

**CONTACT LIST**

**DEVELOPER – REPUBLIC INVESTMENT GROUP**

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**ENGINEER**

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5690 WEBSTER ST.  
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20120 E. MAINSTREET  
PARKER, CO. 80138  
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STORM WATER – JACOB JAMES

**TOWN OF PARKER POLICE DEPARTMENT**

19,600 E. PARKER SQUARE DR.  
PARKER, CO. 80134 303-841-9800

**PARKER WATER AND SANITATION DISTRICT**

18100 E. WOODMAN DR.  
PARKER, CO. 80134 303-841-4627

**FIRE & LIFE SAFETY**

RANDALL CAPRA  
20120 E. MAINSTREET  
CENTENNIAL, CO 80138 303-205-3169

**UTILITY NOTIFICATION COMPANY OF COLORADO**

2801 SOUTH YOUNGFIELD ST, SUITE 301  
GOLDEN, CO 80401 800-922-1987  
LOCAL CALLS (811)

**XCEL ENERGY**

MICHELLE O'NAN  
5909 E. 38TH AVE.  
DENVER, CO 80207 303-329-1618

**COMCAST**

KEVIN YOUNG  
6850 S. TUCSON WAY  
ENGLEWOOD, CO 80112 720-490-3867

**CENTURY LINK**

WILLIAM BENSON  
9750 E. COSTILLA AVE.  
ENGLEWOOD, CO 80220 303-792-6069

**COLORADO DEPARTMENT OF HEALTH**

4210 E. 11TH AVE.  
DENVER, CO 303-320-8333

**INTERMOUNTAIN RURAL ELECTRIC ASSOCIATION**

JASON JENKS  
5496 NORTH US HWY 85  
P.O. BOX DRAWER A  
SEDALIA, CO 80135 303-688-3100

**URBAN DRAINAGE FLOOD CONTROL DISTRICT (UDFCD)**

MIKE SARMENTO  
2480 W. 26TH AVE.  
SUITE 156-B  
DENVER, CO 80211 303-455-6277

**BASIS OF BEARINGS:**

THE SOUTH LINE OF THE SOUTHEAST QUARTER OF SECTION 29  
TOWNSHIP6 SOUTH, RANGE 66 WEST, 6TH P.M. BEARING SOUTH  
89°15'13" WEST AS SHOWN ON FINAL PLAT DOUGLAS 234  
FILING NO. 1

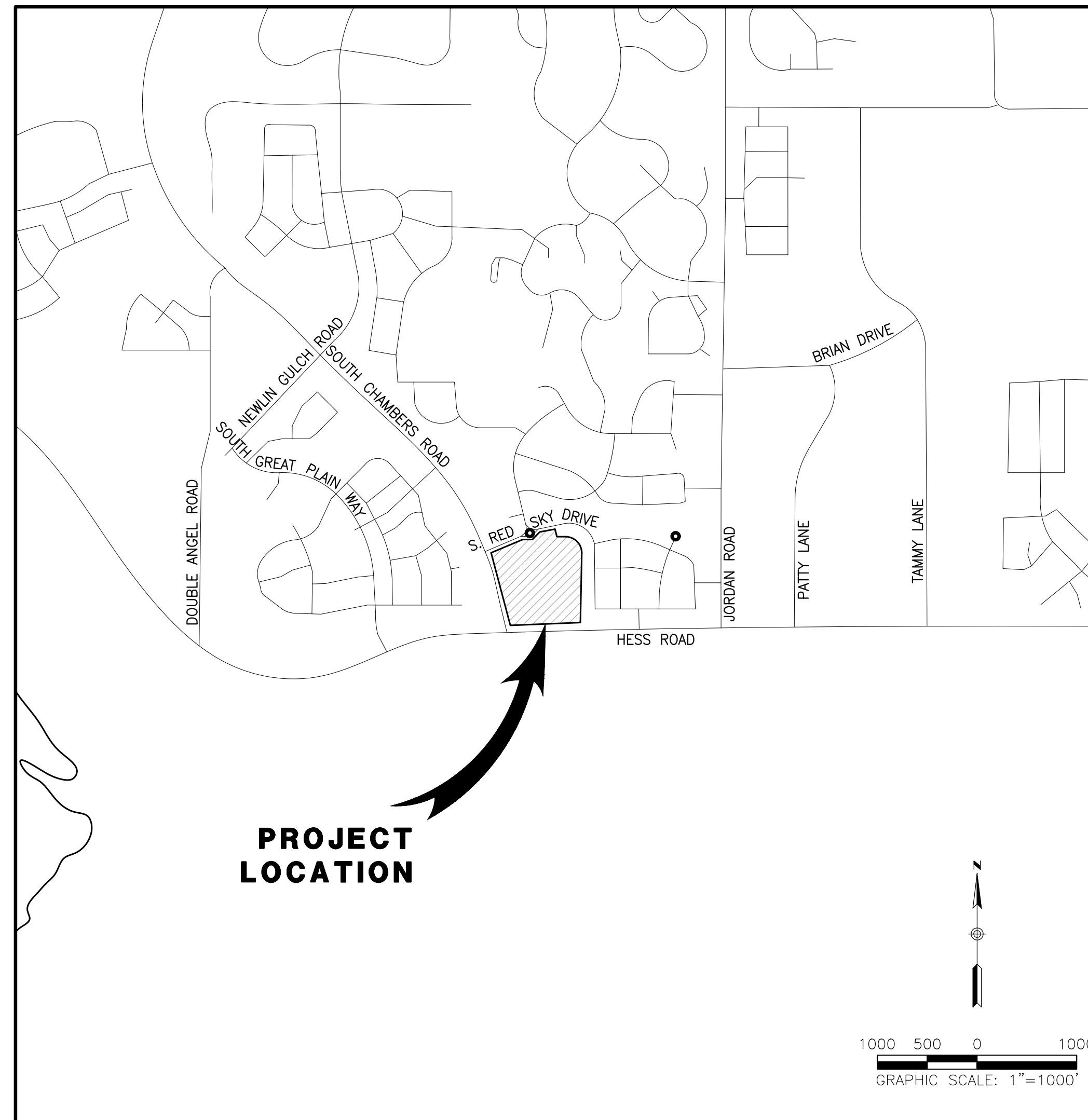
**BENCHMARKS:**

BENCHMARK IS BY GPS OBSERVATION NAVD 88. SET SITE  
BENCHMARK AT NORTHWEST CORNER OF INLET ON CHAMBERS  
RD. APPROXIMATELY 200'+/- NORTH OF HESS RD.

**ROADWAY & STORM DRAIN CONSTRUCTION PLANS FOR**

**DOUGLAS 234, FILING NO. 6**

A PORTION OF THE SOUTHWEST QUARTER OF THE SOUTHEAST QUARTER OF SECTION 29,  
TOWNSHIP 6 SOUTH, RANGE 66 WEST, 6TH PRINCIPAL MERIDIAN,  
TOWN OF PARKER, COUNTY OF DOUGLAS, STATE OF COLORADO  
13.80 ACRES, 11 COMMERCIAL LOTS



**VICINITY MAP**

SCALE 1"= 1000'

**ABBREVIATIONS**

BOW	BACK OF WALK	PT	POINT OF TANGENCY
B/P	BOTTOM OF PIPE	PVC	POLYVINYL CHLORIDE PIPE
BC	BEGIN CURVE	PVC	POINT OF VERTICAL CURVE
BM	BENCHMARK	PVI	POINT OF VERTICAL INTERSECTION
BOV	BLOW OFF VALVE	PVT	POINT OF VERTICAL TANGENCY
CL	CENTERLINE	P	PAVEMENT
CY	CUBIC YARD	R	RADIUS
E	ELECTRIC	RCP	REINFORCED CONCRETE PIPE
EC	END CURVE	RIM	RIM ELEVATION
EL	ELEVATION	R.O.W.	RIGHT OF WAY
E/P	EDGE OF PAVEMENT	REM	REMOVAL
EX.	EXISTING	RT	RIGHT
FG	FINISHED GRADE	SAN	SANITARY SEWER
FH	FIRE HYDRANT	SF	SQUARE FOOT
FL	FLOW LINE	SDMH	STORM DRAIN MANHOLE
FS	FINISHED SURFACE	SMH	SANITARY MANHOLE
GV	GATE VALVE	STA.	STATION
HYD.	HYDRANT	STD	STANDARD
INL	INLET	SY	SQUARE YARDS
INV	INVERT	TFI	TOP FRONT OF INLET
LP	LOW POINT	T	TELEPHONE
LT	LEFT	TB	THRUST BLOCK
MAX.	MAXIMUM	TC	TOP OF CURB
MH	MANHOLE	T/P	TOP OF PIPE
MIN.	MINIMUM	TEMP	TEMPORARY
PC	POINT OF CURVE	TRANS	TRANSFORMER
PCC	POINT OF COMPOUND CURVE	VB	VALVE BOX
PCR	POINT OF CURVE RETURN	WM	WATER MAIN
PGL	PROFILE GRADE LINE		
PI	POINT OF INTERSECTION		
PL	PROPERTY LINE		
PROP.	PROPOSED		

PLAN SHEET INDEX	
NO.	TITLE
1	COVER SHEET
2	NOTES
3	EXISTING CONDITIONS
4	OVERALL GRADING PLAN
5	INITIAL CBMP PLAN
6	INTERIM-FINAL CBMP PLAN
7-14	CBMP DETAILS
15	SLICEROO DRIVE PLAN & PROFILE
16	INTERSECTION DETAILS
17	SIGNING & STRIPING
18-20	STORM DRAIN PLAN & PROFILE
21-22	STANDARD DETAILS

**LEGEND**

	EXISTING	PROPOSED
PROPERTY BOUNDARY	---	---
PROPERTY LINE	---	---
RIGHT-OF-WAY	---	---
CENTERLINE	---	---
CURB	---	---
CURB & GUTTER	---	---
DAYLIGHT LINE	---	---
EASEMENT	---	---
FLOWLINE	---	---
SLOPE	---	---
CONTOURS	---	---
WATER LINE	W	W
SANITARY SEWER LINE	S	S
STORM DRAIN LINE	SD	SD
GAS LINE	G	G
ELECTRIC LINE	E	E
FENCE-GENERAL USE	---	---
OVERHEAD WIRES	OHE	OHE
TYPICAL LATERALS	---	---
SANITARY SEWER MANHOLE	SS	SS
STORM DRAIN MANHOLE	SD	SD
STORM DRAIN INLET	---	---
FIRE HYDRANT	---	---
WATER VALVE	---	---
STREET LAMP	---	---
AC PAVING	---	---
CONCRETE	---	---
DG TRAIL/CRUSHER FINES	---	---

**SIGHT DISTANCE CERTIFICATION:**

"I, THE UNDERSIGNED, HEREBY CERTIFY THAT THE PROPOSED SUBDIVISION IS DESIGNED WITH ADEQUATE CORNER SIGHT DISTANCE FOR ALL INTERSECTIONS, AND PEDESTRIAN CROSSING LOCATIONS. CONSIDERATION OF LANDSCAPING IMPACTS TO THE CORNER SIGHT DISTANCE AT BOTH INITIAL AND MATURE GROWTH HAS BEEN REVIEWED. THE MINIMUM CORNER SIGHT DISTANCE IS IN COMPLIANCE WITH AASHTO "POLICY ON GEOMETRIC DESIGN OF HIGHWAY AND STREETS".

I HEREBY AFFIRM THAT THESE CONSTRUCTION PLANS WERE PREPARED BY ME OR UNDER MY DIRECT SUPERVISION.

TROY BALES  
RICK ENGINEERING COMPANY

**TOWN OF PARKER APPROVALS**

THE TOWN OF PARKER REVIEW CONSTITUTES GENERAL COMPLIANCE WITH THE TOWN'S STANDARDS AND APPROVED VARIANCES, SUBJECT TO THESE PLANS BEING STAMPED, SIGNED, AND DATED BY THE PROFESSIONAL ENGINEER OF RECORD. REVIEW BY THE TOWN DOES NOT CONSTITUTE APPROVAL OF THE PLAN DESIGN OR ACCURACY AND CORRECTNESS OF ENGINEERING CALCULATION. ERRORS IN THE DESIGN OR CALCULATIONS REMAIN THE RESPONSIBILITY OF THE REGISTERED PROFESSIONAL ENGINEER WHOSE STAMP AND SIGNATURE ARE AFFIXED TO THIS DOCUMENT.

THIS REVIEW DOES NOT CONSTITUTE APPROVAL OF ANY PRIVATE ON-SITE IMPROVEMENTS WHICH MAY BE SHOWN. CONSTRUCTION CANNOT COMMENCE UNTIL ALL REQUIRED DRAINAGE/TRAFFIC REPORT(S), FINAL DEVELOPMENT PLAN(S), SPECIAL REVIEW(S), GRADING PERMIT, AND/OR OTHER PERMITS ARE COMPLETE, APPROVED AND ON FILE WITH THE TOWN OF PARKER.

TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORKS DATE

**ROADWAY & STORM DRAIN PLANS FOR  
DOUGLAS 234  
FILING, NO. 6**  
DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
9801 EAST EASTER AVE  
CENTENNIAL, CO 80112  
303.537.8020  
Tucson - San Diego - Riverside - Orange  
Sacramento - San Luis Obispo - Phoenix  
rickengineering.com

**811**  
Know what's below.  
Call before you dig!

REVISIONS:		
NO.	DESCRIPTION	DATE:
1	-	-

SCALE: NTS  
DATE: 5/6/2020  
DRAWN BY: WD  
CHECKED BY: JS  
JOB NO: D01173-A

**COVER SHEET**

PROJECT NO.  
D01173-A  
DRAWING NO.  
1  
SHEET NO. 1 OF 22 SHEETS

PRELIMINARY  
NOT FOR CONSTRUCTION

C:\WORK\2020\2020-05-06 - 5:37PM - COVER SHEET.dwg 2020-05-06 5:37PM

**RICK ENGINEERING COMPANY SPECIAL NOTES:**

- 1. ANY QUESTION RAISED RELATIVE TO THE ACCURACY OF IMPROVEMENT INSTALLATION SHALL NOT BE RAISED SUBSEQUENT TO COMPLETION OF THE WORK UNLESS ALL SURVEY STAKES ARE MAINTAINED INTACT. SHOULD SUCH STAKES NOT BE PRESENT AND VERIFIED AS TO THEIR ORIGIN, NO CLAIM FOR ADDITIONAL COMPENSATION FOR CORRECTION SHALL BE PRESENTED TO ANY PARTY AND SUCH WORK SHALL BE CORRECTED BY THE CONTRACTOR AT HIS EXPENSE.
- 2. CONTRACTOR AGREES THAT HE SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR JOBSITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. AND THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR THE ENGINEER.
- 3. NEITHER THE OWNER, NOR THE ENGINEER OF WORK WILL ENFORCE SAFETY MEASURES OR REGULATIONS. THE CONTRACTOR SHALL DESIGN, CONSTRUCT, AND MAINTAIN ALL SAFETY DEVICES, INCLUDING SHORING, AND SHALL BE SOLELY RESPONSIBLE FOR CONFORMING TO ALL LOCAL, STATE, AND FEDERAL SAFETY AND HEALTH STANDARDS, LAWS AND REGULATIONS.
- 4. THE CONTRACTOR SHALL FOLLOW THE GUIDELINES AND REGULATIONS AS SET FORTH BY OSHA
- 5. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT ALL UTILITIES ARE BUILT IN ACCORDANCE WITH THESE PLANS. IF THERE ARE ANY QUESTIONS REGARDING THESE PLANS OR FIELD STAKES, THE CONTRACTOR SHALL REQUEST AN INTERPRETATION BEFORE DOING ANY WORK BY CALLING THE ENGINEER OF WORK AT 303-537-8020. THE CONTRACTOR SHALL ALSO TAKE THE NECESSARY STEPS TO PROTECT THE PROJECT AND ADJACENT PROPERTY FROM ANY EROSION AND SILTATION THAT RESULT FROM HIS OPERATIONS BY APPROPRIATE MEANS (TEMPORARY EROSION CONTROL BMP'S) UNTIL SUCH TIME THAT THE PROJECT IS COMPLETED AND ACCEPTED FOR MAINTENANCE BY WHATEVER OWNER, AGENCY OR ASSOCIATIONS IS TO BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE.
- 6. THE EXISTENCE AND LOCATION OF UNDERGROUND UTILITIES OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED FROM A SEARCH OF THE AVAILABLE RECORDS AND UNDERGROUND UTILITY LOCATING SERVICE AT AN ASCE 38-02 MINIMUM LEVEL B. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO OTHER EXISTING UTILITIES EXCEPT AS SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN HEREON AND ANY OTHER NOT OF RECORD OR NOT SHOWN ON THESE PLANS. ALL DAMAGES THERETO CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE APPROPRIATE SPECIFICATIONS AND AT THE EXPENSE OF THE CONTRACTOR.
- 7. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE ALL UNDERGROUND PIPELINES, TELEPHONE AND ELECTRIC CONDUITS AND STRUCTURES IN ADVANCE OF ANY CONSTRUCTION AND TO OBSERVE ALL POSSIBLE PRECAUTIONS TO AVOID ANY DAMAGE TO SUCH. THE ENGINEER AND/OR OWNER WILL NOT GUARANTEE ANY LOCATIONS AS SHOWN ON THESE PLANS OR THOSE OMITTED FROM SAME.
- 8. CONTRACTOR SHALL MAKE EXPLORATORY EXCAVATIONS AND LOCATE EXISTING UNDERGROUND FACILITIES SUFFICIENTLY AHEAD OF CONSTRUCTION TO PERMIT REVISIONS TO PLANS IF REVISIONS ARE NECESSARY BECAUSE OF ACTUAL LOCATIONS OF EXISTING FACILITIES.
- 9. BEFORE EXCAVATING FOR THIS CONTRACT, THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UNDERGROUND UTILITIES WITH THE APPROPRIATE UTILITY COMPANY.
- 10. LOCATION AND ELEVATION OF IMPROVEMENTS TO BE MET BY WORK TO BE DONE SHALL BE CONFIRMED BY FIELD MEASUREMENTS PRIOR TO CONSTRUCTION OF NEW WORK.
- 11. CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS REQUIRED TO PROTECT ADJACENT PROPERTIES DURING THE GRADING OPERATIONS.
- 12. WHERE TRENCHES ARE WITHIN EASEMENTS OR WITHIN 10' OF ANY BUILDING, A SOILS REPORT SHALL BE SUBMITTED TO THE ENGINEER OF WORK BY A QUALIFIED SOILS ENGINEER WHICH INDICATES THAT TRENCH BACKFILL WAS COMPACTED UNDER THE OBSERVATION OF THE SOILS ENGINEER AND IN ACCORDANCE WITH THE ABOVE-NAMED SPECIFICATIONS.
- 13. ALL FRAMES, COVERS, VALVE BOXES AND MANHOLES SHALL BE ADJUSTED TO FINISHED GRADE UPON COMPLETION OF PAVING OR RELATED CONSTRUCTION.

**GENERAL NOTES:**

- 1. ALL CONSTRUCTION SHALL BE IN CONFORMANCE WITH THE PWSO ENGINEERING STANDARDS AND SPECIFICATIONS, LATEST VERSION, THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA, THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA, THE ENVIRONMENTAL PROTECTION AGENCY AND THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION.
- 2. THE CONTRACTOR SHALL CONTACT ALL APPROPRIATE UTILITY COMPANIES AND THE TOWN OF PARKER PRIOR TO ANY CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ANY EXISTING UTILITY (INCLUDING DEPTHS) WHICH MAY CONFLICT WITH THE PROPOSED CONSTRUCTION. ALL EXISTING UTILITIES SHALL BE PROTECTED FROM DAMAGE BY THE CONTRACTOR. DAMAGED UTILITIES SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE. ALL ITEMS SHOWN ON THE PLANS AS EXISTING ARE APPROXIMATE LOCATIONS ONLY. THE ACTUAL LOCATIONS MAY VARY FROM THE PLANS, ESPECIALLY IN THE CASE OF UNDERGROUND UTILITIES. WHENEVER THE CONTRACTOR DISCOVERS A DISCREPANCY IN LOCATIONS CONTACT THE PWSO IMMEDIATELY.
- 3. PWSO ENGINEER AND OTHER APPROVING AGENCIES ARE TO BE NOTIFIED 48 HOURS PRIOR TO CONSTRUCTION.
- 4. THE CONTRACTOR SHALL OBTAIN, AT HIS EXPENSE, ALL PERMITS NECESSARY TO PERFORM THE PROPOSED WORK, INCLUDING STREET CUTS, UTILITY INTERFERENCE AND TRAFFIC CONTROLS.
- 5. ALL CONCRETE SHALL BE A MINIMUM OF CLASS A, 6 SACK, TYPE II, 3000-POUND COMPRESSION STRENGTH. ALL CONCRETE ASSOCIATED WITH ROADWAY CONSTRUCTION SHALL BE CLASS D.
- 6. THE DESIGN ENGINEER SHALL SUBMIT ONE (1) SET OF "AS-BUILT" DRAWINGS AND ELECTRONIC FILES TO THE DISTRICT ENGINEER FOR APPROVAL BY DISTRICT. AFTER APPROVAL HAS BEEN GRANTED BY THE DISTRICT ENGINEER, FULL-SIZED PRINTS SHALL BE TRANSMITTED TO THE PWSO OFFICE.
- 7. NO WORK SHALL BE BACKFILLED UNTIL THE CONSTRUCTION HAS BEEN INSPECTED AND APPROVED FOR BACKFILLING BY THE DISTRICT ENGINEER OR REPRESENTATIVE OF THE DISTRICT ENGINEER. ALL BACKFILL MATERIAL SHALL BE COMPACTED TO 95% STANDARD PROCTOR DENSITY. COMPACTION TESTS MUST BE SUBMITTED TO DISTRICT ENGINEER PRIOR TO PROBATIONARY ACCEPTANCE.
- 8. ALL CURBS SHALL BE CAREFULLY AND FIRMLY BACKFILLED BY THE CONTRACTOR. ANY EXCESS MATERIAL FROM SUBGRADE TRIMMING SHALL NOT BE LEFT IN THE STREET CORE, BUT SHALL BE SPREAD UNIFORMLY IN A 20-FOOT WIDE ZONE BEHIND THE WALK.
- 9. REFER TO OVERLOT GRADING AND EROSION CONTROL PLANS FOR GRADING NOTES.
- 10. EXPANSION JOINTS WILL BE PLACED IN ALL SIDEWALKS AND CURB AND GUTTER AT PCS, PTS, AND AT ANY FIXED OBJECT, BUT IN NO CASE MORE THAN 500 FT APART.
- 11. THE APPLICANT IS PBSI CHAMBERS & HESS UNITS, LLC.
- 12. THE UTILITY CONTRACTOR THAT INSTALLS SANITARY SEWER, STORM SEWER AND WATER SHALL, AFTER PROPERLY COMPLETING THE WORK, WALK THE SITE WITH THE OWNER'S REPRESENTATIVE(S) TO INSPECT MANHOLES, HYDRANTS, VALVE BOXES, AND INLETS PRIOR TO THE START OF WORK BY THE CURB, GUTTER AND WALK CONTRACTOR. A SIMILAR FIELD INSPECTION SHALL OCCUR WITH THE UTILITY CONTRACTOR AND CURB CONTRACTOR TO INSPECT THESE ITEMS PLUS CURB STOP BOXES AND CONCRETE WORK PRIOR TO THE START OF WORK BY THE PAVING CONTRACTOR. THESE FIELD INSPECTIONS ARE INTENDED TO HELP ESTABLISH RESPONSIBILITY FOR ANY NECESSARY REPAIRS, CLEANING, ETC. THAT ARE NEEDED. THESE INSPECTIONS WITH A REPRESENTATIVE OF THE OWNER ARE IN ADDITION TO ANY INSPECTIONS BY REPRESENTATIVES OF THE TOWN OF PARKER OR PARKER WATER AND SANITATION DISTRICT.
- 13. ALL TESTING AND INSPECTION FOR COMPLIANCE WITH TOWN OF PARKER STANDARDS SHALL BE COMPLETED BEFORE ANY CONDITIONAL ACCEPTANCE.
- 14. THE DESIGN ENGINEER SHALL SUBMIT ONE (1) SET OF ROLLED "AS-BUILT" DRAWINGS AND ELECTRONIC FILES TO THE DISTRICT ENGINEER FOR APPROVAL PRIOR TO PRINTING MYLAR DRAWINGS FOR THE DISTRICT. AFTER APPROVAL HAS BEEN GRANTED BY THE DISTRICT ENGINEER, FULL SIZED MYLAR PRINTS SHALL BE TRANSMITTED TO THE PWSO OFFICE AND AN ADDITIONAL SET OF ROLLED PRINTS SHALL BE TRANSMITTED TO THE DISTRICT ENGINEER'S OFFICE PRIOR TO PROBATIONARY ACCEPTANCE.
- 15. TRENCHES SHALL BE EXCAVATED AND THE PIPE EXPOSED FOR INSPECTION AT ANY LOCATION ON THE PROJECT IF SO ORDERED BY THE INSPECTOR.
- 16. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR THE PROPER FUNCTIONING OF LINES (WATER AND SEWER) FOR UP TO TWO (2) YEARS FROM THE DATE OF PROBATIONARY ACCEPTANCE OF LINES BY THE DISTRICT. ANY MALFUNCTION DURING THIS PERIOD OF GUARANTEE SHALL BE REMEDIED BY THE CONTRACTOR TO THE SATISFACTION OF THE DISTRICT ENGINEER AT NO COST TO THE DISTRICT.
- 17. SURFACE GRADES ARE TO BE WITHIN PLUS OR MINUS ONE FOOT OF FINISHED GRADE AND VERIFICATION OF COMPACTION RESULTS OBTAINED PRIOR TO THE INSTALLATION OF WATER AND SEWER LINES. RESULTS MUST BE SUBMITTED TO THE DISTRICT ENGINEER.
- 18. NO WORK SHALL BE BACKFILLED UNTIL THE CONSTRUCTION HAS BEEN INSPECTED AND APPROVED FOR BACKFILLING BY THE DISTRICT ENGINEER OR REPRESENTATIVE OF THE DISTRICT ENGINEER.
- 19. ALL SERVICES WILL BE PERMANENTLY MARKED ON CURB FACE AS FOLLOWS:  
"X" FOR SANITARY SERVICE SEWERS  
"V" FOR WATER SERVICES
- 20. NO TREES ALLOWED IN EASEMENTS OR WITHIN 8' OF WATER OR SANITARY SEWER MAINS IN RIGHT OF WAY. NO TREES ALLOWED WITHIN 7' OF THE EDGE OF ANY STORM SEWER.
- 21. BENCHMARKS:  
DOUGLAS COUNTY:
- 22. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION, PROTECTION, AND REPAIR OF ALL UTILITIES ENCOUNTERED DURING CONSTRUCTION, WHETHER SHOWN ON THESE PLANS OR NOT. CONTRACTOR SHALL CONTACT REPRESENTATIVES OF THE RESPECTIVE UTILITIES AND HAVE ALL UTILITIES FIELD LOCATED PRIOR TO CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL LOCATIONS OF EXISTING STRUCTURES AND UTILITIES SHOWN ON THE DRAWINGS, TO ASCERTAIN WHETHER ANY STRUCTURES AND UTILITIES MAY EXIST, AND REPAIR AND/OR REPLACE ANY STRUCTURES AND/OR UTILITIES THAT ARE DAMAGED BY THE CONTRACTOR.
- 23. ALL DIMENSIONS AND RADII ARE TO FLOW LINE UNLESS OTHERWISE NOTED.
- 24. THE CONTRACTOR SHALL FURNISH THE ENGINEER WITH A SET OF "AS-BUILT" PLANS MARKED WITH ALL DEVIATIONS FROM THE CONSTRUCTION PLANS PRIOR TO INITIAL ACCEPTANCE.
- 25. ALL STATIONING IS ON CENTERLINE OF RIGHT OF WAY UNLESS OTHERWISE NOTED.

