



# Victory Crossing

## Design Standards

December 2011



# ACKNOWLEDGEMENTS

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# PROJECT OVERVIEW

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## Part 1.1 Project Overview

### 1.1.1 Project Vision

These design standards have been developed to position Victory Crossing as a desirable location for future economic growth while underscoring the objective to develop the site as a premier sports and entertainment venue within the region. To implement this vision, the Victory Crossing Design Standards sets out the criteria to:

- Develop Victory Crossing as a new and vibrant center for Commerce City.
- Encourage a high quality appearance for the project area while preserving both public and private investment within the development.
- Promote pedestrian-friendly design throughout the project with interconnected sidewalks, plazas and trails.
- Allow for flexibility, individuality, creative and artistic expression in the design process that also accommodates future technological innovations.
- Ensure that development relates appropriately to nearby public streets, neighborhoods and open spaces especially to the Rocky Mountain Arsenal – National Wildlife Refuge.
- Promote and utilize sustainable practices in the design and development process in order to preserve the surrounding natural resources that provides Victory Crossing a desirable setting.

### 1.1.2 Major Influences

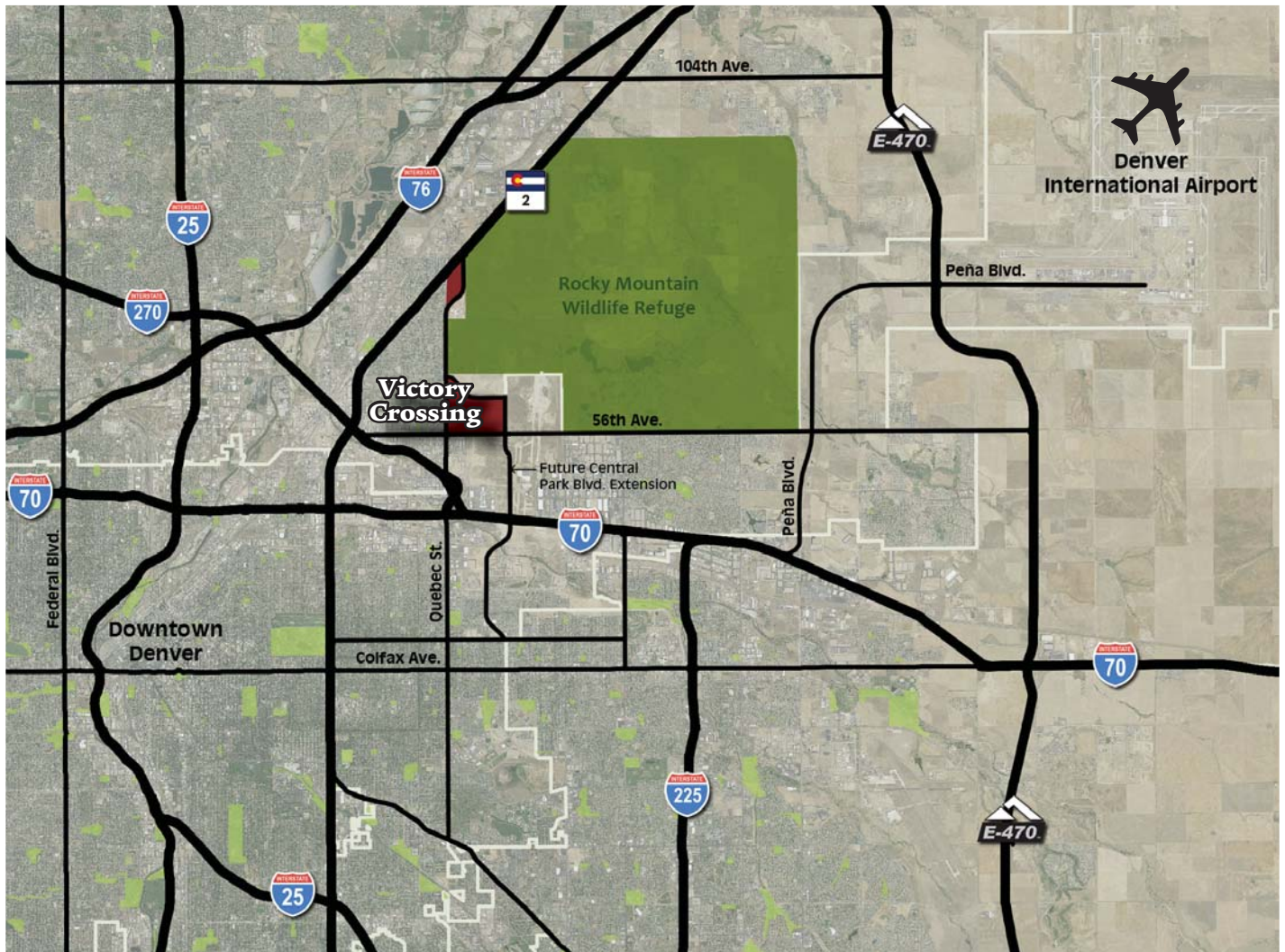
Victory Crossing is favorably positioned in the metro area as a project that will continue to attract significant interest for many years to come. The site is one of the largest remaining infill properties within the E-470 beltway and is well served by both public utilities and transportation. Commerce City chose to locate their new Civic Center in Victory Crossing because of its central location and the development opportunities it will offer in the future. Improvements made to the site, through significant public and private sector investments have prepared Victory Crossing for development today.



# PROJECT OVERVIEW

Victory Crossing is also unique in that it was part of a historic relic of America's wartime past. A 9,000+ acre parcel of land referred to as the Rocky Mountain Arsenal historically contained manufacturing, testing and storage operations for munitions. After much study and remediation by the federal government, a 917 acre portion of this larger parcel was deeded by the U.S. Department of Defense to Commerce City. The remaining 8,000+ acres was dedicated as a wildlife preserve and will forever be protected as open space.

The entire southern and eastern property lines are adjacent to future phases of development within the Stapleton project located in Denver. Regional improvements associated with Stapleton include a new interchange on I-70 leading to a north/south arterial road called Central Park Boulevard. This road will be extended north and ultimately intersecting with Prairie Parkway allowing for quick and easy access to the interstate. Another major road improvement is the expansion and extension of 56th Avenue to an existing interchange on Peña Boulevard providing a more direct connection to Denver International Airport.



### 1.1.2 Purpose

This document is one of many associated with a public/private relationship between Commerce City and the developers of Victory Crossing - Kroenke Sports Enterprises. These design standards are incorporated by reference into the 917 acre\* Planned Unit Development (PUD) Zone Document for Prairie Gateway approved on June 6th, 2005. Amendments to this document will most likely be made from time to time as a response to future market conditions. Notwithstanding, these standards are intended to ensure consistency in the quality, composition and treatment of site, landscape and architectural design.

*\* This area includes the Conservation Areas and open space identified on the PUD Zone Document.*

These standards will serve a framework in the design and planning of Victory Crossing in order to ensure the compatibility and appropriateness of future development with that of the Commerce City Civic Center and Dick's Sporting Goods Park (the Stadium). The design concept of Victory Crossing is not intended to become an isolated development, but to integrate a new and dynamic employment, sports and entertainment center within greater Commerce City. This project also serves to provide retail and professional services for residents and employees in the area as well as for the needs of future population growth.

The standards outlined in this document shall be officially known and cited as the **“Victory Crossing Design Standards,”** although they may be referred to periodically as the “design standards” or simply the “standards”.

## Part 1.2 Relationship to the PUD Zone Document

### 1.2.1 Intent

The Victory Crossing Design Guidelines have been incorporated by reference into the Prairie Gateway Planned Unit Development (PUD) Zone Document approved on June 6th, 2005 and serve to address the physical relationship between future development with adjacent properties, the public realm and the natural environment for a more attractive, efficient, and livable community. Generally this document will:

1. Provide appropriate standards of development to ensure a high quality appearance for Victory Crossing and promote pedestrian-friendly design while also allowing flexibility, individuality, creativity, and artistic expression.
2. Strengthen and protect the image, identity, and unique character of Victory Crossing and thereby to enhance future business opportunities.
3. Protect and enhance the public and private investment including the Commerce City Civic Center and Dick's Sporting Goods Park, by encouraging physical development that is of high quality and is compatible with the character, scale, and function of its surrounding area.
4. Encourage developments that relate well to adjoining public streets, open spaces, and neighborhoods surrounding the Victory Crossing project.
5. Promote sustainable design whenever possible.
6. Ensure connectivity between all the individual projects and uses within the area.

As mentioned previously, this document will inevitably change over time. Victory Crossing is anticipated to be built out over the course of several years and there will be amendments to accommodate future demands caused by a changing market. These design standards will serve as the controlling document over the entire development history of Victory Crossing. The standards contained herein will need to provide both flexibility as well as continuity over time. As such, amendments made to the Victory Crossing Design Standards that do not directly change any specific development requirements of the Prairie Gateway PUD Zone Document, may be considered without being deemed a formal amendment to the zone document itself. This determination will be made by the Director of Community Development, and is outlined in these standards in “Section 4: Implementation Process”.

### 1.2.2 Conflicting Provisions and Relationship with Other Regulations

The Victory Crossing Design Standards will specifically govern the design and development of land described and contained within the Prairie Gateway PUD Zone District. Unless specified otherwise, the design standards replace, augment or modify the Commerce City’s policies set forth in the Land Development Code, as amended. In addition to the requirements of these standards, Victory Crossing shall comply with other applicable City land development regulations, ordinances, and requirements except as noted herein. When the provisions of these design standards are inconsistent with provisions found in other parts of the Land Development Code, or in any other City ordinance or regulation, these standards shall govern. On matters where these standards are silent, the Land Development Code, specifically Article VII, Sections 21-7650 through 21-7656 – Commercial Design Standards shall prevail.



## Part 1.3 Establishment of Design Districts

### 1.3.1 Introduction

The overall master plan for Victory Crossing is based on defining specific areas of development or Design Districts within the project area. The plan identifies six districts, each of which has specific characteristics based on location, visibility and use. This document is organized around each of these unique areas of development. In addition to the individual district definitions, there are site development standards such as landscape, wayfinding and lighting common to the overall project. The six Design Districts are referred to as:

- the Soccer District
- the Victory Plaza District
- the Merchant District
- the Central Park Boulevard District
- the Prairie Parkway District
- the North Gateway District

The unique characteristics of each of these areas are identified within their respective Sections. Each district serves a specific function to the overall development of Victory Crossing.

### 1.3.2 District Map



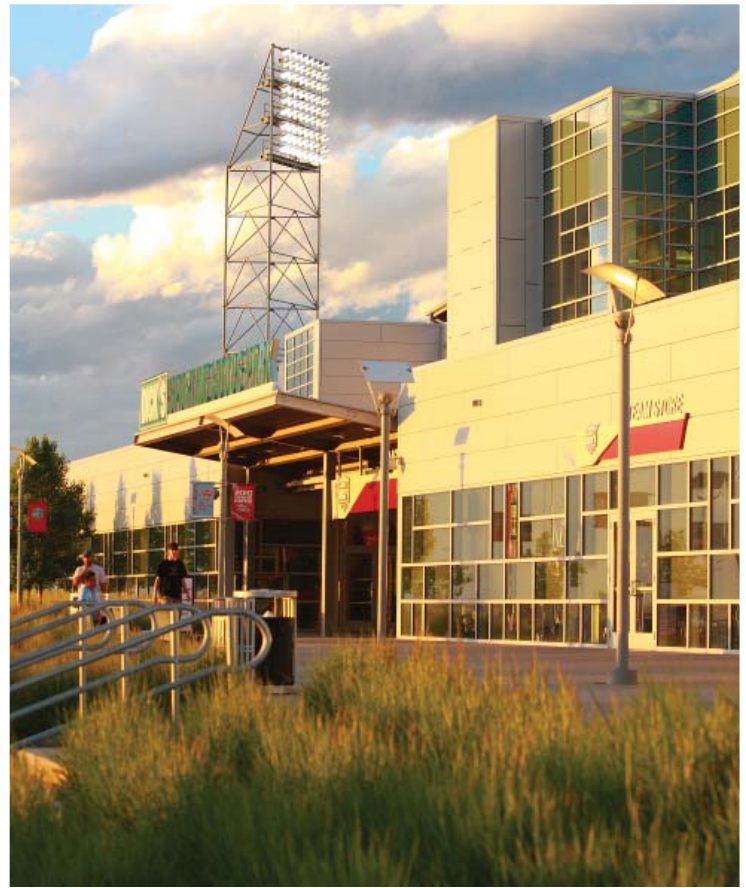
### **1.3.3 Interpretation of District Boundaries**

The boundaries shown on the District Map should not be considered strict definitions between each area. The lines separating individual districts should be considered transitional “zones” that may share certain characteristics as well as uses. If a question is raised in the development process as to which district standard is applicable, an interpretation will be made by the Director of Community Development.



# DESIGN DISTRICTS

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# DESIGN DISTRICTS

## Part 2.1 Soccer District

### 2.1.1 Introduction

The stadium for Dick's Sporting Goods Park (DSG Park) has become a prominent visual icon for sporting events, entertainment, and youth soccer in the region. The location of the stadium has been well thought out in terms of a central location, centralized parking and easy access. The area surrounding the stadium complex has been designed and built to support the events taking place inside. Pedestrian walkways have been designed to radiate from the central stadium location and connect the stadium with recreational soccer fields and event parking.

The field complex located outside of the stadium creates a green foreground for Dicks Sporting Goods Park and provides a unique opportunity for soccer and other field sports in Commerce City and the region. The concept includes four complexes or pods containing six fields (24 in total) located east and south of the Stadium. While there are some similarities between the accessory structures within the field complex, each pod has been designed to have its own distinctive character. These individual characteristics are built into the designs through physical relationships to the stadium, graphics and signage along with individual pod sponsors.

## 2.1.2 Site Planning

### 1. Orientation to the street:

Building facades within this district create a defined edge parallel to the adjacent street. Special emphasis has been placed on the pedestrian right-of-way. These areas have been designed to have a minimum of 20 feet with tree lawns, hardscapes, pedestrian lighting, and other site planning strategies in order to create a buffer between the sidewalk and street. An urban atmosphere is desired with ample sidewalk space to accommodate pedestrian circulation and ancillary outdoor events.

### 2. Plaza Space:

Two significant plaza spaces have been identified within this district. The first plaza space is located where 60th Avenue terminates into Dicks Sporting Goods Park. This area becomes the “front door” of the Stadium District as it passes through the pedestrian corridor along 60th Avenue. The second plaza space is found at the southern entry into the stadium at Valentia Street within Victory Crossing. These plaza areas extend into the surrounding design districts and should be considered as a transitional space connecting with the commercial and retail uses found throughout the project.

### 3. Pedestrian Connectivity:

- i. Future development surrounding Dick’s Sporting Goods Park shall provide and enhance pedestrian connectivity to the stadium.
- ii. Crosswalks at major vehicular intersections must be defined as a pedestrian crossing with a striped crosswalk on the road surface.
- iii. Important views of Dick’s sporting Goods Park orienting pedestrians to the stadium should be defined and developed throughout the site.
- iv. Whenever possible, utilize shared parking between adjacent buildings and the stadium to reduce the amount of parking stalls required for the development.
- v. Create adequate pedestrian connections to future transit locations, regional bike paths and greenways.

- vi. The landscape associated with parking lots should be designed allowing for the parking surfaces to be utilized for a number of activities. In order to serve multiple functions, the landscape area surrounding parking fields should be designed to be durable enough to accommodate active pedestrian related uses.





### 2.1.3 Stadium Architecture

#### 1. Design Intent:

Dick's Sporting Goods Park has been designed and built to have a major impact on the surrounding development. Its form provides both pedestrian scale while at the same time establishes an iconic presence as the visual anchor for the entire region. The architecture of the stadium has raised the level of expectations for future development in the surrounding area. It has been designed to energize the Victory Crossing project as well as celebrate the sport of soccer. Innovative use of materials, lighting, and architectural scale are used to capture the excitement that surrounds professional soccer and other sporting events planned for the stadium and on adjacent fields. Similarly, new development surrounding the stadium should also relate to the overall theme of active outdoor experiences and lifestyles.

#### 2. Pedestrian Scale adjoining the Stadium

Proposed buildings surrounding the stadium, particularly in those locations next to major entrances, should be scaled appropriately to provide for pedestrian comfort and convenience.

