



Town of Parker Community Development Department  
Development Review Division  
Attention: **Julia Duncan**

Application Type: **Subdivision Plan – Replat**  
Status: **First Referral**  
Application Name: Compark Village South  
Case/AP#: **SUB21-065**  
Referral Received: October 11, 2021  
Comments Due: November 9, 2021

Application Location: Generally located west of Chambers Road on the south side of the future Belford Avenue extension

Review date: **November 8, 2021**  
Plan reviewer: **Randall L. Capra, [rcapra@parkeronline.org](mailto:rcapra@parkeronline.org)**  
Phone: 303.805.3168

**TOWN OF PARKER - FIRE/LIFE SAFETY:**

**C REVIEWED FOR CODE COMPLIANCE w/COMMENTS**

- 1<sup>st</sup> Submittal – Reviewed November 8, 2021, 2021
- 2<sup>nd</sup> Submittal – Reviewed April 7, 2022 (*Comments in red, italicized font*)
- 3<sup>rd</sup> Submittal – Reviewed (**Comments in bold, red, italicized font**)
- 4<sup>th</sup> Submittal – Reviewed (**Comments in bold, red, italicized font with a yellow highlight**)

**Narrative:** The applicant, Century at Compark Village, is proposing a replat to create to establish a buildable lot; the site is located on the south side of future Belford Avenue west of Chambers Road.

**Code Reference:** 2018 International Fire Code, 2018 International Building Code, 2017 NEC

*Note – Effective January 1, 2022, the Town of Parker will have adopted the 2020 NEC and the 2021 International Code Council family of codes i.e. IBC, IFB, IMC, IPC, etc.*

#### UNRESOLVED ISSUES/COMMENTS

*Note – The applicant has not provided the required response to this letter with this submittal; comments will be addressed based upon information provided or noted as a red-lined requirement. Lack of acknowledgment does not exempt applicant from the responsibility of addressing all issues as required below (some comments will be acknowledged based upon the applicant's comments to SP21-118):*

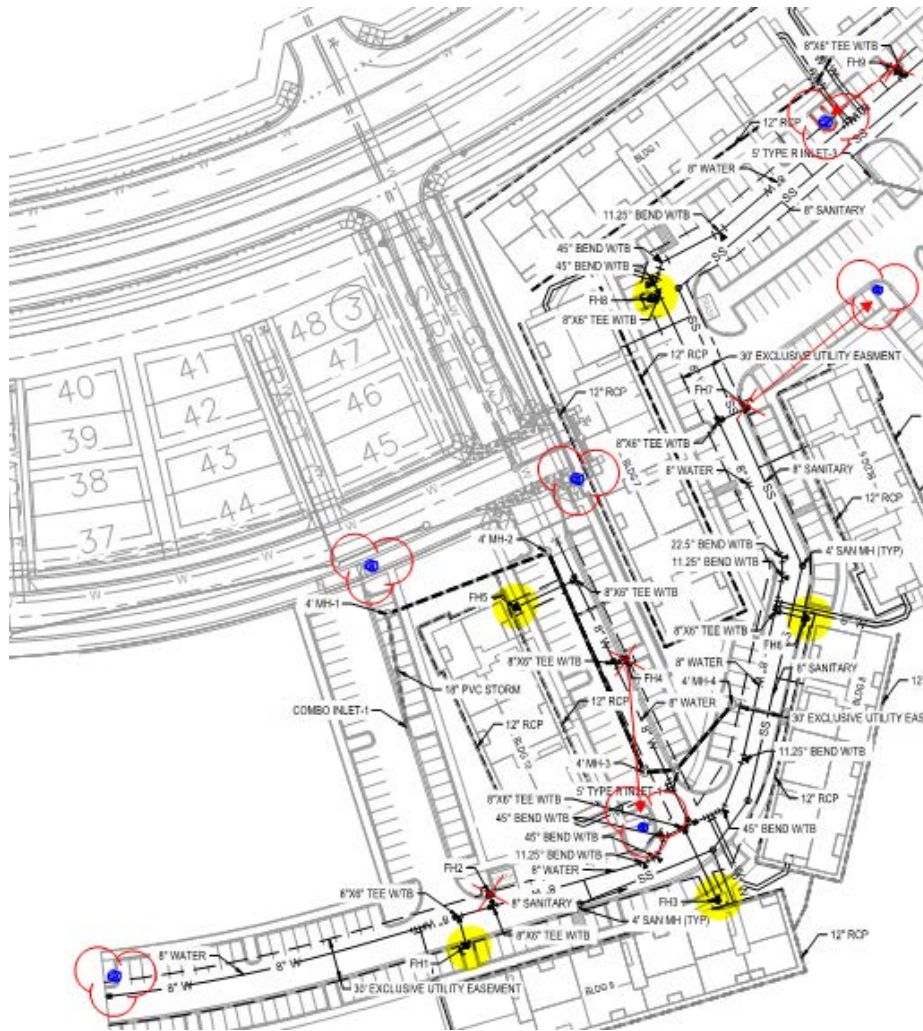
1. The applicant shall be aware that **no vertical construction on this site will be allowed until such time that curb gutter and first lift of asphalt are installed**; a site inspection will be required to ensure that this requirement is made prior to allowing vertical construction to commence. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*
2. The Town of Parker requires that fire apparatus access roads meet the clear-width and weight-bearing standards of the jurisdiction in which the project is located. Roadways shall be designed to support the imposed weight of fire apparatus, 30-ton two axle and 40-ton three axle vehicles. An unimpeded clear width of 20-feet shall be maintained at all times. As such, any location where parked vehicles would obstruct this clear width requirement will require “NO PARKING – FIRE LANE” signage. This signage shall be red on white. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*
3. The applicant shall ensure that all dead-end access roads do not exceed 150 feet (as measured from the center point of the street perpendicular to the dead end) without providing an approved turn-around. Address as applicable. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*