**ROADWAY NOTES:**

- 1. PAVING SHALL NOT COMMENCE UNTIL A SOILS REPORT AND PAVEMENT DESIGN IS APPROVED BY THE ENGINEERING DIVISION AND SUB-GRADE COMPACTION TESTS ARE SUBMITTED TO AND APPROVED BY THE DEVELOPMENT REVIEW ENGINEER.
- 2. STANDARD TOWN OF PARKER CURB RAMPS ARE TO BE CONSTRUCTED AT ALL CURB RETRUNS AND "T" INTERSECTIONS.
- 3. ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
- 4. ALL ELEVATIONS ARE ON USGS DATUM WITH DATE. RANGE POINT OR MONUMENT SHALL BE SHOWN ON THE CONSTRUCTION PLANS.
- 5. EXCEPT WHERE OTHERWISE PROVIDED FOR IN THESE PLANS AND SPECIFICATIONS, THE COLORADO DEPARTMENT OF HIGHWAYS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE COLORADO DEPARTMENT OF HIGHWAYS M-STANDARDS AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL, LATEST EDITION, SHALL APPLY.

**CONSTRUCTION NOTES:**

- 1. ALL MATERIALS AND WORKMANSHIP SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE COLORADO DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE LATEST EDITION OF THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL AND THE TOWN OF PARKER STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL.
- 2. ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT. THE TOWN RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
- 3. A PRE-CONSTRUCTION MEETING SHALL BE SCHEDULED A MINIMUM OF 48 HOURS AND A MAXIMUM OF 96 HOURS PRIOR TO THE START OF CONSTRUCTION. A PRE-CONSTRUCTION MEETING WILL NOT BE SCHEDULED UNTIL THE GRADING PERMIT AND ALL OTHER NECESSARY PERMITS HAVE BEEN OBTAINED.
- 4. A DEVELOPMENT REVIEW ENGINEER SHALL BE CONTACTED A MINIMUM OF 24 HOURS PRIOR TO A NECESSARY INSPECTION. IF A DEVELOPMENT REVIEW ENGINEER IS NOT AVAILABLE AFTER PROPER NOTICE OF CONSTRUCTION ACTIVITY HAS BEEN PROVIDED, THE PERMITTEE MAY COMMENCE WORK IN THE DEVELOPMENT REVIEW ENGINEER'S ABSENCE; HOWEVER, TOWN OF PARKER RESERVES THE RIGHT TO REJECT THE IMPROVEMENT IF SUBSEQUENT TESTING REVEALS AN IMPROPER INSTALLATION.
- 5. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION. FOR INFORMATION, CONTACT DENVER INTER-UTILITY GROUP AT 303-534-6700 OR 1-800-922-1987. THE CONTRACTOR SHALL FIELD VERIFY SIZE AND HORIZONTAL AND VERTICAL LOCATIONS OF EXISTING FACILITIES PRIOR TO CONSTRUCTION, AND NOTIFY THE ENGINEER OF ANY DISCREPANCIES.
- 6. THE CONTRACTOR SHALL HAVE ONE (1) SIGNED COPY OF THE PLANS, APPROVED BY THE TOWN OF PARKER PUBLIC WORKS DIRECTOR, AND ONE (1) COPY OF THE ROADWAY DESIGN AND CONSTRUCTION CRITERIA AT THE JOB SITE AT ALL TIMES.
- 7. ALL PROSPED STREET CUTS TO EXISTING PAVEMENTS FOR UTILITIES, STORM SEWER, OR FOR OTHER PURPOSES ARE LISTED AND REFERENCED BELOW:  
WATER TIE IN:  
SANITARY SEWER CONNECTION  
STORM SEWER CONNECTION  
A PLAN FOR TRAFFIC CONTROL DURING CONSTRUCTION SHALL BE SUBMITTED TO THE TOWN OF PARKER FOR ACCEPTANCE WITH THE RIGHT OF WAY PERMIT USE APPLICATION. A PERMIT WILL NOT BE ISSUED WITHOUT AN APPROVED TRAFFIC CONTROL PLAN FOR USE DURING CONSTRUCTION.
- 9. ALL TRENCHES SHALL BE ADEQUATELY SUPPORTED AND THE SAFETY OF WORKERS PROVIDED FOR AS REQUIRED BY THE MOST RECENT OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) "SAFETY AND HEALTH REGULATIONS FOR CONSTRUCTION"
- 10. COMPACTION OF ALL TRENCHES MUST BE ATTAINED AND COMPACTION TEST RESULTS SUBMITTED TO THE DEVELOPMENT REVIEW ENGINEER IN PRELIMINARY FORM PRIOR TO PAVING AND IN FINAL FORM PRIOR TO PROBATIONARY ACCEPTANCE.
- 11. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING EROSION AND SEDIMENT CONTROL MEASURES AT ALL TIMES DURING CONSTRUCTION. THE PLAN MAY BE MODIFIED AS FIELD CONDITIONS WARRANT WITH APPROVAL FROM THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- 12. THE CONTRACTOR SHALL PROVIDE, ERECT, AND MAINTAIN PROPER TRAFFIC CONTROL DEVICES UNTIL THE SITE IS OPEN TO TRAFFIC. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT FOR APPROVAL PRIOR TO CONSTRUCTION.
- 13. PLANS ARE APPROVED FOR ONE (1) YEAR FROM THE DATE SHOWN ON THE TOWN OF PARKER SIGNATURE BLOCK. PLANS SHALL BE RESUBMITTED TO THE TOWN FOR APPROVAL AFTER ONE (1) YEAR. THE COST OF THE PLAN RE-REVIEW AND RE-ACCEPTANCE WILL BE CHARGED BACK TO THE DEVELOPER INCLUDING ALL TIME AND EXPENSES OF THE TOWN OF PARKER PUBLIC WORKS DEPARTMENT.
- 14. REPAIR OF ANY DAMAGE TO EXISTING IMPROVEMENTS OR LANDSCAPING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- 15. ALL DAMAGED EXISTING CURB, GUTTER, AND SIDEWALK SHALL BE REPAIRED PRIOR TO ACCEPTANCE OF COMPLETED IMPROVEMENTS.
- 16. ALL CONSTRUCTION ACTIVITIES MUST COMPLY WITH THE STATE OF COLORADO PERMITTING PROCESS FOR "STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITY". FOR INFORMATION, PLEASE CONTACT COLORADO DEPARTMENT OF HEALTH, WATER QUALITY CONTROL DIVISION, WOOD-PE-B2, 4300 CHERRY DRIVE SOUTH, DENVER, COLORADO, 80222-1530. ATTN: PERMITS AND ENFORCEMENT SECTION. PHONE: 303-692-3500
- 17. IF DEWATERING IS REQUIRED, A STATE CONSTRUCTION DEWATERING DISCHARGE PERMIT IS REQUIRED FOR DISCHARGES TO A STORM SEWER, CHANNEL, IRRIGATION DITCH, ANY STREET THAT IS TRIBUTARY TO THE FOREMENTIONED FACILITIES OR ANY WATER OF THE UNITED STATES.
- 18. ALL REFERENCES TO BOOKS, PAGES, MAPS AND RECEPTION NUMBER ARE PUBLIC DOCUMENTS ON FILE WITH THE COUNTY CLERK AND RECORDERS OFFICE.

**UDFCD NOTE:**

- 1. THE URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MAY CONDUCT SITE VISITS DURING PROJECT CONSTRUCTION WITHIN OR NEAR THE CHANNEL TO OBSERVE CONSTRUCTION FOR CONFORMANCE WITH THE APPROVED PLANS AND SPECIFICATIONS. COORDINATION WITH THE UDFCD CONSTRUCTION MANAGER IS REQUIRED 48 HOURS PRIOR TO ANY CONSTRUCTION. CONTACT MIKE SARMENTO, UDFCD CONSTRUCTION MANAGER AT 303-455-6277, WSARMENTO@UDFCD.ORG

**PARKER STANDARD STORM DRAINAGE INFRASTRUCTURE NOTES:**

- 1. ALL STORM DRAINAGE IMPROVEMENTS ARE SUBJECT TO COMPLIANCE WITH THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) CURRENT EDITION OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, M & S STANDARDS, AND ALL STANDARD SPECIAL PROVISIONS CURRENTLY USED BY CDOT, WITH THE MODIFICATIONS SET FORTH IN THE TOWN OF PARKER'S STORM DRAINAGE AND ENVIRONMENTAL CRITERIA MANUAL (SDECM), AS AMENDED.
- 2. THE CONTRACTOR SHALL COMPLY WITH THE "COLORADO WATER QUALITY CONTROL ACT" (TITLE 25, ARTICLE 8 CRS), THE "PROTECTION OF FISHING STREAMS" TITLE 33, ARTICLE 5, CRS), THE "CLEAN WATER ACT" (33 USC 1344), CHERRY CREEK RESERVOIR CONTROL REGULATION NO. 72" (5 CCR 1002-72), THE REGULATION PROMULGATED, CERTIFICATION OR PERMITS ISSUES, AND THE REQUIREMENTS PRESENTED IN THE SDECM REVISION TO SECTION 107 AND THE CONSTRUCTION BMP PLAN. IN THE EVENT OF CONFLICTS BETWEEN THESE REQUIREMENTS AND WATER QUALITY CONTROL LAWS, RULES, OR REGULATIONS OF OTHER FEDERAL, OR STATE AGENCIES, THE MORE RESTRICTIVE LAWS, RULES, OR REGULATIONS SHALL APPLY.
- 3. INSPECTIONS: CONSTRUCTION SHALL NOT BEGIN UNTIL A GRADING PERMIT HAS BEEN ISSUED FOR THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE TOWN OF PARKER ENGINEERING DEPARTMENT (PUBLIC WORKS) TO SCHEDULE INSPECTIONS A MINIMUM OF 48 HOURS PRIOR THE CONSTRUCTION OF ALL DRAINAGE INFRASTRUCTURE (STORM SEWERS, INLETS, MANHOLES, ENERGY DISSIPATORS, RIPRAP, GROUTED BOULDERS, DETENTION POND OUTLET STRUCTURES, FOREBAYS, TRICKLE CHANNELS, ETC). FAILURE TO NOTIFY THE ENGINEERING DEPARTMENT FOR INSPECTIONS MAY RESULT IN NON-ACCEPTANCE OF THE INFRASTRUCTURE BY THE TOWN. URBAN DRAINAGE AND FLOOD CONTROL DISTRICT MUST ALSO BE NOTIFIED IN A SIMILAR MANNER FOR ALL MAINTENANCE ELIGIBLE DRAINAGE INFRASTRUCTURES (CONSULT WITH STORMWATER ENGINEERING DIVISION).
- 4. STRUCTURAL BACKFILL (CDOT CLASS 1) SHALL BE COMPACTED TO CONFORM TO CDOT STANDARD SPECIFICATION 203.03. STRUCTURAL BACKFILL (CDOT CLASS 2) SHALL CONFORM TO CDOT STANDARD SPECIFICATION 203.07. AT THE CONTRACTOR'S OPTION, STRUCTURAL BACKFILL (SQUEEGEE) MEETING THE GRADATION REQUIREMENTS CONTAINED IN REVISION OF SECTION 206 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM, MAY BE SUBSTITUTED FOR STRUCTURE BACKFILL

- (CLASS 1) OR CLASS 2) FOR BACKFILLING OF CULVERT PIPES, STORM SEWER PIPES, MANHOLES AND INLET STRUCTURES; HOWEVER, THE TOP 2 FEET BELOW SUBGRADE ELEVATION SHALL BE THE REQUIRED EMBANKMENT MATERIAL.
- 5. ALL EXCAVATIONS SHALL MEET OSHA REQUIREMENTS.
- 6. TESTING: PROBATIONARY ACCEPTANCE OF STORM DRAINAGE IMPROVEMENTS WILL BE CONTINGENT UPON SATISFACTORY TESTING RESULTS. IN ALL CASES WHERE TESTS INDICATE COMPACTION LESS THAN THAT REQUIRED BY TOWN SPECIFICATIONS, ADDITIONAL COMPACTION AND TESTS WILL BE REQUIRED UNTIL THE SPECIFICATIONS ARE MET. FREQUENCY OF TESTING WILL BE AS FOLLOWS: 1 TEST FOR SUBGRADE AND 1 TEST FOR BACKFILL AT EVERY ABOVE GROUND APPURTENANCE (MANHOLES, INLETS, ETC) 1 TEST EVERY 200 LF OF MAINLINE TRENCH EVERY 1 FOOT OF BACKFILL LIFT AND WITHIN 1 FOOT FROM ALL STRUCTURES.
- 7. ALLOWABLE STORM SEWER CONDUIT MATERIAL WITHIN THE TOWN OF PARKER SHALL BE LIMITED TO REINFORCED CONCRETE PIPE (RCP) CONFORMING TO CDOT STANDARD SPECIFICATION 706.02.
- 8. ALL RCP JOINTS SHALL BE MANUFACTURED IN ACCORDANCE WITH ASTM C443. RUBBER GASKETS SHALL BE USED ON ALL PIPE JOINTS CONFORMING TO ASTM C443. ALL RCP SECTIONS SHALL BE JOINED IN SUCH A TOWN OF PARKER PAGE 2 OF 2 MANNER THAT THE ENDS ARE FULLY ENDED AND THE INNER SURFACES ARE REASONABLE FLUSH. AVERAGE JOINT GAP THAT EXCEEDS 1/8 INCH SHALL BE FILLED WITH AN APPROVED FLEXIBLE PLASTIC SEALANT.
- 9. JOINT RESTRAINTS AND TOE-WALLS, CONFORMING TO CDOT M&S STANDARD PLAN NO. M-601-11 SHALL BE USED ON ALL RCP FLARED END SECTION OUTFALLS.
- 10. EPOXY COATED REBAR SHALL BE USED AS REINFORCING STEEL ON ALL STORM INLETS AND STRUCTURES. REFERENCE CDOT M&S STANDARD PLAN NO. M-604-10, 11, 12, AND 13.
- 11. CDOT CLASS D CONCRETE SHALL BE USED FOR ALL CONCRETE DRAINAGE STRUCTURES.
- 12. PRE-CAST INLETS AND MANHOLE BASES SHALL NOT BE USED WITHIN THE TOWN OF PARKER RIGHT-OF-WAY, WITH THE EXCEPTION OF CDOT TYPE C AND D INLETS.
- 13. TWO- (2) MANHOLE ACCESS POINTS ARE REQUIRED ON ALL TYPE "R" CURB INLETS GREATER THAN OR EQUAL TO TEN (10) FEET IN LENGTH AS PRESENTED IN CDOT M&S STANDARD PLAN NO. M-604-12.
- 14. ALL GROUTING (BOULDERS, RIPRAP) SHALL BE IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- 15. ALL BOULDERS AND RIPRAP SHALL BE SELECTED AND PLACED IN ACCORDANCE WITH THE REVISION OF SECTION 506 OF THE CDOT STANDARD SPECIFICATIONS AS PRESENTED IN THE SDECM.
- 16. CONTRACTOR SHALL REFER TO THE TOWN OF PARKER'S CONSTRUCTION BEST MANAGEMENT PRACTICES DETAILS AND NOTES FOR ALL REQUIREMENTS RELATING TO RE-VEGETATION, SEDIMENT AND EROSION CONTROL REQUIREMENTS FOR CONSTRUCTION ACTIVITIES.
- 17. PIPE BELLS SHALL NOT BE CAST INTO MANHOLE BASES OR INLETS.

**SIGNAGE AND STRIPING NOTES:**

- 1. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE MOST RECENT VERSION OF THE FEDERAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), COLORADO SUPPLEMENTAL MUTCD, AND THE TOWN OF PARKER ROADWAY DESIGN AND CONSTRUCTION CRITERIA MANUAL. FURTHER SPECIFICATIONS AND ILLUSTRATIONS ARE LOCATED IN THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) M&S STANDARDS.
- 2. A FIELD INSPECTION OF LOCATION AND INSTALLATION OF ALL SIGNS, AND MARKING SHALL BE PERFORMED BY THE TOWN OF PARKER.
- 3. THE CONTRACTOR INSTALLING SIGNS SHALL BE RESPONSIBLE FOR THE LOCATING AND PROTECTING OF ALL UNDERGROUND UTILITIES.
- 4. TYPE III (LIGHTED) BARRICADES SHALL BE SET AT THE ENDS OF ROADWAYS SEPARATING FINISHED (AND/OR ACCEPTED) AND UNFINISHED CONSTRUCTION AREA AND SHALL BE MAINTAINED BY THE CONTRACTOR/DEVELOPER. "ROAD CLOSED AHEAD" AND "TO BE EXTENDED" WARNING SIGNS SHALL BE INSTALLED APPROPRIATELY IN ADVANCE OF THE TYPE III BARRICADES.
- 5. SPECIAL CARE SHALL BE TAKEN IN SIGN LOCATIONS TO ENSURE AN UNOBSTRUCTED VIEW OF EACH SIGN.
- 6. WHERE STOP SIGN CONTROL IS APPROPRIATE, 36-INCH STOP SIGNS SHALL BE USED FOR APPROACHES TO ANY ROADWAY THAT IS CLASSIFIED AS A COLLECTOR OR GREATER.
- 7. A 7-FOOT MINIMUM POST LENGTH SHALL BE MAINTAINED FROM THE BOTTOM OF THE SIGN PANEL TO THE GROUND. THIS REQUIREMENT FOR VERTICAL CLEARANCE IS FOR ALL SIGNS.
- 8. DELINEATION OF ROADWAYS SHALL BE AS SPECIFIED IN THE COLORADO DEPARTMENT OF TRANSPORTATION M&S STANDARDS.
- 9. RAISED MEDIAN ISLAND NOSES SHALL HAVE R4-7 SIGNS AT EACH END.
- 10. SIGNAGE AND STRIPING HAVE BEEN DETERMINED BY INFORMATION AVAILABLE AT THE TIME OF REVIEW. PRIOR TO THE INITIATION OF ANY WARRANTY PERIOD, THE TOWN OF PARKER RESERVES THE RIGHT TO REQUIRE MODIFICATIONS TO EXISTING OR INSTALLATION OF, ADDITIONAL SIGNAGE AND/OR PAVEMENT MARKING IF IT IS DETERMINED THAT CONDITIONS WARRANT SUCH MODIFICATION ACCORDING TO THE MUTCD OR THE CDOT M&S STANDARDS. ALL SIGNAGE AND STRIPING SHALL FALL UNDER THE REQUIREMENTS OF THE TWO-YEAR WARRANTY PERIOD FOR NEW CONSTRUCTION. ADDITIONALLY, ALL PAVEMENT MARKING SHALL NOT LIFT OR PEEL DURING THE FIRST YEAR AFTER INSTALLATION
- 11. DIAMOND GRADE MATERIAL SHALL BE USED ON ALL STOP SIGN AND OVERHEAD SIGNS. ALL OTHER ROADSIDE SIGNS SHALL USE HIGH-INTENSITY GRADE SHEETING. NO FLOURESCENT YELLOW-GREEN SHEETING SHALL BE USED UNLESS SPECIFICALLY APPROVED BY THE TOWN OF PARKER.
- 12. ALL STREET NAME SIGNS SHALL BE HIGH INTENSITY, EXTRUDED, GREEN AND WHITE BLADES. ARTERIAL AND COLLECTOR STREET NAME SIGNS SHALL BE 9-INCH BLADES AND HAVE MIXED CASE LETTERING. LOCAL STREET NAME SIGNS SHALL BE 6-INCH BLADES WITH ALL MIXED CASE LETTERING.
- 13. ALL LAYOUTS FOR INTERNALLY ILLUMINATED STREET NAME SIGNS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND APPROVAL PRIOR TO FABRICATION.
- 14. ALL REMOVED SIGNS SHALL BE RETURNED TO THE TOWN OF PARKER.

**TOWN OF PARKER APPROVALS**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORKS DATE

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS COUNTY, COLORADO**  
**DOUGLAS 234**  
**FILING, NO. 6**



REVISIONS:

NO.	DESCRIPTION	DATE:
1	-	-

SCALE: NTS  
DATE: 5/6/2020  
DRAWN BY: WD  
CHECKED BY: JS  
JOB NO: D01173-A

**NOTES SHEET**

PROJECT NO. D01173-A  
DRAWING NO. 2  
SHEET NO. 2 OF 22 SHEETS

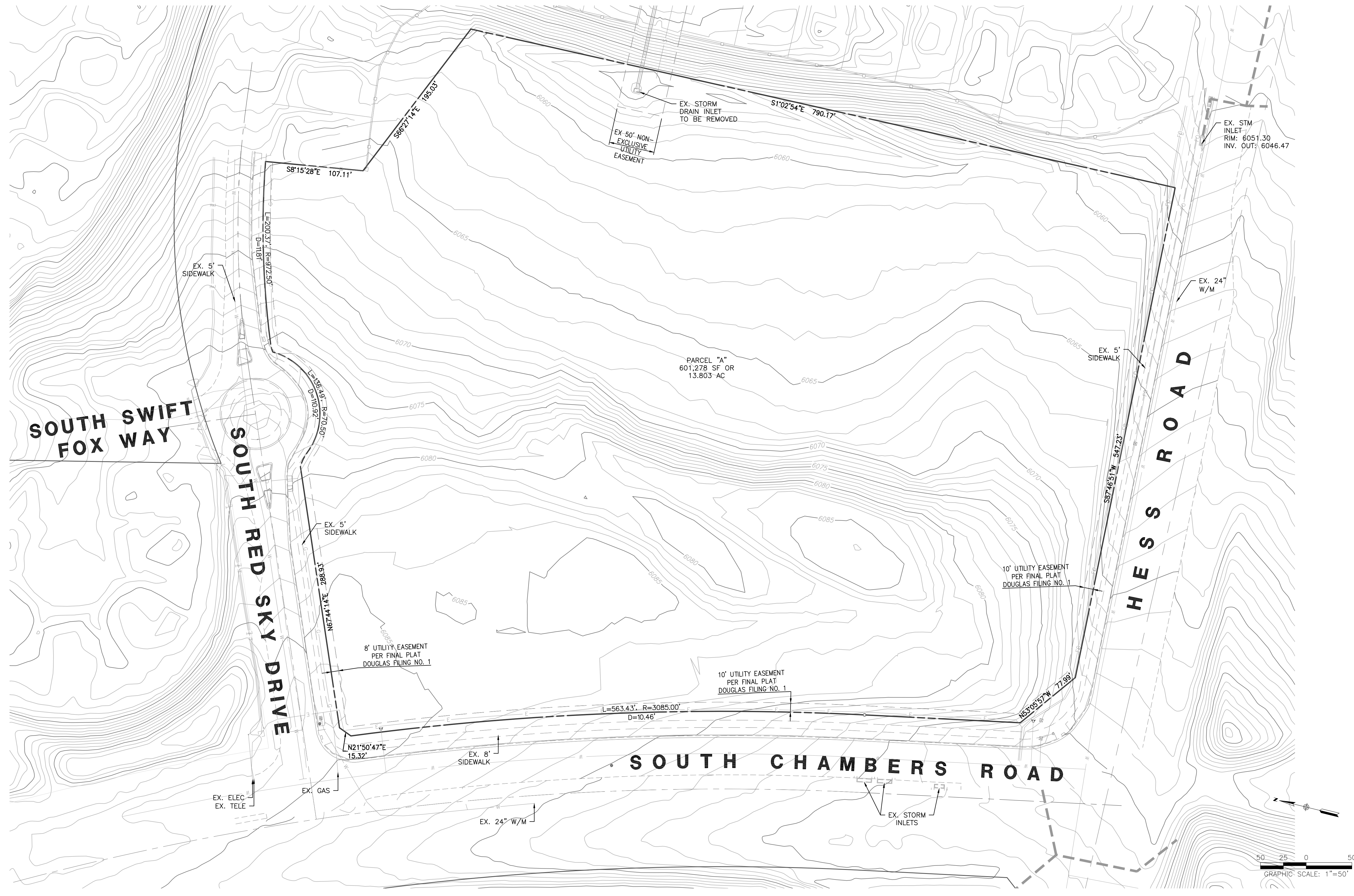
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TOWNSHIP 6 SOUTH, RANGE 66 WEST, 6TH PRINCIPAL MERIDIAN,  
TOWN OF PARKER, COUNTY OF DOUGLAS, STATE OF COLORADO  
13.80 ACRES, 11 COMMERCIAL LOTS



**ROADWAY & STORM DRAIN PLANS FOR  
DOUGLAS 234  
FILING, NO. 6**  
DOUGLAS COUNTY, COLORADO

**RICK**  
ENGINEERING COMPANY  
9801 EAST EASTER AVE  
CENTENNIAL, CO 80112  
303.537.8020  
Tucson - San Diego - Riverside - Orange  
Sacramento - San Luis Obispo - Phoenix  
rickengineering.com



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SCALE: NTS  
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JOB NO: D01173-A

