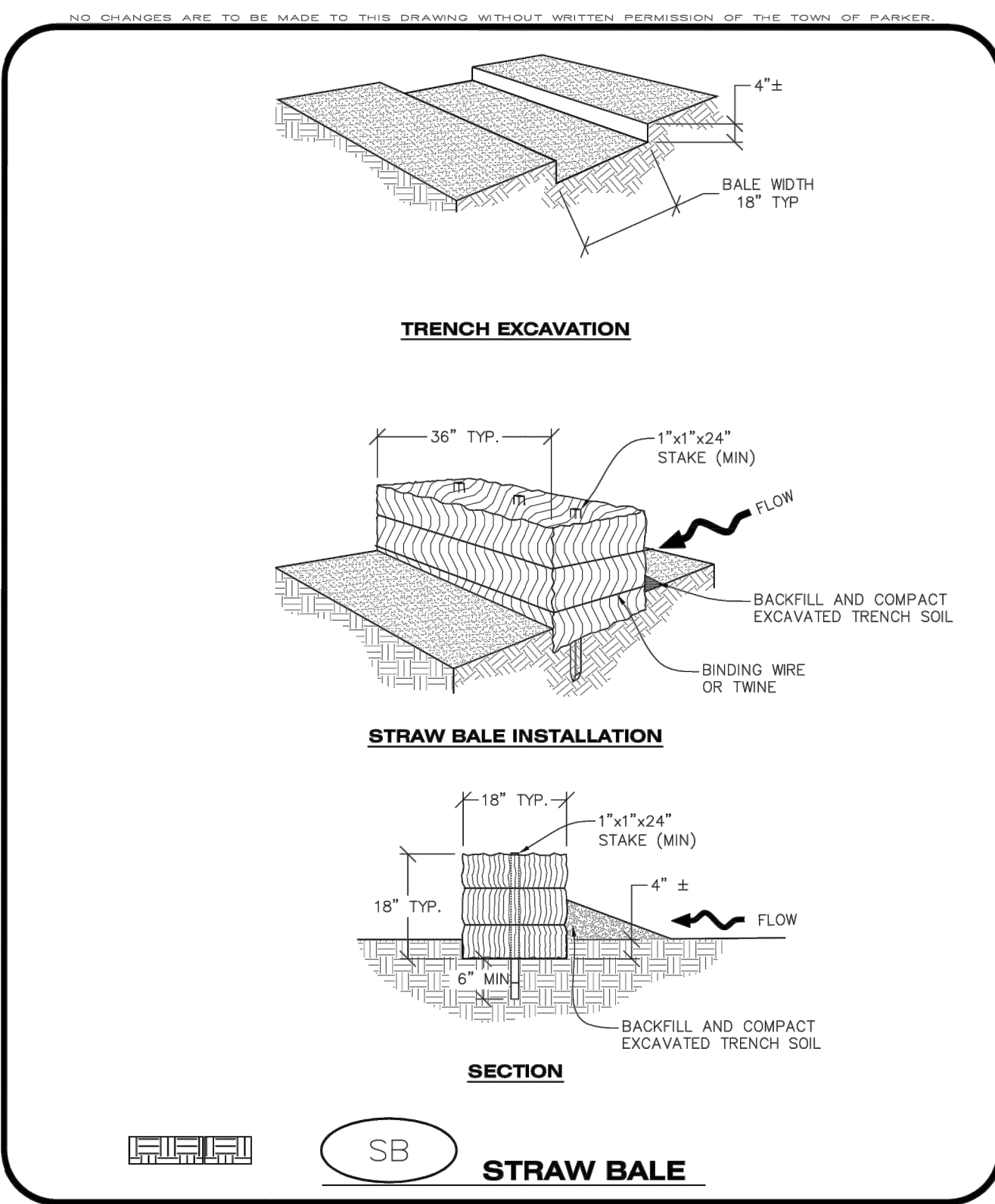
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 Kimley-Horn and Associates, Inc.

PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001EC\_DT  
**C3.7**



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 THIS DOCUMENT IS THE PROPERTY OF KIMLEY-HORN AND ASSOCIATES, INC. AND IS TO BE USED ONLY FOR THE PROJECT AND LOCATION SPECIFICALLY IDENTIFIED HEREIN. ANY REUSE OR MODIFICATION OF THIS DOCUMENT WITHOUT WRITTEN PERMISSION OF KIMLEY-HORN AND ASSOCIATES, INC. SHALL BE WITHOUT LIABILITY TO KIMLEY-HORN AND ASSOCIATES, INC.



**CBMP** | SB  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
 Oct. 2013

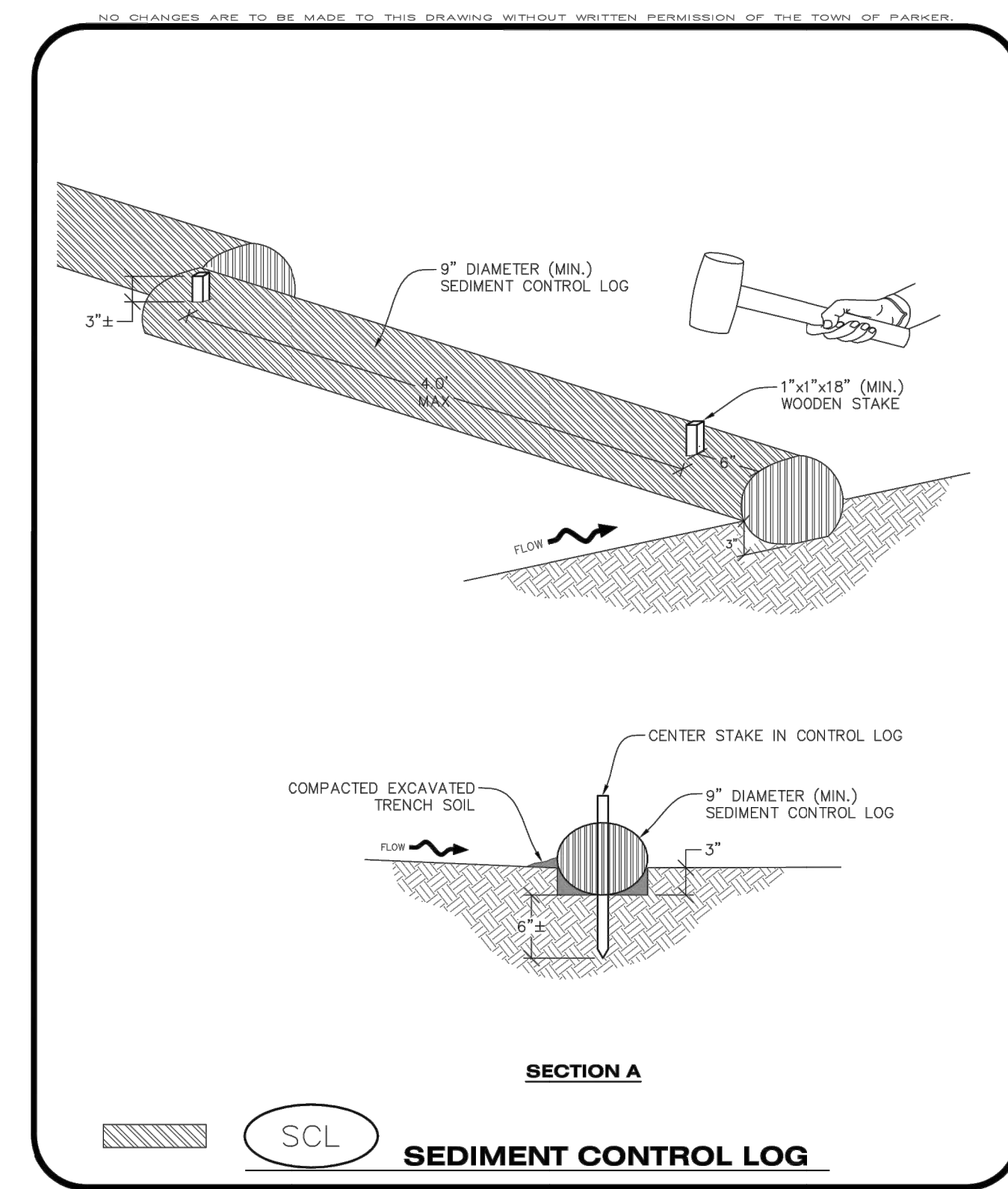
**STRAW BALE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF STRAW BALES.
- TYPICAL STRAW BALES SHALL BE APPROXIMATELY 36"x18"x18".
- TWO (2) WOODEN STAKES SHALL BE USED TO HOLD EACH BALE IN PLACE. WOODEN STAKES SHALL BE A MINIMUM OF 1"x1"x24".
- WOODEN STAKES SHALL BE PLACED APPROXIMATELY 6" INTO THE GROUND.
- STRAW BALES SHALL BE SPACED AND POSITIONED ACCORDING TO DETAILS.

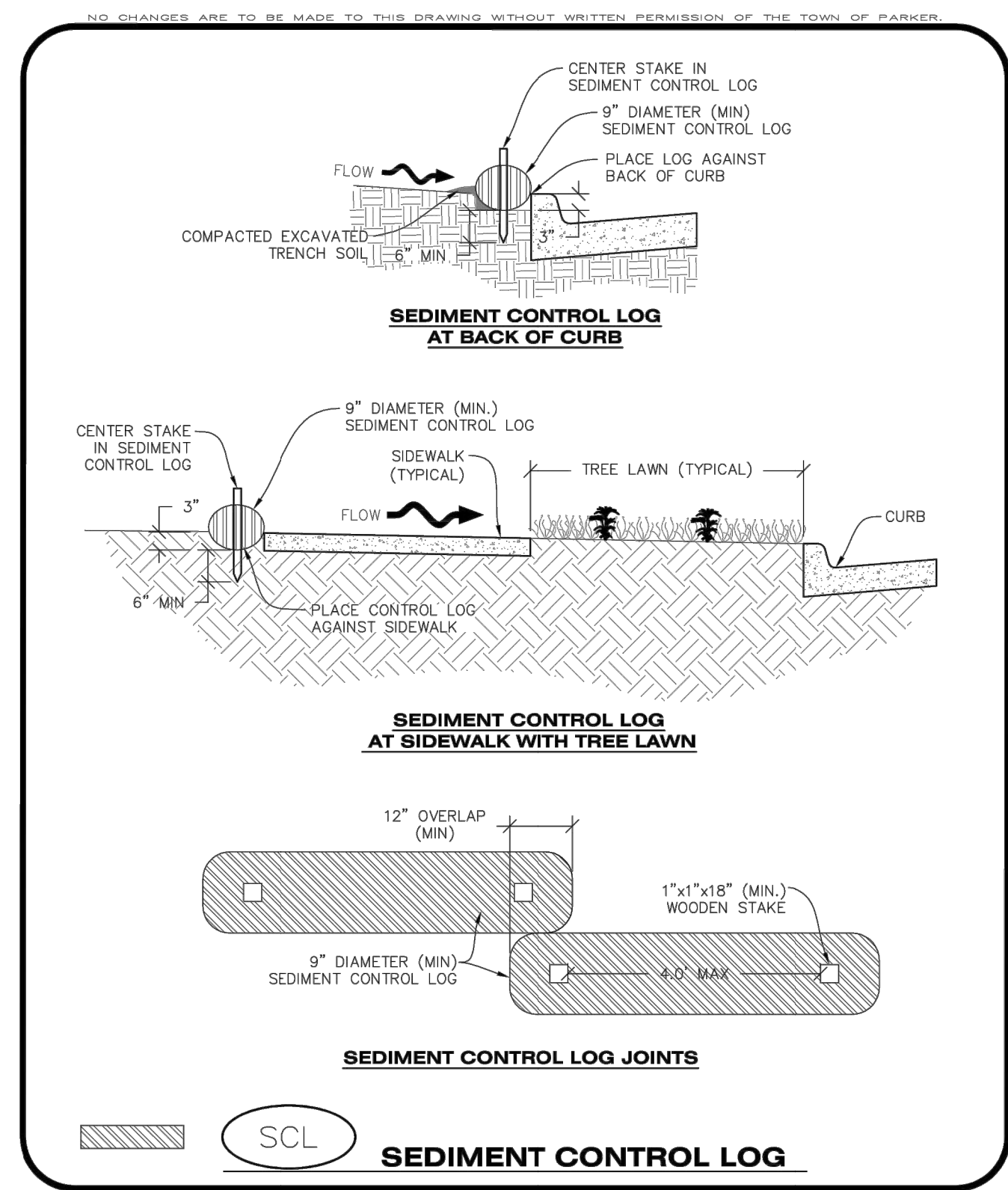
**STRAW BALE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STRAW BALES.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF THE STRAW BALE.
- STRAW BALES MAY NEED TO BE REPLACED IF THEY BECOME HEAVILY SOILED, ROTTEN, OR OTHERWISE DAMAGED.
- STRAW BALES SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN INSPECTOR.
- WHEN THE STRAW BALES ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE STRAW BALES MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**CBMP** | SB  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
 Oct. 2013



**CBMP** | SCL  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
 Oct. 2013



**CBMP** | SCL  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
 Oct. 2013

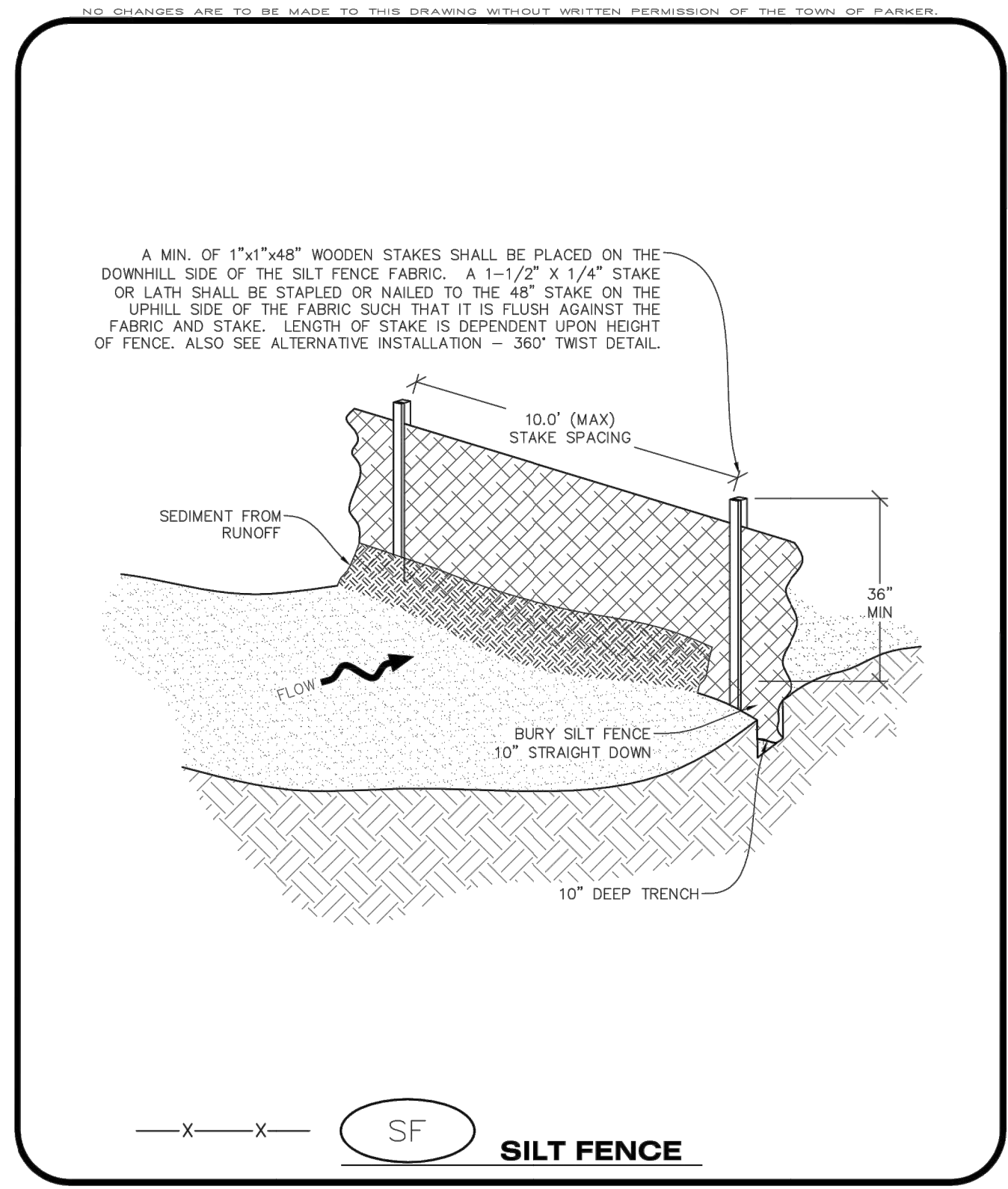
**SEDIMENT CONTROL LOG INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT CONTROL LOGS.
- ALL SEDIMENT CONTROL LOGS SHALL BE INSTALLED FREE OF DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- SEDIMENT CONTROL LOGS SHALL BE INSTALLED IMMEDIATELY ADJACENT TO AN IMPERVIOUS SURFACE SUCH AS A CURB HEAD, SIDEWALK, INLET LID, ETC. NO GAPS SHALL EXIST BETWEEN THE SEDIMENT CONTROL LOG AND THE IMPERVIOUS SURFACE.
- A UNIFORM 3" DEEP ANCHOR TRENCH (APPROX.) IN THE SHAPE OF A HALF-SPHERE SHALL BE EXCAVATED USING A TRENCHER, SPADE-SHAPED SHOVEL, OR PICK. THE ANCHOR TRENCH SHALL BE SIZED TO ALLOW FOR THE SEDIMENT CONTROL LOG TO SEAT TIGHTLY AGAINST THE ANCHOR TRENCH.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH AND PROPERLY COMPACTED.
- ANCHOR TRENCH SHALL BE RELATIVELY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT.
- ALL SEDIMENT CONTROL LOGS SHALL BE PLACED 3" (APPROX.) BELOW THE GROUND AND PULLED TIGHT ON BOTH ENDS TO REMOVE ANY CURVES OR SNAGS.
- THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS RELATIVELY FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED AGAINST THE GROUND AND SEDIMENT CONTROL LOG USING A SHOVEL, OR SIMILAR DEVICE.
- SEDIMENT CONTROL LOG STAKES SHALL BE MADE OF WOOD AND SECURELY ANCHOR THE SCL IN PLACE.
- STAKES SHALL BE PLACED ON 4.0' CENTERS AND EMBEDDED APPROXIMATELY 6" INTO THE GROUND. STAKES THAT ARE BROKEN PRIOR TO OR DURING INSTALLATION SHALL BE REPLACED.
- SEDIMENT CONTROL LOGS SHALL OVERLAP A MINIMUM OF 12". THE OVERLAPPING SHALL OCCUR ON THE UP-GRADE SIDE OF THE LOGS.
- SEDIMENT CONTROL LOGS SHALL BE STAKED WITHIN 6" FROM EACH END.
- SEDIMENT CONTROL LOGS THAT ARE INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN THE CONCRETE AND THE LOG. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

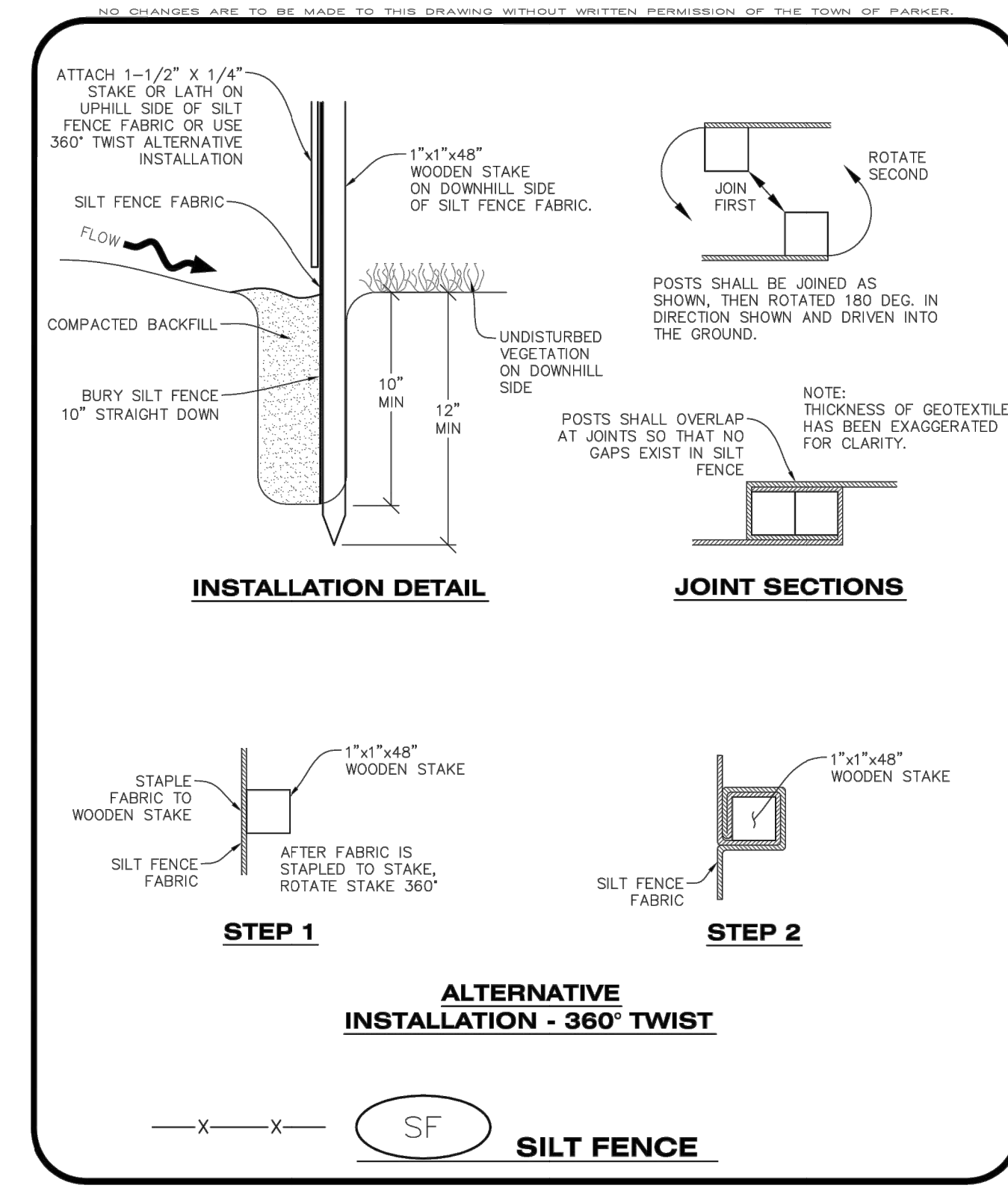
**SEDIMENT CONTROL LOG INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEDIMENT CONTROL LOGS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF EXPOSED LOG.
- SEDIMENT CONTROL LOGS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- SEDIMENT CONTROL LOGS SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR OR DAMAGE THAT WOULD PREVENT THE SCL FROM FUNCTIONING AS DESIGNED.
- WHEN THE SEDIMENT CONTROL LOGS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SEDIMENT CONTROL LOGS MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**CBMP** | SCL  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
 Oct. 2013



**CBMP** | SF  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 4  
 Oct. 2013



**CBMP** | SF  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 4  
 Oct. 2013

**SILT FENCE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SILT FENCE.
- ALL SILT FENCE SHALL BE INSTALLED IN GOOD CONDITION AND FREE OF ANY DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- A UNIFORM 10" DEEP ANCHOR TRENCH SHALL BE EXCAVATED USING A TRENCHER.
- A 10" DEEP ANCHOR SLIT SHALL BE FORMED IF USING A STATIC SLICING METHOD.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH.
- ANCHOR TRENCH SHALL BE GENERALLY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT OF THE SILT FENCE.
- THE ANCHOR TRENCH SHALL BE THOROUGHLY BACKFILLED WITH SOIL THAT IS GENERALLY FREE OF ROCKS AND DEBRIS.
- ALL EXCAVATED MATERIAL SHALL BE PLACED ON THE UP-GRADE SIDE OF THE SILT FENCE.
- STAKES SHALL BE POSITIONED ON THE DOWNHILL SIDE OF THE SILT FENCE FABRIC AND PLACED ON 10.0' CENTERS OR LESS. STAKES SHALL BE EMBEDDED A MINIMUM OF 12" INTO THE GROUND. A WOODEN LATH SHALL BE ATTACHED TO THE OPPOSING (UPHILL) SIDE OF THE STAKE FOR ADDED STRENGTH AND SUPPORT. THE LATH SHALL HAVE THE FOLLOWING DIMENSIONS: 1"x4"x24".
- SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD NOT BE SIGNIFICANT SAGGING ALONG ANY PORTION OF THE SILT FENCE AFTER IT HAS BEEN ANCHORED TO THE STAKES.
- SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES AND LATHS USING STAPLES OR NAILS OF AN APPROXIMATE LENGTH ENOUGH STAPLES AND NAILS SHOULD BE PLACED ALONG THE LATH TO ENSURE PROPER ATTACHMENT.
- SILT FENCE FABRIC SHALL MEET THE FOLLOWING MANDATORY REQUIREMENTS:

PROPERTIES	TEST METHOD	MANDATORY REQUIREMENTS
GRAB TENSILE STRENGTH	ASTM D 4632	≥ 124 LBS
MULLEN BURST STRENGTH	ASTM D 3786	≥ 300 PSI
PUNCTURE STRENGTH	ASTM D 4833	≥ 60 LBS
TRAPEZOID TEAR STRENGTH	ASTM D 4533	≥ 65 LBS
UV RESISTANCE	ASTM D 4355	≥ 80% AT 500 HOURS OF UV EXPOSURE
FLOW RATE	ASTM D 4491	≥ 10 GAL/MIN/FT2

- AN ORIGINAL PRODUCT SPECIFICATION SHEET FROM THE SILT FENCE MANUFACTURER SHALL BE MADE AVAILABLE AT THE REQUEST OF THE TOWN'S INSPECTOR. THE PRODUCT SPECIFICATION SHEET SHALL PROVIDE THE RESULTS FOR THE TEST METHODS ABOVE.
- SILT FENCE JOINTS SHALL BE CONNECTED ACCORDING TO THE ATTACHED DRAWING.
- SILT FENCE THAT IS INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN CONCRETE AND THE SILT FENCE. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

**CBMP** | SF  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 4  
 Oct. 2013



NO. REVISION BY DATE  
 PROJECT NO. 096502001  
 DRAWING NAME 096502001EC\_DT  
 C3.8  
**Kimley-Horn**  
 2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
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 DESIGNED BY: DLS  
 DRAWN BY: EC2  
 CHECKED BY: DLS  
 DATE: 11/14/19  
**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
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NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SILT FENCE INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SILT FENCE.
2. ACCUMULATED SEDIMENT SHALL BE REMOVED REGULARLY.
3. SILT FENCE SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
4. SILT FENCE SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR AND/OR DAMAGE.
5. WHEN THE SILT FENCE IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SILT FENCE MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).