Note – The access road on the west end of the site is shown to have a dead end that exceeds 150 feet (as measured from the center-line perpendicular to the dead end; this dead end will be required to have an approved turn-around (i.e. cul-de-sac, hammer head, etc.) that is constructed of curb and gutter and asphalt (until such time that a future connection can be made at this point. Address this issue when resubmitting.

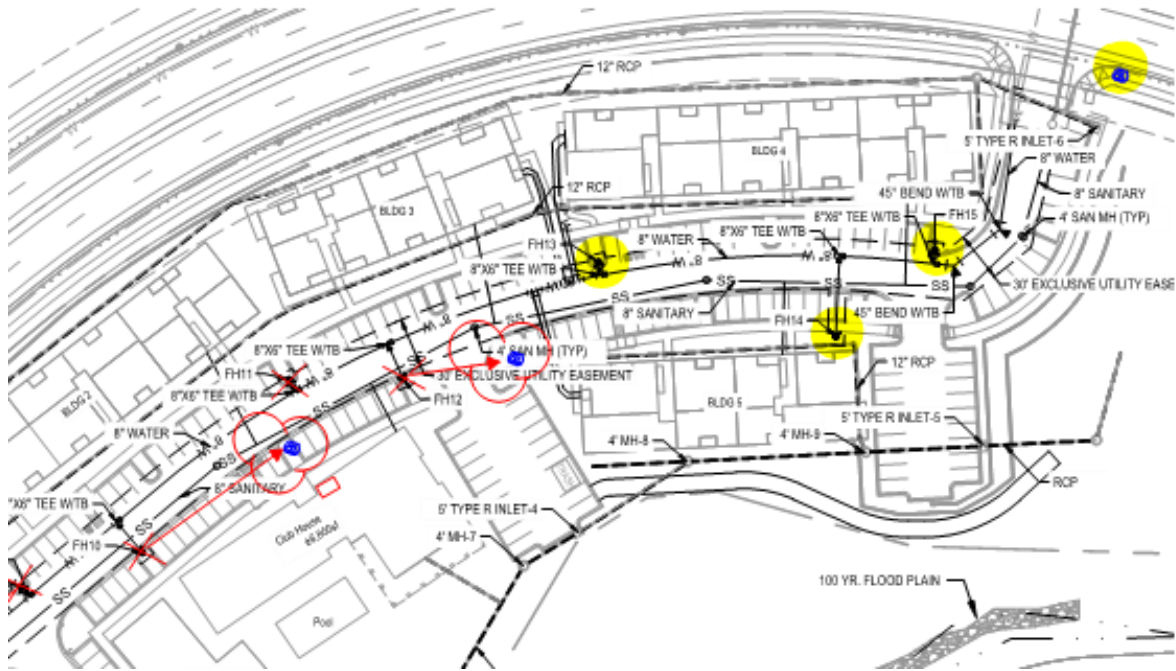
4. The applicant shall provide an auto turn analysis (**for the entire site**) indicating that apparatus can navigate all portions of this site. NO PARKING – FIRE LANE signage is required for any portion of the access where the 20-foot clear width requirement cannot be met for buildings less than 30 feet in height and 26-foot clear width requirement for buildings that exceed 30 feet. This would require that any access road less than 28 feet would require fire lane signage on both sides of the drive and any access road that is 28-feet to 34-feet would require the signage on one side of the drive. Note – all end cap islands would require fire lane signage and the store front portion of the drive as well. *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
5. The builder shall be made aware that vertical construction (either down or up) will not be allowed until the following can be met (inspections by Fire Life Safety for the site will be required prior to going vertical): *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
  - Permanent access is required for all structures; curb, gutter, and first lift of asphalt must be installed
  - All fire hydrants are installed, accepted, and are fully operable
  - All streets signs are installed
  - Addresses are provided/permanently posted for each structure that is to be constructed
  - Dead-end access roads cannot exceed 150 feet without an approved turn-around
6. The builder shall be made aware that no more than 30 units can be under construction without having two points of access provided/completed/accepted. Address this issue when responding. *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
7. The applicant shall be aware that this filing will require that adjacent filings be completed such that a minimum of two points of access are provided when the combination of buildings exceeds 30 residences i.e. two – fifteen unit buildings or one twenty-four unit building (a second building would not be allowed as the number of units would exceed 30 units). *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
8. The riser rooms shall be relocated to the front of the buildings (the riser rooms can be located on the side of the buildings though they will need to be located more curbside as similarly shown for Bldg 3, Bldg 5, Bldg 6, etc.). Address this issue when resubmitting. *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
9. The applicant shall ensure that the fire sprinkler control valve room is sized to accommodate both the fire sprinkler riser and the fire alarm control panel (FACP); the sprinkler riser is allowed to enter no further than 24-inches into the building and a three-foot clearance shall be provided in front (and to the sides) of the riser. The FACP is required to be located on an interior wall and a three-foot clearance is required in front of the FACP. Address this comment when resubmitting ensuring that all buildings meet these criteria. Note – The riser room location identified (and shown with a red box below noted in item #13, Eastern Portion below) shall have exterior access with immediate access into the building... either through the riser room or adjacent to the riser room (this information was also provided at time of the pre-application meeting). *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