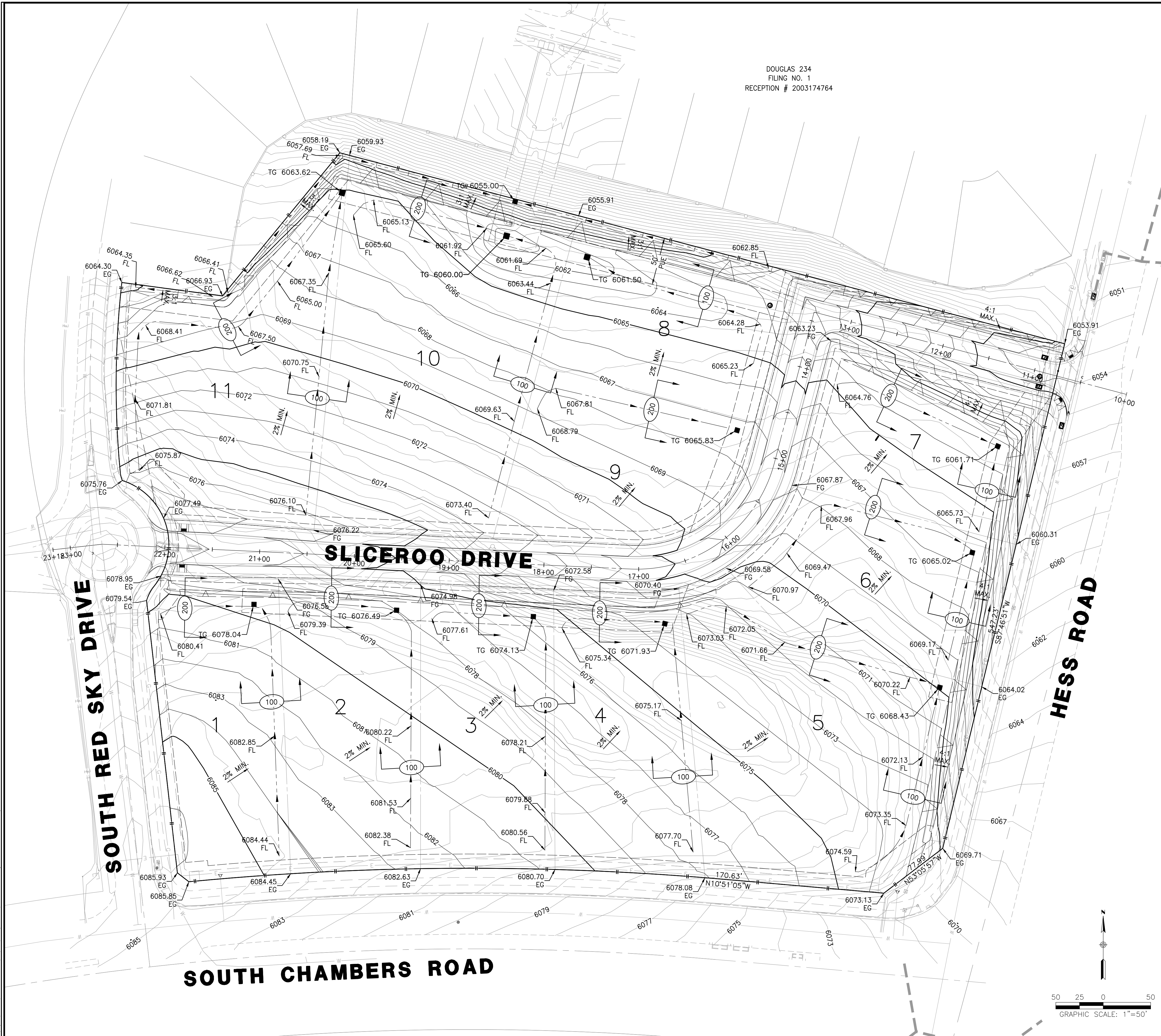
**EXISTING  
CONDITIONS**

PROJECT NO.  
D01173-A  
DRAWING NO.  
3  
SHEET NO. 3 OF 22 SHEETS

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NOT FOR CONSTRUCTION

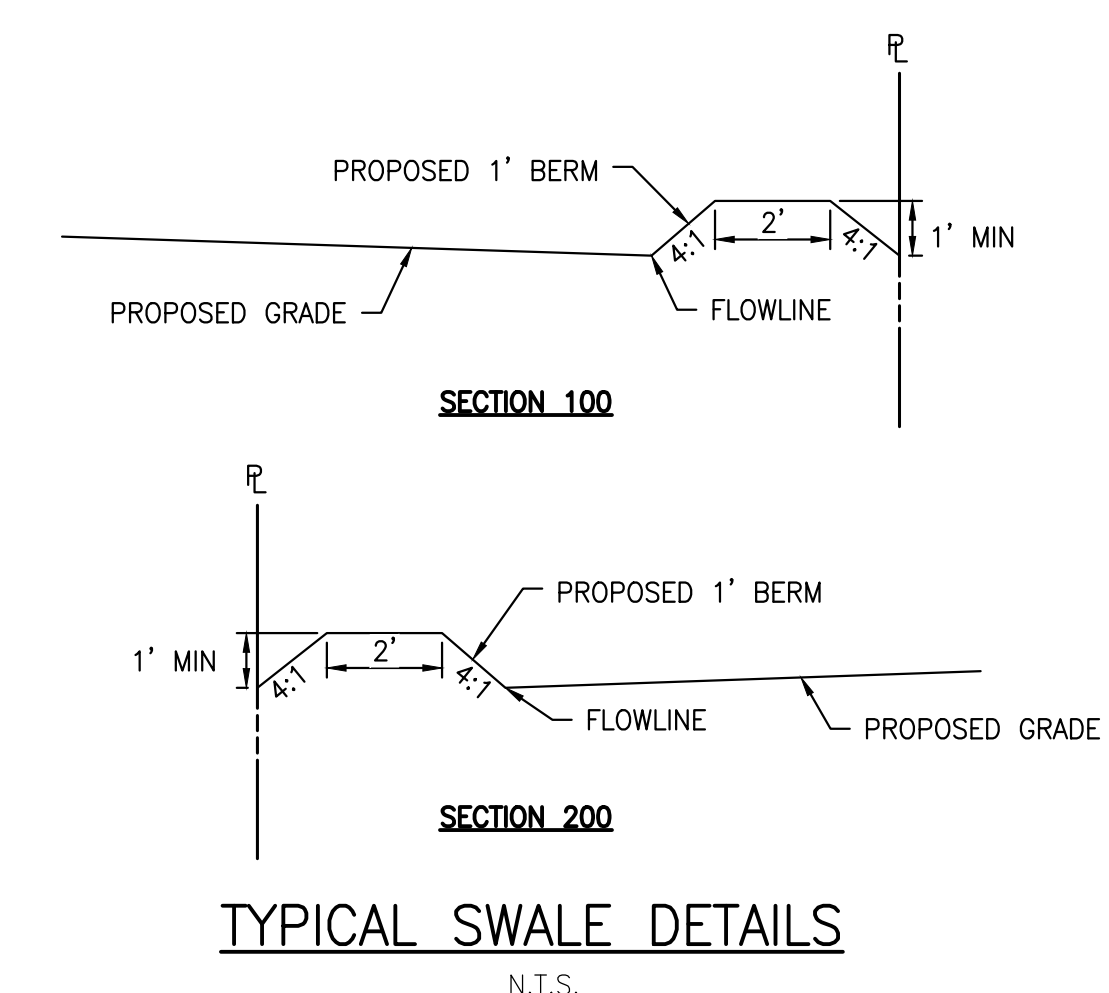
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DOUGLAS 234  
 FILING NO. 1  
 RECEPTION # 2003174764



**GRADING NOTES**

- CONTRACTOR SHALL REFER TO GEOTECHNICAL ENGINEER'S REPORT TO ENSURE REQUIREMENTS FOR MINIMUM GROUND SLOPE ARE MET.
- RETAINING WALL DESIGN PROVIDED BY OTHERS.
- ALL ADA RAMPS SHALL BE CONSTRUCTED WITH A MAXIMUM CROSS SLOPE OF 2.0%.
- MEET EXISTING GRADE AT PROPERTY LIMITS UNLESS NOTED OTHERWISE.
- THE CONTRACTOR SHALL CALL 811 PRIOR TO ANY WORK TO LOCATE UTILITIES AND SHALL CONTACT THE OWNER SHOULD UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEMENT.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
- IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITION OR BETTER.
- EXISTING TOPOGRAPHY SHOWN REPRESENTS SITE CONDITIONS AS PREPARED BY MANHARD CONSULTING ON AUGUST 31, 2018. CONTRACTOR SHALL FIELD CHECK EXISTING ELEVATIONS AND CONDITIONS PRIOR TO CONSTRUCTION AND NOTIFY ARCHITECT AND ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION. IF THE CONTRACTOR DOES NOT ACCEPT EXISTING TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, THEN THE CONTRACTOR SHALL SUPPLY, AT THEIR EXPENSE, A TOPOGRAPHIC SURVEY BY A REGISTERED LAND SURVEYOR TO THE OWNER FOR REVIEW.
- TRANSITIONS FROM DEPRESSED CURB TO FULL HEIGHT CURB SHALL BE TAPERED AT 2:1 UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL REFER TO THE SOIL EROSION AND SEDIMENT CONTROL PLAN AND DETAILS FOR CONSTRUCTION SCHEDULING AND EROSION CONTROL MEASURES TO BE INSTALLED PRIOR TO BEGINNING GRADING OPERATIONS.



**TOWN OF PARKER APPROVALS**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

**ROADWAY & STORM DRAIN PLANS FOR  
 DOUGLAS 234  
 FILING, NO. 6**  
 DOUGLAS COUNTY, COLORADO

**RICK**  
 ENGINEERING COMPANY  
 9801 EAST EASTER AVE  
 CENTENNIAL, CO 80112  
 303.537.8020

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 CHECKED BY: JS  
 JOB NO: D01173-A

**OVERALL  
 GRADING  
 PLAN**

PROJECT NO. D01173-A  
 DRAWING NO. 4  
 SHEET NO. 4 OF 22 SHEETS

PRELIMINARY  
 NOT FOR CONSTRUCTION

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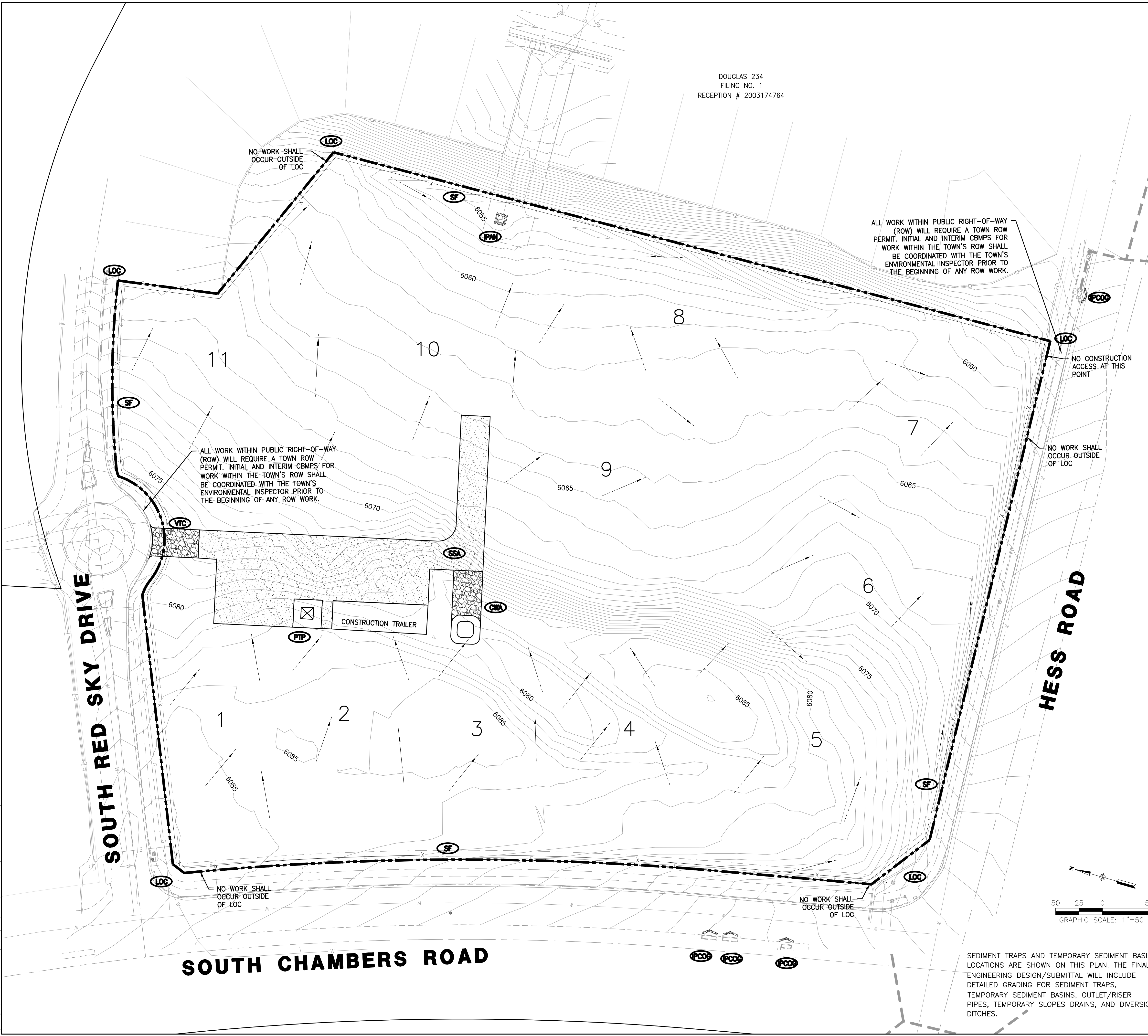
DOUGLAS 234  
 FILING NO. 1  
 RECEPTION # 2003174764

**NOTES:**

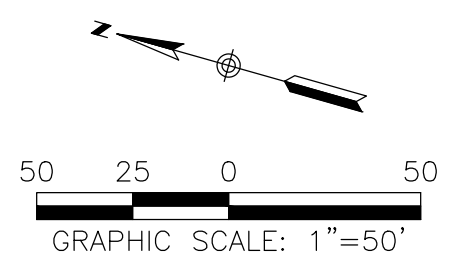
1. SILT FENCE TO BE INSTALLED IN AREAS OF SIGNIFICANT TOPOGRAPHY AND SHALL HAVE STAKES @ 5'-0" O.C.
2. ALL SILT FENCE AND OTHER SEDIMENT BMP'S MUST BE PLACED IMMEDIATELY BEHIND THE BACK OF CURB OR SIDEWALK. IF GAP EXISTS AFTER INSTALLATION, ECB MAY BE NECESSARY.
3. LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
4. ALL CONSTRUCTION TRASH MUST BE THOROUGHLY CLEANED UP ON A DAILY BASIS.
5. REFER TO IRRIGATION AND LANDSCAPE PLANS FOR TEMPORARY IRRIGATION PLANS.
6. ALL PERMANENT SLOPES STEEPER THAN 4:1 NEED ECB.
7. CONCRETE WASHOUT, STABILIZED STAGING AREA, PORTABLE TOILETS AND SOIL STOCKPILE LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR/DEVELOPER AND UPDATED ON THE STORMWATER MANAGEMENT PLAN AS REQUIRED BY THE STATE ISSUED STORMWATER DISCHARGE PERMIT. ANY CBMP PLAN CHANGES SHALL BE COORDINATED THROUGH THE TOWN'S ENVIRONMENTAL INSPECTOR.

**LEGEND**

- CWA CONCRETE WASHOUT AREA
- DD DIVERSION DITCH
- IPW INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
- PCOI INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
- LOC LIMITS OF CONSTRUCTION
- PTP PORTABLE TOILET PROTECTION
- SF SILT FENCE
- SSA STABILIZED STAGING AREA
- VTC VEHICLE TRACKING CONTROL



SEDIMENT TRAPS AND TEMPORARY SEDIMENT BASIN LOCATIONS ARE SHOWN ON THIS PLAN. THE FINAL ENGINEERING DESIGN/SUBMITTAL WILL INCLUDE DETAILED GRADING FOR SEDIMENT TRAPS, TEMPORARY SEDIMENT BASINS, OUTLET/RISER PIPES, TEMPORARY SLOPES DRAINS, AND DIVERSION DITCHES.



**TOWN OF PARKER APPROVALS**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
 DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
 9801 EAST EASTER AVE  
 CENTENNIAL, CO 80112  
 303.537.8020  
 Tucson - San Diego - Riverside - Orange  
 Sacramento - San Luis Obispo - Phoenix  
 rickengineering.com

**811**  
 Know what's below.  
 Call before you dig.

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NO.	DESCRIPTION	DATE:
1		

SCALE: 1" = 50'  
 DATE: 5/6/2020  
 DRAWN BY: WD  
 CHECKED BY: JS  
 JOB NO: D01173-A

**INITIAL CBMP PLAN**

PROJECT NO. D01173-A  
 DRAWING NO. 5  
 SHEET NO. 5 OF 22 SHEETS

PRELIMINARY NOT FOR CONSTRUCTION

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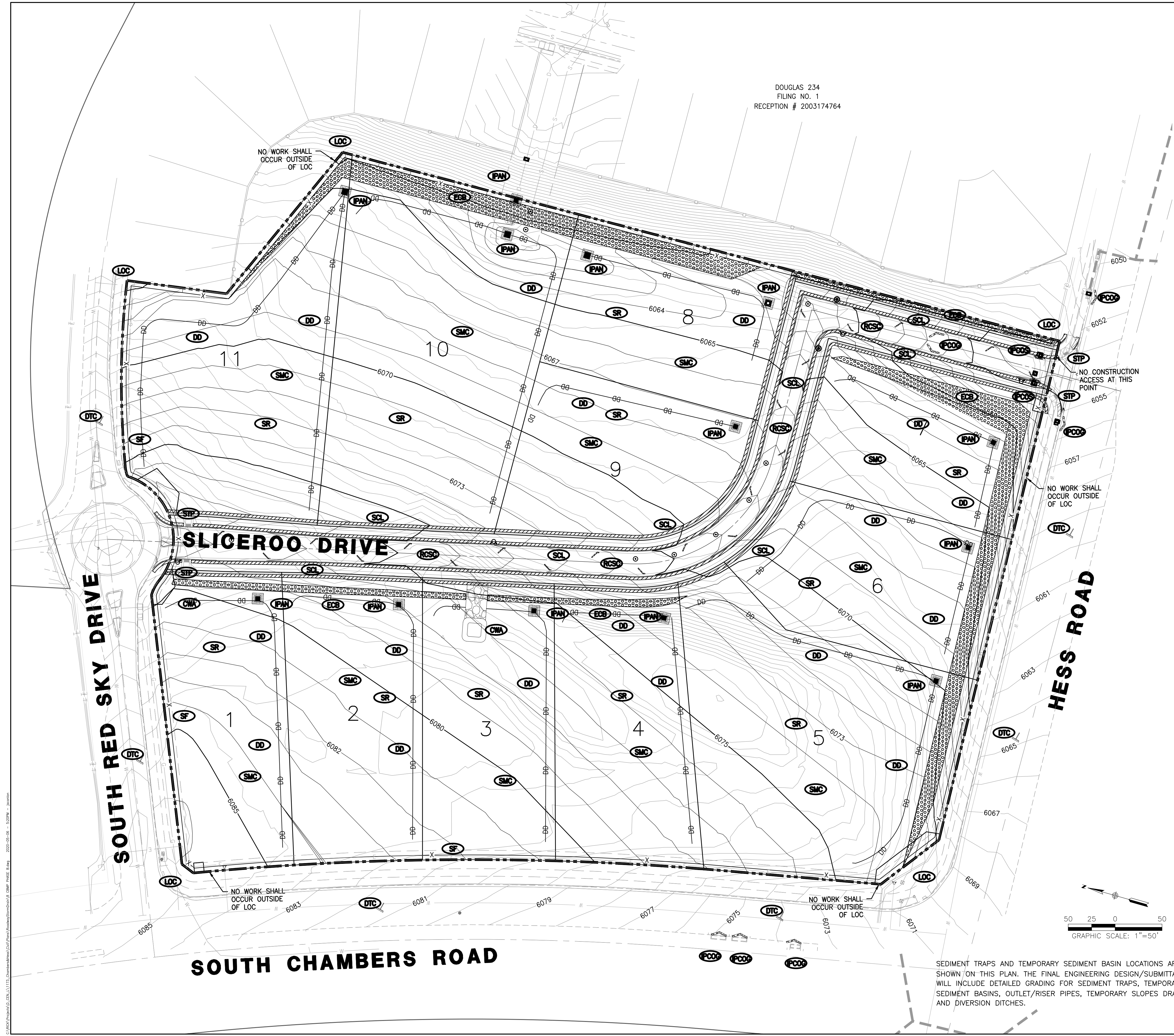
DOUGLAS 234  
 FILING NO. 1  
 RECEPTION # 2003174764

**NOTES:**

1. SILT FENCE TO BE INSTALLED IN AREAS OF SIGNIFICANT TOPOGRAPHY AND SHALL HAVE STAKES @ 5'-0" O.C.
2. ALL SILT FENCE AND OTHER SEDIMENT BMP'S MUST BE PLACED IMMEDIATELY BEHIND THE BACK OF CURB OR SIDEWALK. IF GAP EXISTS AFTER INSTALLATION, ECB MAY BE NECESSARY.
3. LOT PROTECTION (LP) IS REQUIRED ON COMMERCIAL LOTS WHEN LANDSCAPING IS NOT POSSIBLE.
4. ALL CONSTRUCTION TRASH MUST BE THOROUGHLY CLEANED UP ON A DAILY BASIS.
5. REFER TO IRRIGATION AND LANDSCAPE PLANS FOR TEMPORARY IRRIGATION PLANS.
6. ALL PERMANENT SLOPES STEEPER THAN 4:1 NEED ECB.
7. CONCRETE WASHOUT, STABILIZED STAGING AREA, PORTABLE TOILETS AND SOIL STOCKPILE LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR/DEVELOPER AND UPDATED ON THE STORMWATER MANAGEMENT PLAN AS REQUIRED BY THE STATE ISSUED STORMWATER DISCHARGE PERMIT. ANY CBMP PLAN CHANGES SHALL BE COORDINATED THROUGH THE TOWN'S ENVIRONMENTAL INSPECTOR.
8. EROSION CONTROL BLANKET (ECB) MAY BE UTILIZED OVER THE ENTIRE TREE LAWN IN LIEU OF THE SCL.

**LEGEND**

- CONCRETE WASHOUT AREA
- DEBRIS TRASH CONTROL
- INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT
- INLET PROTECTION, CURB ON-GRADE, TYPE R INLET
- INLET PROTECTION, CURB ON SUMP, TYPE R INLET
- LIMITS OF CONSTRUCTION
- SIDEWALK TRANSITION PROTECTION
- ROUGH CUT STREET CONTROL
- SEDIMENT CONTROL LOG
- EROSION CONTROL BLANKET
- SILT FENCE
- SEEDING, MULCHING AND CRIMPING
- SURFACE ROUGHING
- DIVERSION DITCH



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**TOWN OF PARKER APPROVALS**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING

DATE

**ROADWAY & STORM DRAIN PLANS FOR  
 DOUGLAS 234  
 FILING, NO. 6**  
 DOUGLAS COUNTY, COLORADO

**RICK**  
 ENGINEERING COMPANY  
 9801 EAST EASTER AVE  
 CENTENNIAL, CO 80112  
 303.537.8020  
 Tucson - San Diego - Riverside - Orange  
 Sacramento - San Luis Obispo - Phoenix  
 rickengineering.com

**811**  
 Know what's below.  
 Call before you dig.

REVISIONS:

NO.	DESCRIPTION	DATE
1		

SCALE: 1" = 50'  
 DATE: 5/6/2020  
 DRAWN BY: WD  
 CHECKED BY: JS  
 JOB NO: D01173-A

**INTERIM-FINAL  
 CBMP  
 PLAN**

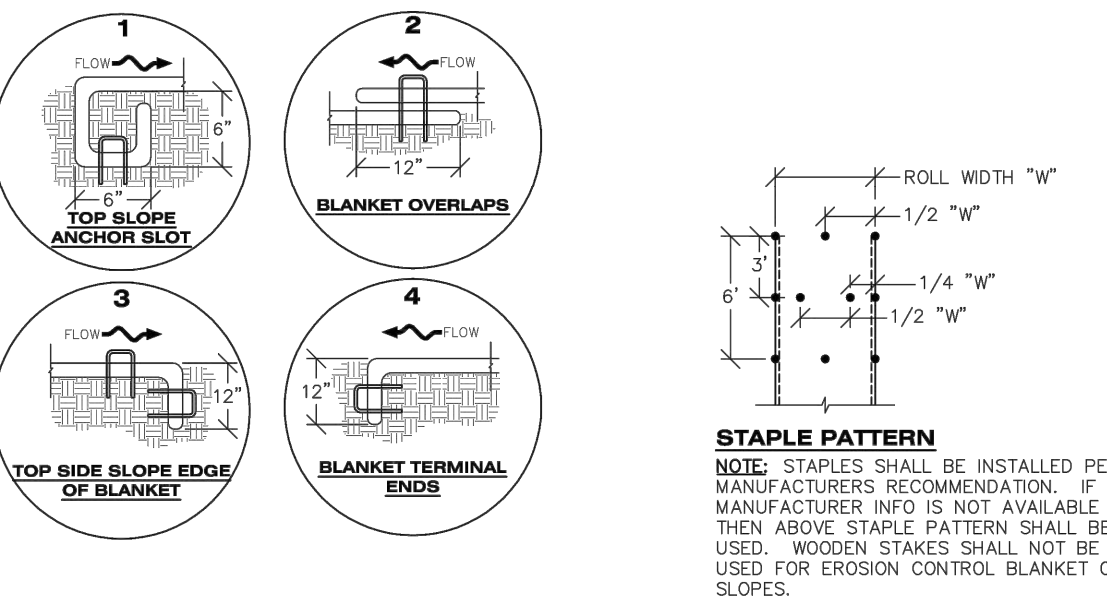
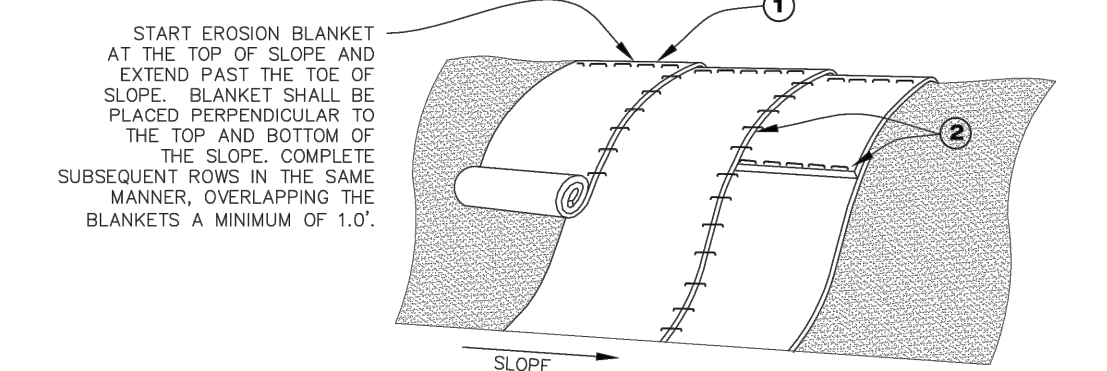
PROJECT NO. D01173-A  
 DRAWING NO. 6  
 SHEET NO. 6 OF 22 SHEETS

PRELIMINARY  
 NOT FOR CONSTRUCTION





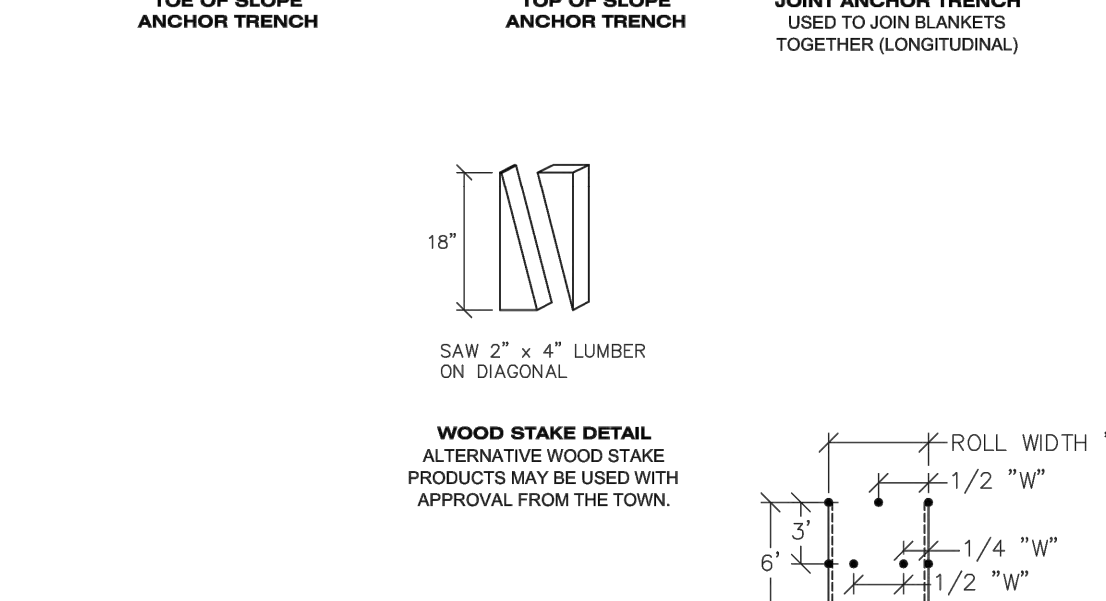
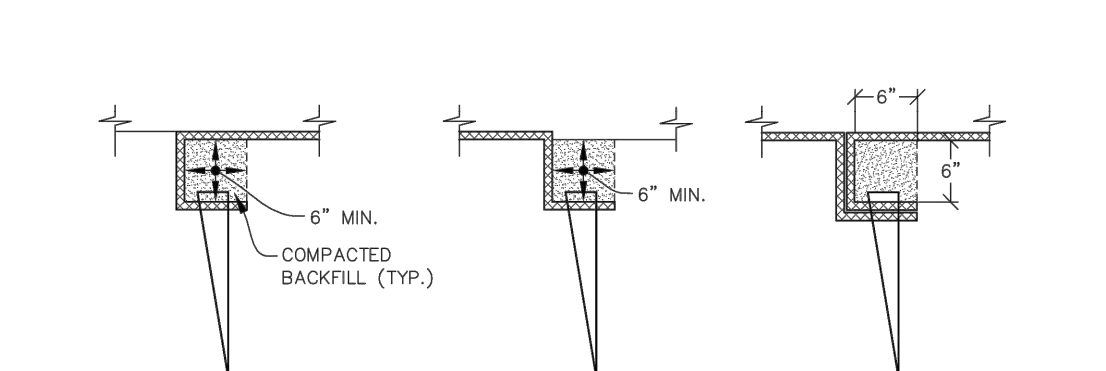
NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER



**EROSION CONTROL BLANKET (SLOPE)**  
ECB  
1 OF 3  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **ECB** | **1 OF 3** | **Oct. 2013**

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**EROSION CONTROL BLANKET (CHANNEL)**  
ECB  
2 OF 3  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **ECB** | **2 OF 3** | **Oct. 2013**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

**EROSION CONTROL BLANKET INSTALLATION NOTES**  
1. SEE THE PLAN VIEW FOR THE LOCATION(S) OF THE EROSION CONTROL BLANKETS.  
2. EROSION CONTROL BLANKETS USED FOR CHANNEL PROTECTION SHALL BE PROPERLY SELECTED BY THE DESIGN ENGINEER BASED ON CURRENT AND FUTURE FLOW RATES WITHIN THE CHANNEL. BASED ON THESE CALCULATIONS, TURF REINFORCEMENT MATTING OR RIPRAP MAY BE NECESSARY IN LIEU OF EROSION CONTROL BLANKETS.  
3. IMMEDIATELY PRIOR TO BLANKET INSTALLATION, SOIL SURFACE SHALL BE SMOOTH AND FREE OF ANY GAPS, VOIDS, WEEDS, ROCKS, STICKS, OR OTHER MISCELLANEOUS DEBRIS.  
4. EROSION CONTROL BLANKET SHALL THEN BE INSTALLED ACCORDING TO THE DETAILED DRAWINGS.  
5. ANY DAMAGED OR REMAINING STAPLES OR STAKES SHALL BE REMOVED FROM THE SITE.  
6. ALL EROSION CONTROL BLANKETS FOR SLOPE PROTECTION INSTALLED IN THE TOWN SHALL BE DOUBLE NET, STRAW OR EXCELISOR.  
7. IN MOST CASES, EROSION CONTROL BLANKETS FOR SLOPE PROTECTION ARE TO REMAIN IN PLACE PERMANENTLY.