**CBMP** | SF  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 4 OF 4  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SEEDING AND MULCHING SHALL BE PERFORMED ACCORDING TO THE ACCOMPANYING DETAIL(S) AND TEXT. NO EXCEPTIONS SHALL BE MADE**

1. SEE PLAN VIEW FOR:
  - LOCATION(S) OF SEEDING AND MULCHING
  - TYPE OF SEED MIX
2. SEED MIXES MAY CONFORM TO THE TABLE PROVIDED WITH THE SMC NOTES OR ALTERNATIVES MAY BE ALLOWED WITH PRIOR PERMISSION BY THE TOWN'S INSPECTOR.
3. SEEDING MAY BE PERFORMED YEAR ROUND ASSUMING THE SOIL IS NOT FROZEN. SEEDING DURING TIMES OF EXTREME TEMPERATURES SHOULD BE AVOIDED IF POSSIBLE.
4. AT THE BEGINNING OF THE LAND DISTURBANCE ACTIVITIES, IT IS HIGHLY RECOMMENDED THAT AN APPROPRIATE AMOUNT OF NATIVE TOPSOIL BE STRIPPED FROM THE SITE AND STOCKPILED. ALL AREAS, PRIOR TO PERMANENT SEEDING AND MULCHING, WILL LIKELY NEED TO BE COVERED WITH AN APPROPRIATE LAYER OF TOPSOIL. THIS REQUIREMENT APPLIES TO ALL AREAS WHERE NATIVE SEEDING IS SPECIFIED ON THE CBMP PLAN AND/OR LANDSCAPING PLANS.
5. IT IS STRONGLY RECOMMENDED THAT SAMPLES FROM THE STRIPPED TOPSOIL BE PROPERLY COLLECTED AND TESTED BY A QUALIFIED LABORATORY TO ENSURE ADEQUATE NUTRIENT CONTENT PRIOR TO SEEDING AND MULCHING. IF IT IS DISCOVERED THAT THE TOPSOIL IS VOID OF THE NUTRIENTS NECESSARY TO SUCCESSFULLY ESTABLISH THE REQUIRED VEGETATION, THEN THE APPROPRIATE AMENDMENTS SHALL BE ADDED.
6. ALL AREAS TO BE SEEDED AND MULCHED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING DETAILS AND NOTES. SURFACE ROUGHENING SHALL OCCUR AFTER PLACEMENT OF THE TOPSOIL.
7. WHEN INSTALLED WITH A DRILL SEEDER, SEED SHALL BE PLACED AT A DEPTH OF 1/4 - 1/2 INCH. ROW SPACING SHALL BE NO MORE THAN 6-INCHES.
8. ALL AREAS INCAPABLE OF BEING DRILL SEEDED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING NOTES OR EFFECTIVELY ROUGHENED USING A HARROW OR OTHER SUCH IMPLEMENT. ALL AREAS SHALL BE UNIFORMLY HAND BROADCASTED WITH THE PROPER SEED MIX APPLIED AT TWO TIMES THE DRILL SEEDED RATE. BROADCASTED AREAS SHALL THEN BE RE-HARROWED OR RE-RAKED USING A HARD-TIPPED RAKE TO ENSURE THAT SEEDS ARE BURIED TO AN APPROXIMATE DEPTH OF 1/4 - 1/2 INCH.
9. AFTER SEEDING HAS BEEN COMPLETED, MULCH SHALL BE UNIFORMLY APPLIED AT A RATE OF 2 TONS/ACRE (4,000 LBS/ACRE). MULCH SHALL BE MECHANICALLY CRIMPED TO A DEPTH OF 2 INCHES USING A CRIMPER. MULCH SHALL BE HAND CRIMPED AND COVERED WITH A TACKIFIER IN AREAS WHERE MECHANICAL CRIMPING IS NOT POSSIBLE. WHEN SOILS PERMIT, ALL MULCH SHALL BE CRIMPED SUCH THAT THE INDIVIDUAL PIECES OF STRAW OR HAY FORM EXAGGERATED V-SHAPES PROTRUDING OUT OF THE GROUND SEVERAL INCHES.
10. IN CERTAIN INSTANCES, IT MAY BE NECESSARY TO APPLY A TACKIFIER IN ORDER TO HELP WITH STRAW DISPLACEMENT. TACKIFIER SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.


**CBMP** | SMC  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SEEDING AND MULCHING MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEEDING AND MULCHING.
2. ANY SEEDED AND MULCHED AREAS THAT BECOME DAMAGED SHALL BE REPAIRED WITHIN THE TIME FRAME SPECIFIED BY THE TOWN'S INSPECTOR.

**WEED MANAGEMENT**

1. ALL HERBICIDES SHALL BE APPLIED BY COMMERCIAL PESTICIDE APPLICATORS LICENSED BY THE COLORADO DEPARTMENT OF AGRICULTURE AS QUALIFIED APPLICATORS. THE CONTRACTOR SHALL FURNISH DOCUMENTATION OF SUCH LICENSING PRIOR TO HERBICIDE APPLICATION.
2. HERBICIDE APPLICATION METHOD SHALL BE SUCH THAT PLANT GROWTH OUTSIDE THE DESIGNATED TREATMENT AREAS WILL NOT BE DAMAGED. ALL DAMAGE CAUSED BY IMPROPER HERBICIDE APPLICATION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
3. HERBICIDES SHALL BE APPLIED DURING THE APPROPRIATE SEASONS, WHEN TARGET PLANTS ARE ACTIVELY GROWING.
4. AFTER THE GRASS SEED IS ESTABLISHED, APPROPRIATE HERBICIDES SHALL BE APPLIED TO CONTROL THE REMAINING WEEDS TO ENSURE A TIMELY RETURN OF THE FINANCIAL SECURITY. PROPER TIMING OF HERBICIDE APPLICATIONS ARE NECESSARY TO ACHIEVE THE SUPPRESSION OF WEED SEED PRODUCTION AND DEPLETION OF WEED ROOT MASS. ULTIMATELY, THE HERBICIDES USED SHALL BE BASED UPON THE TARGET WEEDS.
5. HERBICIDE TREATMENTS SHALL CONTINUE AT AN APPROPRIATE RATE UNTIL IT IS EVIDENT THAT WEED GROWTH PRESENCE AND GROWTH IS MINIMAL AND MAY BE CONTROLLED THROUGH MOWING AND/OR ANNUAL HERBICIDE TREATMENT.


**CBMP** | SMC  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**TOWN OF PARKER, SEED MIX 1**

- 20% CANADA WILDRIE
- 15% CRESTED WHEATGRASS
- 15% SLENDER WHEATGRASS
- 10% ANNUAL RYEGRASS
- 10% SHEEP FESCUE
- 10% BIG BLUESTEM
- 10% SIDEOTS GRAMA
- 5% CANADA BLUEGRASS
- 5% BLUE GRAMA

**SEEDING RATE:**  
 DRILLED: 25 LBS/ACRE  
 BROADCAST: 50 LBS/ACRE

**TOWN OF PARKER, SEED MIX 2**

- 22% SLENDER WHEATGRASS
- 18% SODAR STREAMBANK WHEATGRASS
- 13% ARIZONA FESCUE
- 13% BLUE GRAMA
- 12% BUFFALOGRASS
- 12% BARLEY OR DATS
- 5% SPIKE MUHLY
- 5% INDIAN RICEGRASS

**SEEDING RATE:**  
 DRILLED: 25 LBS/ACRE  
 BROADCAST: 50 LBS/ACRE

**TOWN OF PARKER, SEED MIX 3 (LOW-GROWTH MIX)**

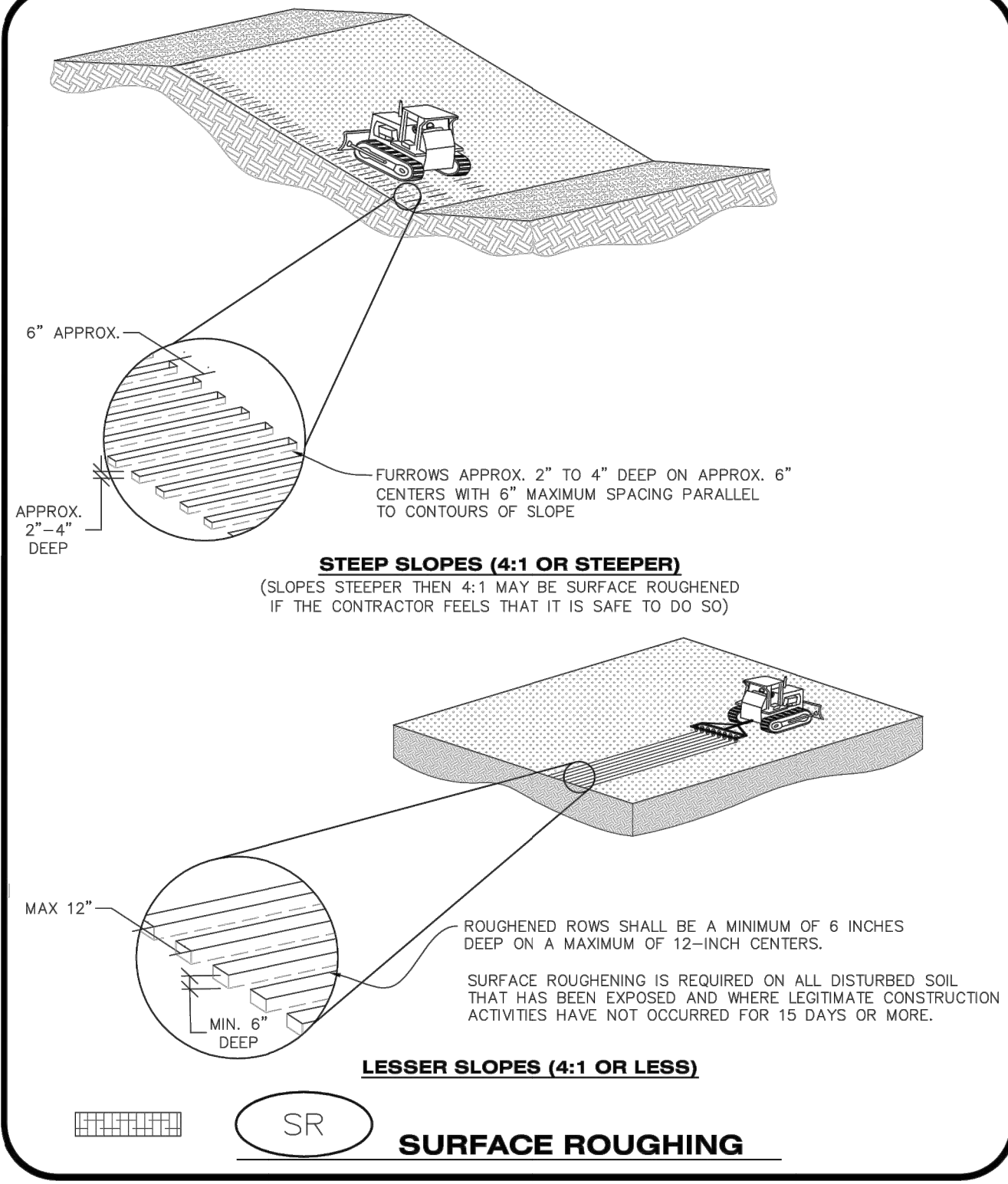
- 25% EPHRAIM CRESTED WHEATGRASS
- 23% SHEEP FESCUE
- 18% PERENNIAL RYEGRASS
- 13% CANADA BLUEGRASS
- 12% BARLEY OR DATS
- 9% BLUE FESCUE

**SEEDING RATE:**  
 DRILLED: 25 LBS/ACRE  
 BROADCAST: 50 LBS/ACRE

**SEED MIX 4:**  
 OTHER SEED MIXES APPROVED BY THE TOWN OF PARKER


**CBMP** | SMC  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 3 OF 3  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.




**CBMP** | SR  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER.

**SURFACE ROUGHENING INSTALLATION NOTES**

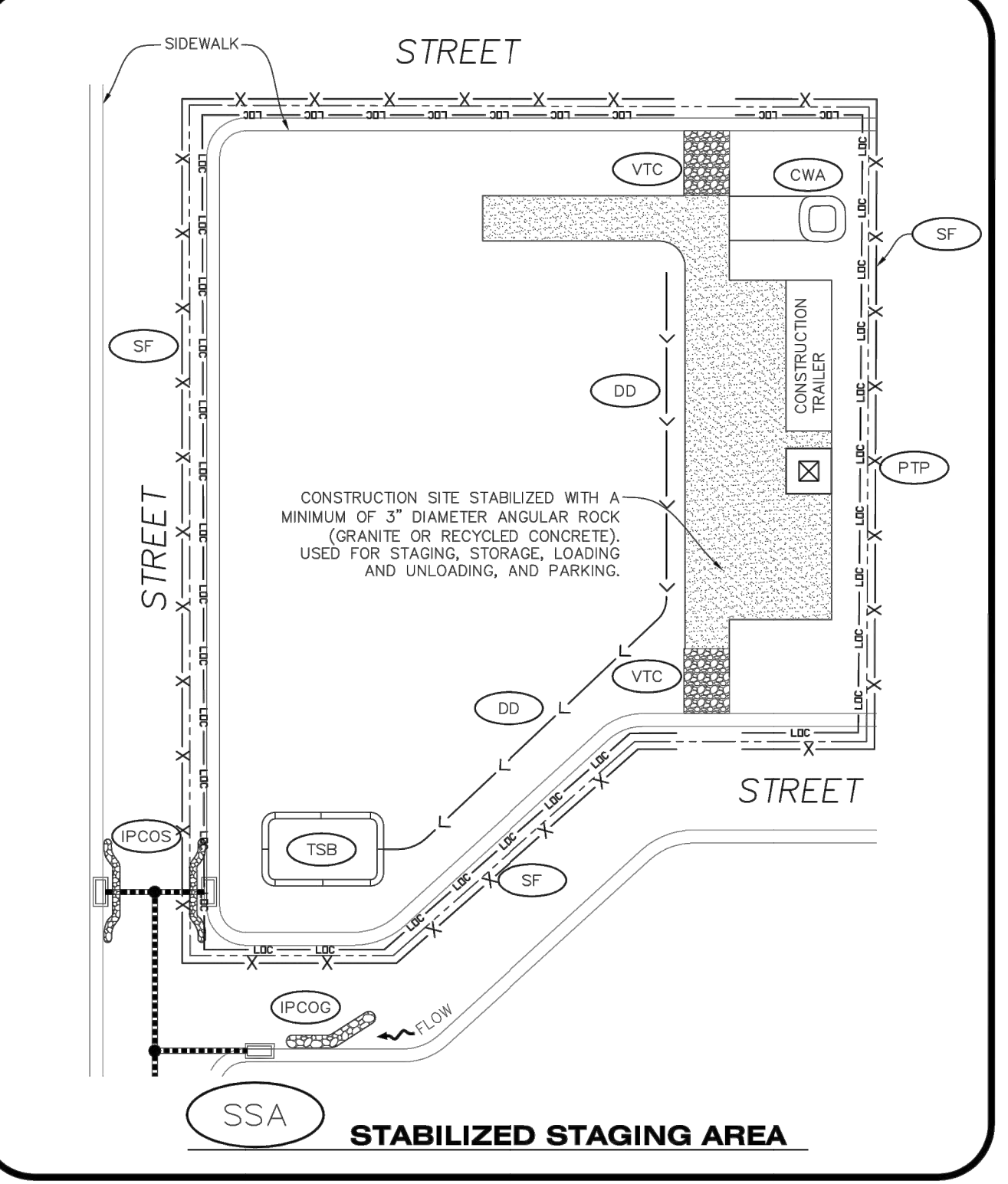
1. SEE CBMP PLAN FOR LOCATION(S) OF SURFACE ROUGHENING.
2. DISTURBED AREAS THAT REMAIN INACTIVE FOR 15 DAYS OR MORE MUST RECEIVE SURFACE ROUGHENING OR ANOTHER APPROVED BMP FROM THE SDCM. DETERMINATION OF JOB SITE INACTIVITY IS AT THE DISCRETION OF THE TOWN'S INSPECTOR.
3. FOR STEEP SLOPES (3:1 OR STEEPER), IT IS ACCEPTABLE TO "TRACK" THE SLOPES, ACCORDING TO THE CBMP DETAILS.
4. SCHEDULES FOR REQUIRING STABILIZATION MAY BE MODIFIED BY THE PERMITTEE TO ALLOW FOR SPECIAL CONSIDERATIONS SUCH AS STABILIZING ACCESS AREAS AND AREAS IN CLOSE PROXIMITY TO CONTINUING CONSTRUCTION.

**SURFACE ROUGHENING INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL PROACTIVELY INSPECT THE SURFACE ROUGHENING.


**CBMP** | SR  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

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**CBMP** | SSA  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 1 OF 2  
Oct. 2013

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**STABILIZED STAGING AREA INSTALLATION NOTES**

1. SEE CBMP PLAN FOR LOCATION OF STAGING AREA. CONTRACTOR MAY MODIFY LOCATION AND SIZE OF STABILIZED STAGING AREA WITH TOWN APPROVAL.
2. STABILIZED STAGING AREA SHALL BE LARGE ENOUGH TO FULLY CONTAIN PARKING, STORAGE, AND LOADING OPERATIONS.
3. THE STABILIZED STAGING AREA SHALL CONSIST OF A MINIMUM OF 3" DIAMETER OF ANGULAR ROCK (GRANITE OR RECYCLED CONCRETE).
4. SSA FOR SMALLER SITES MAY NOT BE PRACTICAL. IN THESE AND SIMILAR SITUATIONS, VARIANCES MAY BE PERMITTED BY THE TOWN.

**STABILIZED STAGING AREA INSPECTION AND MAINTENANCE NOTES**

1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE STAGING AREA.
2. STABILIZED STAGING AREA SHALL BE ENLARGED AS NECESSARY TO CONTAIN PARKING, STORAGE, LOADING, AND UNLOADING.