10. The Clubhouse is not shown to be sprinklered; this building is required to be sprinklered (per discussions in the preapplication meetings held for this project). *While not acknowledged specific to this application, applicant has noted/acknowledged/addressed on the site plan submittal named SP21-118.*
11. The overall utility plan shall identify each underground fire line (UFL) by name length and size (as measured from the “T” in the access road to the flange within the building); see below for an example and address when resubmitting. **Not satisfied; applicant has indicated that the notations have been provided without providing a plan set to verify. This issue must be addressed prior to sign/stamp of this document by this office.**

12. The utility plans shall have the Fire Life Safety signature block provided on the cover page and the overall utility page. Address this issue when resubmitting. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*
13. The hydrant distribution has been redlined to accommodate response more effectively; the overall utility drawing shall include the hydrant distribution for all filings and the following criteria. See below for information on hydrant location criteria and on revisions to the hydrant layout (hydrants have been deleted, added, and relocated while shown at entry ways if not currently addressed): *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*
- Hydrants shall be located no further than 40 feet of the building to be served.
  - Hydrants serving buildings with fire suppression systems shall be located in the general area of the riser room.
  - Divided roadways require that hydrants be spaced at 250 foot intervals on a stagger (on each side of the road); these hydrants shall be located 500 feet apart on each respective side of the road.
  - Hydrants shall be located at the entry into each sub area.
  - Hydrants shall be located at the end of most cul-de-sacs.
  - Hydrants shall be located at the corners of all streets where possible
  - Hydrant spacing shall meet the requirements of the 2018 IFC Chapter 5 and appendix B and C

**Western Portion:**



**Eastern Portion**



- 14. The applicant shall be aware that storage tanks used for fuel (either for site development or temp heat, etc.) are required to be “permitted” through the building department before being brought out to any construction site. This information shall be disseminated to all construction teams as applicable. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*
- 15. The applicant shall provide an overall site plan that shows the proposed street names; these names shall be approved prior to approval of this application. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*
- 16. The applicant shall provide an address plate with addressing assigned for all lots prior to approval of this application. *While not acknowledged specific to this application, applicant has noted/ acknowledged/addressed on the site plan submittal named SP21-118.*

**GENERAL COMMENTS**

All engineering documents submitted to the Town of Parker shall bear the wet signature and seal of the engineer or architect in responsible charge of the design.