#### 3. Building Articulation & Massing

Because Dick's Sporting Goods Park stadium is the most predominate feature within the Victory Crossing project, future development – even that within surrounding Design Districts - must somehow relate to this important structure. The following information provides the standards applied in the development of the stadium and should offer insight into the design of future projects within the vicinity.

- i. Long lengths of elevation were broken down with changes of materials, variation of parapet height, fenestration, and building undulation. No single run of façade has exceeded 110' without a change of at least one of the previously discussed techniques to break down the massing.
- ii. All facades have a visual interest with no elevation treated as a “back of building”. All buildings have articulated facades and 360 degree architecture.
- iii A partial canopy has been provided over the seating bowl. This structure was designed to represent the mountain backdrop along the Front Range while providing visual interest to the stadium. The form of the canopy creates an overall design theme for the stadium structure. Sensitivity to these same mountain views from the interior of the stadium have been incorporated into the design. While it is a visually dominate structure, the stadium was built to provide the necessary cover as well as maintaining an experience of openness important to the Colorado lifestyle. Sustainable design principals were incorporated into the canopy development to capture prevailing breezes, preserve views, enhance solar protection, and to help mitigate noise.

#### 4. Building Materials:

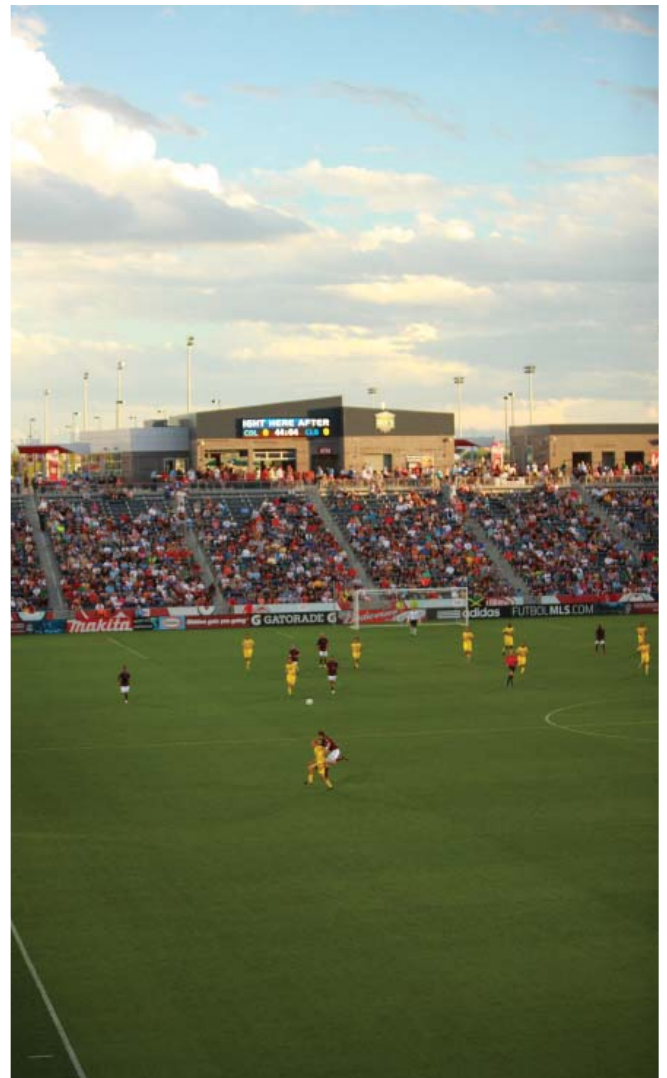
- i. Building materials used in the construction of the stadium set a general precedent for the rest of the Victory Crossing project. Materials used in the design and development of the stadium have included:
  - Ground face or split face concrete masonry units (CMU)
  - Brick veneer
  - Aluminum composite panels
  - Steel, copper, zinc panels – corrugated, profile, flat seam or composite panels.
  - Terracotta veneer
  - Natural or synthetic stone veneer
  - Fabric awnings
  - Steel or wood trellises
  - Heavy timber or glue laminated wood products
  - Natural finish wood siding
  - Metal standing seam, or built up roofing
  - Stucco
- ii. Early in the design process several building materials were determined to be inappropriate for a visually prominent building such as the stadium. It was felt that these materials did not meet the standards required to create a timeless and permanent structure reflecting the significant investment made by Kroenke Sports. Materials that were intentionally not used for the stadium are as follows:
  - Cement plank or vinyl siding or panels
  - E.I.F.S.
  - Painted masonry units

#### 5. Building Heights:

The stadium has been designed and built to be one of the tallest structures within Victory Crossing. The height of the stadium structure represents its importance not only to Victory Crossing but to the region itself. The overall height limits of the structure are:

- i. Stadium seating (seating bowl) – built at a maximum height of 140'

- ii. Canopy structure – maximum height of 140'
- iii. Electronic signage – maximum height of 80' above service level located north of the stadium building
- iv. Stadium lighting – maximum height of 140'
- v. Entrance structures – 50'



### 2.1.4 Field Complex Design

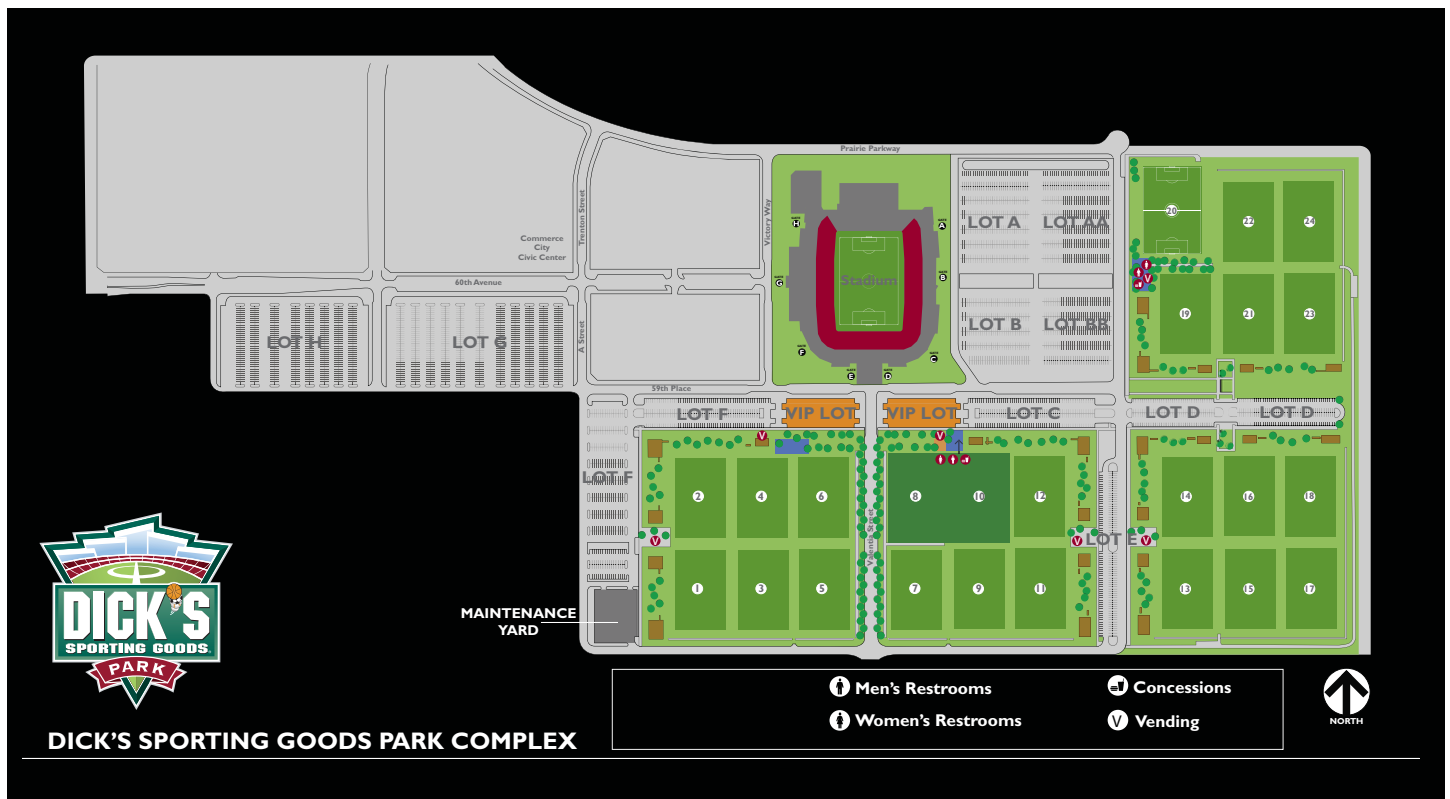
One of the key aspects of the field complex design is the large rectilinear landforms framed by large, bermed separations between pods. These distinctive landforms compliment the ½ mile length of the site and maintain axial relationships to the stadium while preserving views to the Front Range from the stadium. More than a large cluster of playing fields, the use of these distinctive landforms and the accompanying structures provides a dramatic foreground surrounding Dick's Sporting Goods Park.

#### a) Field Complex Fencing:

From time to time perimeter fencing within the field complex will need replacing. Fencing standards currently in place include a 10' black vinyl chain link for the areas located around the Rapid's practice field and maintenance areas. This fencing may include windscreen that is black, dark green or appropriate team colors. The windscreen may include graphics of various colors and/or advertising. Climbing varieties of plant material may also be planted at the base of the fence to provide a natural screen.

### 2.1.5 Noise Mitigation

Careful planning has gone into reducing the possibility of noise impact to the surrounding areas. The location of the Stadium Parcel was chosen to maximize the distance from the nearest neighborhoods, and the decision was made to add a canopy roof structure to possibly help reduce noise emanating from the stadium proper. In addition to this, a hotline number has been established to facilitate open communication with the surrounding residents and help mitigate the possibility of noise impact from the stadium. Hotline calls and complaints shall be investigated and recorded so that areas which generate frequent complaints can be tracked and monitored during future events and so that modifications can be made as needed.



## Part 2.2 Victory Plaza District

### 2.2.1 Introduction

The Victory Plaza District is centered along the 60th Avenue corridor and provides for a vibrant and pedestrian oriented city center within Victory Crossing. The district will include a broad mix of uses ranging from the municipal complex for Commerce City and indoor sporting facilities to hotels, retail, restaurants and office development. 60th Avenue serves both as a promenade and gateway to Dick's Sporting Goods Park. As a central axis connecting Quebec Street to the stadium, it creates opportunities for many public uses from festival and celebration space to outdoor markets.

### 2.2.2 Building Scale and Composition

1. All building fronts must be three-dimensional in form and shall have interesting modulated storefront planes. Projections or recesses may occur at any point along the storefront, provided there are appropriate and creatively detailed transitions between buildings.
2. New buildings and projects located along 60th Avenue shall orient towards the street in order to provide continuity for the existing pedestrian environment. The road is designed and built to accommodate a safe and comfortable pedestrian environment with wide sidewalks and traffic calming techniques.
3. Where ever possible, outdoor seating and dining is strongly encouraged within this district.
4. The stadium at Dick's Sporting Goods Park is the most prominent structure in Victory Crossing and will remain as the central focus of the district. It also serves as the eastern terminus of 60th Avenue from Quebec Street. Because of its importance, the placement of any building or structure on 60th Avenue shall not detract, obscure, or hide the stadium building.

5. Building designs shall incorporate the following:
  - i. Front building elevations shall have a strong presence of form and substance of materials
  - ii. Dynamic entrances, including such elements as columns, arches, and pediments, are encouraged
  - iii. Materials shall be employed in innovative and creative ways for dramatic effect
  - iv. Materials shall be reliable and durable natural products such as metal, wood and stone
  - v. Awnings, arcades and canopies of a variety of types, designs and colors are encouraged
  - vi. Unique and creative detailing of building features, such as raised panels, moldings, coffers, etc.
  - vii. Entries should be recessed or otherwise defined to create visual interest along the street.
  - viii. Flags and banners are encouraged along 60th Avenue.
6. Discouraged building types:
  - i. Drive-thru facilities are strongly discouraged and may only be considered for bank buildings and certain municipal functions. When a drive-thru is considered in this district, the canopy structure and queuing lanes shall not be visible from 60th Avenue.
  - ii. Franchise architecture is strongly discouraged within this district. Architectural design shall be considered unique to the Victory Plaza District and not utilize "off-the-shelf" design forms found elsewhere.



### 2.2.3 Pedestrian and Vehicular Connectivity

Victory Crossing has been designed to provide a pedestrian-friendly environment. Special attention has been paid to the design of plazas and walkways with the intention of focusing pedestrian traffic along 60th Avenue and providing safe and convenient non-vehicular linkages between the Soccer District and all locations within the project area.

1. Within individual building sites, safe and convenient pedestrian and bikeway systems shall be integrated with vehicular traffic to directly connect buildings, parking areas, open space, transit stops, services, on-site amenities, and other areas of interest. Street systems will be designed to accommodate pedestrians, bicycles as well as vehicles by providing safe access via entry streets and driveways, circulation and connectivity through internal streets and blocks, connections from internal streets to external streets, and drive aisles for safe and efficient access to parking areas.
2. All development within the Victory Plaza District shall provide and contribute to an on-site system of pedestrian walkways, sidewalks, and bikeways integrated with vehicular traffic to provide continuous access to all land uses within Victory Crossing and to land uses on adjacent properties.
  - i. Cross access pedestrian easements may be required whenever necessary to ensure that adjacent parcels have adequate access to accommodate existing or future ownership or development patterns.
  - ii. Safe and convenient bicycle and pedestrian access from the development site will be provided to existing and designated public bike paths or greenways located on or adjacent to the development site.

3. Each development shall provide an on-site system of pedestrian walkways and/or public sidewalks throughout the District. The on-site pedestrian circulation system shall provide efficient access routes between the intended points of travel. Specifically, onsite pedestrian connections shall be provided to and between the following points:

- i. The primary entrance or entrances to each building housing a principal use
- ii. Existing or planned bus/transit stops, and “park and ride” locations
- iii. Greenways and trail systems
- iv. On-site amenities

### 2.2.4 Build-to Lines along 60th Avenue

The intent of a build-to line is to pull the building façade close to the street and street-side sidewalk. Primary building façades along the street will be aligned to create a wall that defines the public realm while retaining sufficient width for pedestrians and hardscapes. The street edge finishes the public realm by providing opportunities for traffic calming and pedestrian safety. The build-to line listed below allows the buildings to be pulled up to the street sidewalk and provides for a more comfortable scale while providing for canopies, arcades and plazas which create shade and shield the pedestrian from the elements. Projecting signs, balconies, arcades, awnings and outdoor seating may extend closer to the street provided that a minimum of six feet of vertically and horizontally unobstructed space is preserved. Where primary façades are located along private roads or driveways, the build-to line will be measured from the edge of curb.