**CBMP** | SSA  
 CONSTRUCTION BEST MANAGEMENT PRACTICES | 2 OF 2  
Oct. 2013

NO.	REVISION	BY	DATE

**Kimley»Horn**  
 2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**CBMP DETAILS**

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**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

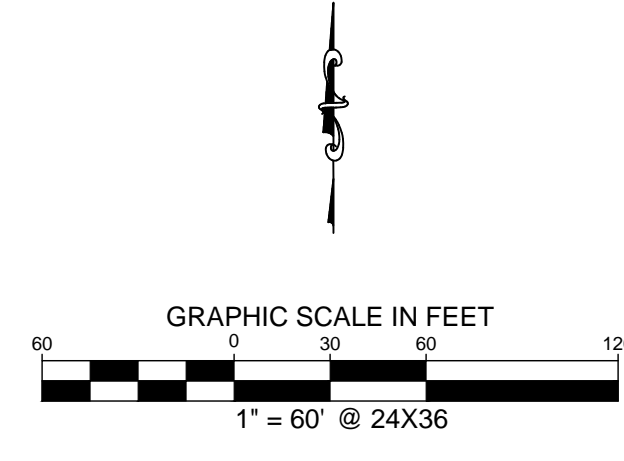
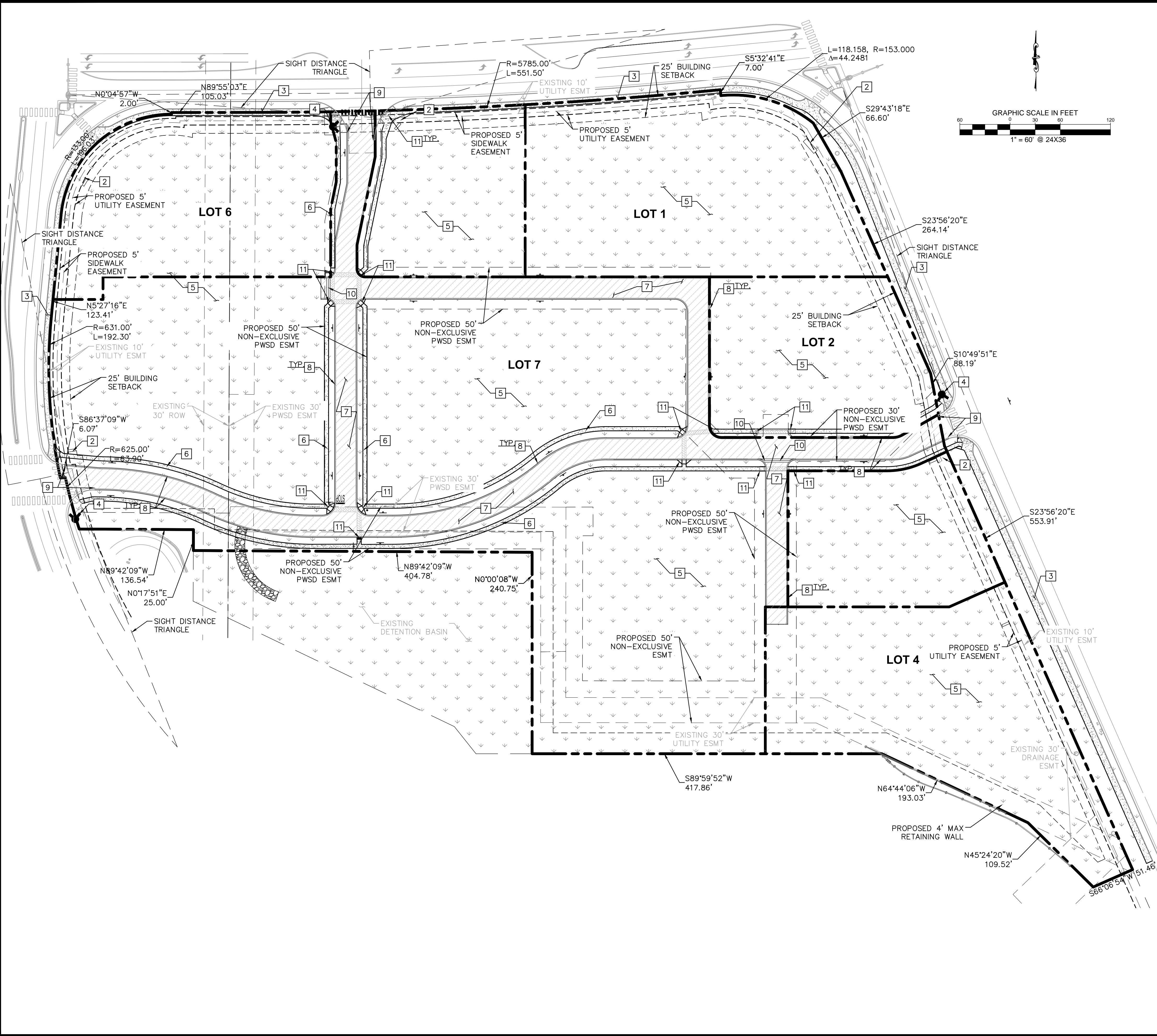
PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001EC\_DT  
**C3.9**







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**LEGEND**

	PROPERTY LINE
	LOT LINE
	EXISTING UTILITY EASEMENT
	PROPOSED UTILITY EASEMENT
	EXISTING CURB AND GUTTER
	PROPOSED CURB AND GUTTER
	SETBACK LINE
	HEAVY DUTY ASPHALT PAVING
	CONCRETE SIDEWALK
	PROPOSED FINAL STABILIZATION/LANDSCAPE
	DECORATIVE CONCRETE PAVEMENT
	PROPOSED FIRE HYDRANT

- KEY NOTES**
- 1 PROPOSED 5' WIDE PEDESTRIAN CONNECTION
  - 2 PROPOSED MONUMENT SIGN.
  - 3 PROPOSED 8' WIDE SIDEWALK
  - 4 PROPOSED FIRE HYDRANT.
  - 5 PROPOSED PERMANENT STABILIZATION. REFER TO LANDSCAPE PLANS
  - 6 PROPOSED 5' CONCRETE SIDEWALK.
  - 7 PROPOSED HEAVY DUTY ASPHALT PAVING.
  - 8 PROPOSED 18" VERTICAL CURB
  - 9 PROPOSED CURB TRANSITION FROM 18" CURB AND GUTTER (ON-SITE) TO 30" CURB AND GUTTER (OFF-SITE).
  - 10 PROPOSED CROSSSPAN GUTTER
  - 11 PROPOSED ADA COMPLIANT CURB RAMP

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR

**Kimley»Horn**  
 2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

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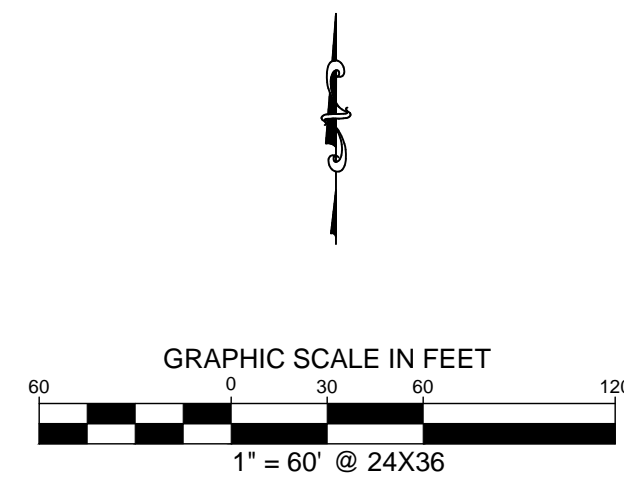
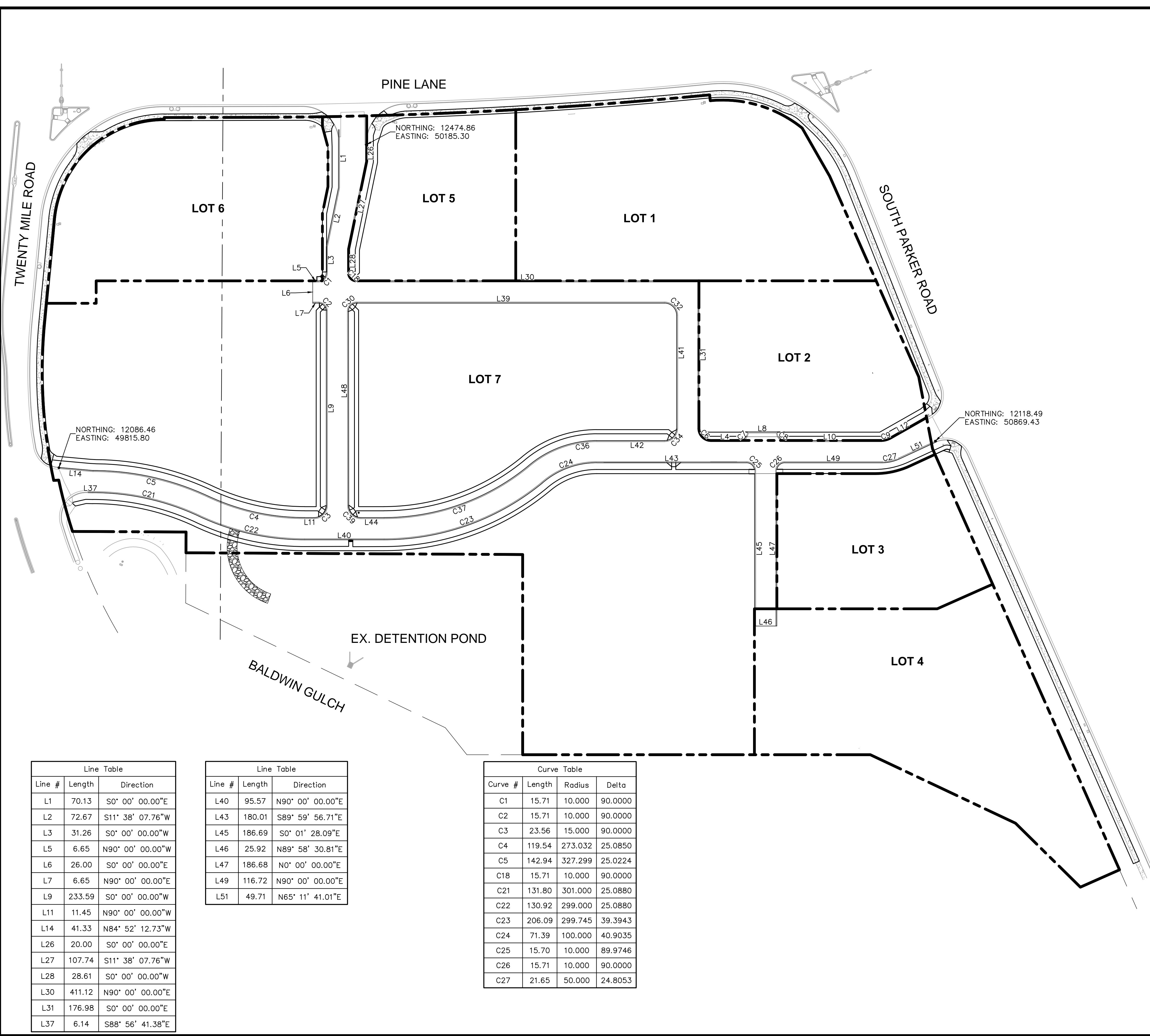
**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**SITE PLAN**

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PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001SP\_SP  
**C4.0**



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- - - - - PROPERTY LINE  
 = = = = = PROPOSED CONCRETE CURB  
 — — — — — EXISTING CURB

**BASIS OF BEARINGS**

BEARINGS SHOWN HEREON ARE BASED UPON THE WEST LINE OF THE SW 1/4 OF SECTION 10, TOWNSHIP 6 SOUTH, RANGE 66 WEST, OF THE 6TH PRINCIPAL MERIDIAN BEARING S00°17'51"W AS REFERENCED ON THE PLAT OF PARKER AUTO PLAZA FILING NO.1 AND BOUND BY THE MONUMENTS SHOWN HEREON.

Line #	Length	Direction
L1	70.13	S0° 00' 00.00"E
L2	72.67	S11° 38' 07.76"W
L3	31.26	S0° 00' 00.00"W
L5	6.65	N90° 00' 00.00"W
L6	26.00	S0° 00' 00.00"E
L7	6.65	N90° 00' 00.00"E
L9	233.59	S0° 00' 00.00"W
L11	11.45	N90° 00' 00.00"W
L14	41.33	N84° 52' 12.73"W
L26	20.00	S0° 00' 00.00"E
L27	107.74	S11° 38' 07.76"W
L28	28.61	S0° 00' 00.00"W
L30	411.12	N90° 00' 00.00"E
L31	176.98	S0° 00' 00.00"E
L37	6.14	S88° 56' 41.38"E

Line #	Length	Direction
L40	95.57	N90° 00' 00.00"E
L43	180.01	S89° 59' 56.71"E
L45	186.69	S0° 01' 28.09"E
L46	25.92	N89° 58' 30.81"E
L47	186.68	N0° 00' 00.00"E
L49	116.72	N90° 00' 00.00"E
L51	49.71	N65° 11' 41.01"E

Curve #	Length	Radius	Delta
C1	15.71	10,000	90.0000
C2	15.71	10,000	90.0000
C3	23.56	15,000	90.0000
C4	119.54	273.032	25.0850
C5	142.94	327.299	25.0224
C18	15.71	10,000	90.0000
C21	131.80	301.000	25.0880
C22	130.92	299.000	25.0880
C23	206.09	299.745	39.3943
C24	71.39	100.000	40.9035
C25	15.70	10,000	89.9746
C26	15.71	10,000	90.0000
C27	21.65	50,000	24.8053

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR

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 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

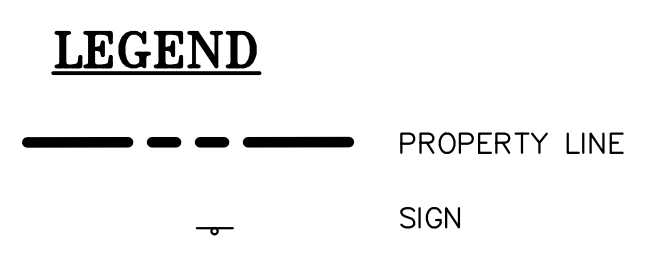
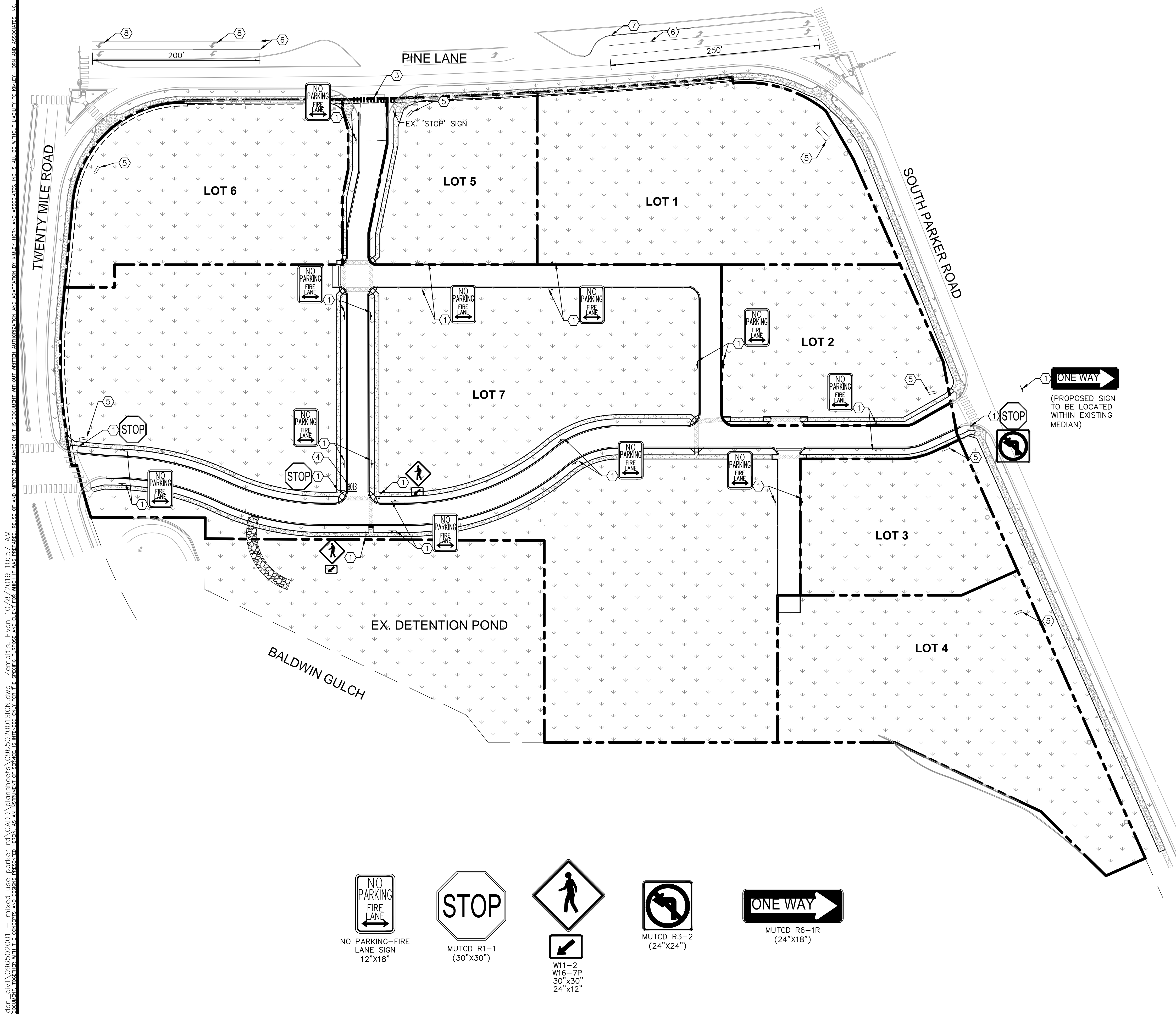
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 CHECKED BY: DLS  
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**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**HORIZONTAL CONTROL PLAN**

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 Kimley-Horn and Associates, Inc.

PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001HC  
**C4.1**





**SIGNING AND STRIPING NOTES**

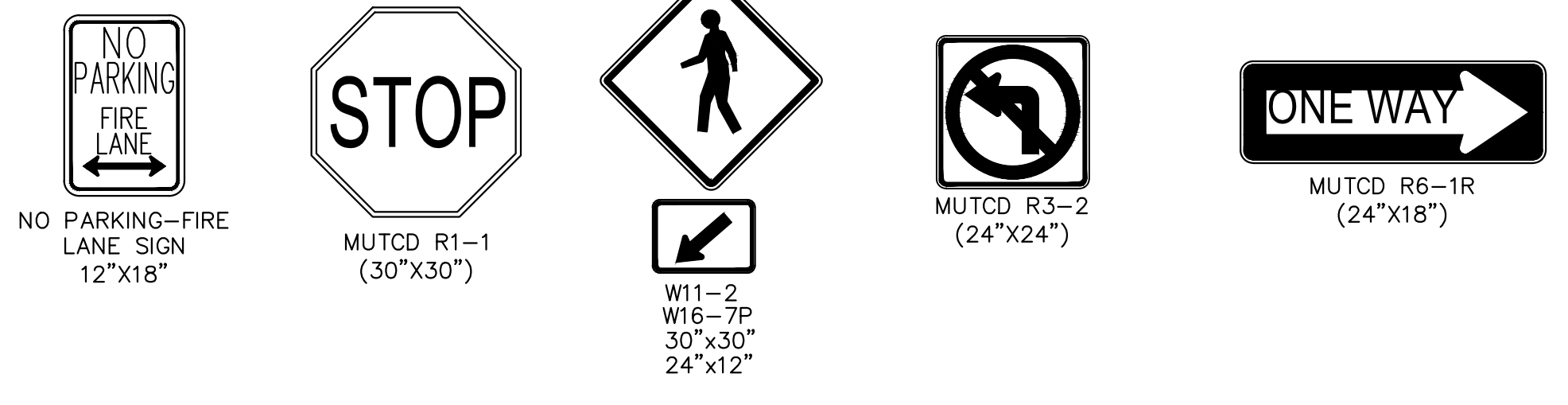
- ① ALL SIGNS AND ROADWAY MARKINGS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- ② NOT USED
- ③ 10' CROSSWALK STRIPING SHALL BE 2' WIDE SOLID RECESS PREFORMED PLASTIC AT 4'-0" O.C. PERPENDICULAR TO THE PATH OF TRAVEL
- ④ 12" STOP BAR. SEE DETAIL ON SHEET C4.3
- ⑤ PROPOSED MONUMENT SIGN (REFER TO ARCH. PLANS)
- ⑥ EXTEND EXISTING 6" SOLID WHITE STRIPE
- ⑦ 6" SOLID DOUBLE YELLOW STRIPE
- ⑧ DIRECTIONAL PAVEMENT MARKING. SEE DETAIL SHEET C4.3

- NOTES:
- 1. SEE SIGN POST DETAIL ON SHEET C4.4 FOR INSTALLATION AND POST/BASE DIMENSIONS.
  - 2. ALL PARKING ADJACENT TO LANDSCAPE ISLANDS SHALL BE 10' WIDE FROM STRIPE TO FACE OF CURB.
  - 3. CONTRACTOR TO REMOVE ALL EXISTING PAVEMENT MARKINGS IN CONFLICT WITH THE FINAL SIGNAGE AND STRIPING PLAN.

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_



NO.	REVISION	BY	DATE	APPR

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 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

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 DATE: 11/14/19

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**SIGNAGE AND STRIPING PLAN**

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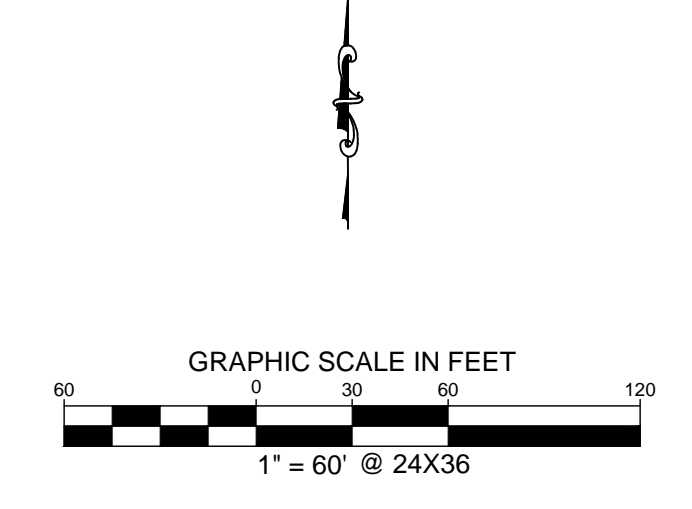
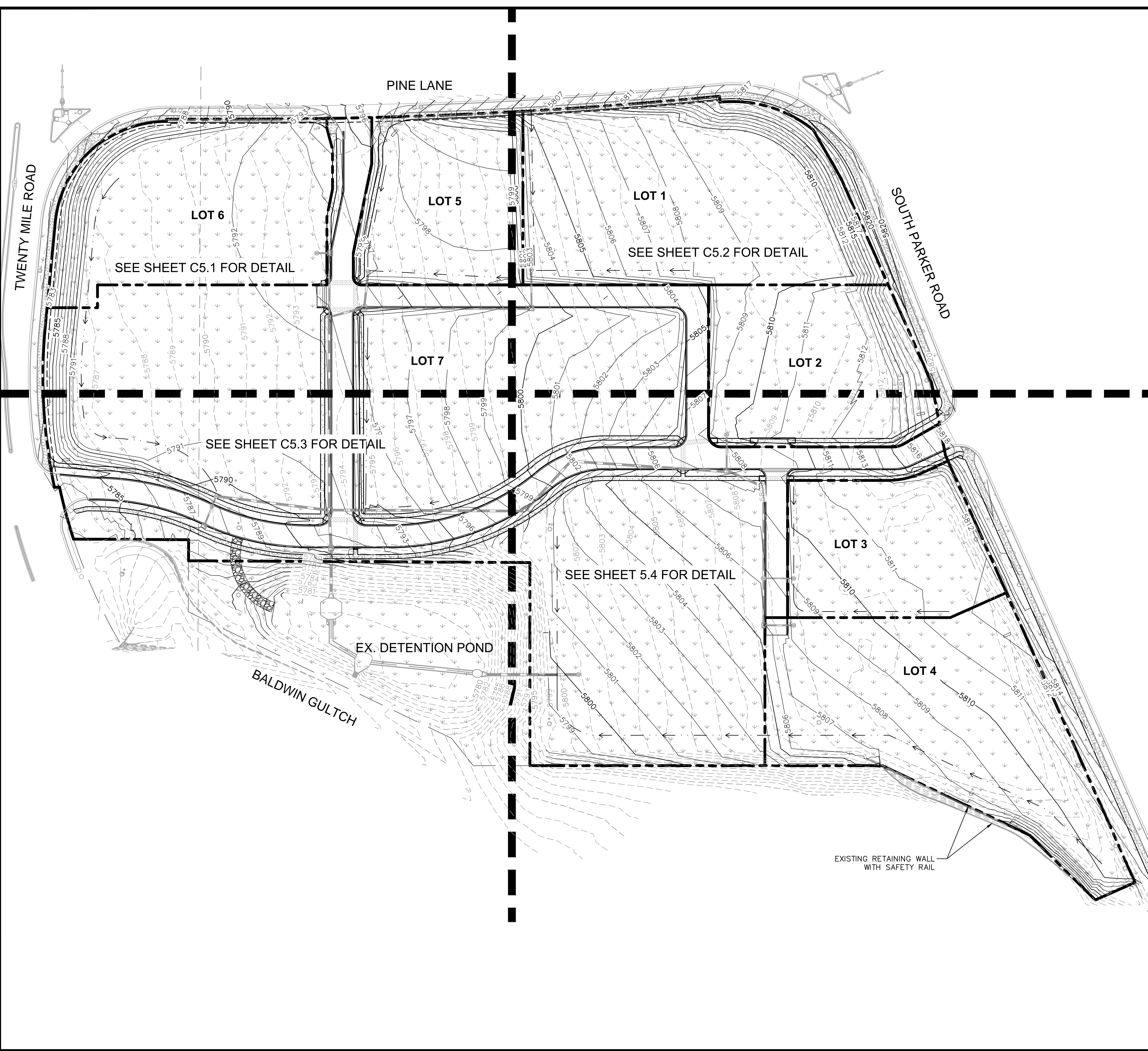
PROJECT NO.  
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 DRAWING NAME  
 096502001SIGN  
**C4.2**



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**LEGEND**

	EXISTING PROPERTY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	PROPOSED STORM INLET

**BENCHMARK**

DOUGLAS CONTROL MONUMENT #1.095035, A 3 1/4" ALUMINUM CAP.

ELEVATION = 5906.34 FEET (NAVD 1988), AS PUBLISHED BY DOUGLAS COUNTY.