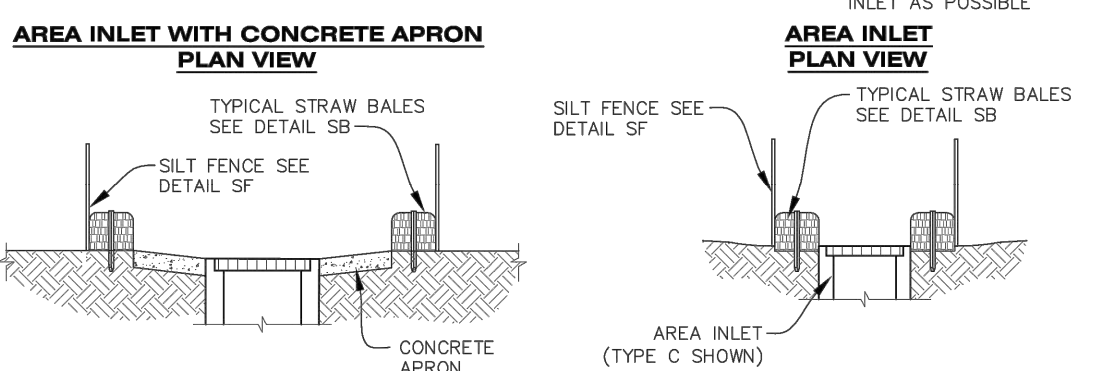
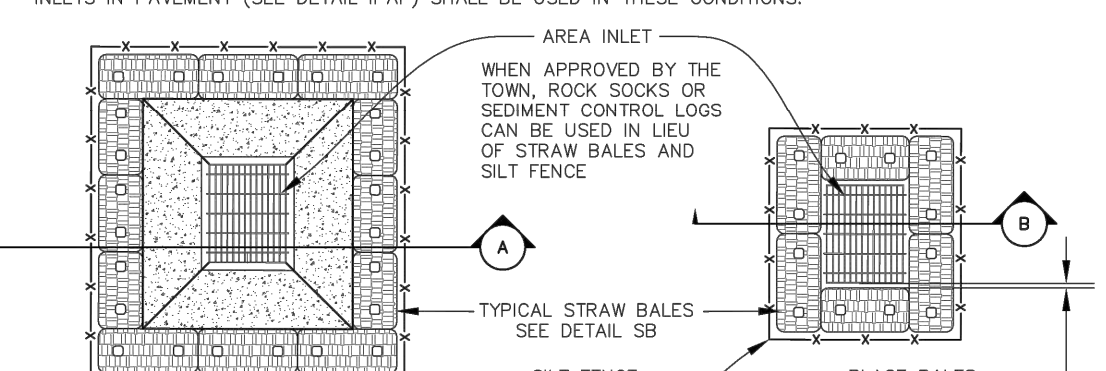
MANUFACTURER	PRODUCT NAME
NORTH AMERICAN GREEN	ST150
APPROVED EQUAL	APPROVED EQUAL

**EROSION CONTROL BLANKET MAINTENANCE NOTES**  
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE EROSION CONTROL BLANKETS AND MAKE ANY NECESSARY REPAIRS.

**EROSION CONTROL BLANKET (CHANNEL)**  
ECB  
3 OF 3  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **ECB** | **3 OF 3** | **Oct. 2013**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER



**INLET PROTECTION FOR AREA INLETS NOT IN PAVEMENT**  
IPAN  
1 OF 1  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPAN** | **1 OF 1** | **Oct. 2013**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

**AREA INLET PROTECTION INSTALLATION NOTES**  
1. SEE PLAN VIEW FOR LOCATION(S) OF AREA INLET PROTECTION.  
2. THE AREA INLET PROTECTION SHOWN ON CBMP PLANS SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF NEW INLETS. AN INTERIM STYLE OF INLET PROTECTION MAY BE ALLOWED UNTIL THE INSTALLATION OF THE GUTTER AND/OR PAVEMENT.

**AREA INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**  
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE AREA INLET PROTECTION.  
2. AREA INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.  
3. WHEN THE AREA INLET PROTECTION IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE AREA INLET PROTECTION MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).  
4. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.

**AREA INLET PROTECTION (SLOPE)**  
IPAN  
1 OF 1  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPAN** | **1 OF 1** | **Oct. 2013**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

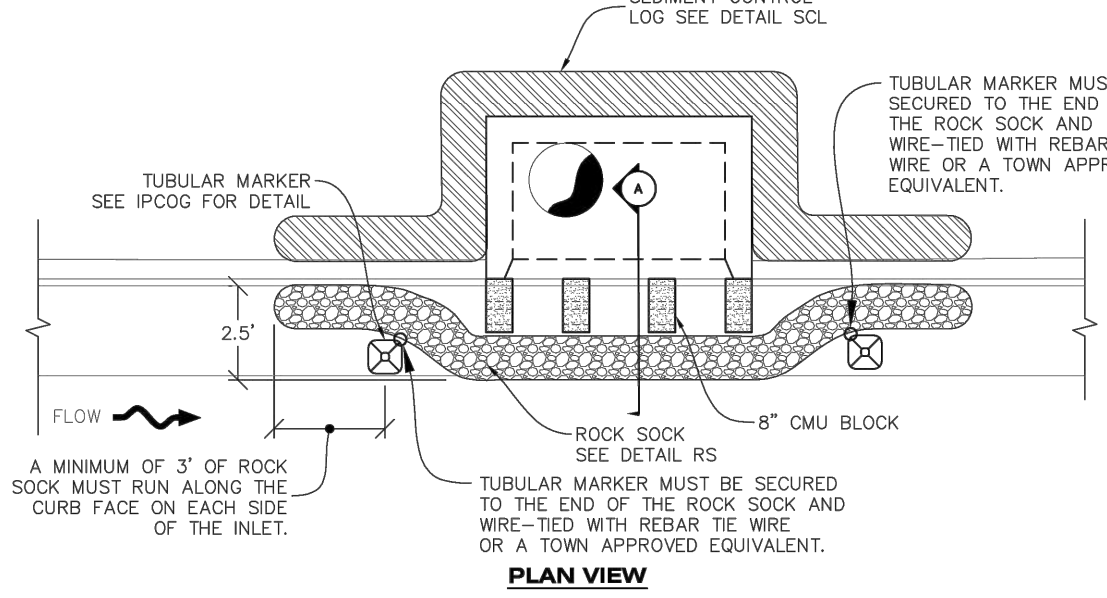
**INLET PROTECTION, CURB ON-GRADE INSTALLATION NOTES**  
1. SEE CBMP PLAN FOR LOCATION(S) OF ON-GRADE INLET PROTECTION.  
2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).  
3. ROCK SOCK FOR ON-GRADE INLET PROTECTION SHALL BE ONE CONTINUOUS PIECE.  
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL, SIZED TO KEEP ROCK FROM SPILLING OUT.  
5. ROCK SOCK SHALL BE PLACED 5.0' UPHILL OF THE INLET OPENING.  
6. TUBULAR MARKER SHALL BE A MINIMUM OF 3.0' HIGH WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.  
7. THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE APPLICABLE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE INLET.

**ON-GRADE INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**  
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE ON-GRADE INLET PROTECTION.  
2. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.  
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.  
4. ON-GRADE INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**INLET PROTECTION, CURB ON SUMP, TYPE R INLET**  
IPCOG  
3 OF 3  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPCOG** | **3 OF 3** | **Oct. 2013**

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**INLET PROTECTION, CURB ON SUMP, TYPE R INLET**  
IPCOG  
1 OF 2  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPCOG** | **1 OF 2** | **Oct. 2013**

NO CHANGES ARE TO BE MADE TO THIS DRAWING WITHOUT WRITTEN PERMISSION OF THE TOWN OF PARKER

**CURB INLET PROTECTION INSTALLATION NOTES**  
1. SEE CBMP PLAN FOR LOCATION(S) OF CURB INLET PROTECTION.  
2. CRUSHED ROCK SHALL BE 2.0"-3.0" IN SIZE WITH A FRACTURED FACE (ALL SIDES).  
3. ROCK SOCK SHALL BE ONE CONTINUOUS PIECE OR SHALL BE CONSTRUCTED USING WIRE WRAPPED JOINTS (SEE DETAIL RS).  
4. ROCK SOCK SHALL BE CONSTRUCTED USING CHICKEN WIRE OR OTHER APPROVED MATERIAL SIZED TO KEEP ROCK FROM SPILLING OUT.  
5. ROCK SOCK SHALL EXTEND 3.0' ALONG THE CURB BEYOND LOCATIONS WHERE IT RETURNS TO CONTACT CURB FACE.  
6. TUBULAR TRAFFIC MARKERS SHALL BE A MINIMUM OF 36" IN HEIGHT WITH REFLECTIVE BANDS AND OCTAGON SHAPED BASES.  
7. THE CURB INLET PROTECTION SHOWN ON CBMP PLAN SHALL BE INSTALLED ON EXISTING INLETS PRIOR TO ANY LAND DISTURBING ACTIVITIES OR IMMEDIATELY AFTER THE INSTALLATION OF THE FIRST LIFT OF ASPHALT ON ROADWAYS DRAINING TO THE CURB INLET. CMU BLOCKS OR THE ROCK SOCK SHALL BE USED AS INTERIM PROTECTION UNTIL THE FIRST LIFT OF ASPHALT IS INSTALLED.

**CURB INLET PROTECTION INSPECTION AND MAINTENANCE NOTES**  
1. THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE CURB INLET PROTECTION.  
2. ACCUMULATED SEDIMENT SHALL BE REMOVED AS SOON AS POSSIBLE, IMMEDIATELY IN MOST CASES.  
3. ROCK SOCKS SHALL BE REPLACED IF THEY BECOME HEAVILY SOILED OR DAMAGED.  
4. CURB INLET PROTECTION SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.

**CURB INLET PROTECTION (SLOPE)**  
IPCOG  
2 OF 2  
Oct. 2013

**Parker COLORADO** | **CBMP** | **CONSTRUCTION BEST MANAGEMENT PRACTICES** | **IPCOG** | **2 OF 2** | **Oct. 2013**

**TOWN OF PARKER APPROVALS**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORKS DATE

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
9801 EAST EASTER AVE  
CENTENNIAL, CO 80112  
303.537.8020  
Tucson - San Diego - Riverside - Orange  
Sacramento - San Luis Obispo - Phoenix  
rickengineering.com

**811**  
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Call before you dig.

NO.	DESCRIPTION	DATE:
△	-	-

SCALE: NTS  
DATE: 5/6/2020  
DRAWN BY: WD  
CHECKED BY: JS  
JOB NO: D01173-A

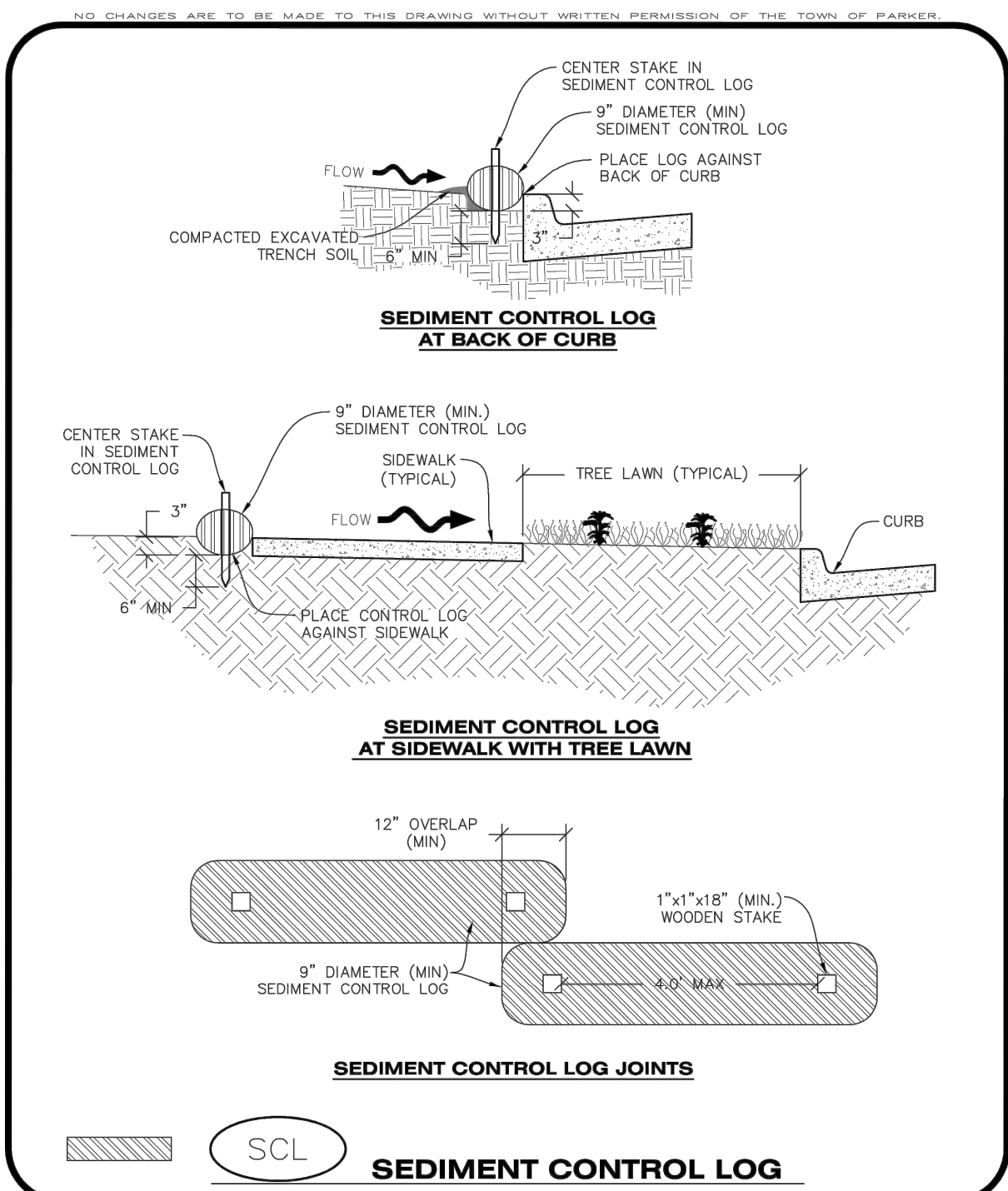
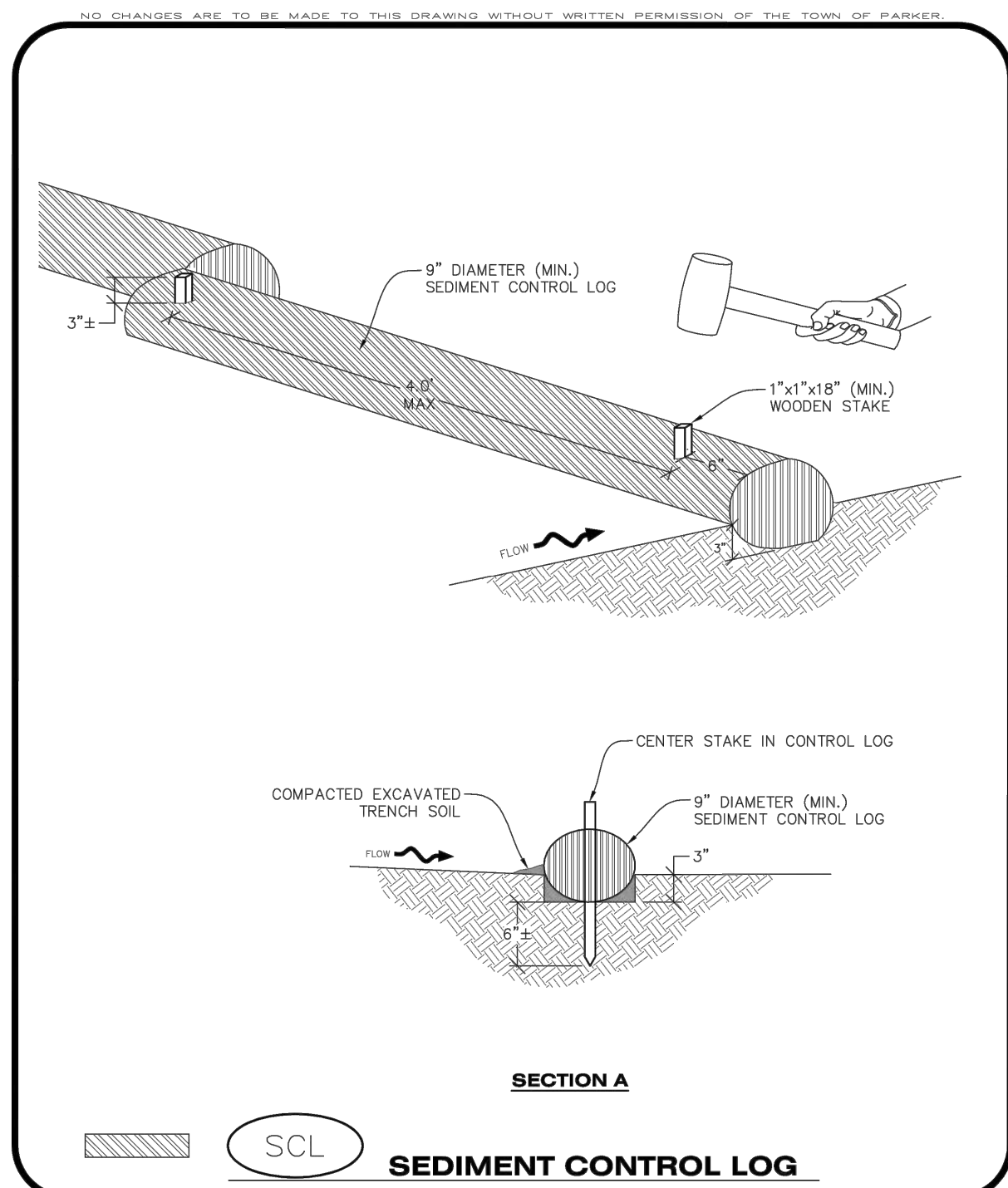
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PROJECT NO: D01173-A  
DRAWING NO: 9  
SHEET NO: 9 OF 22 SHEETS

PRELIMINARY NOT FOR CONSTRUCTION

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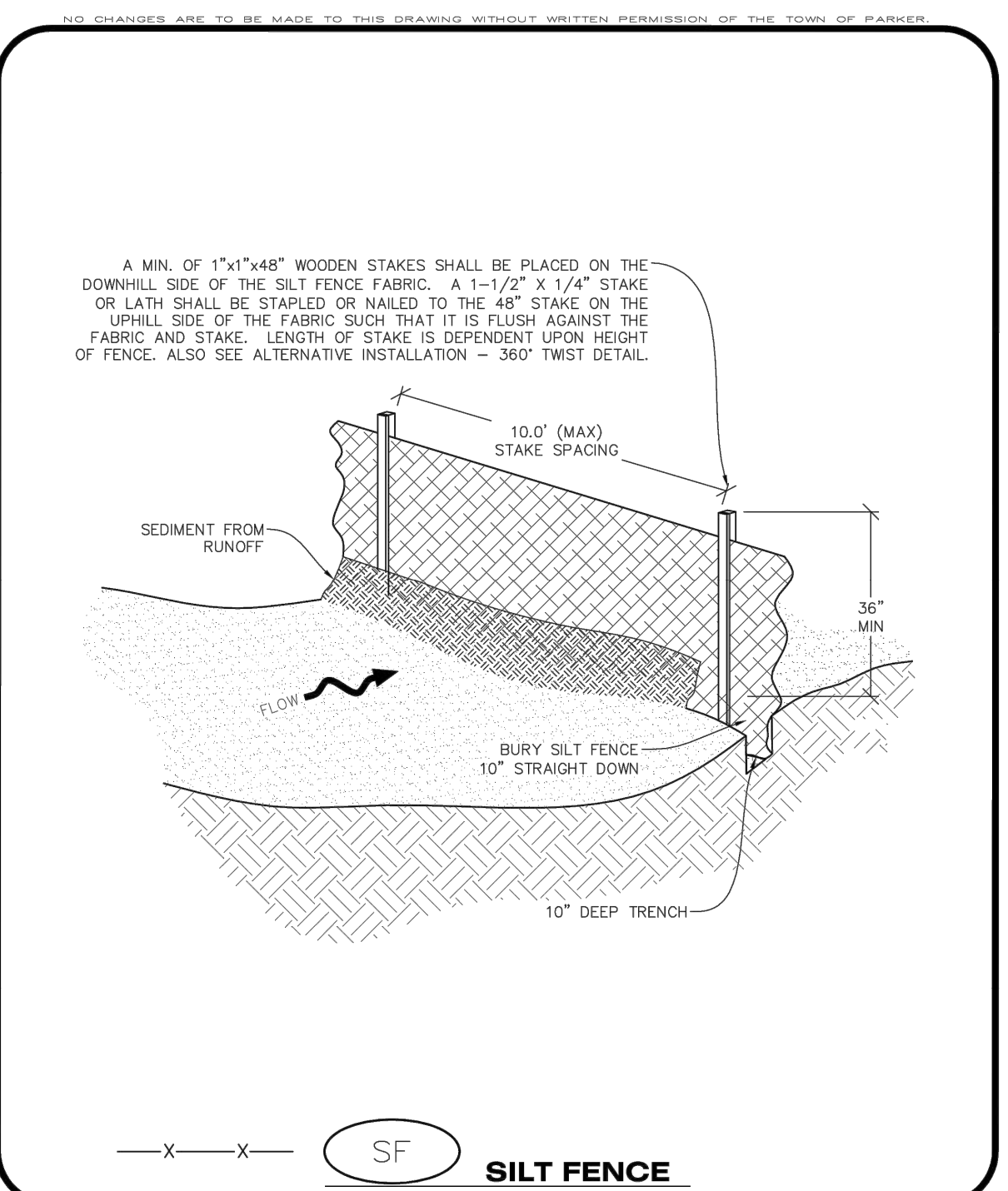


**SEDIMENT CONTROL LOG INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SEDIMENT CONTROL LOGS.
- ALL SEDIMENT CONTROL LOGS SHALL BE INSTALLED FREE OF DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- SEDIMENT CONTROL LOGS SHALL BE INSTALLED IMMEDIATELY ADJACENT TO AN IMPERVIOUS SURFACE SUCH AS A CURB HEAD, SIDEWALK, INLET LID, ETC. NO GAPS SHALL EXIST BETWEEN THE SEDIMENT CONTROL LOG AND THE IMPERVIOUS SURFACE.
- A UNIFORM 3" DEEP ANCHOR TRENCH (APPROX.) IN THE SHAPE OF A HALF-SPHERE SHALL BE EXCAVATED USING A TRENCHER, SPADE-SHAPED SHOVEL, OR PICK. THE ANCHOR TRENCH SHALL BE SIZED TO ALLOW FOR THE SEDIMENT CONTROL LOG TO SEAT TIGHTLY AGAINST THE ANCHOR TRENCH.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH AND PROPERLY COMPACTED.
- ANCHOR TRENCH SHALL BE RELATIVELY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT.
- ALL SEDIMENT CONTROL LOGS SHALL BE PLACED 3" (APPROX.) BELOW THE GROUND AND PULLED TIGHT ON BOTH ENDS TO REMOVE ANY CURVES OR SNAGS.
- THE UPHILL SIDE OF THE SEDIMENT CONTROL LOG SHALL BE BACKFILLED WITH SOIL THAT IS RELATIVELY FREE OF ROCKS AND DEBRIS. THE SOIL SHALL BE TIGHTLY COMPACTED AGAINST THE GROUND AND SEDIMENT CONTROL LOG USING A SHOVEL, OR SIMILAR DEVICE.
- SEDIMENT CONTROL LOG STAKES SHALL BE MADE OF WOOD AND SECURELY ANCHOR THE SCL IN PLACE.
- STAKES SHALL BE PLACED ON 4.0' CENTERS AND EMBEDDED APPROXIMATELY 6" INTO THE GROUND. STAKES THAT ARE BROKEN PRIOR TO OR DURING INSTALLATION SHALL BE REPLACED.
- SEDIMENT CONTROL LOGS SHALL OVERLAP A MINIMUM OF 12". THE OVERLAPPING SHALL OCCUR ON THE UP-GRADE SIDE OF THE LOGS.
- SEDIMENT CONTROL LOGS SHALL BE STAKED WITHIN 6" FROM EACH END.
- SEDIMENT CONTROL LOGS THAT ARE INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN THE CONCRETE AND THE LOG. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