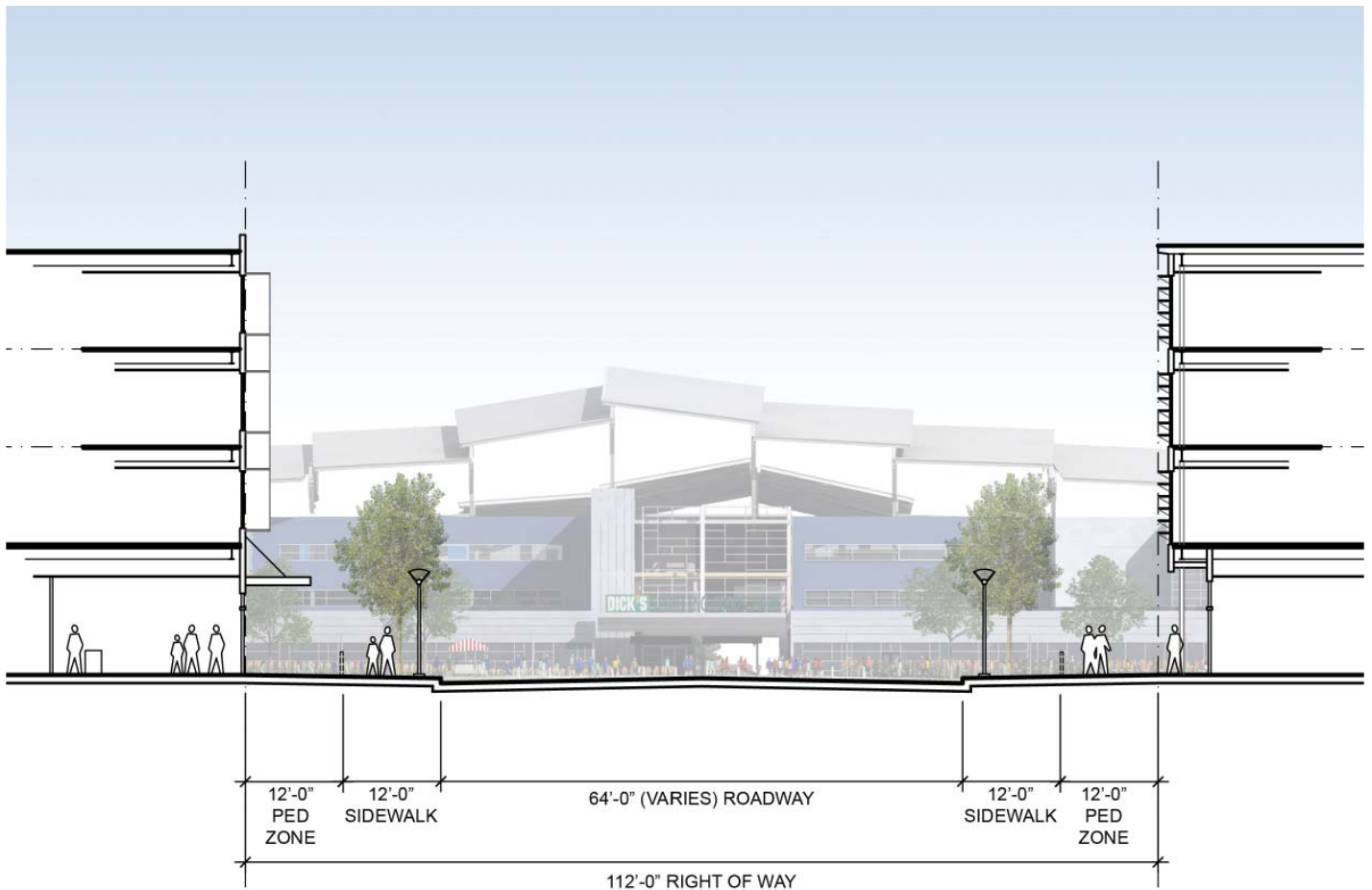
1. Build-to lines for 60th Avenue
  - i. Front - 0 to 5 feet
  - ii. Side/Rear – 10 to 24 feet

### 2.2.5 60th Avenue Pedestrian Zone and Street Sections

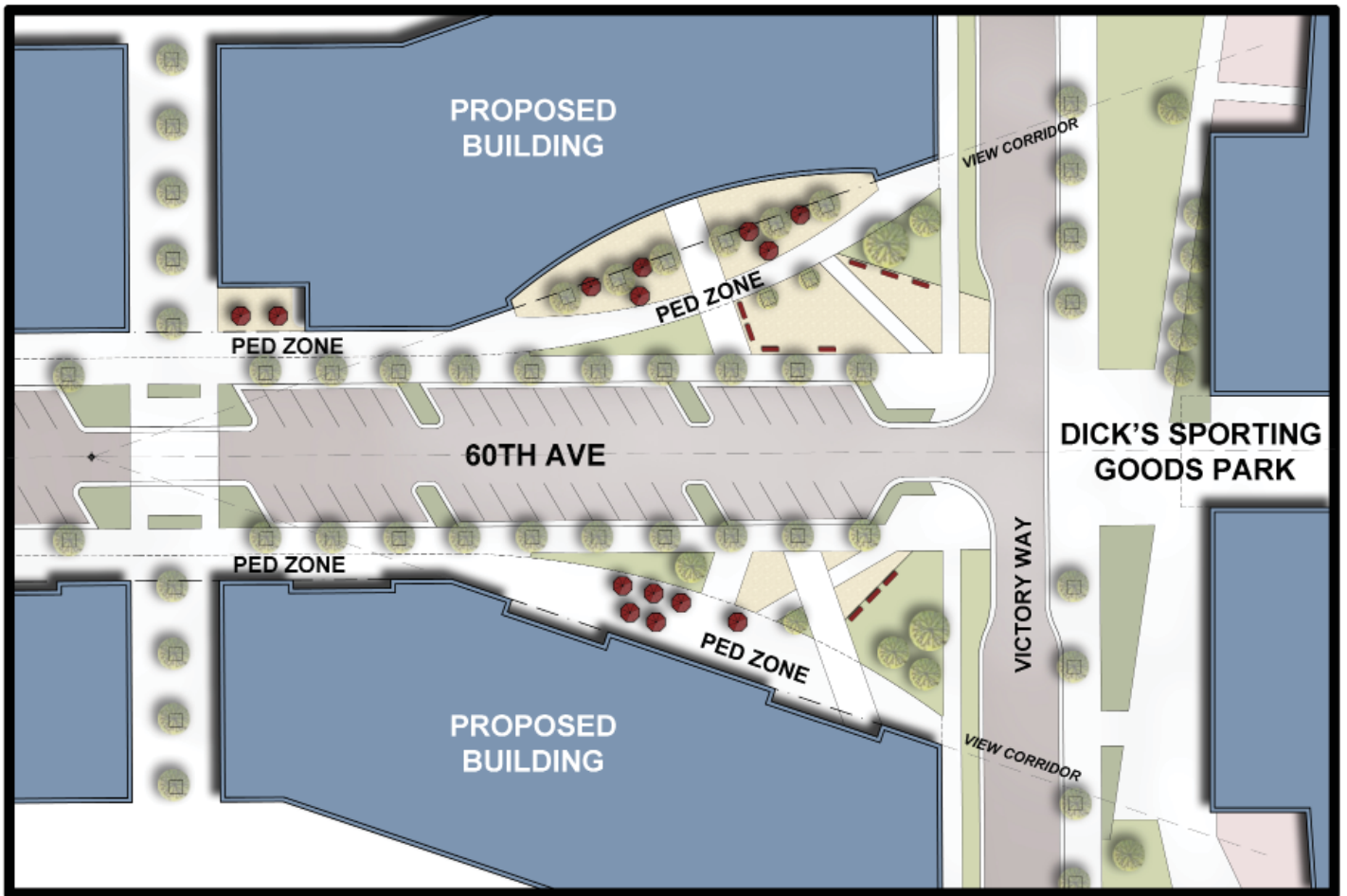
A pedestrian zone is established adjacent to 60th Avenue supporting various uses that serve to activate the public space between the street and the building façade. Uses such as restaurants, pubs, cafés and coffee shops are highly desirable and are strongly encouraged within this zone especially when coupled with outdoor seating and interesting retail displays.

The design of outdoor dining areas will be considered on a case-by-case basis. When adjacent to a sidewalk, plaza space or public right-of-way, a decorative fence or landscape feature will be required to separate the seating area from the pedestrian corridor within the right-of-way.

1. Whenever possible, paving within the Pedestrian Zone should be different in terms of color or material from that of the adjacent sidewalk.
2. Front setbacks within or adjacent to the Pedestrian Zone should contain elements of landscaping. These may be in-ground plantings, planter boxes or decorative pots as long as they don't obstruct access to adjoining structures or public sidewalks.
3. Public art that depicts and supports Victory Crossing as a sports and entertainment venue is encouraged to be located within plaza areas along 60th Avenue.



## 2.2.6 Victory Plaza Design



*Note: This graphic is conceptual in nature and not meant to represent an approved plan or plan element.*

## 2.2.7 Materials and Details

1. Victory Crossing requires high-quality materials, well-designed details and well-executed construction. The success of storefront design is largely dependent on how creatively the materials and design details are used to develop a unique and well executed image for each building. The following are minimum standards for the use and installation of materials for storefront construction:
  - i. All materials used in storefronts shall be durable and non-corrosive. The use of natural materials is encouraged. A durable base material, impact resistant and washable, is required on all storefronts.
  - ii. Careful attention to detail is required in all connections and transitions with other materials. A defined edge shall be established between different materials and surfaces.



2. Masonry, Brick and CMU:
  - i. Brick is to be used for its richness in material, texture, and durability and therefore is considered the primary building material to be used along 60th Avenue.
  - ii. CMU may be used sparingly and shall not be used in more than 25% of any given building elevation.
  - iii. Colors used for masonry materials are earth tones as found in the prairie and range from soft brown, gray green, muted gold, tan, terra cotta, beige and warm reds. Bright building colors are not allowed to be used on the project for primary colors of the building. Warm blacks and charcoal colors can be used as accent colors, and cannot be more than 10% of the total elevation area of the building façade.
3. Stone:
  - i. Natural stone (granite, marble, limestone, slate, etc.) shall be used to bring substance and elegance to the storefront design. Being highly durable, stone is particularly appropriate as a base, as well as for paving, columns, fascias, and trims.
  - ii. Some stone varieties, such as slate, limestone and others, provide rich texture and color in an unpolished finish. Many granites and marbles can also be specially textured, sandblasted, flame-finished, honed, carved, or split-faced.
4. Plaster and Stucco:
  - i. Plaster and stucco finishes may be considered for limited applications. Plaster or stucco finishes shall be used in combination with other higher visual quality materials such as stone or metal and should not dominate as the primary storefront material.
  - ii. All plaster and stucco components are encouraged to have integral color.
5. Ceramic Tile:
  - i. Ceramic tile can introduce rich, decorative texture and graphic quality to storefront design. It may be used as an accent or in a limited application such as a column or arch cladding material.

- ii. Tile patterns used over large areas shall be made with appropriately-scaled tiles, and shall incorporate sophisticated design and accents of natural stone or other material. Small and intricate mosaic tile patterns shall be used for detail or accent areas only.

6. Metals:

- i. Metals such as brass, bronze, pewter, or stainless steel can be used for limited applications such as hardware, trim and panels when detailed appropriately. Metal finishes such as textured or brushed stainless steel, galvanized, sandblasted and etched metals are encouraged to be considered for creative design solutions.
- ii. The following criteria apply:
  - Metals shall be detailed to conceal seams and lap joints. All seams and joints shall be even and straight.
  - Metal panels shall be fabricated from either heavy-gauge material or thinner gauge material attached to solid backing, to prevent any deformation.
  - Polished metals shall be solid and not a plated finish. Polished metals shall have a clear lacquer finish to prevent tarnishing.
  - Unique finishes such as rusted, etched, or imprinted steel may be considered for special design objectives. Patina finishes are encouraged.

7. Painted and Stained Woods:

- i. Painted and stained woods are best used in window frames, as molding and decorative trims, and for solid panel areas such as decorative base bulkheads. It can also be used for dimensional architectural elements such as columns and entablatures.
- ii. Wood paneling and plank construction are discouraged unless used in highly imaginative ways.



8. Windows and Glass:

- i. Window configuration and glazing comprise the largest compositional elements of the storefront. They establish much of the store design character and define the relationship between interior and exterior. Design, size, shape, layout, proportions, and patterns shall be carefully considered.
- ii. Multi-paned windows can establish a variety of styles from traditional to contemporary. Repetitive, symmetrical and multiple small panes generally convey a more traditional character, which is encouraged.
- iii. Glass shall be predominantly clear to emphasize merchandise display. Reflective glass is strongly discouraged. Decorative glazing, such as colored, beveled, sandblasted or etched glass, may be used to create accent patterns or interest.
- iv. The following criteria apply:
  - Stained, etched, beveled or other types of specialty glazing are encouraged to create accent patterns and visual interest. Smaller glazing panes may vary depending upon use and compliance with code. Adjacent panes of glass may be joined with finished wood or metal frames.
  - Where glass panels meet other materials, the sealant shall be of a color matched to the finish materials.
  - Reflective or mirrored glazing is not permitted.
- v. Sandblasted, stained, etched and/or leaded glass is encouraged and should be creatively incorporated into the building design.

#### 9. Materials Requiring Special Consideration:

In certain instances, special consideration may be given to the use of the following materials if its application is highly original, creative and essential to the theme or design concept of the proposed building:

- Formed plastics or plastic laminates
- Rough sawn lumber, re-sawn lumber, and wood paneling
- Vinyl or fabric wall coverings
- Rough stucco or stucco with patterned finish
- Spandrel glass
- Smoked or tinted glazing
- Metal sheathing such as steel, aluminum, titanium or copper.

#### 10. Prohibited Materials:

Use of the following materials are strongly discouraged for all exterior finishes:

- E.I.F.S.
- Mirrored or “blacked-out” glazing
- Plexiglas

### 2.2.8 Awnings and Arcades

11. Awnings and arcades not only provide sunshade and weather protection, but can also provide interesting and unique architectural features. Creative materials and forms of awnings are encouraged to promote identity of each building and tenant space.

12. Awnings shall be subject to these standards and architectural conditions.

- i. The lowest point of any awning or shading element shall be 8'-0" minimum above finished floor level.
- ii. Awning framing shall be a natural, polished or painted metal.
- iii. Awnings shall be maintained in good condition and regularly cleaned.
- iv. Awning materials such as plastic or vinyl are strongly discouraged.

13. Signage may be incorporated into awnings, whether painted, silk screened, or fabricated into the awning itself. Signage shall be proportional to the

awning, and shall neither clash with nor overpower the awning design. Sign wording is limited to the tenant's name or used for the building's identity.

14. Logos on awnings are strongly encouraged, provided they are proportional to the overall size of the awning.

15. Awnings may be indirectly or internally illuminated so that the awning functions as a contained light source.



### 2.2.9 Service Areas

1. Provide separation between pedestrian circulation areas and all transformers, gas meters, and electrical meters. Service areas shall be hidden from view using landscaping, screen walls, or other creative solutions.

2. Provide a solid masonry enclosure that will screen the full height of all permanent dumpsters or compactors on all sides. All enclosures must be accessible through a pair of opaque and durable steel-framed doors that can withstand the rough treatment associated with trash collection. Temporary dumpsters do not need to be screened.

3. If the air handling units and mechanical exhaust vents on the roof are not incorporated into the architectural design of the building, provide complete screening of these systems with either a parapet or roof screen.

## Part 2.3 Merchant District

### 2.3.1 Introduction

The Merchant District represents a significant “edge” for the Victory Crossing project. As a regional north/south arterial, Quebec is commonly recognized today as the “front door” to the Stadium complex and Civic Center. Over time, there will be additional entries into the project, but for now it remains the principal means of access to Victory Crossing, Dicks Sporting Goods Park and the Civic Center. Because of this, new projects within this area should be well-planned and designed to give a positive “first impression” of Commerce City and Victory Crossing.

The central use and function of the Merchant District is to provide for convenience retail, service stations and restaurant opportunities within this area of Commerce City. Sites located in this district are well suited to this type of development in that they are highly visible and are provided good access. Because of its regional significance as an arterial roadway, the traffic volumes associated with Quebec Street create a strong demand for these uses outside of market created by Victory Crossing.

Of particular importance to this district is the ability to direct the driving public to the various destinations within Victory Crossing. Monumentation signage and a unified architectural treatment along this corridor will assist visitors in orienting around the proposed development. Pad sites, restaurants and other convenience oriented retail will be the prominent use within this district. As such, each new project within this corridor will require conformity to a central theme in terms of site planning and landscape design to prevent visual clutter along Quebec Street.



### 2.3.2 Pad Sites and Buildings

Because of the depth of the district property along Quebec Street, buildings within this district will be highly visible. All new projects will be required to have “four-sided” or 360° architectural treatment along with screening of service areas and utility connections. Exceptions to this standard include new commercial buildings immediately adjacent to the USPO Bulk Mail Center. In these specific circumstances, the rear elevation of a proposed building may not need to conform to the “four-sided” intent of these standards.

1. Each new building within the district will place special emphasis on entries with unique architectural design. Entry areas shall be highlighted by unique architectural treatments.
2. All building frontages (with entry areas) shall include a minimum of one street tree per 30’ of frontage on the average. Trees can be located in tree grates, shrub beds and other landscape areas or in planters.
3. Minimum width from curb to primary building line within a pad site is 12’. No sidewalk shall be narrower than six feet (6’) or eight feet (8’) with a curb overhang adjacent to a parking area.
4. Whenever possible, parking fields shall be located behind building groupings and not front along Quebec Street.
5. Where ever possible, outdoor seating and dining is strongly encouraged within this district.
6. Fueling Area Canopies are allowed in this District provided they are architecturally compatible with the primary building associated with this use (See 3.5.3 Fueling Area Canopies).