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATIONS. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR

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 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

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**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**OVERALL GRADING PLAN**

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**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

PROJECT NO.  
096502001

DRAWING NAME  
096502001OGD

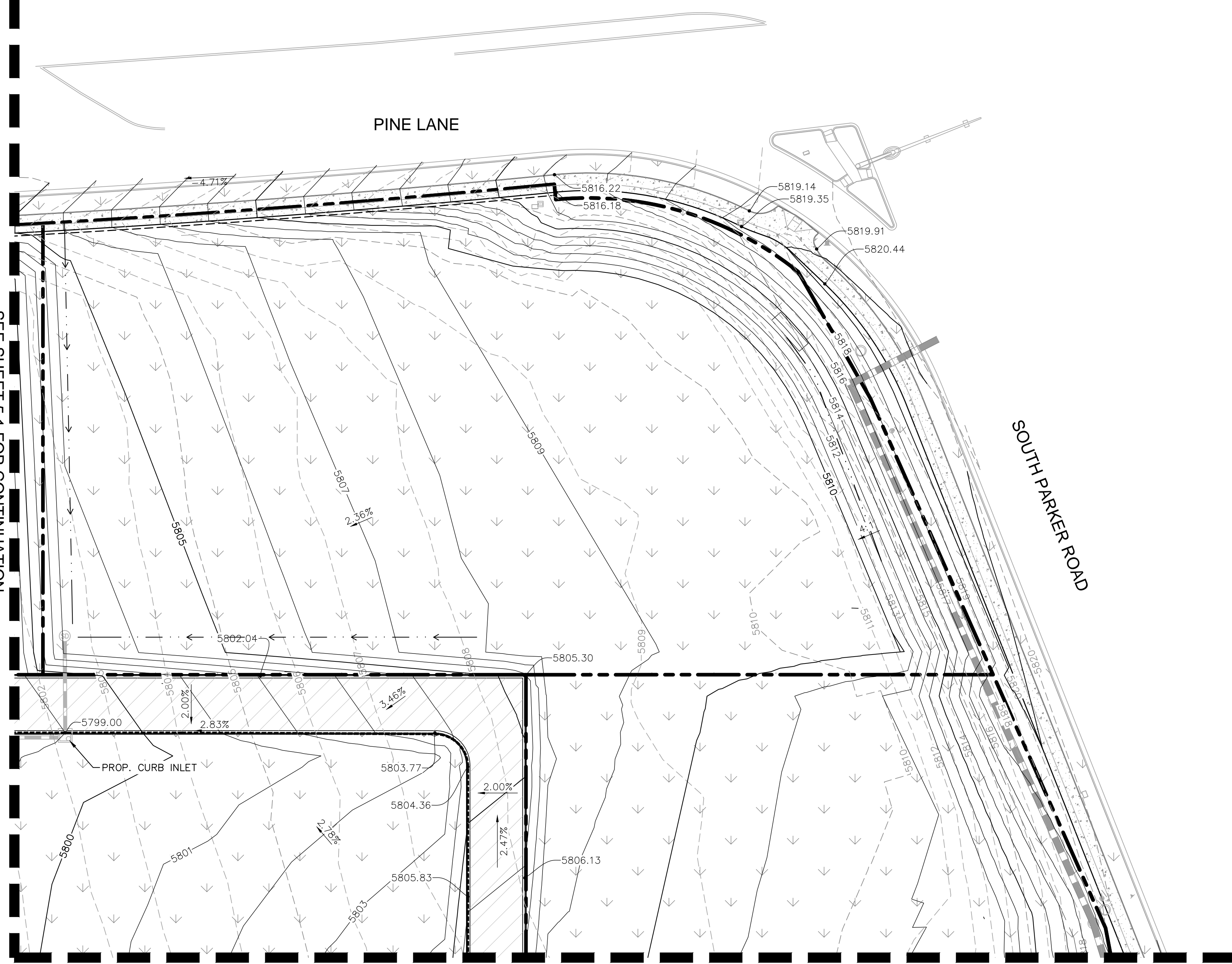
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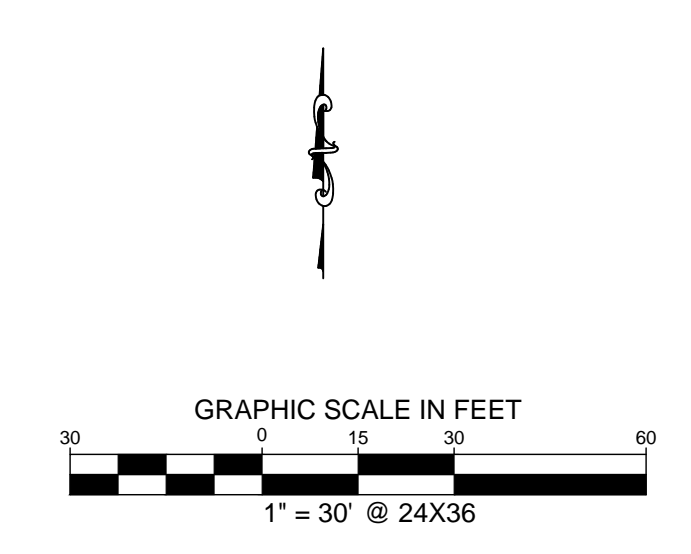


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SEE SHEET 5.1 FOR CONTINUATION



SEE SHEET C5.4 FOR CONTINUATION



**LEGEND**

- PROPERTY LINE
- - - - -5770- - - - - EXISTING CONTOUR
- 5771— PROPOSED CONTOUR
- 1.00% PROPOSED FLOW ARROW WITH SLOPE
- 5771 PROPOSED SPOT ELEVATION
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM INLET
- TP TOP OF PAVEMENT
- TS TOP OF SLAB (DUMPSTER PAD)
- SW TOP OF SIDEWALK
- ME MATCH EXISTING ELEVATION
- PROPOSED VALLEY
- PROPOSED RIDGE
- CATCH CURB

**BENCHMARK**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR

**Kimley»Horn**  
 2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 728-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
 DETAILED GRADING PLAN

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 Kimley-Horn and Associates, Inc.

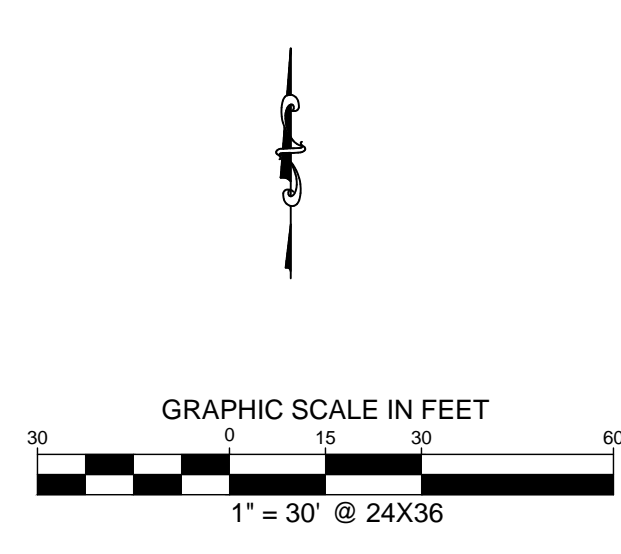
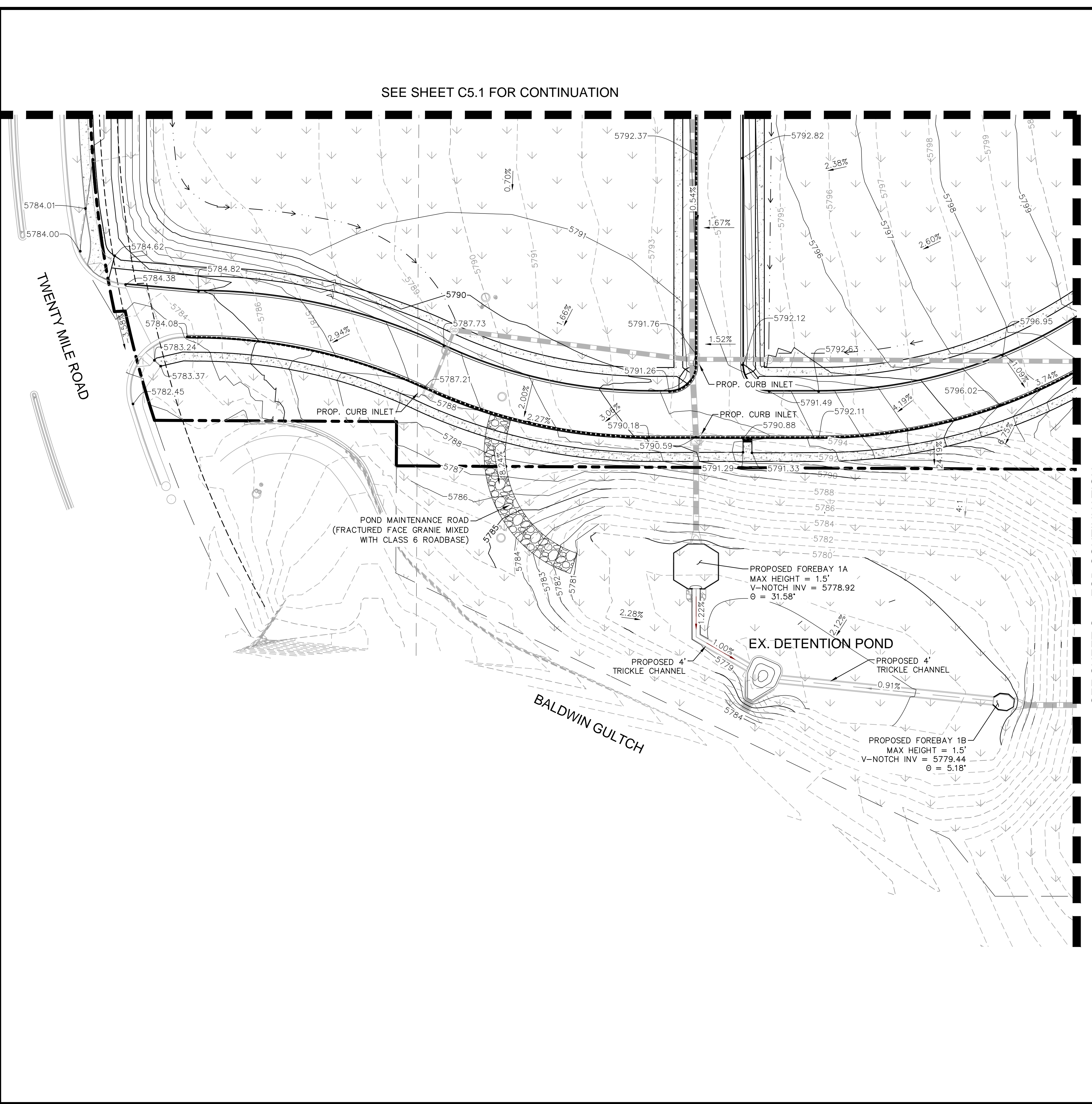
PROJECT NO.  
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**LEGEND**

	PROPERTY LINE
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED FLOW ARROW WITH SLOPE
	PROPOSED SPOT ELEVATION
	EXISTING STORM SEWER
	PROPOSED STORM SEWER
	PROPOSED STORM INLET
	TOP OF PAVEMENT
	TOP OF SLAB (DUMPSTER PAD)
	TOP OF SIDEWALK
	MATCH EXISTING ELEVATION
	PROPOSED VALLEY
	PROPOSED RIDGE
	CATCH CURB

**BENCHMARK**

DOUGLAS CONTROL MONUMENT #1.095035, A 3 1/4" ALUMINUM CAP.

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR

**Kimley»Horn**

2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

**PARKER & PINE**  
 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
 DETAILED GRADING PLAN

**PRELIMINARY**  
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**Kimley»Horn**  
 Kimley-Horn and Associates, Inc.

PROJECT NO.  
096502001

DRAWING NAME  
096502001GD

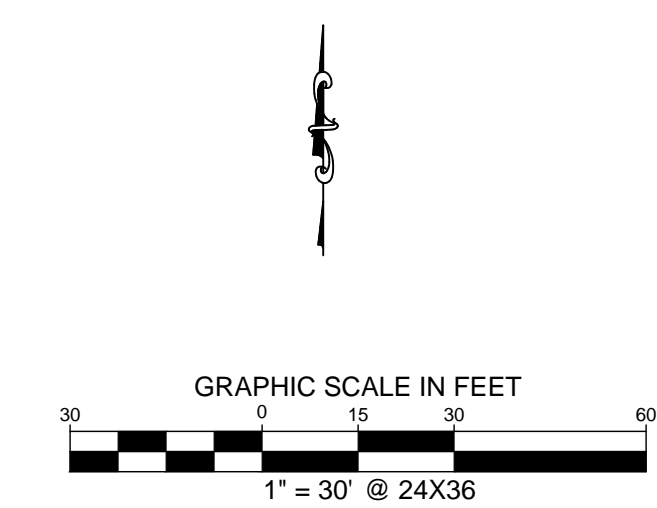
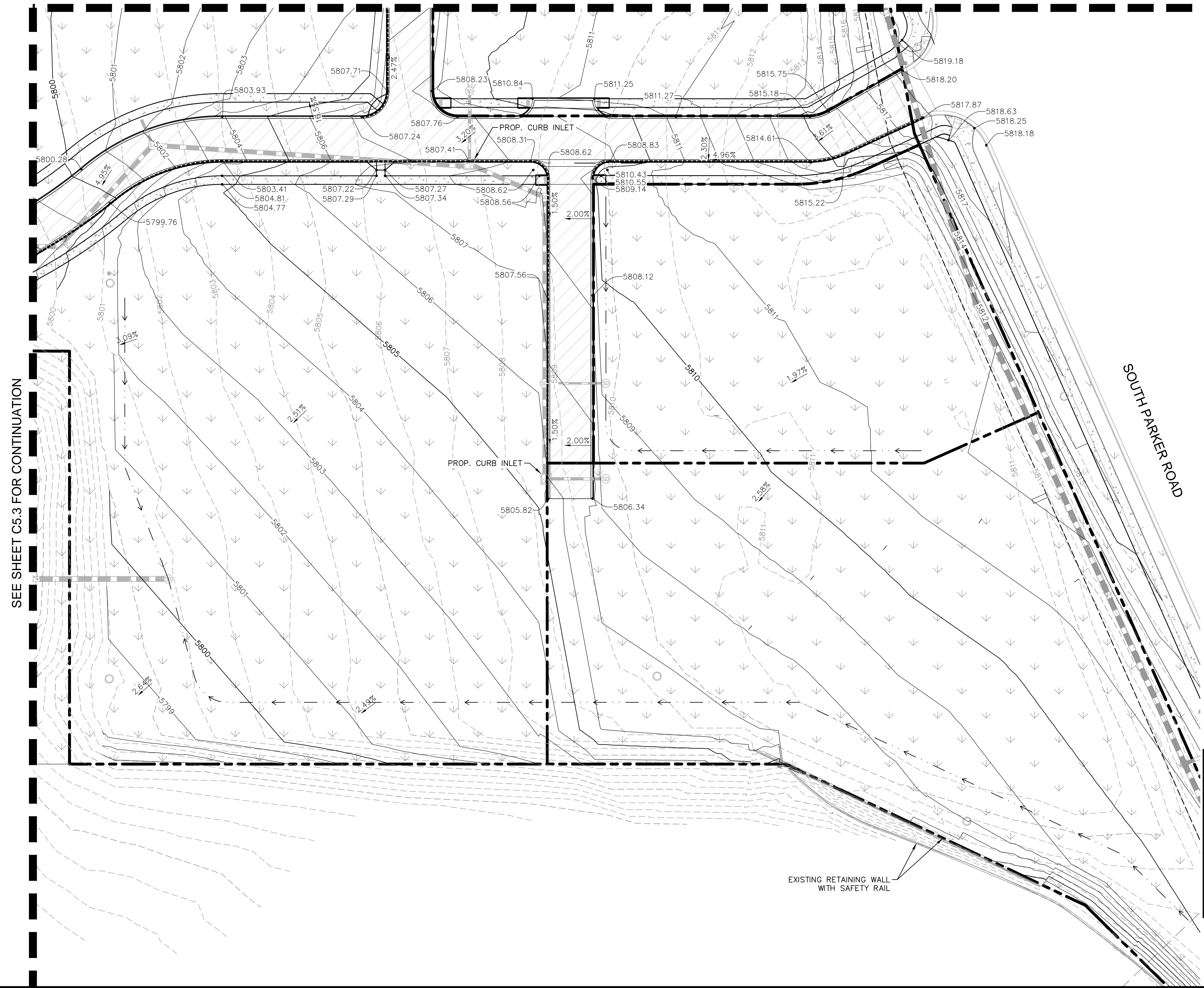
C5.3



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SEE SHEET C5.3 FOR CONTINUATION

SEE SHEET C5.2 FOR CONTINUATION



**LEGEND**

- PROPERTY LINE
- EXISTING CONTOUR
- PROPOSED CONTOUR
- PROPOSED FLOW ARROW WITH SLOPE
- PROPOSED SPOT ELEVATION
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM INLET
- TP TOP OF PAVEMENT
- TS TOP OF SLAB (DUMPSTER PAD)
- SW TOP OF SIDEWALK
- ME MATCH EXISTING ELEVATION
- PROPOSED VALLEY
- PROPOSED RIDGE
- CATCH CURB

**BENCHMARK**

DOUGLAS CONTROL MONUMENT #1.095035, A 3 1/4" ALUMINUM CAP.  
 ELEVATION = 5906.34 FEET (NAVD 1988), AS PUBLISHED BY DOUGLAS COUNTY.

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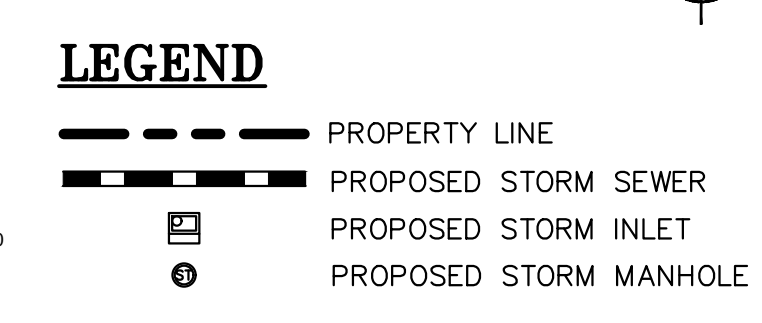
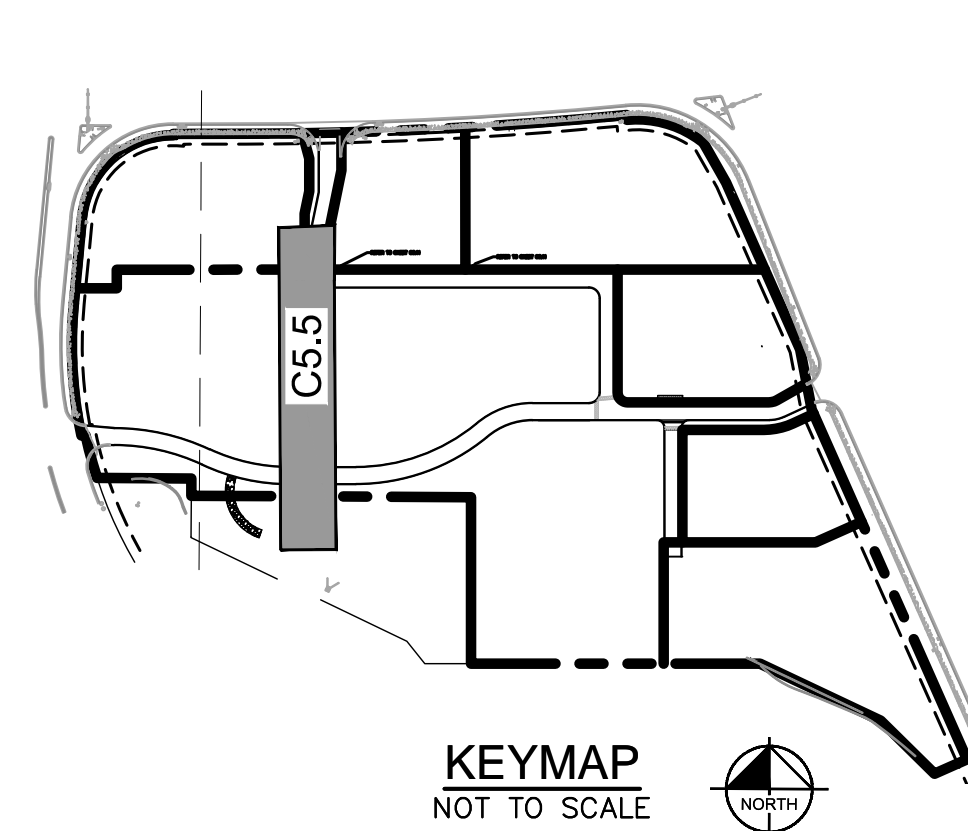
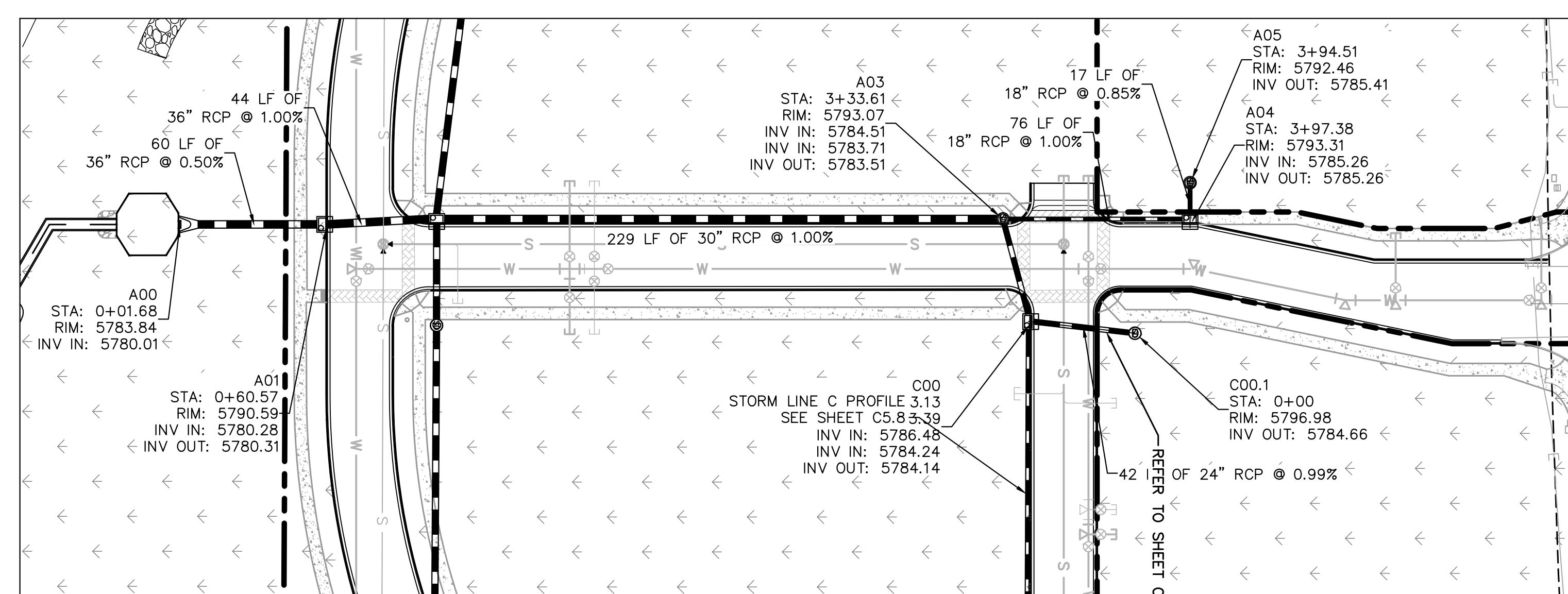
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PROJECT NO.  
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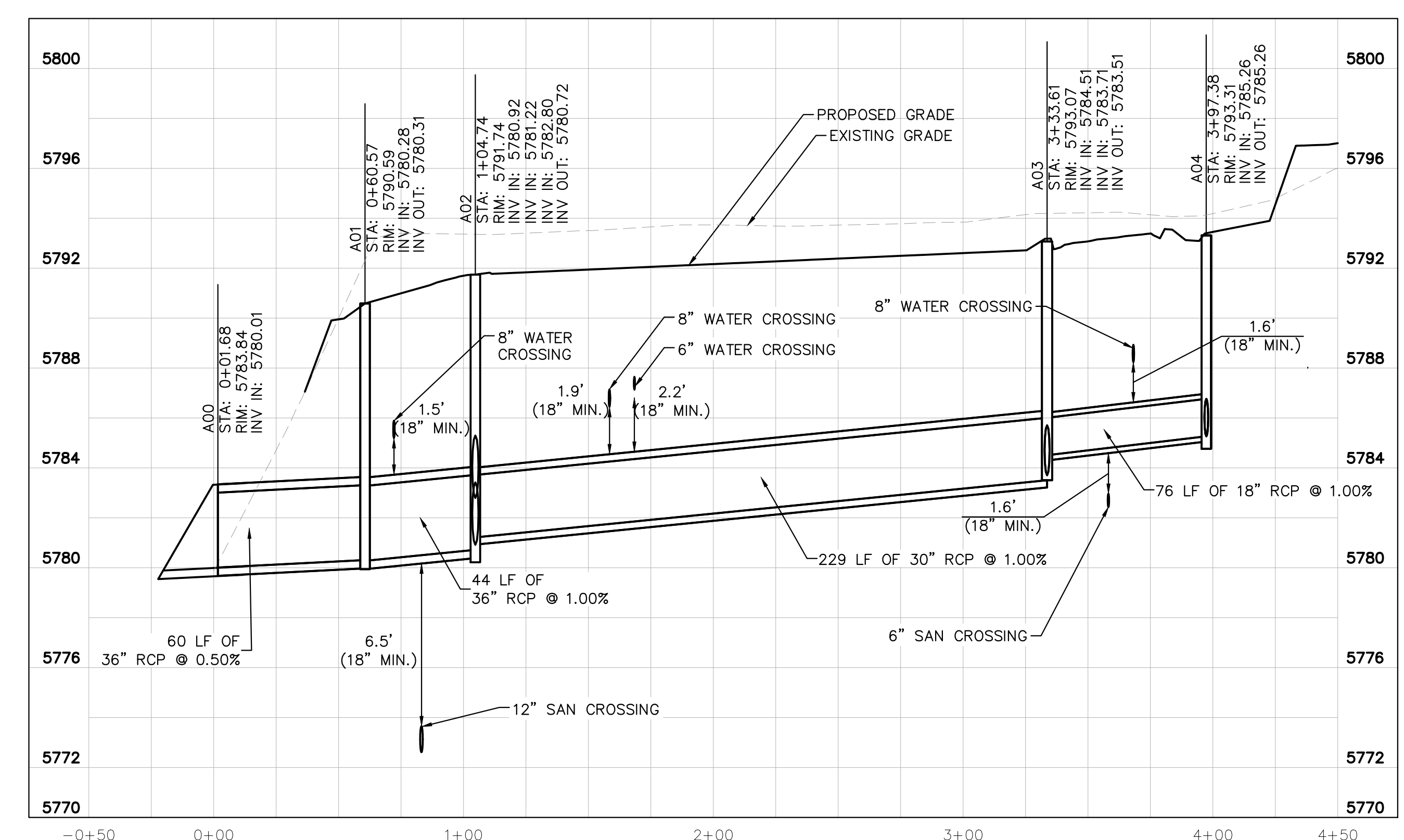
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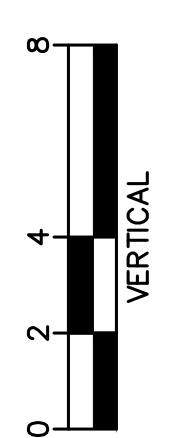