**SEDIMENT CONTROL LOG INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SEDIMENT CONTROL LOGS.
- ACCUMULATED SEDIMENT SHALL BE REMOVED ONCE THE SEDIMENT HAS REACHED A DEPTH EQUAL TO 1/2 THE HEIGHT OF EXPOSED LOG.
- SEDIMENT CONTROL LOGS SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- SEDIMENT CONTROL LOGS SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR OR DAMAGE THAT WOULD PREVENT THE SCL FROM FUNCTIONING AS DESIGNED.
- WHEN THE SEDIMENT CONTROL LOGS ARE REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SEDIMENT CONTROL LOGS MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

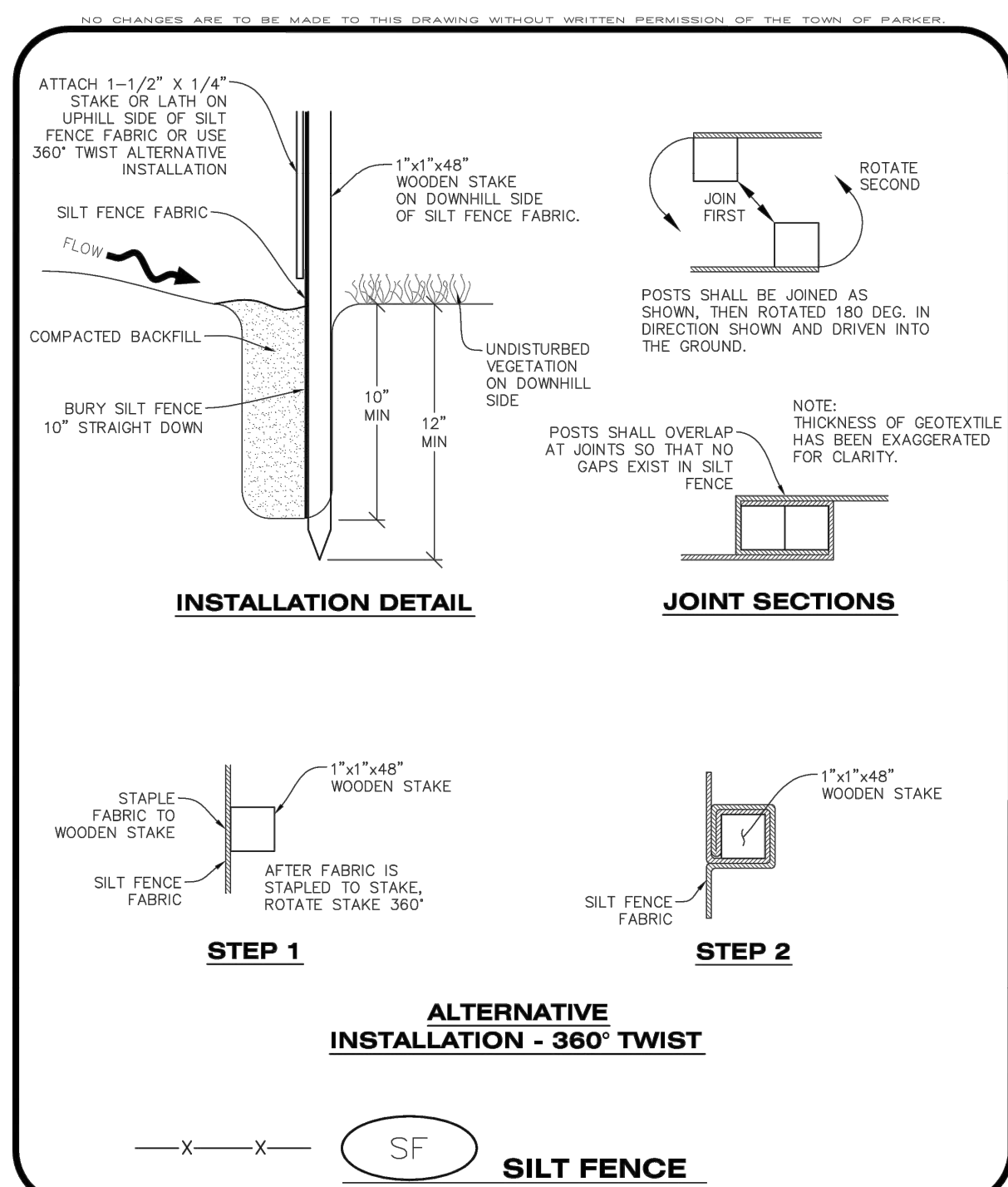


**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SCL** 1 OF 3  
 Oct. 2013

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SCL** 2 OF 3  
 Oct. 2013

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SCL** 3 OF 3  
 Oct. 2013

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SF** 1 OF 4  
 Oct. 2013



**SILT FENCE INSTALLATION NOTES**

- SEE CBMP PLAN FOR LOCATION(S) OF SILT FENCE.
- ALL SILT FENCE SHALL BE INSTALLED IN GOOD CONDITION AND FREE OF ANY DEFECTS INCLUDING RIPS, HOLES AND OBVIOUS WEAR.
- A UNIFORM 10" DEEP ANCHOR TRENCH SHALL BE EXCAVATED USING A TRENCHER.
- A 10" DEEP ANCHOR SLIT SHALL BE FORMED IF USING A STATIC SLICING METHOD.
- EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF THE ANCHOR TRENCH.
- ANCHOR TRENCH SHALL BE GENERALLY FREE OF ROCKS OR OTHER DEBRIS PRIOR TO THE PLACEMENT OF THE SILT FENCE.
- THE ANCHOR TRENCH SHALL BE THOROUGHLY BACKFILLED WITH SOIL THAT IS GENERALLY FREE OF ROCKS AND DEBRIS.
- ALL EXCAVATED MATERIAL SHALL BE PLACED ON THE UP-GRADE SIDE OF THE SILT FENCE.
- STAKES SHALL BE POSITIONED ON THE DOWNHILL SIDE OF THE SILT FENCE FABRIC AND PLACED ON 10.0' CENTERS OR LESS. STAKES SHALL BE EMBEDDED A MINIMUM OF 12" INTO THE GROUND. A WOODEN LATH SHALL BE ATTACHED TO THE OPPOSING (UPHILL) SIDE OF THE STAKE FOR ADDED STRENGTH AND SUPPORT. THE LATH SHALL HAVE THE FOLLOWING DIMENSIONS: 1"x4"x24".
- SILT FENCE SHALL BE PULLED TIGHT AS IT IS ANCHORED TO THE STAKES. THERE SHOULD NOT BE SIGNIFICANT SAGGING ALONG ANY PORTION OF THE SILT FENCE AFTER IT HAS BEEN ANCHORED TO THE STAKES.
- SILT FENCE FABRIC SHALL BE ANCHORED TO THE STAKES AND LATHS USING STAPLES OR NAILS OF AN APPROXIMATE LENGTH. ENOUGH STAPLES AND NAILS SHOULD BE PLACED ALONG THE LATH TO ENSURE PROPER ATTACHMENT.
- SILT FENCE FABRIC SHALL MEET THE FOLLOWING MANDATORY REQUIREMENTS:

PROPERTIES	TEST METHOD	MANDATORY REQUIREMENTS
GRAB TENSILE STRENGTH	ASTM D 4632	> 124 LBS
MULLEN BURST STRENGTH	ASTM D 3786	> 300 PSI
PUNCTURE STRENGTH	ASTM D 4833	> 60 LBS
TRAPEZOID TEAR STRENGTH	ASTM D 4533	> 65 LBS
UV RESISTANCE	ASTM D 4355	> 80% AT 500 HOURS OF UV EXPOSURE
FLOW RATE	ASTM D 4491	> 10 GAL/MIN/FT2

- AN ORIGINAL PRODUCT SPECIFICATION SHEET FROM THE SILT FENCE MANUFACTURER SHALL BE MADE AVAILABLE AT THE REQUEST OF THE TOWN'S INSPECTOR. THE PRODUCT SPECIFICATION SHEET SHALL PROVIDE THE RESULTS FOR THE TEST METHODS ABOVE.
- SILT FENCE JOINTS SHALL BE CONNECTED ACCORDING TO THE ATTACHED DRAWING.
- SILT FENCE THAT IS INSTALLED BEHIND CURBS AND SIDEWALKS MUST BE DONE SO THAT NO MORE THAN A 2" GAP EXISTS BETWEEN CONCRETE AND THE SILT FENCE. EROSION CONTROL BLANKETING (ECB) BETWEEN THE GAP MAY BE REQUIRED IN INSTANCES WHERE THIS DOES NOT OCCUR.

**SILT FENCE INSPECTION AND MAINTENANCE NOTES**

- THE EROSION CONTROL SUPERVISOR SHALL REGULARLY INSPECT THE SILT FENCE.
- ACCUMULATED SEDIMENT SHALL BE REMOVED REGULARLY.
- SILT FENCE SHALL REMAIN IN PLACE AND PROPERLY MAINTAINED UNTIL VEGETATIVE COVER HAS REACHED A CONSISTENT DENSITY OF AT LEAST 70% OF FULL VEGETATIVE COVER AND EROSION AND SEDIMENTATION IS NO LONGER A POSSIBILITY AS DETERMINED BY THE TOWN'S INSPECTOR OR AS OTHERWISE DIRECTED BY THE TOWN'S INSPECTOR.
- SILT FENCE SHALL BE REPLACED WHEN THERE ARE ANY SIGNS OF WEAR AND/OR DAMAGE.
- WHEN THE SILT FENCE IS REMOVED, ANY DISTURBED AREAS ASSOCIATED WITH THE INSTALLATION, MAINTENANCE, AND/OR REMOVAL OF THE SILT FENCE MAY NEED TO BE ROUGHENED, SEEDED, MULCHED, AND CRIMPED PER THE TOWN'S SPECIFICATIONS (SEE DETAIL SMC).

**SEEDING AND MULCHING SHALL BE PERFORMED ACCORDING TO THE ACCOMPANYING DETAIL(S) AND TEXT. NO EXCEPTIONS SHALL BE MADE**

- SEE PLAN VIEW FOR:
  - LOCATION(S) OF SEEDING AND MULCHING
  - TYPE OF SEED MIX
- SEED MIXES MAY CONFORM TO THE TABLE PROVIDED WITH THE SMC NOTES OR ALTERNATIVES MAY BE ALLOWED WITH PRIOR PERMISSION BY THE TOWN'S INSPECTOR.
- SEEDING MAY BE PERFORMED YEAR ROUND ASSUMING THE SOIL IS NOT FROZEN. SEEDING DURING TIMES OF EXTREME TEMPERATURES SHOULD BE AVOIDED IF POSSIBLE.
- AT THE BEGINNING OF THE LAND DISTURBANCE ACTIVITIES, IT IS HIGHLY RECOMMENDED THAT AN APPROPRIATE AMOUNT OF NATIVE TOPSOIL BE STRIPPED FROM THE SITE AND STOCKPILED. ALL AREAS, PRIOR TO PERMANENT SEEDING AND MULCHING, WILL LIKELY NEED TO BE COVERED WITH AN APPROPRIATE LAYER OF TOPSOIL. THIS REQUIREMENT APPLIES TO ALL AREAS WHERE NATIVE SEEDING IS SPECIFIED ON THE CBMP PLAN AND/OR LANDSCAPING PLANS.
- IT IS STRONGLY RECOMMENDED THAT SAMPLES FROM THE STRIPPED TOPSOIL BE PROPERLY COLLECTED AND TESTED BY A QUALIFIED LABORATORY TO ENSURE ADEQUATE NUTRIENT CONTENT PRIOR TO SEEDING AND MULCHING. IF IT IS DISCOVERED THAT THE TOPSOIL IS VOID OF THE NUTRIENTS NECESSARY TO SUCCESSFULLY ESTABLISH THE REQUIRED VEGETATION, THEN THE APPROPRIATE AMENDMENTS SHALL BE ADDED.
- ALL AREAS TO BE SEEDED AND MULCHED SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING DETAILS AND NOTES. SURFACE ROUGHENING SHALL OCCUR AFTER PLACEMENT OF THE TOPSOIL.
- WHEN INSTALLED WITH A DRILL SEEDER, SEED SHALL BE PLACED AT A DEPTH OF 1/4 - 1/2 INCH. ROW SPACING SHALL BE NO MORE THAN 6-INCHES.
- ALL AREAS INCAPABLE OF BEING DRILL SEEDER SHALL BE SURFACE ROUGHENED ACCORDING TO THE SURFACE ROUGHENING NOTES OR EFFECTIVELY ROUGHENED USING A HARROW OR OTHER SUCH IMPLEMENT. ALL AREAS SHALL BE UNIFORMLY HAND BROADCASTED WITH THE PROPER SEED MIX APPLIED AT TWO TIMES THE DRILL SEEDER RATE. BROADCASTED AREAS SHALL THEN BE RE-HARROWED OR RE-RAKED USING A HARD-TIPPED RAKE TO ENSURE THAT SEEDS ARE BURIED TO AN APPROXIMATE DEPTH OF 1/4 - 1/2 INCH.
- AFTER SEEDING HAS BEEN COMPLETED, MULCH SHALL BE UNIFORMLY APPLIED AT A RATE OF 2 TONS/ACRE (4,000 LBS/ACRE). MULCH SHALL BE MECHANICALLY CRIMPED TO A DEPTH OF 2 INCHES USING A CRIMPER. MULCH SHALL BE HAND CRIMPED AND COVERED WITH A TACKIFIER IN AREAS WHERE MECHANICAL CRIMPING IS NOT POSSIBLE. WHEN SOILS PERMIT, ALL MULCH SHALL BE CRIMPED SUCH THAT THE INDIVIDUAL PIECES OF STRAW OR HAY FORM EXAGGERATED V-SHAPES PROTRUDING OUT OF THE GROUND SEVERAL INCHES.
- IN CERTAIN INSTANCES, IT MAY BE NECESSARY TO APPLY A TACKIFIER IN ORDER TO HELP WITH STRAW DISPLACEMENT. TACKIFIER SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S SPECIFICATIONS.

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SF** 2 OF 4  
 Oct. 2013

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SF** 3 OF 4  
 Oct. 2013

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SF** 4 OF 4  
 Oct. 2013

**CBMP** CONSTRUCTION BEST MANAGEMENT PRACTICES  
**SMC** 1 OF 3  
 Oct. 2013

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
 DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
 9801 EAST EASTER AVE  
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 CHECKED BY: JS  
 JOB NO: D01173-A

**CBMP DETAILS 5**

PROJECT NO: D01173-A  
 DRAWING NO: 11  
 SHEET NO. 11 OF 22 SHEETS

**TOWN OF PARKER APPROVALS**  
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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORKS DATE

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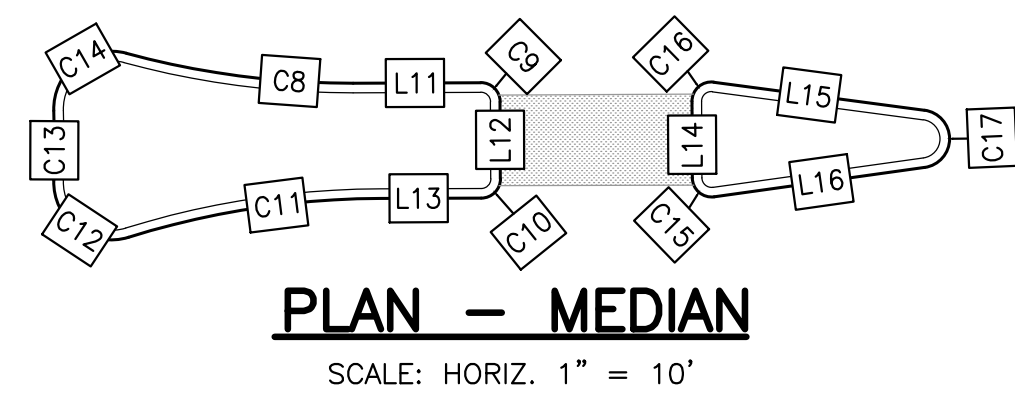






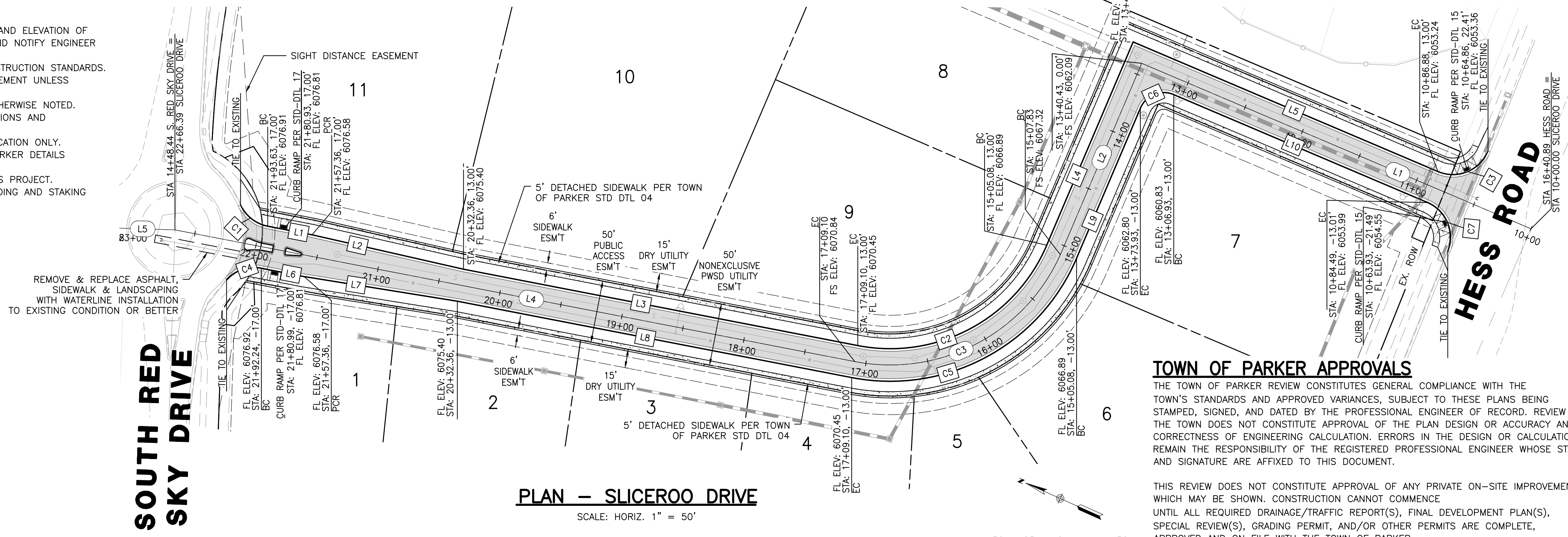
**ROADWAY NOTES**

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- ALL ROAD ELEVATIONS SHOWN IN PROFILE ARE CENTERLINE OF PAVEMENT UNLESS OTHERWISE NOTED.
- ALL STATIONING OFFSETS ARE TAKEN FROM CENTERLINE UNLESS OTHERWISE NOTED. REFER TO FINAL PLAN FOR RIGHT-OF-WAY AND CENTERLINE DIMENSIONS AND HORIZONTAL CONTROL.
- ACCESSIBLE RAMP SYMBOLS SHOWN ON THESE PLANS ARE FOR LOCATION ONLY. ACCESSIBLE RAMPS SHALL BE CONSTRUCTED PER THE TOWN OF PARKER DETAILS INCLUDED IN THIS PLAN SET.
- REFER TO SHEET 22 FOR TYPICAL STREET SECTIONS USED FOR THIS PROJECT.
- REFER TO SHEET 17 FOR DETAILED INTERSECTION AND MEDIAN GRADING AND STAKING INFORMATION.



CURB LINE & CURVE TABLE				
CURVE #/LINE #	LENGTH	BEARING/DELTA	RADIUS	TYPE
C1	28.89'	054°15'55"	30.50'	TYPE 2 SECTION II-B
C2	186.34'	077°55'45"	137.00'	TYPE 2 SECTION II-B
C3	48.60'	091°17'40"	30.50'	TYPE 2 SECTION II-B
C4	30.73'	057°43'44"	30.50'	TYPE 2 SECTION II-B
C5	221.70'	077°55'45"	163.00'	TYPE 2 SECTION II-B
C6	32.20'	090°00'00"	20.50'	TYPE 2 SECTION II-B
C7	47.30'	088°50'50"	30.50'	TYPE 2 SECTION II-B
C8	11.95'	015°22'48"	44.50'	TYPE 2 SECTION I-B
C9	1.57'	090°00'00"	1.00'	TYPE 2 SECTION I-B
C10	1.57'	090°00'00"	1.00'	TYPE 2 SECTION I-B
C11	13.32'	017°08'53"	44.50'	TYPE 2 SECTION I-B
C12	5.72'	109°10'54"	3.00'	TYPE 2 SECTION I-B
C13	3.62'	003°40'19"	56.50'	TYPE 2 SECTION I-B
C14	5.60'	107°01'06"	3.00'	TYPE 2 SECTION I-B
C15	1.71'	097°45'55"	1.00'	TYPE 2 SECTION I-B
C16	1.71'	097°45'55"	1.00'	TYPE 2 SECTION I-B
L1	4.31'	164°28'11"	1.50'	TYPE 2 SECTION I-B
L2	36.28	S13°07'09"E	N/A	TYPE 2 SECTION II-B
L3	125.06	S11°17'10"E	N/A	TYPE 2 SECTION II-B
L4	323.25	S13°07'09"E	N/A	TYPE 2 SECTION II-B
L5	177.65	N88°57'06"E	N/A	TYPE 2 SECTION II-B
L6	266.55	S1°02'54"E	N/A	TYPE 2 SECTION II-B
L7	34.88	S13°07'09"E	N/A	TYPE 2 SECTION II-B
L8	125.06	S14°57'07"E	N/A	TYPE 2 SECTION II-B
L9	323.25	S13°07'09"E	N/A	TYPE 2 SECTION II-B
L10	131.15	N88°57'06"E	N/A	TYPE 2 SECTION II-B
L11	221.89	S1°02'54"E	N/A	TYPE 2 SECTION II-B
L12	6.64	S89°48'14"W	N/A	TYPE 2 SECTION I-B
L13	4.00	N0°11'46"W	N/A	TYPE 2 SECTION I-B
L14	5.25	N89°48'14"E	N/A	TYPE 2 SECTION I-B
L15	3.98	N0°11'46"W	N/A	TYPE 2 SECTION I-B
L16	11.07	S82°25'51"E	N/A	TYPE 2 SECTION I-B
L17	11.07	S82°02'20"W	N/A	TYPE 2 SECTION I-B

CENTERLINE LINE & CURVE TABLE				
CURVE #/LINE #	RADIUS	LENGTH	DELTA (Δ)	DIRECTION
C3	150.00'	204.02'	77°55'45"	N52° 05' 01.35"W
L1	N/A	340.43'	N/A	N1° 02' 54.00"W
L2	N/A	164.65'	N/A	S88° 57' 06.00"W
L4	N/A	557.29'	N/A	N13° 07' 08.70"W
L5	N/A	51.95'	N/A	N22° 15' 46.00"W

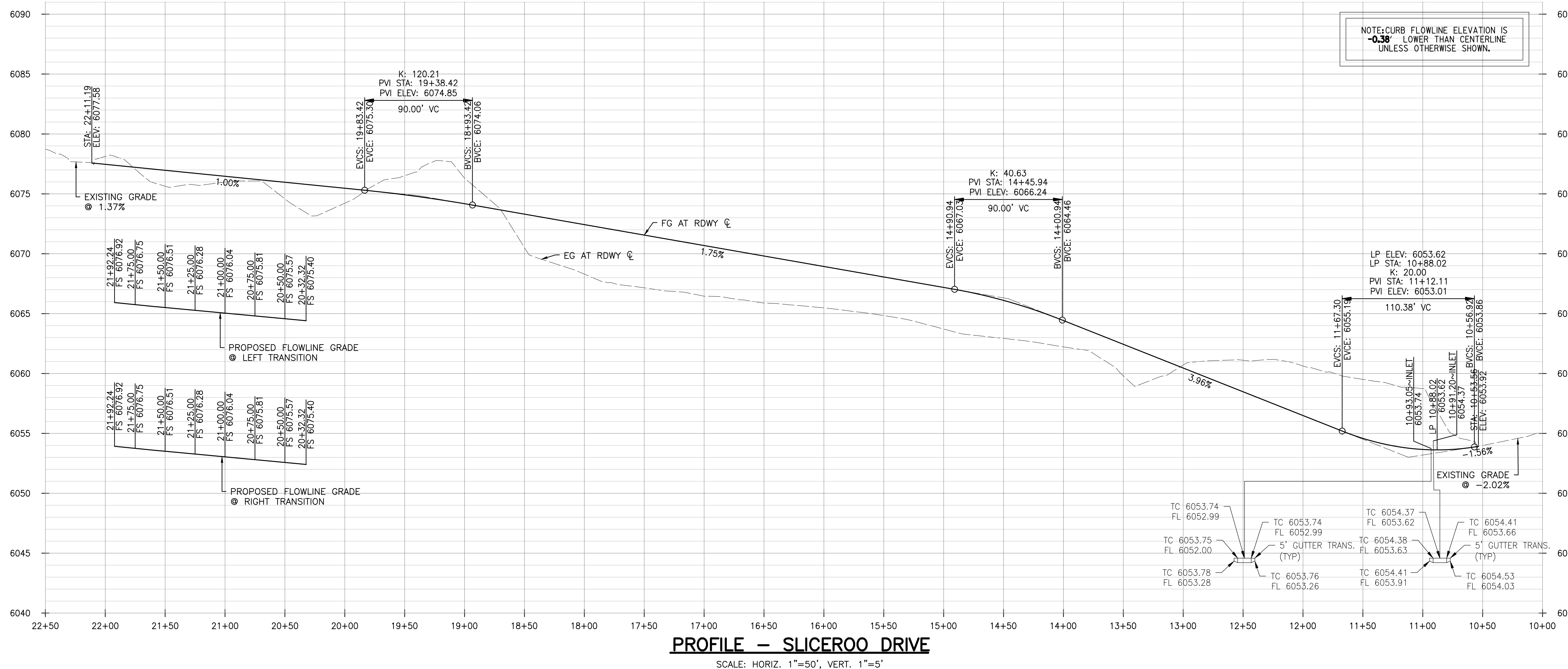


**TOWN OF PARKER APPROVALS**

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TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_