### 2.3.3 Pedestrian and Vehicular Connectivity

The overall design intent of Victory Crossing is to provide for a safe and comfortable pedestrian environment. This is also true of the Merchant District. Sidewalks, plazas spaces and urban trails should all be designed to encourage pedestrian traffic between 60th Avenue, the Soccer District, and future employment areas within Victory Crossing. While the parking within the Merchant District is not intended to be shared with the larger venues within the Victory Crossing project, providing for pedestrian connectivity is still an important consideration within the district.

1. Because it is considered a limited access arterial roadway, driveways are already identified along Quebec Street. There may be an opportunity to allow future right-in/right-out intersections in select areas along this corridor subject to the City's approval.
2. Each new project should be designed to interconnect in terms of shared access agreements, combined parking fields and common driveways.

### 2.3.4 Materials and Details

1. Brick, natural stone and integrally colored pre-cast concrete are the preferred building materials to be used within this district. Integrally colored split-face or ground face units may be used as accents only.
2. All building entries shall be highlighted and protected by a well-designed arcade, vestibule, awning, or canopy.
3. The scale of any entry feature should be proportional to the square footage of the building or in-line tenant space. No entry element shall exceed beyond 1/3 the building height over parapet line.
4. The side and rear elevations of all buildings located on a pad site will be consistent with the front façade.
5. At least 20% of the outer length of the proposed building's exterior will contain one or more architectural features. Exterior elevations within a screened loading court area are exempt from this provision.

6. Screen wall treatments should be constructed of brick, pre-cast concrete and/or integrally colored textured concrete masonry. These walls may shield a majority of the buildings rear elevation as the outer wall face. They are encouraged to utilize the following architectural elements:

- Metal trellis/panel attachments
- Brick, cut stone or pre-cast concrete
- Brick or cast stone pilasters

### 2.3.5 Roof Forms

1. Roofs may vary in slope and shall be covered with zinc-coated metals, copper, natural-looking clay, metal, concrete or slate tiles, high-profile three dimensional fiberglass/asphalt shingles or other materials compatible with the architectural character of Victory Crossing. Overhanging eaves, sloped roofs, hip roofs, towers and multiple roof planes are encouraged to create visual interest within the district.
2. Parapet walls must be used to conceal flat roofs. Where flat parapets occur, they shall be capped by either an architectural cornice or metal coping.
3. All rooftop mechanical units throughout Victory Crossing shall be screened from view. Screen walls must not detract from the building's architectural intent. Determination of the appropriateness of the screen wall design shall be at the discretion of the city staff.



### 2.3.6 Awnings and Arcades

1. Awnings and arcades not only provide sunshade and weather protection, but can also provide interesting and unique architectural features. Creative materials and forms of awnings are encouraged to promote identity of each building and tenant space.
2. Awnings shall be subject to these standards and architectural conditions.
  - i. The lowest point of any awning or shading element shall be 8'-0" minimum above finished floor level.
  - ii. Awning framing shall be a natural, polished or painted metal.
  - iii. Awnings shall be maintained in good condition and regularly cleaned.
  - iv. Awning materials such as plastic or vinyl are strongly discouraged.
3. Signage may be incorporated into awnings, whether painted, silk screened, or fabricated into the awning itself. Signage shall be proportional to the awning, and shall neither clash with nor overpower the awning design. Sign wording is limited to the tenant's name or used for the building's identity.
4. Logos on awnings are strongly encouraged, provided they are proportional to the overall size of the awning.
5. Awnings may be indirectly or internally illuminated so that the awning functions as a contained light source.

### 2.3.7 Drive-Thrus

Because of the location along a major arterial roadway, uses associated with drive-thru facilities are allowed to locate within the Merchant District.

1. Drive-thru lanes must provide for stacking of multiple vehicles in front of the order board, and between the order area and pick-up window(s). The drive-thru lanes must not block access to parking stalls or any pedestrian access point to the building. Stacking of cars must be designed

to prevent the encroachment into drive aisles, parking lots, or streets. For further information see Section 21-7326 of the Commerce City Land Development Code.

2. Whenever possible, drive-thru lanes should be located internally to the project and not adjacent to sidewalks or other public rights-of-way. This is particularly important for those sites adjoining the Victory Plaza District or along Quebec Street. In no case shall a drive-thru lane be located next to a pedestrian plaza or other outdoor seating area.

### 2.3.8 Service Areas

1. Provide separation between pedestrian circulation areas and all transformers, gas meters, and electrical meters. Service areas shall be hidden from view using landscaping, screen walls, or other creative solutions.
2. Provide a solid masonry enclosure that will screen the full height of all permanent dumpsters or compactors on all sides. All enclosures must be accessible through a pair of opaque and durable steel-framed doors that can withstand the rough treatment associated with trash collection. Temporary dumpsters do not need to be screened.
3. If the air handling units and mechanical exhaust vents on the roof are not incorporated into the architectural design of the building, provide complete screening of these systems with either a parapet or roof screen.



## Part 2.4 Central Park Boulevard District

### 2.4.1 Introduction

The Central Park Boulevard (CPB) District is located in the southeast corner of Victory Crossing with frontage along 56th Avenue and the extension of Central Park Boulevard. The significance of this district and its location is that it is positioned to become one of the projects most significant entries. Central Park Boulevard will connect to an interchange on I-70 that will provide a more direct connection to the Soccer District and with the improvements planned for 56th Avenue. Victory Crossing will also have a more convenient connection to Denver International Airport eight miles east of the project. Increased traffic resulting from improvements to 56th Avenue will exceed that of Quebec Street at some time in the not so distant future. Coupled with these significant transportation improvements, the Central Park Boulevard District will benefit from future rail transit connections and a regional trail network coming through the adjoining Stapleton project in the City and County of Denver. The CPB District is one of the largest districts in the Victory Crossing project and can accommodate a broad mix of uses ranging from retail, hospitality, office and restaurants. This district also has a major gateway into the stadium complex along Valentia Street. Similar to 60th Avenue, this north/south street serves both as a promenade and gateway to Dick's Sporting Goods Park. As a southern gateway to the stadium, and its future connections to Stapleton, Valentia creates many opportunities for a vibrant and pedestrian oriented development site.

Because the CPB District is located within an area of so many transportation improvements, this area can provide several opportunities for entry signage and monumentation. Special attention should be placed on the northwest corner of Central Park Boulevard and 56th Avenue as well as around the intersections of Valentia/56th and Prairie Parkway/Central Park Boulevard. Area should be reserved at these key intersections to develop a signage program for Victory Crossing and Commerce City similar to that found on Quebec Street.

### 2.4.2 Building Scale and Composition

1. All building fronts must be three-dimensional in form and shall have interesting modulated storefront planes. Projections or recesses may occur at any point along the storefront, provided there are appropriate and creatively detailed transitions between buildings.
2. New buildings and projects located along Valentia Street shall orient towards the street in order to provide continuity for the existing pedestrian environment. The road is designed and built to accommodate a safe and comfortable pedestrian environment with wide sidewalks with extensive tree lawns alongside the street.
3. Similar to the Victory Plaza District, the stadium at Dick's Sporting Goods Park is the most prominent structure in the area and will remain as the central focus of this district as well. It also serves as the northern terminus of Valentia Street from Stapleton to the south. Because of its importance, the placement of any building or structure on Valentia Street shall not detract, obscure, or hide the stadium building.



4. Building designs shall incorporate the following:
  - i. Front building elevations shall have a strong presence of form and substance of materials
  - ii. Dynamic entrances, including such elements as columns, arches, and pediments, are encouraged
  - iii. Materials shall be employed in innovative and creative ways for dramatic effect
  - iv. Materials shall be reliable and durable natural products such as metal, wood and stone
  - v. Awnings, arcades and canopies of a variety of types, designs and colors are encouraged
  - vi. Unique and creative detailing of building features, such as raised panels, moldings, coffers, etc.
  - vii. Entries should be recessed or otherwise defined to create visual interest along the street.
  - viii. Flags and banners are also encouraged along Valentia Street.
5. Where ever possible, outdoor seating and dining is strongly encouraged within this district.
6. Discouraged building types:
  - i. Architectural design shall be considered unique to the CPB District and not utilize “off-the-shelf” design forms found elsewhere in the metro area. Standardized or franchise architecture is discouraged within this district.

### 2.4.3 Pedestrian and Vehicular Connectivity

The overall concept behind Victory Crossing has been to provide a pedestrian-friendly environment. Special attention should be paid to the design of plazas and walkways with the intention of focusing pedestrian traffic along Valentia Street in order to provide safe and convenient non-vehicular linkages between the Soccer District and all locations within the project area.

1. Within individual building sites, safe and convenient pedestrian and bikeway systems shall be integrated with vehicular traffic to directly connect buildings, parking areas, open space, transit stops, services, on-site amenities, and other areas of interest. Street systems will be designed to accommodate pedestrians, bicycles as well as

vehicles by providing safe access via entry streets and driveways, circulation and connectivity through internal streets and blocks, connections from internal streets to external streets, and drive aisles for safe and efficient access to parking areas.

2. All development within the Central Park Boulevard District shall provide and contribute to an on-site system of pedestrian walkways, sidewalks, and bikeways integrated with vehicular traffic to provide continuous access to all land uses within Victory Crossing and to land uses on adjacent properties.
  - i. Cross access pedestrian easements may be required whenever necessary to ensure that adjacent parcels have adequate access to accommodate existing or future ownership or development patterns.
  - ii. Safe and convenient bicycle and pedestrian access from the development site will be provided to existing and designated public bike paths or greenways located on or adjacent to the development site.
3. Each development shall provide an on-site system of pedestrian walkways and/or public sidewalks throughout the district. The on-site pedestrian circulation system shall provide efficient access routes between the intended points of travel. Specifically, onsite pedestrian connections shall be provided to and between the following points:
  - i. The primary entrance or entrances to each building housing a principal use
  - ii. Existing or planned bus/transit stops, and “park and ride” locations
  - iii. Greenways and regional trail systems
  - iv. On-site amenities

### 2.4.4 Build-to Lines along Valentia Street

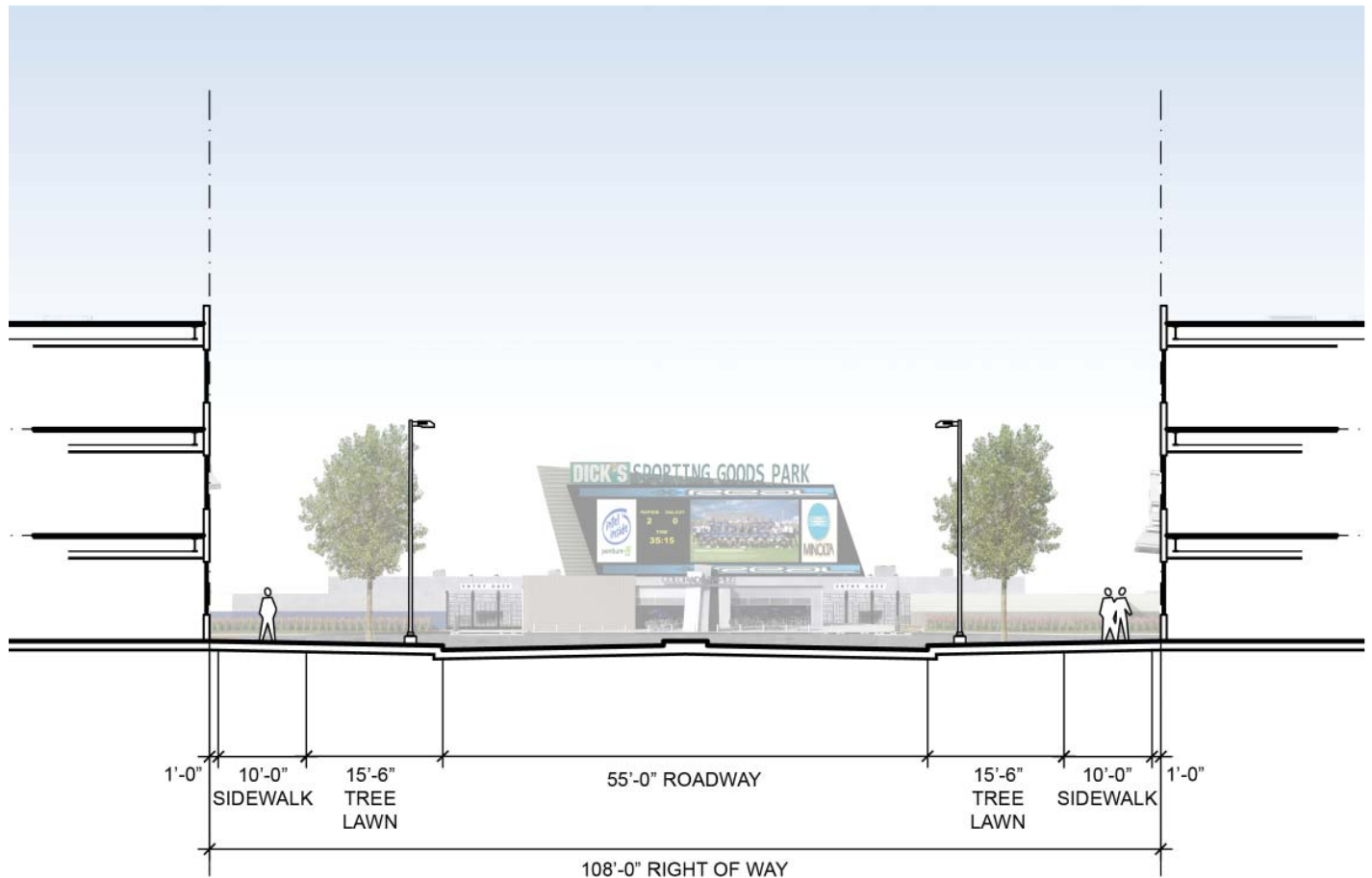
The intent of a build-to line is to pull the building façade close to the street and street-side sidewalk. Primary building façades along the street will be aligned to create a wall that defines the public realm while retaining sufficient width for

pedestrians and hardscapes. The street edge finishes the public realm by providing opportunities for traffic calming and pedestrian safety. The build-to line within this district also accommodates a larger landscape zone between the building and street. This is unique to the CPB District in that it provides for a larger pedestrian zone along Valentia and wider space for a formal tree lawn within this corridor. The build-to line listed below allows the buildings to be pulled up to the street sidewalk and provides for a more comfortable scale while providing for canopies, arcades and plazas which create shade and shield the pedestrian from the elements. Projecting signs, balconies, arcades, awnings and outdoor seating may extend closer to the street provided that a minimum of six feet of vertically and horizontally unobstructed space is preserved. Where primary façades are located along private roads or driveways, the build-to line will be measured from the edge of curb.

1. Build-to lines for Valentia Street
  - i. Front - 0 to 10 feet
  - ii. Side/Rear – 10 to 20 feet

### 2.4.5 Valentia Street Pedestrian Zone and Street Sections

Valentia Street is a primary entry into Victory Crossing that terminates at the southern gate of Dick’s Sporting Goods Park. It also provides the most direct access to the Field Complex within the Soccer District and oftentimes is the most utilized streets within the project area. This street also serves as a central axis and focal point within the Central Park Boulevard District. Valentia Street is designed to allow for a wider landscape strip or tree-lawn between the street and sidewalk. This area provides an additional buffer for wider sidewalks that will accommodate a strong pedestrian link between Victory Crossing and the Stapleton project located south of 56th Avenue. A pedestrian zone is established between the sidewalk and the build-to line along Valentia Street intended to support various uses such as restaurants, pubs, cafés and coffee shops.



The design of outdoor dining areas will be considered on a case-by-case basis. When adjacent to a sidewalk, plaza space or public right-of-way, a decorative fence or landscape feature will be required to separate the seating area from the pedestrian corridor within the right-of-way.

1. Whenever possible, paving within the Pedestrian Zone should be different in terms of color or material from that of the adjacent sidewalk.
2. Front setbacks within or adjacent to the Pedestrian Zone should contain elements of landscaping. These may be in-ground plantings, planter boxes or decorative pots as long as they don't obstruct access to adjoining structures or public sidewalks.

#### 2.4.6 Materials and Details

3. Victory Crossing requires high-quality materials, well-designed details and well-executed construction. The success of architectural design is largely dependent on how creatively the materials and design details are used to develop a unique and well executed image for each building. The following are minimum standards for the use and installation of materials for storefront construction:
  - i. All materials used in all building elevations shall be durable and non-corrosive. The use of natural materials is encouraged. A durable base material, impact resistant and washable, is required on all building fronts.
  - ii. Careful attention to detail is required in all connections and transitions with other materials. A defined edge shall be established between different materials and surfaces.
4. Masonry, Brick and CMU:
  - i. Brick and other similar masonry products are to be used for its texture and durability and should be considered the primary building material to be used within the district.
  - ii. Flat or split-faced CMU may be used sparingly and shall not be used in more than 25% of any given building elevation.