STRUCTURE TABLE		
STRUCTURE NAME:	DESCRIPTION:	LOCATION:
A00	OUTLET	N: 11940.6071 E: 50137.5692
A01	TYPE R 10' INLET	N: 12000.1427 E: 50137.5726
A02	TYPE R MODIFIED INLET	N: 12044.3094 E: 50137.5726
A03	5' DIA MANHOLE	N: 12273.1682 E: 50135.4060
A04	TYPE R 5' INLET	N: 12348.8235 E: 50137.5726
A05	5' DIA MANHOLE	N: 12348.8235 E: 50120.7393
B00	TYPE R 10' INLET	N: 12026.8637 E: 49987.5740
B01	5' DIA MANHOLE	N: 12060.7173 E: 50001.3228
B02	5' DIA MANHOLE	N: 12044.1939 E: 50178.4126
B03	TYPE R 5' INLET	N: 12046.5109 E: 50366.5611
B04	5' DIA MANHOLE	N: 12102.1929 E: 50421.7791
B05	TYPE R 5' INLET	N: 12092.1007 E: 50606.3725
B05.1	5' DIA MANHOLE	N: 12133.6552 E: 50606.5543
B06	5' DIA MANHOLE	N: 12071.7851 E: 50650.2930
B07	5' DIA MANHOLE	N: 11963.3147 E: 50650.2930
B07.1	5' DIA MANHOLE	N: 11963.3062 E: 50685.9988
B08	TYPE R 10' INLET	N: 11907.6200 E: 50652.4644
B08.1	5' DIA MANHOLE	N: 11907.6120 E: 50685.9988
C00	TYPE R 5' INLET	N: 12284.8966 E: 50176.9060
C00.1	5' DIA MANHOLE	N: 12326.5470 E: 50181.6014
C01	TYPE R 5' MANHOLE	N: 12284.2299 E: 50375.1453
C01.1	5' DIA MANHOLE	N: 12328.8687 E: 50374.9646
D00	OUTLET	N: 11849.1405 E: 50322.3179
D01	5' DIA MANHOLE	N: 11849.2713 E: 50430.9191



STORM LINE A PROFILE [STA 0+00 TO 4+50]

HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=4'



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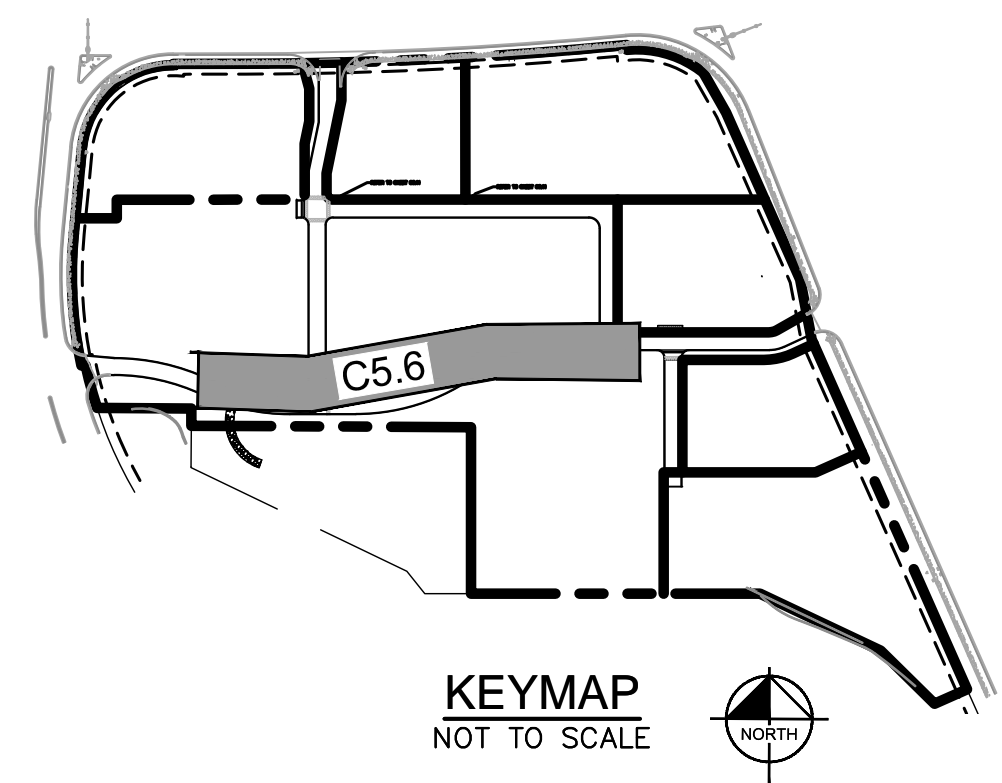
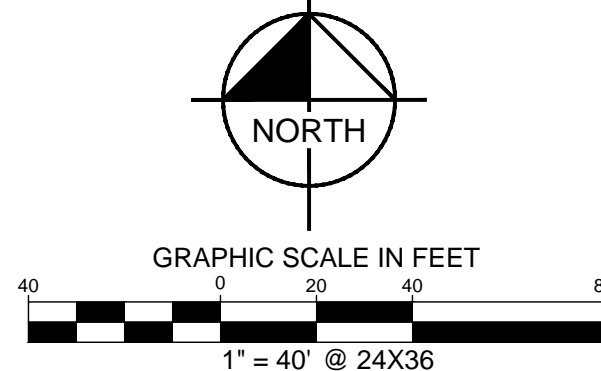
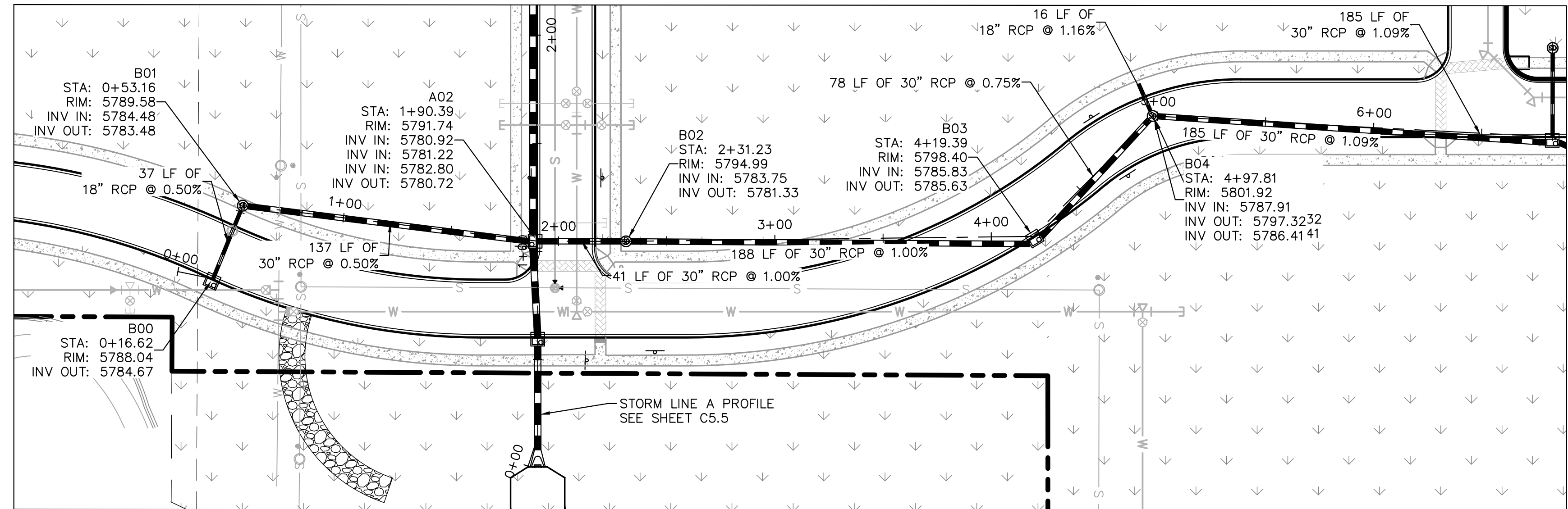
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**STORM-A PLAN AND PROFILE**

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PROJECT NO.  
 096502001  
 DRAWING NAME  
 096502001ST  
**C5.5**

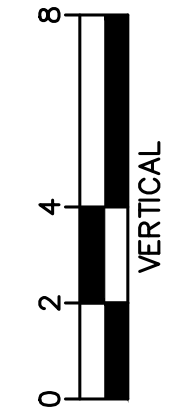
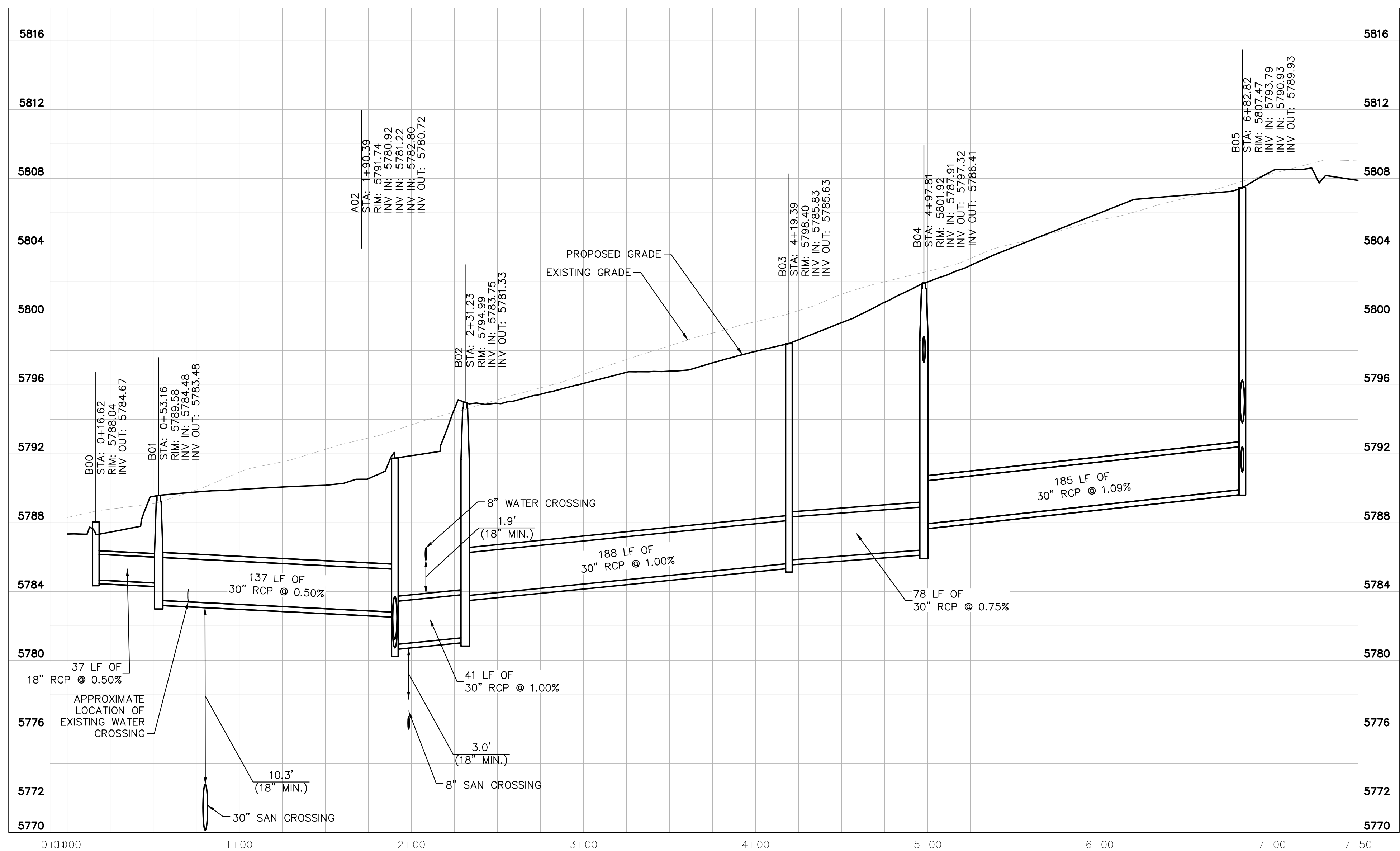


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- LEGEND**
- PROPERTY LINE
  - - - - - PROPOSED STORM SEWER
  - PROPOSED STORM INLET
  - PROPOSED STORM MANHOLE

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D00	OUTLET	N: 11849.1405 E: 50322.3179
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STORM LINE B PROFILE [STA 0+00 TO 7+00]

HORIZONTAL SCALE: 1"=40'  
 VERTICAL SCALE: 1"=4'

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 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

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 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

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 PARKER, CO  
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**STORM-B PLAN AND PROFILE**

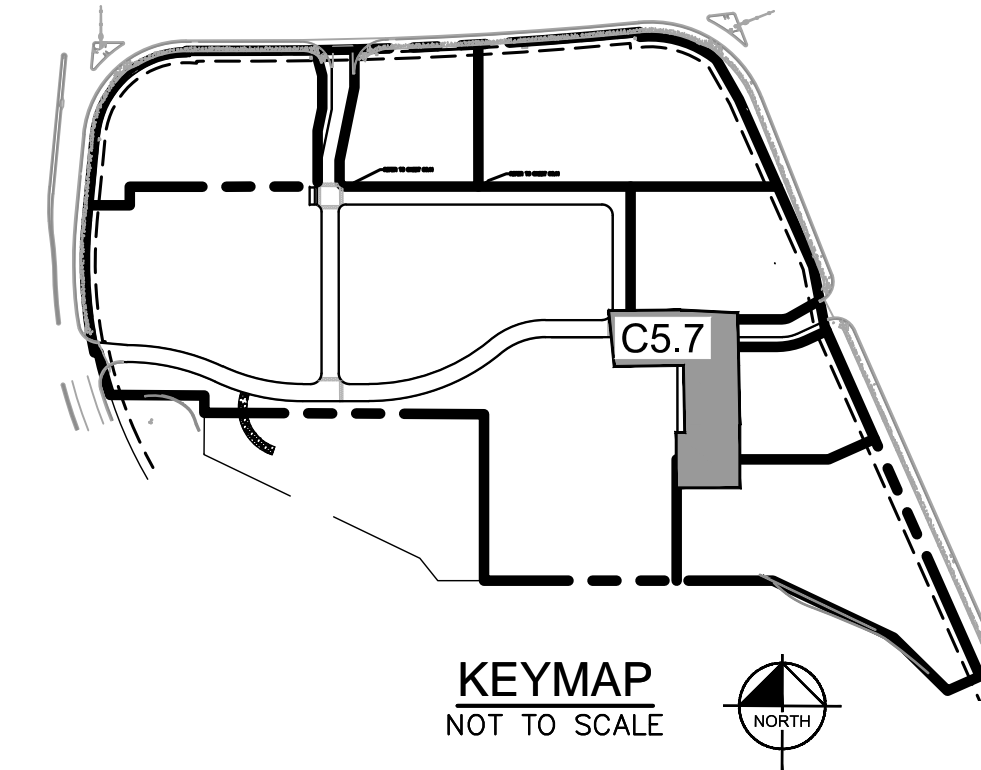
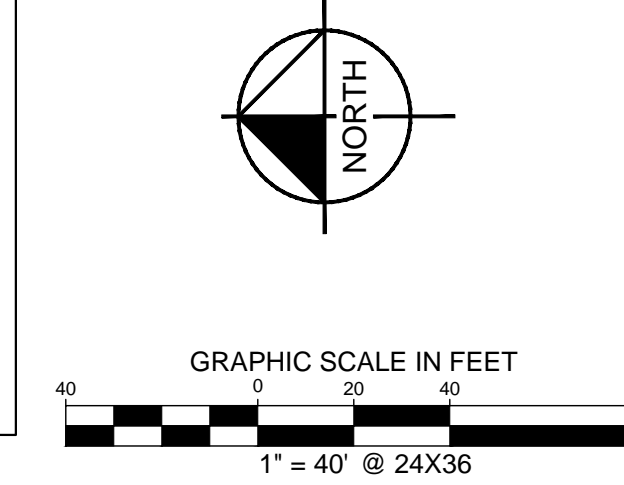
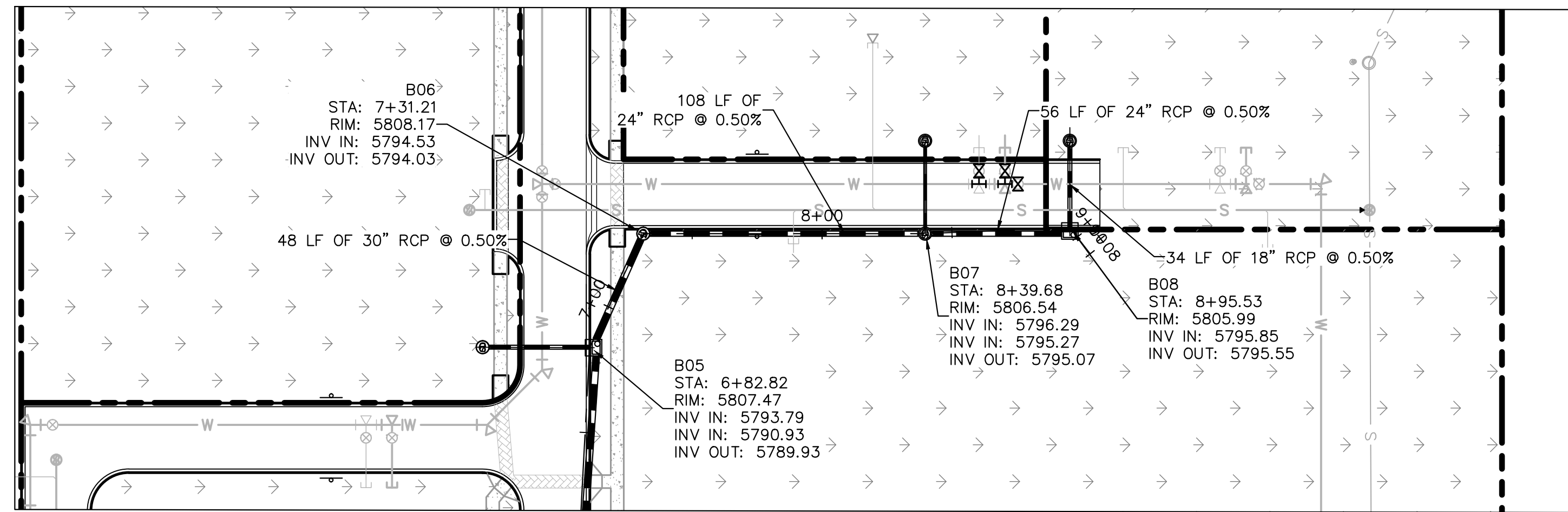
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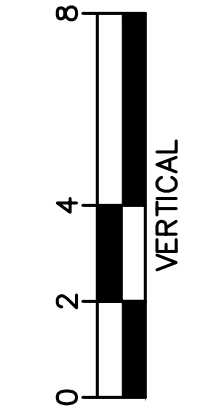
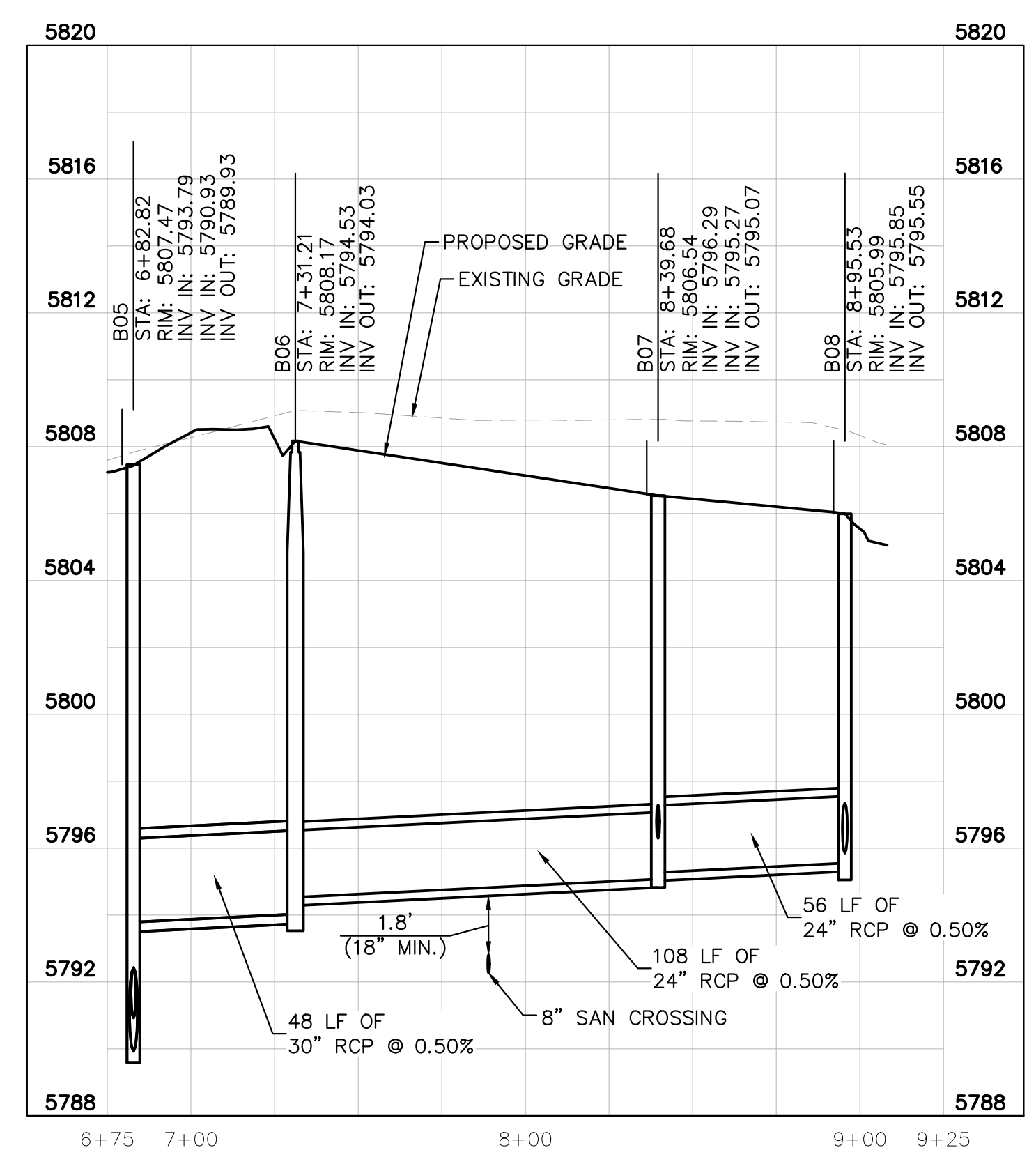
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- LEGEND**
- PROPERTY LINE
  - PROPOSED STORM SEWER
  - PROPOSED STORM INLET
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STORM LINE B PROFILE [STA 7+00 TO 9+00]