**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
9801 EAST EASTER AVE  
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**SLICEROO DR PLAN & PROFILE**

PROJECT NO: D01173-A  
DRAWING NO: 15

SHEET NO. 15 OF 22 SHEETS

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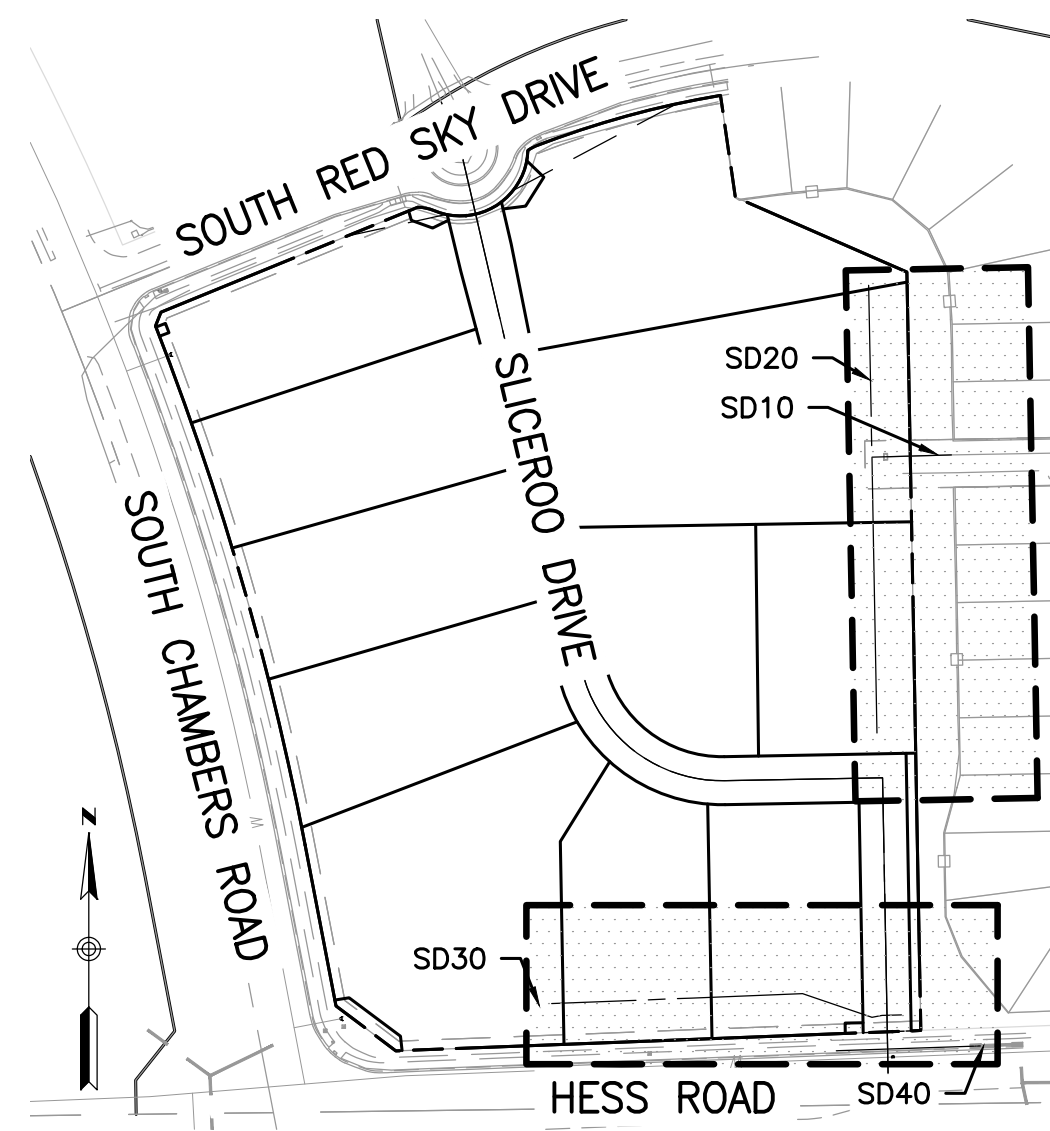
**STORM SEWER NOTES**

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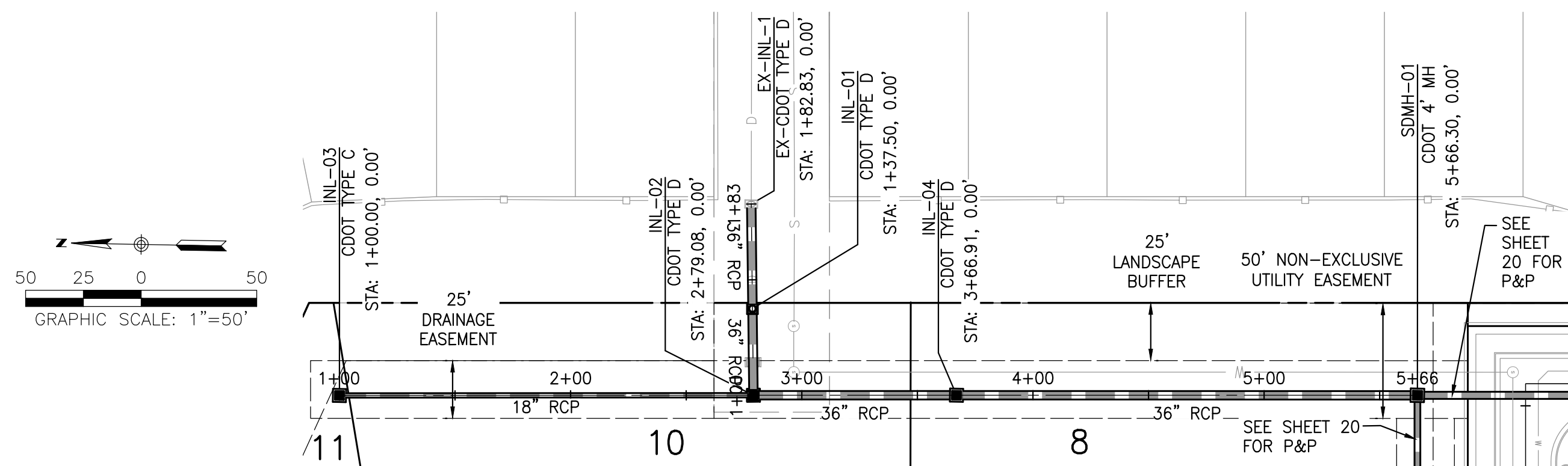


**KEY MAP**

SCALE: HORIZ. 1" = 200'

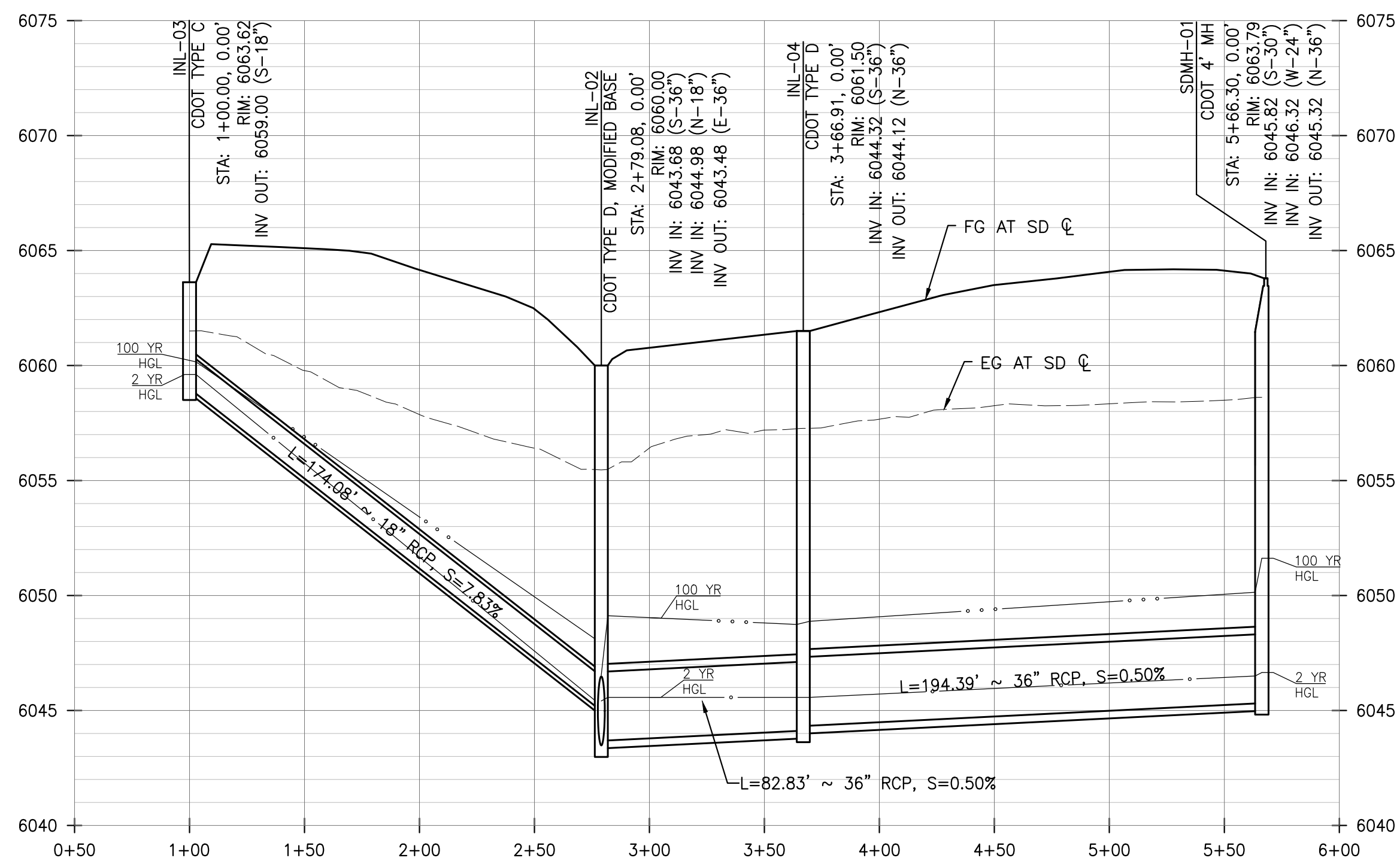
TOWN OF PARKER, DIRECTOR OF ENGINEERING

DATE



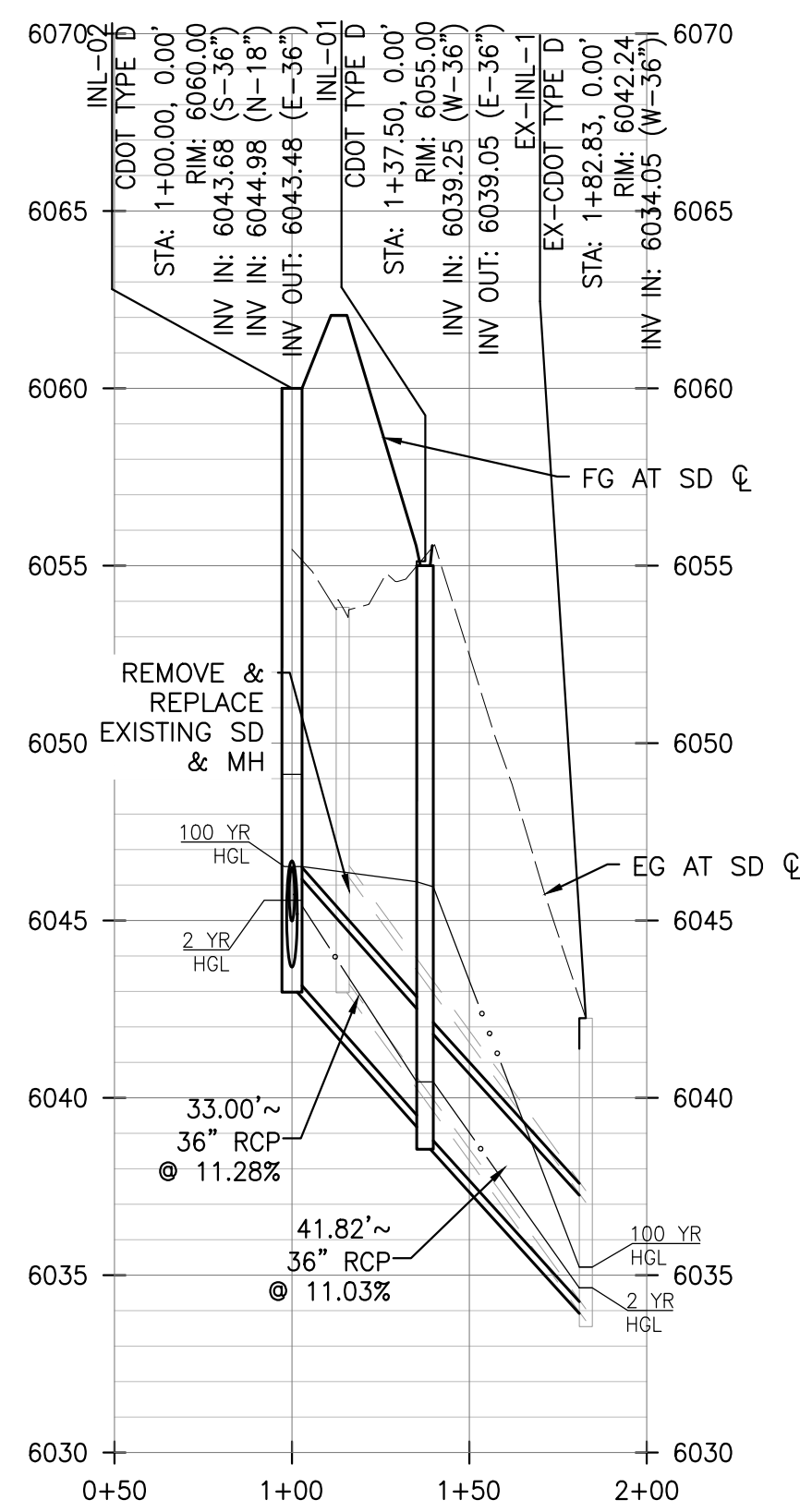
**PLAN - SD10 & SD20**

SCALE: HORIZ. 1" = 50'



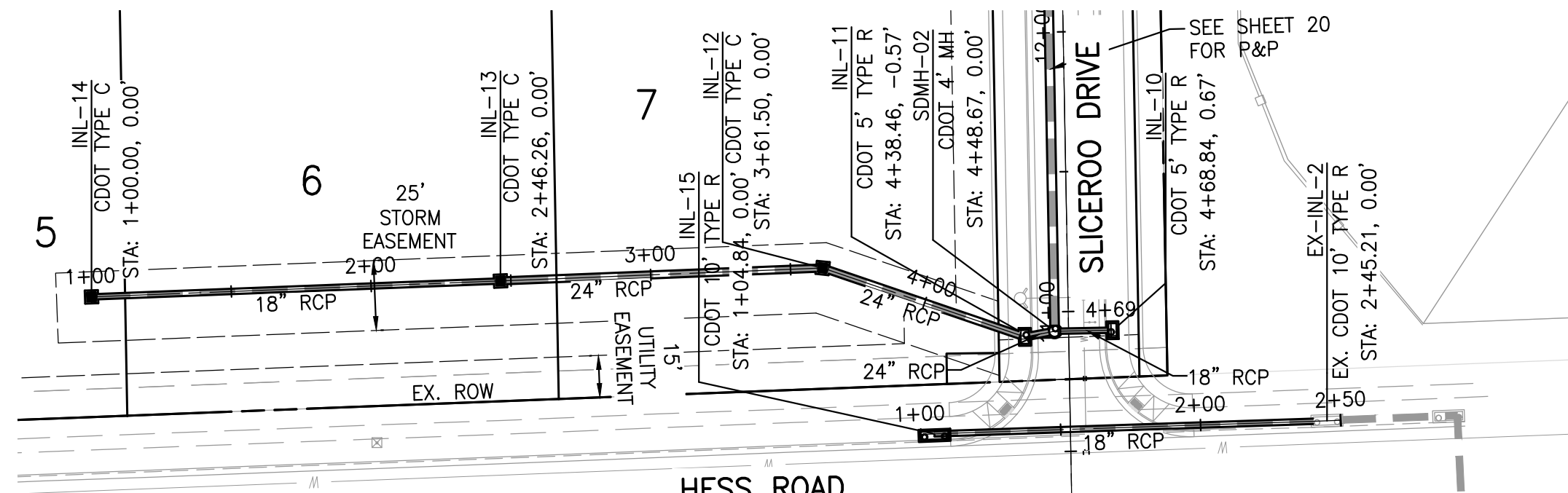
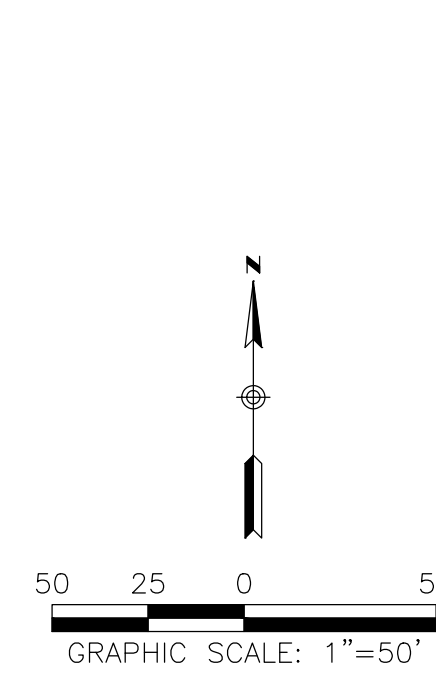
**PROFILE - SD20**

SCALE: HORIZ. 1"=50', VERT. 1"=5'



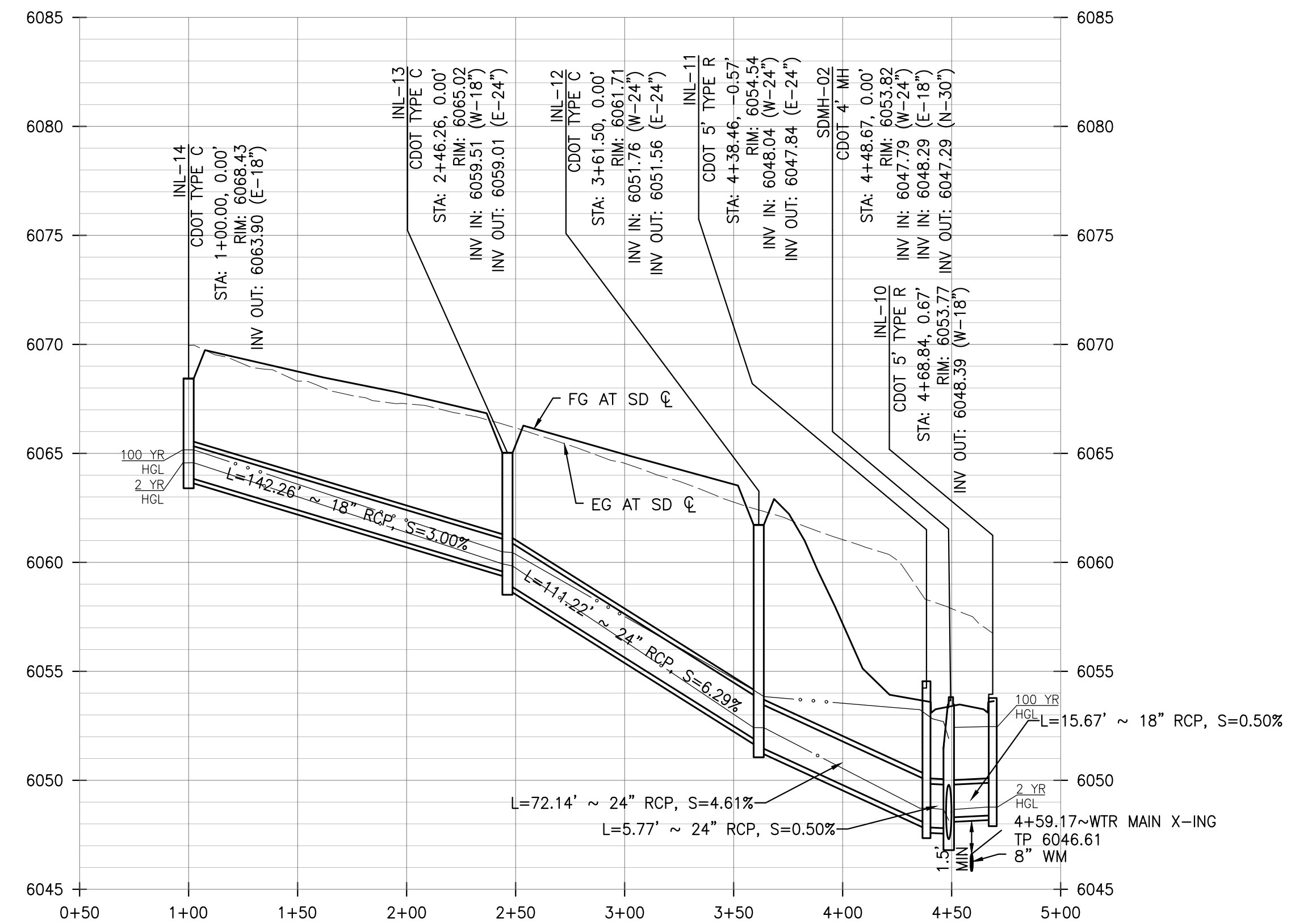
**PROFILE - SD10**

SCALE: HORIZ. 1"=50', VERT. 1"=5'



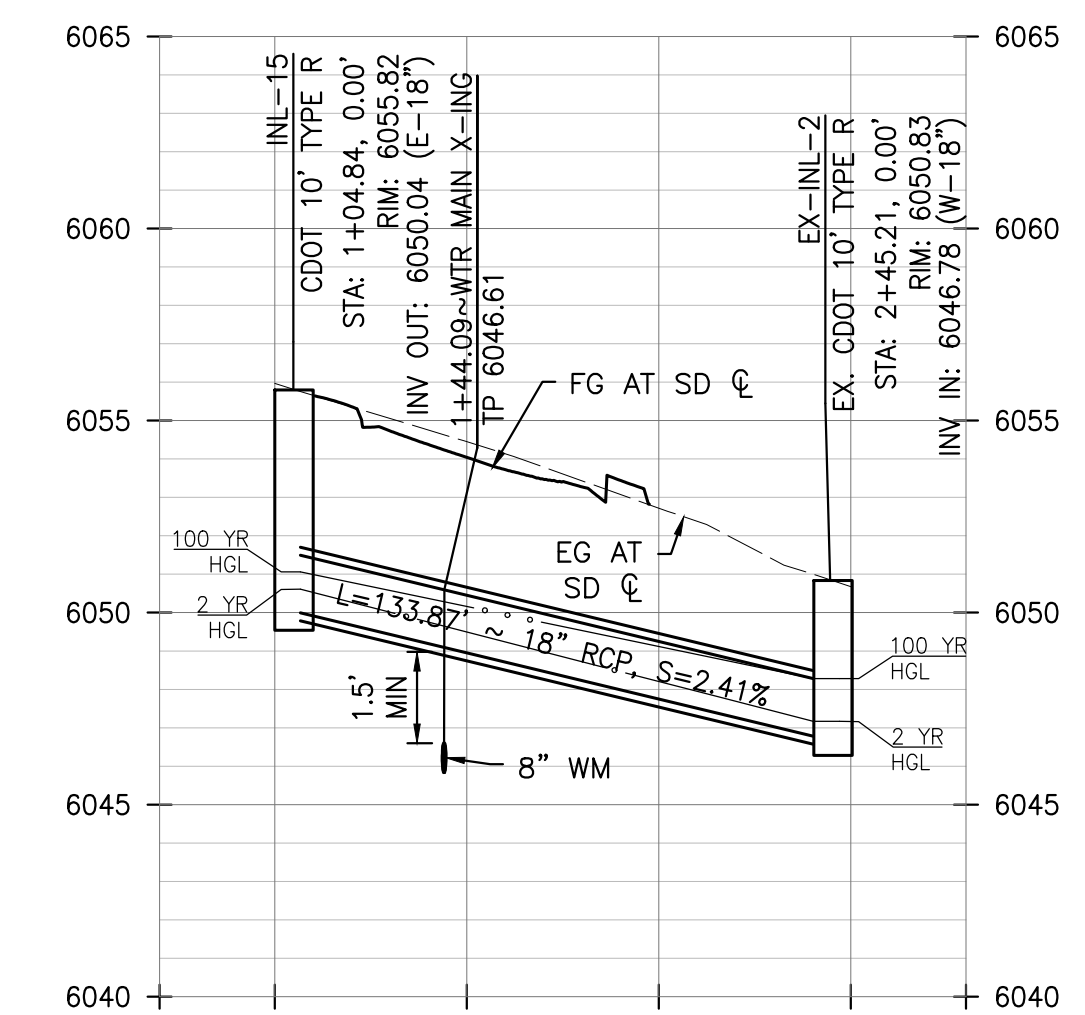
**PLAN - SD30 & SD40**

SCALE: HORIZ. 1" = 50'



**PROFILE - SD30**

SCALE: HORIZ. 1"=50', VERT. 1"=5'



**PROFILE - SD40**

SCALE: HORIZ. 1"=50', VERT. 1"=5'

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
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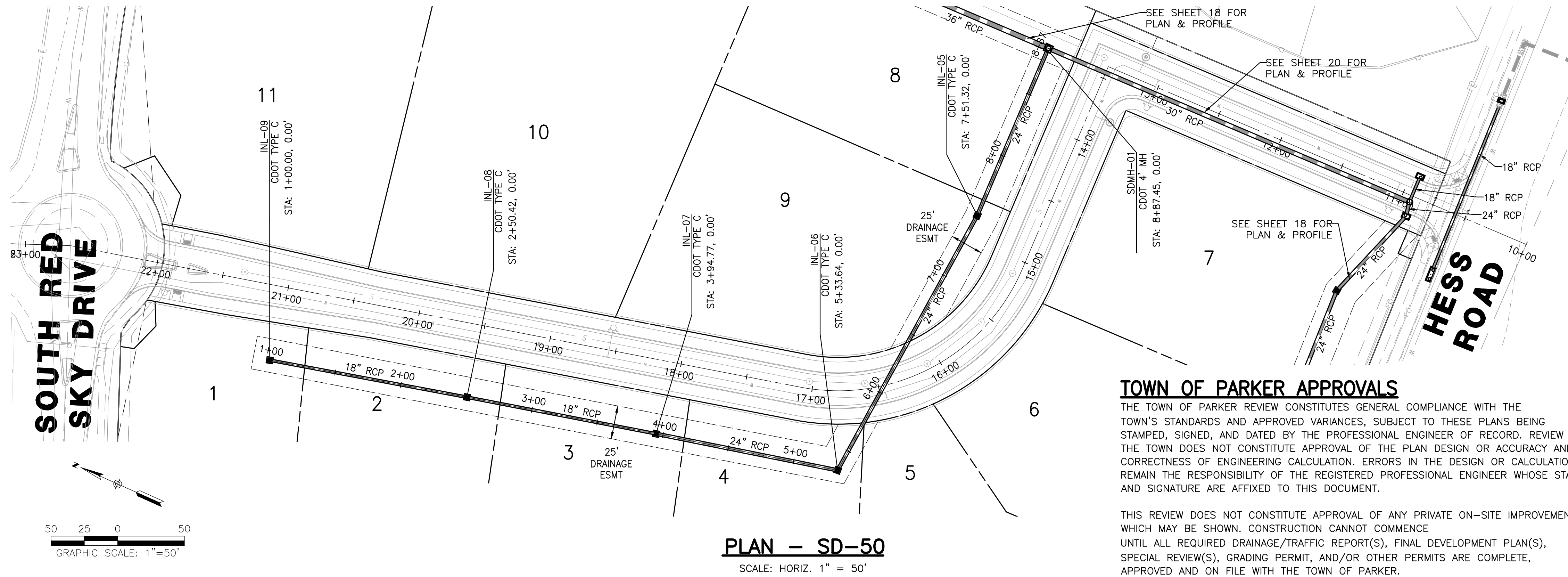
**STORM DRAIN PLAN & PROFILE**

PROJECT NO. D01173-A  
DRAWING NO. 18  
SHEET NO. 18 OF 22 SHEETS

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- A BLANKET DRAINAGE AND PUBLIC ACCESS EASEMENT IS DEDICATED ACROSS THE PRIVATE DRIVE TRACT AS SHOWN ON THE MINOR DEVELOPMENT PLAN.