- iii. Colors used for masonry materials are earth tones as found in the prairie and range from soft brown, gray green, muted gold, tan, terra cotta, beige and warm reds. Bright building colors are not allowed to be used on the project for primary colors of the building. Warm blacks and charcoal colors can be used as accent colors, and cannot be more than 10% of the total elevation area of the building façade.
5. Stone:
    - i. Natural stone (granite, marble, limestone, slate, etc.) shall be used to bring substance and elegance to the storefront design. Being highly durable, stone is particularly appropriate as a base, as well as for paving, bulkheads, columns, fascias, and trims.
    - ii. Some stone varieties, such as slate, limestone and others, provide rich texture and color in an unpolished finish. Many granites and marbles can also be specially textured, sandblasted, flame-finished, honed, carved, or split-faced.
  6. Plaster and Stucco:
    - i. Plaster and stucco finishes may be considered for limited applications. Plaster or stucco finishes shall be used in combination with other higher visual quality materials such as stone or metal and should not dominate as the primary storefront material.
    - ii. All plaster and stucco components are encouraged to have integral color.



## 7. Ceramic Tile:

- i. Ceramic tile when used on buildings fronting Valentia Street can introduce rich, decorative texture and graphic quality at the pedestrian level. It may be used as an accent or in a limited application such as a column or arch cladding material.
- ii. Tile patterns used over large areas shall be made with appropriately-scaled tiles, and shall incorporate sophisticated design and accents of natural stone or other material. Small and intricate mosaic tile patterns shall be used for detail or accent areas only.

## 8. Metals:

- i. Metals such as brass, bronze, pewter, or stainless steel can be used for limited applications such as hardware, trim and panels when detailed appropriately. Metal finishes such as textured or brushed stainless steel, galvanized, sandblasted and etched metals are encouraged to be considered for creative design solutions.
- ii. The following criteria apply:
  - Metals shall be detailed to conceal seams and lap joints. All seams and joints shall be even and straight.
  - Metal panels shall be fabricated from either heavy-gauge material or thinner gauge material attached to solid backing, to prevent any deformation.
  - Polished metals shall be solid and not a plated finish. Polished metals shall have a clear lacquer finish to prevent tarnishing.
  - Unique finishes such as rusted, etched, or imprinted steel may be considered for special design objectives. Patina finishes are encouraged.

## 9. Painted and Stained Woods:

- i. Painted and stained woods are best used in window frames, as molding and decorative trims, and for solid panel areas such as decorative base bulkheads. It can also be used for dimensional architectural elements such as columns and entablatures.

- ii. Wood paneling and plank construction are discouraged unless used in highly imaginative ways.

## 10. Windows and Glass:

- i. Window configuration and glazing comprise the largest compositional elements of the storefront. They establish much of the store design character and define the relationship between interior and exterior. Design, size, shape, layout, proportions, and patterns shall be carefully considered.
- ii. Multi-paned windows can establish a variety of styles from traditional to contemporary. Repetitive, symmetrical and multiple small panes generally convey a more traditional character, which is encouraged.
- iii. Glass shall be predominantly clear to emphasize merchandise display. Reflective glass is strongly discouraged. Decorative glazing, such as colored, beveled, sandblasted or etched glass, may be used to create accent patterns or interest.



- iv. When glazing is considered, the following criteria apply:
  - Stained, etched, beveled or other types of specialty glazing are encouraged to create accent patterns and visual interest. Smaller glazing panes may vary depending upon use and compliance with code. Adjacent panes of glass may be joined with finished wood or metal frames.
  - Where glass panels meet other materials, the sealant shall be of a color matched to the finish materials.
  - Reflective or mirrored glazing is not permitted.
- v. Sandblasted, stained, etched and/or leaded glass is encouraged and should be creatively incorporated into the building design.

#### 11. Materials Requiring Special Consideration:

In certain instances, special consideration may be given to the use of the following materials if its application is highly original, creative and essential to the theme or design concept of the proposed building:

- Formed plastics or plastic laminates
- Rough sawn lumber, re-sawn lumber, and wood paneling
- Vinyl or fabric wall coverings
- Rough stucco or stucco with patterned finish
- Spandrel glass
- Smoked or tinted glazing
- Metal sheathing such as steel, aluminum, titanium or copper.
- E.I.F.S.

#### 12. Prohibited Materials:

The use of the following materials is strongly discouraged for all exterior finishes:

- Mirrored or “blacked-out” glazing
- Plexiglas

### 2.4.7 Awnings and Arcades

1. Awnings and arcades not only provide sunshade and weather protection, but can also provide interesting and unique architectural features. Creative materials and forms of awnings are

encouraged to promote identity of each building and tenant space.

2. Awnings shall be subject to these standards and architectural conditions.
  - i. The lowest point of any awning or shading element shall be 8'-0" minimum above finished floor level.
  - ii. Awning framing shall be a natural, polished or painted metal.
  - iii. Awnings shall be maintained in good condition and regularly cleaned.
  - iv. Awning materials such as plastic or vinyl are strongly discouraged.
3. Signage may be incorporated into awnings, whether painted, silk screened, or fabricated into the awning itself. Signage shall be proportional to the awning, and shall neither clash with nor overpower the awning design. Sign wording is limited to the tenant's name or used for the building's identity.
4. Logos on awnings are strongly encouraged, provided they are proportional to the overall size of the awning.
5. Awnings may be indirectly or internally illuminated so that the awning functions as a contained light source.

### 2.4.8 Drive-Thrus

Because of the location along 56th Avenue and the potential traffic projected for this corridor, uses associated with drive-thru facilities are allowed to locate within the CPB District.

1. Drive-thru lanes must provide for stacking of multiple vehicles in front of the order board, and between the order area and pick-up window(s). The drive-thru lanes must not block access to parking stalls or any pedestrian access point to the building. Stacking of cars must be designed to prevent the encroachment into drive aisles, parking lots, or streets. For further information see Section 21-7326 of the Commerce City Land Development Code.

2. Whenever possible, drive-thru lanes should be located internally to the project and not adjacent to sidewalks or other public rights-of-way. This is particularly important for those sites adjoining the Soccer District or along Valentia Street. In no case shall a drive-thru lane be located next to a pedestrian plaza or other outdoor seating area.

#### 2.4.9 Service Areas

1. Provide separation between pedestrian circulation areas and all transformers, gas meters, and electrical meters. Service areas shall be hidden from view using landscaping, screen walls, or other creative solutions.
2. Provide a solid masonry enclosure that will screen the full height of all permanent dumpsters or compactors on all sides. All enclosures must be accessible through a pair of opaque and durable steel-framed doors that can withstand the rough treatment associated with trash collection. Temporary dumpsters do not need to be screened.
3. If the air handling units and mechanical exhaust vents on the roof are not incorporated into the architectural design of the building, provide complete screening of these systems with either a parapet or roof screen.

### Part 2.5 Prairie Parkway District

#### 2.5.1 Introduction

The Prairie Parkway District is located along Quebec Street to the west and Prairie Parkway to the north. While the district has extensive frontage along Quebec, its most unique feature is the district's proximity to the large open space within the Rocky Mountain Arsenal – National Wildlife Refuge (RMA-NWR). Just across Prairie Parkway is a pristine tract of land preserved as public open space. This RMA-NWR currently has an extensive network of regionally linked trails along with a visitor's center that attracts thousands of people every year. The Prairie Parkway District is located to take advantage of the views to the north and west as well as accessing directly to a variety of recreational activities the

RMA-NWR has to offer. The intersection of Quebec Street and Prairie Parkway represents a major entryway into the project area and provides convenient access to the large parking lots supporting the stadium.

This site provides for an extensive mixed use development that can accommodate larger retail users, corporate and other multi-tenant offices as well as convenience retail and service stations along Quebec Street. In addition to these uses, this district is also well suited for large outdoor/indoor recreational venues. The district also abuts the Commerce City Civic Center and could generate the demand for additional office users.

The Prairie Parkway District is similar in use to that of the Central Park Boulevard District with the distinction of having a large amount of passive open space adjoining the site. As a result, the development pattern for this area should be less dense and more conducive to campus-like environments with an emphasis on naturally landscaped sites in order relate to the larger open space tract to the north.

#### 2.5.2 Building Scale and Composition

1. All building fronts must be three-dimensional in form and shall have interesting modulated storefront planes. Projections or recesses may occur at any point along the storefront, provided there are appropriate and creatively detailed transitions between buildings.
2. All buildings will be required to have "four-sided" or 360° architectural treatment along with screening of service areas and utility connections.
3. Where ever possible, outdoor seating and dining is strongly encouraged within this district.
4. Building designs shall incorporate the following:
  - i. Front building elevations shall have a strong presence of form and substance of materials
  - ii. Dynamic entrances, including such elements as columns, arches, and pediments, are encouraged
  - iii. Materials shall be employed in innovative and creative ways for dramatic effect

- iv. Materials shall be reliable and durable natural products such as metal, wood and stone
  - v. Awnings, arcades and canopies of a variety of types, designs and colors are encouraged
  - vi. Unique and creative detailing of building features, such as raised panels, moldings, coffers, etc.
5. Discouraged building types:
    - i. Architectural design shall be considered unique to the Prairie Parkway District and not utilize “off-the-shelf” design forms found elsewhere in the metro area. Standardized or franchise architecture is discouraged within this district.
  6. All building frontages (with entry areas) shall include a minimum of one street tree per 30’ of frontage on the average. Trees can be located in tree grates, shrub beds and other landscape areas or in planters.
  7. Minimum width from curb to primary building line within a pad site is 12’. No sidewalk shall be narrower than six feet (6”) or eight feet (8”) with a curb overhang adjacent to a parking area.
  8. Whenever possible, parking fields shall be located behind building groupings and not front along Quebec Street or Prairie Parkway.
  9. Buildings located next to major intersections such as Quebec Street and Prairie Parkway should be located as close as possible to the public right-of-way associated with these streets.
  10. Fueling Area Canopies are allowed in certain portions of this district provided they are architecturally compatible with the primary building associated with this use (See 3.5.3 Fueling Area Canopies).

### 2.5.3 Pedestrian and Vehicular Connectivity

Special attention should be paid to the design and placement of naturally landscaped areas in relation to the RMA-NWR property to the north. In addition to these linkages to the natural open space north of the district, pedestrian connections are also required to the Soccer District and all locations within Victory Crossing.

1. Within individual building sites, safe and convenient pedestrian and bikeway systems shall be integrated with vehicular traffic to directly connect buildings, parking areas, open space, transit stops, services, on-site amenities, and other areas of interest. Street systems will be designed to accommodate pedestrians, bicycles as well as vehicles by providing safe access via entry streets and driveways, circulation and connectivity through internal streets and blocks, connections from internal streets to external streets, and drive aisles for safe and efficient access to parking areas.
2. All development within the Prairie Parkway District shall provide and contribute to an on-site system of pedestrian walkways, sidewalks, and bikeways integrated with vehicular traffic to provide continuous access to all land uses within the overall project area and to land uses on adjacent properties.
  - i. Cross access pedestrian easements may be required whenever necessary to ensure that adjacent parcels have adequate access to accommodate existing or future ownership or development patterns.
  - ii. Safe and convenient bicycle and pedestrian access from the development site will be provided to existing and designated public bike paths or greenways located on or adjacent to the development site.



3. Each development shall provide an on-site system of pedestrian walkways and/or public sidewalks throughout the district. The on-site pedestrian circulation system shall provide efficient access routes between the intended points of travel. Specifically, onsite pedestrian connections shall be provided to and between the following points:

- i. The primary entrance or entrances to each building housing a principal use
- ii. Existing or planned bus/transit stops, and “park and ride” locations
- iii. Greenways, open space, parks and regional trail systems
- iv. On-site amenities

#### 2.5.4 Materials and Details

1. Victory Crossing requires high-quality materials, well-designed details and well-executed construction. The success of architectural design is largely dependent on how creatively the materials and design details are used to develop a unique and well executed image for each building. The following are minimum standards for the use and installation of materials for storefront construction:

- i. All materials used in all building elevations shall be durable and non-corrosive. The use of natural materials is encouraged. A durable base material, impact resistant and washable, is required on all building fronts.
- ii. Careful attention to detail is required in all connections and transitions with other materials. A defined edge shall be established between different materials and surfaces.

2. Masonry, Brick and CMU:

- i. Brick and other similar masonry products are to be used for its texture and durability and should be considered the primary building material to be used within the district.
- ii. Flat-faced CMU may be used sparingly and shall not be used in more than 25% of any given building elevation.
- iii. Colors used for masonry materials are earth tones as found in the prairie and range from soft

brown, gray green, muted gold, tan, terra cotta, beige and warm reds. Bright building colors are not allowed to be used on the project for primary colors of the building. Warm blacks and charcoal colors can be used as accent colors, and cannot be more than 10% of the total elevation area of the building façade.

3. Stone:

- i. Natural stone (granite, marble, limestone, slate, etc.) shall be used to bring substance and elegance to the storefront design. Being highly durable, stone is particularly appropriate as a base, as well as for paving, bulkheads, columns, fascias, and trims.
- ii. Some stone varieties, such as slate, limestone and others, provide rich texture and color in an unpolished finish. Many granites and marbles can also be specially textured, sandblasted, flame-finished, honed, carved, or split-faced.

4. Plaster and Stucco:

- i. Plaster and stucco finishes may be considered for limited applications. Plaster or stucco finishes shall be used in combination with other higher visual quality materials such as stone or metal and should not dominate as the primary storefront material.
- ii. All plaster and stucco components are encouraged to have integral color.



#### 5. Ceramic Tile:

- i. Ceramic tile when used on buildings fronting Quebec Street can introduce rich, decorative texture and graphic quality at the pedestrian level. It may be used as an accent or in a limited application such as a column or arch cladding material.
- ii. Tile patterns used over large areas shall be made with appropriately-scaled tiles, and shall incorporate sophisticated design and accents of natural stone or other material. Small and intricate mosaic tile patterns shall be used for detail or accent areas only.

#### 6. Metals:

- i. Metals such as brass, bronze, pewter, or stainless steel can be used for limited applications such as hardware, trim and panels when detailed appropriately. Metal finishes such as textured or brushed stainless steel, galvanized, sandblasted and etched metals are encouraged to be considered for creative design solutions.
- ii. The following criteria apply:
  - Metals shall be detailed to conceal seams and lap joints. All seams and joints shall be even and straight.
  - Metal panels shall be fabricated from either heavy-gauge material or thinner gauge material attached to solid backing, to prevent any deformation.
  - Polished metals shall be solid and not a plated finish. Polished metals shall have a clear lacquer finish to prevent tarnishing.
  - Unique finishes such as rusted, etched, or imprinted steel may be considered for special design objectives. Patina finishes are encouraged.

#### 7. Painted and Stained Woods:

- i. Painted and stained woods are best used in window frames, as molding and decorative trims, and for solid panel areas such as decorative base bulkheads. It can also be used for dimensional architectural elements such as columns and entablatures.

- ii. Wood paneling and plank construction are discouraged unless used in highly imaginative ways.

#### 8. Windows and Glass:

- i. Window configuration and glazing comprise the largest compositional elements of the storefront. They establish much of the store design character and define the relationship between interior and exterior. Design, size, shape, layout, proportions, and patterns shall be carefully considered.
- ii. Multi-paned windows can establish a variety of styles from traditional to contemporary. Repetitive, symmetrical and multiple small panes generally convey a more traditional character, which is encouraged.
- iii. Glass shall be predominantly clear to emphasize merchandise display. Reflective glass is strongly discouraged. Decorative glazing, such as colored, beveled, sandblasted or etched glass, may be used to create accent patterns or interest.
- iv. When glazing is considered, the following criteria apply:
  - Stained, etched, beveled or other types of specialty glazing are encouraged to create accent patterns and visual interest. Smaller glazing panes may vary depending upon use and compliance with code. Adjacent panes of glass may be joined with finished wood or metal frames.



- Where glass panels meet other materials, the sealant shall be of a color matched to the finish materials.
- Reflective or mirrored glazing is not permitted.
- v. Sandblasted, stained, etched and/or leaded glass is encouraged and should be creatively incorporated into the building design.