HORIZONTAL SCALE: 1"=40'  
 VERTICAL SCALE: 1"=4'

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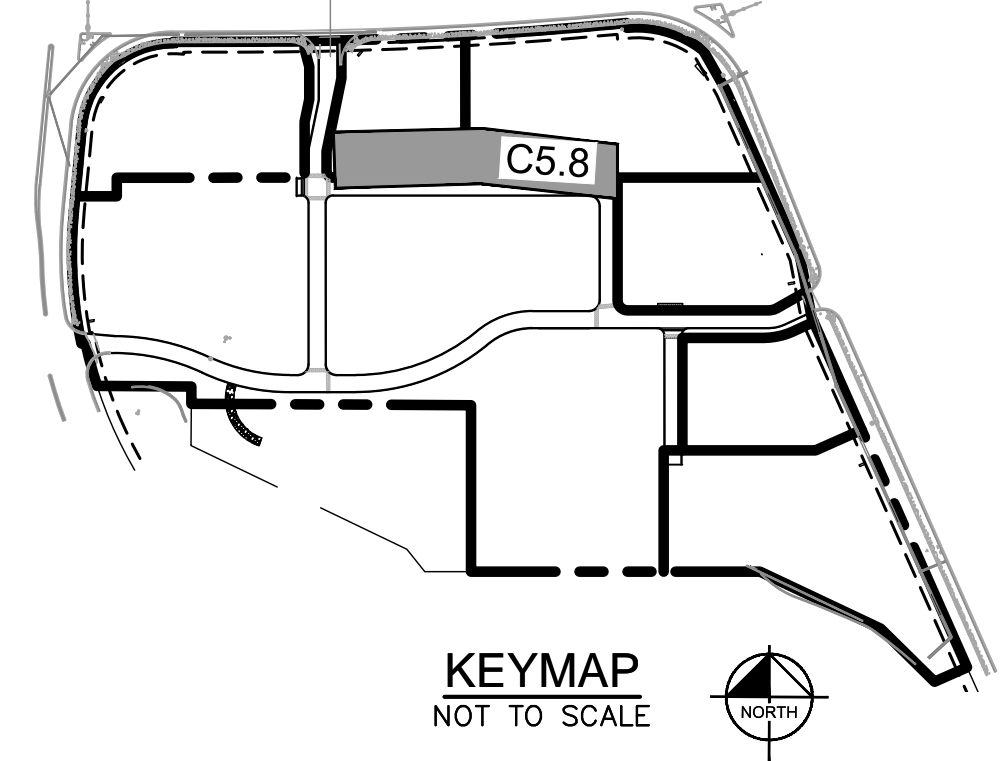
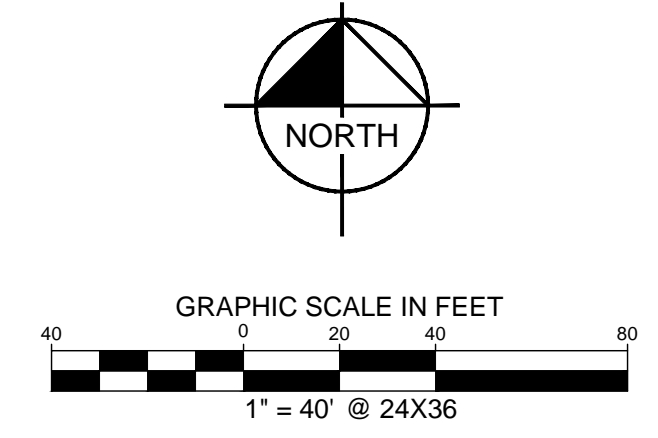
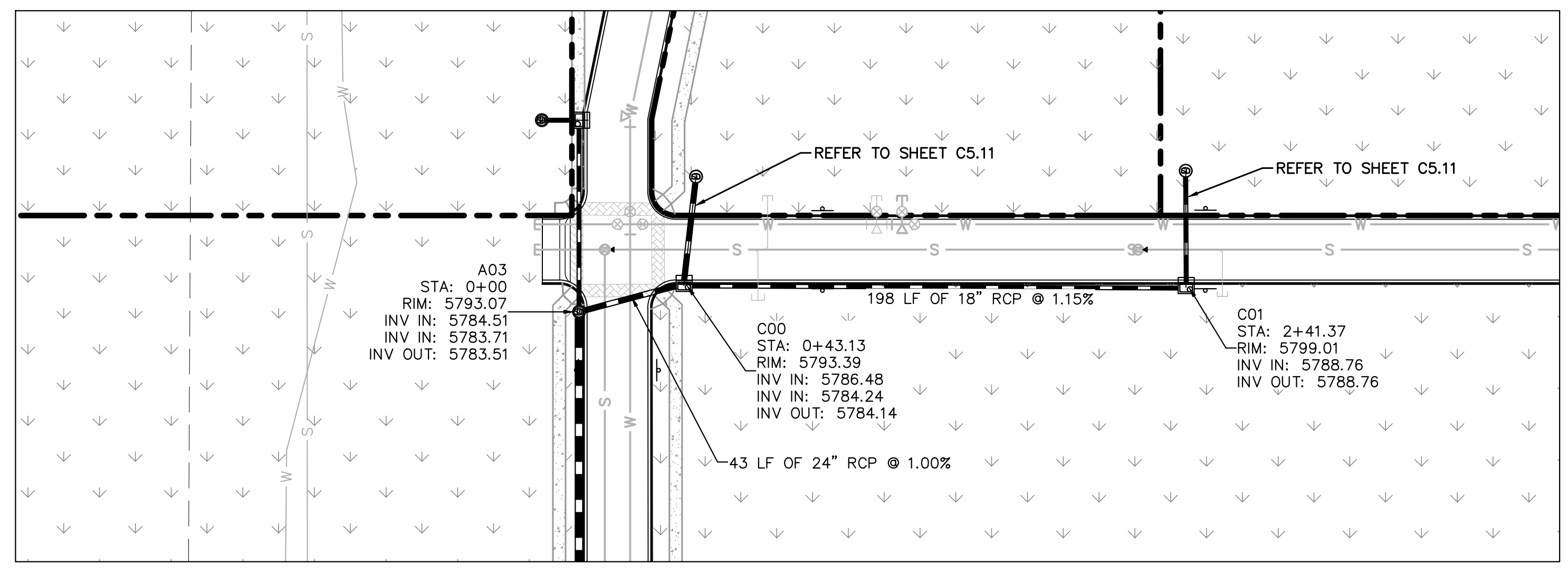
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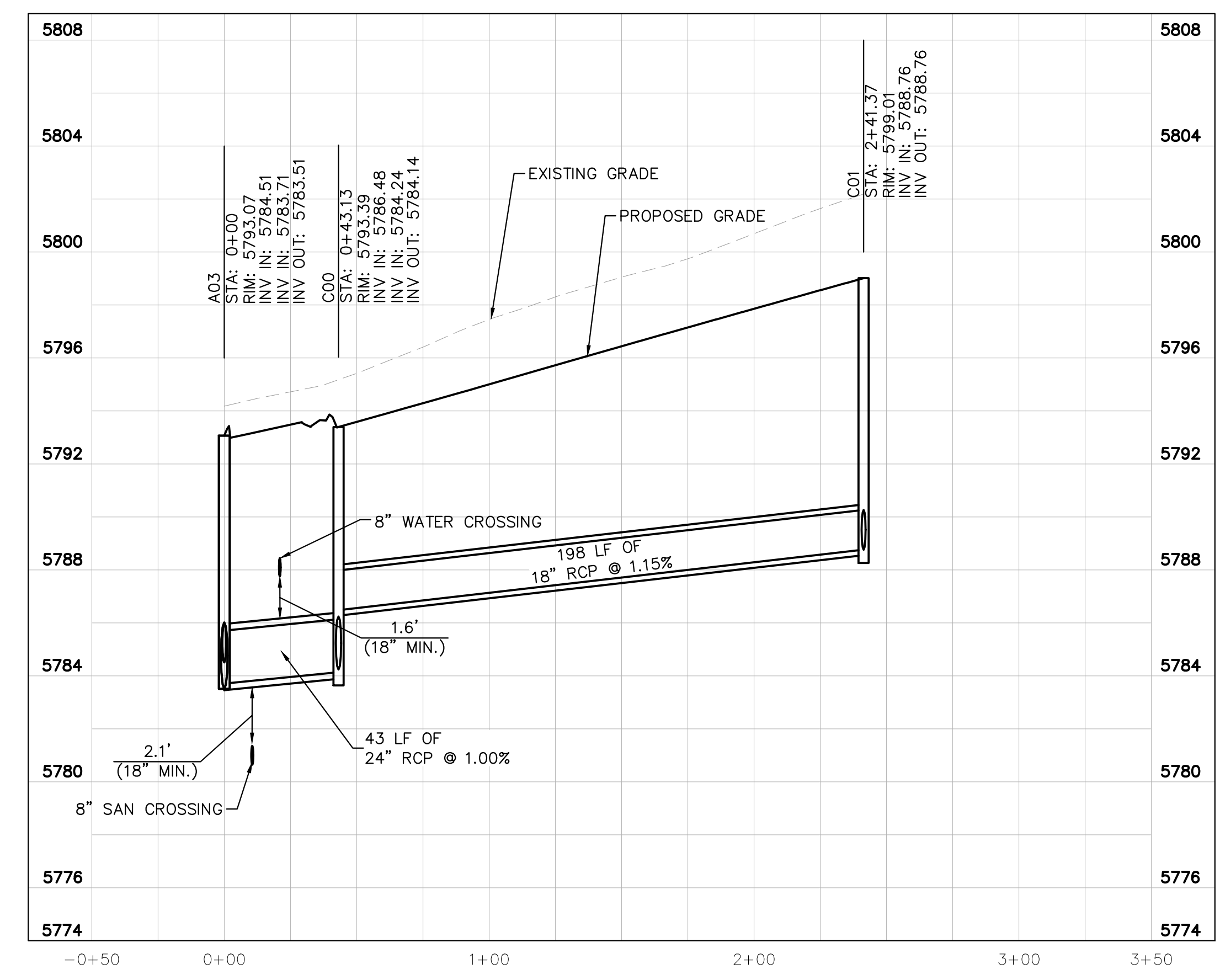
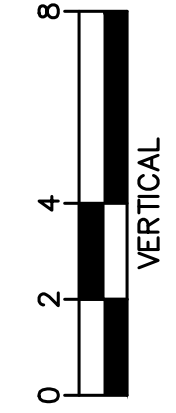


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- LEGEND**
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B07	5' DIA MANHOLE	N: 11963.3147 E: 50650.2930
B07.1	5' DIA MANHOLE	N: 11963.3062 E: 50685.9988
B08	TYPE R 10' INLET	N: 11907.6200 E: 50652.4644
B08.1	5' DIA MANHOLE	N: 11907.6120 E: 50685.9988
C00	TYPE R 5' INLET	N: 12284.8966 E: 50176.9060
C00.1	5' DIA MANHOLE	N: 12326.5470 E: 50181.6014
C01	TYPE R 5' MANHOLE	N: 12284.2299 E: 50375.1453
C01.1	5' DIA MANHOLE	N: 12328.8687 E: 50374.9646
D00	OUTLET	N: 11849.1405 E: 50322.3179
D01	5' DIA MANHOLE	N: 11849.2713 E: 50430.9191



STORM LINE C PROFILE [STA 5+00 TO 10+00]

HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=4'

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

NO.	REVISION	BY	DATE	APPR

**Kimley»Horn**  
 2019 KIMLEY-HORN AND ASSOCIATES, INC.  
 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

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 CONSTRUCTION DOCUMENTS  
**STORM-C PLAN AND PROFILE**

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PROJECT NO.  
096502001  
 DRAWING NAME  
096502001ST  
**C5.8**

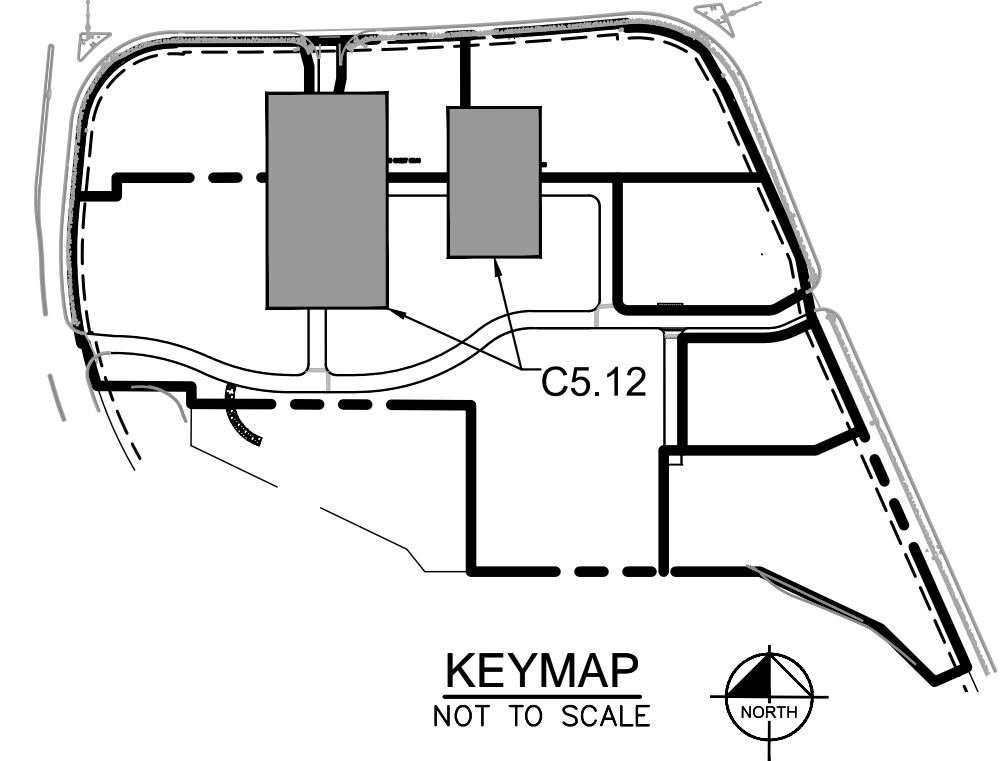
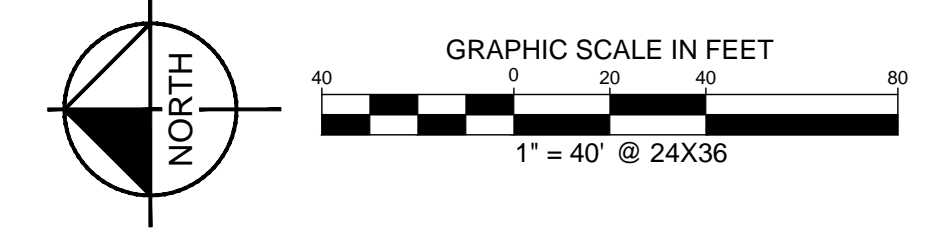
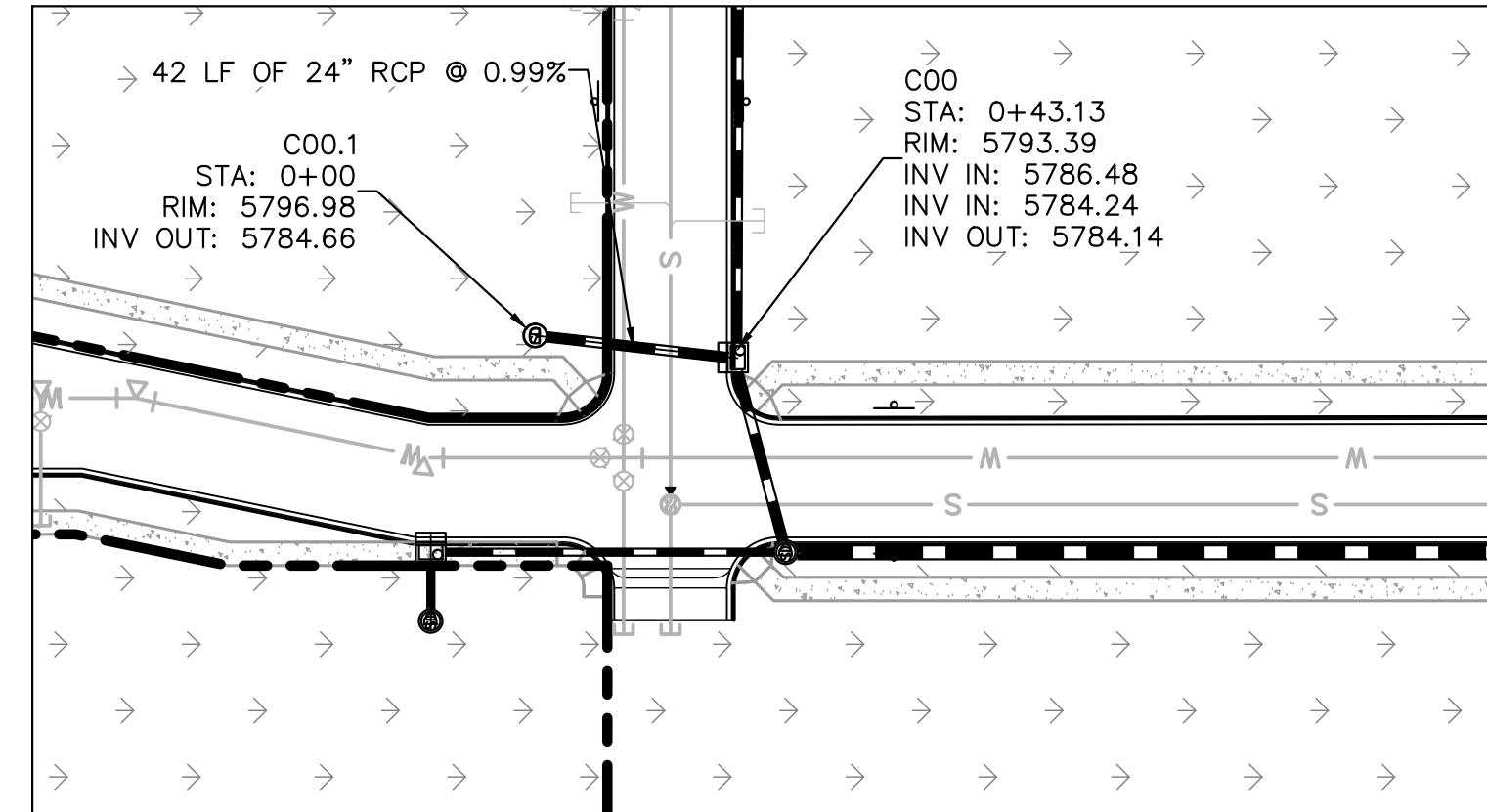
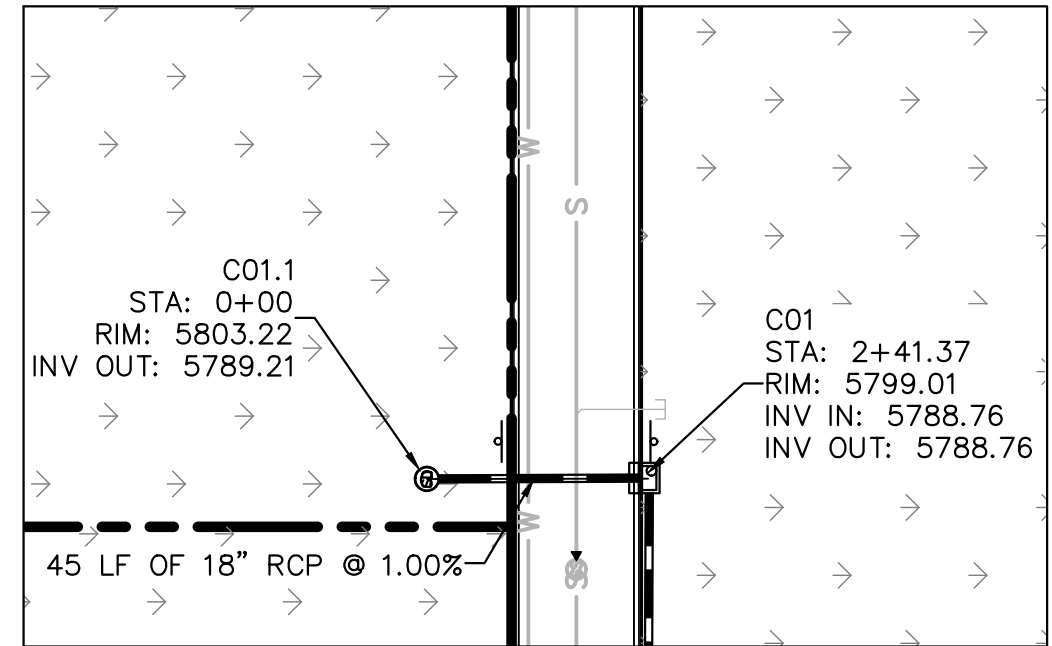
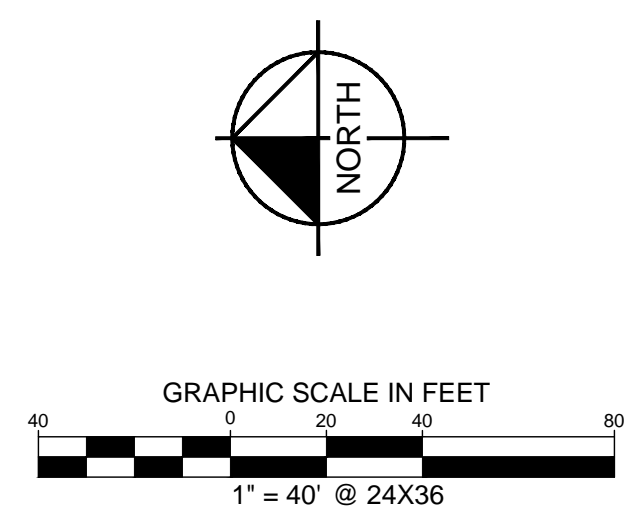