Water distribution and site access components shall be installed and in service prior to the construction of any portion of the structure, except by special permit issued by the Fire District.

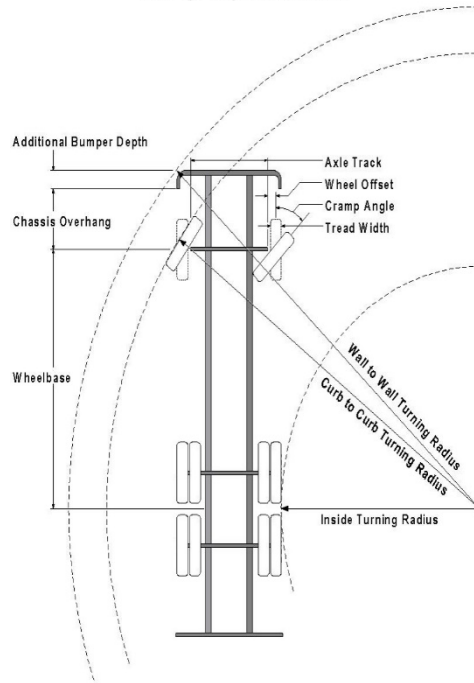
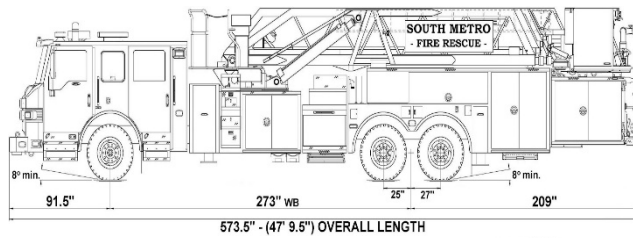
**Underground fire lines**

When thrust blocks are used as part of the pipe restraint system, submitted plans for permit shall provide detailed documentation that the thrust blocks satisfy all requirements of 16 NFPA 24: 10.8.2, including specific thrust block dimensions and mathematical calculations for block dimensions per guidelines provided in Annex A.10.8.2.

**FIRE CODE REVIEW BLOCK**

**FIRE ACCESS ROAD DESIGN CRITERIA**

**VEHICLE SPECIFICATIONS** are provided for the largest apparatus in use by South Metro Fire Rescue. Fire Apparatus Access Roads shall be capable of accommodating this apparatus.



**Vehicle Specifications**

Length: 47' 9.5"

Width: 8' 5" - (10' 1" mirror to mirror)

Height: 10' 9"

Wheelbase: 273 in.

Design load: 80,000 pounds

Inside Cramp Angle: 40°

Axle Track: 83"

Wheel Offset: 5.3"

Tread Width: 13.5"

**Turning Radii:**

Inside Turn: 26 ft. 1 in.

Curb to curb: 41 ft. 11 in.

Wall to wall: 46 ft. 8 in.

**Where objects are present adjacent to the fire apparatus access road, particularly on turns and turn arounds which require backing, a reasonable safety margin shall be provided to prevent potential damage to the property and to the fire apparatus.**

PRIVATE ROADS that provide access to more than two dwellings or one or more commercial buildings shall be constructed to meet the roadway standards approved by the South Metro Fire Rescue Authority for fire apparatus access. Private roads that do not meet the roadway standard may be accepted provided that alternative methods and materials are incorporated into the subdivision that addresses the fire and life safety of the citizens.

**FIRE CODE REVIEW BLOCK**

All fire hydrants shall be installed according to water utility standards. The number and locations of the fire hydrants as shown on the Overall Utility Plan are correct as specified by the Town of Parker, Community Development Department.

\_\_\_\_\_  
Fire Code Official or Designated Representative

\_\_\_\_\_  
Date

(NOTE - Underground Fire Line (UFL) submittal documents must meet the requirements of NFPA 24 when submitting for review.)