9. Materials Requiring Special Consideration:

In certain instances, special consideration may be given to the use of the following materials if its application is highly original, creative and essential to the theme or design concept of the proposed building:

- Formed plastics or plastic laminates
- Rough sawn lumber, re-sawn lumber, and wood paneling
- Vinyl or fabric wall coverings
- Rough stucco or stucco with patterned finish
- Spandrel glass
- Smoked or tinted glazing
- Metal sheathing such as steel, aluminum, titanium or copper.
- E.I.F.S.

11. Prohibited Materials:

The use of the following materials is strongly discouraged for all exterior finishes:

- Mirrored or “blacked-out” glazing
- Plexiglas



## 2.5.5 Awnings and Arcades

1. Awnings and arcades not only provide sunshade and weather protection, but can also provide interesting and unique architectural features. Creative materials and forms of awnings are encouraged to promote identity of each building and tenant space.
2. Awnings shall be subject to these standards and architectural conditions.
  - i. The lowest point of any awning or shading element shall be 8'-0" minimum above finished floor level.
  - ii. Awning framing shall be a natural, polished or painted metal.
  - iii. Awnings shall be maintained in good condition and regularly cleaned.
  - iv. Awning materials such as plastic or vinyl are strongly discouraged.
3. Signage may be incorporated into awnings, whether painted, silk screened, or fabricated into the awning itself. Signage shall be proportional to the awning, and shall neither clash with nor overpower the awning design. Sign wording is limited to the tenant's name or used for the building's identity.
4. Logos on awnings are strongly encouraged, provided they are proportional to the overall size of the awning.
5. Awnings may be indirectly or internally illuminated so that the awning functions as a contained light source.



### 2.5.6 Drive-Thrus

Uses associated with drive-thru facilities may be allowed on properties located along Quebec Street only. All drive-thru facilities shall be located away from public rights-of-way and screened from trails or other open space amenities.

1. Drive-thru lanes must provide for stacking of multiple vehicles in front of the order board, and between the order area and pick-up window(s). The drive-thru lanes must not block access to parking stalls or any pedestrian access point to the building. Stacking of cars must be designed to prevent the encroachment into drive aisles, parking lots, or streets. For further information see Section 21-7326 of the Commerce City Land Development Code.
2. Whenever possible, drive-thru lanes should be located internally to the project and not adjacent to sidewalks. This is particularly important for those sites adjoining the Victory Plaza District or along Quebec Street. In no case shall a drive-thru lane be located next to a pedestrian plaza, park site, open space tract or other outdoor use area.

### 2.5.7 Service Areas

1. Provide separation between pedestrian circulation areas and all transformers, gas meters, and electrical meters. Service areas shall be hidden from view using landscaping, screen walls, or other creative solutions.
2. Provide a solid masonry enclosure that will screen the full height of all permanent dumpsters or compactors on all sides. All enclosures must be accessible through a pair of opaque and durable steel-framed doors that can withstand the rough treatment associated with trash collection. Temporary dumpsters do not need to be screened.
3. If the air handling units and mechanical exhaust vents on the roof are not incorporated into the architectural design of the building, provide complete screening of these systems with either a parapet or roof screen.



## Part 2.6 North Gateway District

### 2.6.1 Introduction

The North Gateway District is physically separated from Victory Crossing and is located 1.5 miles north of 60th Avenue. A large tract of open space that has been preserved along Quebec Parkway\* separates the two sites. The northern boundary of the district is State Highway 2 and even though there is extensive frontage along this arterial, access to this highway is very limited. East of the district is the Rocky Mountain Arsenal – National Wildlife Refuge (RMA-NWR). Views to the east include a bison herd and vast expanses of short-grass prairie preserved as open space. South of the district is a regional High School and water sanitation plant.

\* Quebec Street becomes Quebec Parkway north of 60th Street

Within Victory Crossing, this district is planned for a broad spectrum of commercial uses. Because the North Gateway District is close to warehousing and “heavier” commercial development along the highway corridor, it is allowed to contain a Commercial Flex type of construction within the area. Uses contained within areas identified as Commercial Flex are not considered traditional industrial or light industrial activities. Flex properties may include single building sites as well as multiple building complexes for tenants that require adaptable spaces for the growth and development of their commercial enterprises. Uses may include, but are not limited to computer hardware and software development, bio-medical research, pharmaceutical manufacturing, environmental, energy or other “high technology” engineering pursuits. As is the case for many high tech activities, there may be a minor element of light assembly, fabrication or warehousing involved in the on-site operations of a company. However the scale of these operations and its intended containment within the primary building, a standard industrial classification is not warranted within the Commerce City Land Development Code.

Because of the district's proximity to an established residential neighborhood, uses associated with Commercial Flex are prohibited within certain areas.



### 2.6.2 Building Scale and Composition

1. All buildings are encouraged to have “four-sided” or 360° architectural treatment along with screening of service areas and utility connections.
2. Building designs shall incorporate the following:
  - i. Front building elevations shall have a strong presence of form and substance of materials
  - ii. Easily identifiable building entrances.
  - iii. Materials shall be employed in innovative and creative ways for dramatic effect.
3. Minimum width from curb to primary building line is eight feet (8'). No sidewalk shall be narrower than six feet (6') or eight feet (8') with a curb overhang adjacent to a parking area.
4. Whenever possible, parking fields and loading docks shall be located behind building groupings and not front along Quebec Parkway or State Highway 2.
5. If the air handling units and mechanical exhaust vents on the roof are not incorporated into the architectural design of the building, provide complete screening of these systems with either a parapet or roof screen.
6. Fueling Area Canopies are allowed in certain portions of this district provided they are architecturally compatible with the primary building associated with this use (See 3.5.3 Fueling Area Canopies).

### 2.6.3 Pedestrian and Vehicular Connectivity

1. Cross access easements for vehicles may be required whenever necessary to ensure that adjacent parcels have adequate access to accommodate existing or future ownership or development patterns.
2. Each development shall provide an on-site system of pedestrian walkways and/or public sidewalks throughout the district. The on-site pedestrian circulation system shall provide efficient access routes between the intended points of travel. Specifically, onsite pedestrian connections shall be provided to and between the following points:
  - i. The primary entrance or entrances to each building housing a principal use
  - ii. Existing or planned bus/transit stops, and future “park and ride” locations
  - iii. Greenways, open space, parks and regional trail systems
  - iv. On-site amenities



### 2.6.4 Materials and Details

1. Regardless of location, all new projects within Victory Crossing will be built of high-quality materials, with well-designed details and well-executed construction. The success of architectural design is largely dependent on how creatively the materials and design details are used to develop a unique and well-executed image for each building. The following are minimum standards for the use and installation of materials for new construction:
  - i. All materials used on each building elevations shall be durable and non-corrosive. A durable base material, impact resistant and washable, is required on all building fronts.
  - ii. Careful attention to detail is required in all connections and transitions with other materials. A defined edge shall be established between different materials and surfaces.
2. Masonry, Brick and CMU:
  - i. Brick and other similar masonry products are to be used for its texture and durability and should be considered the primary building material to be used within the district.
  - ii. Flat and split-faced CMU may be used and shall not be applied on more than 50% of any given building elevation.
  - iii. Colors used for masonry walls are earth tones as found in the prairie and range from soft brown, gray green, muted gold, tan, terra cotta, beige and warm reds. Bright building colors are not allowed to be used on the project for primary colors of the building.
3. Plaster and Stucco:
  - i. Plaster and stucco finishes may be considered for limited applications. Plaster or stucco finishes shall be used in combination with other higher visual quality materials such as stone or metal and should not dominate as the primary storefront material.
  - ii. All plaster and stucco components are encouraged to have integral color.

#### 4. Metals:

- i. If metals are considered as an architectural treatment, the following criteria apply:
  - Metals shall be detailed to conceal seams and lap joints. All seams and joints shall be even and straight.
  - Metal panels shall be fabricated from either heavy-gauge material or thinner gauge material attached to solid backing, to prevent any deformation.
  - Unique finishes such as rusted, etched, or imprinted steel may be considered for special design objectives. Patina finishes are encouraged.

#### 5. Painted and Stained Woods:

- i. Painted and stained woods are best used in window frames, as molding and decorative trims, and for solid panel areas such as decorative base bulkheads. It can also be used for dimensional architectural elements such as columns and entablatures.
- ii. Wood paneling and plank construction are discouraged unless used in highly imaginative ways.



#### 2.6.5 Drive-Thrus

Uses associated with drive-thru facilities may be allowed on properties fronting Quebec Parkway only. All drive-thru facilities shall be located away from public rights-of-way and screened from trails or other open space amenities.

1. Drive-thru lanes must provide for stacking of multiple vehicles in front of the order board, and between the order area and pick-up window(s). The drive-thru lanes must not block access to parking stalls or any pedestrian access point to the building. Stacking of cars must be designed to prevent the encroachment into drive aisles, parking lots, or streets. For further information see Section 21-7326 of the Commerce City Land Development Code.
2. Whenever possible, drive-thru lanes should be located internally to the project and not adjacent to sidewalks. In no case shall a drive-thru lane be located next to existing residential development, open space or any other outdoor use area.

#### 2.6.6 Loading Docks and Trash Enclosures

The visual impacts of loading docks and trash storage areas should be minimized, particularly in relationship to views from public roadways and along pedestrian areas. Placement and design of screening for these facilities is a priority for all sites.

1. Locate loading docks, trash containers and service areas out of view from adjacent streets, properties, pedestrian pathways and open space corridors. To protect views, screen facilities with architectural elements and/or evergreen landscaping. Architectural screening for loading docks, service areas and trash enclosures should be a minimum height of six feet and incorporate materials and finishes similar and compatible with those of the primary structures. All trash enclosures must be accessible through a pair of opaque and durable steel-framed doors that can withstand the rough treatment associated with trash collection.

2. Locate loading docks, service, and trash collection areas so they do not encroach into any setbacks.
3. Locate parking areas for trucks, research trailers and service vehicles used in association with the principal use, away from public parking lots and major pedestrian gathering areas. Unless totally out of view, Provide architectural or landscape screening for these parking areas unless they are located completely out of the view from adjoining properties. Materials, supplies or equipment being stored on a site must be concealed inside a closed building.
4. Clearly identify all service driveways in order to discourage the use of main entrances for deliveries.
5. Whenever feasible, align service areas and loading docks with those of adjacent buildings and parcels so that service driveways and truck courts may be shared.
6. Avoid placing service areas and loading docks where they are visible from adjacent “non flex” buildings or where they will impact adjoining residential neighborhoods.
7. Locate any accessory structures on site so they are not between the primary building and any roadway adjacent to the front or side yard of the site. Accessory structures must be screened by landscaping and constructed of materials identical to, or compatible with the primary structure on the site.
8. Locate all loading docks or other areas where exhaust fumes from vehicles may accumulate away from pedestrian areas, outdoor seating areas and air intakes of adjacent buildings.





# SITE DESIGN STANDARDS

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## Part 3.1 Signage

### 3.1.1 Introduction

Victory Crossing is comprised of an intensive mix of land uses that will require a comprehensive approach to project signage. Dick's Sporting Goods Park has become well established in the metro area as a regional venue that attracts large crowds throughout the soccer season. The areas surrounding the Stadium such as the Field Complex also generate large numbers of people for music festivals and other sporting events. Because of these activities and the potential to bring in thousands of visitors into the area for any one event, signage that provides effective orientation and wayfinding is essential to the ongoing success of Victory Crossing. This is especially important because of the surrounding office and retail uses within the project area. A clear and understandable signage program will lessen the possible conflicts between each use area within Victory Crossing.

### 3.1.2 Approach

The diversity of uses within Victory Crossing creates the need for a variety of sign functions and applications. The Stadium complex has a unique set of circumstances that warrants a signage program suited to this specific venue. Similarly the retail component within the Merchant District will need a different set of sign standards than that of an office project within the Prairie Gateway District. A unifying theme within the sign standards will be required in order to both preserve visual continuity in the Victory Crossing brand as well as providing the traveling public an opportunity to orient on-site. In order to meet the diverse requirements for signage within Victory Crossing while preserving a central theme, these standards are intended to accomplish the following:

1. Provide for an easily identifiable brand for Victory Crossing.
2. Support the economic growth and development of all development within the project in a manner that preserves the identity of Victory Crossing and the surrounding natural environment.
3. Create excitement for the game of soccer and highlight the regional significance of Dick's Sporting Goods Park.



# SITE DESIGN STANDARDS

4. Reduce the potential for visual clutter along major arterial roadways such as Quebec Street and 56th Avenue.
5. Assist visitors, emergency personnel and employees to locate destinations easily.
6. Facilitate efficient traffic circulation for the safety and comfort of the public.

In order to accommodate the variety of uses and functions required for signage within Victory Crossing, a series of three standards are outlined in these standards. They are;

1. First Tier signs associated with the Soccer District and the Victory Plaza design districts.
2. Second Tier signs that cover all “themed” wayfinding signage throughout Victory Crossing.
3. Third Tier signs that represent traditional commercial signage found within all other areas of the development.

### 3.1.3 First Tier Signage

First Tier signage represents a unique opportunity to establish a strong visual identity for Victory Crossing by supporting Dick’s Sporting Goods Park and the properties surrounding the stadium complex. This type of signage includes scoreboards coupled with motion graphics that are meant to be seen outside of the stadium itself. The purpose of this signage is to generate excitement about the venue and showcase the stadium as an iconic image within the region. The signage program for Dick’s Sporting Goods Park should employ extensive motion graphics associated with the game of soccer. Much of the technology anticipated for this type of signage program is in the early stages of development and will be realized in the next couple years.

This type of electronic imagery is also allowed to be applied outside of the walls of the stadium as well as throughout the Victory Plaza design district. These applications outside of the stadium are meant to support and enhance the signage graphics used for Dicks Sporting Goods Park and not detract from the architectural environment within the district. This type of signage allows for the advertising of the Colorado Rapids, Dick’s Sporting Goods Park, and

special events. Advertising of corporate sponsors will also be allowed provided they are associated directly with the stadium venue.

1. First Tier signage is meant to be directed towards the pedestrian level experience while at the same time allowed to be viewed from most areas within Victory Crossing.
2. Because this type of graphic standard is not considered traditional signage normally permitted through traditional sign codes, they will be considered on a case-by-case basis by the Development Review Team described in Part 4.
3. A comprehensive sign plan that includes location standards and design guidelines will be required to be in place before any consideration is given to proposed First or Second Tier signage.

### 3.1.4 Second Tier Signage

Second Tier signage is used primarily for wayfinding within the larger Victory Crossing project area. The use of Second Tier signs is allowed throughout the overall project area and may overlap signage applications within First and Third Tier areas.

The purpose of Second Tier signs is to provide a systematic approach to direction finding for pedestrians and motorists within Victory Crossing. These signs consist of a variety of vehicular and pedestrian signs comprised of maps, banners, kiosks, parking identification as well as arrival and entry signs. This type of signage program also establishes the thematic image for the project as a whole. A Second Tier signage program should be developed to:

1. Provide an attractive and consistent message to the traveling public.
2. Be readable to both pedestrians and motorists.
3. Identify the boundaries of Victory Crossing and establish a strong visual identity for the project area.
4. Compliment the architectural environment including streetscapes, plazas and parking lots.
5. Enhance and First and Third Tier signage programs.

Because most of this signage is directed toward the venues within Dick's Sporting Goods Park, wayfinding begins at the earliest opportunity on the property. Entry monuments already exist along Quebec Street at the major intersections of 58th, 60th and Prairie Parkway. There are other opportunities in the future for the same type of entry signage when Central Park Boulevard is extended north to Prairie Parkway as well. These new applications should be designed in a similar manner to resemble the existing monuments on Quebec.

1. A comprehensive sign plan that includes location standards and design guidelines will be required to be in place before any consideration is given to proposed First or Second Tier signage.

### 3.1.5 Third Tier Signage

Third Tier signs are those found in traditional commercial and retail developments within Victory Crossing. This type of signage is utilized to identify individual buildings as well as planned retail projects within the Merchant, Central Park Boulevard, Prairie Gateway and North Gateway Districts.

Third Tier signs are permitted as any other sign within Commerce City and are required to conform to the existing application process defined by the City's sign code. First and Second Tier signage may be allowed within these commercial and retail use areas as well and not to be included in any sign application process required for Third Tier signs. First and Second Tier signs will be considered separately and not counted in the total signage allowed calculations applied to Third Tier signs.