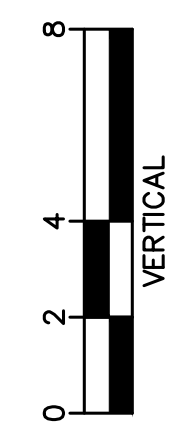
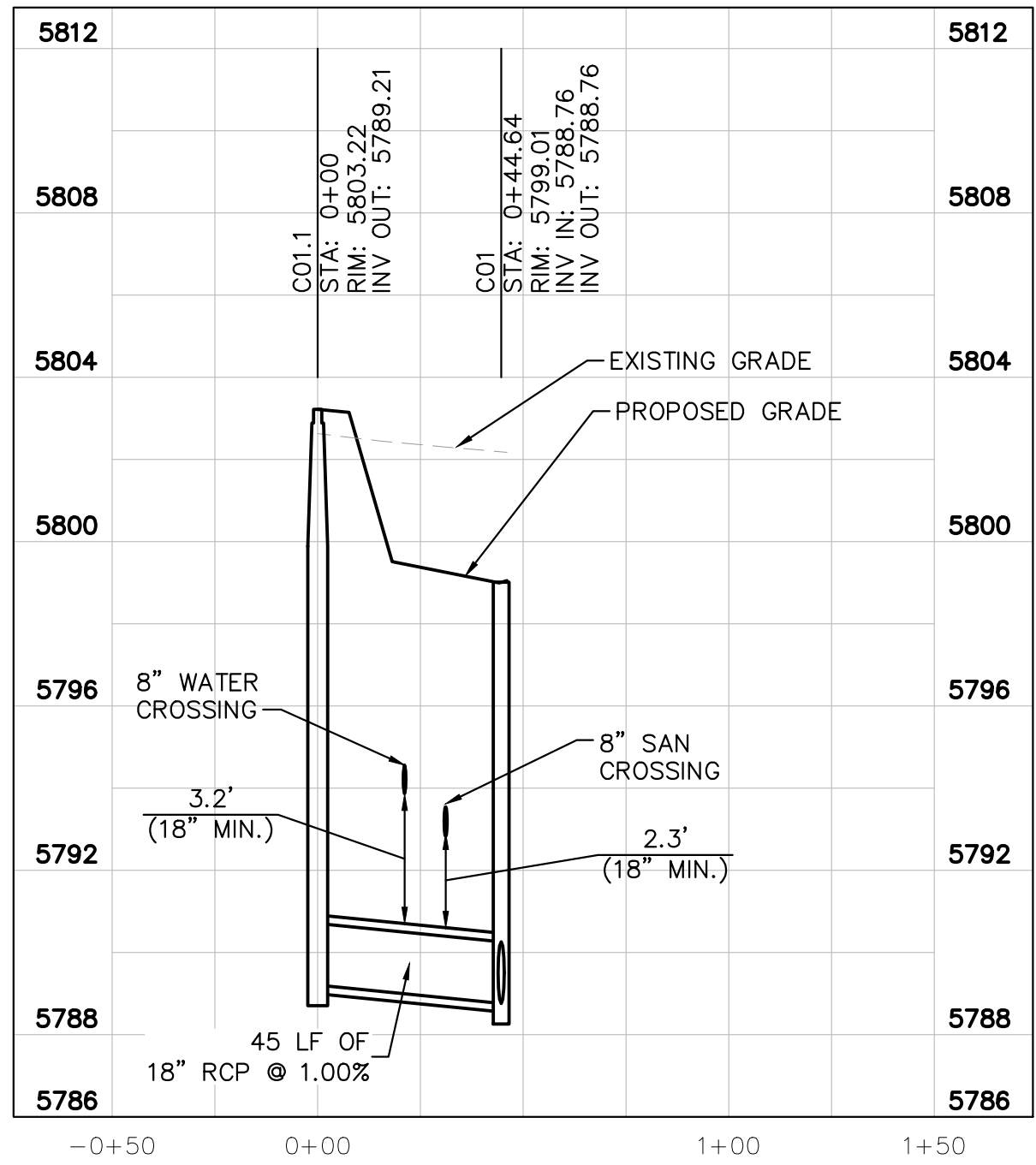


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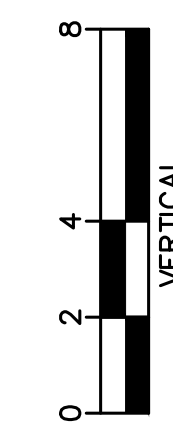
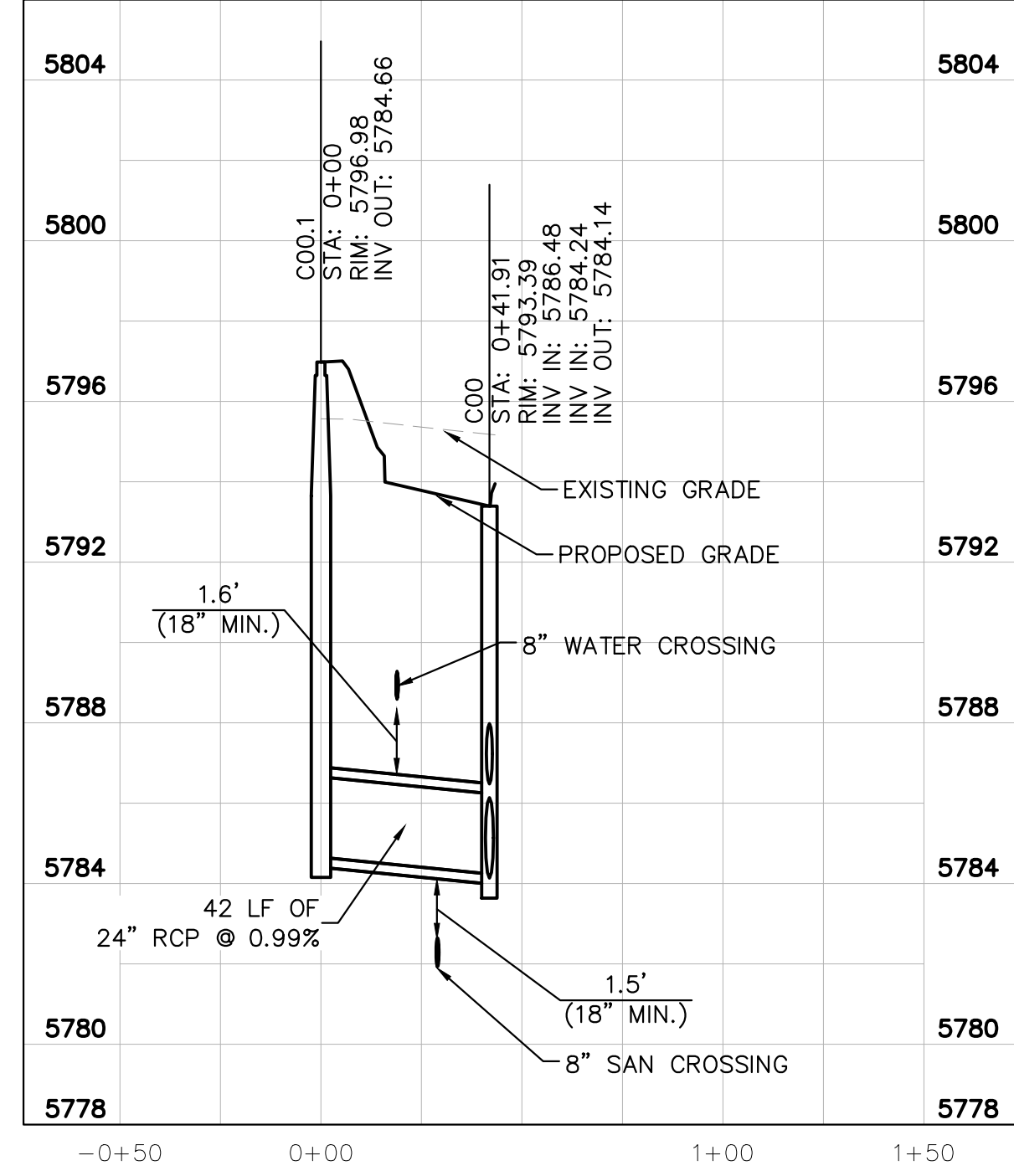
- LEGEND**
- PROPERTY LINE
  - - - - - PROPOSED STORM SEWER
  - PROPOSED STORM INLET
  - PROPOSED STORM MANHOLE

STRUCTURE TABLE		
STRUCTURE NAME:	DESCRIPTION:	LOCATION:
A00	OUTLET	N: 11940.6071 E: 50137.5692
A01	TYPE R 10' INLET	N: 12000.1427 E: 50137.5726
A02	TYPE R MODIFIED INLET	N: 12044.3094 E: 50137.5726
A03	5' DIA MANHOLE	N: 12273.1682 E: 50135.4060
A04	TYPE R 5' INLET	N: 12348.8235 E: 50137.5726
A05	5' DIA MANHOLE	N: 12348.8235 E: 50120.7393
B00	TYPE R 10' INLET	N: 12026.8637 E: 49987.5740
B01	5' DIA MANHOLE	N: 12060.7173 E: 50001.3228
B02	5' DIA MANHOLE	N: 12044.1939 E: 50178.4126
B03	TYPE R 5' INLET	N: 12046.5109 E: 50366.5611
B04	5' DIA MANHOLE	N: 12102.1929 E: 50421.7791
B05	TYPE R 5' INLET	N: 12092.1007 E: 50606.3725
B05.1	5' DIA MANHOLE	N: 12133.6552 E: 50606.5543
B06	5' DIA MANHOLE	N: 12071.7851 E: 50650.2930
B07	5' DIA MANHOLE	N: 11963.3147 E: 50650.2930
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D00	OUTLET	N: 11849.1405 E: 50322.3179
D01	5' DIA MANHOLE	N: 11849.2713 E: 50430.9191



STORM LATERAL C.2 PROFILE  
[STA 0+00 TO 1+00]

HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=4'



STORM LATERAL C.1 PROFILE  
[STA 0+00 TO 1+00]

HORIZONTAL SCALE: 1"=40'  
VERTICAL SCALE: 1"=4'

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NO.	REVISION	BY	DATE	APPR

**Kimley»Horn**  
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 4582 South Ulster Street, Suite 1500  
 Denver, Colorado 80237 (303) 228-2300

DESIGNED BY: DLS  
 DRAWN BY: ECZ  
 CHECKED BY: DLS  
 DATE: 11/14/19

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 PARKER, CO  
 CONSTRUCTION DOCUMENTS  
**STORM-C LATERALS PLAN AND PROFILE**

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PROJECT NO.  
096502001  
 DRAWING NAME  
096502001ST  
**C5.12**

I:\den\_civil\09560201.dwg - revised use perker (A) CAUTION: This sheet is a preliminary drawing. It is not to be used for construction. All dimensions and notes are subject to change without notice. The user of this drawing shall be responsible for verifying all dimensions and notes. The user of this drawing shall be responsible for verifying all dimensions and notes. The user of this drawing shall be responsible for verifying all dimensions and notes.

**GENERAL NOTES**

1. SINCE ALL PIPE ENTRIES INTO THE BASE ARE VERTICAL, THE DIMENSIONS SHOWN ARE TYPICAL ACTUAL DIMENSIONS AND QUANTITIES FOR CONCRETE AND REINFORCEMENT SHALL BE AS REQUIRED IN THE WORK.
2. THE PRECAST FLAT TOP MAY BE USED ON ANY MANHOLE, THE ECCENTRIC CONE MAY BE USED WHEN THE MANHOLE "H" HEIGHT IS AT LEAST 8 FT.
3. THE MANHOLE RING FRAME SHALL BE SET IN A BED OF GROUT. THE FRAME SHALL BE CONCREDED WITH A CONCRETE GROUT IN UNPAVED AREA OR A CONCRETE COLLAR IN PAVED AREA. SEE DETAILS ON SHEETS 2 AND 3.
4. DESIGN OF BOX BASE IS BASED ON STRAIGHT RISE OF PIPE OR CHANGE IN DIRECTION OF LESS THAN 45° SLOPE. DESIGN IS REQUIRED FOR 45° OR GREATER.
5. PRECAST MANHOLES AND REINFORCEMENT SHALL CONFORM TO AASHTO M 198 (ASTM C 478).
6. CAST-IN-PLACE MANHOLES SHALL BE CLASS B CONCRETE.
7. STEPS SHALL BE REQUIRED WHEN THE MANHOLE DEPTH EXCEEDS 3 FT. 6 IN. AND SHALL CONFORM TO AASHTO M 199.
8. ALL REINFORCING STEEL SHALL BE GRADE 60 AND TYPICALLY COATED. VERTICAL STEEL SHALL BE PLACED AT CENTERLINE OF WALL. ALL BARS SHALL HAVE A 2 IN. MINIMUM CLEARANCE.
9. ALL PIPE ENTRIES INTO THE BASE OF MANHOLE SHALL BE CONNECTED BY OVER CHANNELIZATION UNLESS FOR PIPE SIZE, SHAPE, SLOPE AND DIRECTION OF FLOW DETAILS SHOWN ARE TYPICAL FOR INSTALLATIONS WITH ALL SHEETS OF SAME RELATIVE ELEVATION FOR EXCESSIVE ELEVATION DIFFERENCE BETWEEN INVERTS SPECIAL BASE CHANNEL DETAILS WILL BE SHOWN ON THE PLANS.
10. FLOW CHANNELS AND INVERTS SHALL BE FORMED BY SHAPING WITH CLASS B CONCRETE OR APPROVED GROUT.
11. STEPS SHALL BE SET 2 FT. MINIMUM BEYOND OUTSIDE WALL SURFACE OF MANHOLE AND BE SATISFACTORILY PLUGGED.
12. THE SLOPE OF THE MANHOLE COVER SHALL MATCH THE MANHOLE RING AND COVER 1/4" MIN. TO 1/2" MAX.

**QUANTITIES FOR CONCRETE MANHOLE BOX BASE**

MARK	SIZE	TYPE	WT. (LBS.)	NO.	FORMULAS
401	4	I	0.668	1	401 BAR LENGTH = 32" + 2W + 1.0.
402	4	III	0.668	1	402 BAR LENGTH = 1.0 + 2W
501	5	I	1.043	1	501 BAR LENGTH = 24" + 1.0 + 2W
502	5	I	1.043	1	502 NUMBER BARS REQ'D = 3 + (24W/L) + 1
503	5	II	1.043	1	503 NUMBER BARS REQ'D = 2 + (24W/L) + 1
504	5	I	1.043	1	504 BAR LENGTH = 32" + 2W + 1.0.
1001	11	I	5.313	1	1001 BAR LENGTH = 21" + 1.0 + 2W
1002	11	I	5.313	1	1002 BAR LENGTH = 21" + 1.0 + 2W

**Computer File Information**  
 Creation Date: 07/24/12  
 Last Modification Date: 07/24/12  
 Drawing File Name: M-604-20.dwg  
 CAD Ver.: MicroStation V8  
 Scale: Not to Scale  
 Units: English

**Sheet Revisions**

Date	Comments

**Colorado Department of Transportation**  
 4301 East Arkansas Avenue  
 Denver, Colorado 80222  
 Phone: (303) 757-8663  
 Fax: (303) 757-8820

**MANHOLES**  
 STANDARD PLAN NO. M-604-20  
 Project Development Branch DD/LTA Issued By: Project Development Branch July 4, 2012  
 Sheet No. 1 of 3

**PRECAST MANHOLE BASES NOTES**

1. THE BASE SHALL BE FORMED MONOLITHICALLY WITH BOTTOM RISER SECTION.
2. PRECAST MANHOLE BASES SHALL FIT THE CONDITIONS AND LOCATIONS FOR WHICH THEY ARE INTENDED WITHOUT ANY FIELD MODIFICATIONS. ANY MANHOLE BASE WHICH REQUIRES FIELD CUTTING OR MODIFICATION IN ORDER TO FIT THE LOCATION INTENDED WILL BE REJECTED BY THE ENGINEER AND REMOVED AND REPLACED BY THE CONTRACTOR AT HIS COST TO THE PROJECT.
3. PRECAST MANHOLE BASES SHALL BE BEDDED ON AN APPROVED GRANULAR BEDDING MATERIAL AS SHOWN ABOVE.

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**MANHOLES**  
 STANDARD PLAN NO. M-604-20  
 Project Development Branch DD/LTA Issued By: Project Development Branch July 4, 2012  
 Sheet No. 2 of 3

**T-BASE MANHOLE NOTES**

1. THE T-BASE SECTION SHALL BE SHOP-FABRICATED FOR DELIVERY TO THE CONSTRUCTION SITE AS A COMPLETE UNIT.
2. THESE DETAILS SHOW ONLY THE CONCEPTUAL AND STANDARD DIMENSIONAL REQUIREMENTS FOR TYPICAL MANHOLES. THE CONTRACTOR SHALL FURNISH DETAILED SHOP DRAWINGS FOR APPROVAL PRIOR TO FABRICATION. THE DETAILS SHOWN HEREIN APPLY TO 48 IN. AND GREATER CONCRETE PIPES.
3. EXCEPT FOR CLASS OF PIPE, SPECIFICATIONS FOR THE MANHOLE SHALL BE THE SAME AS THOSE REQUIRED FOR THE ADJOINING PIPE.
4. THE T-BASE SECTION SHALL MAINTAIN ITS INTERNAL SHAPE AND FLOW AREA GROUTING OR FILLING SHALL BE APPLIED SO AS TO NOT DISTURB THE NORMAL FLOW OR REDUCE THE AREA.

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**MANHOLES**  
 STANDARD PLAN NO. M-604-20  
 Project Development Branch DD/LTA Issued By: Project Development Branch July 4, 2012  
 Sheet No. 3 of 3

**GENERAL NOTES**

1. ON ROADWAY CURVES WITH A RADIUS OF 1500 FT. OR LESS, CURBS AND GUTTERS ARE TO BE PLACED IN THE ARC OF THE CURVE UNLESS OTHERWISE NOTED BY THE PLANS. A MAXIMUM CHORD LENGTH OF 30 FT. MAY BE USED WHEN THE CURVE RADIUS IS GREATER THAN 1500 FT.
2. CONCRETE SHALL BE CLASS B.
3. PROFILE GRADE OF CURBS AND GUTTERS SHALL BE LOCATED AT THE FLOW LINE.
4. CURB TYPE 4 (KEYWAY) MAY BE USED IN LIEU OF CURB AND GUTTER TYPE 2 (SECTIONS B AND IM) UNLESS OTHERWISE SPECIFIED ON THE PLANS.
5. GUTTER CROSS SLOPES MAY BE ADJUSTED TO FACILITATE DRAINAGE FOR PROFILE GRADES AS SHOWN ON THE PLANS.
6. THICKNESS OF CURB AND GUTTER SECTION SHALL MATCH CONCRETE PAVEMENT THICKNESS IF SHOWN ON THE PLANS. CURB AND GUTTER SHALL BE CLASS B CONCRETE IF PLACED MONOLITHICALLY WITH CONCRETE PAVEMENT.
7. INCREASE SIDEWALK THICKNESS TO 6 IN. AT LOCATIONS SHOWN ON THE PLANS.
8. MINIMUM SIDEWALK WIDTH IS 4 FT.
9. EXPANSION JOINTS SHALL BE INSTALLED WHEN ADJUTING EXISTING CONCRETE OR FIXED STRUCTURE EXPANSION JOINT MATERIAL SHALL BE 1/2" IN THICK AND SHALL EXTEND THE FULL DEPTH OF CONTACT SURFACE.
10. GUTTER CROSS SLOPES SHALL BE 1/2" IN 10 FT. WHEN DRAINING AWAY FROM CURB AND IN 1/4" IN 10 FT. WHEN DRAINING TOWARD CURB WITH EXCEPT TO IMMEDIATELY ADJACENT TO CURB RAMPS - SEE STANDARD PLAN M-608-5 FOR SLOPE REQUIREMENTS.
11. WHEN TIE BARS ARE REQUIRED, THE GUTTER THICKNESS SHALL BE INCREASED TO THE PAVEMENT THICKNESS TO BARS SHALL BE PROVIDED AT 2 FT. INTERVALS. THEY SHALL BE INSERTED 1/2" AND 1/2" INTO THE GUTTERS.
12. THIS AREA SHALL BE FORMED MONOLITHICALLY WITH CURB AND GUTTER AND PAD FOR AS "CONCRETE PAVEMENT".
13. FLOW LINE LOCATION WILL BE ESTABLISHED BY 1/2" SHOWN ON PLANS.