**TOWN OF PARKER APPROVALS**

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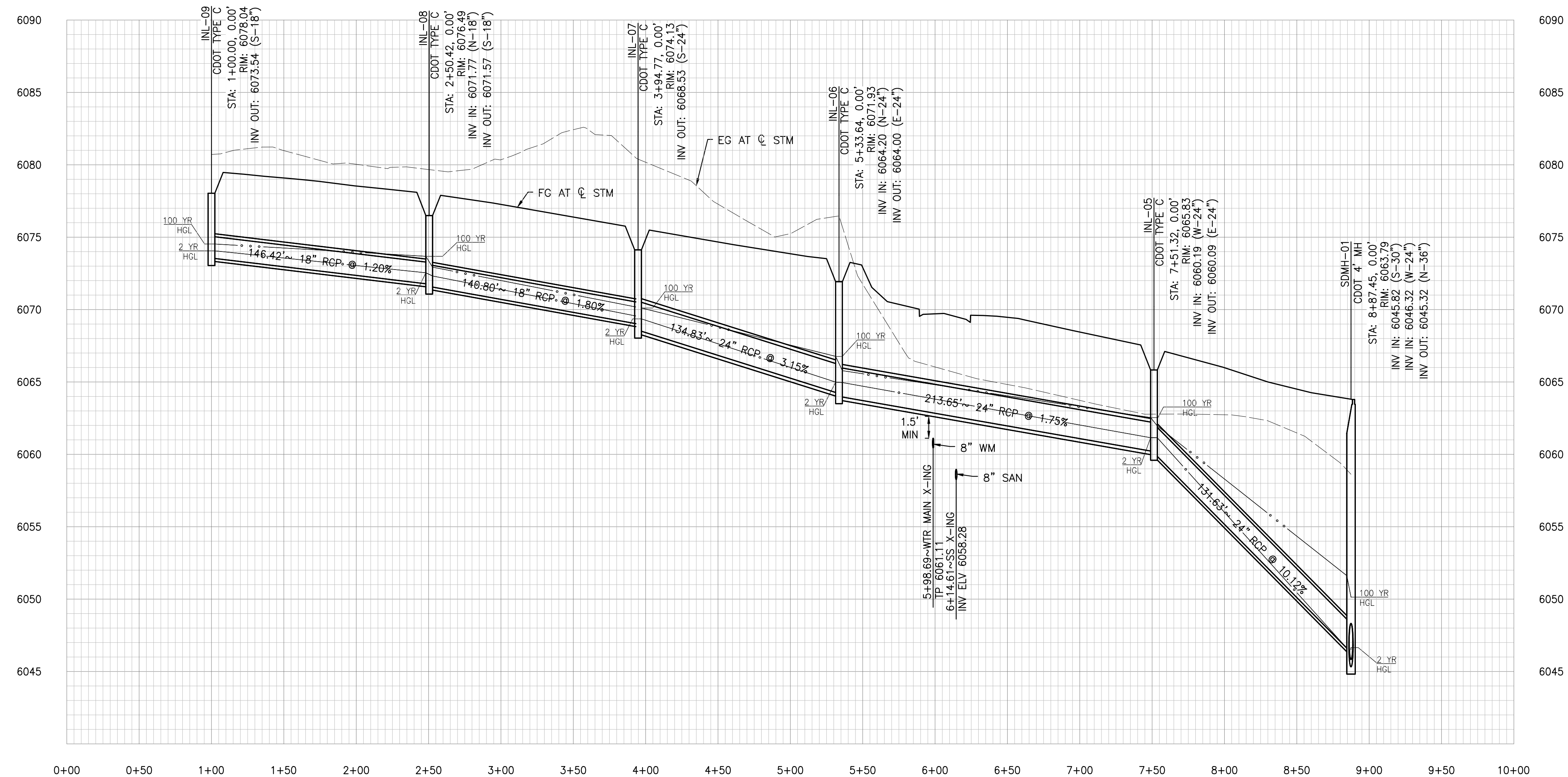
TOWN OF PARKER, DIRECTOR OF ENGINEERING

DATE



**PLAN - SD-50**

SCALE: HORIZ. 1" = 50'



**PROFILE - SD-50**

SCALE: HORIZ. 1"=50', VERT. 1"=5'

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
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**STORM DRAIN PLAN & PROFILE**

PROJECT NO. D01173-A

DRAWING NO. 19

SHEET NO. 19 OF 22 SHEETS

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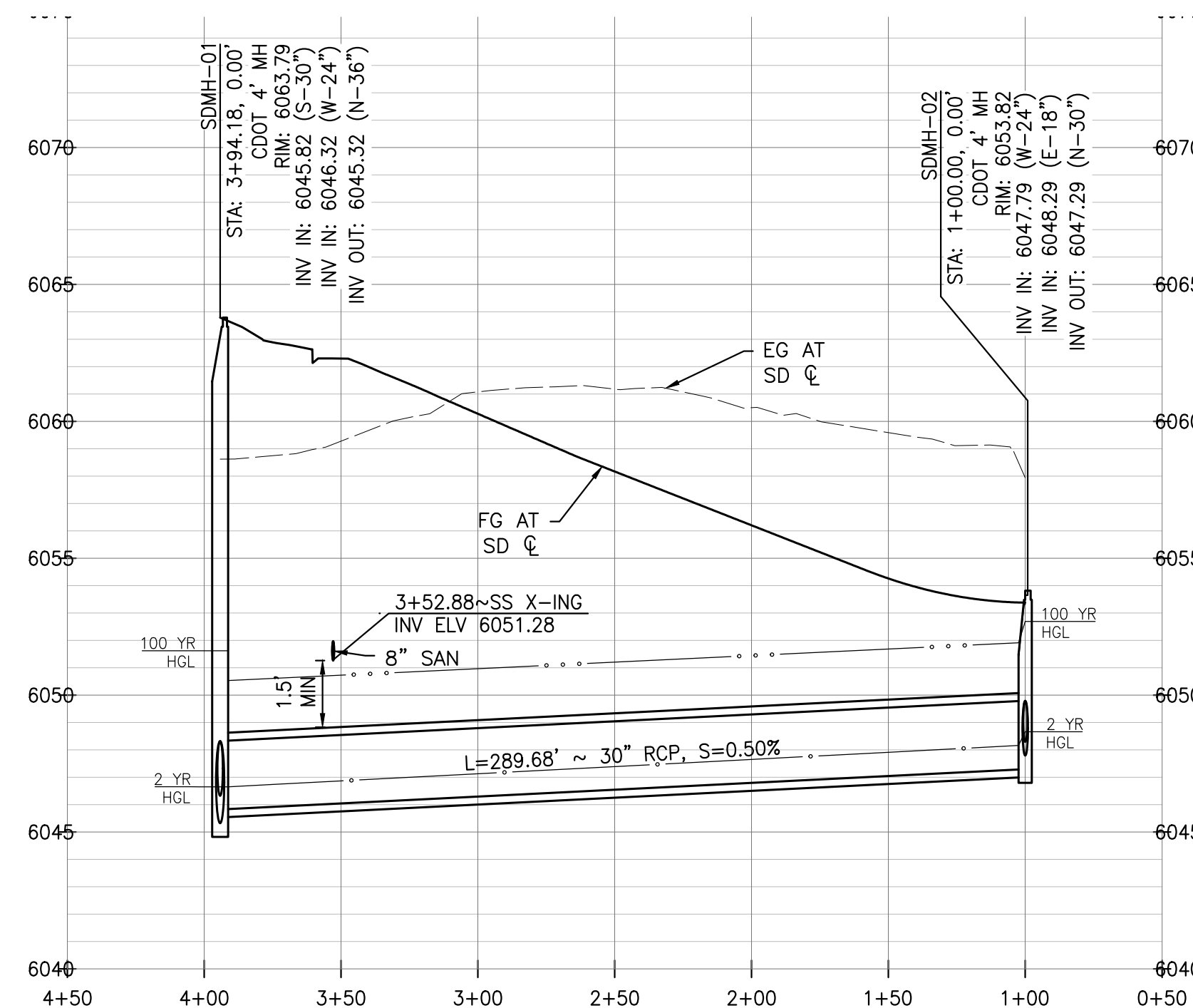
TOWN OF PARKER, DIRECTOR OF ENGINEERING \_\_\_\_\_ DATE \_\_\_\_\_



**KEY MAP**  
SCALE: HORIZ. 1" = 200'



**PLAN - SD-60**  
SCALE: HORIZ. 1" = 50'



**PROFILE - SD-60**  
SCALE: HORIZ. 1"=50', VERT. 1"=5'

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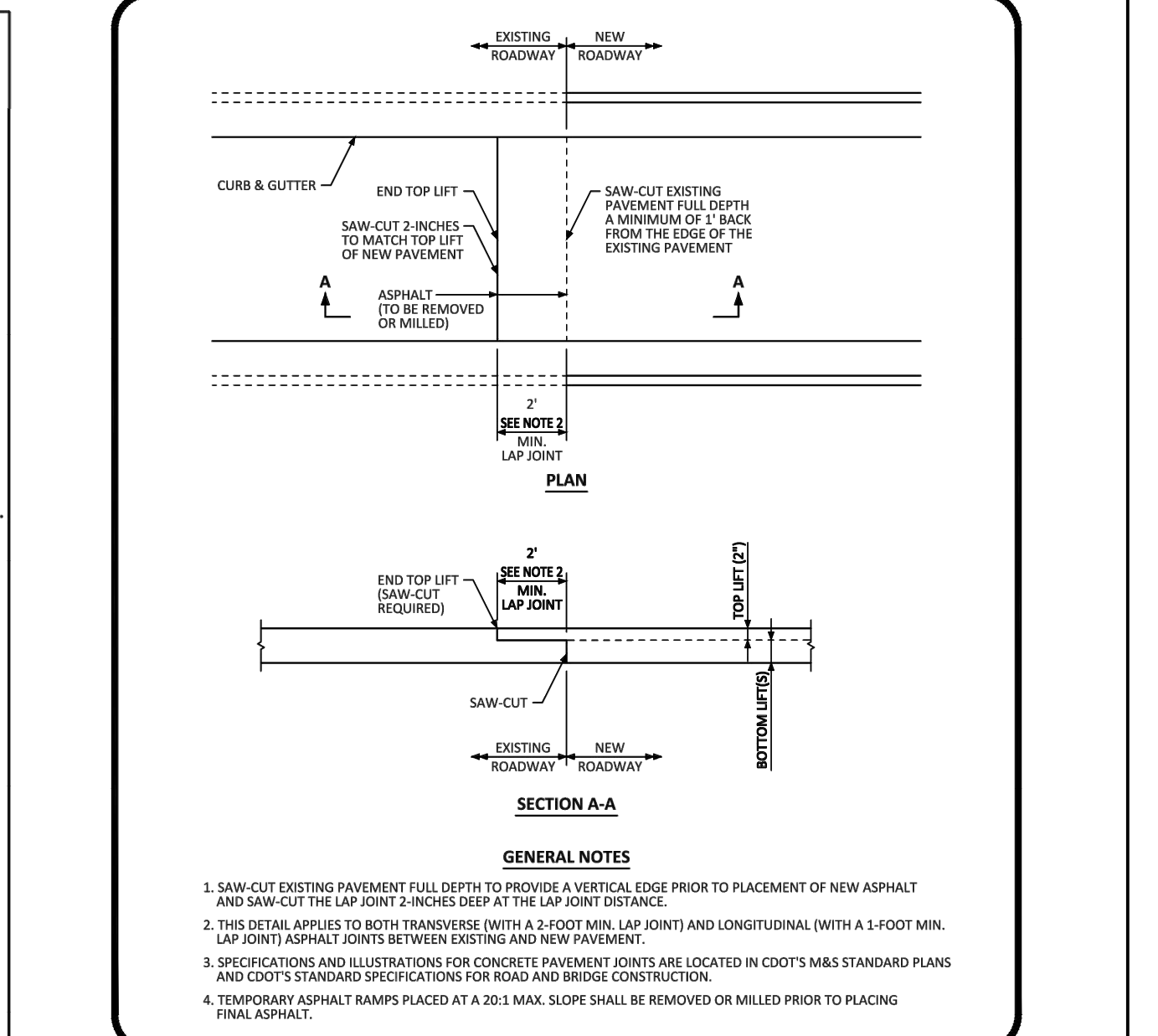
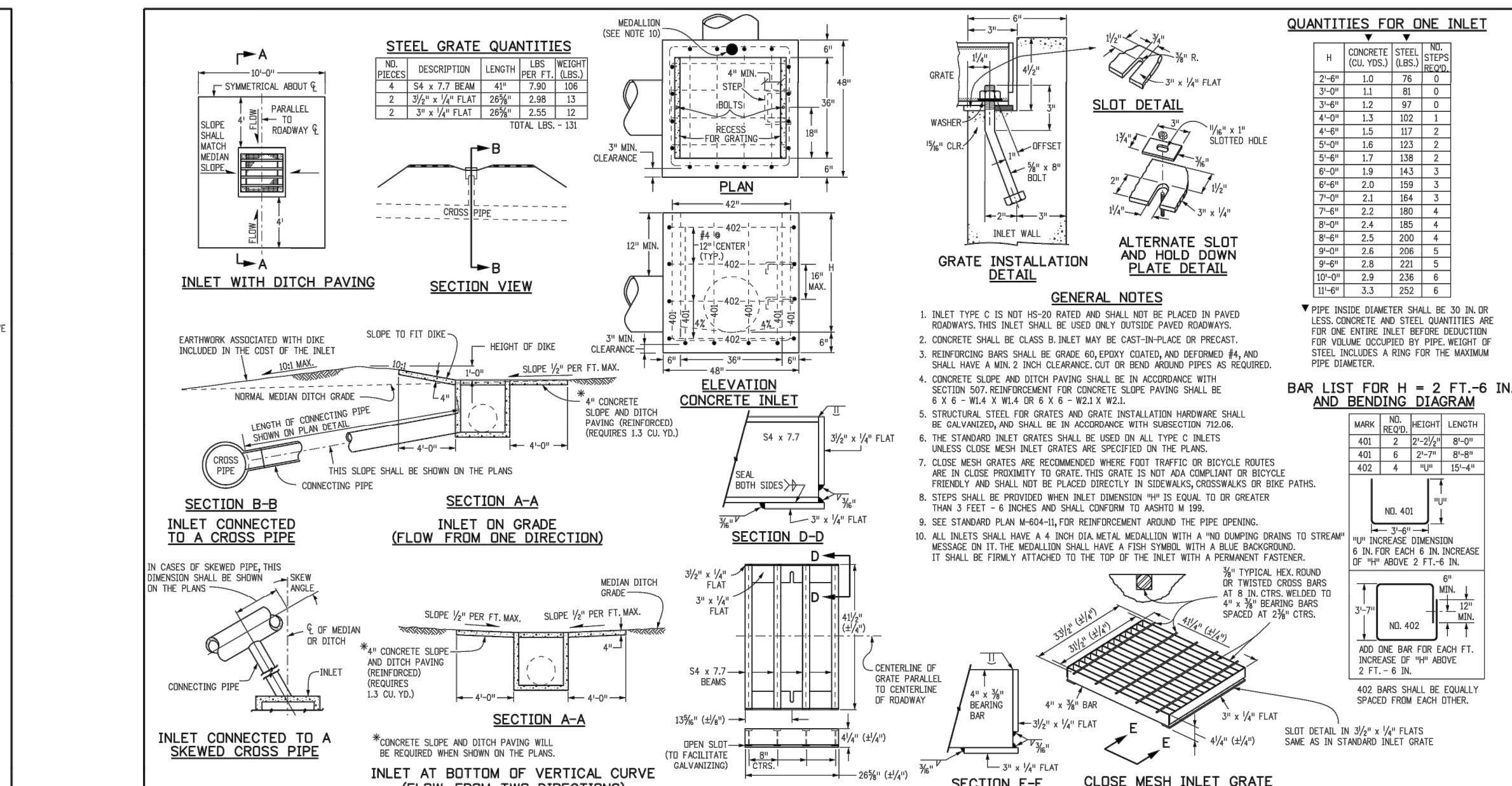
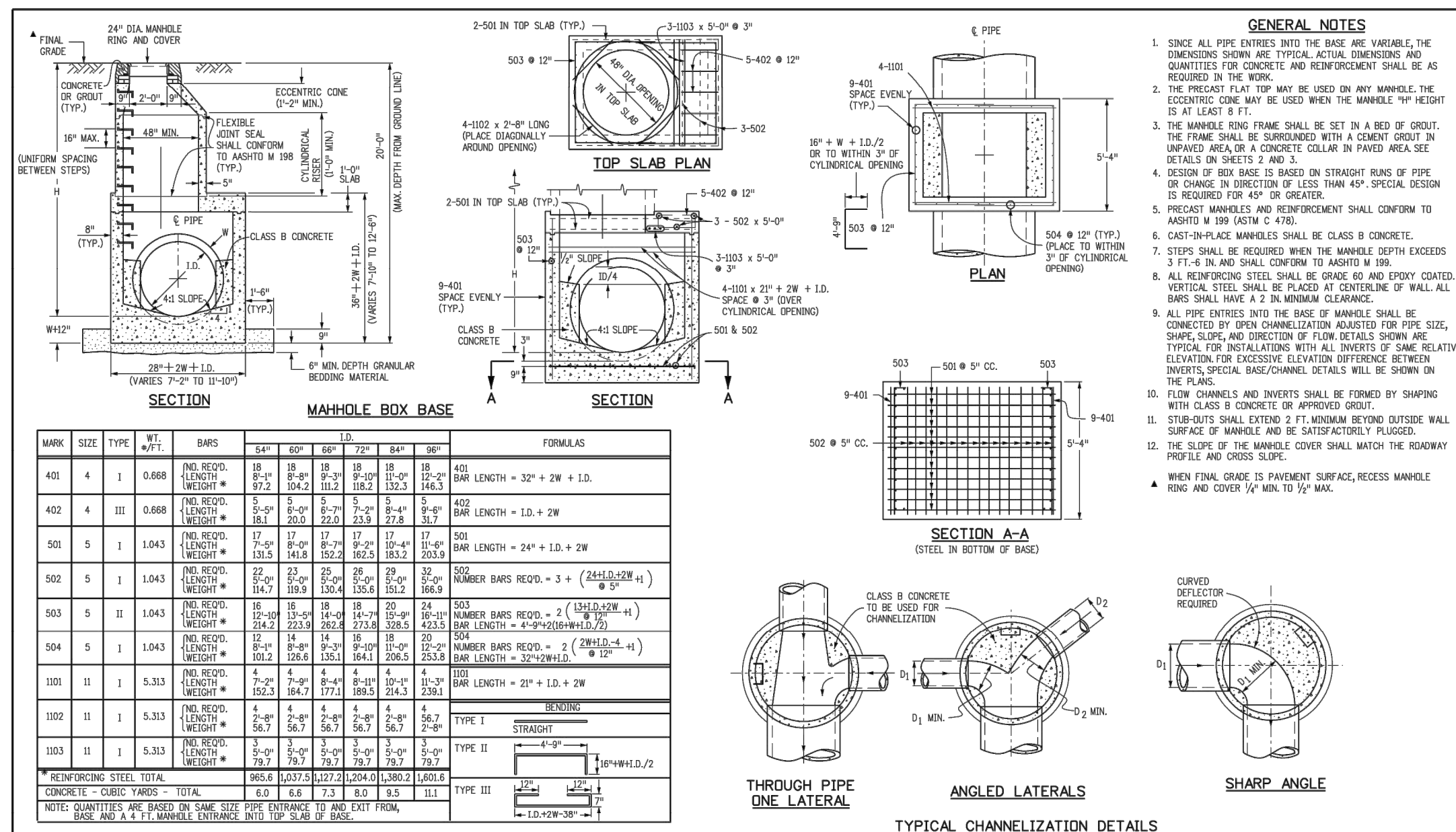
NO.	DESCRIPTION	DATE:
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SCALE: NTS  
DATE: 5/6/2020  
DRAWN BY: WD  
CHECKED BY: JS  
JOB NO: D01173-A

**STORM DRAIN PLAN & PROFILE**

PROJECT NO. D01173-A  
DRAWING NO. 20  
SHEET NO. 20 OF 22 SHEETS

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Computer File Information: M-604-20, Standard Sheet No. 1 of 3

Colorado Department of Transportation: 2029 West Howard Place, Denver, CO 80204

Project Development Branch: JBK

Issued by the Project Development Branch July 31, 2019

Computer File Information: M-604-11, Standard Sheet No. 1 of 1

Colorado Department of Transportation: 2029 West Howard Place, Denver, CO 80204

Project Development Branch: JBK

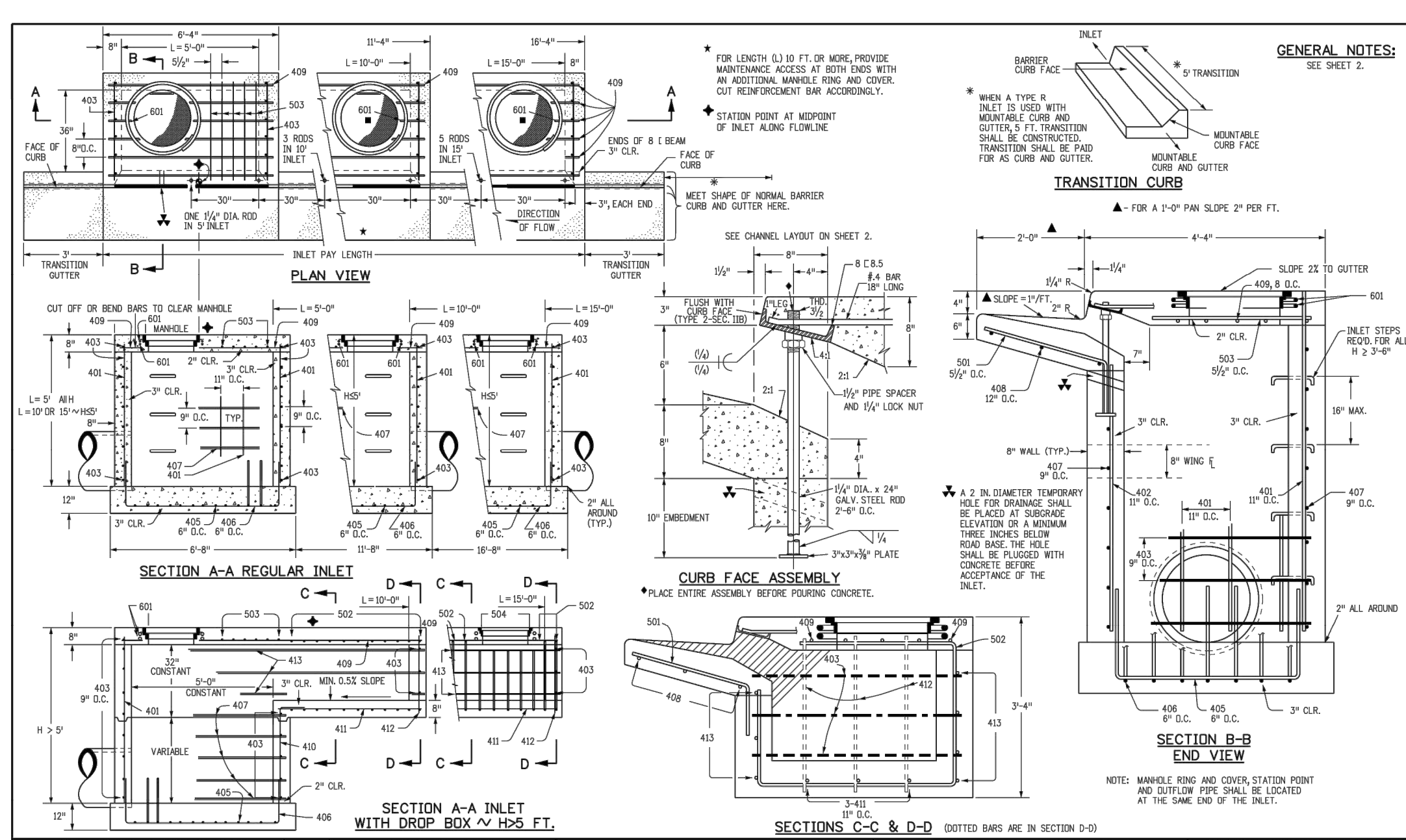
Issued by the Project Development Branch July 31, 2019