## Part 3.2 Parking

### 3.2.1 Introduction

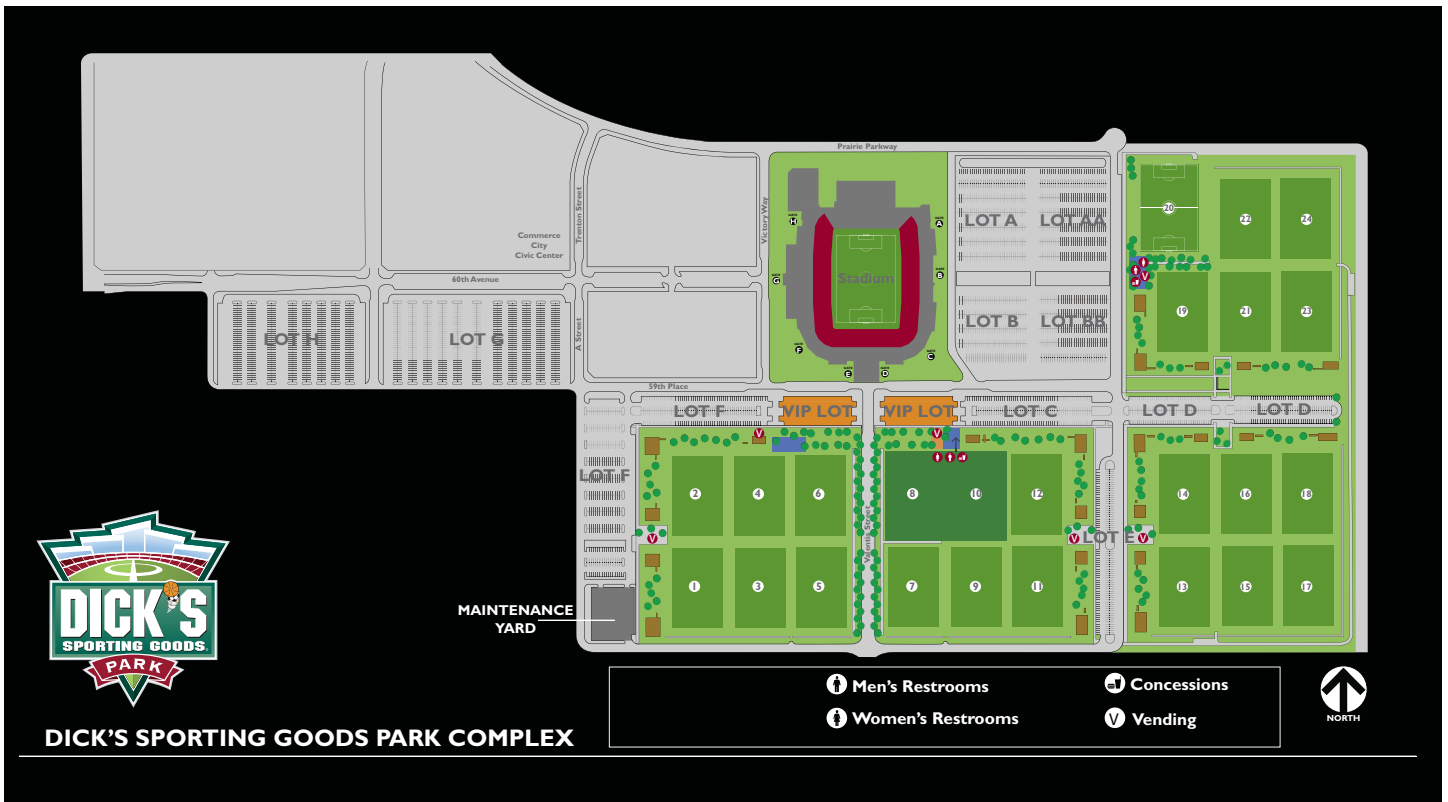
Surface parking will utilize the largest area of land within Victory Crossing and how it is executed will have a direct bearing on the projects overall image, efficiency and marketability. Parking must meet the needs of both a growing employment sector as well as the soccer and field complex. There are a number of strategies and techniques that can be used to meet these challenges. Because of the unique combination of uses within Victory Crossing the most important strategy involves maximizing opportunities for shared parking within the project area without impacting commerce and the surrounding residential neighborhoods.

### 3.2.2 Purpose

The purpose of this Section is to outline an approach for the future development of a comprehensive parking plan for Victory Crossing. **Existing parking spaces that are currently paved and in support of the entire Soccer District cannot be changed until a comprehensive parking plan is developed.** A plan of this type is strongly recommended before new development is discussed in the Victory Plaza, Prairie Parkway and Central Park Boulevard design districts.

The work scope of a parking plan recommended by these standards should address specific issues relating to the location and design of off-street parking areas that accommodates motor vehicles while balancing the needs of pedestrians, bicyclists, and transit users. Furthermore, the plan should consider:

- The location and layout of parking areas planned to minimize conflict with pedestrians as well as contribute to efficient automobile access and circulation.
- Minimizing any potential impact to adjoining residential neighborhoods.
- An operations and management plan including possible shuttle systems for remote locations, phasing, and traffic circulation.
- Evaluating specific opportunities for shared parking.



### 3.2.3 Soccer District Parking

Parking has been designed to accommodate both the field complex and Dick's Sporting Goods Park through the use of a shared parking plan. Parking lots have been designed and built to accommodate both the field complex as well as the stadium during events at both locations. The following illustration depicts the approved parking layout for the Stadium and adjoining Field Complex.

Note: This illustration represents the plan as it was created in 2008 and may be changed provided that new parking areas are developed as suitable alternatives.



### 3.2.4 Shared Parking Guidelines

Because of the infrequent, yet predictable nature of parking around the stadium, shared parking strategies may be considered for specific locations within Victory Crossing. As the property continues to develop, new office and retail uses will inevitably replace large surface parking lots close to Dick's Sporting Goods Park. Many of these new projects will have compatible schedules with the stadium; however there will need to be formal arrangements in place before parking can be shared in the future. There are other locations within Victory Crossing that cannot be considered suitable for any type of shared parking arrangement. These sites would be associated with retail uses along Quebec, Central Park Boulevard and 56th Avenue. The following map indicates specific locations around the stadium that may be suitable for shared parking provided an acceptable agreement is in place.

Shared parking options may be considered in other locations throughout Victory Crossing; however these options may not be considered reciprocal for stadium and field complex parking outside of the Shared Parking Opportunity Zone Map.

1. Within the Shared Parking Opportunity Zone, a percentage amount of the off-street parking required for a new development may be administratively reduced with an acceptable

parking plan. An acceptable shared parking plan must demonstrate through a parking demand study that sufficient parking can be met at various times throughout the day and will not impact the surrounding properties or adjacent neighborhoods. The parking demand study shall provide information and evidence about the anticipated parking demand at peak times during a day and the distance relationship between available shared parking spaces and the specific uses served.

2. Where shared parking is provided, a shared parking and cross access agreement between the cooperating property owners shall be approved by the Director of Community Development (Director) and recorded prior to the issuance of a building permit. This agreement must be recorded as a deed restriction on both properties and cannot be modified or revoked without the consent of the Director. If any requirements for shared parking are violated, the affected property owners must provide a remedy satisfactory to the Director.

### 3.2.5 Supplemental Parking Standards

1. Credit for required parking may be allowed for on-street parking spaces located within three hundred (300) feet of the subject use and may be allowed to be credited to meet up to twenty-five percent (25%) of the minimum required off-street parking spaces.
2. Reductions of the minimum off-street parking requirements by up to fifteen percent (15%) is permitted when the proposed development is sited within one-quarter (1/4) mile of a major transit facility such as a regionally designated "park and ride" site.
3. Parking areas should be separated from buildings by raised concrete walkways, landscaped strips, or both.
4. Parking aisle configurations should be designed to discourage speeding.
5. All developments served by on-site parking in

surface lots or parking structures shall provide either a sidewalk along the perimeter of the site or a designated pedestrian walkway through the parking lot, extending from the spaces furthest from the building served, to either a building entrance or to a sidewalk or walkway leading to the entrance.

6. The width of any sidewalk shall not be less than 6 feet, or 8 feet in areas with curb overhang adjacent to a parking lot.

### 3.2.6 Parking Structures

Off-street parking facilities in above-grade structures shall comply with the following standards:

1. Parking structures shall utilize designs and materials compatible with the architectural character of the buildings they serve.
2. Blank walls are strongly discouraged. Parking structures shall be visibly similar in character and scale to adjacent buildings.
3. Except on specific elevations facing the USPO Bulk Mail Facility, each side of the parking structure shall have architecturally articulated facades.
4. Buffering along the street edge with landscaped berms or planters is encouraged.
5. Vehicle entries to off-street parking structures shall be integrated into the placement and design of adjacent buildings. Parking structure facilities shall have user vehicle access from locations that minimize conflicts with pedestrian circulation.

### 3.2.7 Parking Management Plan

The Director of Community Development may consider additional reductions in required parking for office uses where the developer implements an enforceable lease obligation of tenants, or an employer demonstrates and documents a program of employee incentives for carpooling, use of mass transit, staggered working hours or other measures designated to reduce the use of vehicles for daily access to the site.

## Part 3.3 Landscape Design Standards

### 3.4.1 Introduction

Landscape design provides a significant contribution to the quality of any project and must be an integral part of the development process. More than adding trees and shrubs to a building site, proper landscape design helps define entryways, define parking areas, help screen service areas, and provides for the general health and comfort of pedestrians and other users of the site. In general landscape requirements are covered in the Commerce City Land Development Code - Article VII, Development and Design Standards, Division 5, Landscaping. In addition to the requirements defined by the City, there are standards that are also applicable to Victory Crossing.

1. The use of select landscape materials should be durable and designed to withstand a high level of pedestrian activity.
2. Landscape treatments should be designed to maximize visual impact while minimizing water consumption.
3. Landscape design should be used in conjunction with certain engineering principals associated with Low Impact Development.

### 3.3.2 Landscape Design for Low Water Use

Xeriscaping is encouraged in all landscaping applications. All landscape and irrigation plans shall be designed to incorporate water conservation materials and techniques. Xeriscape entails an appropriately designed environment that is efficient in terms of water use. Incorporating xeric concepts into the landscape is strongly encouraged without compromising or reducing the overall quality of the design.

The irrigation of landscape materials is an integral portion to the sustainability of the landscape. The irrigation plan should contain a design that incorporates Low Impact Development principles that provide for the adequate water requirements of proposed plant materials as well as a reduced impact on water resources. The plan should incorporate an increased amount of innovative, low volume techniques and a water management program. The proposed system design shall be based on water use needs of materials and categorized

into water use areas. An effort to conserve and minimize water usage should be a priority of the landscape irrigation plan and program.

1. Incorporate a planting scheme that provides for “zones” of water demand by grouping plant materials based on water usage.
2. Limit high-irrigation turf and plantings to high use areas such as those specifically intended for active recreational use.
3. Use xeric or drought tolerant plants and turf where practical, such as areas not intended for active uses.
4. Incorporate drought tolerant plant species that are suitable to the region and contain minimal water requirements.
5. The use of water conserving grasses, such as fescue sods, is encouraged. The use of bluegrass turf species shall be limited.
6. All planting areas shall be ameliorated and receive a minimum of 5 cubic yards of amendment per one thousand (1,000) square feet.
7. Provide an increased amount of mulch within all planting beds to increase moisture retention and reduce maintenance tasks.
8. Provide an advanced, low volume irrigation measuring and scheduling system.
9. An efficient, low volume system shall be employed that incorporates water conservation principles.
10. Reduced volume irrigation heads are encouraged in all areas with drip irrigation wherever possible.

### 3.4.3 Controlling and Tracking Water Use

1. The irrigation system and controllers should contain a weather based controller, rain and moisture sensors to prevent irrigation system operation when adequate moisture is present.
2. An irrigation water consumption tracking program shall be implemented to better utilize water in the future as plant materials grow and so that water requirements may be reduced due to establishment of materials. The irrigation water

usage should be carefully monitored so that the minimum amount of irrigation necessary for adequate plant material growth is applied.

## Part 3.4 Site Development Standards

### 3.4.1 Introduction

Unless otherwise noted, these Site Development Standards are applicable to the entire project area within Victory Crossing.

### 3.4.2 Lighting

Site illumination is a critical design factor in the development of Victory Crossing. Much of the activity generated within this project occurs during the evening hours when lighting is required. Because many games scheduled during the soccer season are evening events, the site will need to be appropriately illuminated to accommodate larger crowds moving from the stadium and field complex to the supporting restaurants, pubs and coffee shops planned for the project. Lighting for these events should be done in a manner that minimizes glare and other effects on neighboring properties and residential neighborhoods.

More than just providing for safety considerations, site lighting can also serve to embellish architectural features and create interest along the major roadways such as 60th Avenue and Valentia Street. This is particularly true in the Victory Plaza and Central Park Boulevard Districts. Uplighting coupled with other site lighting and illuminated pendent signs should be designed in a manner that doesn't create a glare or visual hazard to pedestrians or vehicles.

1. Street lighting fixtures located within the project area shall be dark bronze and set at a maximum height of 15 feet.
2. Parking lot lights shall not exceed a height of 30 feet. Fixtures shall exhibit cut-off capabilities to minimize off-site illumination spillage. Luminaries shall be metal halide or other type of white lighting. Over-lighting areas and high contrast between properties should be avoided.
3. Ornamental lights at sidewalks and pedestrian plazas shall include double pole-mounted, single pole-mounted and wall-mounted fixtures.

4. Night-time illumination of architectural features of a building or accent lighting with the use of decorative lights that are consistent with the architectural character of the center is encouraged.
5. Lighting for fields shall minimize light pollution and upward light emissions to the greatest extent possible.
6. Appropriate diffusers, lenses and/or glare shielding devices shall be included in all light fixtures.
7. Emergency and exit lighting exposed at any exterior elevation shall be consistent in design and placement with the building's architectural design. Dual-function light fixtures integrated into the exterior design are preferred over dedicated emergency fixtures. All emergency lighting shall be full cut-off and should not produce glare or light spillage to adjoining areas.

### 3.4.3 Visual Screening

Visual screening is often times required to hide certain aspects of a project such as trash storage, truck loading docks and utility connections. Any type of screening, particularly the use of walls and fences should only be considered when no other architectural solutions are available to the designer. Consideration should be first given to the placement of these facilities in the site planning process to minimize the visual effect they may present. Locating trash storage and loading docks away from public streets and walkways will minimize the use of walls and fences along these corridors.

1. Landscape or other materials shall be used to screen areas of low visual interest. These elements include, but are not limited to, trash receptacles, service areas, loading docks, blank walls, and utility boxes and pedestals.
2. Screening shall be provided on all sides, except where an opening is required for access. The access side shall permit access, yet screen the area when not required for access.
3. Required screening shall be in the form of landscaping, walls, fences, gates, berms, buildings or a combination of these techniques.

4. The screening around trash enclosures shall be a minimum of six (6) feet in height. Trash enclosure areas, freestanding dumpsters or compactors shall be fully-screened with walls of complementary materials of the adjacent buildings and provided with durable painted metal gates.

### 3.4.4 Fueling Area Canopies

1. The materials, colors and forms on the fueling area canopy will complement the architectural style of the principal structure on site.
2. Bright or florescent colors should not be used as the predominant canopy fascia color. Bright accent colors may be used as decorative trim or bands on the fascia as long as they do not exceed 30% of the canopy fascia. Additional fascia color bands or designs in excess of 30% are permitted but only if considered as logos, and counted toward the total allowable sign code area for the site.
3. Lighting fixtures on the underside of canopies shall be flush with, or recessed above the underside of the canopy surface.
4. A maximum of 25% per side of each canopy fascia area visible from any public street may be internally illuminated, and no portion of any fascia may be externally illuminated.

### 3.4.5 Temporary Uses/Organized Events

Temporary Uses and Organized Events are the terminology used for special activities, events, or structures that are beneficial to the public for limited periods of time, even though these activities would not traditionally comply with building, fire, zoning, or other local codes. Examples could include construction and office trailers, seasonal uses, temporary tents, carnivals, and large-scale special events. All Temporary Uses and Organized Events which occur within the Victory Crossing Development shall be regulated as follows:

1. Soccer District. Temporary Uses and Organized Use Events for the Soccer District shall be governed under the Permitting Matrix, which is a separate document to these design standards.

2. All other Districts. Temporary Uses and Organized Events shall follow the requirements and processes for the C-3 (Regional Commercial District) classification found within Articles III and V of the LDC.