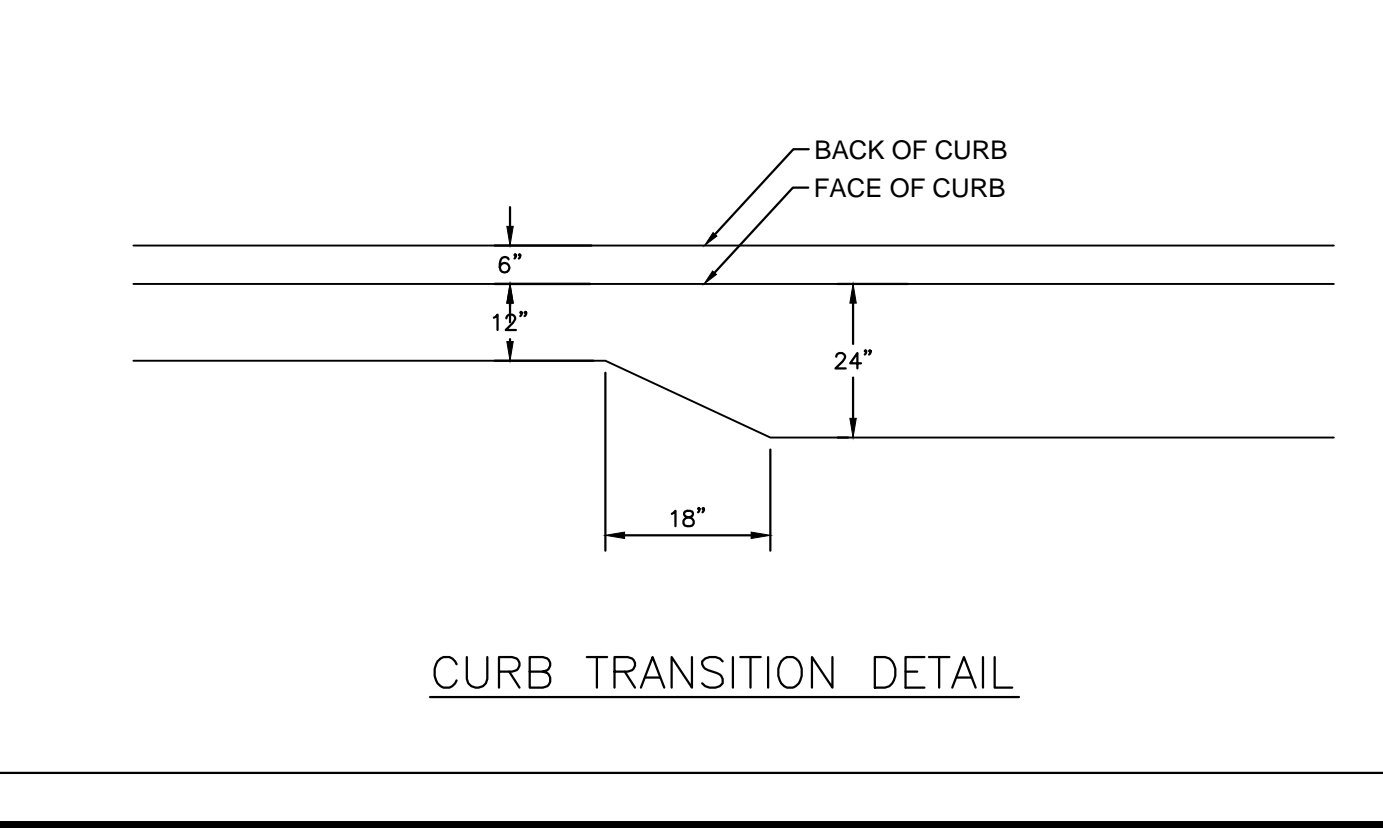
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 4301 East Arkansas Avenue  
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**CURB, GUTTERS, AND SIDEWALKS**  
 STANDARD PLAN NO. M-609-1  
 Project Development Branch DLM/LTA Issued By: Project Development Branch on July 4, 2012  
 Sheet No. 1 of 4



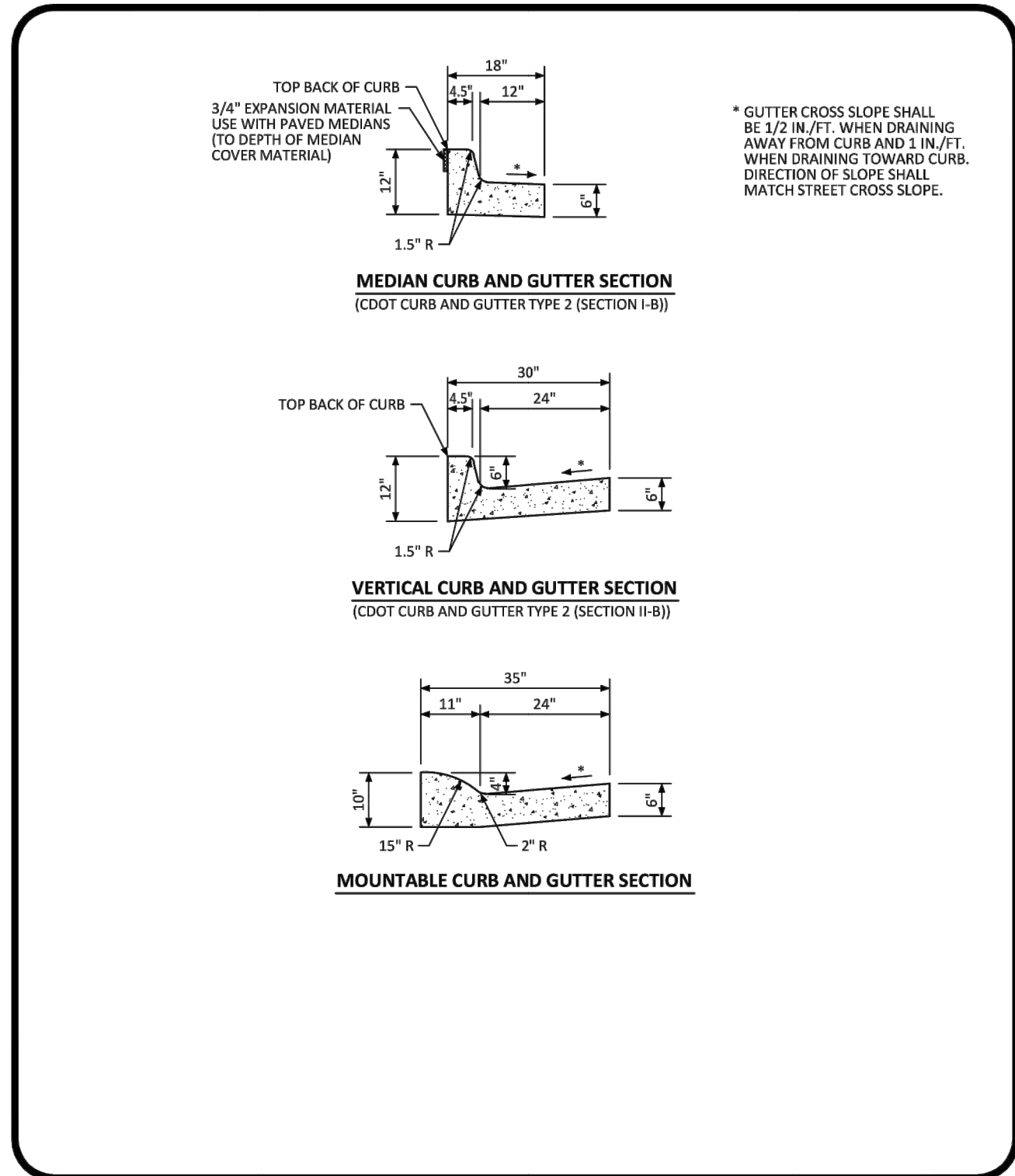
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 PROJECT NO. 096502001  
 DRAWING NAME 096502001P-W-DT  
 C6.0



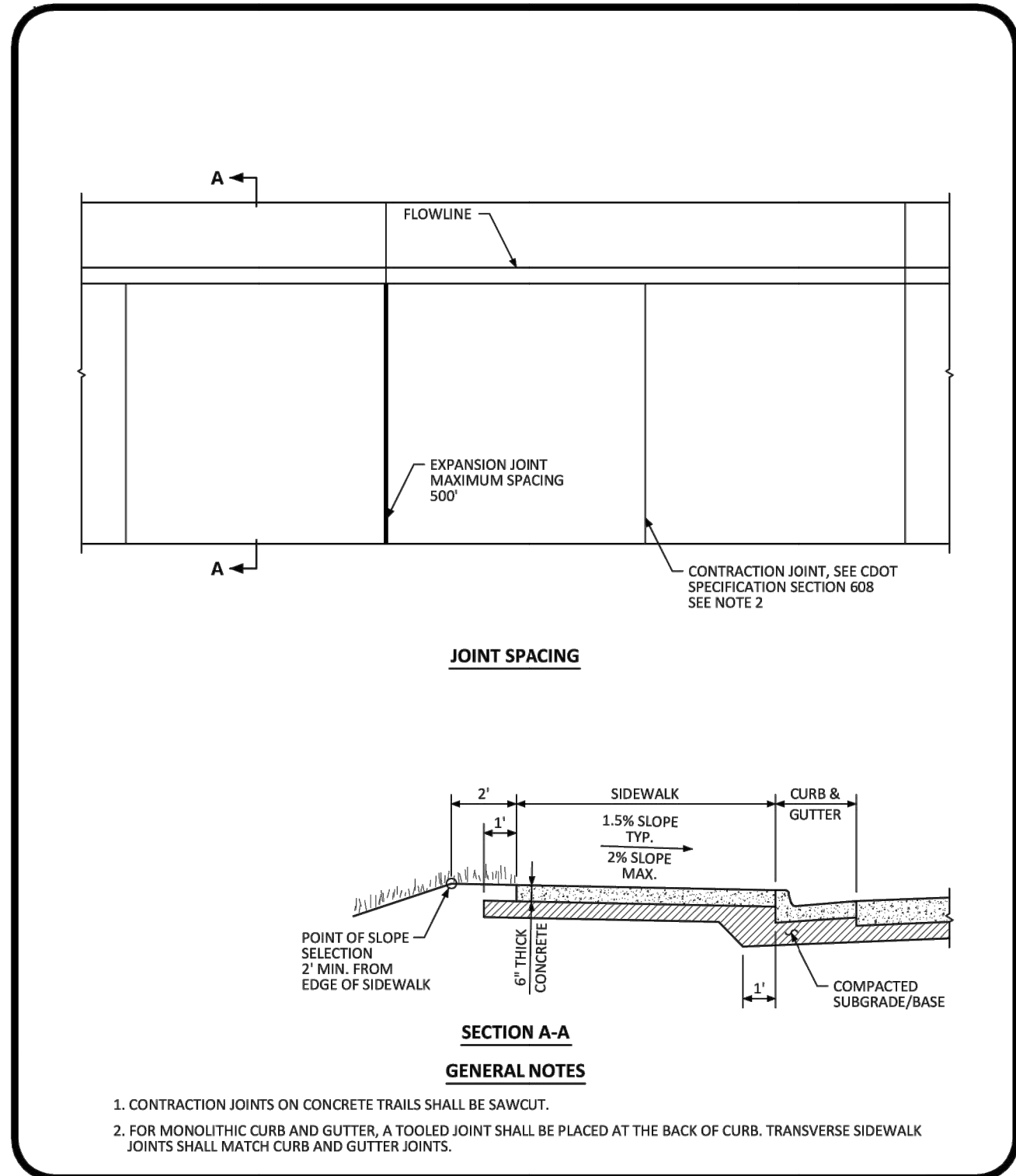
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**CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL**

DATE AUGUST 2014

DETAIL 3

1 OF 1



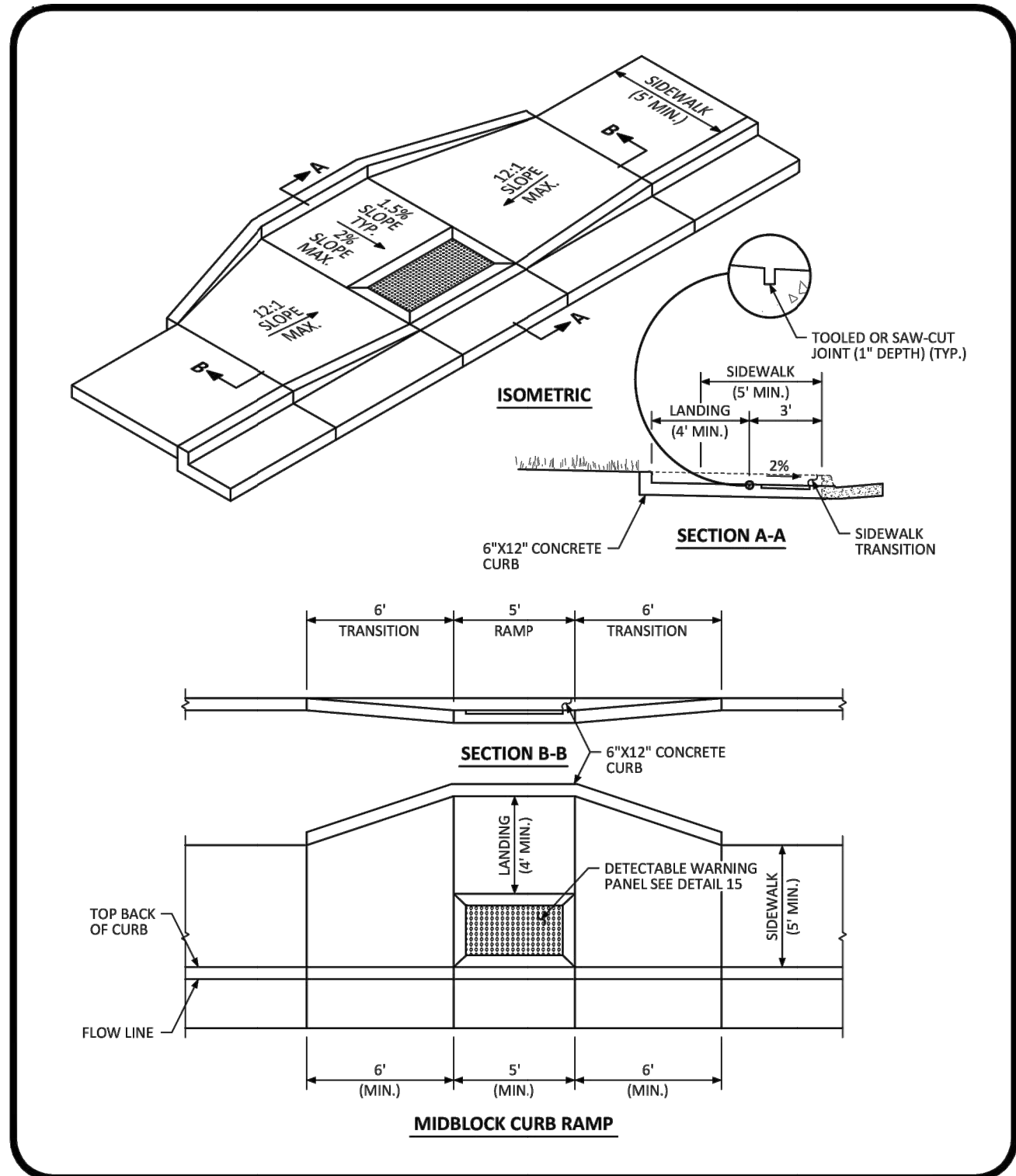
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**ATTACHED SIDEWALK STANDARD DETAIL**

DATE AUGUST 2014

DETAIL 4

2 OF 2



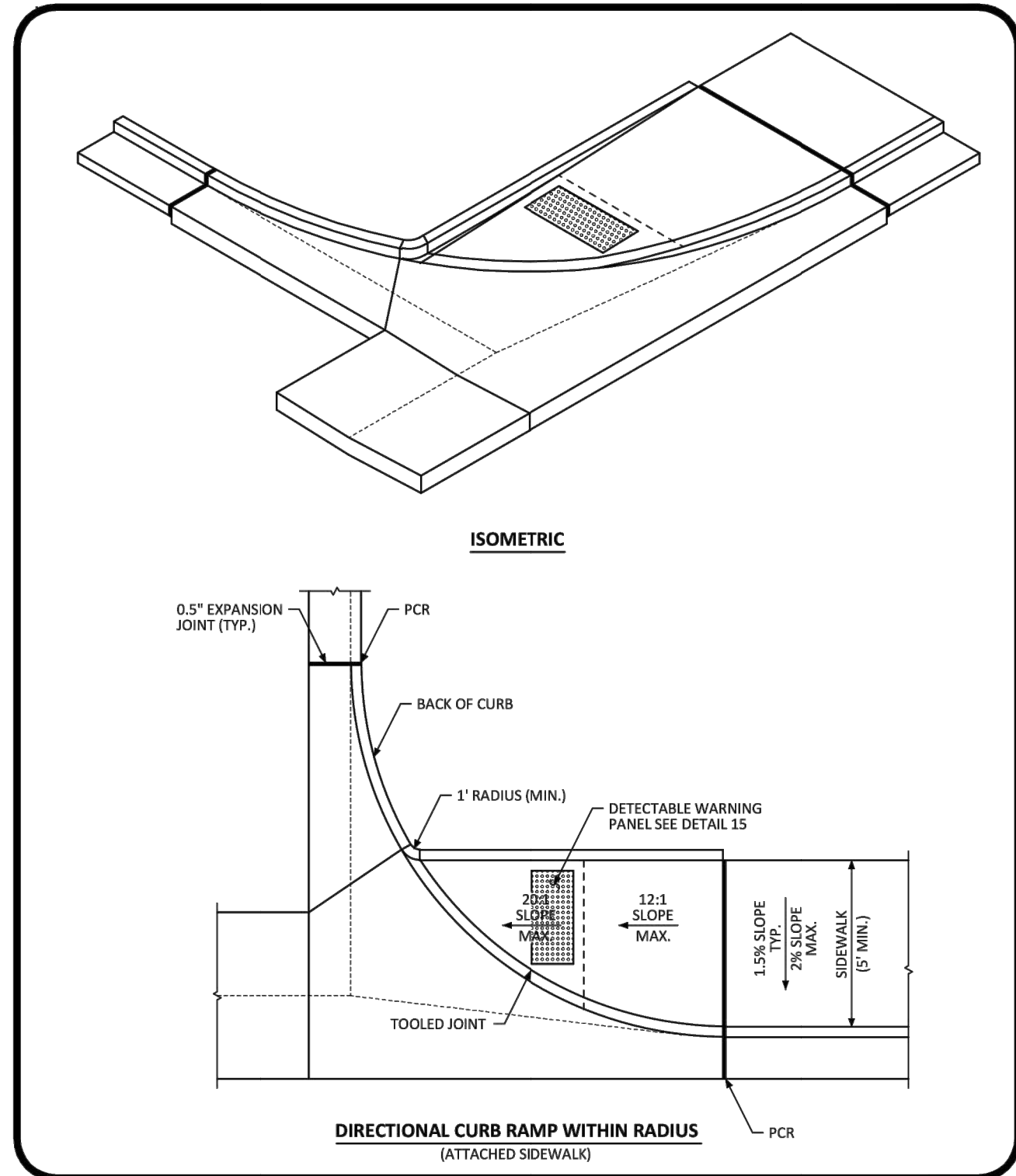
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**MIDBLOCK CURB RAMP LAYOUT STANDARD DETAIL**

DATE AUGUST 2014

DETAIL 17

2 OF 2



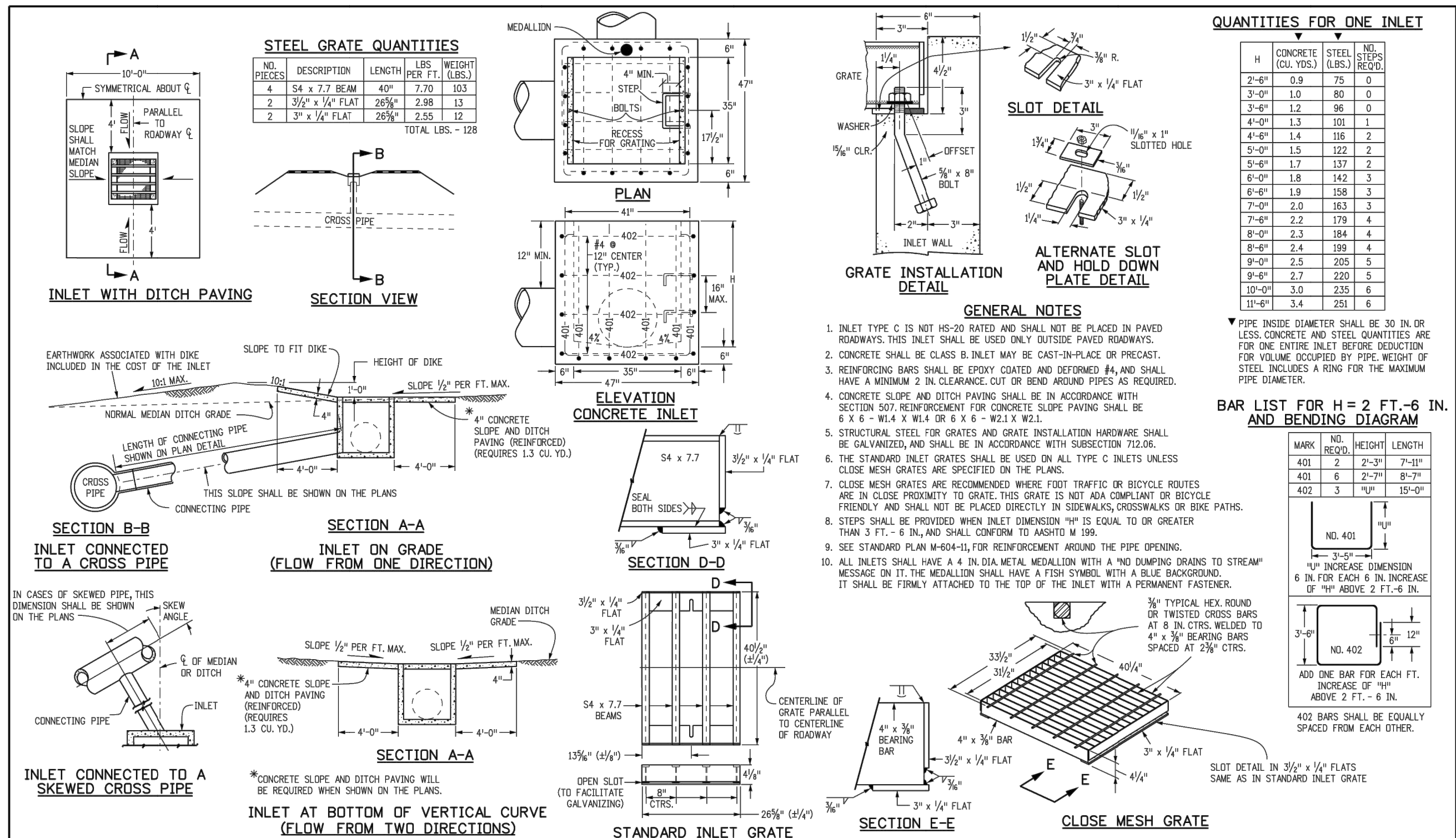
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**DIRECTIONAL CURB RAMP WITHIN RADIUS LAYOUT STANDARD DETAIL**

DATE AUGUST 2014

DETAIL 18

1 OF 2



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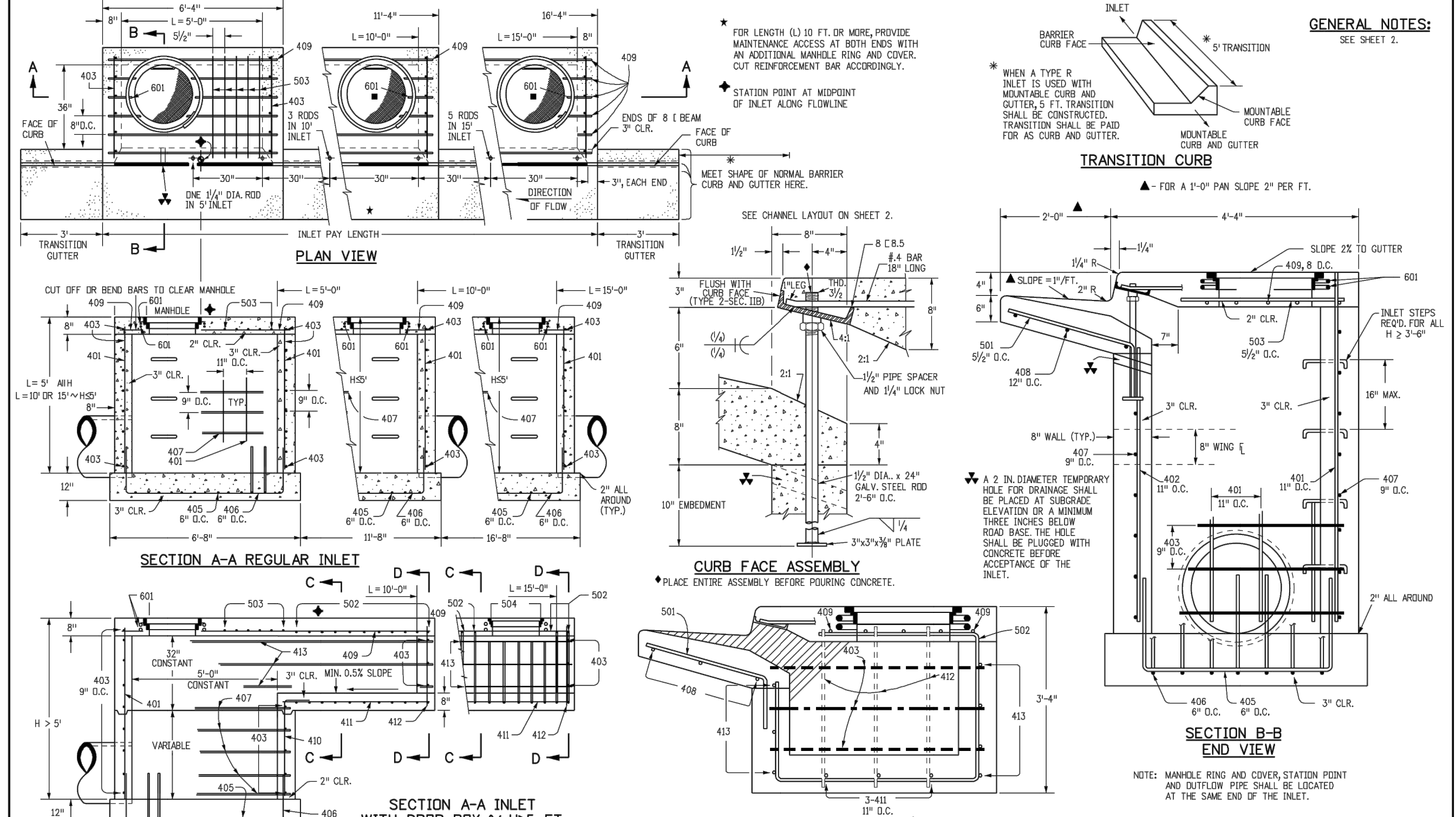
STANDARD PLAN NO. M-604-10

Sheet No. 1 of 1

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Project Development Branch DD/LTA

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**CURB INLET TYPE R**

STANDARD PLAN NO. M-604-12

Sheet No. 1 of 2

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DESIGNED BY: DLS  
DRAWN BY: ECZ  
CHECKED BY: DLS  
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DETAILS

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PROJECT NO.  
095602001

DRAWING NAME  
095602001PW-DT

C6.1