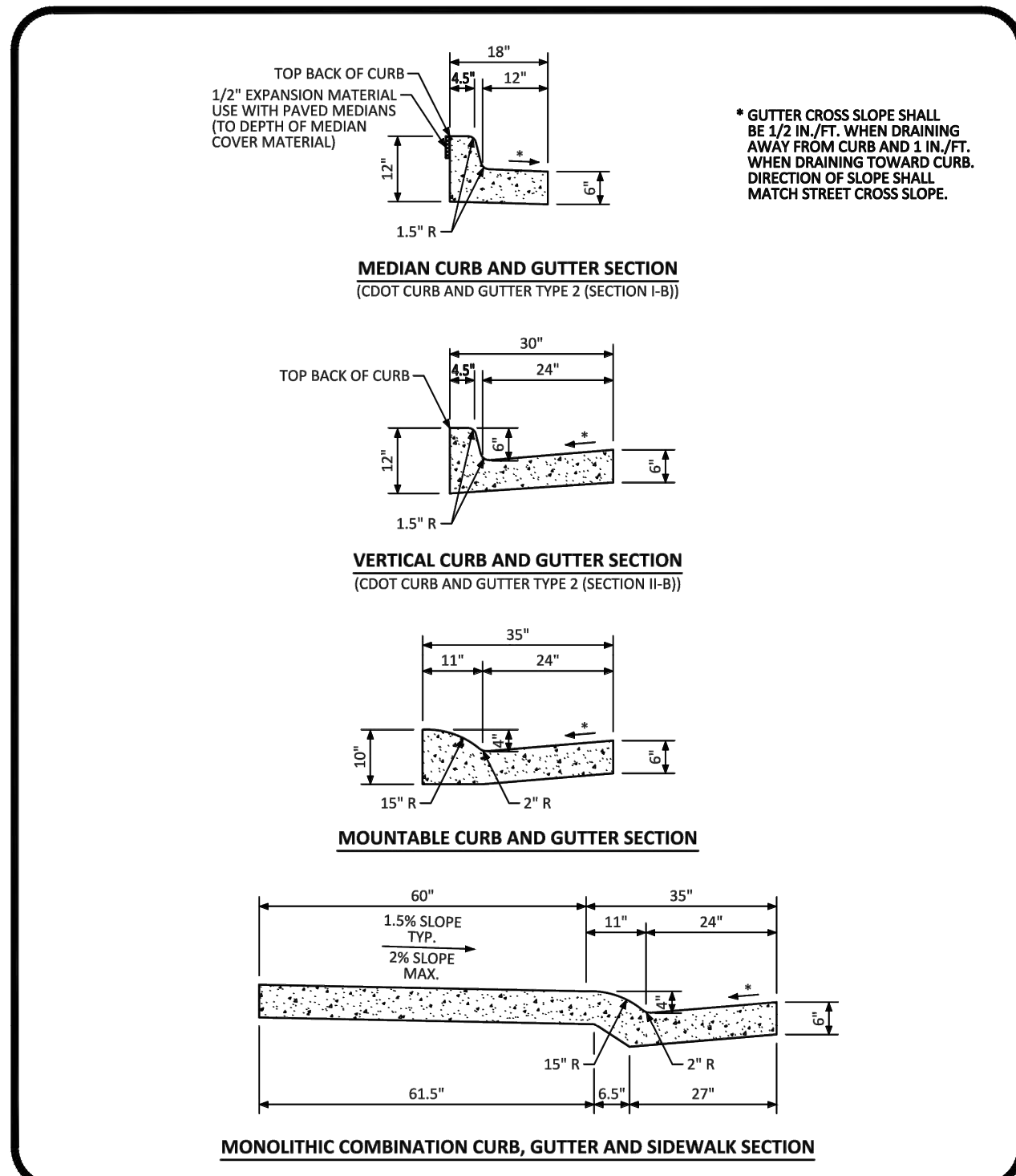
ASPHALT CONNECTION BETWEEN EXISTING AND NEW PAVEMENT STANDARD DETAIL

DATE: DECEMBER 2016

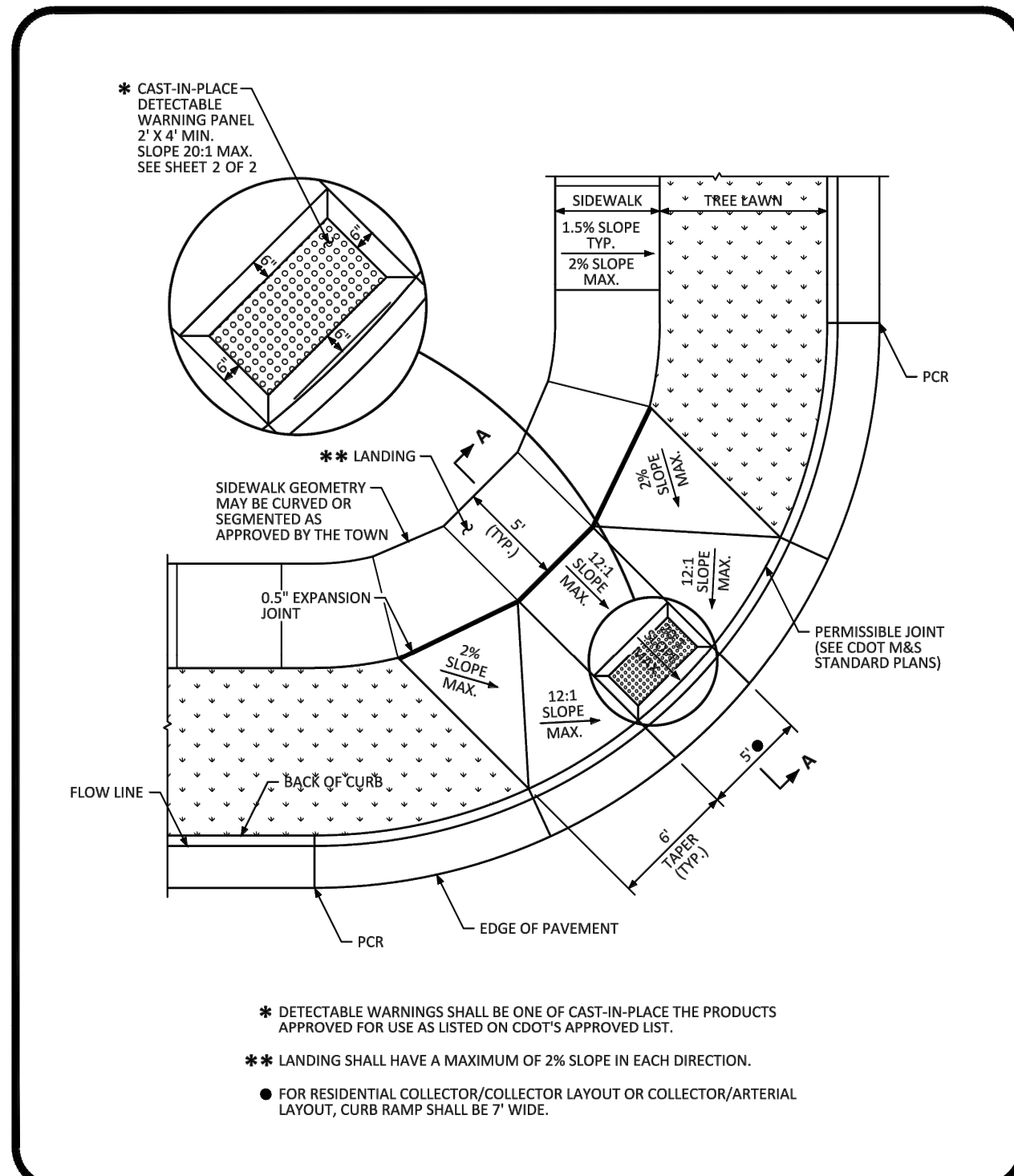
DETAIL: 26

1 OF 1

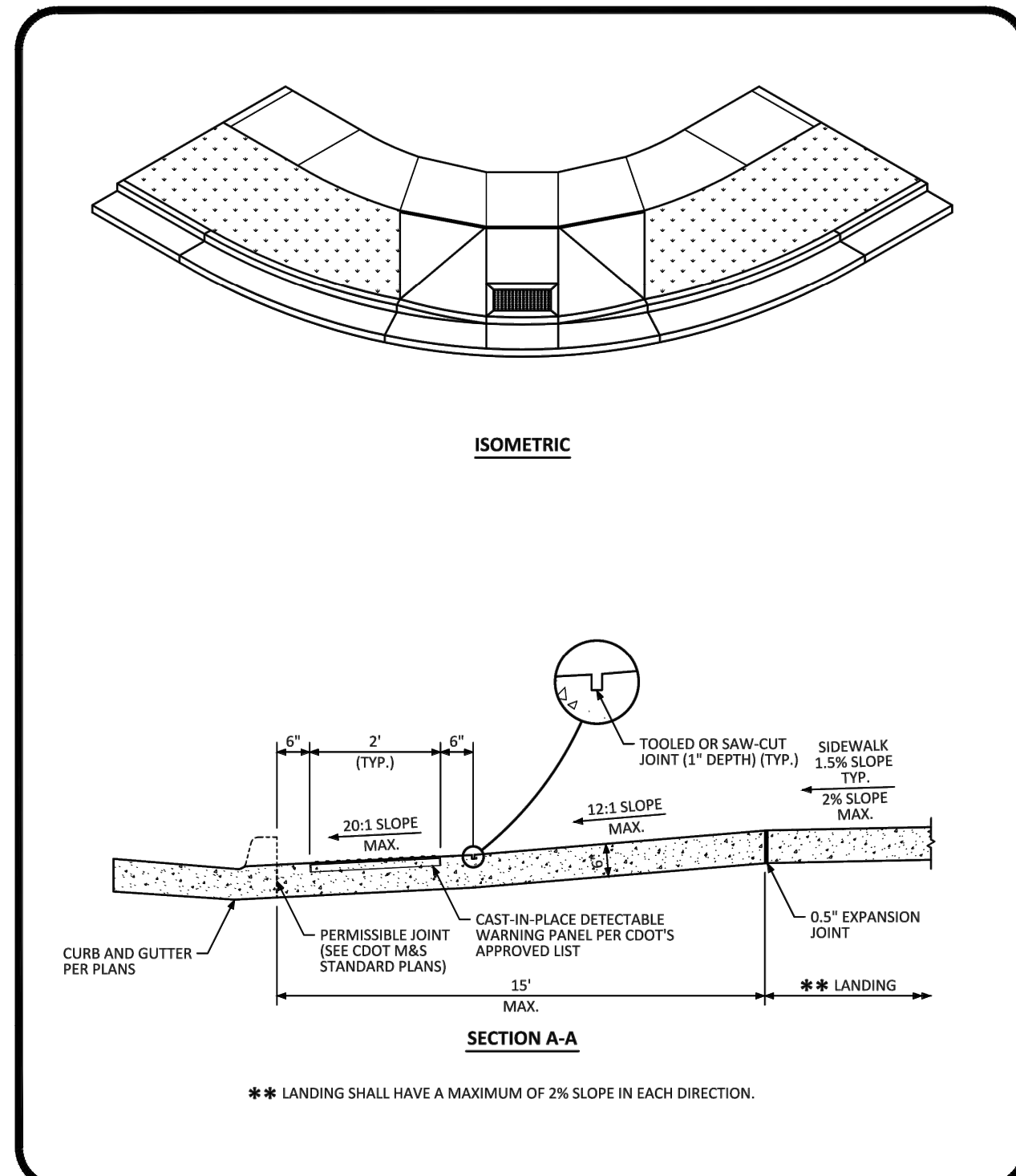




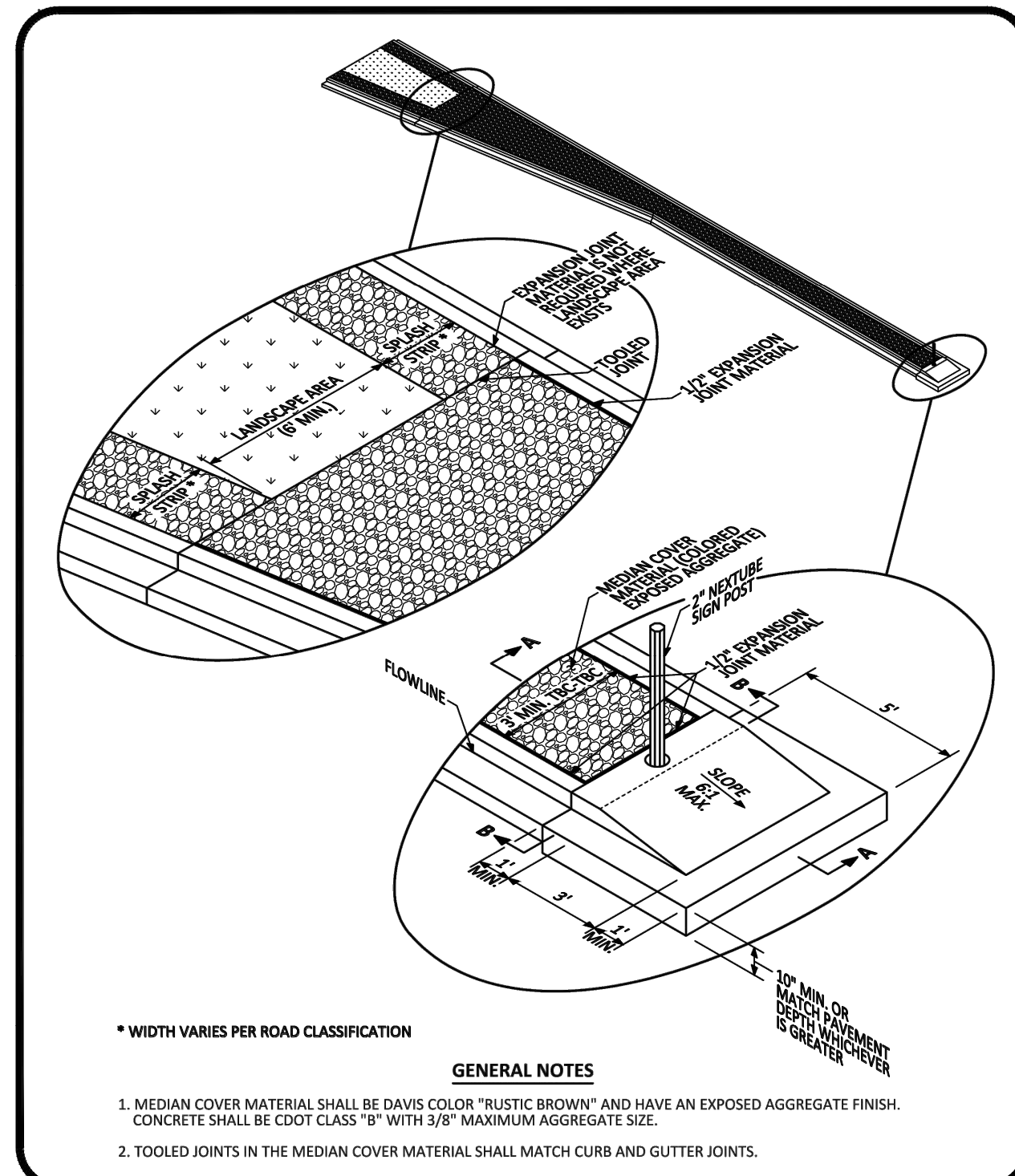
**PARKER COLORADO** CURB, GUTTER, AND SIDEWALK SECTIONS STANDARD DETAIL DATE DECEMBER 2016 DETAIL 3 1 OF 1



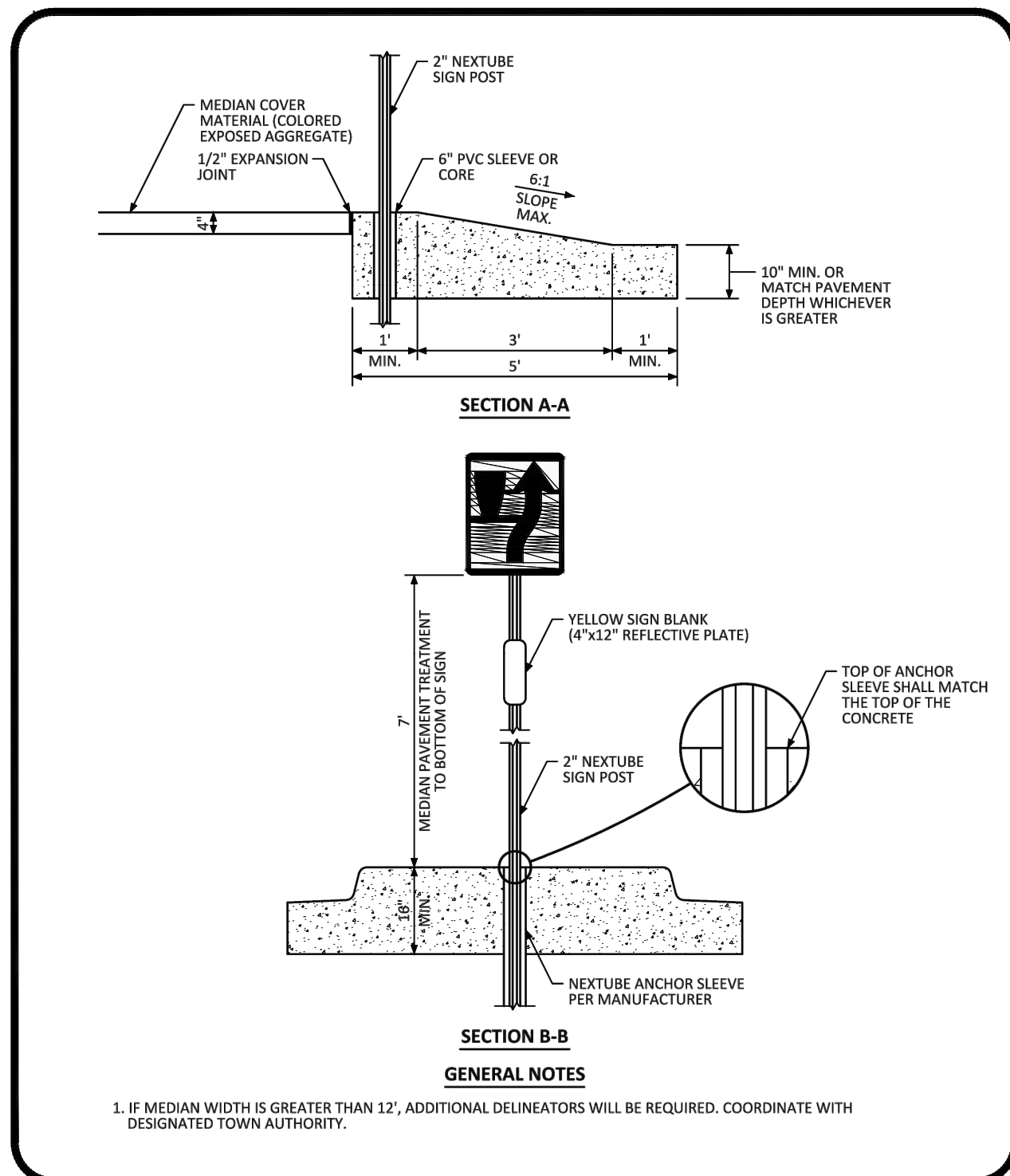
**PARKER COLORADO** DIAGONAL CURB RAMP (DETACHED SIDEWALK) LAYOUT STANDARD DETAIL DATE DECEMBER 2016 DETAIL 15 1 OF 2



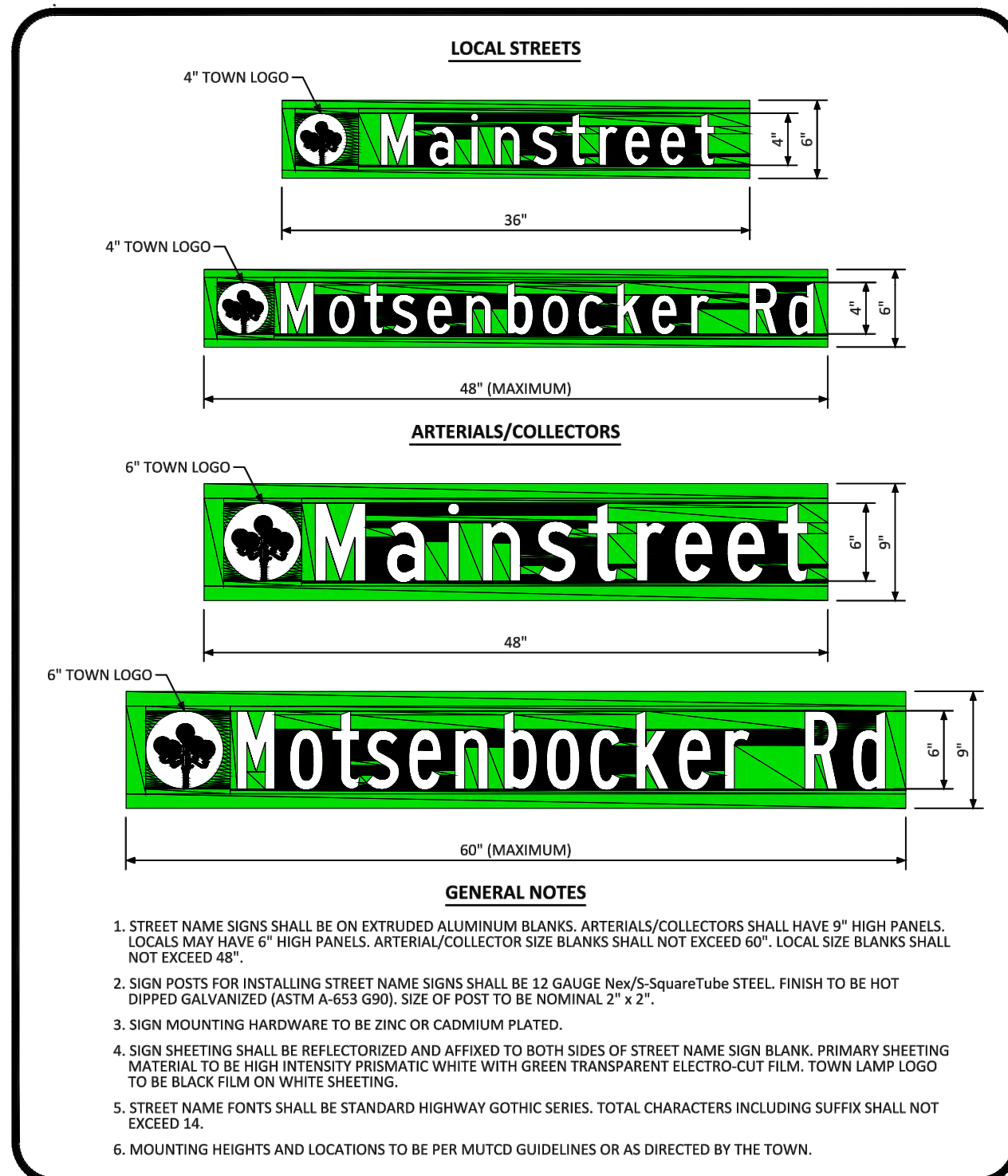
**PARKER COLORADO** DIAGONAL CURB RAMP LAYOUT STANDARD DETAIL DATE DECEMBER 2016 DETAIL 15 2 OF 2



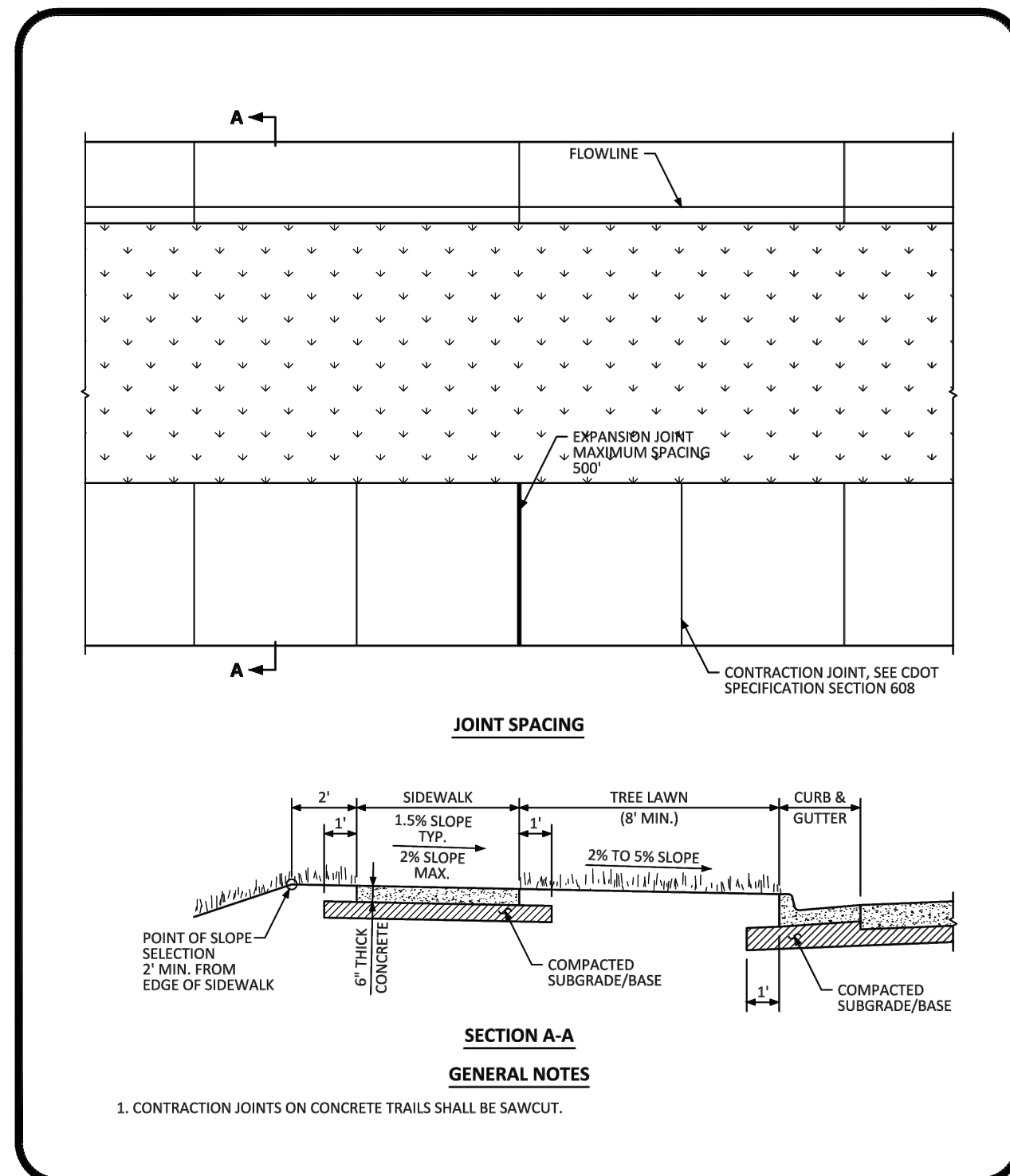
**PARKER COLORADO** MEDIAN BULL NOSE ISOMETRIC STANDARD DETAIL DATE DECEMBER 2016 DETAIL 27 1 OF 2



**PARKER COLORADO** MEDIAN BULL NOSE TYPICAL SECTIONS STANDARD DETAIL DATE DECEMBER 2016 DETAIL 27 2 OF 2



**PARKER COLORADO** GROUND MOUNTED STREET NAME PANELS DATE DECEMBER 2016 DETAIL 32 1 OF 1



**PARKER COLORADO** DETACHED SIDEWALK STANDARD DETAIL DATE DECEMBER 2016 DETAIL 4 1 OF 2

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TOWN OF PARKER, DIRECTOR OF ENGINEERING/PUBLIC WORKS DATE

**ROADWAY & STORM DRAIN PLANS FOR DOUGLAS 234 FILING, NO. 6**  
DOUGLAS COUNTY, COLORADO

**RICK ENGINEERING COMPANY**  
9801 EAST EASTER AVE  
CENTENNIAL, CO 80112  
303.537.8020  
Tucson - San Diego - Riverside - Orange  
Sacramento - San Luis Obispo - Phoenix  
rickengineering.com



REVISIONS:

NO.	DESCRIPTION	DATE:
1		

SCALE: NTS  
DATE: 5/6/2020  
DRAWN BY: WD  
CHECKED BY: JS  
JOB NO: D01173-A

**STANDARD DETAILS**

PROJECT NO. D01173-A  
DRAWING NO. 22  
SHEET NO. 22 OF 22 SHEETS

PRELIMINARY NOT FOR CONSTRUCTION