**Process:** When required, a permit shall be obtained for each temporary use or organized event, in accordance with the requirements of the LDC and building code. Unless otherwise specified by the director, no temporary uses/organized event shall be reviewed or approved by the DRT.

**Exception:** City sponsored events, either in whole or in part, shall be exempt from these requirements.

### 3.4.6 Accessory Structures

All Accessory Structures which are placed within the Victory Crossing Development shall be regulated as follows:

1. Soccer District: Accessory Structures shall follow the requirements and processes contained within the Permitting Matrix. In addition to these requirements, the architectural design of accessory structures shall be similar in character to the principle building on-site. This includes all vendor kiosks, ticket booths, and any other permanent structure. Character similarities include building design, color, and material selection. Accessory structures are not required to mimic the principal structure in appearance.
2. All Other Districts: Accessory Structures shall follow the requirements and processes contained within Article V of the LDC. In addition to these requirements, the architectural design of accessory structures shall be similar in character to the principle building on-site. This includes all vendor kiosks, ticket booths, and any other permanent structure. Character similarities include building design, color, and material selection. Accessory structures are not required to mimic the principal structure in appearance.

**Process:** When required, a building permit shall be obtained for each accessory structure, in accordance with the requirements of the LDC and building code. Unless otherwise specified by the director, no accessory structure building permit shall be reviewed by the DRT.





# IMPLEMENTATION PROCESS

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## Part 4.1 Project Review Process

### 4.1.1 Introduction

The City shall administratively review all development applications within the Victory Crossing project for compliance with the Land Development Code (LDC), these design standards, the PUD Zone Document, and applicable subdivision criteria. This review process includes but is not limited to the evaluation of proposed land uses, building heights, setbacks, street, utility and curb cuts standards, traffic and drainage standards, parking provisions, landscape and sign standards, and architectural compatibility.

### Development Review Team (DRT)

The Development Review Team (DRT) assists in the review of development and land-use applications. The organization, qualifications, and procedures are contained within Article II of the LDC. For the purposes of development contained within Victory Crossing, the following addition for the DRT is included below:

### Organization and Qualifications.

The DRT is composed of city staff and representatives of outside agencies that have an interest in or would be affected by a proposed site development or land use activity. The Director shall maintain a list of current members and may revise the list at his discretion. Copies of the list are available for inspection in the office of the Director. For all development applications contained within the Victory Crossing area, DRT shall also include a representative from the stadium owner/master developer and may also include a design professional, when appropriate. With the exception of municipal related uses located within the Civic Center, no requests for development permits shall be considered without the review, comment and recommendation by the stadium owner or master developer provided through the DRT process.

Decisions and recommendations made by the DRT will be determined by consensus of the entire group. In the event that consensus cannot be reached and all measures have been taken to achieve an agreement, the matter will be referred to the City Council by a memorandum prepared by



# IMPLEMENTATION PROCESS

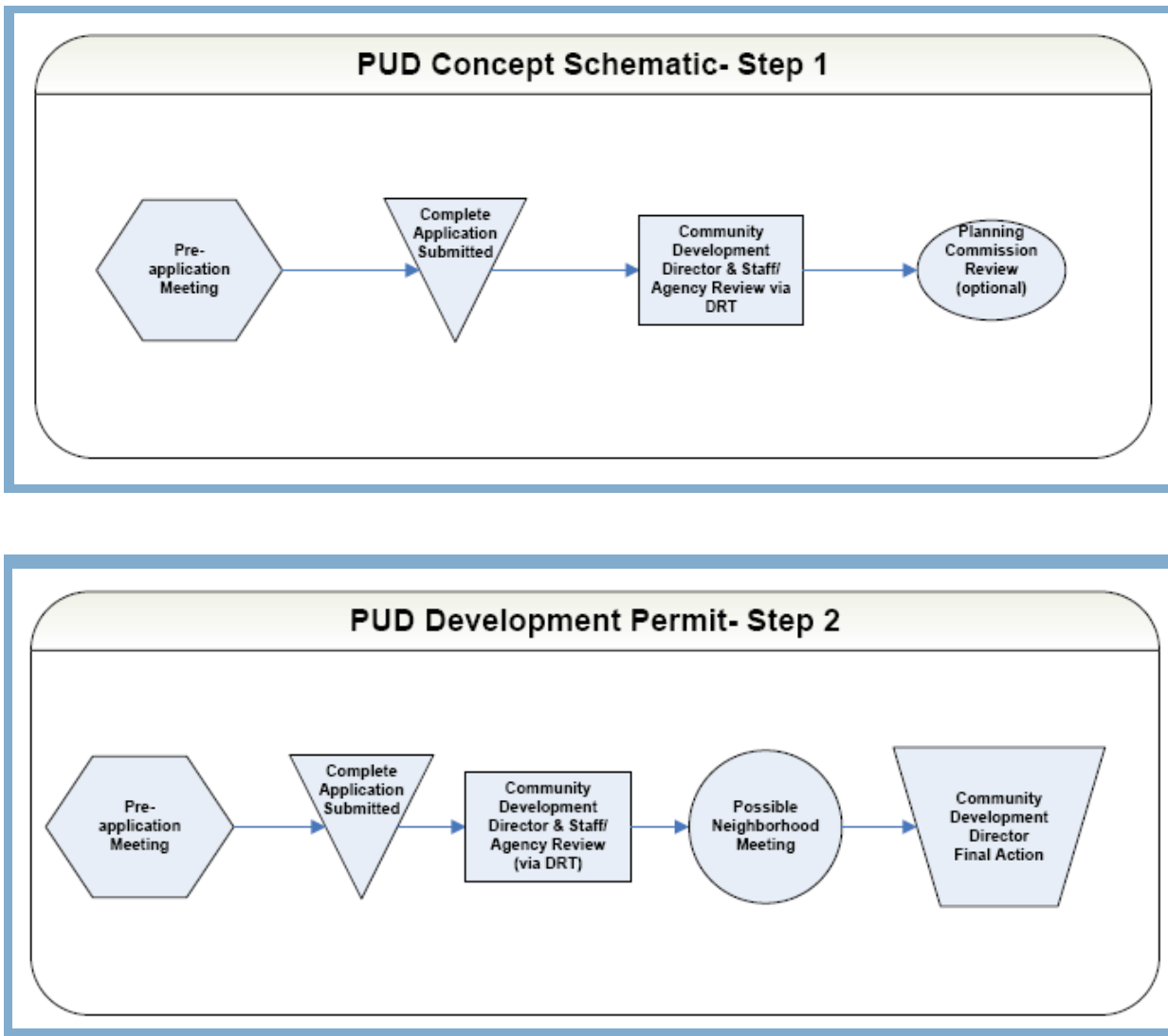
the Director of Community Development. The City Council will make a final determination by a simple majority vote.

#### 4.1.2 PUD Development Permits

The design review process described within these standards is part of the PUD Development Permit procedure found in Article III of the Commerce City Land Development Code. Because of the high profile, strategic location, and important partnership found within the Victory Crossing, all PUD Development Permits submitted to the city shall follow the following process.

#### Process Chart of PUD Development Permit Review

Figure 4.1



## Part 1: Concept Schematic Plan:

1. **Description.** A concept schematic plan for the Victory Crossing is a generalized land use/site plan for an area proposed to be developed within the Victory Crossing property. It is a required step that allows early, informal evaluation of a proposed development before substantial expenses have been incurred. A concept plan provides city staff, the master developer, and the applicant an opportunity to determine the development's conformance with the comprehensive plan, the requirements of the Land Development Code, and these Design Standards, and assists the applicant in the preparation of a final PUD Development Permit.
2. **Review.** The Director and the DRT review concept plans. The Director may provide comment letters to applicants based upon such reviews. In addition, if requested by the Director, the Planning Commission may review the concept schematic and provide further comment. Any comments contained in the letter are informational only and shall not represent a commitment on behalf of the City regarding the acceptability of the plan.

## Part 2: PUD Development Permit

3. **Description.** A PUD Development Permit represents the last stage of review required prior to the issuance of building permits or other permits for improvements or land uses within the Victory Crossing PUD Zone District. The construction, modification, or alteration of any building or structure and all specific land uses within the Victory Crossing PUD shall be governed by the PUD Development Permit. A PUD Development Permit is only issued when all details of the proposed PUD development (or a portion of that development) have been finalized and have been reviewed for consistency with the terms of the approved PUD zone document, these Design Standards, and all design and development standards in the Land Development Code that

have not been waived or modified by the terms of the approved PUD zone document. No PUD Development Permit for any property included within Victory Crossing shall be accepted by the city without having a previously reviewed and submitted Concept Schematic in place for the same, relevant property.

4. **Review.** The Director and DRT review applications for PUD Development Permits. The Director is authorized to approve, approve with conditions, or deny the application based on the approval criteria below.
5. **City Council Review.** The Director may refer to the Planning Commission for review and to City Council for a decision any PUD Permit application to which the Director is assigned responsibility as the decision maker under the terms of the LDC. In such event, written notice of the referral shall be sent to the applicant and the matter shall be set for Planning Commission and City Council consideration as soon as practical, but in no event shall the city be required to hold a special meeting nor deviate from standard policies for placing matters upon the Planning Commission's and City Council's agendas. The City Council's decision shall be based upon the relevant approval criteria below.
6. **Approval Criteria.** An application for a PUD Development Permit may be approved if it:
  - (a) Complies with City standards;
  - (b) Is consistent with and meets the intent and applicability and specific district standards of the Victory Crossing Design Standards;
  - (c) Is consistent with any previously approved PUD Zone Document, rezoning concept plan, or other plans or land use approvals;
  - (d) Provides adequate mitigation for any significant adverse impacts resulting from the use;
  - (e) Creates a positive precedent for the future cumulative development of the immediate area;

- (f) Provides utilities, drainage, and other necessary facilities in accordance with the final PUD permit; and
- (g) If the proposed PUD permit includes mixed-use areas, then:
  - (i) The elements of the PUD Development Plan, such as streets, structures, parking areas, pedestrian walkways, courtyards, plazas, landscaping, service areas, open spaces, bicycle movement provisions, screening, lighting, and maintenance and storage facilities are arranged and designed to further the purpose and intent of section 21-4370 (PUD Zone District);
  - (ii) Design and location of ingress and egress minimize traffic congestion on public and private streets.

(7) **Lapse.** A PUD Development Permit shall lapse and be of no further force and effect if a building permit application for the development, or for a phase of the development identified in the approved PUD Development Permit, has not been submitted within the time-frame set forth in the PUD Development Permit or, if no time-frame was specified, within two years of the date of approval.

(8) **Control of PUDs Following Construction.** After a PUD has been approved, the use of the land and the construction, modification, or alteration of any building or structures within the PUD shall be governed by the approved PUD Development Permit. Modification to an approved PUD development permit, other than minor modifications allowed pursuant to Article III of the Land Development Code, requires the submission of a new PUD application.

(9) **Appeals.** Any appeals for any PUD development permit application shall follow the processes established in Article III of the LDC.

#### 4.1.3 All other Land Development Applications.

All other land development applications which may be applicable to the Victory Crossing property, including, but not limited to variances, conditional use permits, subdivisions, vacations, etc. shall follow their applicable process contained in Article III of the Land Development Code. The Development Review Team shall include a representative from the stadium/master developer and may also include a design professional, when appropriate. No concept plan process is required for these types of applications.

#### 4.1.4 Amendments to the Victory Crossing Design Standards

The standards contained herein will need to provide both flexibility as well as continuity over time. As such, amendments made to the Victory Crossing Design Standards that do not directly change any specific development requirements of the Prairie Gateway PUD Zone Document, may be considered without being deemed a formal amendment to the zone document itself. This determination will be made by the Director of Community Development based upon the review criteria stated below:

**Approval Criteria** - The Director may approve an amendment to these design standards if the proposed amendment:

- (a) Is consistent with the overall intent of the design standards, comprehensive plan, and PUD Zone Document;
- (b) Is consistent with the purposes set forth in Section 1 of these Design Standards;
- (c) Is necessary or desirable because of changing social values, new planning concepts, or other social or economic conditions;
- (d) Will not have a negative effect on the immediate area;
- (e) Will not have a negative effect on the future development of the area; and
- (f) Will promote the public health, safety, and general welfare of the people of Commerce City.



# DEFINITIONS

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**Accessory Structure:** A detached structure located on the same parcel with the main building, the use of which is in conjunction with the primary structure.

**Build-to Line:** A line at which a building is to be placed. Different from a set-back line, a built-to line may vary along a property line leading to a undulating plane of building façades along a Pedestrian Zone or public street.

**Commercial Flex:** A building providing its occupants the flexibility of utilizing the space. Usually provides a configuration allowing a flexible amount of office, retail or showroom space in combination with light assembly, laboratory, research and development, warehouse distribution, etc. Generally these buildings are constructed with load-bearing floors, loading dock facilities, overhead doors and high ceilings. The front architectural elevations of most flex structures are developed to resemble a typical office building with little or no common areas.

**Concrete Masonry Unit (CMU):** A concrete block. Ground Face CMU is a block whose exterior surface has a smooth texture with some aggregate exposed, while a Split Face CMU has an exterior face that has a rough, stone-like texture.

**Dick's Sporting Goods Park:** An 18,000-seat FIFA sanctioned soccer stadium and home of the Colorado Rapids. Built and operated by Kroenke Sports Enterprises, the overall project cost was \$131 million which included construction of the stadium, a surrounding 24-field complex, along with infrastructure improvements leading to the complex.

**Director:** Director of Community Development - Commerce City, Colorado.

**EIFS:** Exterior Insulating Finish System - commonly known as "synthetic stucco". Typically composed of a waterproofed polystyrene board attached to a wall and finished with a fiberglass mesh and resin coated surface. While the surface resembles stucco, such a system is susceptible to impact damage, leakage and fading of the color coating.

## DEFINITIONS

**Encouraged:** Compliance with a particular standard is recommended but not required. This term is applied to criteria believed to enhance the quality of individual projects within Victory Crossing, but not a specific requirement to the successful execution of the project. Failing to meet such “encouraged” criteria shall not be grounds for denying design approval. Similarly, the term “discouraged” is meant as a recommendation that a specific design element or application not be used in a particular manner. While both terms “encouraged” and “discouraged” are considered the basis for certain recommendations in these standards – the term “discouraged” will be applied vigorously and may be considered important in final design approval.

**Four-sided or 360 Degree Architecture:** Architectural treatments, as defined by these standards and applied to each elevation or side of a proposed structure.

**Franchise Architecture:** Refers to a type of architectural style found mostly in fast food and casual dining restaurants as well as convenience retail and service stations. This architectural design generally is specified by a company or franchisor oftentimes with very little compatibility to the surrounding development.

**Land Development Code:** City of Commerce City Land Development Code, 2009 (as amended).

**Pedestrian Zone:** An area located between the street curb and the build-to line used for a variety of pedestrian related activities including walkways, plazas, restaurant, pub and café seating. The Pedestrian Zone may be located on public right-of-way (with permission by the City), as well as on private property. Within Victory Crossing, Pedestrian Zone designations are found along 60th Avenue and Valentia Street.

**Planned Unit Development (PUD) Zone Document:** Referencing the “Prairie Gateway Planned Unit Development (PUD) Zone Document” as it was approved by Commerce City on June 6th, 2005. The list of allowed land uses and certain development criteria for Victory Crossing (formerly known as Prairie Gateway) is contained within this document.

**Plaza:** An open area at ground level accessible to the public and unobstructed from its lowest level to the sky. Any portion of a plaza occupied by landscaping, statuary, pools and open recreational facilities shall be considered part of the plaza. The term plaza shall not mean off-street loading areas, parking areas or driveways.

**Traffic Calming:** A technique applied to the design of public roadways meant to slow traffic within areas of higher pedestrian activity.

**USPO Bulk Mail Center:** United States Post Office Bulk Mail Center – located along 56th Avenue and surrounded on three sides by Victory Crossing. This development is not located on the property or within Commerce City.

**Wayfinding:** A system that assists motorists and pedestrians to destinations within and around a project. As a systematic approach, wayfinding techniques may include signs, maps, gateways, banners and informational kiosks.

**Xeric or Xeriscape:** A landscape design concept based on horticultural principals meant to reduce water usage and maintenance in the landscape. Xeriscape is not meant to reduce water needs to a minimum resulting in the stress of the planting materials, nor is it meant to eliminate irrigation.

